



City Council Regular Meeting

Monday, November 4, 2024
6:00 p.m.

Chandler City Council Chambers
88 E. Chicago St., Chandler, AZ



Our Vision

We are a world class City that provides an exceptional quality of life.

Our Brand

A safe, diverse, equitable and inclusive community that connects people, chooses innovation and inspires excellence.

Innovative Focus

Innovation is the lifeblood of our community. The introduction of new ideas and methods is rooted in Chandler's culture and heritage. This thread of innovation embodies how we connect, plan and serve our city to be a contemporary, financially responsible and safe place to live and work.



Back row
Vice Mayor OD Harris, Mayor Kevin Hartke, Councilmember Mark Stewart

Front row
Councilmember Jane Poston, Councilmember Angel Encinas,
Councilmember Matt Orlando, Councilmember Christine Ellis

Pursuant to Resolution No. 4464 of the City of Chandler and to A.R.S. 38-431.02, notice is hereby given to the members of the Chandler City Council and to the general public that the Chandler City Council will hold a **REGULAR MEETING** open to the public on Monday, November 4, 2024, at 6:00 p.m., in the Chandler City Council Chambers, 88 E. Chicago Street, Chandler, Arizona. One or more members of the Chandler City Council may attend this meeting by telephone.

Persons with disabilities may request a reasonable modification or communication aids and services by contacting the City Clerk's office at 480-782-2181 (711 via AZRS). Please make requests in advance as it affords the City time to accommodate the request.

Agendas are available in the Office of the City Clerk, 175 S. Arizona Avenue.



Regular Meeting Agenda

City Council Strategic Framework Focus Areas: Legend



Community Safety



Neighborhoods



Quality of Life



Sustainability and Technology



Connectivity



Economic Vitality

Call to Order

Roll Call

Invocation- Ms. Parvin Fallas, Baha'i Faith

Pledge of Allegiance

Scheduled Public Appearances

1. Service Recognitions
2. Recognition: American Public Works Association Accreditation

Consent Agenda

Items listed on the Consent Agenda may be enacted by one motion and one vote. If a discussion is required by members of the governing body, the item will be removed from the Consent Agenda for discussion and determination will be made if the item will be considered separately.

Proposed Motion: Move to approve the Consent Agenda of the November 4, 2024, Regular Meeting, Items 1 - 3.



City Clerk

1. Approval of Minutes

Move City Council approve the Council meeting minutes of the Special Meeting Budget Kickoff of October 14, 2024, the Regular Meeting of October 14, 2024, the Study Session of October 14, 2024, and the Regular Meeting of October 17, 2024.



Development Services

2. Final Adoption of Ordinance No. 5110 Rezoning, PLH24-0001, Toll Brothers Uptown, Located Approximately 1/4 Mile North of the Northeast Corner of Arizona Avenue and Warner Road


Move City Council adopt Ordinance No. 5110 approving PLH24-0001 Toll Brothers Uptown, Rezoning from Regional Commercial (C-3) to Planned Area Development (PAD) for multi-family residential, subject to the conditions as recommended by Planning and Zoning Commission.

Council Focus Area(s):  

3. **Final Adoption of Ordinance No. 5112, Rezoning, PLH24-0023 The District Downtown, Located at the Northwest Corner Arizona Avenue and the Loop 202 SanTan Freeway**

Rezoning

Move City Council adopt Ordinance No. 5112 approving PLH24-0023 The District Downtown, Rezoning from PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) to PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet, subject to the conditions as recommended by Planning and Zoning Commission.

Council Focus Area(s): 

Adjourn



City Council Memorandum City Clerk's Office Memo No. N/A

Date: November 04, 2024
To: Mayor and Council
From: Dana DeLong, City Clerk
Subject: Approval of Minutes

Proposed Motion:

Move City Council approve the Council meeting minutes of the Special Meeting Budget Kickoff of October 14, 2024, the Regular Meeting of October 14, 2024, the Study Session of October 14, 2024, and the Regular Meeting of October 17, 2024.

Attachments

Minutes of the Special Meeting - Budget Kickoff held on October 14, 2024
Minutes of the Regular Meeting held on October 14, 2024
Minutes of the Study Session held on October 14, 2024
Minutes of the Regular Meeting held on October 17, 2024

Meeting Minutes

City Council Special Meeting

October 14, 2024 | 4:00 p.m.
Chandler City Council Chambers
88 E. Chicago St., Chandler, AZ



Call to Order

The meeting was called to order by Mayor Kevin Hartke at 4:00 p.m.

Roll Call

Council Attendance

Mayor Kevin Hartke
Vice Mayor OD Harris
Councilmember Angel Encinas
Councilmember Christine Ellis
Councilmember Mark Stewart
Councilmember Matt Orlando
Councilmember Jane Poston

Appointee Attendance

Joshua Wright, City Manager
Kelly Schwab, City Attorney
Dana DeLong, City Clerk

Staff in Attendance

Tadd Wille, Assistant City Manager
Andy Bass, Deputy City Manager
Matt Dunbar, Budget & Policy Director
Matt Burdick, Communications & Public Affairs Director
Alexis Apodaca, Mayor & Council Public Affairs Senior Manager
Ryan Peters, Strategic Initiatives Director

Discussion

1. FY 2025-26 Budget Kickoff Work Session
 1. FY 2024-25 Budget Actions
 2. Budget Process
 3. Strategic Framework Focus Areas
 4. FY 2025-26 Budgetary Impacts
 5. Capital Guidelines

6. Financial Policies and Budget Practices
7. Key Dates

MAYOR HARTKE called for a presentation.

JOSHUA WRIGHT, City Manager, introduced the discussion item.

MATT DUNBAR, Budget & Policy Director, presented the following presentation.

- FY 2025-26 Budget Kickoff with Mayor and Council
- Agenda
 - I. FY 2024-25 Budget Actions
 - II. Budget Process
 - III. Strategic Framework Focus Areas
 - IV. FY 2025-26 Budgetary Impacts
 - V. Capital Guidelines
 - VI. Financial Policies and Budget Practices
 - VII. Key Dates
- FY 2024-25 Budget Actions
 - Sales tax rates unchanged - one of the lowest in AZ
 - Reduced property tax rate from \$1.0926 to \$1.0826 per \$100 of assessed value – 9th year of reduction
 - No changes to Water/Wastewater/Reclaimed & Solid Waste Rates
 - Added increases to capital budgets to compensate for inflation affected projects, maintain aging infrastructure, as well as finishing planned parks and arterial streets
 - Continued to address pension debt through additional one-time funding to maintain the paid off status off the Public Safety Personnel Retirement System (PSPRS) unfunded liability (+7M / \$205.3M to date)
 - Added funding for new two-year labor association commitments and general employee pay increases
 - Added 48 positions, 34 of which (70%) were converted from one-time funded to ongoing funding or have an offset from grants or other revenue and 5 were to fill the needs of new capital projects
 - Budget Stabilization Reserve continues at \$10M

MAYOR HARTKE asked if the seven million set aside for PSPRS is an estimate.

MR. DUNBAR said yes, it is a placeholder value for now to maintain Chandler's current funded status.

COUNCILMEMBER STEWART asked if the 34 positions converted from one-time funded to ongoing funded were majority public safety positions.

MR. DUNBAR said a majority were public safety positions.

MAYOR HARTKE noted that this would be the first year seeing impacts from the rental tax going away.

MR. DUNBAR said yes, next year the full impact will be understood.

MR. DUNBAR continued the presentation.

- Where We Are in the Budget Process
 - Kickoff allows for Council direction on Strategic Focus Areas, policies & guidelines before budget process begins & Resident Budget Survey initiated
 - Decision Package and CIP recommendations submitted by departments in December
 - Financial forecast, funding decisions brought to Council during workshops 1 (February), 2 (March) and 3 on the CIP (March)
 - Proposed budget brought to Council for discussion at all-day briefing (May 2nd)
- FY 2025-26 Budget Theme “Strength in Numbers”
- Strategic Framework Guides Our Decision Making
 - Council Retreat Coming Spring 2025
 - Focus Areas
 - Economic Vitality
 - Sustainability and Technology
 - Connectivity
 - Neighborhoods
 - Quality of Life
 - Community Safety
 - All CIP and Decision Package adds are tied to their related Focus Area
- Fiscal Year (FY) 2025-26 Budgetary Impacts - The Brightside
 - Personnel/Staffing
 - PSPRS unfunded liability continues its paid off status, which allows for freed up ongoing funding (biggest impact will be this upcoming FY)
 - Making progress on the number of vacancies
 - Added positions (54% Police/Fire) to reduce pressure in high need areas and shift one-time funded positions to ongoing
 - Local Economic Impacts
 - Local revenues in line with budget
 - Industrial construction impacts are still being seen in our monthly revenues
 - New retail and entertainment concepts continue to open that bring destination-based experiences to Chandler
 - State shared income tax will level downward to new normal
 - Long tradition of strong financial management
 - AAA upgrade on ETRO Bonds and maintained on GO Bonds

MAYOR HARTKE asked if the number of positions filling vacancies in public safety roles included over hires.

MR. DUNBAR said this does not include over hires, only FTEs which includes any FTEs added last fiscal year. There is a total of eight vacancies and 10 over hires.

COUNCILMEMBER ORLANDO commented that the goal is then to fill 18 remaining public safety vacancies.

MR. DUNBAR said the early hires are not considered in the vacancy amount. Early hires wait for retirements to occur to then fill a FTE spot.

MAYOR HARTKE asked if there are 23 vacancies in total.

MR. WRIGHT said that is correct, there are 15 vacancies including early hires with 8 current vacancies.

MR. DUNBAR continued the presentation.

- Fiscal Year (FY) 2025-26 Budgetary Impacts - The Challenges
 - Economy & Inflation
 - Costs, driven higher by prior year inflation, remain high; concern of recession continues
 - Bond Election needed for additional authorization in four categories
 - Technology
 - Multi-year citywide Enterprise Resource Planning (ERP) system replacement continues
 - Cybersecurity posture / Infrastructure
 - Governance / speed to delivery
 - Mobility and flexibility / more to do
 - Personnel/Staffing
 - Retirement and vacancy impacts will continue to be an issue
 - ERP system will require significant citywide staff effort
 - Still some reliance on temp labor for critical services
 - Legislative Impacts
 - Legislative Impacts to local tax base and Model City Tax Code continue to be watched (e.g., grocery tax)
 - Full effect of Residential Rental tax removal bill will impact new FY (est. impact \$11M ongoing or about 6% of TPT revenue)
 - Prop 479 in voters' hands in November

MAYOR HARTKE added Proposition 479 affects city transportation projects as well.

MR. DUNBAR agreed, the outcome will have an impact of Chandler's CIP.

MR. DUNBAR continued the presentation.

- Fiscal Year (FY) 2025-26 The Challenges Budgetary Impacts Delineating Ongoing vs. One-Time
 - This year 82% ongoing GF TPT revenues and 18% one-time was budgeted
- Fiscal Year (FY) 2025-26 The Challenges Budgetary Impacts Delineating Ongoing vs. One-Time
 - Evaluation is done for each TPT category and their ongoing versus one-time components

VICE MAYOR HARRIS asked about distribution deadline dates for ARPA funds.

MR. DUNBAR said ARPA funds must be spent or encumbered by December 2024 and spent by December of 2025. Chandler is in line with these deadlines.

VICE MAYOR HARRIS requested information on where the APRA funds are spent or encumbered.

MAYOR HARTKE asked about any funds that may be encumbered but not spent by the deadline.

MR. DUNBAR said some allocations can be transferred between funding sources.

COUNCILMEMBER ORLANDO asked when the flat tax levy impacts will be studied.

MR. DUNBAR answered that the flat tax will be studied in FY 25-26. Currently there is a onetime excess above the normal rate, that later will be ongoing a constant amount.

COUNCILMEMBER ORLANDO asked for more information later.

COUNCILMEMBER ORLANDO asked if the TPT increase is onetime.

MR. DUNBAR said the increase for utilities is ongoing.

MAYOR HARTKE asked when it is expected for heightened construction costs trail off.

MR. DUNBAR said December 2025 is expected to be the end of construction impact from Intel.

MR. DUNBAR continued the presentation.

- Council Priorities for One-Time Dollars
 - 1. Maintain reserves sufficient to meet financial policies
 - 2. Reinvest in existing aging infrastructure, systems, and projects that generate ongoing savings
 - 3. Focus operating & capital spending to move forward strategic focus area action items
 - 4. New initiatives and capital, including sustainability

COUNCILMEMBER STEWART asked if the reserves sufficient to meet financial policies is an amount that carries forward each year that fluctuates and asked if the funds come out of the budget each year.

MR. DUNBAR said it is a percentage of the total budget or percentage of total revenues over a four-year period which may adjust based on revenue incomes. The amount required to sustain the reserve may fluctuate year-to-year.

COUNCILMEMBER STEWART asked if the revenue funds are set aside.

MR. DUNBAR said it is in the general fund balance that is invested.

COUNCILMEMBER STEWART asked if the reserves are ever used or added to.

MR. DUNBAR explained that the reserves are there to be used to maintain the city in case of emergency. It does not need to be funded because it already exists.

COUNCILMEMBER ELLIS said the categories are agreeable and commented that Chandler is always looking for innovative ways to move forward. Councilmember Ellis suggested that priorities three and four could be switched.

MAYOR HARTKE said priority three and four do work together. Projects may meet more than one priority at a time.

MR. DUNBAR explained that priority three focuses on onetime funding that meets Council focus areas. Priority three focuses on existing projects, but four includes new projects that may not have been part of the capital plan. The priorities help gauge viability of budget projects when they meet these Council priorities.

COUNCILMEMBER ORLANDO asked if there are any high yield bonds that could be bought down.

MR. DUNBAR said staff meets with bond representatives frequently. Chandler typically sells bonds every two years. There are some bond sales that could increase returns. The best opportunity may be in March, Chandler typically sells in November. Depending on need and market opportunity, the sale may happen in March or November.

COUNCILMEMBER ORLANDO asked if Chandler is positioned to pay back its debt.

MR. DUNBAR said yes. An option could be to pay down debt early instead of refinancing.

COUNCILMEMBER ELLIS asked about potential downsides to selling bonds early.

MR. DUNBAR explained there is a lot of effort into selling bonds, and changing interest rates could impact any cost savings. There is a cost in selling bonds, there is some risk in the correct timing of a sale. As of now, a spring sale may be beneficial.

Council consensus agreed with the existing Council Priorities for one-time dollars.

MR. DUNBAR continued the presentation.

- Capital Guidelines
 - Minimize increase in secondary property taxes
 - Re-imagine resident amenities scheduled for replacement
 - Prioritize aging infrastructure
 - Finish planned construction of streets, parks, fiber and utility systems
 - Prior to adding capital, ensure related ongoing O&M can be supported
 - Utilize master plans to guide long-term capital investment

- Ensure sufficient bond authorization exists to complete projects desired by residents
- Balance timely completion and coordination of capital projects with impacts to neighborhoods and businesses

MAYOR HARTKE said this is the guiding philosophy for capital.

COUNCILMEMBER STEWART asked what kind of projects Chandler has been able to do with onetime funds.

MR. DUNBAR said many general fund onetime dollars fund projects, for example the Forensic Facility. Cost increases can be covered by onetime funds to complete these projects, instead of seeking bond sales to fund them. Onetime extra revenues above budget become onetime revenues which are used for a significant portion of capital projects.

COUNCILMEMBER STEWART said onetime items may get working into the budget for an ongoing basis.

MR. DUNBAR said onetime items do not always get rolled in to ongoing in the same fiscal year, but they are planned for in upcoming fiscal years. Onetime expenditures from general fund from capital are including in the CIP to plan out for future year allocations.

COUNCILMEMBER STEWART discussed the point of balancing timely completion and coordination of capital projects and mentioned the influx of road projects. Councilmember Stewart asked if this was due to increased use of federal funds.

MR. WRIGHT said there were many factors including federal spending and investing in the community.

MAYOR HARTKE added that some projects are private, but the city does receive some benefits.

MR. DUNBAR continued the presentation.

- Resident Budget Survey
 - Maintain similar Budget Survey process to generate community involvement (1,310 for FY 2023-24 | 1,768 for FY 2024-25)
 - Run from Nov. – Jan.
 - Run ad campaign to encourage participation
 - Offer in English, Spanish, and Mandarin
 - 15 questions, 1-2 in each focus area with comment box
 - Additional updates to questions or process?

COUNCILMEMBER ELLIS asked if there will be paper copies available.

MR. DUNBAR said paper copies of the survey would be available at reception desks of many city facilities.

COUNCILMEMBER ORLANDO noted that he would prefer to hear from residents what is important to them.

MR. DUNBAR said the survey is used as a guidepost to set up the new budget. If there are items that the community deems important, they are considered.

COUNCILMEMBER ORLANDO asked to focus questions on what the community deems important and ask specific targeted questions to determine what is important to residents.

MAYOR HARTKE added that there was a study done specifically on parks that rated priorities. Mayor Hartke asked to consider previous input from residents and incorporate current needs.

MR. DUNBAR explained that the budget survey is one piece of resident feedback included in the budget process. One of the existing CIP guidelines is to use existing surveys and feedback in planning. Residents could rank project priorities that may already be in capital planning.

COUNCILMEMBER ORLANDO reviewed the resident budget survey and inquired how to make the survey more specific.

COUNCILMEMBER ELLIS said the survey asks demographic questions and every area has their own needs. The resident budget survey is applicable to all residents to guide the city's needs in the future.

COUNCILMEMBER ORLANDO said the yes or no questions do not help prioritize items the budget.

MAYOR HARTKE requested realigning fill in the blank questions, while accepting answers if residents are satisfied overall.

COUNCILMEMBER STEWART said that there are two different concepts in the resident budget survey – asking what residents think of the city and what are the budget priorities. It is possible to include both types of questions with some rearranging.

MR. WRIGHT said a draft will be returned before the survey is distributed.

MAYOR HARTKE looks forward to hearing responses from residents. Mayor Hartke asked about using the ILA devices or hosting the budget survey at different cultural events to complete the survey to hear responses in other languages.

MR. DUNBAR said the software used has a variety of languages it supports, the challenge is ensuring content is translated correctly.

MR. DUNBAR continued the presentation.

- Financial Policies
 - Operating Management*
 - Capital Management

- Reserves (updated Apr. 27, 2023)
- Debt Management*
- Long-Range Financial Planning
- Grant Management*
- Investment*
- Accounting, Auditing, and Financial Reporting*
- Pension Funding*
- Will review and bring to Council any recommended updates along with the required update for the pension policy
 - * Updated February 2024
- Council Approved Budgeting Practices Support Financial Sustainability
 - Maintain AAA bond ratings from Moody's, Fitch and S&P
 - Continue adherence to all fiscal policies Remain Structurally balanced
 - Remain Structurally balanced
 - Ongoing revenues support ongoing expenditures
 - One-time revenues support one-time expenditures
 - Maintain strong reserves
 - 15% General Fund contingency reserve
 - Budget Stabilization reserve
 - Balanced expenditure growth
 - Focus on maintaining and/or modernizing existing services
 - Control Primary Property Tax
 - Provide options should the forecast allow for reduction

VICE MAYOR HARRIS asked about the PSPRS paydown and what funds are being released in this budget and any economic impact from large private projects.

MR. DUNBAR said information will be provided at the first budget workshop.

MR. DUNBAR continued the presentation.

- FY 2025-26 Budget Process Updates
 - Continued Modified Zero Based Budget (MZBB)
 - Helps ensure current budgets are right-sized based on services provided
 - Analysis includes review of services, related expenditure category spending trends, and line-item reviews
 - Started with 2 departments/divisions in first year, 3 more in current year
 - Results to be reported at All Day Budget Briefing
- Feedback Received Last Budget Process Additional Thoughts?
 - Council Process changes implemented in FY 2024-25 Budget
 - List department accomplishments without making them the focus of the presentation
 - List all capital projects and decision packages, but focus presentations on the highlights
 - Additional discussion of timing of projects where policy direction may be needed
 - Provide Council a "Review Guide" to help with budget book review process
 - Hold All-Day Budget Briefing on non-Council meeting week

- New process changes for FY 2025-26?

COUNCILMEMBER POSTON said it is helpful to get a deep dive from departments in presentation format.

MAYOR HARTKE asked for clarification.

COUNCILMEMBER POSTON said she would like to see directors exploring the department priorities in depth.

MAYOR HARTKE asked how directors could best present department priorities at the budget briefing. COUNCILMEMBER ELLIS asked for department directors to present more than just bullet points and seeing perspectives of what will happen if the project is done or not done, the needs of the departments - what value is added to the community by these projects.

COUNCILMEMBER ORLANDO asked about receiving the presentations earlier to have time to review.

MR. DUNBAR said the department slides are easy to provide ahead of time.

MAYOR HARTKE commented that spending more time with the Finance team to talk through any individual questions before the all-day budget briefing was helpful to have questions answered in advance.

VICE MAYOR HARRIS added that the previews with the Finance team was helpful.

COUNCILMEMBER ENCINAS asked to clarify if there is a request for presentations on additional discussion as to what the department is planning that year.

COUNCILMEMBER ELLIS explained that they would like to see more future-focused presentation.

COUNCILMEMBER ENCINAS said an example would be to see goals and what resources are needed to accomplish the goals, and what the benefit to the community would be.

COUNCILMEMBER STEWART asked if the budget is expected to be lower this year.

MR. WRIGHT said the budget process is just starting, it may be too early to determine due to many different factors.

MR. DUNBAR continued the presentation.

- Tentative Key Budget Dates
 - Public comment opportunities are now a part of all these budget meetings

COUNCILMEMBER POSTON asked about televising the all day budget briefing.

MR. WRIGHT said it is up to the Council.

MAYOR HARTKE said that the city can try this next year.

- Questions?

Public Comment

None.

Adjourn

The meeting was adjourned at 5:14 pm.

ATTEST: _____
City Clerk

Mayor

Approval Date of Minutes: November 4, 2024

Certification

I hereby certify that the foregoing minutes are a true and correct copy of the minutes of the Special Meeting of the City Council of Chandler, Arizona, held on the 14th day of October 2024. I further certify that the meeting was duly called and held and that a quorum was present.

DATED this _____ day of November, 2024.

City Clerk

Meeting Minutes

City Council Regular Meeting

October 14, 2024 | 6:00 p.m.
Chandler City Council Chambers
88 E. Chicago St., Chandler, AZ



Call to Order

The meeting was called to order by Mayor Kevin Hartke at 6:00 p.m.

Roll Call

Council Attendance

Mayor Kevin Hartke
Vice Mayor OD Harris
Councilmember Angel Encinas
Councilmember Christine Ellis
Councilmember Mark Stewart
Councilmember Matt Orlando
Councilmember Jane Poston

Appointee Attendance

Joshua Wright, City Manager
Kelly Schwab, City Attorney
Dana DeLong, City Clerk

Invocation

The invocation was given by Peggy Schlesinger, Chandler Community of Baha'i Faith.

Pledge of Allegiance

The Pledge of Allegiance was led by Councilmember Poston.

Scheduled Public Appearances

MAYOR HARTKE invited Councilmember Ellis to join him for the recognitions.

1. Service Recognitions

Alicia Morrison Skupin – 10 Years, City Magistrate
Jose Ortiz – 10 Years, Public Works & Utilities
James Kame - 25 Years, Development Services
Renee Moreno – 25 Years, Facilities

2. Recognition: 2024 Achievement of Excellence in Procurement

MAYOR HARTKE recognized Matt Dunbar, Budget and Policy Director; Christina Pryor, Procurement & Supply Senior Manager; Rosenda Contreras, Procurement Senior Specialist; and Saranna Davidson, Procurement Officer for their achievements recognized by the National Procurement Institute.

3. Proclamation: National Friends of the Libraries Week

MAYOR HARTKE read the proclamation and invited Friends of the Chandler Public Library representative Bob Lowry; Tara Anglin, Library Community Engagement Program Manager; and Sue Van Horne, Library Manager to accept.

MR. LOWRY announced that over the past year, the Friends of the Chandler Public Library organization has donated over \$100,000 to support library events and programs, and over \$135,000 for the outreach vehicle Bookmobile. Mr. Lowry thanked library users and the community for their support of Chandler's libraries.

4. Proclamation: Chandler's Celebration of Arbor Day

COUNCILMEMBER ELLIS read the proclamation and invited John Sefton, Community Services Director and Corey Povar, Community Services Assistant Director, to accept.

MR. POVAR thanked Mayor and Council for their support. Mr. Povar shared the Golden Leaf Award granted by the International Society of Arboriculture and invited the community to the Arbor Day event on November 2, 2024, at Desert Breeze Park.

5. Proclamation: National Community Planning Month

MAYOR HARTKE read the proclamation and invited Chandler Planning staff to accept.

KEVIN MAYO, Planning Administrator, thanked Mayor and Council for their support. Mr. Mayo encouraged the community to get involved in the 2026 General Plan update beginning shortly. There will be many opportunities for community members to provide input in the process.

Consent Agenda and Discussion

City Clerk

1. Approval of Minutes

Move City Council approve the Council meeting minutes of the Work Session of September 9, 2024, the Regular Meeting of September 9, 2024, the Study Session of September 9, 2024, the Special Meeting of September 12, 2024, and the Regular Meeting of September 12, 2024.

Public Works and Utilities

2. Final Adoption of Ordinance No. 5106 Granting a Non-Exclusive Power Distribution Easement to Salt River Project Agricultural Improvement and Power District, at No Cost, for Electrical Facilities Relocation Required as Part of the Chandler Heights Road Improvement Project

Move City Council approve final adoption of Ordinance No. 5106 granting a non-exclusive power distribution easement to SRP, at no cost, for electrical facilities relocation required as part of the Chandler Heights Road Improvement Project.

Consent Agenda Motion and Vote

Councilmember Stewart moved to approve the Consent Agenda of the October 14, 2024, Regular City Council Meeting; Seconded by Councilmember Ellis.

Motion carried unanimously (7-0).

Adjourn

The meeting was adjourned at 6:33 p.m.

ATTEST: _____
City Clerk

Mayor

Approval Date of Minutes: November 4, 2024

Certification

I hereby certify that the foregoing minutes are a true and correct copy of the minutes of Regular Meeting of the City Council of Chandler, Arizona, held on the 14th day of October 2024. I further certify that the meeting was duly called and held and that a quorum was present.

DATED this _____ day of November, 2024.

City Clerk

Meeting Minutes

City Council Study Session

October 14, 2024 | 6:00 p.m.
Chandler City Council Chambers
88 E. Chicago St., Chandler, AZ



Call to Order

The meeting was called to order by Mayor Kevin Hartke at 6:33 p.m.

Roll Call

Council Attendance

Mayor Kevin Hartke
Vice Mayor OD Harris
Councilmember Angel Encinas
Councilmember Christine Ellis
Councilmember Mark Stewart
Councilmember Matt Orlando
Councilmember Jane Poston

Appointee Attendance

Joshua Wright, City Manager
Kelly Schwab, City Attorney
Dana DeLong, City Clerk

Consent Agenda and Discussion

Airport

1. Professional Services Agreement No. AI2501.201, with Dibble & Associates Consulting Engineers, Inc., for Hangar Area Pavement Reconstruction Design Services
Move City Council award Professional Services Agreement No. AI2501.201 to Dibble & Associates Consulting Engineers, Inc., for the Hangar Area Pavement Reconstruction Design Services, for an amount not to exceed \$328,578.

City Clerk

2. Resolution No. 5845, Setting the 2025 City Council Regular Meeting Schedule
Move City Council pass and adopt Resolution No. 5845, setting the 2025 City Council Regular Meeting Schedule.

3. Boards and Commissions Member Appointments
Move City Council approve the Board and Commission appointments as recommended.

Community Services

4. Agreement No. CS4-988-4668, Amendment No. 1, for Park Mowing Services
Move City Council approve Agreement No. CS4-988-4668, Amendment No. 1, for Park Mowing Services, with Artistic Land Management, Inc., in an amount not to exceed \$628,950.20, for a one-year term, November 1, 2024, through October 31, 2025.
5. Agreement No. CS4-988-4669, Amendment No. 1, for Park Herbicide Application Services
Move City Council approve Agreement No. CS4-988-4669, Amendment No. 1, for park herbicide application services, with BrightView Landscape Services, Inc., in an amount not to exceed \$320,798.77, for a one-year term, November 1, 2024, through October 31, 2025.
6. Purchase of Furniture for Tumbleweed Recreation Center (TRC) Expansion
Move City Council approve the purchase of furniture for Tumbleweed Recreation Center (TRC) expansion, utilizing the State of Arizona Contract No. CTR067414, with Wist Office Products, in an amount not to exceed \$149,546.35.
7. Sole Source Purchase of Pool Filters
Move City Council approve the sole source purchase of pool filters, from Aquatic Environmental Systems, Inc., an Aquafinity Company, in an amount not to exceed \$221,681.55.

Cultural Development

8. Purchase of Downtown Wayfinding Signage - Phase 1
Move City Council approve the purchase of downtown wayfinding signage, from YESCO, LLC, utilizing the City of Peoria Contract No. P19-0079, in the amount of \$167,574.

KIM MOYERS, Cultural Development Director, presented a briefing on the Downtown Pedestrian and Wayfinding Study.

- July 2021 – City Council approved an agreement with J2 Engineering and Environmental Design to complete a Downtown Pedestrian and Wayfinding Study.
- The Study included a 4 – step approach:
 - 1 – Identify and confirm the goals and needs of the project
 - 2 – Study the existing conditions of the downtown
 - 3 - Develop draft recommendations and present to the city and stakeholders
 - 4 – Finalize the recommendations and prepare the report
- Input was gathered through an internal stake holder group, the Downtown Chandler Community Partnership, surveys to downtown businesses and residents.
- Downtown Pedestrian and Wayfinding Study
- September 2022 – Downtown Pedestrian and Wayfinding Plan completed

- Signage & Wayfinding Improvement Recommendations
 - Garage Vehicular Signs
 - Downtown Directional Signage
 - Pedestrian Identifiers
 - Pedestrian Directional Signage
 - Bicycle Signs
- Pedestrian Improvement Recommendations
 - Enhanced Pedestrian Crossings
 - Placemaking
 - Pedestrian Comfort
- Downtown Pedestrian and Wayfinding Study
- FY23-24 Budget – City Council approved Funding for Phase 1 – Downtown Wayfinding Signage in the amount of \$156,000
- Phase 1 includes a total of 41 signs
- Garage indicator and vehicular signage to the 5 parking garages
- Named garage signs on City-owned parking garages
- Interior garage signage directing vehicles to other garages if full
- Crosswalk signage between City Hall and City Hall parking garage
- Pedestrian Directional Signage
- Wayfinding Signage Phase 1
- Questions?

COUNCILMEMBER ORLANDO asked about the excessive number of signs in the downtown area and whether some will be relocated.

MS. MOYERS said the study aimed to address the overwhelming amount of signage in the growing downtown area. The plan is to replace various parking signs with a clear and consistent theme throughout the downtown. This will include uniform signage and clear directions to free parking garages.

COUNCILMEMBER ORLANDO asked about the timeline for phases two and three.

MS. MOYERS said phase two will focus on the pedestrian path from Chandler Boulevard to Frye. The Council approved funding for this year, so they will move directly into phase two after implementing phase one. Phase two is set to be completed in 2025.

COUNCILMEMBER POSTON asked about the parking garages capacity and why some fill up more quickly than others.

MS. MOYERS said that despite the preference for parking on the west side, there are three parking garages on the east side of Arizona Ave. Efforts are being made to educate and encourage people to consider parking on the east side. Signage is being introduced to guide

people to the other three parking garages across the street. The goal is to show that it is just as convenient to park on the east side as it is on the west side.

Development Services

9. Introduction and Tentative Adoption of Ordinance No. 5110 Rezoning, Preliminary Development Plan, and Preliminary Plat, PLH24-0001/PLT24-0001, Toll Brothers Uptown, Located Approximately 1/4 Mile North of the Northeast Corner of Arizona Avenue and Warner Road

Rezoning

Move City Council introduce and tentatively adopt Ordinance No. 5110 approving PLH24-0001 Toll Brothers Uptown, Rezoning from Regional Commercial (C-3) to Planned Area Development (PAD) for multi-family residential, subject to the conditions as recommended by Planning and Zoning Commission.

Preliminary Development Plan

Move City Council approve Preliminary Development Plan PLH24-0001 Toll Brothers Uptown for site layout and building architecture, subject to the conditions as recommended by Planning and Zoning Commission.

Preliminary Plat

Move City Council approve Preliminary PLT24-0001 Toll Brothers Uptown, subject to the conditions recommended by Planning and Zoning Commission.

10. Introduction and Tentative Adoption of Ordinance No. 5112, Rezoning and Preliminary Development Plan, PLH24-0023 The District Downtown, Located at the Northwest Corner Arizona Avenue and the Loop 202 SanTan Freeway

Rezoning

Move City Council introduce and tentatively adopt Ordinance No. 5112 approving PLH24-0023 The District Downtown, Rezoning from PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) to PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet, subject to the conditions as recommended by Planning and Zoning Commission.

Preliminary Development Plan

Move City Council approve Preliminary Development Plan PLH24-0023 The District Downtown for site layout and conceptual building architecture, subject to the conditions as recommended by Planning and Zoning Commission.

LAUREN SCHUMAN, Principal Planner, presented the following presentation.

- PLH24-0023 The District Downtown
- Background
- 1987 Rezoning

- AG-1 to PAD for office, retail, convention center, hotel, & multi-family as part of a masterplan including
- 1989 – Zoning extended
- 2000 – the zoning vested when apartments built
- 1987 Approved Site Plan
- Background cont.
- 2007 Amendment
- Rezoned PAD for Office, Retail, & Hotel
- Mid-Rise Overlay 120 feet
- Three Phases:
 - 2nd Yellow
 - 3rd Red
- 2010 – Time Extension approved
- Background cont.
- 2012 Revised PDP
- Parking garage centered within development
- Hotel shifted to east; 7-stories
- Phasing Plan Revised
 - 1st 12,000 square feet
 - 2nd No more than four pads can be constructed prior to office building
 - No more than six pads constructed prior to 2nd office building or hotel
- 2012 Approved Site Plan
- Background cont.
- November 2022
- Rezoned PAD for Mix Use office, commercial, & multi-family
- Four office buildings
- One Hotel
- Multiple commercial buildings
- Two drive-throughs
- Two commercial mixed building
- Two multi-family developments
(43.5 du/ac & 52 du/ac)
- Setbacks reduced to 30 feet Arizona Avenue & Pecos Road
- 2022 Phasing Plan
- Commercial development must occur before any multi-family
- Certificate of Completion for commercial before or at same time for multi-family
- 2nd Phase All commercial development must occur adjacent to Arizona Avenue, except mixed-use buildings prior to 2nd multi-family development
- Certificate of Completion for commercial before for multi-family permit can be pilled.
- Request
- Rezoning to PAD for mixed used development including multi-family, office, commercial uses as permitted under Community Commercial (C-2) type uses to PAD for mixed used

development including multi-family, office, commercial uses as permitted under Community Commercial (C-2) type uses & automobile and trucks sales

- Mid-Rise Overlay allowing for heights up to 120 feet
- Preliminary Development Plan (PAD) approval for site layout and conceptual building architecture
- 2022 Approve Plan
- Request
- Zoning stipulation permitting only franchise dealerships
- Phasing Plan
- Landscaping along streets & internal drive
- Western multi-family, commercial at corner, & hotel
- Applicant request future phases as market demands
- Planning & Zoning Commission

Concerns phasing plans & types of uses at downtown gateway

Applicant's Modified Stipulation

"A separate PDP application shall be required for the proposed multi-family on Parcel 4. PDP application for parcel 4 cannot be filed until one hotel and 75% of the commercial building in Phase 2 received Certificate of Completion."

- Recommendations
- Planning & Zoning Commission recommends approval (5-1 Chair Heuman)
- Stipulations in place for uses
- Stipulations for high quality of design
- Consistent General Plan

MAYOR HARTKE asked about shifting from C2 to C3 zoning, inquiring about what other developments could be allowed and whether the C3 designation applies only to the bottom right quadrant or extends to the entire site. He sought clarification on the implications of the C3 designation.

MS. SCHUMAN said the site would only allow for C2-type uses and additional automotive and truck sales.

MAYOR HARTKE asked what guarantees are there that anything other than phase one gets built on this site.

MS. SCHUMAN said she would have to direct the request to the applicant asking to remove the phasing stipulations.

MAYOR HARTKE said that removing the stipulation allows phase one to be built but constrains the center apartment complex. Other than the pictures, no specific statement shows these changes will happen.

MS. SCHUMAN said that is correct.

BRENNAN RAY, Attorney with Burch & Cracchiolo, presented the following presentation.

- Multi-Family – Checks and Balances
 - Questions about additional multi-family on the site:
 - Rezone/PDP Stip 1 – Substantial conformance to the Development Booklet
 - Conceptual Site Plan (Exhibit 2) identifies where land uses can be
 - Rezone Stip 3 – Residential permitted on Parcels 4 & 5
 - PDP Stip 2/Development Booklet – Separate PDP application (City Council approval) for multi-family on Parcel 4
- Any changes or introduction of a different use on a give parcel requires a new application and City Council approval
- Automobile Dealership – Check and Balance
- Recommend Conditions of Approval
- Rezoning
- 5. Users for the automobile and truck sales shall be limited to franchise dealerships only. Franchise dealerships are those dealerships that (a) meet the definition of a “franchise” under A.R.S. 28-4301, (b) have received a New Motor Vehicle Dealer license from the Motor Vehicle Division to sell new vehicles, and (c) have the legal right to sell new vehicles to the public for a specific manufacturer or brand.
- Development Phasing – Checks and Balances
- Questions about development phasing:
 - Rezone/PDP Stip 1 – Substantial conformance to the Development Booklet
 - Phasing Plan (Exhibit 9) identifies what must be built in each phase
 - Language in the Development Booklet identifies what must be built in each phase
 - PDP Stip 4/Development Booklet language require a separate PDP before anything can take place on the 2nd multi-family parcel (Parcel4)
 - (PC) Revised PDP Stip 2 – Cannot file the PDP until one hotel and 75% of the commercial buildings along Arizona Avenue have a Certificate of Completion.
- Any changes to the above requires a new application and City Council approval
- Development Phasing – Checks and Balances
- New PDP Stip 22:
 - The developer shall pull a building permit on a non-residential use within 18 months of the effective date of the Rezoning Ordinance.
- Request City Council approval in accordance with Planning Commission Staff's Recommendations for Approval with the additional PDP Stipulation No.22.

MAYOR HARTKE indicated that the request was to remove certain conditions rather than relocate them. There are concerns about the request and a suggestion to consider 25,000 square feet of development. It is important to avoid just meeting the requirements with a small coffee shop, as it may not align with the intention expressed.

MR. RAY said he would discuss that with the client.

MAYOR HARTKE said he needs to know by Thursday; otherwise, a continuance will be requested. While willing to consider the reasoning, he believes a minor language adjustment will not achieve the intended goals.

MR. RAY said he believes that the existing requirements provide enough oversight. He acknowledged the feedback and mentioned that he would discuss it with the client.

MAYOR HARTKE discussed the possibility of implementing a financial incentive if the agreed-upon time frame is unmet. This could be in the form of a daily fee or another suitable arrangement. It is important to consider this because, as they understand it, there seems to be little motivation for the client to proceed beyond the development of the initial apartment complex. Therefore, having a more comprehensive plan and some assurance about the entire process would be helpful. Mayor Hartke requested that the matter be brought up with the client, and they are looking forward to hearing about it on Thursday.

MR. RAY said that he would discuss the matter with the City Attorney. He is unsure about the necessity of revising the city's criminal or civil code regarding violations of zoning ordinances. However, he assured that he would work with the City Attorney to determine the appropriate action without rewriting the civil code.

MAYOR HARTKE said some of those issues can be linked to the property. He does not see that as a requirement, but he would certainly welcome that conversation.

COUNCILMEMBER ORLANDO asked about the 25,000 square feet of development and needing clarification.

MAYOR HARTKE stated that he is looking for a measure that indicates substantial development will be initiated within a specific timeframe rather than just preparing a 3,000-square-foot pad.

COUNCILMEMBER ORLANDO inquired if building the hotel would meet the requirement. There is some uncertainty about our objective. It appears to be a good approach, but there is still confusion about defining a 25,000-square-foot structure. It is agreed that simply having a coffee shop will not suffice, and the focus is on finding a significant structure to meet our goal.

MAYOR HARTKE said he would look for a significant structure, whether a dealership or something with more than a few shops. However, he was unsure about the proposed hotel specific square footage.

COUNCILMEMBER ORLANDO said the area needs substantial development rather than just one pad being developed.

COUNCILMEMBER ELLIS asked about the status of the mentioned dealerships. Due to the lengthy licensing process, there was uncertainty about their establishment and relocation.

MR. RAY said the three dealerships that initially got them started on this path are still potential options for this site. There was some discussion about one of the dealerships landing somewhere else, but as far as he remembers, the dealership is about 21 miles away at Signal Butte and US 24. According to state statute, this might be an issue because of the 10-mile restriction. The rules for a new dealership versus relocation are different, but based on their conversation, it was confirmed that those three dealerships could still be located on this site. Mr. Ray also explained that there is a process involving franchisees and OEMs.

COUNCILMEMBER ORLANDO asked about the third option that was initially mentioned. He asked whether enough space was available and whether a parcel needed rezoning.

Mr. RAY commented that this plan specifies the locations where land use is designated, so we cannot just build multifamily housing anywhere on the site. As a result, the only allowable location for auto dealerships is the one indicated on the plan. If another dealership wants to establish itself here, we must revisit the process, amend the zoning, and adjust the PDP to allow for a third dealership.

MS. SCHUMAN said if a third dealership existed and fit in parcel one, it could be reviewed administratively. However, it must meet all our site development standards, so the setbacks established within this case are parking and landscaping. So, if a third could fit in parcel one, it could be considered administratively.

COUNCILMEMBER ORLANDO said it is negotiable. There is enough land and room on this canvas to make changes.

MR. RAY said yes, it would require deleting some of the other stuff.

COUNCILMEMBER ORLANDO said a dealership is more valuable than a few retail shops.

11. Resolution No. 5841, Authorizing a License Agreement between Cablevision Lightpath, LLC, and the City of Chandler for the Use of Public Property for the Establishment of Class 4 and Class 5 Telecommunications Systems
Move City Council pass and adopt Resolution No. 5841, authorizing the Mayor to execute the license agreement between Cablevision Lightpath, LLC, and the City of Chandler for the use of facilities in the city's rights-of-way and public places to establish Class 4 and Class 5 Telecommunications Systems, and authorizing the City Manager or designee to execute other documents as needed to give effect to the agreement.
12. Resolution No. 5844, Authorizing an Amended and Restated License Agreement between Crown Castle Fiber, LLC, and the City of Chandler for the Use of Public Property for the Establishment of Class 4 and Class 5 Telecommunications Systems

Move City Council pass and adopt Resolution No. 5844, authorizing the Mayor to execute the amended and restated license agreement between Crown Castle Fiber, LLC, and the City of Chandler for the use of facilities in the city's rights-of-way and public places to establish Class 4 and Class 5 Telecommunications Systems, and authorizing the City Manager or designee to execute other documents as needed to give effect to the agreement.

COUNCILMEMBER ORLANDO asked where the experimental cuts would be made and inquired about the locations and longevity monitoring. They discussed the potential impact on the asphalt and wanted to focus on that issue.

KEVIN SNYDER, Development Services Director, said as it currently stands, there are discussions with Crown Castle on the locations. However, the locations for installing certain technology will be limited to specific city areas and not permitted on arterial streets. Instead, they will only be allowed on local and collector streets. The city will closely monitor this pilot study and will work closely with Crown Castle to ensure that they obtain the necessary permits, including encroachment permits, to work in the right of way. The city will also monitor the introduction and use of the micro trenching technology. It has been specified that Crown Castle will not be allowed to bore under any sidewalks or break open any sidewalks; they will only be allowed to bore under sidewalks. The installation is intended to be limited to about 10,000 lineal feet, with the initial focus anticipated to be in southeast Chandler on local collector streets. The city will collaborate with Crown Castle to minimize any interference within the public space. The micro trenching work will be approximately six inches off the curb, and efforts will be made to ensure minimal impact on the functionality of the roadway and people utilizing the public space.

COUNCILMEMBER ORLANDO asked for data on similar areas over a year and expressed concern about potential issues arising from the 10,000 linear square feet requirement in the new process, seeking help addressing future problems.

MR. SNYDER said that Crown Castle has agreed with the town of Queen Creek to implement the Rapid Connect program for a three-year pilot study in the city. Crown Castle is responsible for meeting the city's technical specifications and designs, addressing any damage to the roadway, and proceeding at their own risk. There is no guarantee that they will be allowed to keep the technology in the right of way after the pilot program ends or expand it elsewhere in the city. The city aims to control the implementation tightly to prevent negative impacts on public spaces or users.

COUNCILMEMBER ORLANDO asked about their indemnification in case of a street issue such as leakage or cracking.

MR. SNYDER agreed, and they would be working hand in hand with Dennis Aust and his team in close coordination.

COUNCILMEMBER STEWART said the technology is being used in Colorado, where they experience various weather conditions. It is already being used in Elk Grove, California, and other similar places, so it is proven technology.

MR. SNYDER said that a representative from Crown Castle can provide more details about the technology which has been used successfully in various locations with different weather conditions.

COUNCILMEMBER STEWART commented this is going to be less intrusive for the neighborhoods.

COUNCILMEMBER ELLIS asked about the duration of the pilot program.

MR. SNYDER said if approved by City Council it would be a three-year pilot program.

COUNCILMEMBER ELLIS said that during the three-year pilot program, the Council will monitor it to ensure compliance. She expressed concern about a past issue with the applicant not staying in the contract and questioned whether this could be another issue in the future.

MR. SNYDER said that there was a dispute over fees, but the city worked proactively with Crown Castle to resolve the issue. It was reported that they have successfully paid all outstanding fees to the city and are in good standing. The city will be working closely with them to implement new technology, even though it is new to the city. Dennis Aust and his team will provide extra oversight to ensure there is no negative impact. While minimal impact on the roadways is expected based on the micro trenching technology, any significant impact will be immediately addressed, potentially including stopping the work. The Council can be assured that the city will closely monitor the situation.

COUNCILMEMBER ELLIS said she met with representatives from Crown Castle and found them to be amicable and cooperative. Despite initial concerns about their obligations, Councilmember Ellis is pleased to hear that they have reached an agreement and look forward to continued collaboration.

13. Preliminary Development Plan, PLH22-0024 Cornerstone Ranch, 3999 S. Dobson Road, Located at the Northeast Corner of Dobson Road and Ocotillo Road
Move City Council approve Preliminary Development Plan PLH22-0024 Cornerstone Ranch, for modifications to existing signage, subject to the conditions as recommended by Planning and Zoning Commission.
14. Agreement No. DS5-918-4808, General Plan Update and Comprehensive Housing Plan

Move City Council approve Agreement No. DS5-918-4808, with Logan Simpson Design, Inc., for the General Plan Update and Comprehensive Housing Plan, in the amount of \$525,000.

LAUREN SCHUMAN, Principal Planner, gave a briefing on 2026 General Plan Update.

- 2026 General Plan Update
- 2016 General Plan
- What is a General Plan?
 - Comprehensive set of board policies guides development
 - Strategic plan; not parcel specific
 - Community goals
- What is a General Plan?
 - State Statute requires update every 10 years
 - 2016 Approved by voters 85.8%
 - No major or minor amendments occurred
- What is a General Plan?
- 17 Elements required by state law
 - Land Use
 - Circulation
 - Bicycling Growth Areas
 - Neighborhood Planning
 - Housing
 - Conservation, Rehabilitation, and Redevelopment
 - Cost of Development
 - Recreation
 - Open Space
 - Energy
 - Conservation
 - Environmental Planning
 - Water Resources
 - Public Services and Facilities
 - Public Buildings
 - Safety
- Why Update the General Plan?
 - Update/re-adoption required by State law every 10 years
 - Address new development trends/factors facing Chandler
 - Approaching build-out
- General Plan Process
- July 2024- Request for Proposal (RFP) posted consultant
- August 2024- RFP's review and conduct interviews
- October 2024- City Council agenda with RFP contract three proposals submitted; Logan Simpson Design

- October – December 2024- Project setup and identify goals
 - Resident Advisory Committee (RAC) created
 - Members appointed by the Mayor & voted on by the City Council- future agenda
 - Committee comprised of interested stakeholders, business members, activists, & residents
 - Guide staff & consultant throughout the update
- January-June 2025
 - Public workshops/input
 - Planning lab-open houses to discuss different topics
 - Partner with local businesses to host events, i.e. ice cream social
- Meet with regional stakeholders (MAG & adjacent cities) & Interdepartmental staff review
- Work Session briefing with hearing boards
 - City Council
 - Park and Recreational board
 - Economic Development Advisory Board
 - Transportation Commission
 - Airport Commission
 - Neighborhood Advisory Committee
- June 2025-Finalize Comprehensive Housing Plan: comply with SB 1162
- August 2025- Council Micro-retreat, RAC Review, Regional Resources Team Review
- October 2025- Initiate 60-day statutory review period Public meetings seeking final comment
- December 2025- 60-day period ends; RAC, Boards and commissions review
- January 2026- Two Planning and Zoning Commission public hearings
- February 2026- City Council agenda
- April 2026- 120 days prior to election; deadline put on ballot
- August 2026- Citizen ratification vote during Primary election
- Questions?

COUNCILMEMBER POSTON asked about the housing plan, pointing out that residents have been asked to join committees for the bond and the general plan. Councilmember Poston inquired whether the general plan and housing committees would be the same or separate and stressed the need for community involvement to expedite one plan over the other.

MS. SCHUMAN said she would refer her question to Leah Powell.

LEAH POWELL, Neighborhood Resources Director, said a stakeholder group focused on the comprehensive housing plan will be created. This plan will be completed before the final draft of the general plan to ensure integration. The team is identifying various stakeholders, including nonprofit providers, affordable housing developers, the business community, and school districts. This diverse representation is essential, as the housing plan aims to serve all income levels and promote a range of housing options.

COUNCILMEMBER POSTON clarified that, while affordable housing is essential, the focus also includes all housing types and their placement within Chandler.

MS. POWELL said that the team would examine various types of housing options. In addition to the current housing available in Chandler, they aim to explore innovative housing solutions that can cater to a diverse audience.

COUNCILMEMBER POSTON asked the group and city staff to be creative in exploring housing options. Councilmember Poston mentioned the success of other cities in transforming businesses into residential spaces despite challenges. She stressed the need to consider all possibilities, including utilizing city-owned property for new housing developments.

MS. POWELL said their approach would be twofold. They would return to the Council in the near future to discuss the public housing stock and continue with the plan to redevelop it. As part of that conversation, they might also consider other city parcels. It was anticipated that this would occur at various times.

MAYOR HARTKE asked whether the housing group would report directly to Ms. Powell. Mayor Hartke also asked if they would be making presentations to the General Plan Committee and how the process between these two concurrent committees operates.

MS. POWELL said that developing a joint plan with the general plan is essential for consistency and continuity. While the team conducts annual housing assessments, that data has not been integrated into a broader city plan. Amy Jacobson and Ms. Powell will oversee the comprehensive housing plan, collaborating closely with the Development Services department. Staff from Ms. Powell's department will also serve as liaisons to ensure a cohesive process. They aim for reports from the comprehensive housing plan to integrate into the final document for the general plan committee, allowing for staff input and thorough vetting. The comprehensive housing plan is expected to be completed before the general plan is finalized.

MAYOR HARTKE asked whether there would eventually be a presentation to the General Plan Committee. The committee's intention is to accept the proposal and integrate it into the general plan rather than allow the two processes to continue functioning separately.

MS. POWELL said that is correct.

COUNCILMEMBER ORLANDO asked about the expected outcomes of the housing plan, noting his understanding of the goals, given the long-standing general plan.

COUNCILMEMBER STEWART asked about the comprehensive housing plan and whether the Home Builders Association, neighbors, and other stakeholders would be involved. He noted that

the department typically focused on veterans, the elderly, and HUD housing and wanted to ensure a broader perspective in the planning process.

MS. POWELL agreed with all the mentioned groups, highlighting the need to focus on various housing types in Chandler, not just affordable options. The goal is to meet the diverse needs of current and future residents by assessing existing housing assets and their locations and identifying any gaps.

COUNCILMEMBER STEWART asked about the process for selecting members for that committee, asking whether the Mayor would handle the selections and if the Council would vote on them.

MAYOR HARTKE said similar to the Bond Committee, he would gather input to form a board for Council approval of the general plan. He noted the importance of close collaboration to avoid conflicts in recommendations and acknowledged the significant work done on both matters, as mentioned by Councilmember Poston.

COUNCILMEMBER POSTON asked if the comprehensive housing plan would be incorporated into the general plan and not returned to them, proceeding together after June.

MS. POWELL said that the team will present information to the Council, including a report from the comprehensive housing plan. This report will also be included in the general plan, serving a dual purpose. She emphasized their commitment to meeting state guidelines and deadlines, formalizing their ongoing compliance in the process.

MAYOR HARTKE said that the Council could certainly hold a study session to ensure everyone is informed and can address any questions before the document becomes finalized for Council input.

15. Professional Services Agreement No. DS2402.201, with Greenlight Traffic Engineering, for the Traffic Signal at Gilbert Road and Amanda Boulevard Design Services
Move City Council award Professional Services Agreement No. DS2402.201 to Greenlight Traffic Engineering, for the Traffic Signal at Gilbert Road and Amanda Boulevard Design Services, in an amount not to exceed \$115,622.46.
16. Use Permit PLH24-0018, Pecos & McQueen Storage, Located at the Southeast Corner of Pecos and McQueen Roads
Move City Council approve Use Permit PLH24-0018, Pecos & McQueen Storage, allowing storage facility uses on the property, subject to the conditions recommended by Planning and Zoning Commission.
17. Entertainment Use Permit PLH24-0022 The Mexicano, 3095 W Chandler Boulevard, Generally Located at the Southwest Corner of Chandler Boulevard and Price Road

Move City Council approve Entertainment Use Permit PLH24-0022 The Mexicano to allow live indoor and outdoor entertainment, speakers and television screens, subject to the conditions recommended by Planning and Zoning Commission.

Facilities and Fleet

18. Agreement No. BF2-912-4485, Amendment No. 2, with ASR Construction Group, LLC; Crawford Mechanical Services, LLC; FPS Civil, LLC; DMS Companies, Inc., dba Hernandez Companies; Kowalski Construction, Inc.; MGC Contractors, Inc.; and Nickle Contracting, LLC, for General Building Maintenance Services

Move City Council approve Agreement No. BF2-912-4485, Amendment No. 2, with ASR Construction Group, LLC; Crawford Mechanical Services, LLC; FPS Civil, LLC; DMS Companies, Inc., dba Hernandez Companies; Kowalski Construction, Inc.; MGC Contractors, Inc.; and Nickle Contracting, LLC, for general building maintenance services, in an amount not to exceed \$3,205,000, for a period of one year, beginning September 1, 2024, through August 31, 2025.

19. Purchase of HVAC Equipment, Installation, and Services

Move City Council approve the purchase of heat, ventilation, and cooling (HVAC) equipment, installation, and services, utilizing multiple vendors available under the Omnia Partners Contract No. R200403, with TDI Industries, Inc.; Contract No. R200402, with Johnson Controls, Inc.; Contract No. R200401, with Daikin Applied Americas, Inc.; and Contract No. 15-JLP-023, with Trane U.S., Inc.; and the Mohave Educational Services Cooperative Contract No. 19F-SMC-0904, with Sun Mechanical Contracting, Inc., in a combined amount not to exceed \$2,535,000.

20. Purchase of Maintenance, Repairs and Purchase of Doors, Locking Systems, Door Hardware, and Operable Walls

Move City Council approve the utilization of the Mohave Educational Services Cooperative Contract No. 21H-DHP-0917, for maintenance, repairs, and purchase of bay doors, locking systems, door hardware, and operable walls, from DH Pace Company, Inc., in an amount not to exceed \$380,000, for the period of one year, September 17, 2024, through September 16, 2025.

21. Purchase of Access Controls and Security Equipment

Move City Council approve the purchase of access controls and security equipment, from APL Access & Security, Inc., utilizing the State of Arizona Contract No. CTR056377, in an amount not to exceed \$825,000.

22. Purchase of Vehicles and Upfitting

Move City Council approve the purchase of vehicles and upfitting, using multiple vendors under State of Arizona contracts, in a combined amount not to exceed \$4,314,940.

COUNCILMEMBER ELLIS says she has concerns about the proposed \$8 million investment in vehicles. The question is whether this is the right time to spend, given inflation, and if the money should go toward vehicles. Councilmember Ellis shared a recent experience riding with the Mayor in a poorly maintained city car, noting the lack of basic features like a light on the roof. She requested a brief explanation of the necessity for this investment now.

MICHAEL HOLLINGSWORTH, Facilities and Fleet Manager, said that the budget is around \$4.1 million, a reduction from last year, which includes fitting costs. He explained that determining which vehicles need replacement involves a comprehensive assessment of factors like mileage, repair costs, and input from the Fleet Advisory Committee. Addressing the timing of replacements, Mr. Hollingsworth acknowledged the dilemma of whether to buy now or later, noting that inflation is unpredictable. He shared his own recent experience with purchasing a vehicle and mentioned that the city's decision-making process is thorough. Initially, they identified 86 vehicles for replacement, which has now been narrowed down to 51. While there are concerns about the economy and rising car prices, he believes that prices are starting to come down. Ultimately, he emphasized that the selected vehicles are essential for replacement at this time.

COUNCILMEMBER ELLIS said she supported the proposal, stating there are no concerns after reviewing it. Councilmember Ellis acknowledged the city's commitment to fiscal responsibility and noted that some individuals had raised questions. She emphasized the need to address those concerns to clarify why action was necessary.

COUNCILMEMBER STEWART said there are concerns about expense reductions compared to the previous year. He pointed out that many residents use their vehicles until they are no longer functional, citing a truck recently turned in with 180,000 miles. He stressed the need to maximize the lifespan of fleet vehicles before replacing them and has been advocating for this for about five years. He is cautious about budgets of four to eight million dollars for new vehicles, wanting to ensure that these decisions respect residents who do not buy new cars annually.

MR. HOLLINGSWORTH said, comparing this year to last year, noting that there were 93 vehicles last year and 51 this year, with a budget of \$4.1 million that includes fitting. Last year, without upfitting, the total was around \$4 million.

He pointed out that upfitting costs have risen significantly, about 30% more than the vehicle purchase price. When creating the initial list of 86 vehicles, they work with the Facilities Advisory Committee to assess needs and vehicle conditions. If a vehicle can last another year, it is removed from the list.

COUNCILMEMBER ORLANDO said that nearly three million dollars is allocated for the purchase of 36 police vehicles, primarily for upgrades. Councilmember Orlando highlighted those police vehicles are overutilized and experience more wear and tear due to being on the road 24/7. He questioned if this explains the greater wear and tear compared to utility vehicles.

MR. HOLLINGSWORTH said that the vehicles endure a significant amount of abuse, and the goal is to equip public safety personnel with the right tools for their jobs. He noted that these vehicles operate in extreme environments, which include both considerable idle time and extensive driving.

COUNCILMEMBER ORLANDO said that the Police department is clearly expanding with additional patrols. He assumed that the other seven vehicles were intended for the Fire department for similar reasons.

MR. HOLLINGSWORTH said it is a very harsh environment.

COUNCILMEMBER ORLANDO said that several vehicles, including two for Community Services and one or two for Cultural Development, are essential for replacement.

COUNCILMEMBER STEWART said that his concerns focus on volunteer cars and departmental vehicles, not public safety. He noted that he had requested a separate budget for Police and Fire about four years ago, as public safety operates independently with its own resources. Councilmember Stewart expressed that combining these budgets makes thorough analysis difficult, and he is hesitant to vote against essential expenses like police vehicles if they are included in a broader, less transparent budget.

MR. HOLLINGSWORTH said that the PD is also a significant source for trickle-down vehicles. We can utilize vehicles in many cases where departments will not need to purchase a new vehicle.

Human Resources

23. Resolution No. 5840, Renewal Request to Industrial Commission of Arizona for Continued Exemption from Requirement to Post Security for Self-Insured Workers' Compensation Program

Move City Council pass and adopt Resolution No. 5840, renewing the request to the Industrial Commission of Arizona for continued exemption from the requirement to post security for the Self-Insurance Workers' Compensation Program.

24. Agreement No. 4046, Calendar Year 2025 Amendment, with Anthem Life Insurance Company, for Public Safety Personnel Long-Term Disability Insurance

Move City Council approve Agreement No. 4046, Calendar Year 2025 Amendment, with Anthem Life Insurance Company, for public safety personnel long-term disability

insurance, in an amount not to exceed \$200,000, for the period of one year, beginning January 1, 2025, through December 31, 2025, and authorize the City Manager or designee to extend the Agreement for an additional year, beginning January 1, 2026, through December 31, 2026 (CY2024), subject to the same terms and conditions.

25. Agreement No. 4802, Employee Benefits Consulting Services
Move City Council approve Agreement No. 4802, with The Segal Company (Western States), Inc., for employee benefits consulting services, in an amount not to exceed \$120,000, for the period of one year, beginning January 1, 2025, through December 31, 2025, with the option of up to four one-year extensions.

Management Services

26. Resolution No. 5838 Authorizing the Submittal of a Pass-Through Gaming Tribal Revenue Sharing Grant Application from Dignity Health Foundation on Behalf of Salt River Pima-Maricopa Indian Community
Move City Council pass and adopt Resolution No. 5838 approving an intergovernmental agreement between the Salt River Pima-Maricopa Indian Community and the City of Chandler for the distribution of a pass-through Indian gaming revenue sharing grant in the amount of \$50,000 to Dignity Health Foundation-East Valley; and authorizing submission of a related pass-through grant application for Post-Hospitalization Services at Dignity Health East Valley for Uninsured/Underinsured Hospital Patients in the East Valley.
27. License Series 12, Restaurant Liquor License Application for Jeffrey Craig Miller, Agent, Phoenix Food Junction, LLC, DBA Honest Indian Restaurant
Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 307042, a Series 12, Restaurant Liquor License, for Jeffrey Craig Miller, Agent, Phoenix Food Junction, LLC, DBA Honest Indian Restaurant, located at 1050 W. Ray Road, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308108.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 305733, a Series 12, Restaurant Liquor License, for Theresa June Morse, Agent, Quik Burrito 3, LLC, DBA Maskadores Taco Shop, located at 2100 W. Chandler Boulevard, Suite 38, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308099.
29. License Series 12, Restaurant Liquor License Application for Jeffrey Craig Miller, Agent, Chandler Spitz, LLC, DBA Spitz Mediterranean
Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 306182, a Series 12, Restaurant Liquor License, for

Jeffrey Craig Miller, Agent, Chandler Spitz, LLC, DBA Spitz Mediterranean, located at 301 S. Arizona Avenue, Suite 2, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308080.

30. License Series 12, Restaurant Liquor License Application for Jeffrey Craig Miller, Agent, Hop Brewery, LLC, DBA Brews Taphouse
Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 303548, a Series 12, Restaurant Liquor License, for Jeffrey Craig Miller, Agent, Hop Brewery, LLC, DBA Brews Taphouse, located at 5055 W. Ray Road, Suite 2, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 166927.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 305465, a Series 10, Beer and Wine Store Liquor License, for Jared Michael Repinski, Agent, Gather 336, LLC, DBA George & Gather Market, located at 336 S. Washington Street, Suite 1, and approval of the City of Chandler, Series 10, Beer and Wine Store Liquor License No. 308052.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 305467, a Series 12, Restaurant Liquor License, for Jared Michael Repinski, Agent, Gather 336, LLC, DBA George & Gather, located at 336 S. Washington Street, Suite 3, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308051.
33. License Series 12, Restaurant Liquor License Application for Larry Warren White, Jr., Agent, Warren's Supper Club, LLC, DBA Warren's Supper Club
Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 281651, a Series 12, Restaurant Liquor License, for Larry Warren White, Jr., Agent, Warren's Supper Club, LLC, DBA Warren's Supper Club, located at 1040 N. 54th Street, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 307989.

Neighborhood Resources

34. Agreement No. 4638, Amendment No. 1, for Temporary Accommodation Services
Move City Council approve Agreement No. 4638, Amendment No. 1, with GAJANAN, LLC, for temporary accommodation services, in an amount not to exceed \$600,000, for the period of one year, beginning October 1, 2024, through September 30, 2025.

RIANN BALCH, Community Resources Manager, gave a briefing on Agenda item 34.

- Agenda Item 34
 - Agreement No. 4638, Amendment No. 1 with Gajanan, LLC, for temporary accommodation services, in an amount not to exceed \$600,000, for the period of one year, beginning October 1, 2024, through September 30, 2025.
 - Operation Open Door Non-Congregate Shelter
 - Begin in 2020
 - Provides non-congregant shelter via hotel/motel rooms at Chandler hotels.
 - Client eligibility criteria and participant agreements.
 - Two dedicated City of Chandler Community Navigators provide wrap-around case management to program participants.
 - Goal is to exit shelter or permanent housing.
 - Operation Open Door: FY 2023-2024
 - Non-congregate shelter and supportive services
 - 171 households, including 356 persons served
 - 98% of the 301 persons who exited the program exited to a positive outcome.
 - Why Do We Contract?
 - Ensures access to 15 one-bedroom units at guaranteed flat rate.
 - Accessible to Police and Fire.
 - Provides safe, dignified accommodations for households to focus on next steps.
 - Reduces unsheltered homelessness in Chandler.
 - Cost of Homelessness
 - The cost of homelessness is high: AVE \$35,578/year*
 - Hospitalization and medical treatment
 - Emergency food and shelter
 - Prison and jails
 - Public safety interaction
 - Environmental clean-up
 - Businesses and neighborhoods
 - Permanent supportive housing reduces the cost by 49.5% (PSH costs \$12,800/year)*
 - Non-Congregate Shelter is a cost effective, best practice in expediting transition from homelessness to permanency.
 - Operation Open Door:
 - In FY 23-24, average cost per household was \$6,514 or \$3,129 per person.
 - 98% households exited to a positive exit.
- *Source: National Alliance to End Homelessness (2017)

COUNCILMEMBER STEWART highlighted that the program's success clarifies why the city is purchasing hotel rooms. Access to these rooms helps alleviate pressure on the police and provides individuals with essential support, including wraparound services. Chandler's efforts are noticeably reducing the number of unhoused individuals. The Navigators assist residents in securing housing and job training, effectively clarifying the purpose of funding for hotel rooms.

MS. BALCH said this gives them a quick way to interact with individuals. They can immediately know where people are, ensure they are safe, confirm that they are getting good sleep, support them in working towards their goals, and help them progress towards permanent housing.

MS. BALCH continued the presentation.

- Actual Expenditures
- FY 2024-2025 Expenditures through September 30, 2024

COUNCILMEMBER ELLIS said that during her first two years in office, she advocated for a program that some opposed due to concerns about accepting federal funding. Councilmember Ellis emphasized that she has personally used the program to help people off the streets, believing in its effectiveness. She pointed out that it saves lives and helps individuals graduate with masters degrees and pursue higher education, lifting families out of poverty. She remarked on the importance of providing a safe place for individuals to rest, which enables them to accept services and regain trust. Councilmember Ellis expressed gratitude for the team's efforts and committed to continuing to invest in the program, recognizing its positive impact on the community.

MS. BALCH thanked Councilmember Ellis for sharing her testimony.

MAYOR HARTKE said Ms. Balch addressed his questions regarding cost comparisons, highlighting the real costs associated with homelessness. He emphasized that neglecting these issues only increases the burden on various other services.

Police Department

35. Agreement No. 4856 for Health Screening Services for the Police Department
Move City Council approve Agreement No. 4856, with JS MD Sigma, PLLC, for health screening services for the Police Department, in an amount not to exceed \$108,000, for the period of one year, beginning November 1, 2024, through October 31, 2025, with the option of up to four one-year extensions.

Public Works and Utilities

36. Resolution No. 5842 Authorizing the Acquisition of Temporary Construction Easements Required for the Kyrene Branch and Highline Canal Shared Use Path Project
Move City Council pass and adopt Resolution No. 5842 authorizing the acquisition of temporary construction easements required for the Kyrene Branch and Highline Canal Shared Use Path Project No. TP2202; authorizing the city's real estate administrator to sign the purchase agreement and any other documents necessary to facilitate the acquisition; authorizing eminent domain proceedings as needed to acquire said real property, and obtain immediate possession thereof; and, relocation assistance as may be required by law.

37. Approval of a Settlement with 13012 E. Chandler Heights, LLC, for the Acquisition of Road Right of Way, a Temporary Construction Easement, Drainage Easements, and Easements for Roosevelt Water Conservation District to Relocate their Irrigation Facilities and Improvements Required for the Chandler Heights Road Improvement Project from McQueen Road to Gilbert Road
Move City Council approve a settlement with 13012 E. Chandler Heights, LLC, in the amount of Three Hundred Thousand and no/100ths dollars (\$300,000.00) plus required statutory interest, for acquisition of road right-of-way, a temporary construction easement, drainage easements, easement for Roosevelt Water Conservation District required to relocate their facilities and improvements required for the Chandler Heights Road Improvement project from McQueen Road to Gilbert Road.
38. Agreement No. WW0-885-4192, Amendment No. 4, with Carbon Activated Corporation, for Activated Carbon
Move City Council approve Agreement No. WW0-885-4192, Amendment No. 4, with Carbon Activated Corporation, for activated carbon, in an amount not to exceed \$960,000, for a one-year period, October 1, 2024, through September 30, 2025.
39. Agreement No. PW5-885-4805, with Univar Solutions USA, LLC, for Aluminum Chlorohydrate
Move City Council approve Agreement No. PW5-885-4805, with Univar Solutions USA, LLC, for Aluminum Chlorohydrate, in an amount not to exceed \$130,000, for a one-year period, November 1, 2024, through October 31, 2025, with the option of up to four additional one-year extensions.
40. Agreement No. 4831, with Tetra Tech, Inc., for the Paseo Vista Recreation Area and Landfill Environmental Consulting Services
Move City Council approve Agreement No. 4831, with Tetra Tech, Inc., for the Paseo Vista Recreation Area and Landfill Environmental Consulting Services, in an amount not to exceed \$200,000, for a one-year period, November 1, 2024, through October 31, 2025, with the option of up to four one-year extensions.
41. Professional Services Agreement No. ST1804.451, with Consultant Engineering, Inc., for the Chandler Heights – Gilbert Road to Val Vista Drive Construction Management Services
Move City Council award Professional Services Agreement No. ST1804.451, to Consultant Engineering, Inc., for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive Construction Management Services, in an amount not to exceed \$1,035,646.10.
42. Construction Agreement No. ST1804.401 with Sunland Asphalt & Construction, LLC, for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive

Move City Council award Construction Agreement No. ST1804.401, to Sunland Asphalt & Construction, LLC, for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive, in an amount not to exceed \$9,784,943.

43. Construction Manager at Risk (CMAR) Pre-Construction Services Agreement No. WA2203.251, with Achen-Gardner Construction, LLC, for Water Main Replacements
Move City Council award CMAR Pre-Construction Services Agreement No. WA2203.251, to Achen-Gardner Construction, LLC, for Water Main Replacements, in an amount not to exceed \$249,641.04.
44. Construction Manager at Risk Construction Services Agreement No. WA2105.402, with PCL Construction, Inc., for the Water Facilities Optimization Improvements - Frye Road Water Production Facility Rehabilitation
Move City Council award CMAR Construction Services Agreement No. WA2105.402, to PCL Construction, Inc., for the Water Facilities Optimization Improvements - Frye Road Water Production Facility Rehabilitation, in an amount not to exceed \$3,849,574.
45. Construction Agreement No. ST2101.401, Change Order No. 4, with Achen-Gardner Construction, LLC, for the Detroit Basin/Gazelle Meadows Park Storm System & Associated Improvements
Move City Council award Agreement No. ST2101.401, Change Order No. 4, to Achen-Gardner Construction, LLC, for the Detroit Basin/Gazelle Meadows Park Storm System & Associated Improvements, in the amount of \$295,637.89.
46. Purchase of Granular Activated Carbon
Move City Council approve the purchase of granular activated carbon, from Calgon Carbon Corporation, utilizing City of Phoenix Contract No. 133070, in an amount not to exceed \$1,059,468.
47. Purchase of Bucket Trucks
Move City Council approve the purchase of two bucket trucks, from Sanderson Ford, Inc., utilizing State of Arizona Contract No. CTR069471, in the amount of \$457,054.19.

Informational

48. Special Event Liquor Licenses and Temporary and Permanent Extensions of Liquor License Premises Administratively Approved
49. August 21, 2024, Planning and Zoning Commission Meeting Minutes
50. Contracts and Agreements Administratively Approved, Month of September 2024

Adjourn

The meeting was adjourned at 7:44 p.m.

ATTEST: _____
City Clerk

Mayor

Approval Date of Minutes: November 4, 2024

Certification

I hereby certify that the foregoing minutes are a true and correct copy of the minutes of the Study Session of the City Council of Chandler, Arizona, held on the 14th day of October 2024. I further certify that the meeting was duly called and held and that a quorum was present.

DATED this _____ day of November, 2024.

City Clerk

Meeting Minutes

City Council Regular Meeting

October 17, 2024 | 6:00 p.m.
Chandler City Council Chambers
88 E. Chicago St., Chandler, AZ



Call to Order

The meeting was called to order by Mayor Kevin Hartke at 6:02 p.m.

Roll Call

Council Attendance

Mayor Kevin Hartke
*Vice Mayor OD Harris
*Councilmember Angel Encinas
Councilmember Christine Ellis
Councilmember Mark Stewart
Councilmember Matt Orlando
Councilmember Jane Poston

Appointee Attendance

Tadd Willie, Assistant City Manager
Kelly Schwab, City Attorney
Dana DeLong, City Clerk

*Vice Mayor Harris and Councilmember Encinas attended telephonically.

Invocation

The invocation was given by Dr. Rae Langness, Baha'i Faith.

Pledge of Allegiance

The Pledge of Allegiance was led by Councilmember Stewart.

Scheduled Public Appearances

MAYOR HARTKE invited Councilmember Orlando to join him for the recognitions.

1. Proclamation - Cybersecurity Awareness Month.

MAYOR HARTKE invited Mitchel Robinson, IT Security Program Manager, Michael Padilla, IT Security Analyst, and Brad Thomas, IT Security Architect.

Brad Thomas introduced the cybersecurity team, highlighting Senior Security Analysts Michael Padilla and Mitchell Robinson, who have been with the team for 20 years. As the acting City Chief Security Officer, he replaces Mr. Owen Zorge, who has 30 years of government service. Mr. Thomas thanked the city leadership and thanked the Mayor and Council for their support. He emphasized the vital role of the residents, stating that without them, there would be no city to defend.

2. Recognition - 2024 Certificate of Achievement for Excellence in Financial Reporting.
MAYOR HARTKE recognized Dawn Lang, Deputy City Manager/Chief Financial Officer and the Accounting team.

Consent Agenda and Discussion

Airport

1. Professional Services Agreement No. AI2501.201, with Dibble & Associates Consulting Engineers, Inc., for Hangar Area Pavement Reconstruction Design Services
Move City Council award Professional Services Agreement No. AI2501.201 to Dibble & Associates Consulting Engineers, Inc., for the Hangar Area Pavement Reconstruction Design Services, for an amount not to exceed \$328,578.

City Clerk

2. Resolution No. 5845, Setting the 2025 City Council Regular Meeting Schedule
Move City Council pass and adopt Resolution No. 5845, setting the 2025 City Council Regular Meeting Schedule.
3. Boards and Commissions Member Appointments
Move City Council approve the Board and Commission appointments as recommended.

Community Services

4. Agreement No. CS4-988-4668, Amendment No. 1, for Park Mowing Services
Move City Council approve Agreement No. CS4-988-4668, Amendment No. 1, for Park Mowing Services, with Artistic Land Management, Inc., in an amount not to exceed \$628,950.20, for a one-year term, November 1, 2024, through October 31, 2025.
5. Agreement No. CS4-988-4669, Amendment No. 1, for Park Herbicide Application Services
Move City Council approve Agreement No. CS4-988-4669, Amendment No. 1, for park herbicide application services, with BrightView Landscape Services, Inc., in an amount not to exceed \$320,798.77, for a one-year term, November 1, 2024, through October 31, 2025.
6. Purchase of Furniture for Tumbleweed Recreation Center (TRC) Expansion
Move City Council approve the purchase of furniture for Tumbleweed Recreation Center (TRC) expansion, utilizing the State of Arizona Contract No. CTR067414, with Wist Office Products, in an amount not to exceed \$149,546.35.

7. Sole Source Purchase of Pool Filters
Move City Council approve the sole source purchase of pool filters, from Aquatic Environmental Systems, Inc., an Aquafinity Company, in an amount not to exceed \$221,681.55.

Cultural Development

8. Purchase of Downtown Wayfinding Signage - Phase 1
Move City Council approve the purchase of downtown wayfinding signage, from YESCO, LLC, utilizing the City of Peoria Contract No. P19-0079, in the amount of

Development Services

9. Introduction and Tentative Adoption of Ordinance No. 5110 Rezoning, Preliminary Development Plan, and Preliminary Plat, PLH24-0001/PLT24-0001, Toll Brothers Uptown, Located Approximately 1/4 Mile North of the Northeast Corner of Arizona Avenue and Warner Road
Rezoning
Move City Council introduce and tentatively adopt Ordinance No. 5110 approving PLH24-0001 Toll Brothers Uptown, Rezoning from Regional Commercial (C-3) to Planned Area Development (PAD) for multi-family residential, subject to the conditions as recommended by Planning and Zoning Commission.
Preliminary Development Plan
Move City Council approve Preliminary Development Plan PLH24-0001 Toll Brothers Uptown for site layout and building architecture, subject to the conditions as recommended by Planning and Zoning Commission.
Preliminary Plat
Move City Council approve Preliminary PLT24-0001 Toll Brothers Uptown, subject to the conditions recommended by Planning and Zoning Commission.
10. Introduction and Tentative Adoption of Ordinance No. 5112, Rezoning and Preliminary Development Plan, PLH24-0023 The District Downtown, Located at the Northwest Corner Arizona Avenue and the Loop 202 SanTan Freeway
Rezoning
Move City Council introduce and tentatively adopt Ordinance No. 5112 approving PLH24-0023 The District Downtown, Rezoning from PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) to PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet, subject to the conditions as recommended by Planning and Zoning Commission.
Preliminary Development Plan
Move City Council approve Preliminary Development Plan PLH24-0023 The District Downtown for site layout and conceptual building architecture, subject to the conditions as recommended by Planning and Zoning Commission.

11. Resolution No. 5841, Authorizing a License Agreement between Cablevision Lightpath, LLC, and the City of Chandler for the Use of Public Property for the Establishment of Class 4 and Class 5 Telecommunications Systems
Move City Council pass and adopt Resolution No. 5841, authorizing the Mayor to execute the license agreement between Cablevision Lightpath, LLC, and the City of Chandler for the use of facilities in the city's rights-of-way and public places to establish Class 4 and Class 5 Telecommunications Systems, and authorizing the City Manager or designee to execute other documents as needed to give effect to the agreement.
12. Resolution No. 5844, Authorizing an Amended and Restated License Agreement between Crown Castle Fiber, LLC, and the City of Chandler for the Use of Public Property for the Establishment of Class 4 and Class 5 Telecommunications Systems
Move City Council pass and adopt Resolution No. 5844, authorizing the Mayor to execute the amended and restated license agreement between Crown Castle Fiber, LLC, and the City of Chandler for the use of facilities in the city's rights-of-way and public places to establish Class 4 and Class 5 Telecommunications Systems, and authorizing the City Manager or designee to execute other documents as needed to give effect to the agreement.
13. Preliminary Development Plan, PLH22-0024 Cornerstone Ranch, 3999 S. Dobson Road, Located at the Northeast Corner of Dobson Road and Ocotillo Road
Move City Council approve Preliminary Development Plan PLH22-0024 Cornerstone Ranch, for modifications to existing signage, subject to the conditions as recommended by Planning and Zoning Commission.
14. Agreement No. DS5-918-4808, General Plan Update and Comprehensive Housing Plan
Move City Council approve Agreement No. DS5-918-4808, with Logan Simpson Design, Inc., for the General Plan Update and Comprehensive Housing Plan, in the amount of \$525,000.
15. Professional Services Agreement No. DS2402.201, with Greenlight Traffic Engineering, for the Traffic Signal at Gilbert Road and Amanda Boulevard Design Services
Move City Council award Professional Services Agreement No. DS2402.201 to Greenlight Traffic Engineering, for the Traffic Signal at Gilbert Road and Amanda Boulevard Design Services, in an amount not to exceed \$115,622.46.
16. Use Permit PLH24-0018, Pecos & McQueen Storage, Located at the Southeast Corner of Pecos and McQueen Roads
Move City Council approve Use Permit PLH24-0018, Pecos & McQueen Storage, allowing storage facility uses on the property, subject to the conditions recommended by Planning and Zoning Commission.
17. Entertainment Use Permit PLH24-0022 The Mexicano, 3095 W Chandler Boulevard, Generally Located at the Southwest Corner of Chandler Boulevard and Price Road

Move City Council approve Entertainment Use Permit PLH24-0022 The Mexicano to allow live indoor and outdoor entertainment, speakers and television screens, subject to the conditions recommended by Planning and Zoning Commission.

Facilities and Fleet

18. Agreement No. BF2-912-4485, Amendment No. 2, with ASR Construction Group, LLC; Crawford Mechanical Services, LLC; FPS Civil, LLC; DMS Companies, Inc., dba Hernandez Companies; Kowalski Construction, Inc.; MGC Contractors, Inc.; and Nickle Contracting, LLC, for General Building Maintenance Services

Move City Council approve Agreement No. BF2-912-4485, Amendment No. 2, with ASR Construction Group, LLC; Crawford Mechanical Services, LLC; FPS Civil, LLC; DMS Companies, Inc., dba Hernandez Companies; Kowalski Construction, Inc.; MGC Contractors, Inc.; and Nickle Contracting, LLC, for general building maintenance services, in an amount not to exceed \$3,205,000, for a period of one year, beginning September 1, 2024, through August 31, 2025.

19. Purchase of HVAC Equipment, Installation, and Services

Move City Council approve the purchase of heat, ventilation, and cooling (HVAC) equipment, installation, and services, utilizing multiple vendors available under the Omnia Partners Contract No. R200403, with TDI Industries, Inc.; Contract No. R200402, with Johnson Controls, Inc.; Contract No. R200401, with Daikin Applied Americas, Inc.; and Contract No. 15-JLP-023, with Trane U.S., Inc.; and the Mohave Educational Services Cooperative Contract No. 19F-SMC-0904, with Sun Mechanical Contracting, Inc., in a combined amount not to exceed \$2,535,000.

20. Purchase of Maintenance, Repairs and Purchase of Doors, Locking Systems, Door Hardware, and Operable Walls

Move City Council approve the utilization of the Mohave Educational Services Cooperative Contract No. 21H-DHP-0917, for maintenance, repairs, and purchase of bay doors, locking systems, door hardware, and operable walls, from DH Pace Company, Inc., in an amount not to exceed \$380,000, for the period of one year, September 17, 2024, through September 16, 2025.

21. Purchase of Access Controls and Security Equipment

Move City Council approve the purchase of access controls and security equipment, from APL Access & Security, Inc., utilizing the State of Arizona Contract No. CTR056377, in an amount not to exceed \$825,000.

22. Purchase of Vehicles and Upfitting

Move City Council approve the purchase of vehicles and upfitting, using multiple vendors under State of Arizona contracts, in a combined amount not to exceed \$4,314,940.

Human Resources

23. Resolution No. 5840, Renewal Request to Industrial Commission of Arizona for Continued Exemption from Requirement to Post Security for Self-Insured Workers' Compensation Program
Move City Council pass and adopt Resolution No. 5840, renewing the request to the Industrial Commission of Arizona for continued exemption from the requirement to post security for the Self-Insurance Workers' Compensation Program.
24. Agreement No. 4046, Calendar Year 2025 Amendment, with Anthem Life Insurance Company, for Public Safety Personnel Long-Term Disability Insurance
Move City Council approve Agreement No. 4046, Calendar Year 2025 Amendment, with Anthem Life Insurance Company, for public safety personnel long-term disability insurance, in an amount not to exceed \$200,000, for the period of one year, beginning January 1, 2025, through December 31, 2025, and authorize the City Manager or designee to extend the Agreement for an additional year, beginning January 1, 2026, through December 31, 2026 (CY2024), subject to the same terms and conditions.
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Management Services

26. Resolution No. 5838 Authorizing the Submittal of a Pass-Through Gaming Tribal Revenue Sharing Grant Application From Dignity Health Foundation on Behalf of Salt River Pima-Maricopa Indian Community
Move City Council pass and adopt Resolution No. 5838 approving an intergovernmental agreement between the Salt River Pima-Maricopa Indian Community and the City of Chandler for the distribution of a pass-through Indian gaming revenue sharing grant in the amount of \$50,000 to Dignity Health Foundation-East Valley; and authorizing submission of a related pass-through grant application for Post-Hospitalization Services at Dignity Health East Valley for Uninsured/Underinsured Hospital Patients in the East Valley.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 307042, a Series 12, Restaurant Liquor License, for Jeffrey Craig Miller, Agent, Phoenix Food Junction, LLC, DBA Honest Indian Restaurant, located at 1050 W. Ray Road, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308108.

28. License Series 12, Restaurant Liquor License Application for Theresa June Morse, Agent, Quik Burrito 3, LLC, DBA Maskadores Taco Shop
Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 305733, a Series 12, Restaurant Liquor License, for Theresa June Morse, Agent, Quik Burrito 3, LLC, DBA Maskadores Taco Shop, located at 2100 W. Chandler Boulevard, Suite 38, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308099.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 306182, a Series 12, Restaurant Liquor License, for Jeffrey Craig Miller, Agent, Chandler Spitz, LLC, DBA Spitz Mediterranean, located at 301 S. Arizona Avenue, Suite 2, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308080.
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Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 305467, a Series 12, Restaurant Liquor License, for Jared Michael Repinski, Agent, Gather 336, LLC, DBA George & Gather, located at 336 S. Washington Street, Suite 3, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 308051.
33. License Series 12, Restaurant Liquor License Application for Larry Warren White, Jr., Agent, Warren's Supper Club, LLC, DBA Warren's Supper Club

Move for recommendation to the State Department of Liquor Licenses and Control for approval of the State Liquor Job No. 281651, a Series 12, Restaurant Liquor License, for Larry Warren White, Jr., Agent, Warren's Supper Club, LLC, DBA Warren's Supper Club, located at 1040 N. 54th Street, and approval of the City of Chandler, Series 12, Restaurant Liquor License No. 307989.

Neighborhood Resources

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Police Department

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Public Works and Utilities

36. Resolution No. 5842 Authorizing the Acquisition of Temporary Construction Easements Required for the Kyrene Branch and Highline Canal Shared Use Path Project
Move City Council pass and adopt Resolution No. 5842 authorizing the acquisition of temporary construction easements required for the Kyrene Branch and Highline Canal Shared Use Path Project No. TP2202; authorizing the city's real estate administrator to sign the purchase agreement and any other documents necessary to facilitate the acquisition; authorizing eminent domain proceedings as needed to acquire said real property, and obtain immediate possession thereof; and, relocation assistance as may be required by law.
37. Approval of a Settlement with 13012 E. Chandler Heights, LLC, for the Acquisition of Road Right of Way, a Temporary Construction Easement, Drainage Easements, and Easements for Roosevelt Water Conservation District to Relocate their Irrigation Facilities and Improvements Required for the Chandler Heights Road Improvement Project from McQueen Road to Gilbert Road
Move City Council approve a settlement with 13012 E. Chandler Heights, LLC, in the amount of Three Hundred Thousand and no/100ths dollars (\$300,000.00) plus required statutory interest, for acquisition of road right-of-way, a temporary construction easement, drainage easements, easement for Roosevelt Water Conservation District required to relocate their facilities and improvements required for the Chandler Heights Road Improvement project from McQueen Road to Gilbert Road.
38. Agreement No. WW0-885-4192, Amendment No. 4, with Carbon Activated Corporation, for Activated Carbon

Move City Council approve Agreement No. WW0-885-4192, Amendment No. 4, with Carbo Activated Corporation, for activated carbon, in an amount not to exceed \$960,000, for a one-year period, October 1, 2024, through September 30, 2025.

39. Agreement No. PW5-885-4805, with Univar Solutions USA, LLC, for Aluminum Chlorohydrate
Move City Council approve Agreement No. PW5-885-4805, with Univar Solutions USA, LLC, for Aluminum Chlorohydrate, in an amount not to exceed \$130,000, for a one-year period, November 1, 2024, through October 31, 2025, with the option of up to four additional one-year extensions.
40. Agreement No. 4831, with Tetra Tech, Inc., for the Paseo Vista Recreation Area and Landfill Environmental Consulting Services
Move City Council approve Agreement No. 4831, with Tetra Tech, Inc., for the Paseo Vista Recreation Area and Landfill Environmental Consulting Services, in an amount not to exceed \$200,000, for a one-year period, November 1, 2024, through October 31, 2025, with the option of up to four one-year extensions.
41. Professional Services Agreement No. ST1804.451, with Consultant Engineering, Inc., for the Chandler Heights – Gilbert Road to Val Vista Drive Construction Management Services
Move City Council award Professional Services Agreement No. ST1804.451, to Consultant Engineering, Inc., for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive Construction Management Services, in an amount not to exceed \$1,035,646.10.
42. Construction Agreement No. ST1804.401 with Sunland Asphalt & Construction, LLC, for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive
Move City Council award Construction Agreement No. ST1804.401, to Sunland Asphalt & Construction, LLC, for the Chandler Heights Road Improvements – Gilbert Road to Val Vista Drive, in an amount not to exceed \$9,784,943.
43. Construction Manager at Risk (CMAR) Pre-Construction Services Agreement No. WA2203.251, with Achen-Gardner Construction, LLC, for Water Main Replacements
Move City Council award CMAR Pre-Construction Services Agreement No. WA2203.251, to Achen-Gardner Construction, LLC, for Water Main Replacements, in an amount not to exceed \$249,641.04.
44. Construction Manager at Risk Construction Services Agreement No. WA2105.402, with PCL Construction, Inc., for the Water Facilities Optimization Improvements - Frye Road Water Production Facility Rehabilitation
Move City Council award CMAR Construction Services Agreement No. WA2105.402, to PCL Construction, Inc., for the Water Facilities Optimization Improvements - Frye Road Water Production Facility Rehabilitation, in an amount not to exceed \$3,849,574.

45. Construction Agreement No. ST2101.401, Change Order No. 4, with Achen-Gardner Construction, LLC, for the Detroit Basin/Gazelle Meadows Park Storm System & Associated Improvements
Move City Council award Agreement No. ST2101.401, Change Order No. 4, to Achen-Gardner Construction, LLC, for the Detroit Basin/Gazelle Meadows Park Storm System & Associated Improvements, in the amount of \$295,637.89.
46. Purchase of Granular Activated Carbon
Move City Council approve the purchase of granular activated carbon, from Calgon Carbon Corporation, utilizing City of Phoenix Contract No. 133070, in an amount not to exceed \$1,059,468.
47. Purchase of Bucket Trucks
Move City Council approve the purchase of two bucket trucks, from Sanderson Ford, Inc., utilizing State of Arizona Contract No. CTR069471, in the amount of \$457,054.19.

Consent Agenda Motion and Vote

Mayor Hartke moved to approve the Consent Agenda of the October 17, 2024, Regular City Council Meeting; Seconded by Councilmember Ellis. Motion carried unanimously (7-0) with the exception of Item No. 2 which passed 6-1, Councilmember Poston dissenting. Item No. 10 passed unanimously, 7-0, amending the stipulations to include the following stipulation: The developer shall pull a building permit on a hotel or a minimum of 25,000 square feet of non-residential uses within 18 months of the effective date of the Rezoning Ordinance. If this condition is not satisfied, then the developer shall be subject to a daily penalty of \$100 until a building permit is pulled. Item No. 16 passed 5/2 with Vice Mayor Harris and Councilmember Encinas dissenting.

Informational

48. Special Event Liquor Licenses and Temporary and Permanent Extensions of Liquor License Premises Administratively Approved
49. August 21, 2024, Planning and Zoning Commission Meeting Minutes
50. Contracts and Agreements Administratively Approved, Month of September 2024

Unscheduled Public Appearances

George Richie, a resident of Chandler, AZ, expressed concerns about the Intel plant near Ocotillo and Dobson, calling it a nuisance. He has been trying to address noise complaints since 2008, noting that the noise can be heard as far as Bogle Junior High and Safeway at Chandler Heights. He often hears disturbing sounds in his home, even through closed windows. Mr. Richie has spoken with the plant manager, Aon Blond, and former Community Engagement Manager, Renee

Alvin, who acknowledged the issue during a visit. He also mentioned the intrusive smells from Intel's fleet of old buses and the overwhelming traffic in the area. He has requested that the police enforce speed limits on roads during early morning hours due to excessive activity. Mr. Richie hopes the city will appoint a staff member to work with him and Intel on these issues, particularly concerning the loud bursts of compressed air that startle residents.

Will Bullock, a resident of 598 E. Knead in Chandler, AZ, spoke about a bill for an alarm permit. He expressed his belief that this requirement is unnecessary, as it simply informs the police about an alarm system, which seems illogical for him to pay for annually. Bullock also noted that the city has outsourced this fee collection, suggesting it could have been added to water bills for convenience. He wishes to opt out of the program altogether. Mr. Bullock addressed the negative media coverage regarding the city and Vice Mayor Harris, emphasizing that if any allegations are true, it raises serious ethical concerns for residents. He called for action on this matter, acknowledging the ongoing investigation by Scottsdale and expressing his dissatisfaction with the situation, which he believes is a significant concern for the community.

Current Events

Mayor's Announcements

MAYOR HARTKE attended the grand opening of Trader Joe's, noting the excitement of standing in line with Councilmembers to welcome attendees. He thanked the Council, the new store team, and the Economic Development team for bringing Trader Joe's to Chandler. He highlighted the store as the community's most requested item for a decade and thanked citizens for their ongoing support and petitions. He encouraged everyone to visit the new store.

MAYOR HARTKE announced the kickoff for "For Our City Chandler," a volunteer initiative that has revitalized neighborhoods for the past 14 years. To avoid heat issues this year, the event will move from October to late March. The Neighborhood Resources team will host the kickoff event, where councilmembers will serve pancakes to volunteers from 7:00 AM to 8:30 AM at Pamela Park. Mark your calendars for the next "For Our City" day on Saturday, March 19. Another event is planned for October 19. For more information, visit our Neighborhood Services page at www.farourcitychandler.org.

MAYOR HARTKE announced the G.A.I.N event to enhance police-community partnerships and citizen involvement in safety. The Council will visit Chandler neighborhoods hosting events this weekend, eager to connect with residents on Saturday.

Council's Announcements

COUNCILMEMBER ELLIS announced that October is Domestic Violence Awareness Month. Join us for a candlelight vigil on Thursday, October 24 at 5:30 PM in the Chandler City Hall Courtyard. This annual event aims to raise awareness and support prevention efforts in our community. All are welcome. For more details, visit chandleraz.gov/candlelightvigil.

COUNCILMEMBER STEWART spoke about the upcoming election and noted that early voting centers are now open, allowing residents to cast their votes. He encouraged everyone to participate in the election and hoped their chosen candidates would win.

COUNCILMEMBER POSTON said that the Mayor and Council attended the long-awaited ribbon-cutting ceremony for Galveston Elementary School. This event highlighted the community's vision, planning, and input in the school's creation. The city is grateful for its ongoing partnership with CUSD and congratulates Principal Alala, his staff, students, and parents. The city is also investing significantly in the Galveston neighborhood to improve the area and the lives of local children. Notably, Principal Alcala is an alumnus of the school, which underscores his dedication to the students.

COUNCILMEMBER POSTON attended the Chamber's recent Women in Leadership event, which focused on self-defense. She found it particularly impactful. Officer Noah Dueker offered practical safety tips, and Joseph Bautista led interactive self-defense instruction. Councilmember Poston expressed her gratitude to Officer Dueker, believing this information is essential for everyone, especially women, to enhance their safety. She commended the Police Department for hosting the self-defense classes, which fill up quickly without much advertising. Councilmember Poston encouraged community members, particularly those who travel alone at night, to take advantage of these opportunities and wanted to express her appreciation for the event.

City Manager's Announcements

None.

Adjourn

The meeting was adjourned at 6:34 p.m.

ATTEST: _____
City Clerk

Mayor

Approval Date of Minutes: November 4, 2024

Certification

I hereby certify that the foregoing minutes are a true and correct copy of the minutes of Regular Meeting of the City Council of Chandler, Arizona, held on the 17th day of October 2024. I further certify that the meeting was duly called and held and that a quorum was present.

DATED this _____ day of November, 2024.

City Clerk



City Council Memorandum Development Services Memo No. 24-052

Date: November 04, 2024
To: Mayor and Council
Thru: Joshua H. Wright, City Manager
 Andy Bass, Deputy City Manager
 Kevin Snyder, Development Services Director
From: David De La Torre, Planning Manager
Subject: PLH24-0001/PLT24-0001 Toll Brothers Uptown
 Final Adoption of Ordinance No. 5110
Request: Rezoning from Regional Commercial District (C-3) to Planned Area
 Development (PAD) for multi-family residential
Location: located approximately 1/4 mile north of the northeast corner of Arizona
 Avenue and Warner Road
Applicant: Jason Morris, Whitey Morris Baugh PLC

Proposed Motion:

Move City Council adopt Ordinance No. 5110 approving PLH24-0001 Toll Brothers Uptown, Rezoning from Regional Commercial (C-3) to Planned Area Development (PAD) for multi-family residential, subject to the conditions as recommended by Planning and Zoning Commission.

Background Data:

- The subject site is approximately 14.1 net acres and currently used as an auto dealership.
- The site is adjacent to another multi-family development currently under construction to the north and a retail development south of the site.
- The General Plan's designation of the North Arizona Avenue Growth Area, High Capacity Transit Corridor, and Employment allow for high density residential to be considered.
- The project consists of 420 dwelling units totaling 30 dwelling units per acre.

Surrounding Land Use Data:

North	Planned Area Development (PAD) for Multi-Family	South	PAD for Commercial
East	PAD for Multi-Family	West	Arizona Avenue, then Regional Commercial District (C-3)

General Plan and Area Plan Designations:

	Existing	Proposed
General Plan	North Arizona Avenue Growth Area, High Capacity Transit Corridor, Employment	No Change

Proposed Development

Proposed Land Use	<ul style="list-style-type: none"> • 420 multifamily dwelling units • 30 dwelling units per acre (du/ac) (staff proposes a maximum of 35 dwelling units per acre to allow for flexibility during the construction phase)
Building Setbacks	<ul style="list-style-type: none"> • Arizona Avenue (west): 20' • North Property line: 10' • South Property line: 10' • East Property line: 20'
Height	47' Parapet/53' Architectural Features (staff proposes a max.height of 55' to permit flexibility during the construction phase)
Roof Design	Varied Parapet Heights
Facade Material	<ul style="list-style-type: none"> • Stucco • Masonry Veneer • Metal Screens • Fiber Cement Panels
Building Accents	<ul style="list-style-type: none"> • Tower Element • Vertical Metal Screen Elements • Multi-height Parapets

Site Design & Open Space	<ul style="list-style-type: none"> • (6) four-story buildings, two (2) of which will front onto Arizona Avenue. Two internal buildings wrap the primary common area, and the remaining two (2) buildings are located toward the back of the development. • Amenities include event and activity lawns, fire pits and grilling stations, putting green, ball court and game tables, dog park, tot lot and pickleball court
Parking	<ul style="list-style-type: none"> • Required: 803 parking spaces • Provided: 803 parking spaces, including private garages, covered and uncovered spaces

Review and Recommendation

The subject site is zoned Regional Commercial District (C-3) and has contained a car dealership since 1986. The site is located within the General Plan's North Arizona Avenue Growth Area that encourages higher densities to support future transit efforts along Arizona Avenue, a high capacity transit corridor. The guidance supports the consideration of multi-family with densities exceeding 18 dwelling units per acre within the larger northern Chandler employment area.

Further policies for this growth area call for pedestrian-oriented improvements, transforming commercial centers into intense mixed-use developments, transitioning land use intensities to be respectful to existing neighborhoods, and enhancing Arizona Avenue as a point of entry through design. The proposed multi-family development aligns with the policies of the North Arizona Avenue Growth Area and the intent of the High-Capacity Transit Corridor, as identified within the General Plan, by increasing density and continuing to implement redevelopment.

The development presents a crafted regional/contextual eclectic design, referencing local characteristics of the existing built environment while considering the dynamic growing nature of this area of Chandler. All buildings feature four-sided architecture, with harmonious transition between the horizontal and vertical volumes articulated through massing and material, providing elements of human scale to the architecture. Redevelopment of the site to multi-family is appropriate and provides more housing within the City's boundaries. For these reasons, staff finds the proposed multi-family development aligns with the goals and policies within the General Plan. Planning and Zoning Commission recommends approval.

Traffic Analysis

A Traffic Impact Study (TIS) was provided by the applicant. The TIS has been reviewed and accepted by City traffic engineering staff.

Public / Neighborhood Notification

- This request was noticed in accordance with the requirements of the Chandler Zoning Code.
- A neighborhood meeting sign was on the site and on social media via NextDoor.
- A neighborhood meeting was held on July 18, 2024. There were no residents in attendance.
- As of the writing of this memo, Planning staff is not aware of any opposition to the request.

Planning and Zoning Commission Vote Report

Planning and Zoning Commission meeting, September 18, 2024

Motion to Approve

In Favor: 6 Opposed: 0 Absent: 1 (Lopez)

Ordinance No. 5110 was introduced and tentatively adopted on October 17, 2024

Recommended Conditions of Approval

Planning and Zoning Commission recommends the City Council approve the rezoning from Regional Commercial District (C-3) to Planned Area Development (PAD) for multi-family residential subject to the following conditions:

1. Development shall be in substantial conformance with the Development Booklet, entitled "Toll Brothers Uptown" and kept on file in the City of Chandler Planning Division, File No. PLH24-0001, modified by such conditions included at the time the Booklet was approved by the Chandler City Council and/or as thereafter amended, modified or supplemented by Chandler City Council.
2. High-density residential shall be permitted at a maximum density of thirty-five (35) dwelling units per acre.
3. Building height shall be limited to a maximum of fifty-five (55) feet in height.

4. Completion of the construction of all required off-site street improvements including but not limited to paving, landscaping, curb, gutter and sidewalks, median improvements, and street lighting to achieve conformance with City codes, standard details, and design manuals.
5. The landscaping in all open-spaces shall be maintained by the property owners' association, and shall be maintained at a level consistent with or better than at the time of planting.
6. The landscaping in all rights-of-way shall be maintained by the adjacent property owner or property owners' association.
7. Minimum building setbacks shall be as follows:

Property Line Location	Minimum Building Setback
Arizona Avenue	20'
North	10'
South	10'
East	20'

Attachments

Ordinance No. 5110
 Vicinity Maps
 Development Booklet
 Community Support
 Traffic Study

ORDINANCE NO. 5110

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF CHANDLER, ARIZONA, AMENDING THE ZONING CODE AND MAP ATTACHED THERETO, BY REZONING A PARCEL FROM REGIONAL COMMERCIAL (C-3) TO PLANNED AREA DEVELOPMENT (PAD) FOR MULTI-FAMILY RESIDENTIAL IN CASE PLH24-0001 (TOLL BROTHERS UPTOWN) LOCATED APPROXIMATELY ¼ MILE NORTH OF THE NORTHEAST CORNER OF ARIZONA AVENUE AND WARNER ROAD WITHIN THE CORPORATE LIMITS OF THE CITY OF CHANDLER, ARIZONA; PROVIDING FOR THE REPEAL OF CONFLICTING ORDINANCES; AND PROVIDING FOR PENALTIES.

WHEREAS, an application for rezoning certain property within the corporate limits of Chandler, Arizona, has been filed in accordance with Article XXVI of the Chandler Zoning Code; and

WHEREAS, the application has been published in a local newspaper with general circulation in the City of Chandler, giving fifteen (15) days' notice of the time, place, and date of public hearing; and

WHEREAS, a notice of such hearing was posted on the property at least seven (7) days prior to the public hearing; and

WHEREAS, the City Council has considered the probable impact of this ordinance on the cost to construct housing for sale or rent; and

WHEREAS, a public hearing was held by the Planning and Zoning Commission as required by the Zoning Code.

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Chandler, Arizona, as follows:

Section 1. Legal Description of Property:

EXHIBIT 'A'

Said parcel is hereby rezoned from Regional Commercial (C-3) to Planned Area Development (PAD) for high-density residential, subject to the following conditions:

1. Development shall be substantial conformance with the Development Booklet entitled "Toll Brothers Uptown" and kept on file in the City of Chandler Planning Division, in File No. PLH24-0001, modified by such conditions included at the time the Booklet was approved by the Chandler City Council and/or as thereafter amended, modified or supplemented by Chandler City Council.

2. High-density residential shall be permitted at a maximum density of thirty-five (35) dwelling units per acre.
3. Building height shall be limited to a maximum of fifty-five (55) feet in height.
4. Completion of the construction of all required off-site street improvements including but not limited to paving, landscaping, curb, gutter and sidewalks, median improvements and street lighting to achieve conformance with City codes, standard details, and design manuals.
5. The landscaping in all open-spaces shall be maintained by the property owner or property owners' association, and shall be maintained at a level consistent with or better than at the time of planting.
6. The landscaping in all right-of-way shall be maintained by the adjacent property owner or property owners' association.
7. Minimum setbacks shall be as follows:

Property Line Location	Minimum Building Setback
Arizona Avenue	20'
North	10'
South	10'
East	20'

Section 2. The Planning Division of the City of Chandler is hereby directed to enter such changes and amendments as may be necessary upon the Zoning Map of said Zoning Code in compliance with this Ordinance.

Section 3. All ordinances or parts of ordinances in conflict with the provisions of this Ordinance, or any parts hereof, are hereby repealed.

Section 4. In any case, where any building, structure, or land is used in violation of this Ordinance, the Planning Division of the City of Chandler may institute an injunction or any other appropriate action in proceeding to prevent the use of such building, structure, or land.

Section 5. If any section, subsection, sentence, clause, phrase or portion of this Ordinance is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, then this entire ordinance is invalid and shall have no force or effect.

Section 6. A violation of this Ordinance shall be a Class 1 misdemeanor subject to the enforcement and penalty provisions set forth in Section 1-8.3 of the Chandler City

Code. Each day a violation continues, or the failure to perform any act or duty required by this Ordinance or the Zoning Code, shall constitute a separate offense.

INTRODUCED AND TENTATIVELY APPROVED by the City Council of the City of Chandler, Arizona, this ____ day of _____, 2024.

ATTEST:

CITY CLERK

MAYOR

PASSED AND ADOPTED by the City Council of the City of Chandler, Arizona, this ____ day of _____, 2024.

ATTEST:

CITY CLERK

MAYOR

CERTIFICATION

I HEREBY CERTIFY that the above and foregoing Ordinance No. 5110 was duly passed and adopted by the City Council of the City of Chandler, Arizona, at a regular meeting held on the ____ day of _____, 2024, and that a quorum was present thereat.

CITY CLERK

APPROVED AS TO FORM:

CITY ATTORNEY TA

Published:

Exhibit "A"

Legal Description

THAT PORTION OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID SECTION 15, BEING MARKED BY A BRASS CAP IN HAND HOLE, FROM WHICH POINT THE WEST QUARTER CORNER OF SAID SECTION 15, BEING MARKED BY A BRASS CAP FLUSH, BEARS NORTH 00°00'00" EAST, A DISTANCE OF 2654.12 FEET;

THENCE NORTH 00°00'00" EAST, ALONG THE WEST LINE OF SAID SOUTHWEST QUARTER, A DISTANCE OF 955.01 FEET TO THE SOUTH LINE OF THE NORTH 1100 FEET OF THE SOUTH 2055 FEET OF SAID SOUTHWEST QUARTER, SAID POINT BEING THE POINT OF BEGINNING;

THENCE CONTINUING NORTH 00°00'00" EAST, ALONG THE WEST LINE OF SAID SOUTHWEST QUARTER, A DISTANCE OF 676.01 FEET TO THE NORTH LINE OF THE SOUTH 1631 FEET OF SAID SOUTHWEST QUARTER;

THENCE NORTH 89°40'45" EAST, ALONG SAID NORTH LINE, A DISTANCE OF 510.00 FEET;

THENCE SOUTH 00°19'15" EAST, A DISTANCE OF 111.00 FEET TO THE NORTH LINE OF THE SOUTH 1520 FEET OF SAID SOUTHWEST QUARTER;

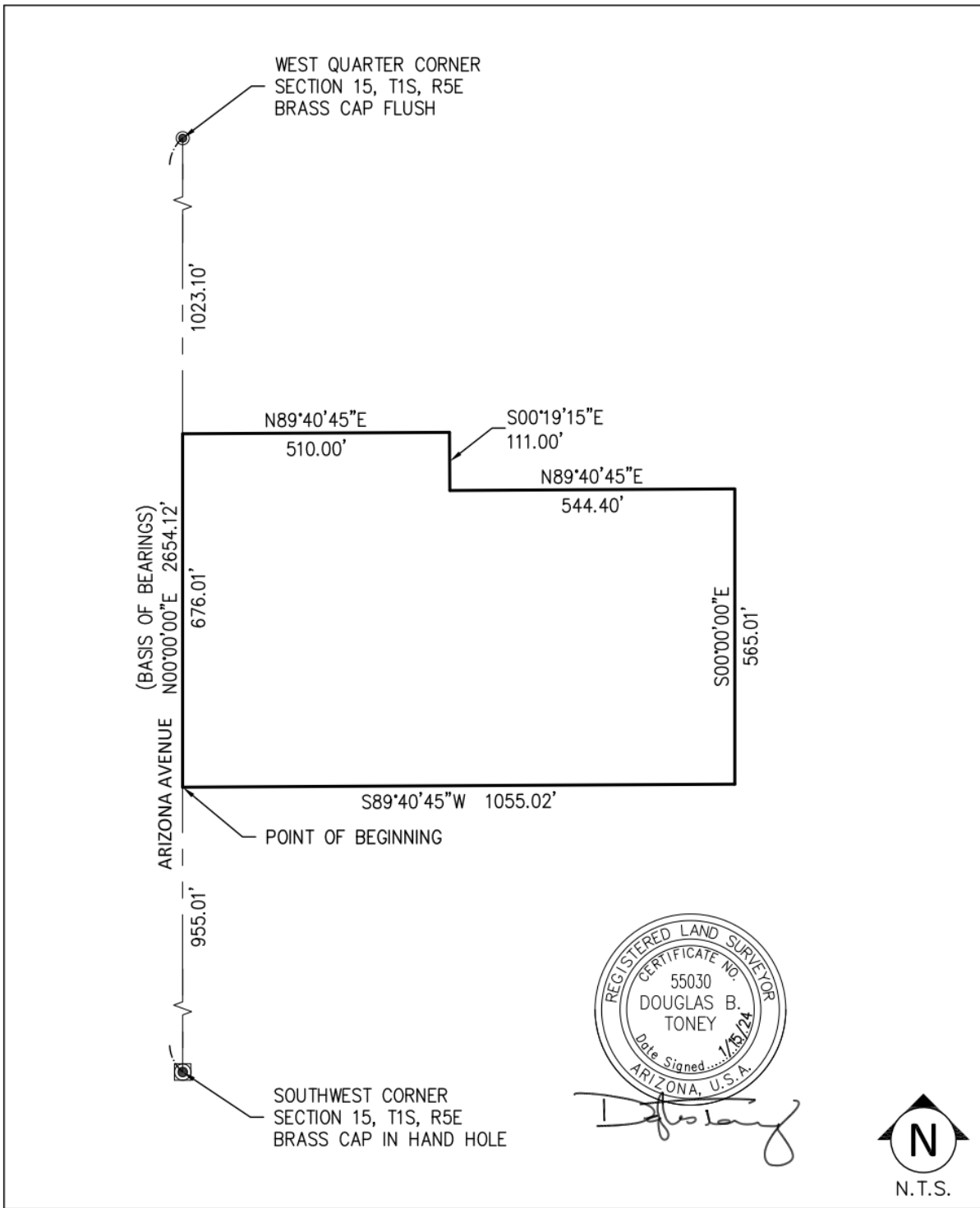
THENCE NORTH 89°40'45" EAST, ALONG SAID NORTH LINE, A DISTANCE OF 544.40 FEET TO THE EAST LINE OF THE WEST 1055 FEET OF SAID SOUTHWEST QUARTER;

THENCE SOUTH 00°00'00" EAST, ALONG SAID EAST LINE, A DISTANCE OF 565.01 FEET TO THE SOUTH LINE OF THE NORTH 1100 FEET OF THE SOUTH 2055 FEET OF SAID SOUTHWEST QUARTER;

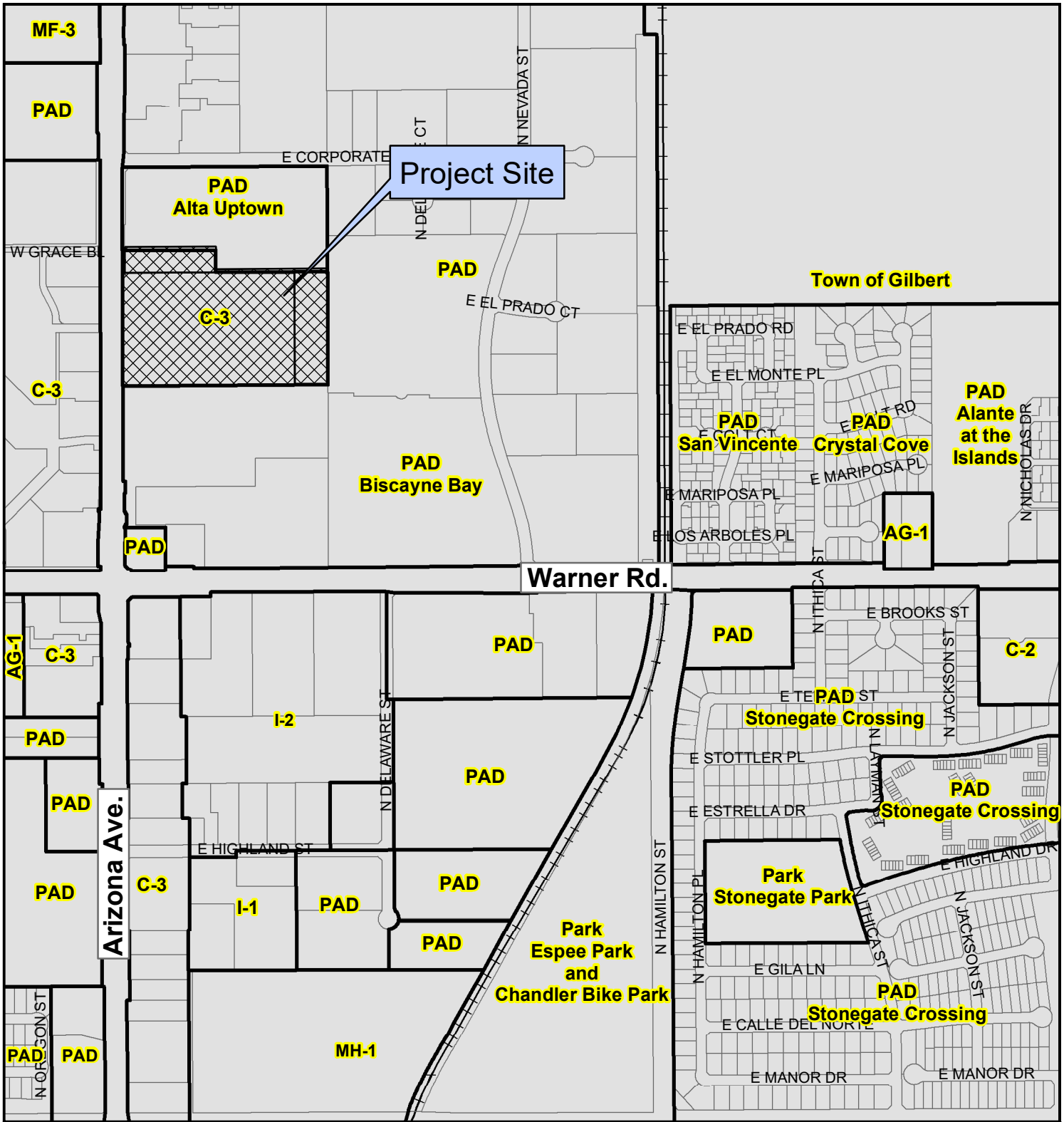
THENCE SOUTH 89°40'45" WEST, ALONG SAID SOUTH LINE, A DISTANCE OF 1055.02 FEET TO THE POINT OF BEGINNING.

CONTAINING 652,729 SQ.FT. OR 14.9846 ACRES, MORE OR LESS.





SHEET 1 OF 1	EXHIBIT REZONE BOUNDARY				1600 N Desert Drive, #210 Tempe, AZ 85288 Phone: (480) 629-8830 Fax: (480) 629-8841 www.bowman.com
	BY: DT	CHK:	QC:		
	BCG PROJECT NO: 051622-01 TASK: 001 CLIENT REF NO:				



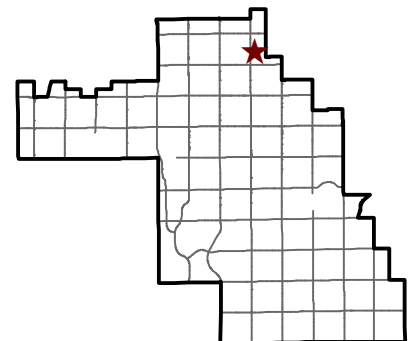
 **PLH24-001 Toll Brothers Uptown**

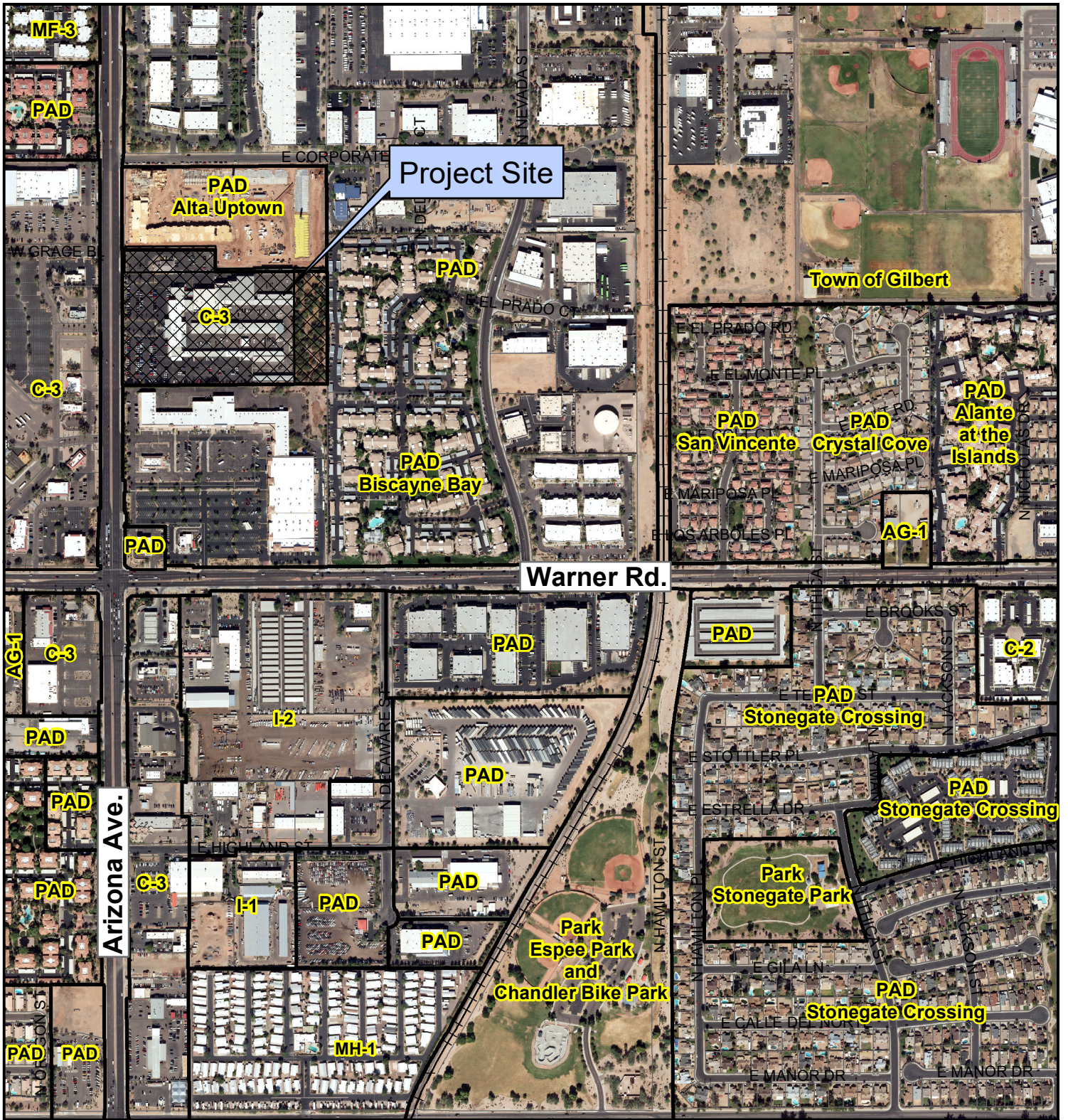
Proposed Project Details

Rezone/PDP
 Approximately 14.1 acres
 Multi-Family Residential (30 du/ac)



City of Chandler Planning Division
chandleraz.gov/planning
 For more information visit:
<https://gis.chandleraz.gov/planning>





PLH24-001 Toll Brothers Uptown



Proposed Project Details

Rezone/PDP

Approximately 14.1 acres

Multi-Family Residential (30 du/ac)

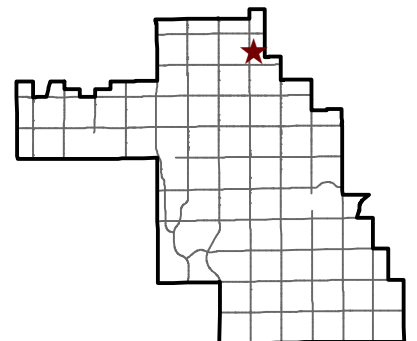


City of Chandler Planning Division

chandleraz.gov/planning

For more information visit:

<https://gis.chandleraz.gov/planning>



Toll Brothers Uptown

Rezoning, Preliminary Development Plan and
Preliminary Plat Request

Case No. PLH24-0001 & PLT24-0001

Submittal: September 3, 2024



WITHEY
MORRIS
BAUGH

Development Team

Developer:



8767 E Via De Ventura, Ste 270
Scottsdale, AZ 85258

Legal Representative:

Withey Morris Baugh PLC

Jason Morris / Stephanie Watney, AICP
2525 East Arizona Biltmore Circle, Suite A-212
Phoenix, Arizona 85016

Architect:

Biltform Architecture

11460 North Cave Creek Road, Suite 11
Phoenix, Arizona, 85020

Civil Engineer:

Bowman:

2420 S. Power Road, Suite 105
Mesa, Arizona, 85209

Traffic Engineer:

Lōkahi, LLC:

10555 N. 114th Street, Suite 105
Scottsdale, AZ 85259

Landscape Architect:

Collaborative Design Studio

7116 E. 1st Avenue, Suite 103
Scottsdale, Arizona, 85251

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I. PLANNED AREA DEVELOPMENT

A. Introduction and Project Overview

On behalf of Toll Bros., Inc, a national developer of high-quality multi-family communities, this application is a request for approval of a Planned Area Development (PAD), Preliminary Development Plan (“PDP”) and Preliminary Plat for the approximately 14.1 net acre property located at the southeast corner of Arizona Avenue and Grace Boulevard, otherwise commonly known as Maricopa County Assessor’s Parcel Numbers 302-29-009E, -009G and -009L (cumulatively, the “Property”) (See **Tab A, Aerial Map**). The Property is well-positioned along Arizona Avenue, a major arterial and designated High-Capacity Transit Corridor, with adjacency to Chandler Mercado, which features a number of experience-oriented commercial tenants, restaurants and more.

As background, the Property was developed approximately 40 years ago as a car dealership, better known today as Earnhardt Chevrolet. To the north of the Property is an in-progress multi-family development which was formerly a parking lot that was used for Earnhardt’s related operations. That site was recently rezoned to a PAD to allow multi-family development due to market circumstances. Similarly, because of the inflationary environment and interest rates, together with locational attributes of the dealership, the Property and viability of it as an auto dealership has been impacted. Conversely, there is a significant statewide shortage of housing, and demand for quality housing – including within this area of Chandler. As such, this PAD provides a framework for re-development of the Property as a high-quality multi-family apartment community (the “Project”).

As detailed herein, the intent of this PAD is to establish site-specific permitted uses, as well development standards and design criteria for redevelopment of the Property as a high-quality market rate multi-family apartment community which will help fill a need for quality and diverse residential housing proximate to transit, existing/future retail, and employment opportunities within the highly anticipated Uptown Chandler area. The proposed Project is a four-story, 420-unit multi-family community with an overall density of 30 dwelling units per acre. The Project will include a mix of studio (44), one-bedroom (213), two-bedroom (131) and three-bedroom (32) unit configurations ranging from 650 square feet to 1,450 square feet. This unit mix offers choice and options to accommodate a variety of lifestyles and transitions, from young graduates to retirees looking for a maintenance free, lock-and-leave lifestyle.

The proposed Toll Brothers Uptown multi-family community is in a unique location along a corridor anticipated for growth. It will be compatible with existing land use patterns in the area and help synergize the area with new residents, which will in turn support other nearby land uses.

B. Relationship to Adjacent Properties

The area surrounding the Property consists of a multi-family development to the east (Zoned PAD); a commercial shopping center to the south (Zoned PAD), which features a number of restaurant and activity-based tenants – all within walking distance of the Property; Arizona Avenue to the west, with a

variety of shops and offices beyond the arterial street (Zoned C-2); and future multi-family to the north (Zoned PAD).

C. General Plan and Zoning

The City of Chandler General Plan Future Land Use Map and Circulation Plan Map designates the Property as Employment / North Arizona Avenue Growth Area, and High Capacity Transit Corridor, respectively. Notably, the General Plan provides that residential densities may be considered in an Employment designation with a compatible mix of uses and an integral component, as described in growth area policies, or otherwise.

Further, the General Plan provides that multi-family “can be considered... within designated high-capacity transit corridors”. As noted, the Property is located within a High-Capacity Transit Corridor, thus this request conforms to the vision of the General Plan. Further, the North Arizona Avenue Growth encourages higher densities to support future transit-oriented efforts along Arizona Avenue, a designated high-capacity transit corridor.

The existing zoning for the Property is C-3 District. As such, this PAD/PDP is requested to rezone the Property and allow multi-family residential as a permitted use at the Property, and to establish site layout and building architecture for the Project, as detailed further below.

D. Permitted Uses

1. Multi-family residential
2. Accessory uses for multi-family residential per Zoning Code Section 35-1001.

II. Preliminary Development Plan

The PDP establishes the site and building standards for development of the Property.

A. Development Standards

The following development standards are proposed for the Project:

Table 1 – Development Standards	
Development Standard	PAD
Building Height	47' (Parapet) 53' (Architectural Features)
Building Setbacks (Min) Front (Arizona Avenue) Side (North Property Line)	20' 10'

Side (South Property Line)	10'
Rear (East Property Line)	20'
Perimeter Landscape Setbacks (Min)	
Front (Arizona Avenue)	20' ¹
Side (North Property Line)	10' ^{2,3}
Side (South Property Line)	10'
Rear (East Property Line)	20'
Min. Accessory Structure Setbacks (Parking Canopies)	10'
Lot Coverage (Max)	30%
Common Open Space	136,804 Square Feet (222 Square Feet/Bedroom)

Parking lot landscape area: In locations on the site plan where the frequency of parking lot planter islands exceeds one (1) planter/ten (10) spaces, additional 9-foot-wide islands may accommodate pedestrian sidewalks to promote pedestrian connectivity, provided that a minimum of five (5) shrubs is provided per island. Where a minimum of four (4) feet of clearance is provided, trees may also be accommodated within select islands, where feasible. Additional islands, not to exceed five (5) total, may also accommodate pedestrian sidewalks as provided above. This minor relief will allow for the required parking to be maintained while meeting the intent of landscape requirements.

B. Site Layout and Design

As designed, the proposed multi-family development features an urban design appropriate for adjacency to a high-capacity transit corridor. The main entry drive to the community includes buildings flanking either side of the entry drive, with the club house as the focal point, which provides a grand sense of arrival. The proposed development consists of six (6) four-story buildings, two (2) of which will front onto Arizona Avenue. Two (2) internal buildings wrap the primary common area, and the remaining two (2) buildings are located toward the back of the development. The height of the buildings is approximately 47 feet to the top of parapet, however the height is strategically extended in some key locations to a 53 feet. This additional height allows for architectural features and embellishment.

As shown on the enclosed conceptual site plan (**Tab B**), the proposed community will be highly amenitized with several meaningful amenity areas, to conceptually include the following:

- +/- 10,800 square foot club house and leasing office;
- Resort style swimming pool area featuring two (2) full-sized swimming pools, spa, sun deck, beach lawn, and pool cabanas;
- Entertainment pavilion with outdoor kitchen;

¹ The landscape setback along Arizona Avenue may include encroachment of view fencing to promote functional use of front yard open space.

² The landscape setback for the first 460 feet of the north (west) perimeter may be reduced to 2 feet to accommodate driveway access and recreational appurtenances. Any such reduction shall not exceed 250 consecutive linear feet.

³ Trash compactor/recycling enclosures are permitted to encroach into the landscape setback.

- Event and activity lawns;
- Fire pits and grilling stations;
- Putting green;
- Ball courts and game tables;
- Dog park;
- Tot lot;
- Pickleball court;
- Pedestrian linkages to/from and throughout the Project, fostering connectivity and a sense of community;
- Alternative amenities analogous to the list above may be approved administratively by the Zoning Administrator in order to accommodate easement or utility conflicts.

The proposed development provides approximately 145,392 square feet of ground floor building area (23.6% lot coverage) and 171,284 square feet (26.2%) of landscaped area, including landscape medians. Primary vehicular access is provided via the main drive along the center of the Project's Arizona Avenue frontage. This access provides efficient access to the clubhouse and leasing office, while ensuring gate-controlled access to the larger community. A secondary full access is provided along the north end of the Project's frontage, which aligns with Grace Boulevard to the west. Additionally, an emergency access only driveway is provided along the south project boundary adjacent to Arizona Avenue. Finally, solid waste is provided to the east of the most northerly driveway and—due to the distance from the balance of units—trash valet is contemplated for resident convenience.

C. Architectural Design and Theme

The architectural approach is a crafted regional/contextual eclectic design, referencing local characteristics of the existing built environment while considering the dynamic and growing nature of this area of Chandler. The two buildings fronting Arizona Avenue flank the project's entry, with impactful tower elements emblazoned with the project name and articulated with vertical metal screen elements. These screens reference a similarly distinctive feature found on nearby Chandler City Hall. At the terminus of the project's entry is another building that surrounds a generously sized and landscaped amenity courtyard. All the buildings feature four-sided architecture, with a harmonious transition between the horizontal and vertical volumes that is articulated through massing and material, providing elements of human scale to the architecture. Use of multi-height parapets, individual deep balcony spaces with a variety of undulation treatments and spacious patios at the ground level provide visual interest and depth to the elevations. The buildings' fenestration creates varying patterns of gridded windows and grouped openings framed within larger scaled massing.

The architectural design is complemented by a desert appropriate landscape palette that surrounds the buildings, providing shade, character and greenery to the pedestrian experience. The central amenity area, a key feature to the community, can be accessed through a well-amenitized club house, providing a pleasing architectural transition to variety of exterior gathering spaces that promote human interaction. To further benefit the resident experience there is also an approximately 2.04-acre landscaped open space area located at the rear of the site, whereby the adjacent buildings are sited lengthwise to take advantage of views. See **Tab C, Perspective Renderings** and **Tab E, Elevations**.

The elevations include complementary colors, materials and textures that take cues from the surrounding area, while also providing a residential look and feel to the architecture. The exterior color palette balances neutral white and contrasting gray hues with darker warm colors of brown lap siding and stucco. In working with staff, the elevations have been fine tuned to include: block-like cladding material that wraps the two buildings facing Arizona Avenue and the clubhouse building; textured masonry veneer at prominent visible locations across all the buildings; elongated bracket detail at the metal canopies; and, distinctive white frame / brown inset articulation primarily at the building corners. These changes have contributed to overall enhancement of the project design. Masonry veneer enhances the base of the buildings with character and texture. See **Tab F, Colors and Materials Palette**.

D. Landscaping and Walls

The Community has been designed with ample open space and recreation areas comprised of quality amenities distributed throughout. The open space and shared recreation areas are intended to encourage resident interaction and create a sense of place. The Community also features a well-planned landscape theme which will contribute to a more livable environment.

The Project's frontage is planned with formal placements of Southern Live Oak and Wilson Olive trees, as well as bold massings of shrubs and accents, resulting in a strong formal streetscape design. Color, texture and massings will create visual interest for people and cars passing by. See **Tab D, Landscape Plan**.

A conceptual wall plan is provided at **Tab D**. The wall plan identifies the north and south boundaries of the Property enclosed with a 6-foot-tall wall. There is an existing wall along the east boundary, which will be painted to match the theme of the Project. Along the Project's Arizona Avenue frontage are a series of yards enclosed with a 6-foot-tall decorative view fence.

E. Parking

There are 803 parking spaces required and provided for the proposed development, for an overall ratio of 1.91 parking spaces per unit. This provided parking includes 135 garage parking spaces, 135 tandem parking spaces, 285 covered parking spaces, and 248 open parking spaces. This ratio meets the requirements for guest parking and will mitigate spillover within the larger area. The overall parking ratio includes 16 accessible parking spaces, and Electric Vehicle parking will be provided at a rate of six percent for a total of 49 spaces for both EV ready and EV capable charging stations.

F. Signage

Conceptual building and monument signage is enclosed herein (**Tab G**). There are two (2) 6-foot-tall project monument signs proposed along the Arizona Avenue frontage, at the main entrance to the development. The proposed monument signage features a masonry veneer base, as well as reverse pan channel lettering pegged off of a painted stucco finish and rear illuminated. The proposed signage is architecturally integrated with the project elevations to ensure a cohesive appearance.

The sign plan also proposed two (2) street facing wall mounted signs, with a maximum sign area of 50 square feet each.

G. Phasing

It is anticipated that the Project will be developed under a single phase, subject to market conditions.

III. Engineering

A. Grading and Drainage

The grading and drainage design will incorporate onsite surface retention basins and underground retention systems. The requirement for this Property is to capture 110% of the 100-year 2-hour storm event. The Property will ultimately outfall to the northwest corner at Arizona Avenue. An approved City of Chandler underground storage system will be utilized.

The Arizona Avenue frontage will be rebuilt to remove and add deceleration lanes. The half-street drainage that historically was coming to the Property will be captured by scuppers and routed to underground retention. **See Tab H, Preliminary Grading and Drainage Plan.**

B. Water and Sewer

This Property is bound on the west by public right-of-way and existing public water and sewer infrastructure. There is an existing 12" sanitary main located within Arizona Avenue that will serve this site. There is an existing 16" water main within Arizona Avenue.

A private 8" sewer tap is planned to connect to an existing manhole in Arizona Avenue.

Two 8" water meters from the existing 16" water main in Arizona Avenue will connect to a private onsite 8" water system loop. The loop will be for domestic water service to the buildings, landscaping water, and multiple fire hydrants onsite as well as the fire sprinkler connection to the building. Onsite fire hydrant coverage will be provided to meet City of Chandler Fire Department requirements.

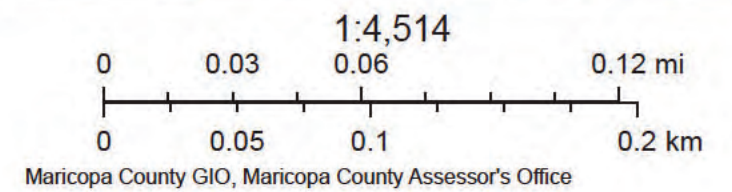
IV. Summary

This proposal will provide new, highly-amenitized and quality housing opportunities within Uptown Chandler, an important area of growth for the City, proximate to existing and future retail and employment opportunities, and major transportation arteries. The Project conforms to the City's established policy goals and will be a compatible and complementary land use for the area.

Site Context Map



January 26, 2024





biltform-architecture
group, inc.
11460 north cave creek road . suite 11
phoenix . arizona 85020
Phone 602.285.9200
www.biltform.com

TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

SITE PLAN EXHIBIT

COLOR

EX1

N.T.S

preliminary not for construction

07/29/2024



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Phone 602.285.9200
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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

PERSPECTIVE
COLOR
N.T.S.
preliminary not for construction

07/29/2024

P1



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phoenix, arizona 85020
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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

PERSPECTIVE - STREET LEVEL
COLOR
N.T.S.

07/29/2024

P2

preliminary not for construction



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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

07/29/2024
PERSPECTIVE
COLOR
N.T.S.
P3
preliminary not for construction



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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

PERSPECTIVE
COLOR
N.T.S.
preliminary not for construction

07/29/2024

P4



biltform
architecture

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Phone 602.285.9200
www.biltform.com

TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

CLUBHOUSE FRONT PERSPECTIVE
COLOR
N.T.S.

07/29/2024

P5

preliminary not for construction



biltform-architecture
group, inc.
11460 north cave creek road, suite 11
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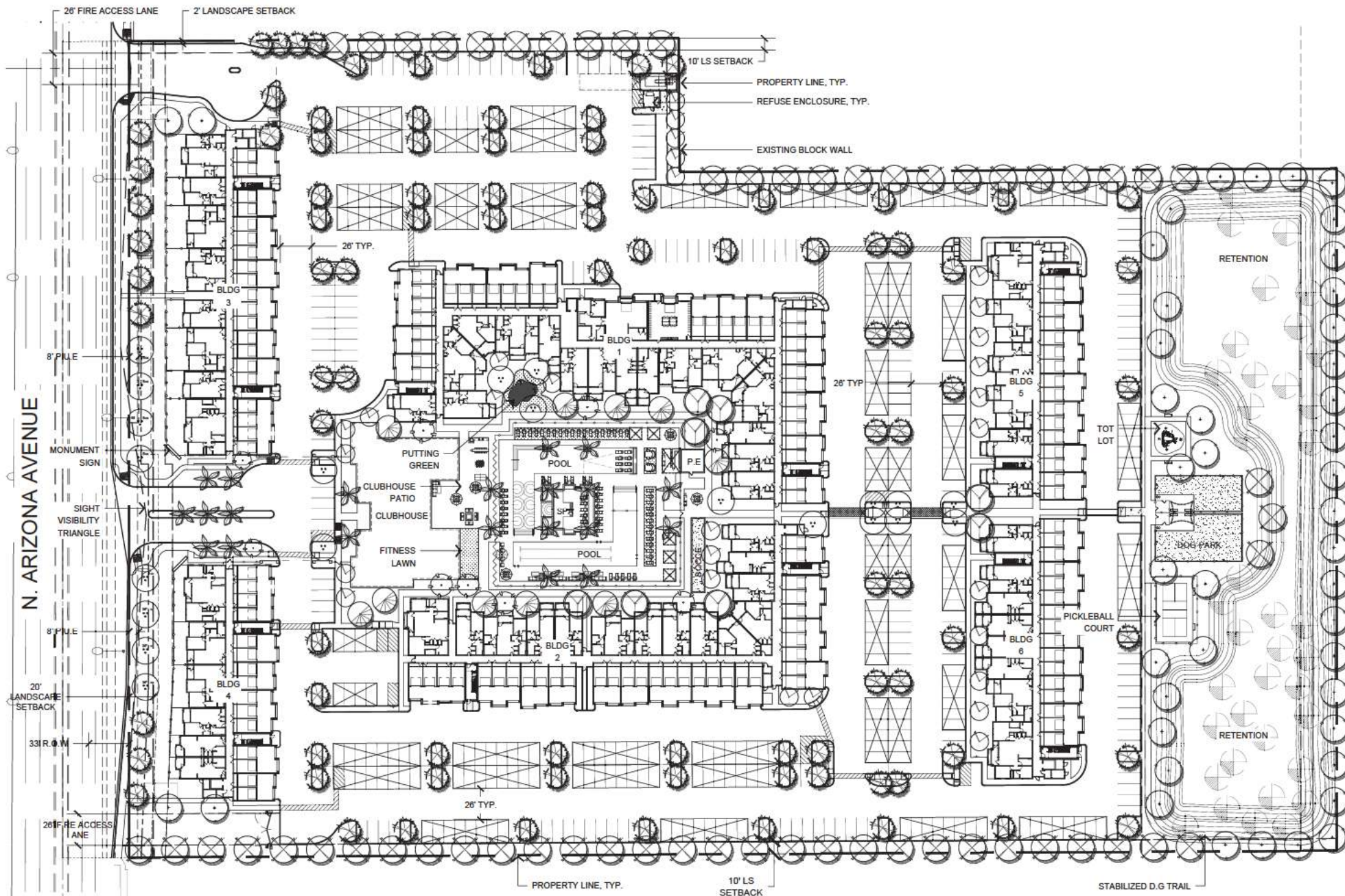
TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

CLUBHOUSE REAR PERSPECTIVE
COLOR
N.T.S.

07/29/2024

P6

preliminary not for construction



PLANT PALETTE

BOTANICAL NAME	COMMON NAME	QTY	SIZE	PLANTING	COMMENTS
TREES					
Acacia salicina	Willow Acacia	45	24" Box	6H X 5W	Standard Trunk Dense Canopy
Caesalpinia gilliesii	Yellow Bird of Paradise	7	24" Box	6H X 5W	Multi - Trunk Dense Canopy
Chitalpa tashkentensis	Chitalpa	10	24" Box	6H X 5W	Dense Canopy
Eucalyptus microtheca	Coolibah	57	24" Box	9H X 4W	Standard Trunk Dense Canopy
Nerium oleander	Red Oleander Tree	32	24" Box	10H X 5W	Standard Trunk Dense Canopy
Olea europaea wilsonii	Wilson Olive	13	24" Box	6H X 5W	Multi - Trunk Dense Canopy
Phoenix dactylifera	Date Palm	8	36" Box	9H X 7W	Dense Canopy Matching Form
Pistacia x 'Red Push'	Red Push Pistache	8	24" Box	6H X 3W	Diamond Cut Standard Trunk Dense Canopy
Quercus virginiana	Southern Live Oak	92	24" Box	6H X 5W	Standard Trunk Dense Canopy

Existing Tree
All trees shall comply with the latest amended edition of the "Arizona Nursery Association - Recommended Tree Specifications." See Section 1903(8)(a), Zoning Code.

ACCENTS

Agave desmetiana 'Variegata'	Variegated Agave	85	5 Gal.	As Per Plan
Aloe barbadensis	Medicinal Aloe	52	5 Gal.	As Per Plan
Bignonia capreolata	Tangerine Beauty	17	5 Gal.	As Per Plan
Bougainvillea 'Torch Glow'	Torch Glow Bougainvillea	33	5 Gal.	As Per Plan
Dasyliion longissima	Toothless Spoon	178	5 Gal.	As Per Plan
Hesperaloe parviflora 'Perla'	Brakelights Red Yucca	100	5 Gal.	As Per Plan
Muhlenbergia rigida 'Nashville'	Nashville Grass	84	5 Gal.	As Per Plan
Pedilanthus macrocarpus	Lady Slipper Plant	86	5 Gal.	As Per Plan
Phoenix roebelenii	Pygmy Date	4	5 Gal.	As Per Plan
Yucca rupicola	Twisted Leaf Yucca	17	5 Gal.	As Per Plan

SHRUBS

Dodonaea viscosa	Hopseed Bush	55	5 Gal.	As Per Plan
Eremophila hygrophana	Blue Bells	64	5 Gal.	As Per Plan
Eremophila glabra	Winter Blaze	78	5 Gal.	As Per Plan
Leucophyllum frutescens 'Heavenly Cloud'	Heavenly Cloud Sage	153	5 Gal.	As Per Plan
Ruellia Penninsularis	Baja Ruellia	238	5 Gal.	As Per Plan
Tecoma 'Bells of Fire'	Bells of Fire Tecoma	217	5 Gal.	As Per Plan
Tecoma 'Orange Jubilee'	'Orange Jubilee' Tecoma	224	15 Gal.	As Per Plan

GROUND COVERS

Callistemon 'Little John'	Little John Dwarf Bottlebrush	97	1 Gal.	As Per Plan
Eremophila glabra 'Mingnew Gold'	Outback Sunrise Outback	393	1 Gal.	As Per Plan
Lantana montevidensis	Purple Trailing Lantana	270	1 Gal.	As Per Plan
Lantana x 'New gold'	New Gold Lantana	102	1 Gal.	As Per Plan
Teucrium chamaedrys	Germander	131	1 Gal.	As Per Plan

MISCELLANEOUS

DG	Decomposed Granite - Granite Express "ExpressRose" 2" Screened, 2" Depth In All Planting Areas (Typ.) - 165,939 SF
	Synthetic Turf - ForeverLawn "Fusion Pro" Turf or Approved Equal - 3,031 SF
	Synthetic Turf - ForeverLawn "EX Putt" Turf or Approved Equal - 348 SF
	Hybrid Bermuda Sod - 3,988 SF

LANDSCAPE SITE DATA

** TOTAL SITE AREA	652,729	SQ. FT.	14.1	AC.
** TOTAL BUILDING AREA	145,392	SQ. FT.	22.3	% OF TOTAL SITE AREA
TOTAL LANDSCAPE AREA	173,656	SQ. FT.	26.6	% OF TOTAL SITE AREA
TOTAL PARKING AREA	140,391	SQ. FT.	21.5	% OF TOTAL SITE AREA
LANDSCAPE IN PARKING AREA	14,410	SQ. FT.	10.3	%
10% MINIMUM OF PARKING LOT INTERIOR SURFACE AREA SHALL BE LANDSCAPED				
** TOTAL TURF AREA	7,345	SQ. FT.	4.2	% OF TOTAL LANDSCAPE AREA
** SHRUBS/GROUNDCOVERS	104,714	SQ. FT.	60.3	% OF TOTAL LANDSCAPE AREA
NORGANIC/GRAVITE	165,939	SQ. FT.	95.6	% OF TOTAL LANDSCAPE AREA

**NOTE: QUANTITIES PROVIDED INCLUDE ONSITE IMPROVEMENTS
STREET FRONTAGE-
20' OF LANDSCAPED AREA ALONG ROW/STREET FRONTAGE LANDSCAPE REQUIRED: 1 TREE AND 6 SHRUBS PER 30 L.F. PLUS 50% LIVE GROUND COVER (NOT INCLUDING TREE CANOPIES)
ARTERIAL STREETS (NON-COMMERCIAL): MINIMUM, 50%- 36" BOX TREES, 50%- 24" BOX TREES
526 L.F. 18 # TREES (REQUIRED) 17 # TREES (PROVIDED)
105 # SHRUBS (REQUIRED) 213 # SHRUBS (PROV DED)
10,105 S.F. OF LANDSCAPE AREA 55 % TOTAL COVERAGE OF ADDITIONAL SHRUBS AND GROUND COVER

CITY OF CHANDLER

PLANT TYPE	MINIMUM PLANTING SIZE
Trees (for residential development adjoining arterial streets)	24-inch box (50% of required trees) 36-inch box (50% of required trees)
Trees (quantities that exceed minimum code standards)	15-gallon
Shrubs	5-gallon
Ground Cover	1-gallon
Vines	1-gallon
Annuals	4-inch pots or flats

PLANT QUANTITY SCHEDULE

LOCATION	REQUIRED	PROV DED
COMMON OPEN SPACE		
	1 Tree/6 Shrubs per 1,000 SF	
TREES	100	100
SHRUBS	600	885
RIGHT-OF-WAY		
	1 Tree/6 Shrubs per 30 LF	
TREES	17	17
SHRUBS	105	213
PARKING AREAS		
	1 Tree/5 Shrubs per Planter	
TREES	69	69
SHRUBS	345	345
LANDSCAPE BUFFER		
	1 Evergreen Tree/6 Shrubs per 30 LF	
TREES	89	90
SHRUBS	534	648

CONCEPTUAL LANDSCAPE PLAN



Collaborative V
Design Studio Inc.
7116 East 1st Ave.,
Suite 103
Scottsdale, Arizona
85251
office: 480-347-0590
fax: 480-656-6012



EXPIRES 08/30/2025

LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
Chandler, AZ

CONCEPTUAL LANDSCAPE PLAN

DESIGNED BY: HR
DRAWN BY: HR
CHECKED BY: PV/MD
DATE: January 25, 2024
REVISIONS: May 20, 2024 City Comments
August 1, 2024 City Comments

2121 N. Arizona Ave.
PDP Submittal

L1.1
1 of 7



PLANT PALETTE

BOTANICAL NAME	COMMON NAME	QTY	PLANTING SIZE	COMMENTS
TREES				
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Eucalyptus microtheca	Coolibah	57	24" Box 9'H X 4'W	Standard Trunk Dense Canopy
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Phoenix dactylifera	Date Palm	8	36" Box 9'H X 7'W	Standard Trunk Dense Canopy
Phoenix dactylifera	Date Palm	21	25" C.T.H.	Matching Form Diamond Cut
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Quercus virginiana	Southern Live Oak	92	24" Box 6'H X 5'W	Standard Trunk Dense Canopy

Existing Tree
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MISCELLANEOUS

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**NOTE: QUANTITIES PROVIDED INCLUDE ONSITE IMPROVEMENTS

STREET FRONTAGE:
20' OF LANDSCAPED AREA ALONG ROW/STREET FRONTAGE LANDSCAPE REQUIRED: 1 TREE AND 6 SHRUBS PER 30 L.F. PLUS 50% LIVE GROUND COVER (NOT INCLUDING TREE CANOPES)
ARTERIAL STREETS (NON-COMMERCIAL): MINIMUM, 50%- 36" BOX TREES, 50%- 24" BOX TREES
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105 # SHRUBS (REQUIRED) 213 # SHRUBS (PROV DED)
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CITY OF CHANDLER

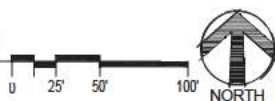
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Trees (quantities that exceed minimum code standards)	15-gallon
Shrubs	5-gallon
Ground Cover	1-gallon
Vines	1-gallon
Annuals	4-inch pots or flats

PLANT QUANTITY SCHEDULE

LOCATION	REQUIRED	PROV DED
COMMON OPEN SPACE		
TREES	1 Tree/6 Shrubs per 1,000 SF	100
SHRUBS		600
RIGHT-OF-WAY		
TREES	1 Tree/6 Shrubs per 30 LF	17
SHRUBS		105
PARKING AREAS		
TREES	1 Tree/5 Shrubs per Planter	69
SHRUBS		345
LANDSCAPE BUFFER		
TREES	1 Evergreen Tree/6 Shrubs per 30 LF	89
SHRUBS		534

CONCEPTUAL LANDSCAPE PLAN

SCALE 1" = 50'-0"



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85251
Office: 480-347-0590
Fax: 480-656-6012



EXPIRES 08/30/2025

LANDSCAPE IMPROVEMENTS
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CONCEPTUAL LANDSCAPE PLAN



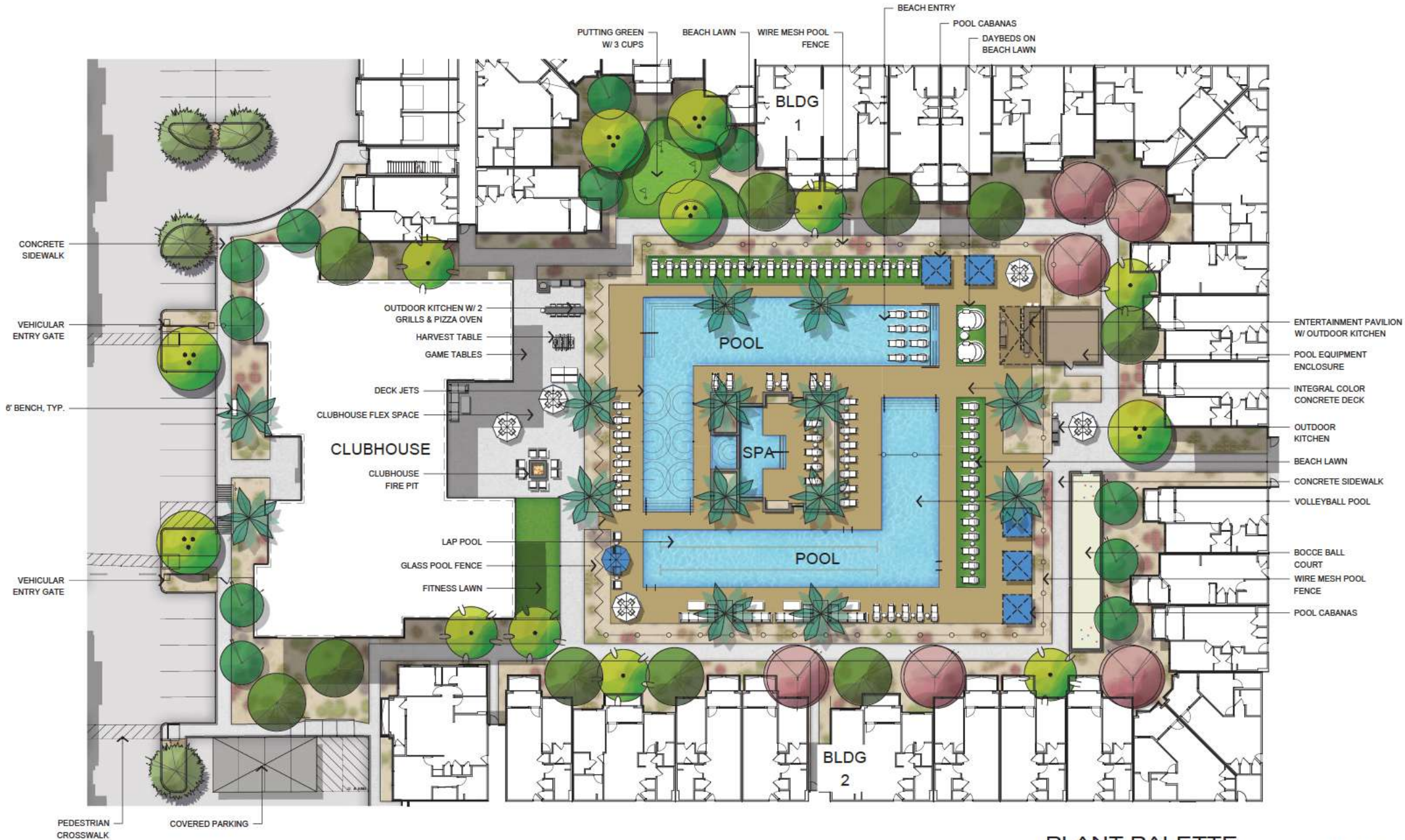
DESIGNED BY: HR
DRAWN BY: HR
CHECKED BY: PV/MD
DATE: January 25, 2024
REVISIONS: May 20, 2024
City Comments
August 1, 2024
City Comments

2121 N. Arizona Ave.
PDP Submittal

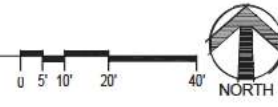
L1.2



EXPIRES 08/30/2025



CONCEPTUAL CENTRAL COURTYARDS PLAN
SCALE 1" = 20'-0"



PLANT PALETTE

BOTANICAL NAME	COMMON NAME	QTY	PLANT NG	COMMENTS
TREES				
<i>Acacia salicina</i>	Willow Acacia	45	24" Box 6'H X 5'W	Standard Trunk Dense Canopy
<i>Caesalpinia gilliesii</i>	Yellow Bird of Paradise	7	24" Box 6'H X 5'W	Multi - Trunk Dense Canopy
<i>Chitalpa tashkentensis</i>	Chitalpa	10	24" Box 6'H X 5'W	Multi - Trunk Dense Canopy
<i>Eucalyptus microtheca</i>	Coolibah	57	24" Box 9'H X 4'W	Standard Trunk Dense Canopy
<i>Nerium oleander</i>	Red Oleander Tree	32	24" Box 10'H X 5'W	Standard Trunk Dense Canopy
<i>Olea europaea 'wilsonii'</i>	Wilson Olive	13	24" Box 6'H X 5'W	Multi - Trunk Dense Canopy
		8	36" Box 9'H X 7'W	Dense Canopy
<i>Phoenix dactylifera</i>	Date Palm	21	25' C.T.H.	Matching Form Diamond Cut
<i>Pistacia x 'Red Push'</i>	Red Push Pistache	8	24" Box 6'H X 3'W	Standard Trunk Dense Canopy
<i>Quercus virginiana</i>	Southern Live Oak	92	24" Box 6'H X 5'W	Standard Trunk Dense Canopy
	Existing Tree			

All trees shall comply with the latest amended edition of the Arizona Nursery Association - Recommended Tree Specifications. * See Section 1903(6)(a), Zoning Code.

LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
Chandler, AZ

AMENITY ENLARGEMENT

DESIGNED BY: HR
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DATE: January 25, 2024
REVISIONS: May 20, 2024 City Comments
August 1, 2024 City Comments

2121 N. Arizona Ave.
PDP Submittal



EXPIRES 08/30/2025

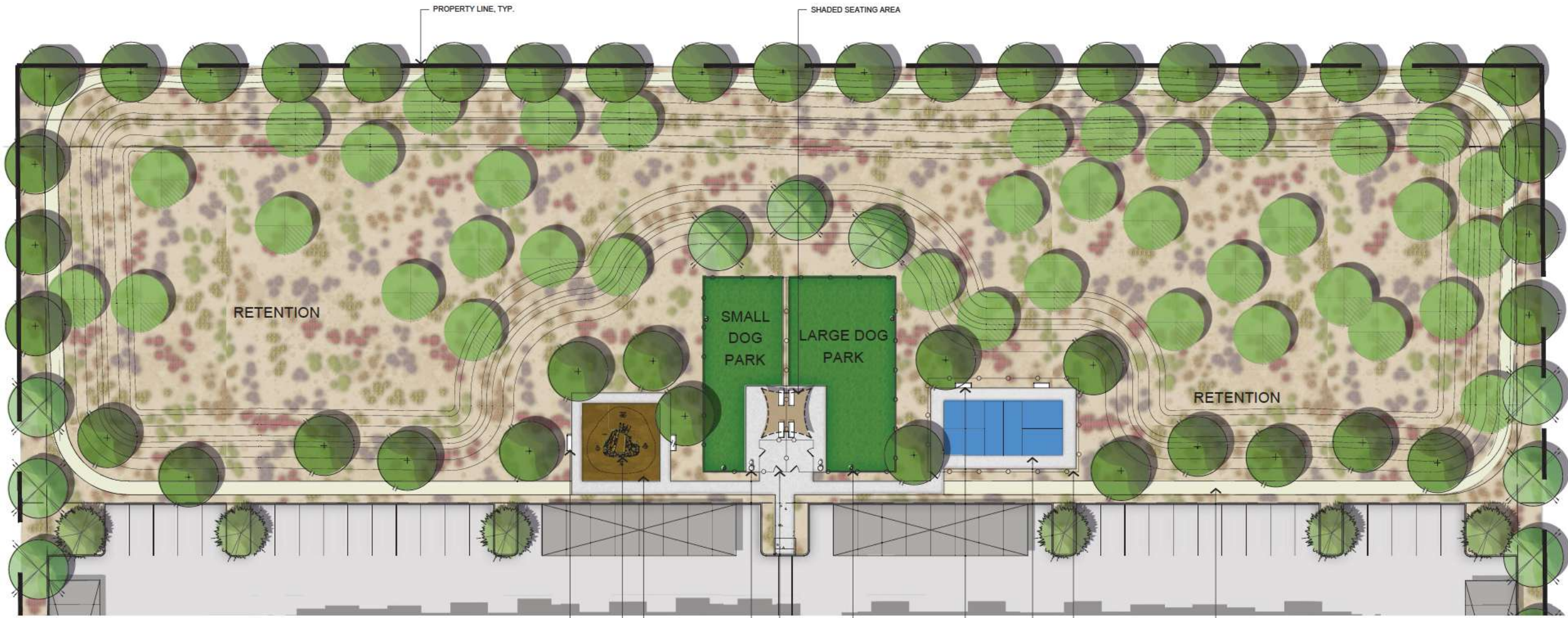
LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
Chandler, AZ

AMENITY ENLARGEMENT

DESIGNED BY:	HR
DRAWN BY:	HR
CHECKED BY:	PV/MD
DATE:	January 25, 2024
REVISIONS:	May 20, 2024 City Comments
	August 1, 2024 City Comments

2121 N. Arizona Ave.
PDP Submittal

L1.4



CONCEPTUAL DOG PARK PLAN
SCALE 1" = 20'-0"
0 5' 10' 20' 40'
NORTH

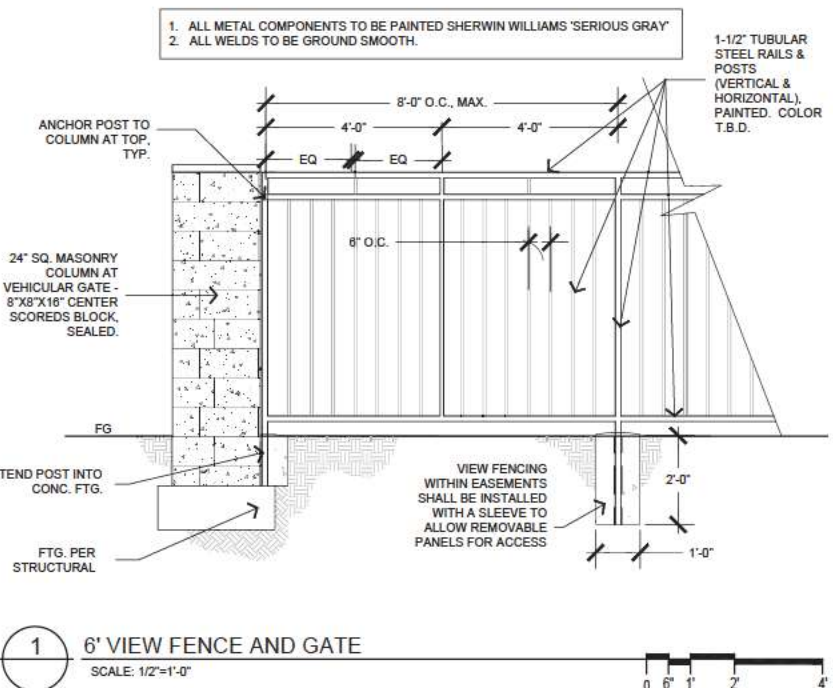
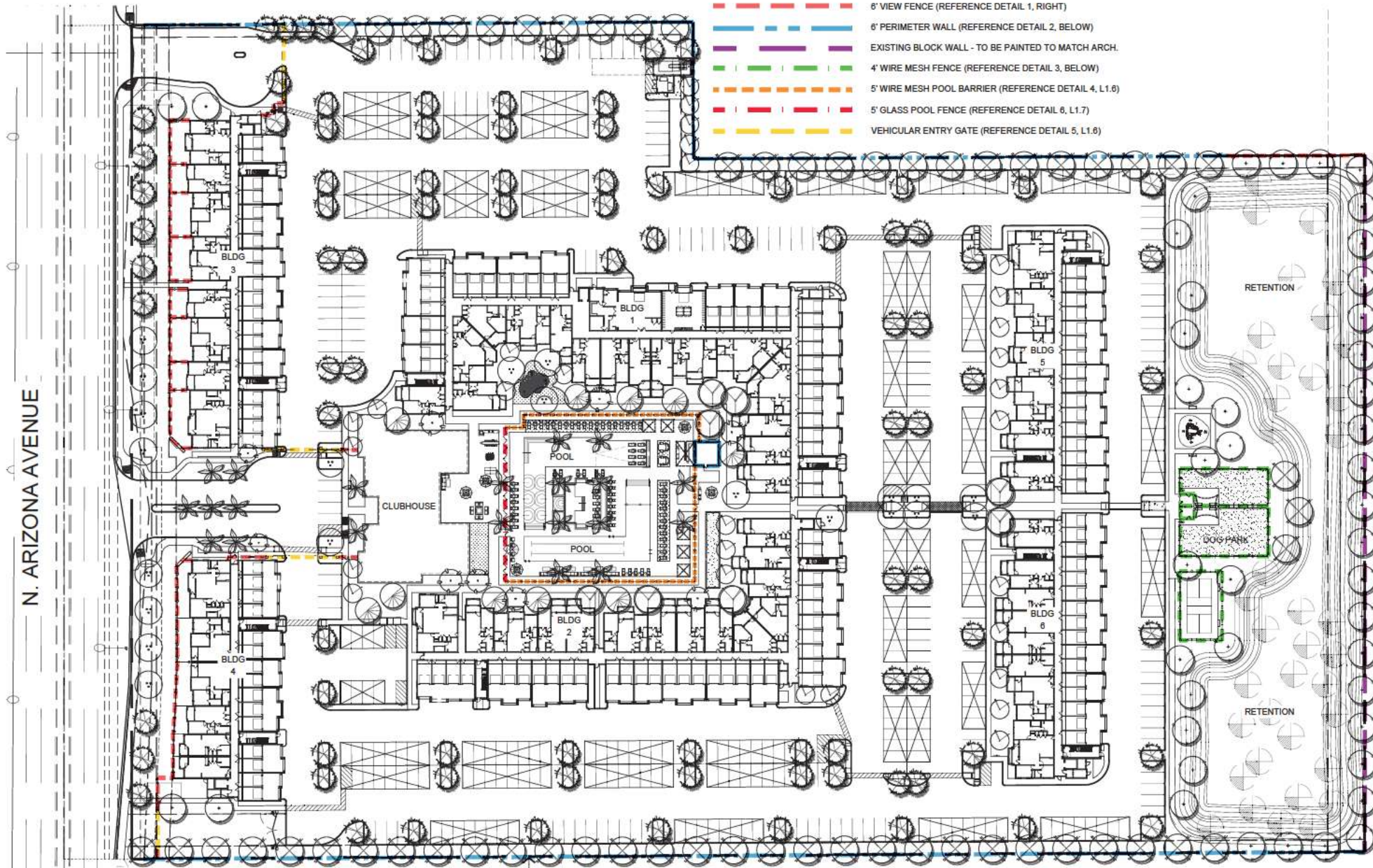
PLANT PALETTE

BOTANICAL NAME	COMMON NAME	QTY	PLANT NG SIZE	COMMENTS
TREES				
Acacia salicina	Willow Acacia	45	24" Box 6'H X 5'W	Standard Trunk Dense Canopy
Caesalpinia gilliesii	Yellow Bird of Paradise	7	24" Box 6'H X 5'W	Multi - Trunk Dense Canopy
Chitalpa tashkentensis	Chitalpa	10	24" Box 6'H X 5'W	Multi - Trunk Dense Canopy
Eucalyptus microtheca	Coolibah	57	24" Box 9'H X 4'W	Standard Trunk Dense Canopy
Nerium oleander	Red Oleander Tree	32	24" Box 10'H X 5'W	Standard Trunk Dense Canopy
Olea europaea 'wilsonii'	Wilson Olive	13	24" Box 6'H X 5'W 36" Box 9'H X 7'W	Multi - Trunk Dense Canopy
Phoenix dactylifera	Date Palm	21	25" C.T.H.	Matching Form Diamond Cut
Pistacia x 'Red Push'	Red Push Pistache	8	24" Box 6'H X 3'W	Standard Trunk Dense Canopy
Quercus virginiana	Southern Live Oak	92	24" Box 6'H X 5'W	Standard Trunk Dense Canopy
Existing Tree				

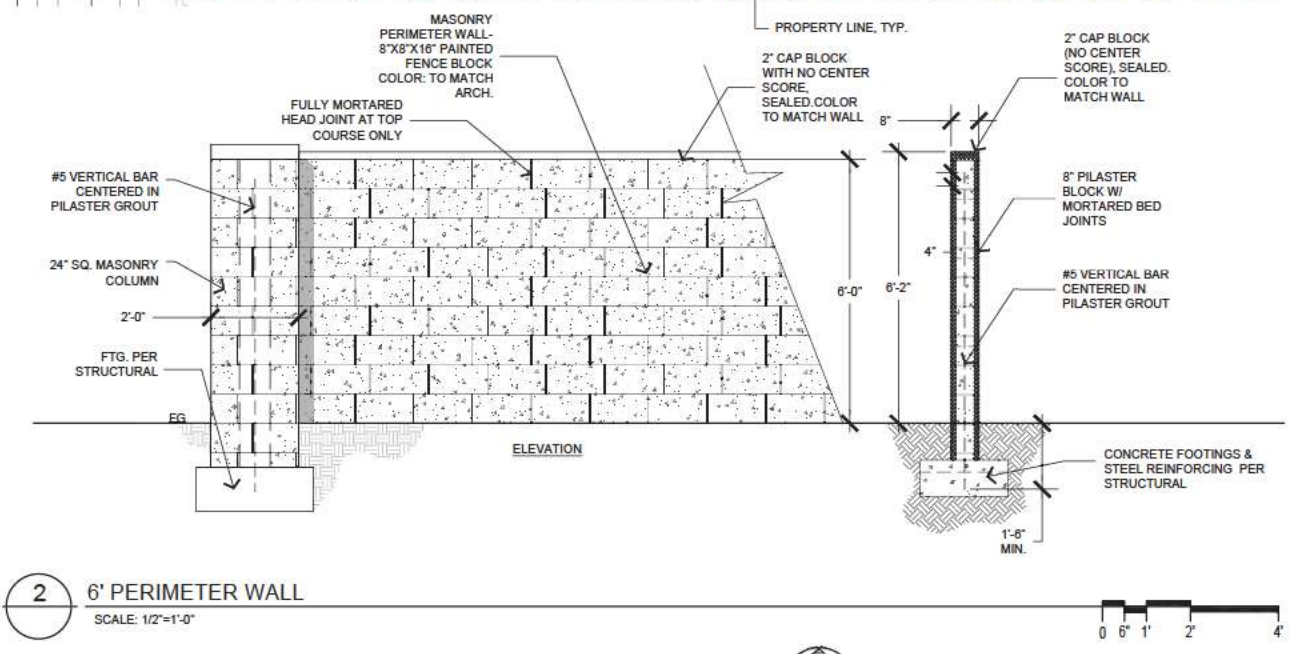
All trees shall comply with the latest amended edition of the "Arizona Nursery Association - Recommended Tree Specifications." See Section 1903(8)(a), Zoning Code.

WALL PLAN LEGEND

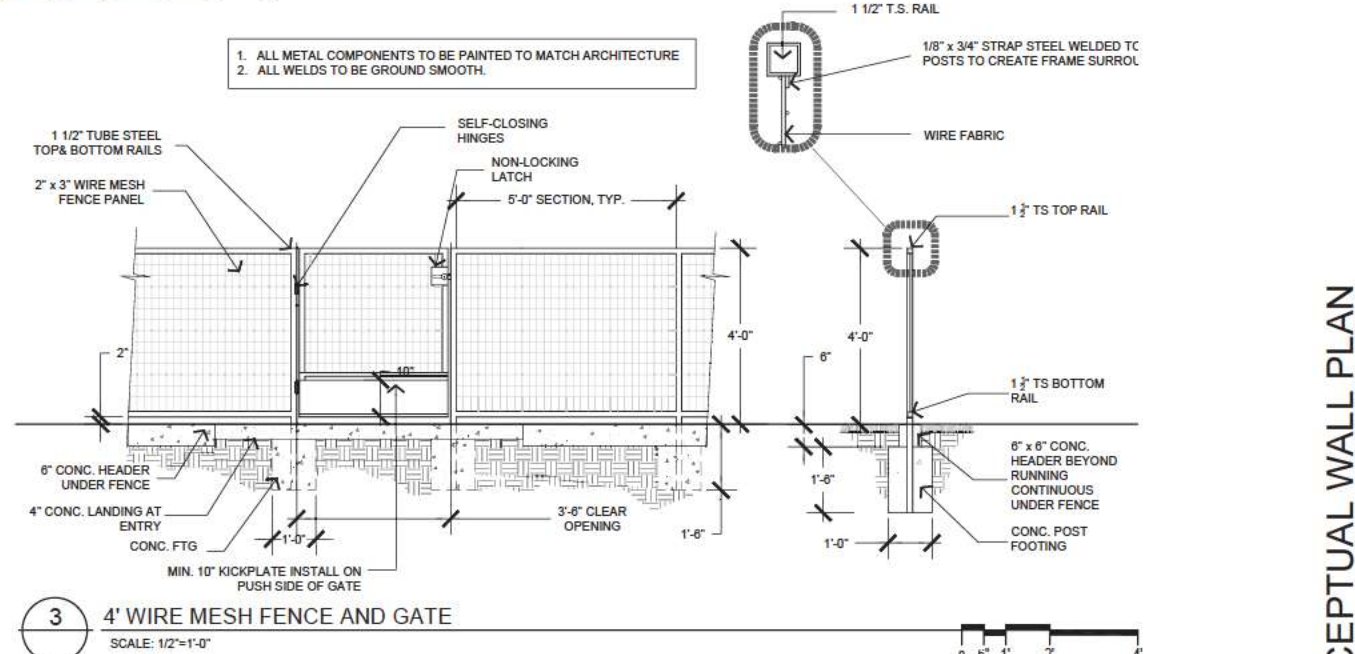
- 6' VIEW FENCE (REFERENCE DETAIL 1, RIGHT)
- 6' PERIMETER WALL (REFERENCE DETAIL 2, BELOW)
- EXISTING BLOCK WALL - TO BE PAINTED TO MATCH ARCH.
- 4' WIRE MESH FENCE (REFERENCE DETAIL 3, BELOW)
- 5' WIRE MESH POOL BARRIER (REFERENCE DETAIL 4, L1.6)
- 5' GLASS POOL FENCE (REFERENCE DETAIL 6, L1.7)
- VEHICULAR ENTRY GATE (REFERENCE DETAIL 5, L1.6)



1 6' VIEW FENCE AND GATE
SCALE: 1/2"=1'-0"



2 6' PERIMETER WALL
SCALE: 1/2"=1'-0"



3 4' WIRE MESH FENCE AND GATE
SCALE: 1/2"=1'-0"



CONCEPTUAL WALL PLAN
SCALE: 1\"/>

- 1. ALL METAL COMPONENTS TO BE PAINTED SHERWIN WILLIAMS 'SERIOUS GRAY'
- 2. ALL WELDS TO BE GROUND SMOOTH.

- 1. ALL METAL COMPONENTS TO BE PAINTED TO MATCH ARCHITECTURE
- 2. ALL WELDS TO BE GROUND SMOOTH.



Collaborative V Design Studio Inc.
7116 East 1st Ave.
Suite 103
Scottsdale, Arizona 85251
office: 480-347-0590
fax: 480-656-6012



EXPIRES 08/30/2025

LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
 Chandler, AZ



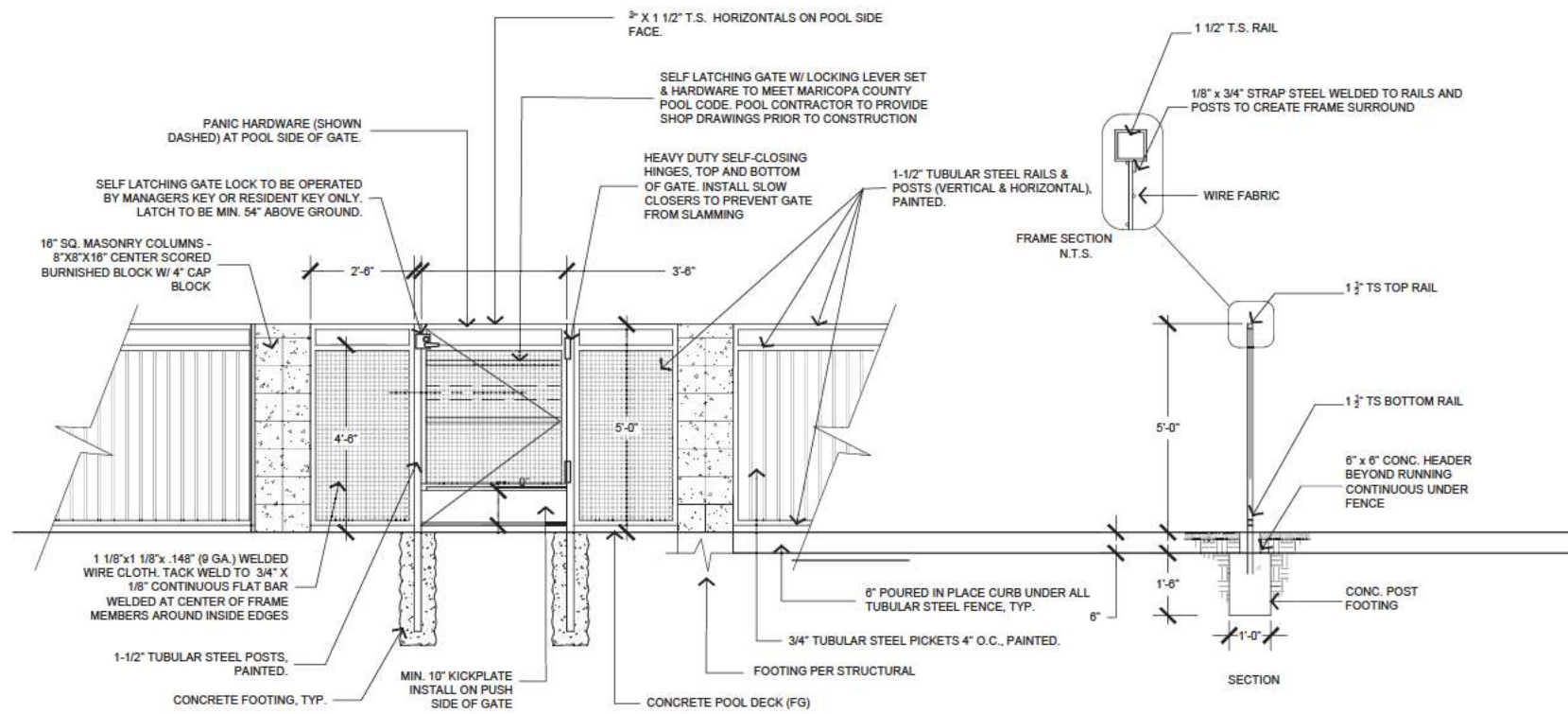
DESIGNED BY: HR
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REVISIONS: May 20, 2024 City Comments
August 1, 2024 City Comments

2121 N. Arizona Ave.
PDP Submittal

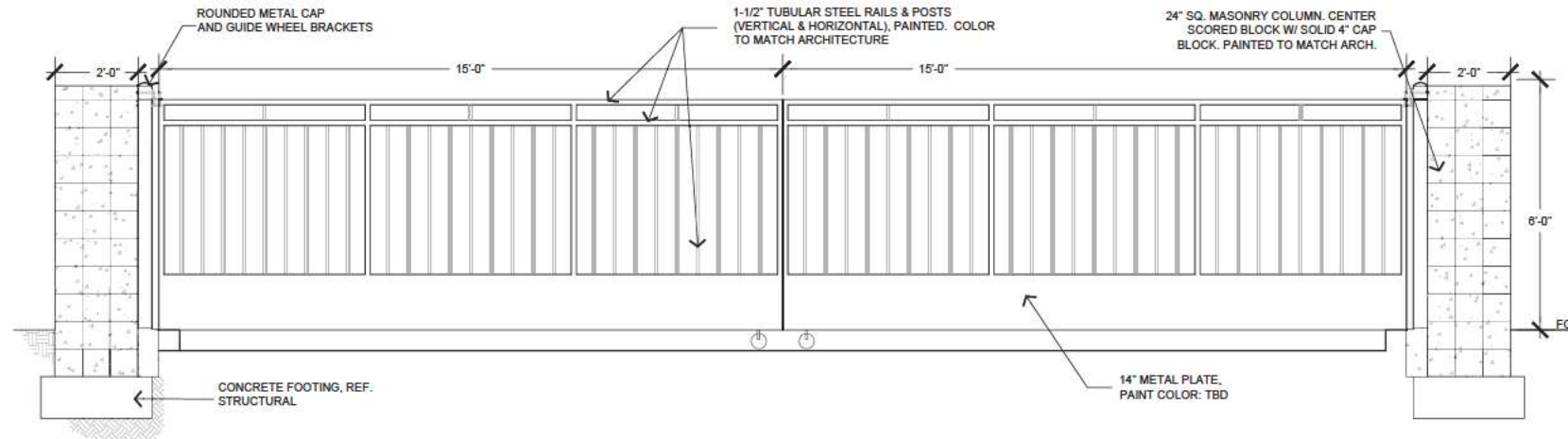
L1.5



EXPIRES 08/30/2025



4 5' WIRE MESH POOL FENCE AND GATE
SCALE: 1/2"=1'-0"



5 VEHICULAR ENTRY GATE
SCALE: 1/2"=1'-0"

LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
Chandler, AZ

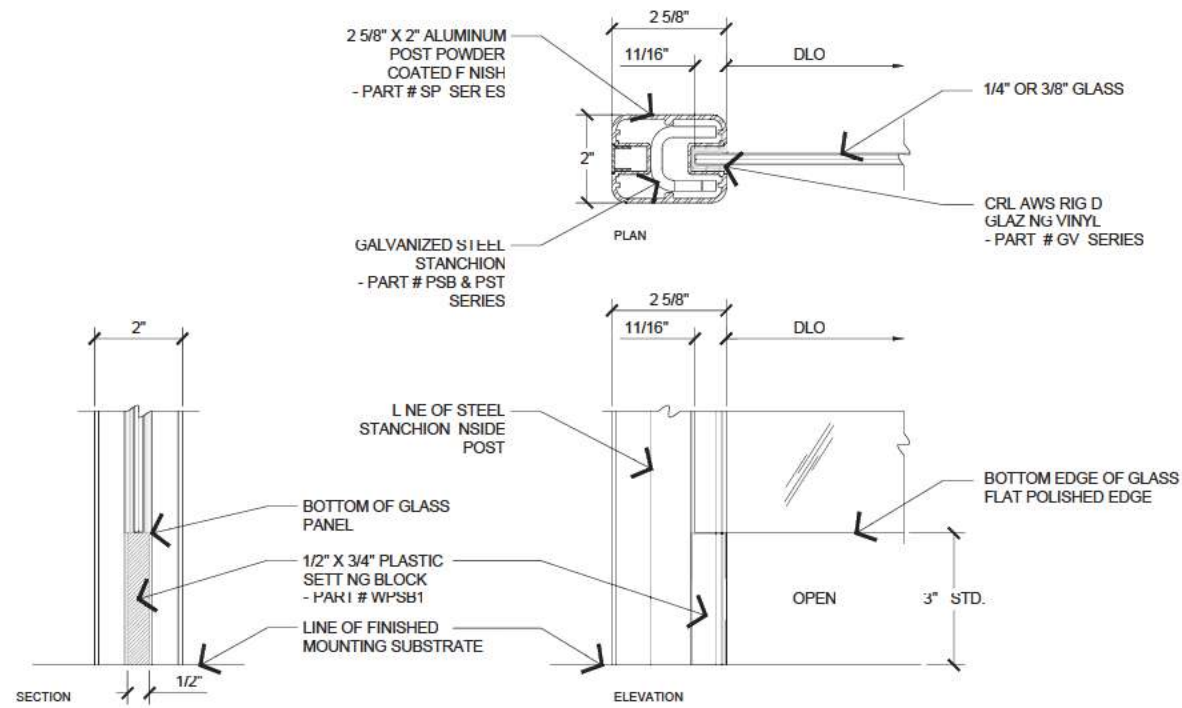
CONCEPTUAL WALL DETAILS

DESIGNED BY:	HR
DRAWN BY:	HR
CHECKED BY:	PV/MD
DATE:	January 25, 2024
REVISIONS:	May 20, 2024 City Comments
	August 1, 2024 City Comments

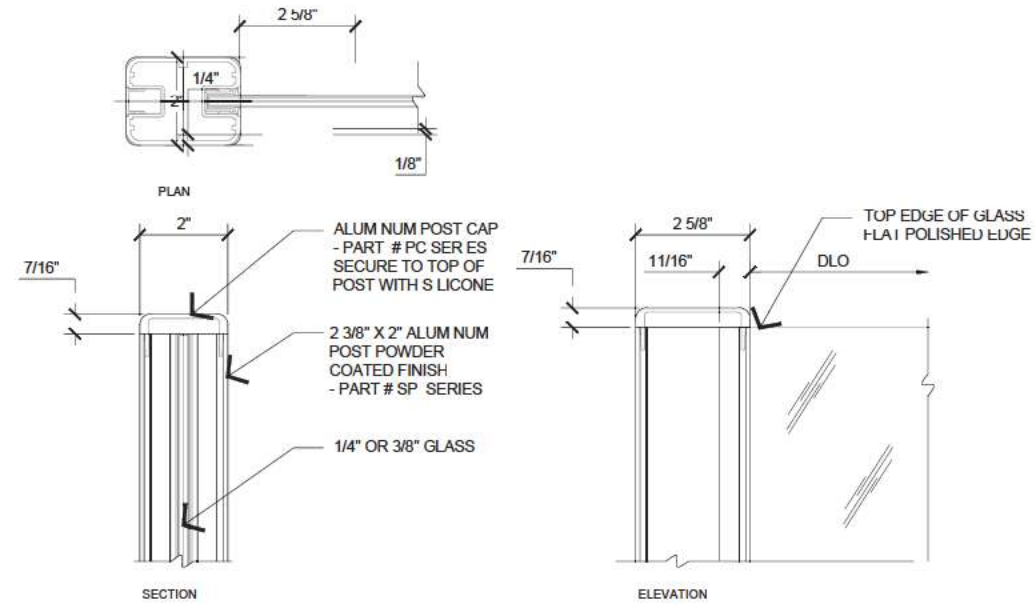
2121 N. Arizona Ave.
PDP Submittal



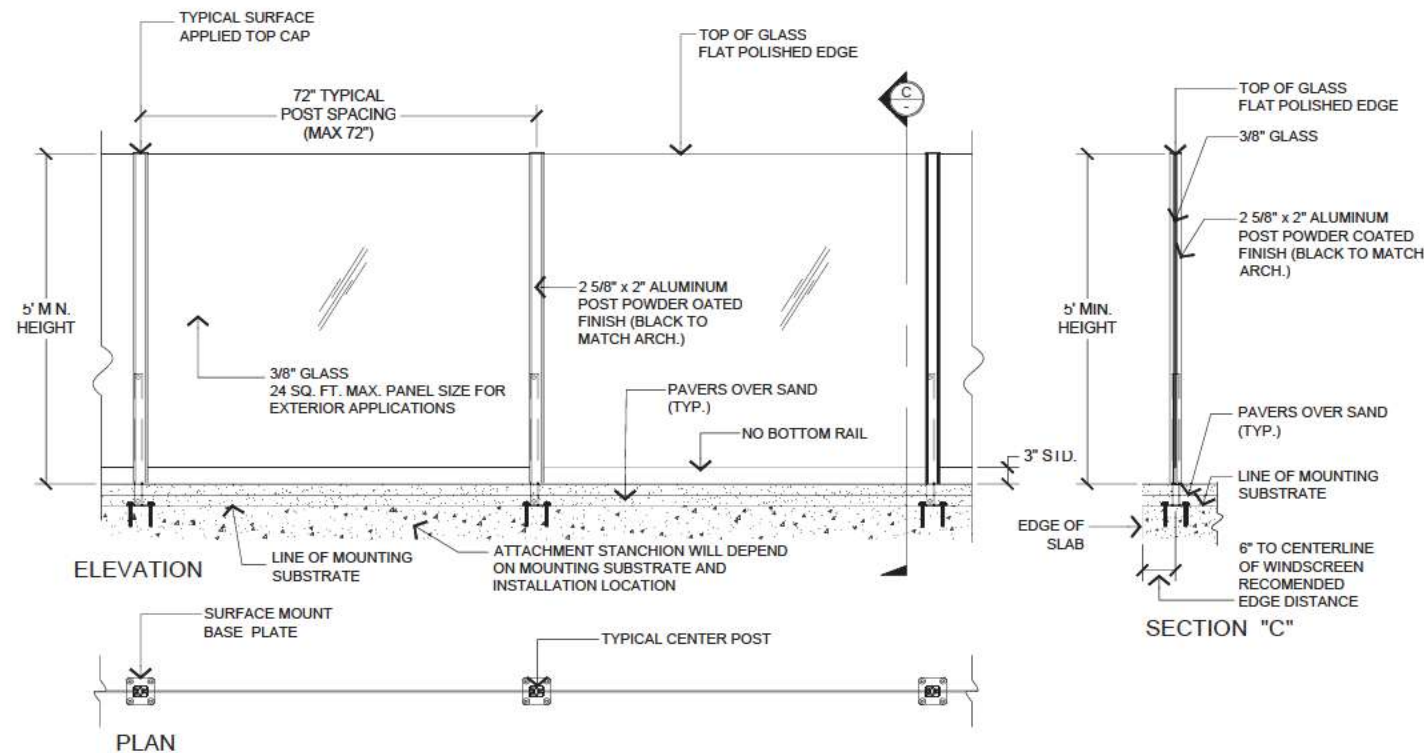
EXPIRES 08/30/2025



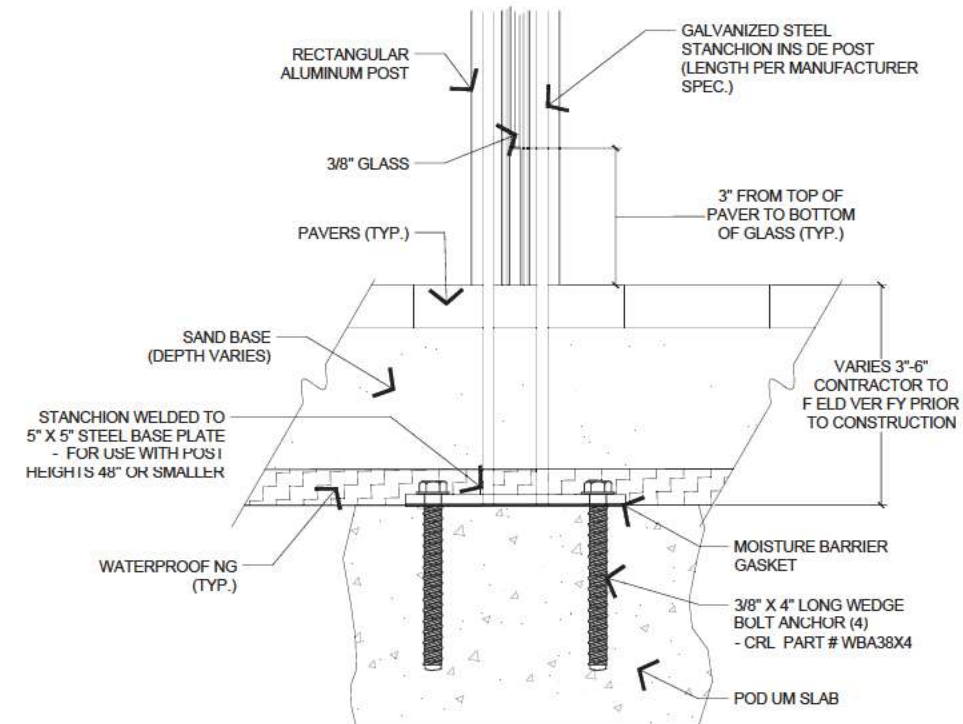
6A DETAIL W/ NO BOTTOM GLASS RAIL
SCALE: 6"=1'-0"



6B DETAIL AT TOP CORNER OF GLASS PANEL
SCALE: 6"=1'-0"



6C TYP. GLASS FENCE - W/ NO BOTTOM RAIL
SCALE: 3/4"=1'-0"



6D POST MOUNT DETAIL FOR GLASS FENCE AND GATE
SCALE: 6"=1'-0"

LANDSCAPE IMPROVEMENTS
2121 N. Arizona Ave.
Chandler, AZ

CONCEPTUAL WALL DETAILS



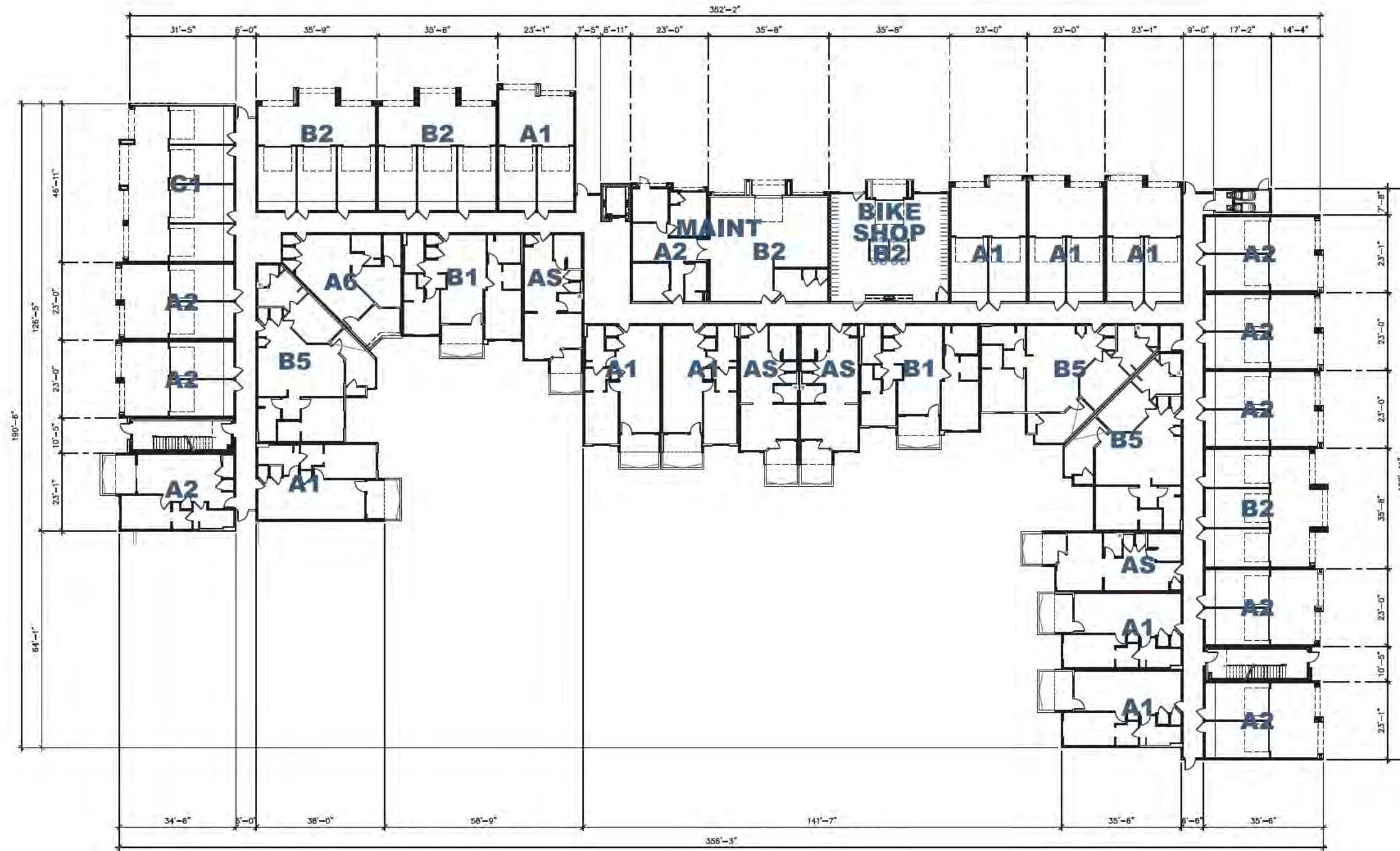
DESIGNED BY: HR
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DATE: January 25, 2024
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August 1, 2024 City Comments

2121 N. Arizona Ave.
PDP Submittal



biform architecture group, inc

11450 north cave creek road, suite 11
 phoenix, arizona 85020
 Phone 602,285,0200 Fax 602,285,5225



FIRST FLOOR PLAN

BUILDING 1 - FLOOR PLANS

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

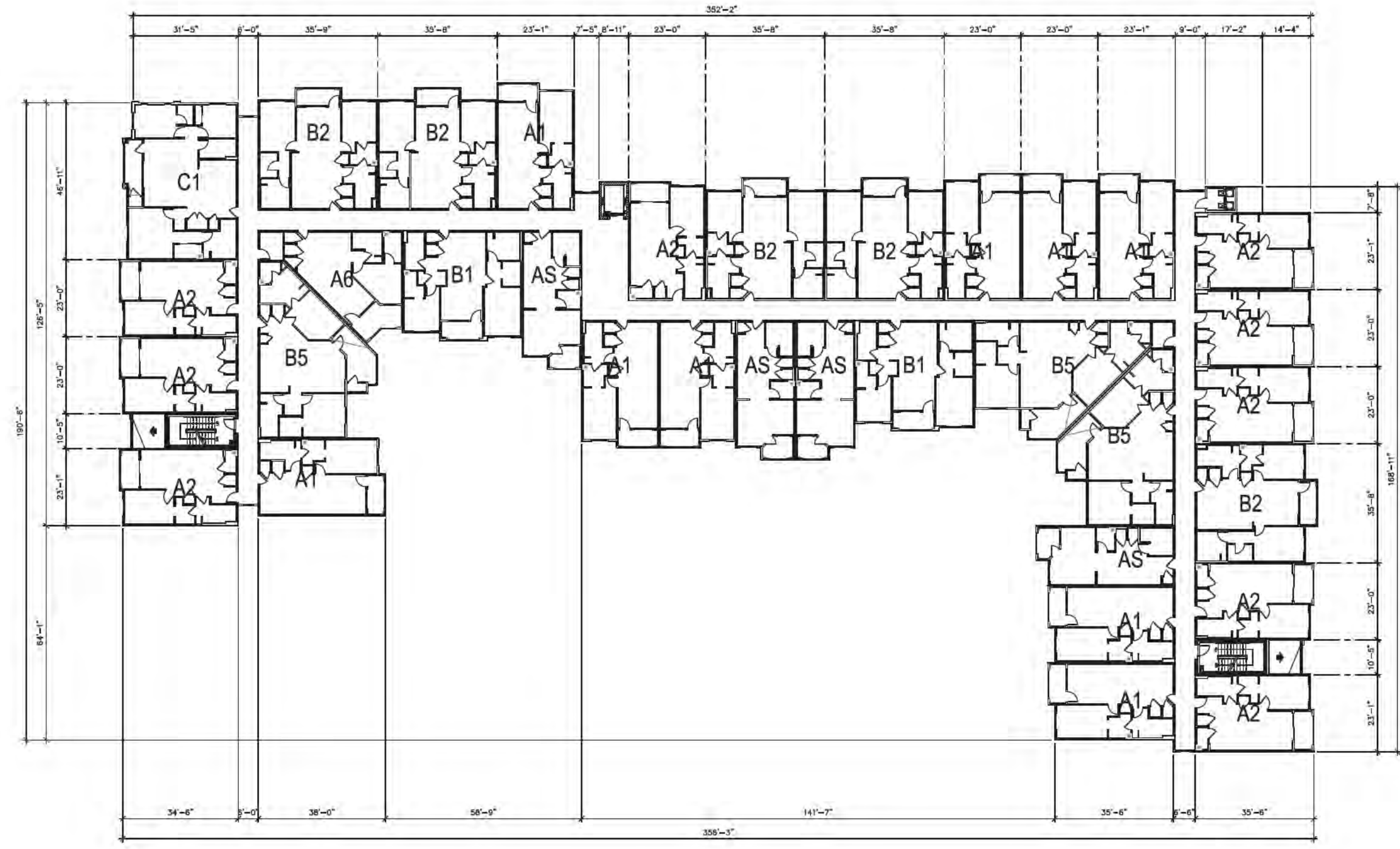
REVISIONS	
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JCB/NC: 25-201
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A3.1



biltform architecture group, inc
 11450 north cave creek road - suite 11
 phoenix - arizona 85020
 Phone 602.285.0200 Fax 602.285.5025



SECOND FLOOR PLAN

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

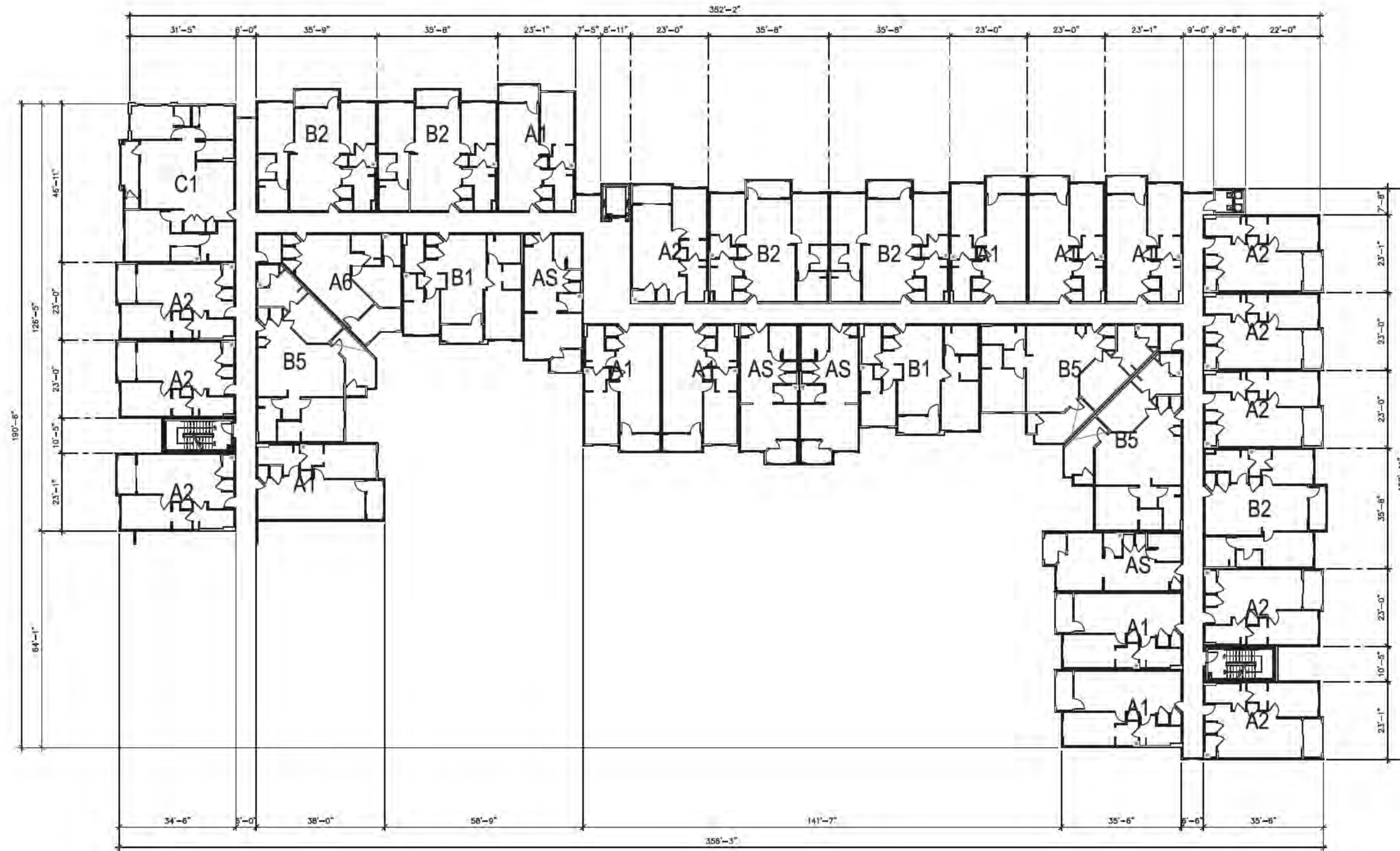
REVISIONS	
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JCB/NC: 25421
 SCALE: 1/8" = 1'-0"
 SHEET NO:



biltform architecture group, inc

11450 north cave creek road, suite 111
 phoenix, arizona 85020
 Phone 602.285.0200 Fax 602.285.5025



THIRD FLOOR PLAN

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS	
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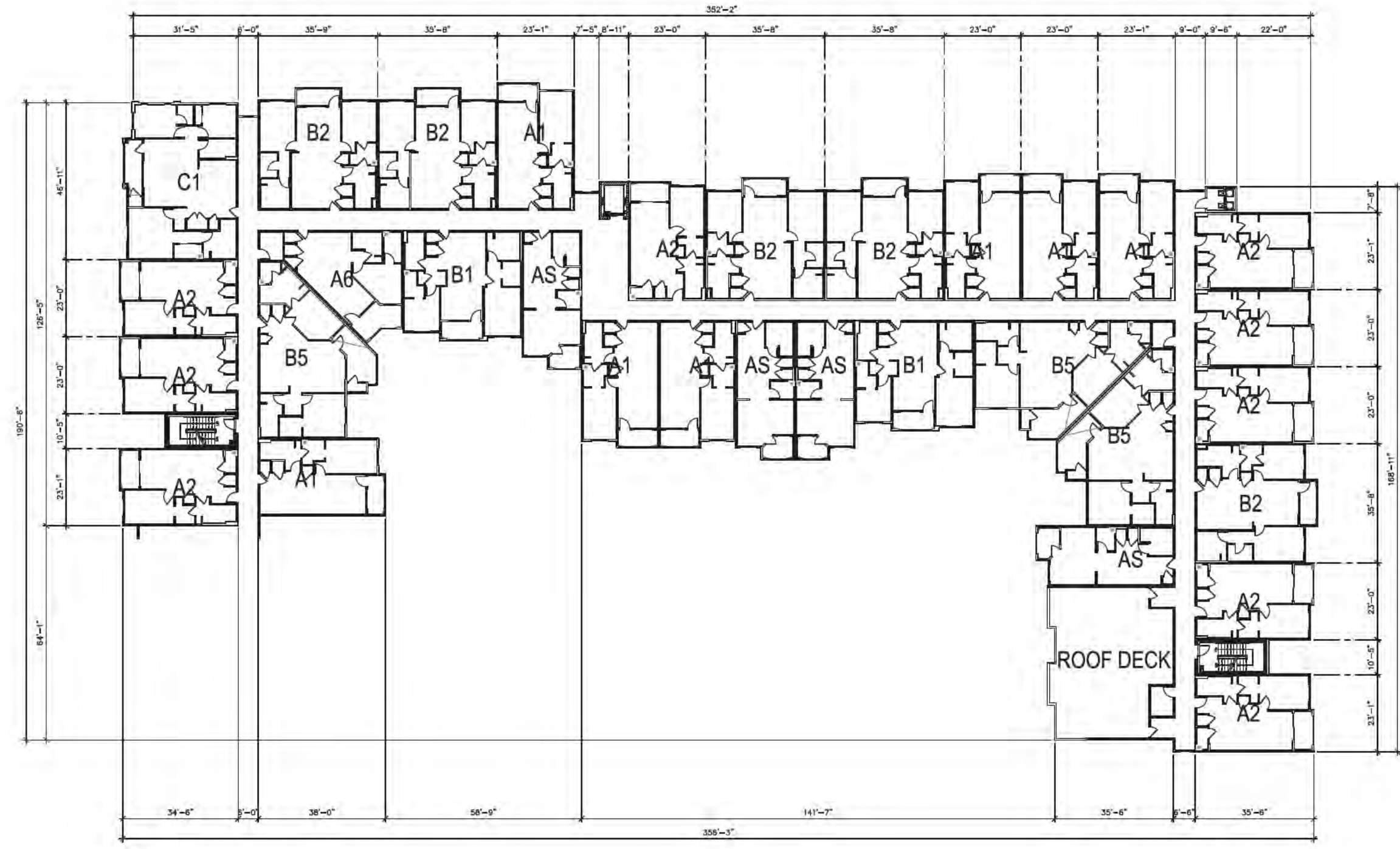
JCB/NC: 25421
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A3.3

BUILDING 1 - FLOOR PLANS



biltform architecture group, inc.
 11450 north cave creek road, suite 11
 phoenix, arizona 85020
 Phone 602.285.0200 Fax 602.285.5025



FOURTH FLOOR PLAN

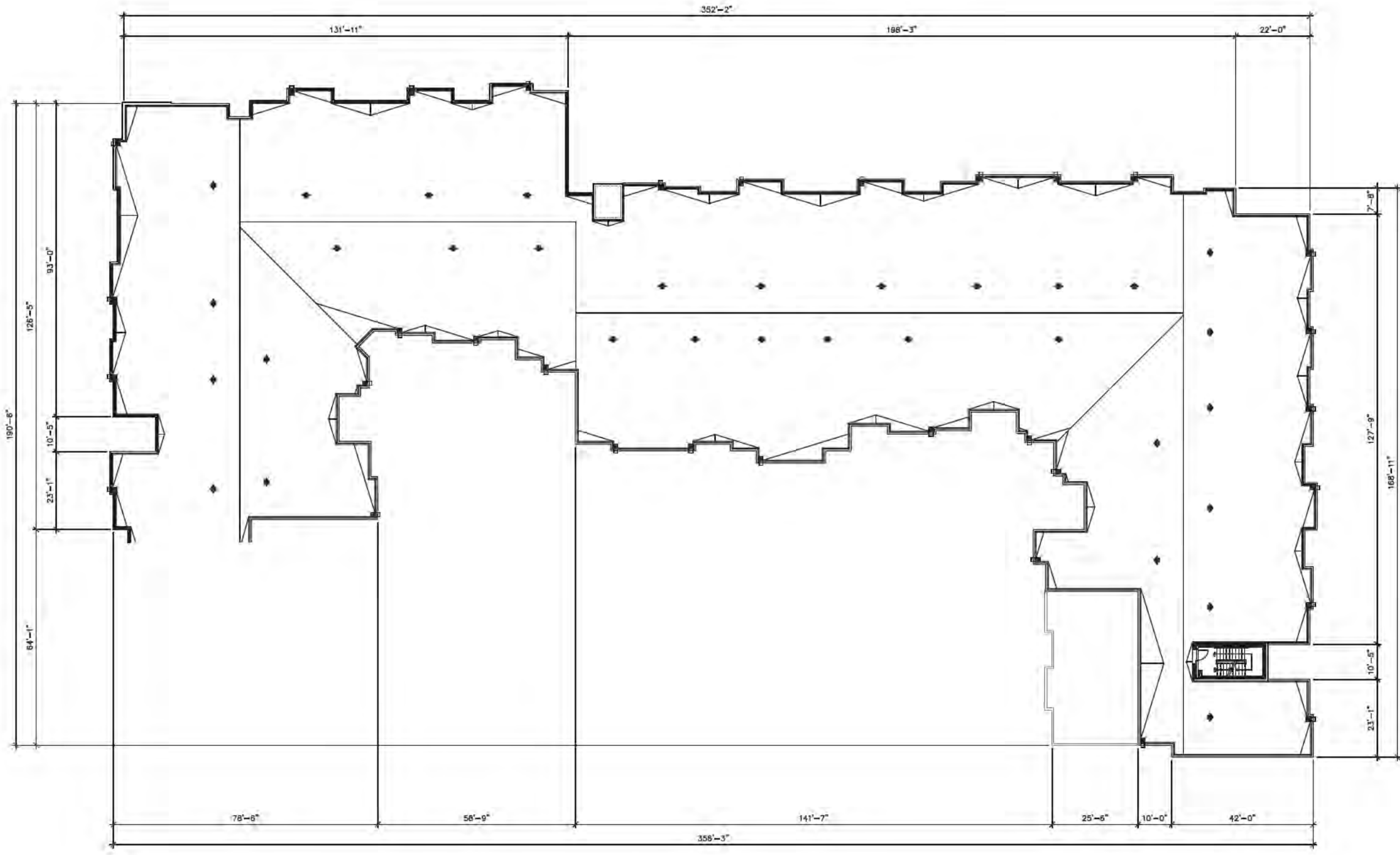
EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS
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JOB NO: 25421
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A3.4



FIRST FLOOR PLAN

BUILDING 1 - FLOOR PLANS



biflorm architecture group, inc
 11450 north cave creek road - suite 11
 phoenix - arizona 85020
 Phone 602.285.0200 Fax 602.285.5225

EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS
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JCR/NC: 25-001
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A3.5

GENERAL ELEVATION NOTES:

- ALL ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY PARAPET WALLS EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT.
- SOLID MASONRY WALLS AND GATES EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT SHALL SCREEN ALL GROUND MOUNTED MECHANICAL EQUIPMENT.
- ANY ROOF ACCESS LADDERS SHALL BE LOCATED INSIDE THE BUILDING. ROOF DRAINAGE SHALL UTILIZE INTERIOR ROOF DRAINS OR BE ARCHITECTURALLY INTEGRATED INTO THE BUILDING DESIGN. ARCHITECTURALLY INTEGRATED ROOF DRAINS SHALL REQUIRE ADDITIONAL ARTICULATION BEYOND PAINT ACCENTS. SECTION 35-1902 (8)(e)(15), ZONING CODE.
- SCREENING SHALL BE ARCHITECTURALLY INTEGRATED FOR THE SERVICE ENTRANCE SECTION (SES) AND ALL UTILITIES. ALL GROUND MOUNTED EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW BY A CONCRETE OR MASONRY WALL WITH SOLID GATES, EQUAL TO OR GREATER IN HEIGHT THAN THE MECHANICAL EQUIPMENT. SECTION 35-1902 (8)(e)13, ZONING CODE.
- SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.

EXTERIOR COLOR SCHEME

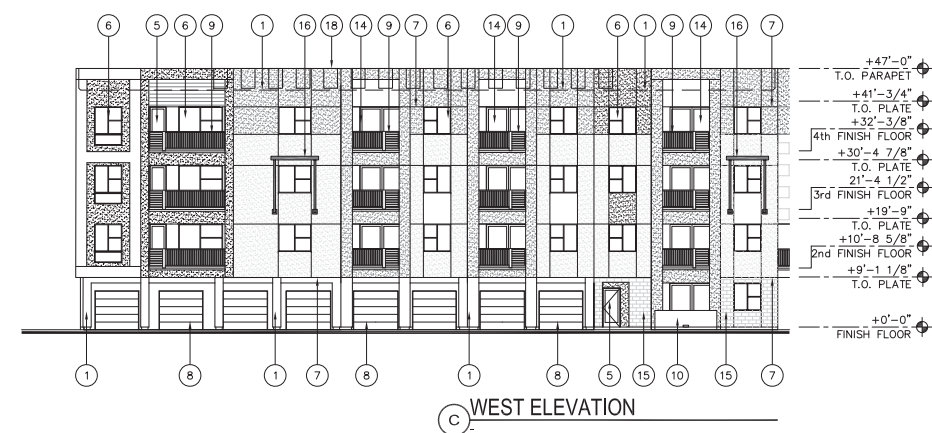
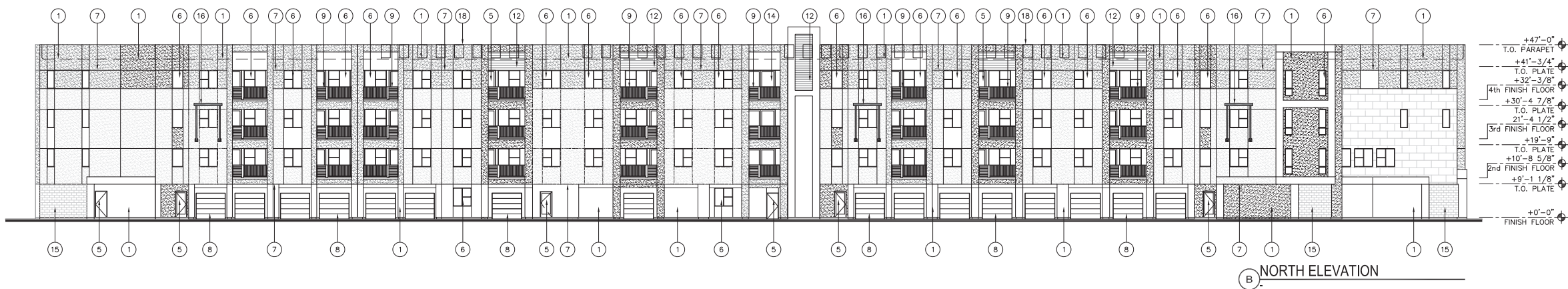
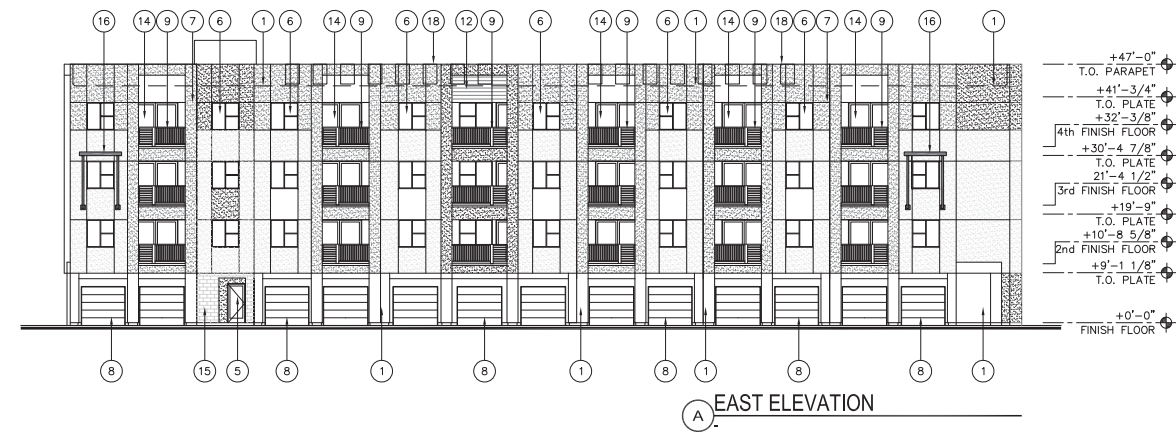
A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS	F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS	G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND	H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK	I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER	J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

ELEVATION KEY NOTES: #

- EXTERIOR STUCCO SYSTEM
- METAL PARAPET CAP
- STOREFRONT SYSTEM
- FOAM POP-OUT
- DOOR PER PLAN
- WINDOW PER PLAN
- STUCCO CONTROL JOINT
- GARAGE DOOR
- METAL PATIO/BALCONY RAILING
- FRAMED PATIO/BALCONY WALL
- TOWER ELEMENT.
- HORIZONTAL LAP SIDING.
- DECORATIVE METAL FEATURE.
- SLIDING GLASS DOOR PER PLAN.
- MASONRY VENEER
- METAL AWINGS
- TELESCOPING DOOR SYSTEM
- MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
- FIBER CEMENT SIDING



biform architecture group, inc.
 11460 north cave creek road, suite 11
 phoenix, arizona 85020
 Phone 602.285.9200 Fax 602.285.9229



EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS:

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JOB NO: 23-031
 SCALE: 3/32" = 1'-0"
 SHEET NO:

A3.6

BUILDING 1 - ELEVATIONS

GENERAL ELEVATION NOTES:

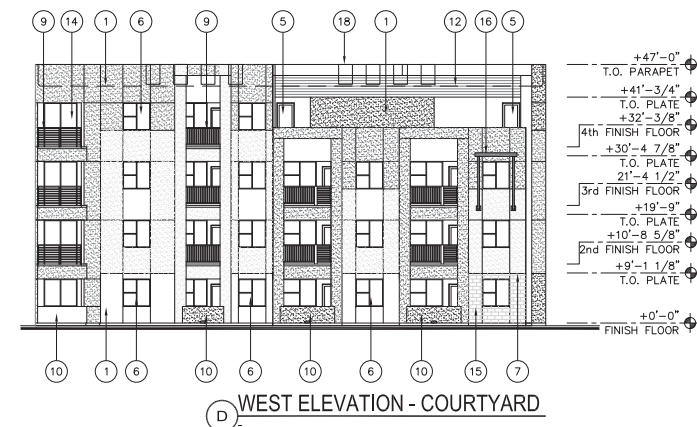
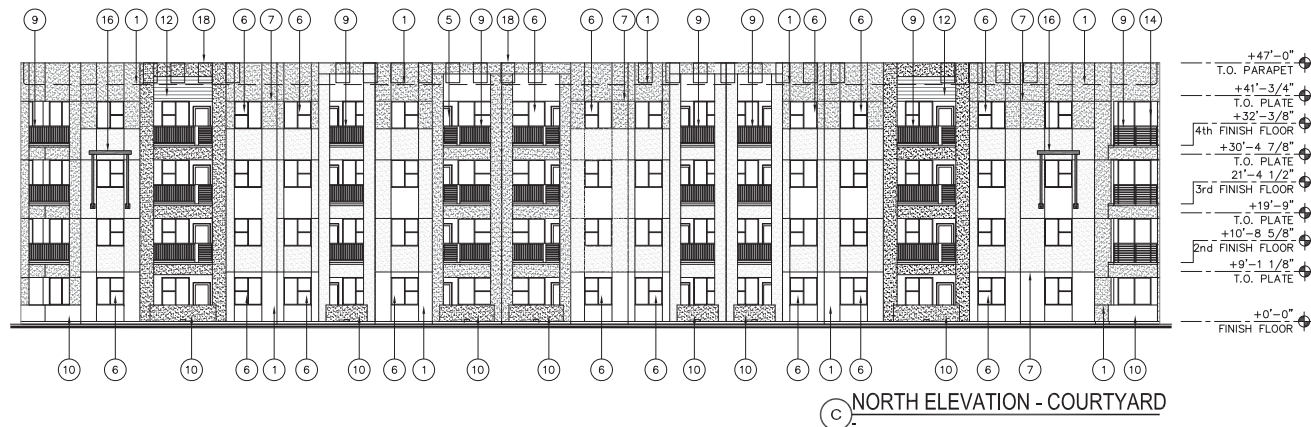
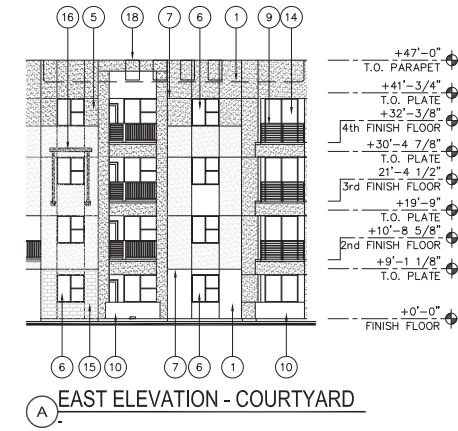
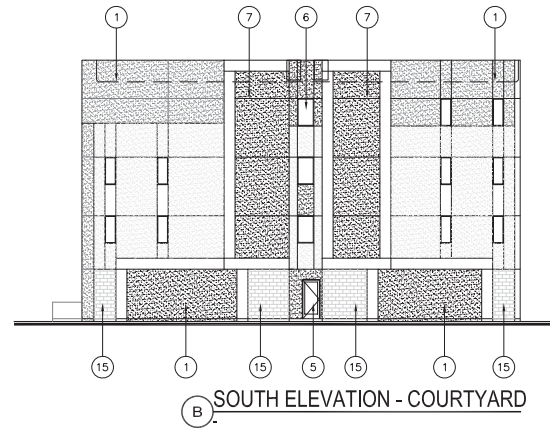
- ALL ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY PARAPET WALLS EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT.
- SOLID MASONRY WALLS AND GATES EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT SHALL SCREEN ALL GROUND MOUNTED MECHANICAL EQUIPMENT.
- ANY ROOF ACCESS LADDERS SHALL BE LOCATED INSIDE THE BUILDING. ROOF DRAINAGE SHALL UTILIZE INTERIOR ROOF DRAINS OR BE ARCHITECTURALLY INTEGRATED INTO THE BUILDING DESIGN. ARCHITECTURALLY INTEGRATED ROOF DRAINS SHALL REQUIRE ADDITIONAL ARTICULATION BEYOND PAINT ACCENTS. SECTION 35-1902 (B)(15), ZONING CODE.
- SCREENING SHALL BE ARCHITECTURALLY INTEGRATED FOR THE SERVICE ENTRANCE SECTION (SES) AND ALL UTILITIES. ALL GROUND MOUNTED EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW BY A CONCRETE OR MASONRY WALL WITH SOLID GATES, EQUAL TO OR GREATER IN HEIGHT THAN THE MECHANICAL EQUIPMENT. SECTION 35-1902 (B)(13), ZONING CODE.
- SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.

EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS	F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS	G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND	H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK	I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER	J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

ELEVATION KEY NOTES: #

- EXTERIOR STUCCO SYSTEM
- METAL PARAPET CAP
- STOREFRONT SYSTEM
- FOAM POP-OUT
- DOOR PER PLAN
- WINDOW PER PLAN
- STUCCO CONTROL JOINT
- GARAGE DOOR
- METAL PATIO/BALCONY RAILING
- FRAMED PATIO/BALCONY WALL
- TOWER ELEMENT.
- HORIZONTAL LAP SIDING.
- DECORATIVE METAL FEATURE.
- SLIDING GLASS DOOR PER PLAN.
- MASONRY VENEER
- METAL AWNINGS
- TELESCOPING DOOR SYSTEM
- MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
- FIBER CEMENT SIDING



biltform architecture group, inc.

11460 north cave creek road, suite 11
phoenix, arizona 85020
Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS:

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JOB NO: 23-031
SCALE: 3/32" = 1'-0"
SHEET NO:

A3.7

BUILDING 1 - ELEVATIONS

© BILTFORM ARCHITECTURE GROUP, INC.



A NORTH ELEVATION



B WEST ELEVATION



C EAST ELEVATION



D SOUTH COURTYARD ELEVATION



biltform-architecture
group, inc.
11460 north cave creek road . suite 11
phoenix . arizona 85020
Phone 602.285.9200
www.biltform.com

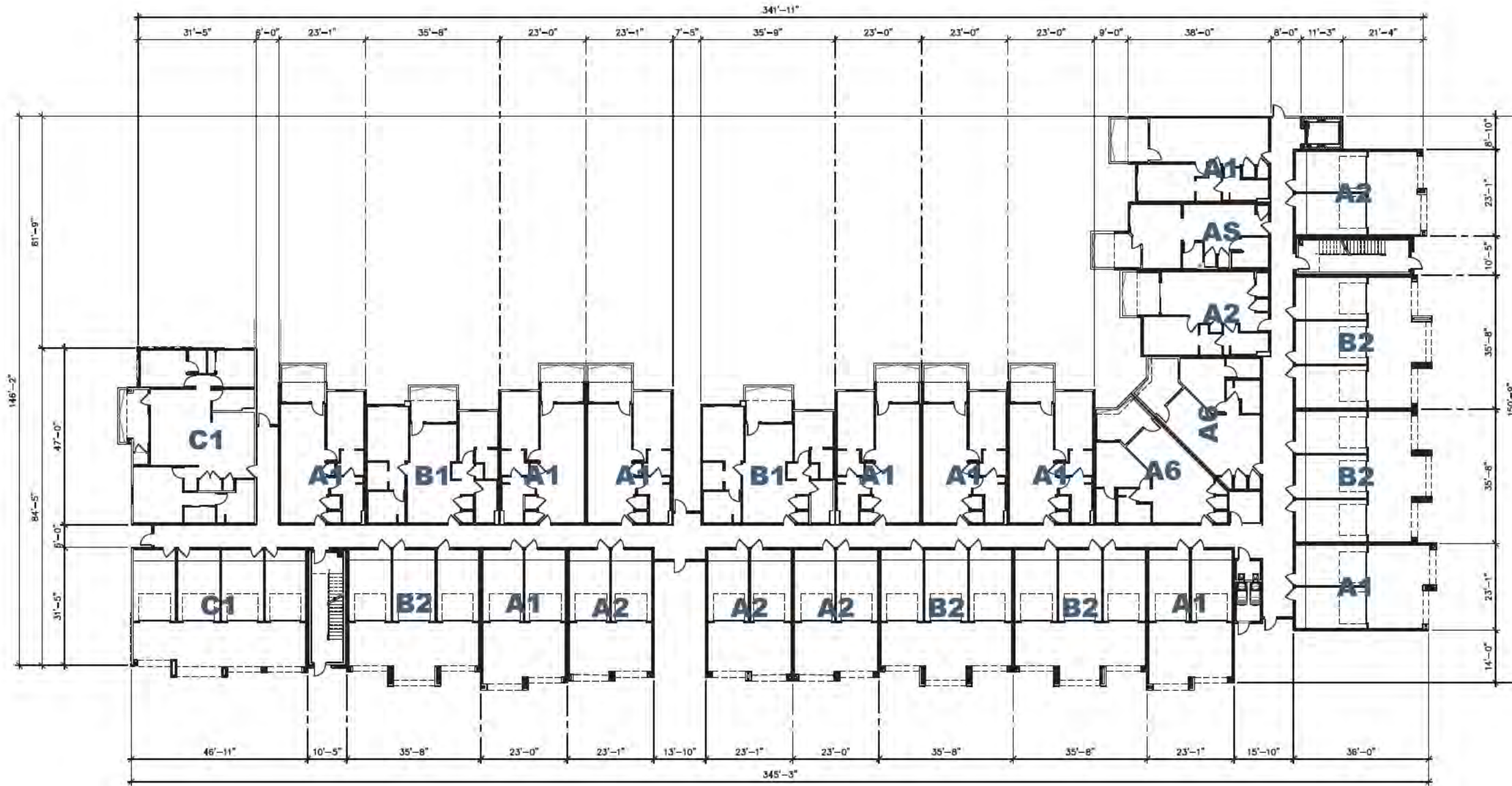
TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

07/29/2024
BUILDING 1
COLOR ELEVATION **EX2**
N.T.S.
preliminary not for construction



biltform architecture group, inc.

11450 north cave creek road - suite 111
duncan, arizona 85020
Phone 602,285,9200 Fax 602,285,9225



EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS	
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JCB/NC: 2/4/21
SCALE: 1/8" = 1'-0"
SHEET NO:

FIRST FLOOR PLAN

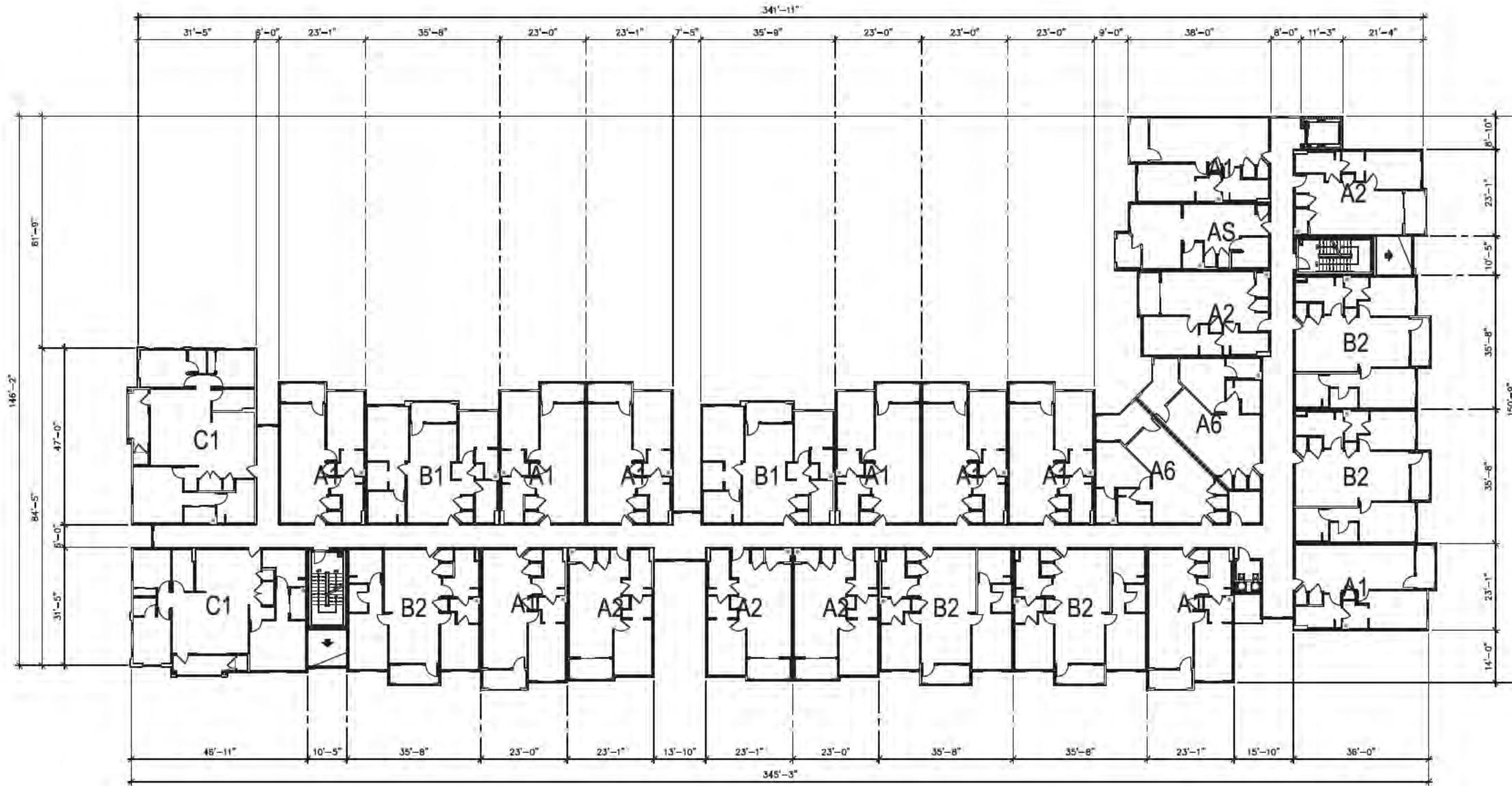
BUILDING 2 - FLOOR PLANS

A4.1



biltform architecture group, inc.

11450 north cave creek road, suite 11
duncan, arizona 85020
Phone 602.285.0200 Fax 602.285.0225



EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS	
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JCB/NC: 2/4/21
SCALE: 1/8" = 1'-0"
SHEET NO:

SECOND FLOOR PLAN

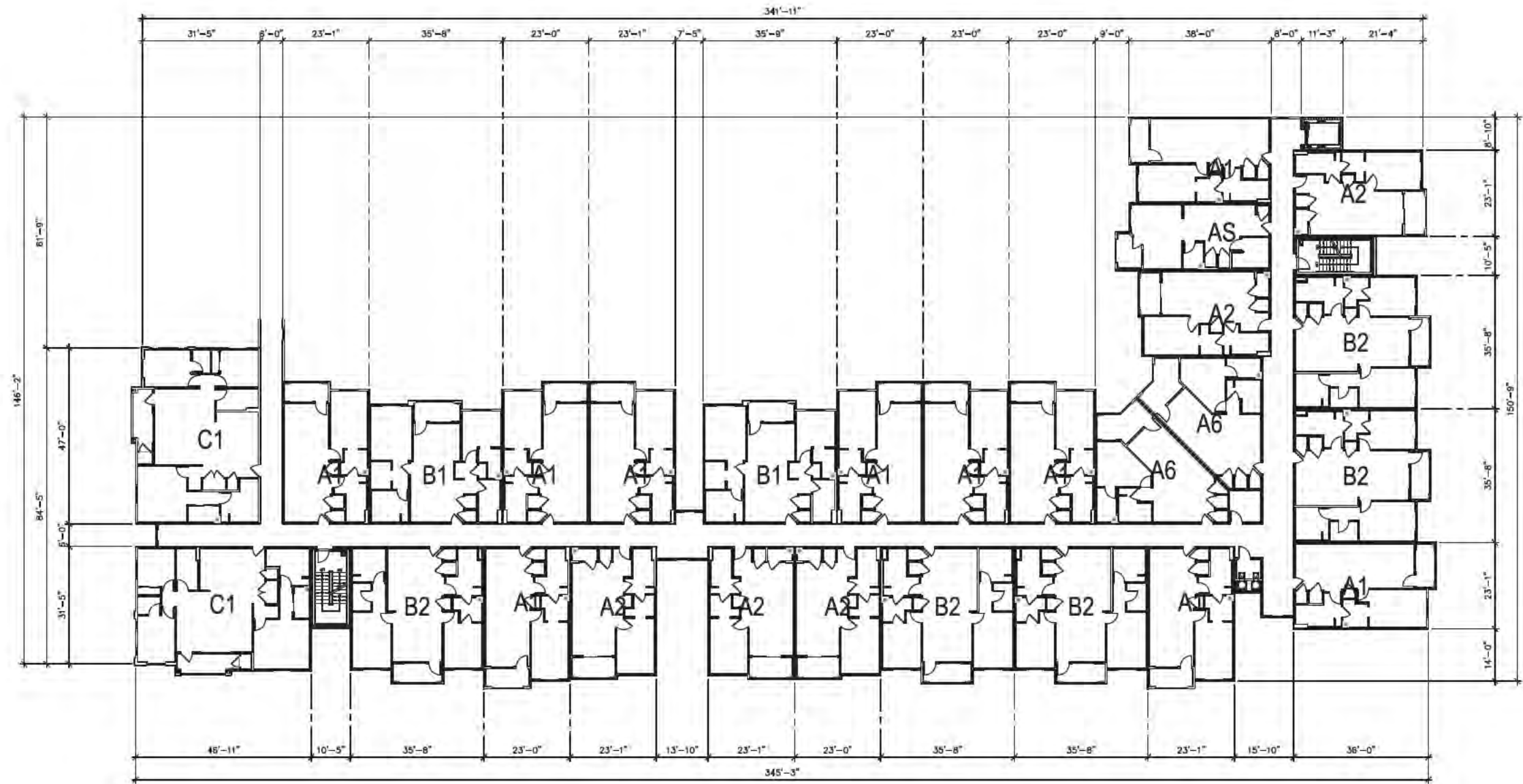
BUILDING 2 - FLOOR PLANS

A4.2



biftform architecture group, inc.

11450 north cave creek road - suite 11
 phoenix - arizona 85020
 Phone 602.285.0200 Fax 602.285.0225



THIRD FLOOR PLAN

BUILDING 2 - FLOOR PLANS

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS	
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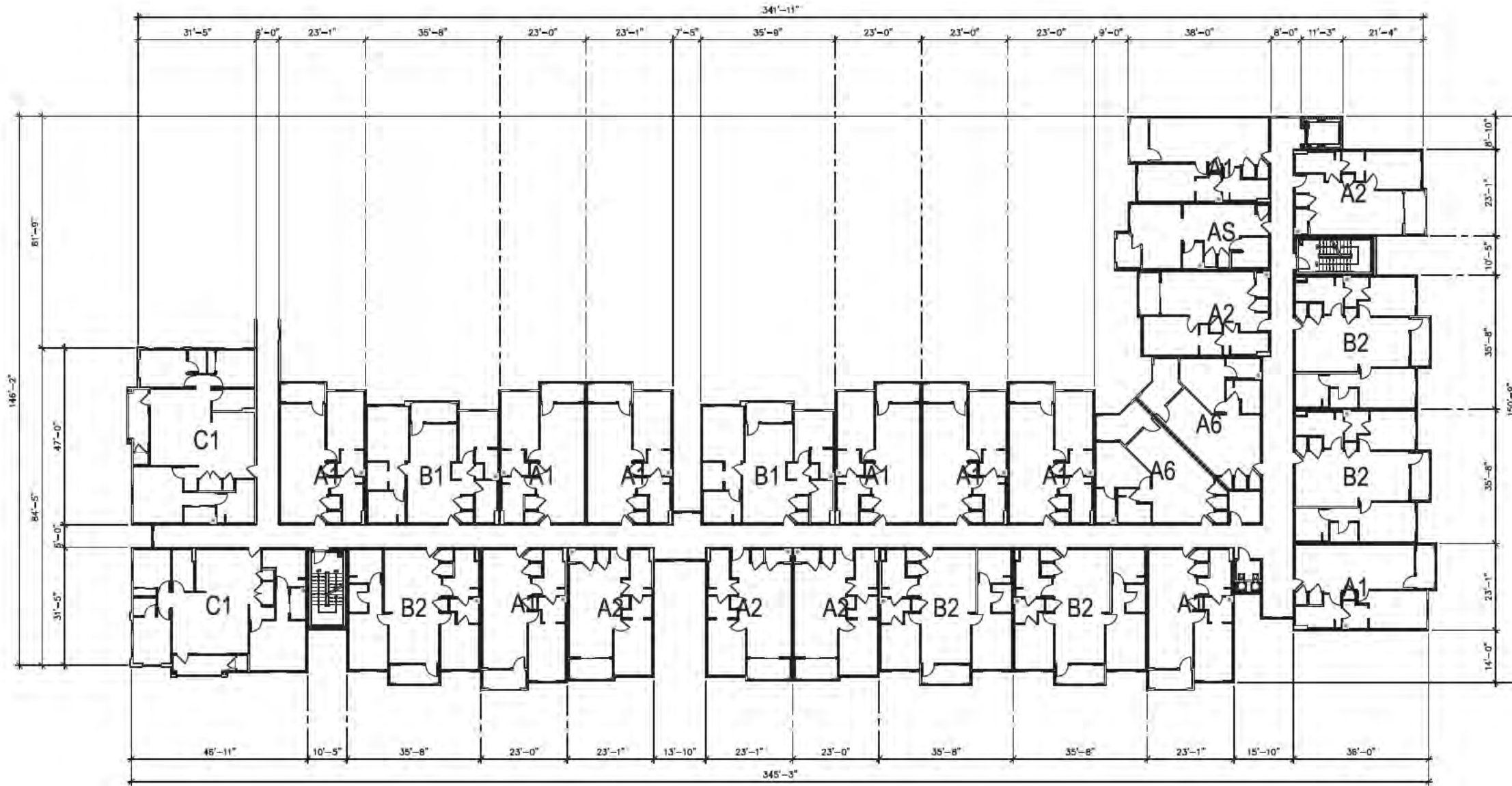
JCB/NC: 25481
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A4.3



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FOURTH FLOOR PLAN

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
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PLANNING
 RESUBMITTAL

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JCB/NC: 2/4/21
 SCALE: 1/8" = 1'-0"
 SHEET NO:

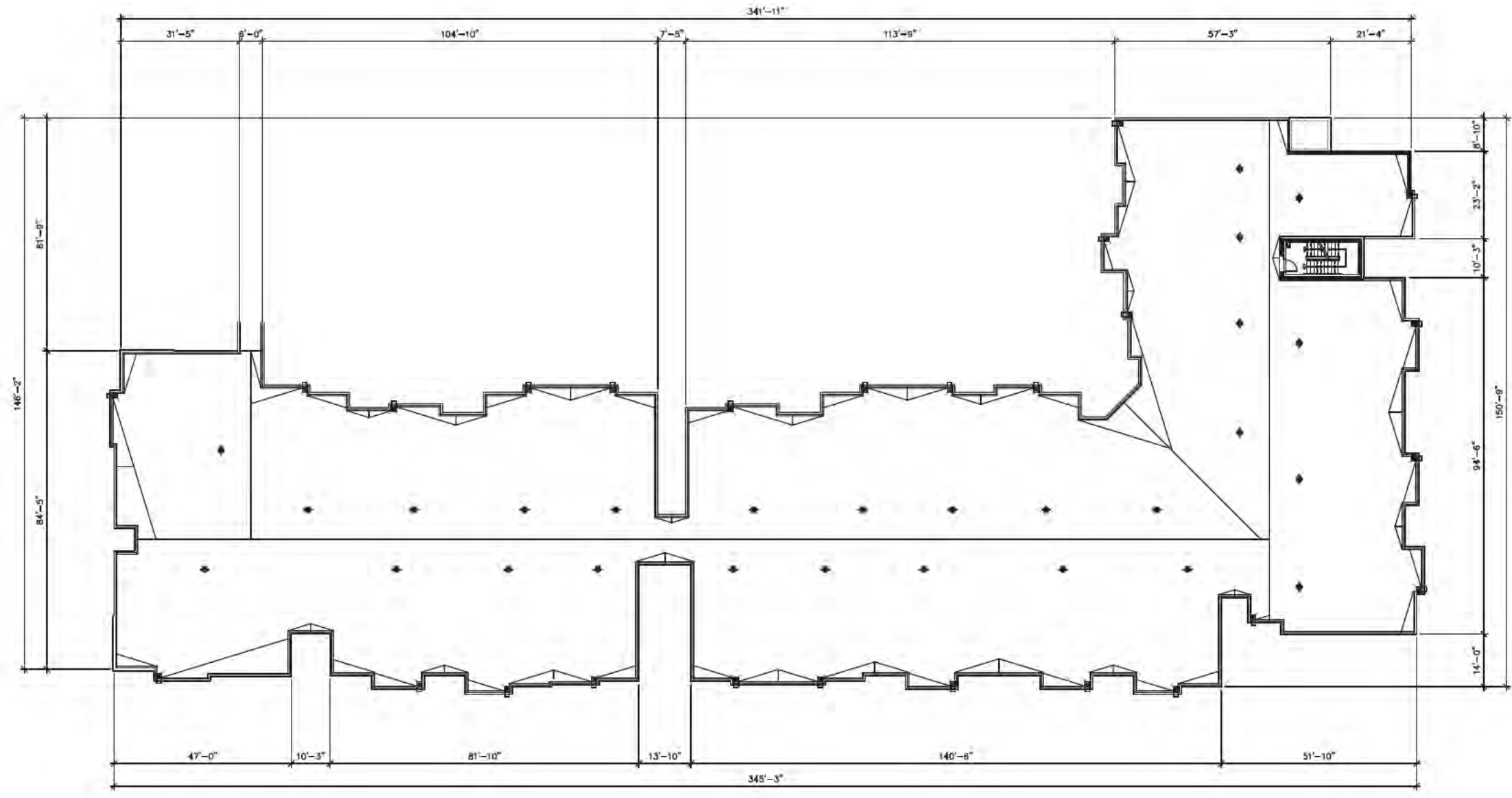
A4.4

BUILDING 2 - FLOOR PLANS



biform architecture
group, inc.

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chandler, arizona 85020
Phone 602.285.0000 Fax 602.285.0225



ROOF PLAN

BUILDING 2 - FLOOR PLANS

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS

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JCR/NC: 2/4/21
SCALE: 1/8" = 1'-0"
SHEET NO:

A4.5

GENERAL ELEVATION NOTES:

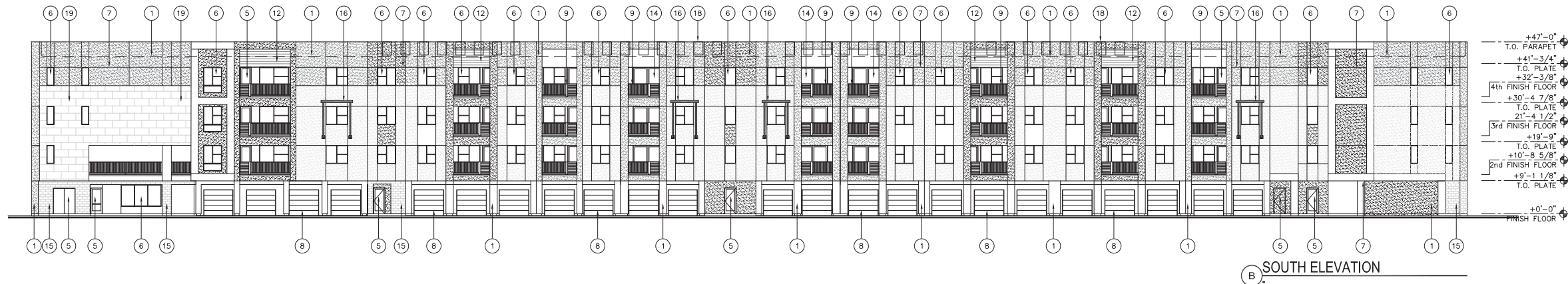
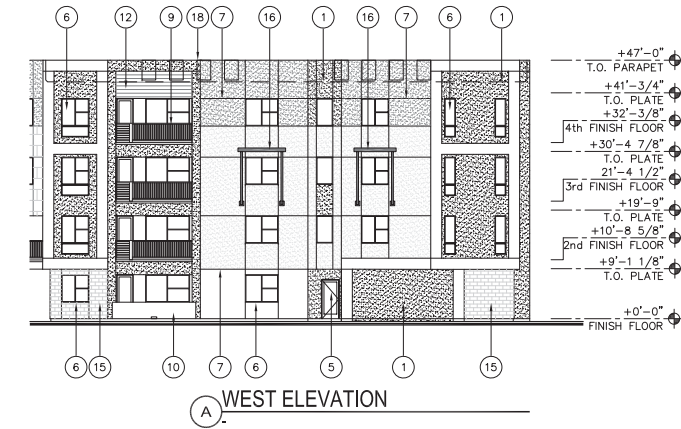
- ALL ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY PARAPET WALLS EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT.
- SOLID MASONRY WALLS AND GATES EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT SHALL SCREEN ALL GROUND MOUNTED MECHANICAL EQUIPMENT.
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- SCREENING SHALL BE ARCHITECTURALLY INTEGRATED FOR THE SERVICE ENTRANCE SECTION (SES) AND ALL UTILITIES. ALL GROUND MOUNTED EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW BY A CONCRETE OR MASONRY WALL WITH SOLID GATES, EQUAL TO OR GREATER IN HEIGHT THAN THE MECHANICAL EQUIPMENT. SECTION 35-1902 (8)(e)13, ZONING CODE.
- SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.

EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS	F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS	G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND	H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK	I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER	J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

ELEVATION KEY NOTES: #

- EXTERIOR STUCCO SYSTEM
- METAL PARAPET CAP
- STOREFRONT SYSTEM
- FOAM POP-OUT
- DOOR PER PLAN
- WINDOW PER PLAN
- STUCCO CONTROL JOINT
- GARAGE DOOR
- METAL PATIO/BALCONY RAILING
- FRAMED PATIO/BALCONY WALL
- TOWER ELEMENT.
- HORIZONTAL LAP SIDING.
- DECORATIVE METAL FEATURE.
- SLIDING GLASS DOOR PER PLAN.
- MASONRY VENEER
- METAL AWINGS
- TELESCOPING DOOR SYSTEM
- MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
- FIBER CEMENT SIDING



biltform architecture group, inc.

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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN Apartment Homes CHANDLER, ARIZONA TOLL BROTHERS APARTMENT LIVING 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING RESUBMITTAL

REVISIONS:

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JOB NO: 23-331 SCALE: 3/32" = 1'-0" SHEET NO:

A4.6

BUILDING 2 - ELEVATIONS

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GENERAL ELEVATION NOTES:

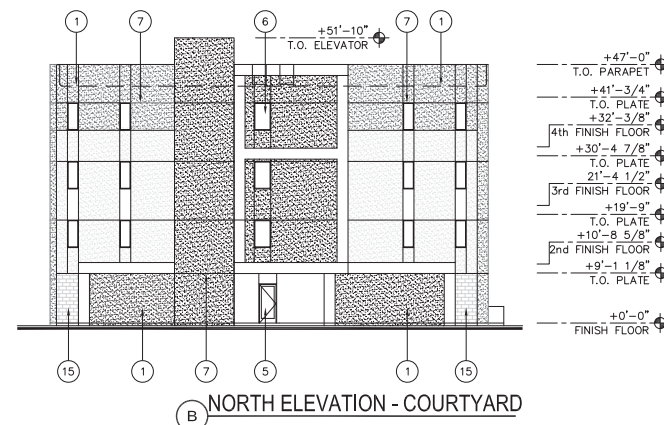
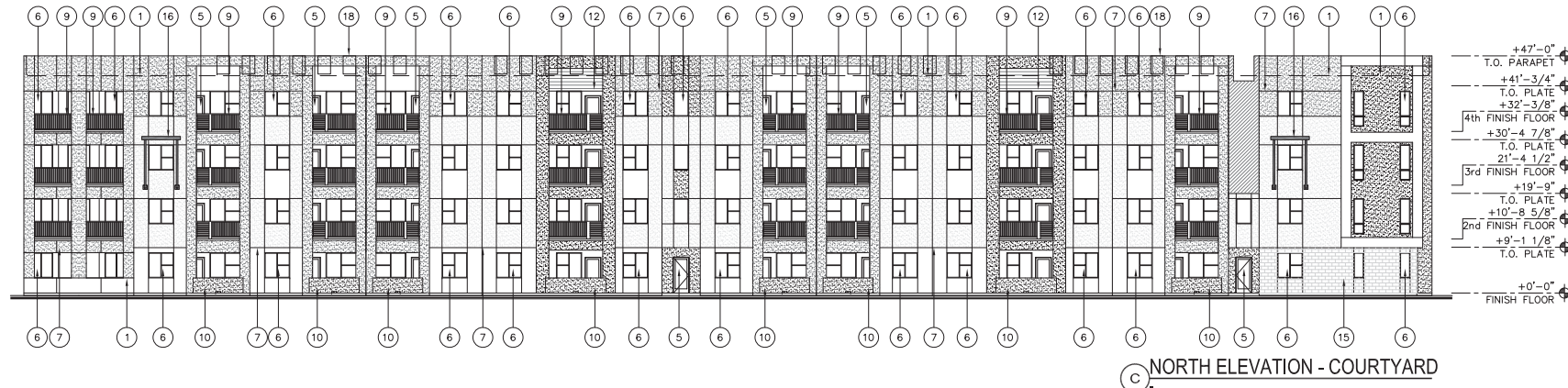
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- SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.

EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS	F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS	G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND	H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK	I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER	J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

ELEVATION KEY NOTES: #

- EXTERIOR STUCCO SYSTEM
- METAL PARAPET CAP
- STOREFRONT SYSTEM
- FOAM POP-OUT
- DOOR PER PLAN
- WINDOW PER PLAN
- STUCCO CONTROL JOINT
- GARAGE DOOR
- METAL PATIO/BALCONY RAILING
- FRAMED PATIO/BALCONY WALL
- TOWER ELEMENT.
- HORIZONTAL LAP SIDING.
- DECORATIVE METAL FEATURE.
- SLIDING GLASS DOOR PER PLAN.
- MASONRY VENEER
- METAL AWINGS
- TELESCOPING DOOR SYSTEM
- MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
- FIBER CEMENT SIDING



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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS:

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JOB NO: 23-031
SCALE: 3/32" = 1'-0"
SHEET NO:

A4.7

BUILDING 2 - ELEVATIONS



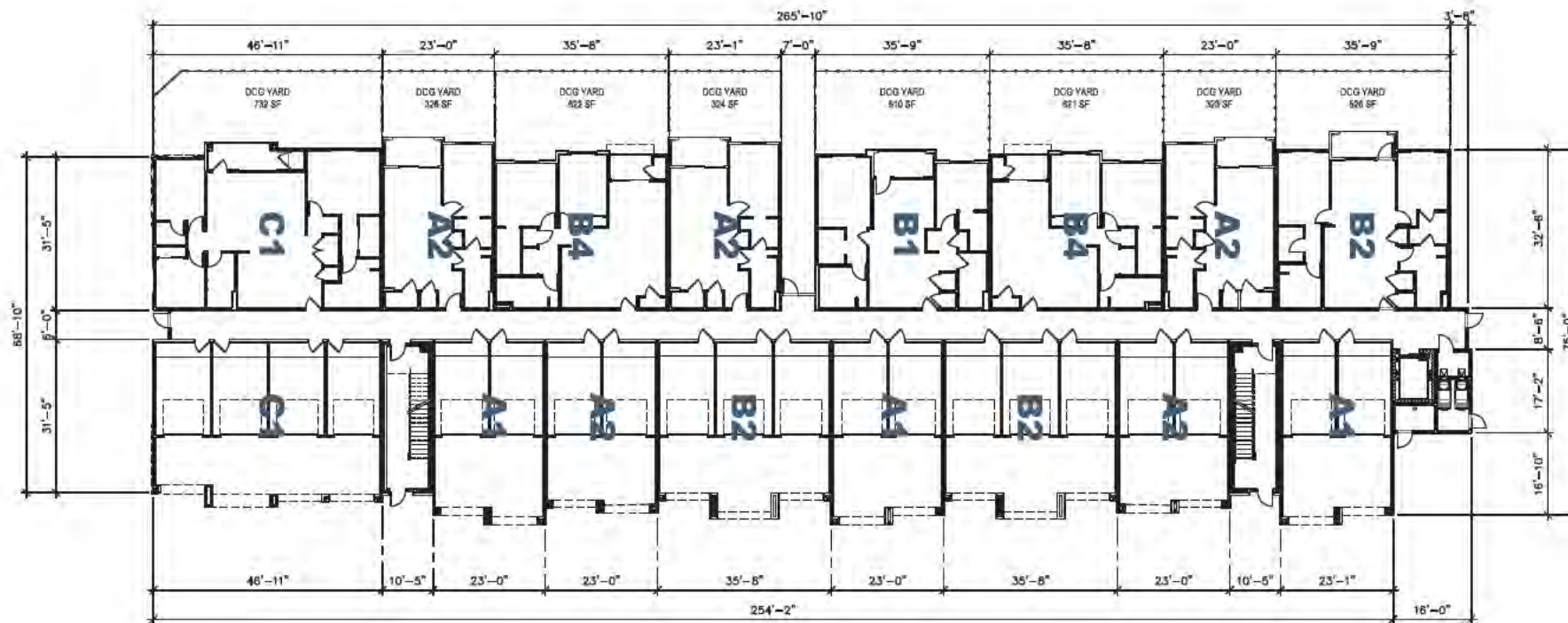
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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

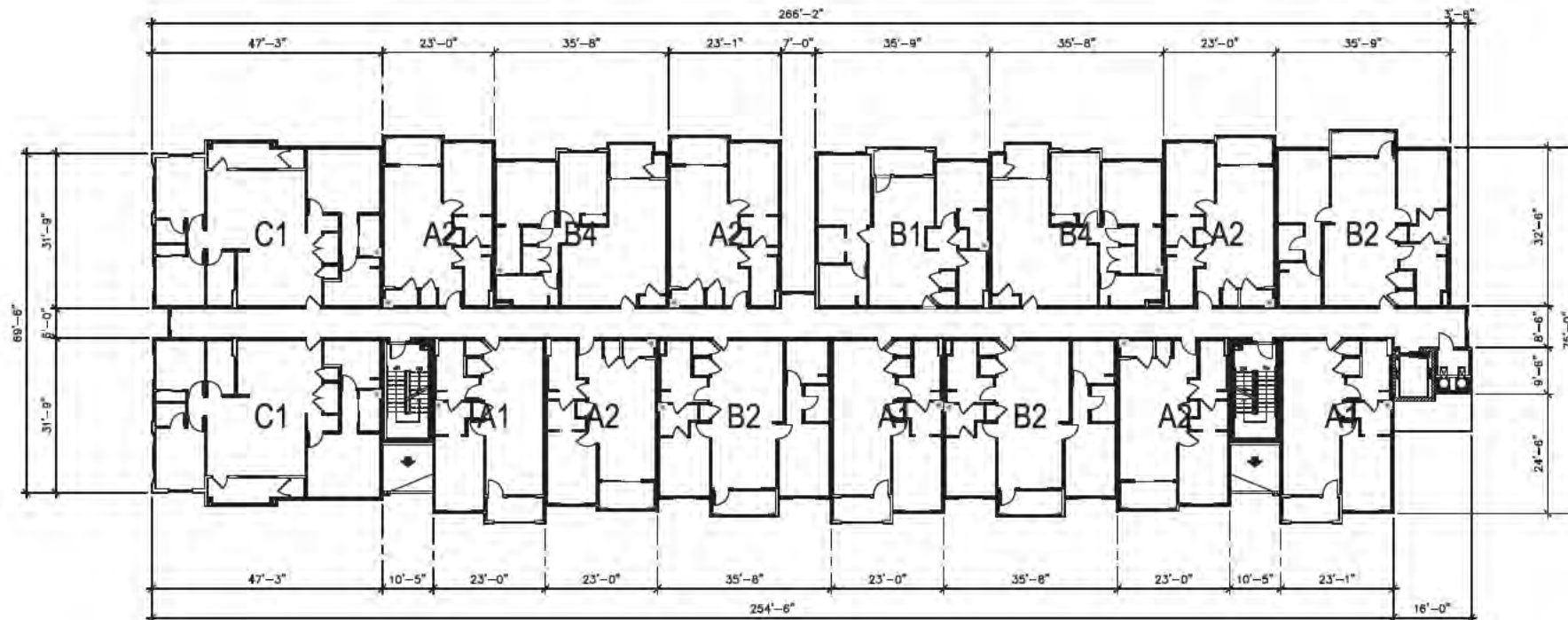
BUILDING 2
COLOR ELEVATION
N.T.S
preliminary not for construction

EX3

07/29/2024



FIRST FLOOR PLAN



SECOND FLOOR PLAN

BUILDING 3 - FLOOR PLANS



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EXPIRES 9/30/25

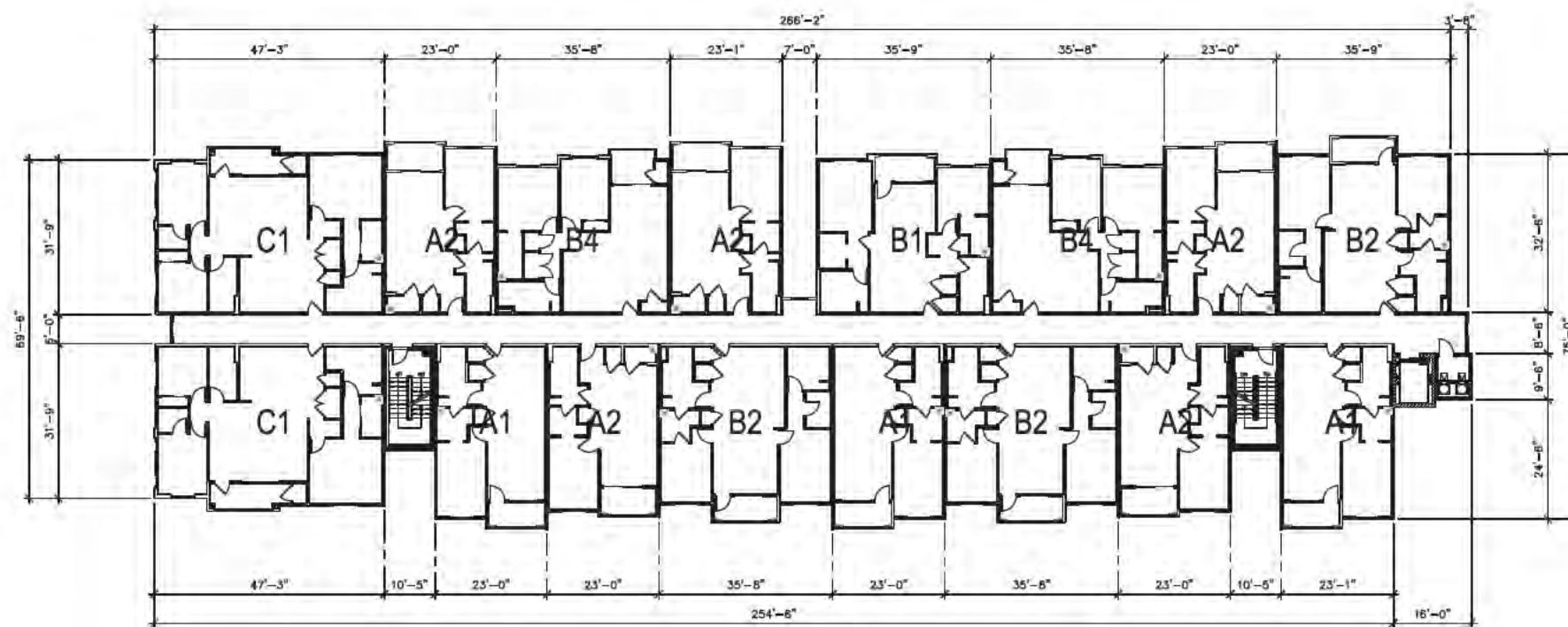
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

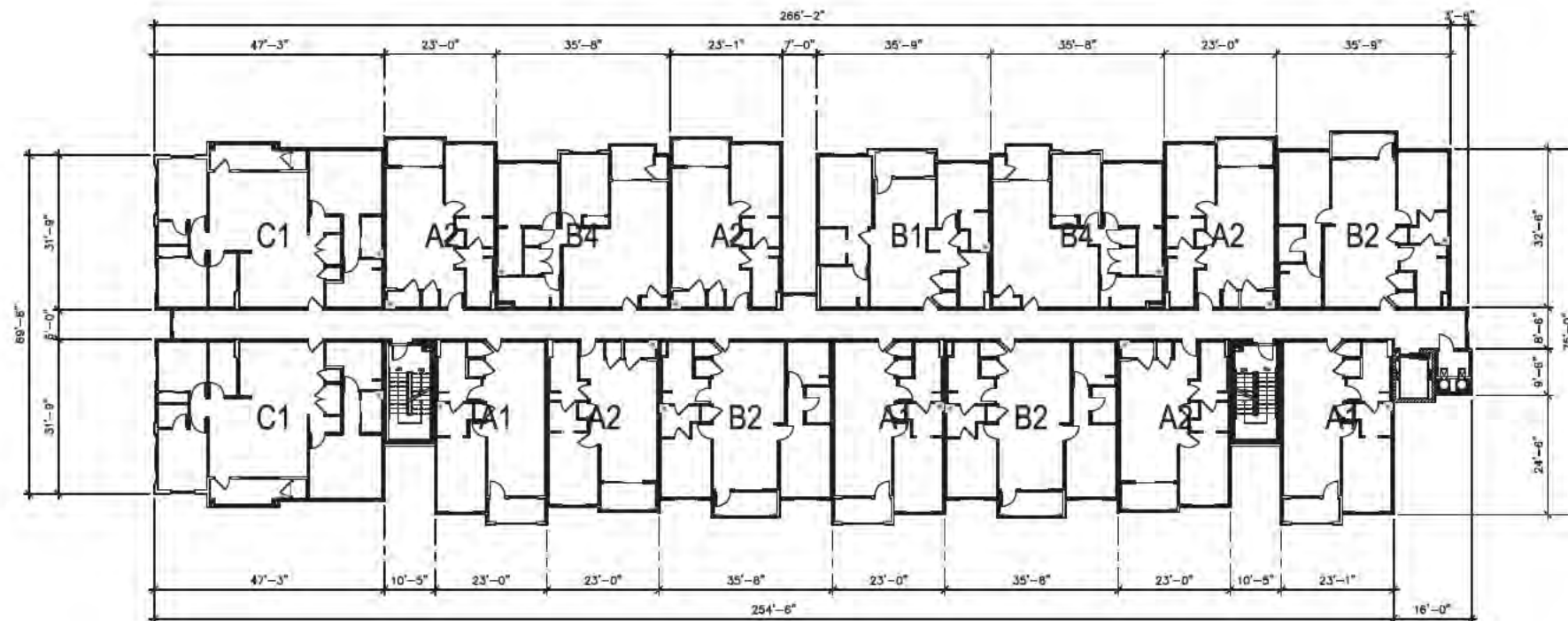
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JCB/NC: 2/4/21
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A5.1



THIRD FLOOR PLAN



FOURTH FLOOR PLAN

BUILDING 3 - FLOOR PLANS



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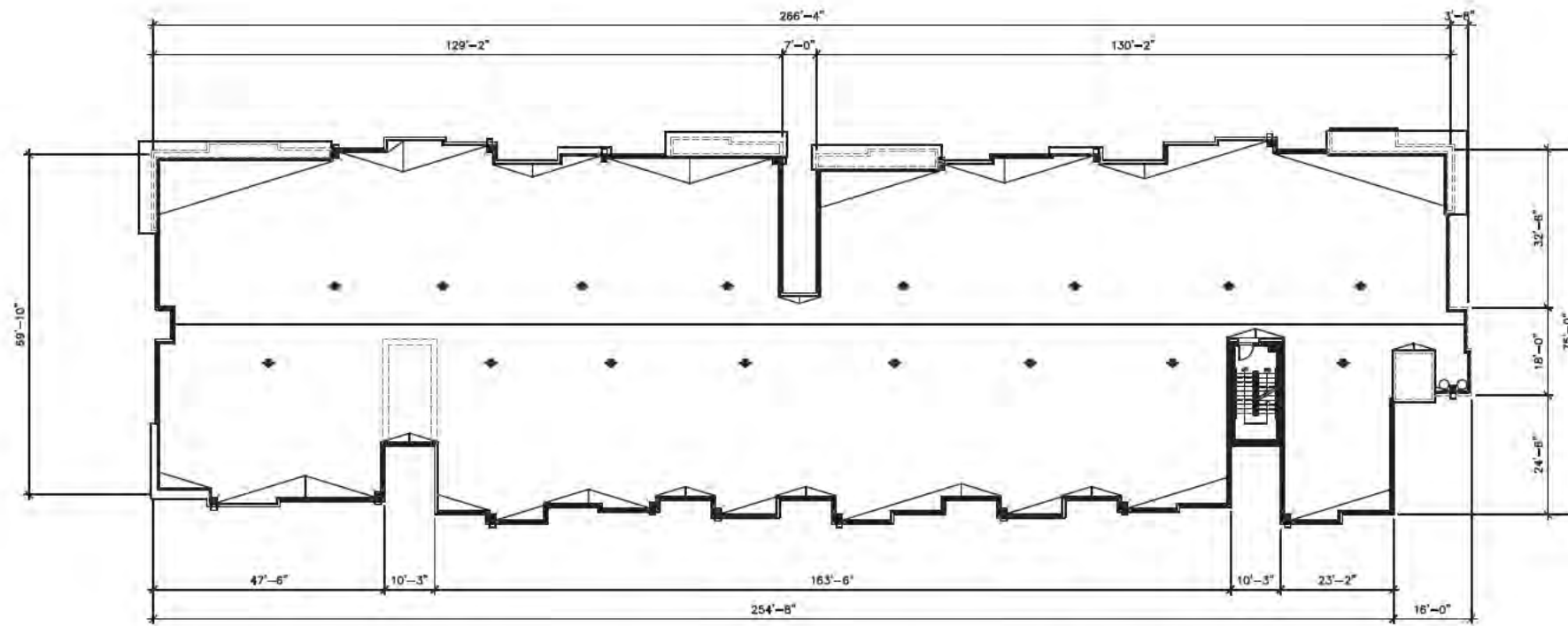
EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING RESUBMITTAL

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JCR/NC: 2/4/21
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A5.2



ROOF PLAN



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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN

Apartment Homes
CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

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JCB/NC: 25-201
SCALE: 1/8" = 1'-0"
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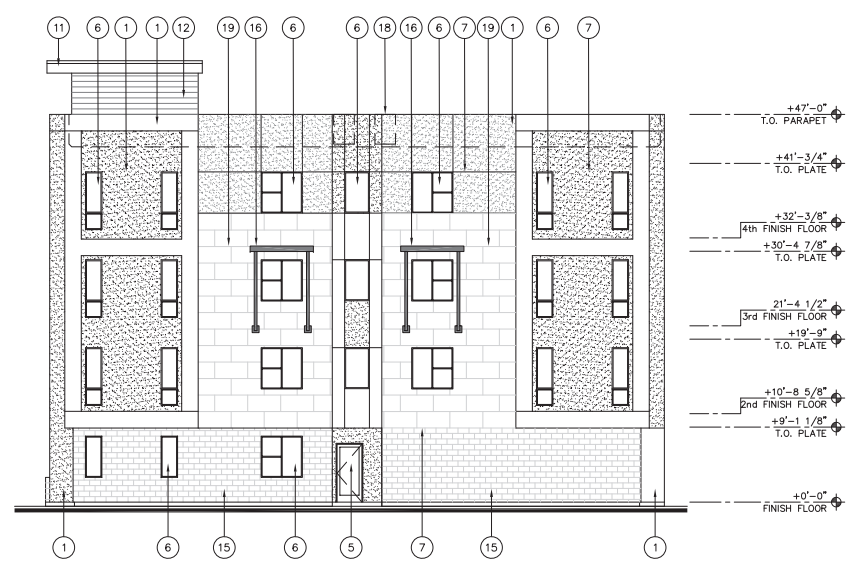
A5.3

BUILDING 3 - FLOOR PLANS

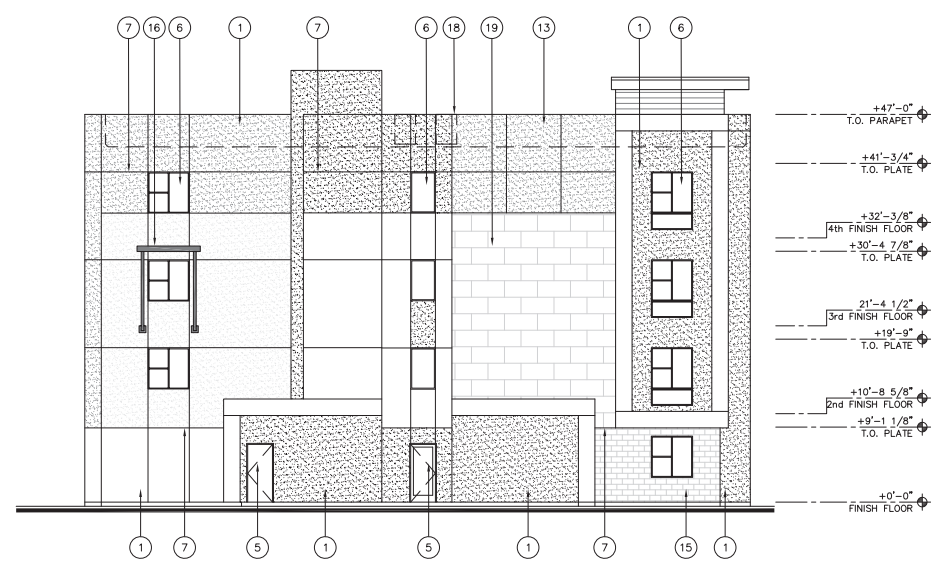


A WEST ELEVATION

- ELEVATION KEY NOTES: #**
- EXTERIOR STUCCO SYSTEM
 - METAL PARAPET CAP
 - STOREFRONT SYSTEM
 - FOAM POP-OUT
 - DOOR PER PLAN
 - WINDOW PER PLAN
 - STUCCO CONTROL JOINT
 - GARAGE DOOR
 - METAL PATIO/BALCONY RAILING
 - FRAMED PATIO/BALCONY WALL
 - TOWER ELEMENT.
 - HORIZONTAL LAP SIDING.
 - DECORATIVE METAL FEATURE.
 - SLIDING GLASS DOOR PER PLAN.
 - MASONRY VENEER
 - METAL AWNINGS
 - TELESCOPING DOOR SYSTEM
 - MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
 - FIBER CEMENT SIDING



C SOUTH ELEVATION



B NORTH ELEVATION

EXTERIOR COLOR SCHEME		
A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE8105 WEATHERED LEATHER
F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

- GENERAL ELEVATION NOTES:**
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 - SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.



D EAST ELEVATION

BUILDING 3 - ELEVATIONS

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS:

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JOB NO: 23-331
SCALE: 3/32" = 1'-0"
SHEET NO:

A5.4



(A) WEST ELEVATION



(C) SOUTH ELEVATION



(B) NORTH ELEVATION



(D) EAST ELEVATION



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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

07/29/2024
BUILDING 3
COLOR ELEVATION
N.T.S.
EX4
preliminary not for construction



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EXPIRES 9/30/25

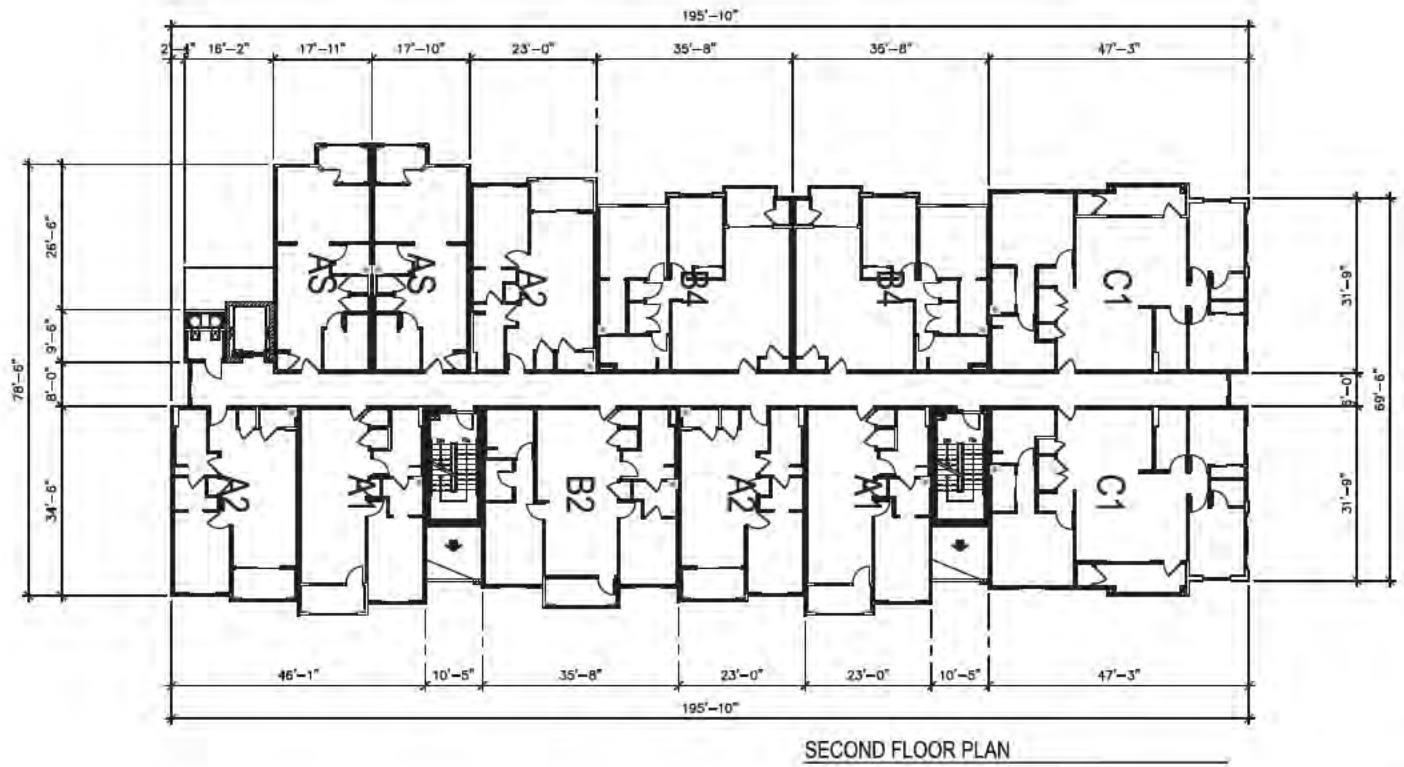
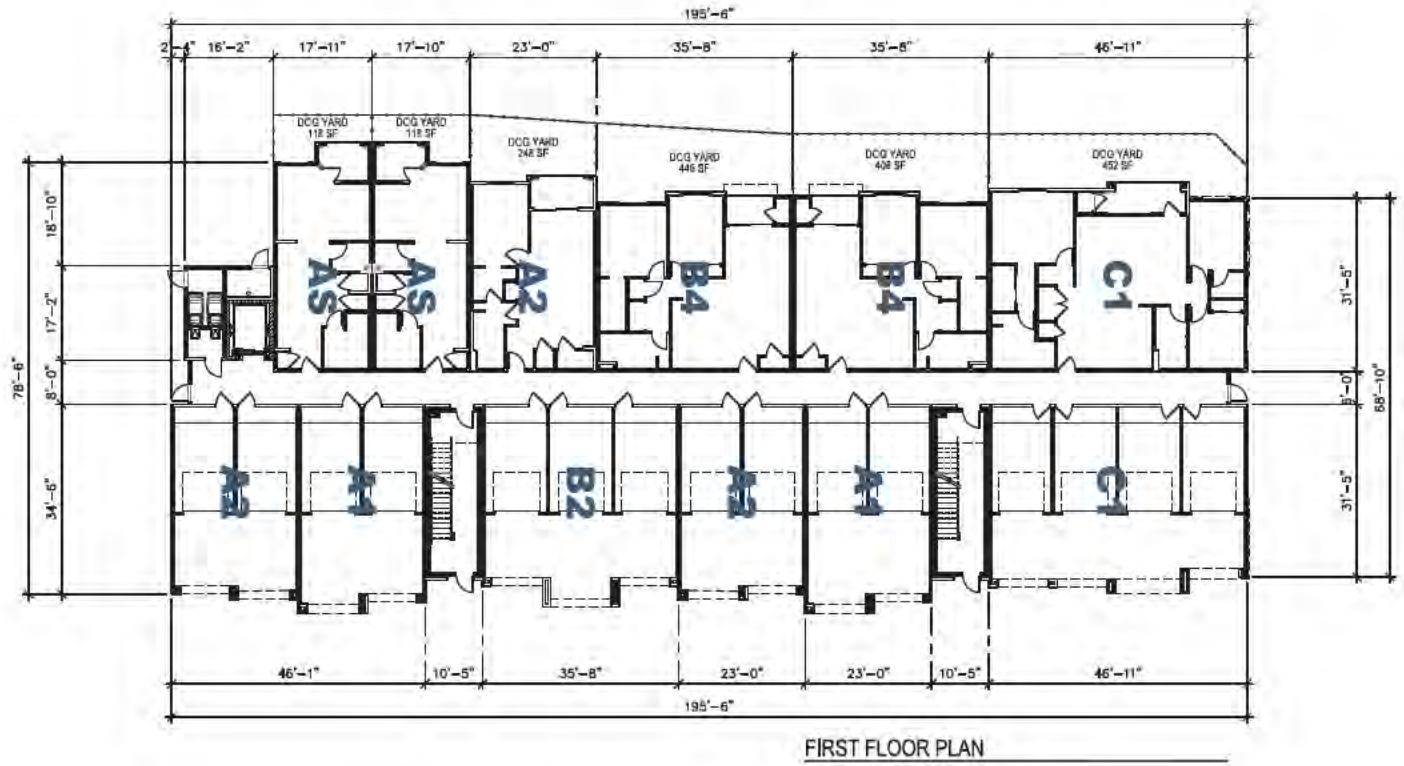
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS
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JCB NO: 24431
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A6.1



BUILDING 4 - FLOOR PLANS



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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN

Apartment Homes
CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING

8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

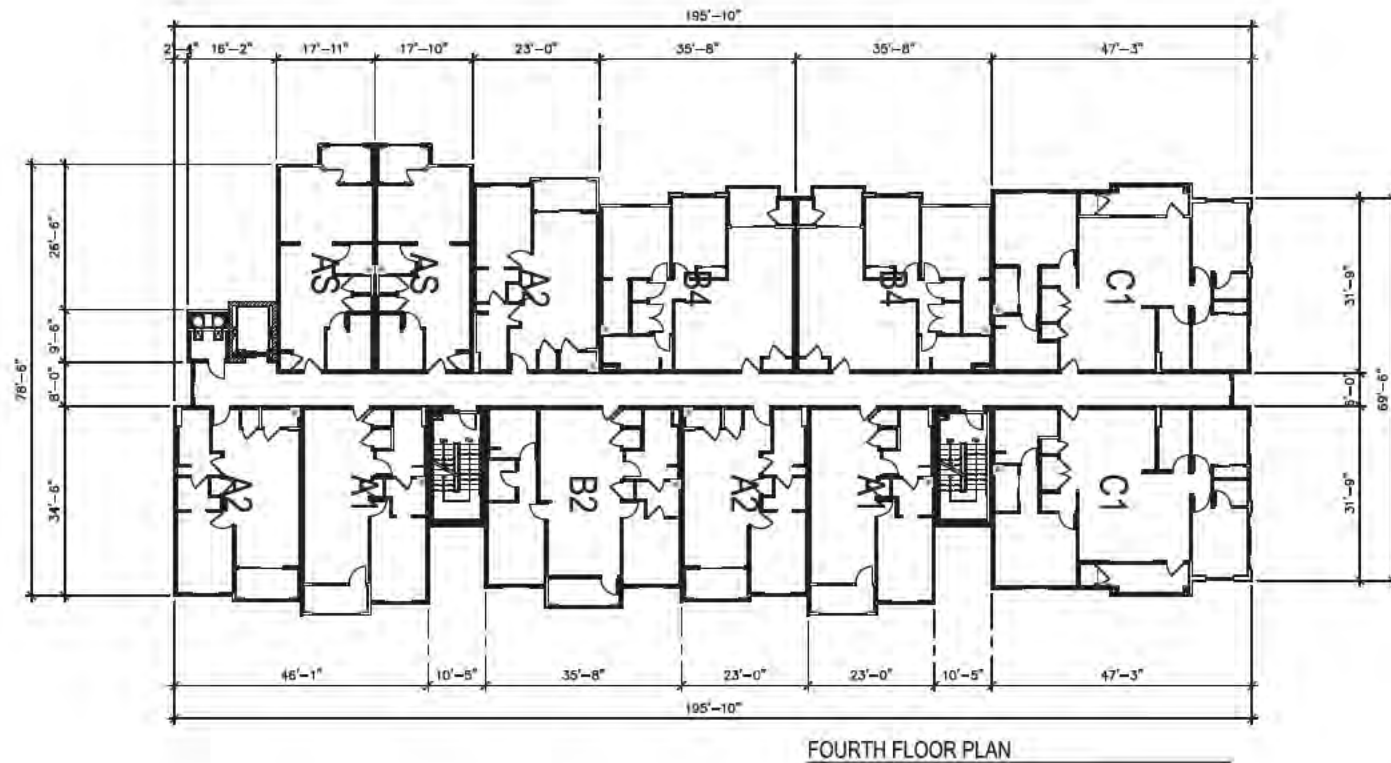
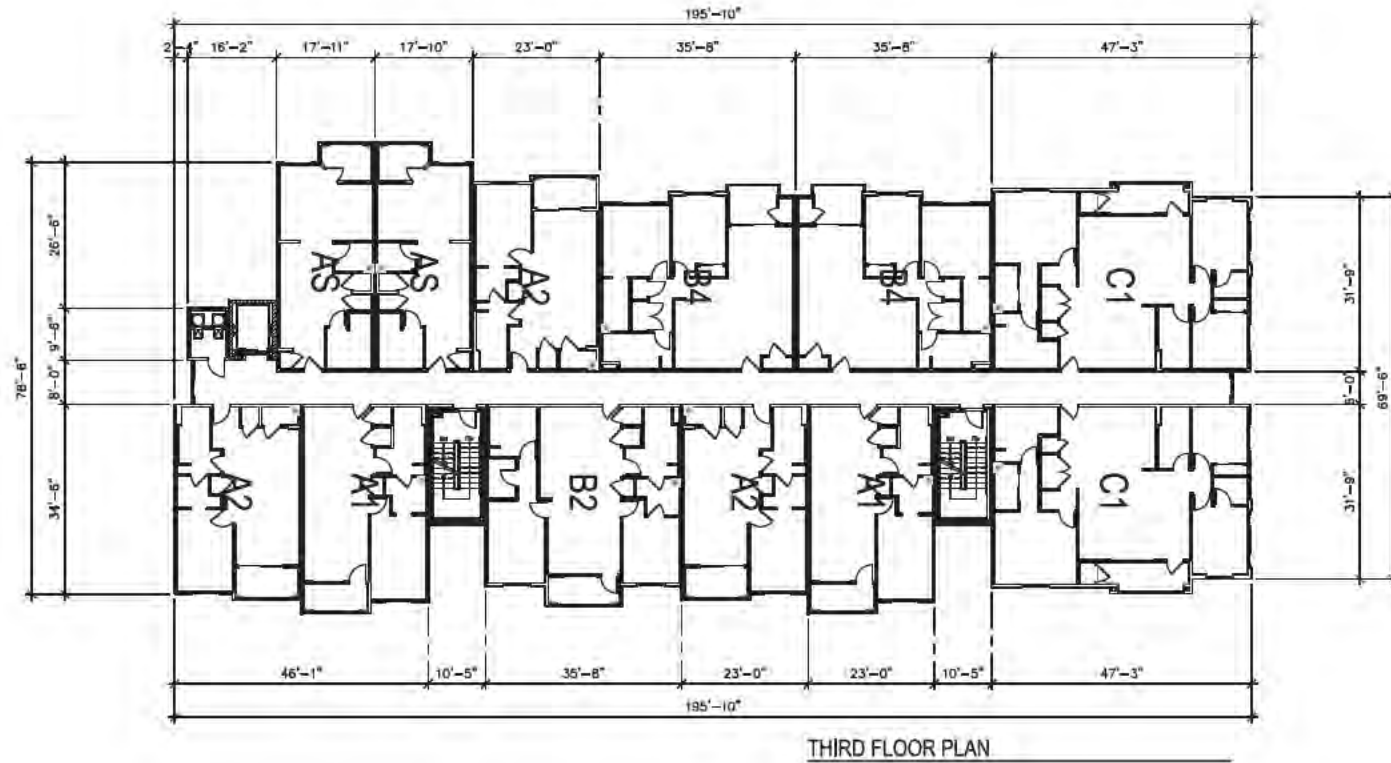
REVISIONS

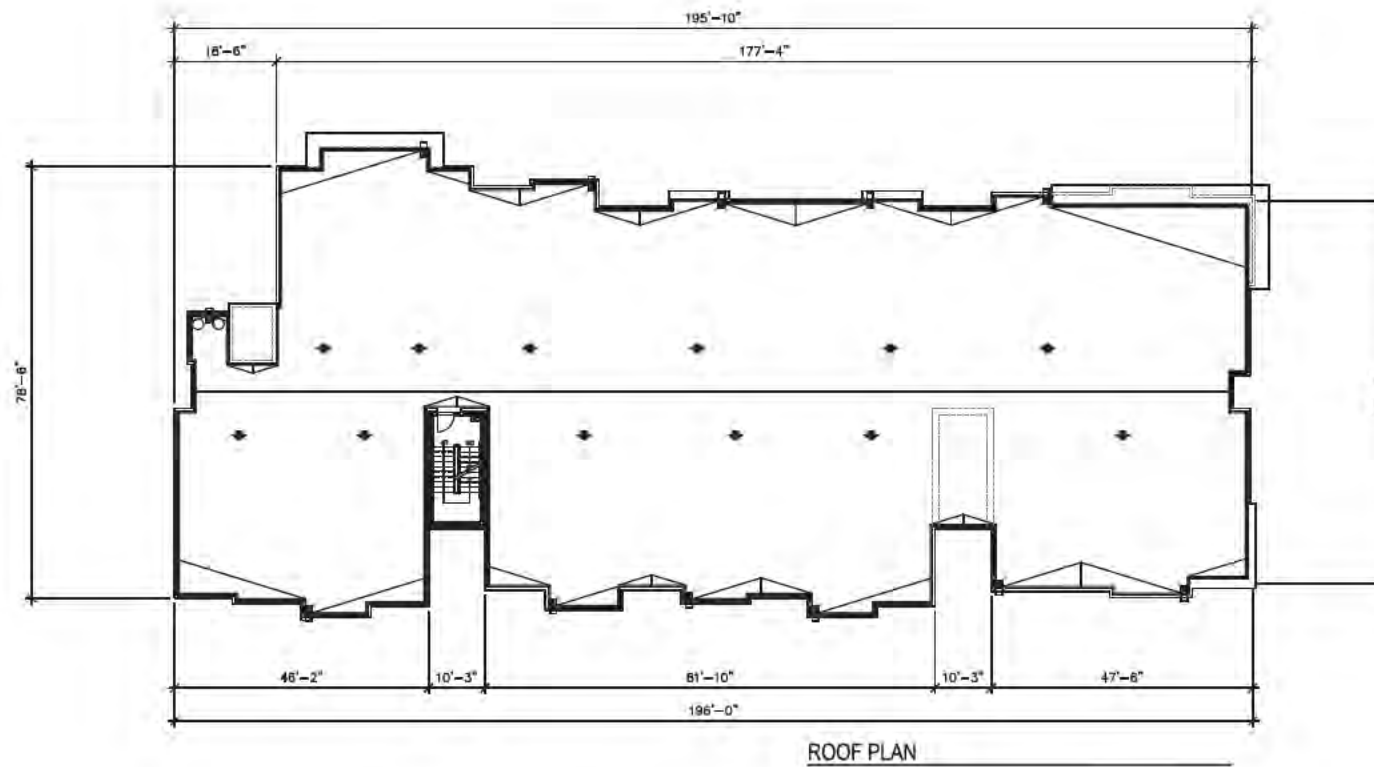
JCB/NC: 2/4/23
SCALE: 1/8" = 1'-0"
SHEET NO:

A6.2

BUILDING 4 - FLOOR PLANS

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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN

Apartment Homes
CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS	
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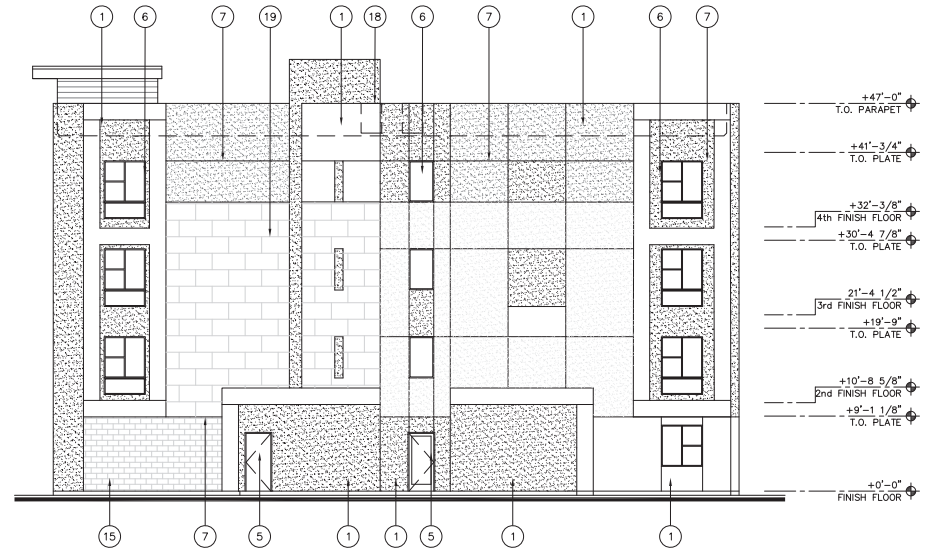
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SHEET NO:

A6.3

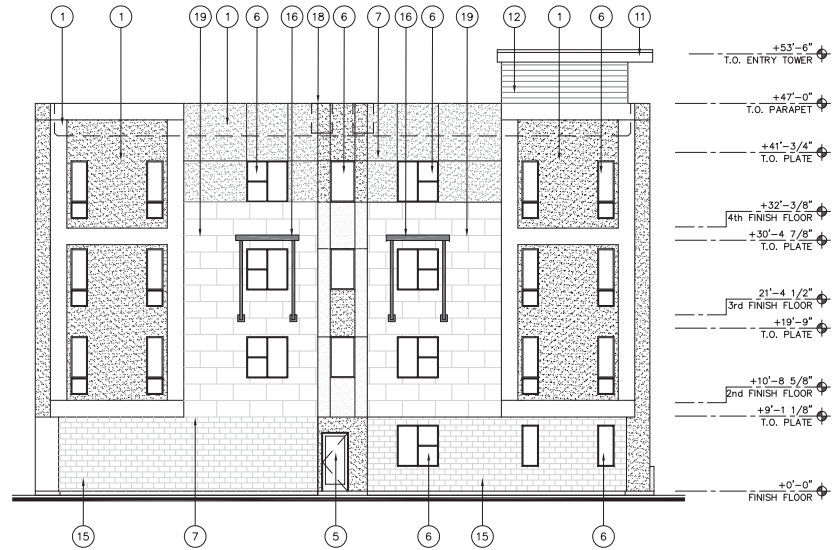
BUILDING 4 - FLOOR PLANS



A WEST ELEVATION



C SOUTH ELEVATION



B NORTH ELEVATION



D EAST ELEVATION

- ELEVATION KEY NOTES: #
- EXTERIOR STUCCO SYSTEM
 - METAL PARAPET CAP
 - STOREFRONT SYSTEM
 - FOAM POP-OUT
 - DOOR PER PLAN
 - WINDOW PER PLAN
 - STUCCO CONTROL JOINT
 - GARAGE DOOR
 - METAL PATIO/BALCONY RAIL
 - FRAMED PATIO/BALCONY WALL
 - TOWER ELEMENT
 - HORIZONTAL LAP SIDING
 - DECORATIVE METAL FEATURE
 - SLIDING GLASS DOOR PER PLAN
 - MASONRY VENEER
 - METAL AWNINGS
 - TELESCOPING DOOR SYSTEM
 - MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
 - FIBER CEMENT SIDING

EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER
F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X10"
H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

- GENERAL ELEVATION NOTES:
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 - SIGNS REQUIRE A SEPARATE SUBMITTAL AND PERMIT.



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 11460 north cave creek road, suite 111
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 Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING RESUBMITTAL

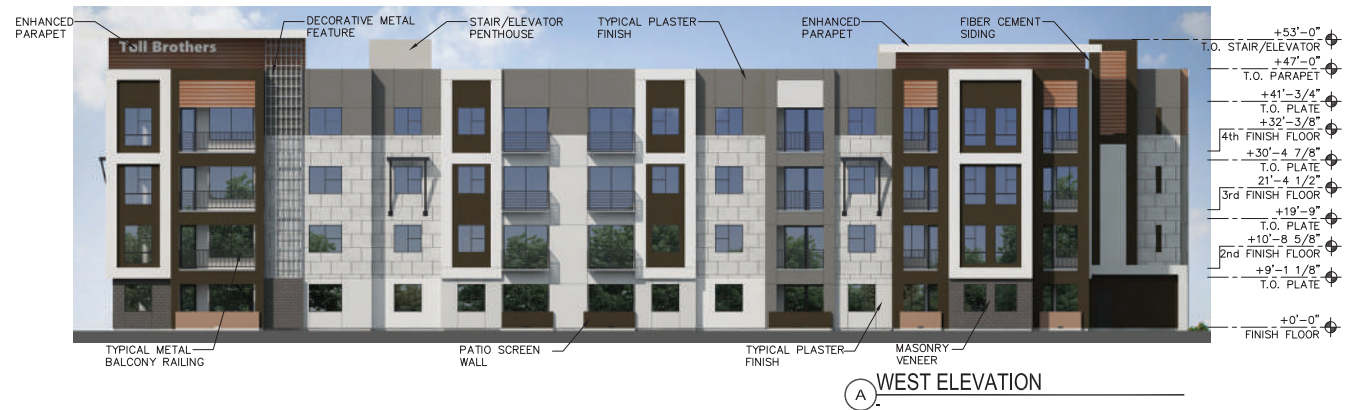
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3	-	-
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5	-	-
6	-	-

JOB NO: 23-331
 SCALE: 3/32" = 1'-0"
 SHEET NO:

A6.4

BUILDING 4 - ELEVATIONS



A WEST ELEVATION



C SOUTH ELEVATION



B NORTH ELEVATION



D EAST ELEVATION



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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

BUILDING 4
COLOR ELEVATION
N.T.S
preliminary not for construction

07/29/2024
EX5



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EXPIRES 9/30/25

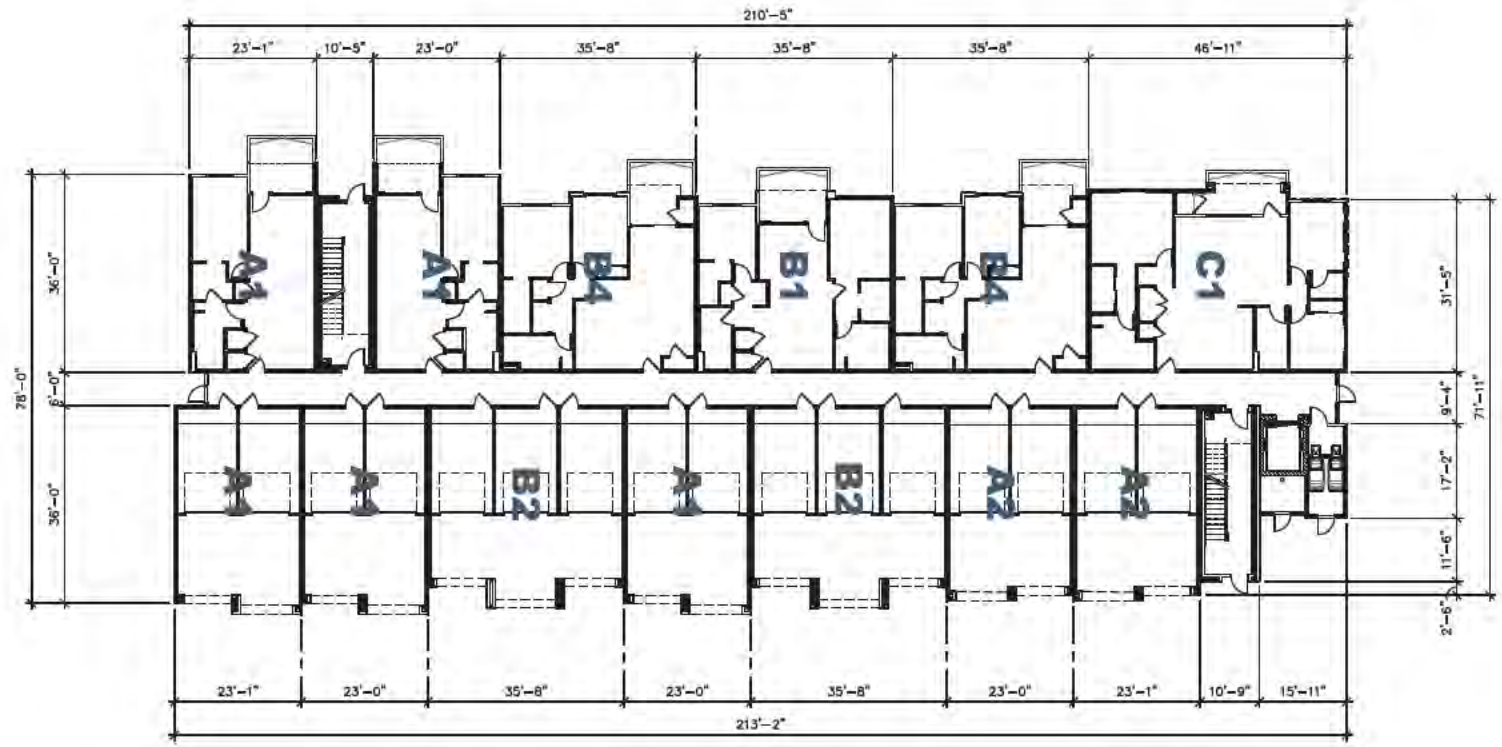
TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

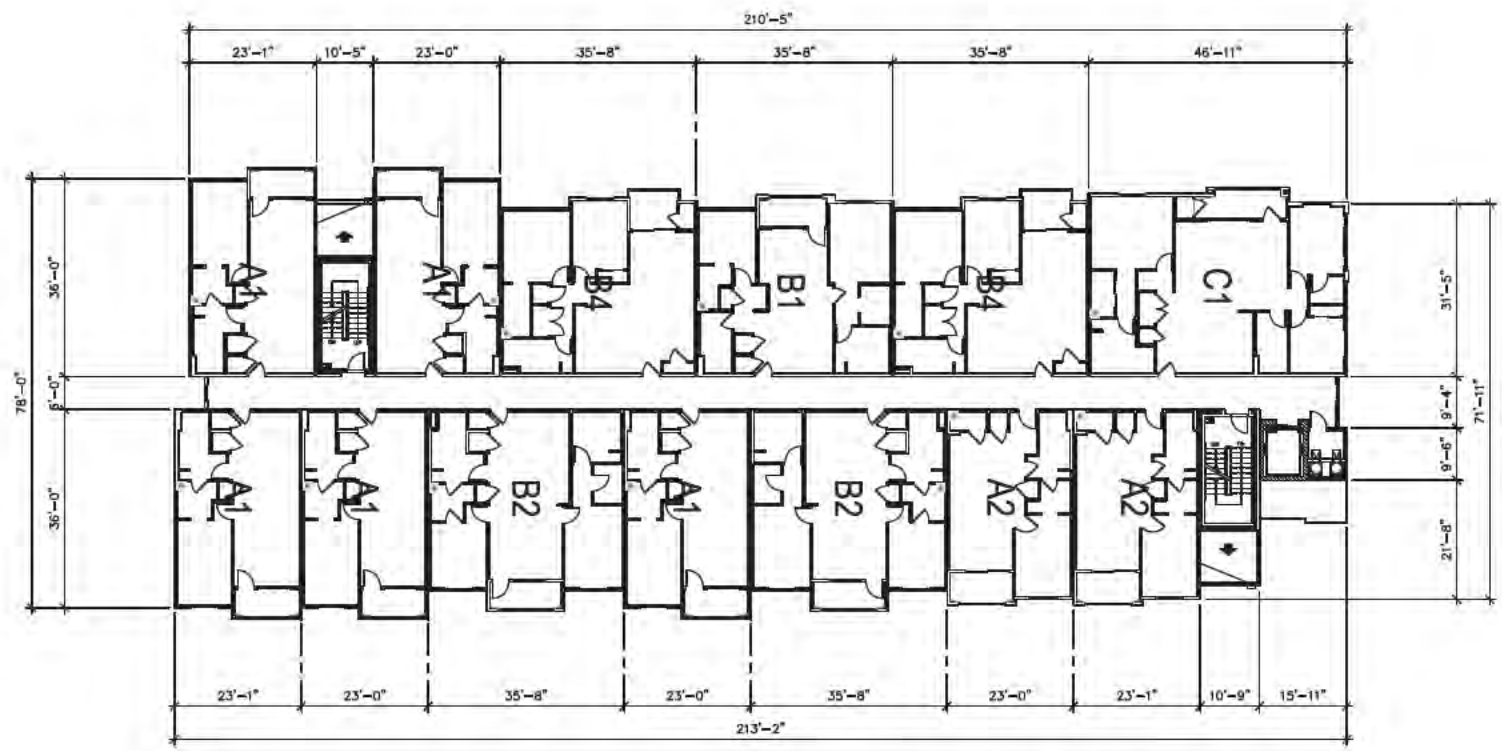
REVISIONS	
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JC/WC: 2/4/23
SCALE: 1/8" = 1'-0"
SHEET NO:

A7.1

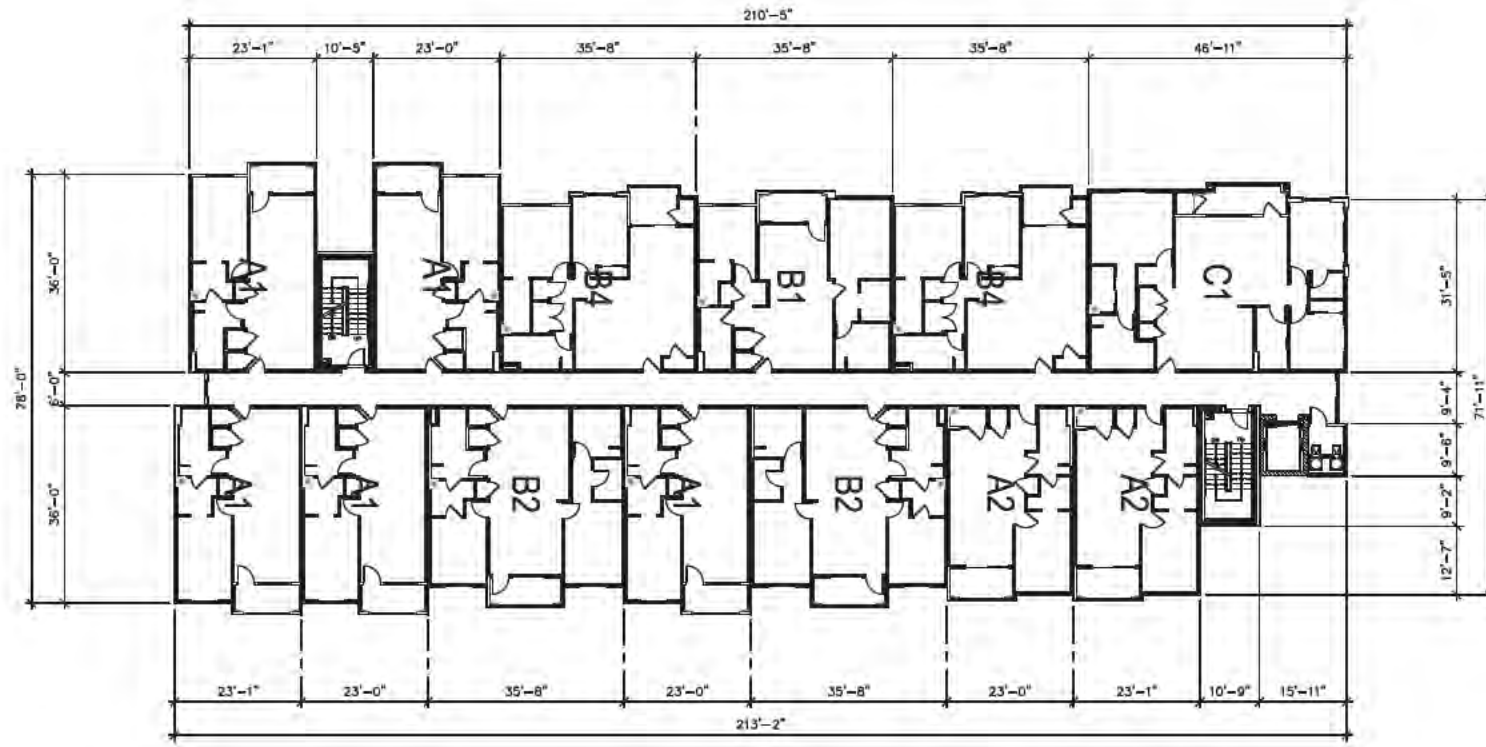


FIRST FLOOR PLAN

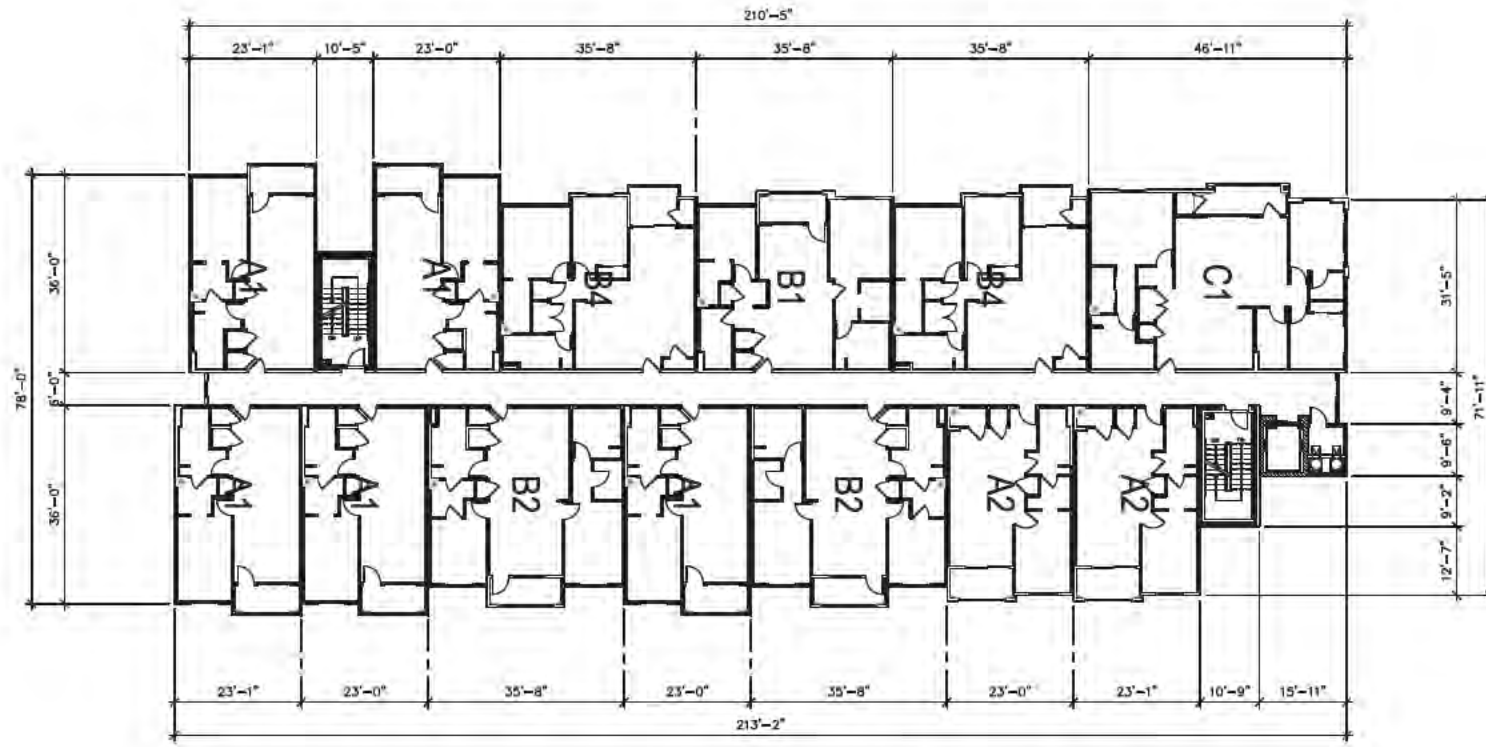


SECOND FLOOR PLAN

BUILDING 5 - FLOOR PLANS



THIRD FLOOR PLAN



FOURTH FLOOR PLAN

BUILDING 5 - FLOOR PLANS



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 Phone 602.285.0200 Fax 602.285.5225

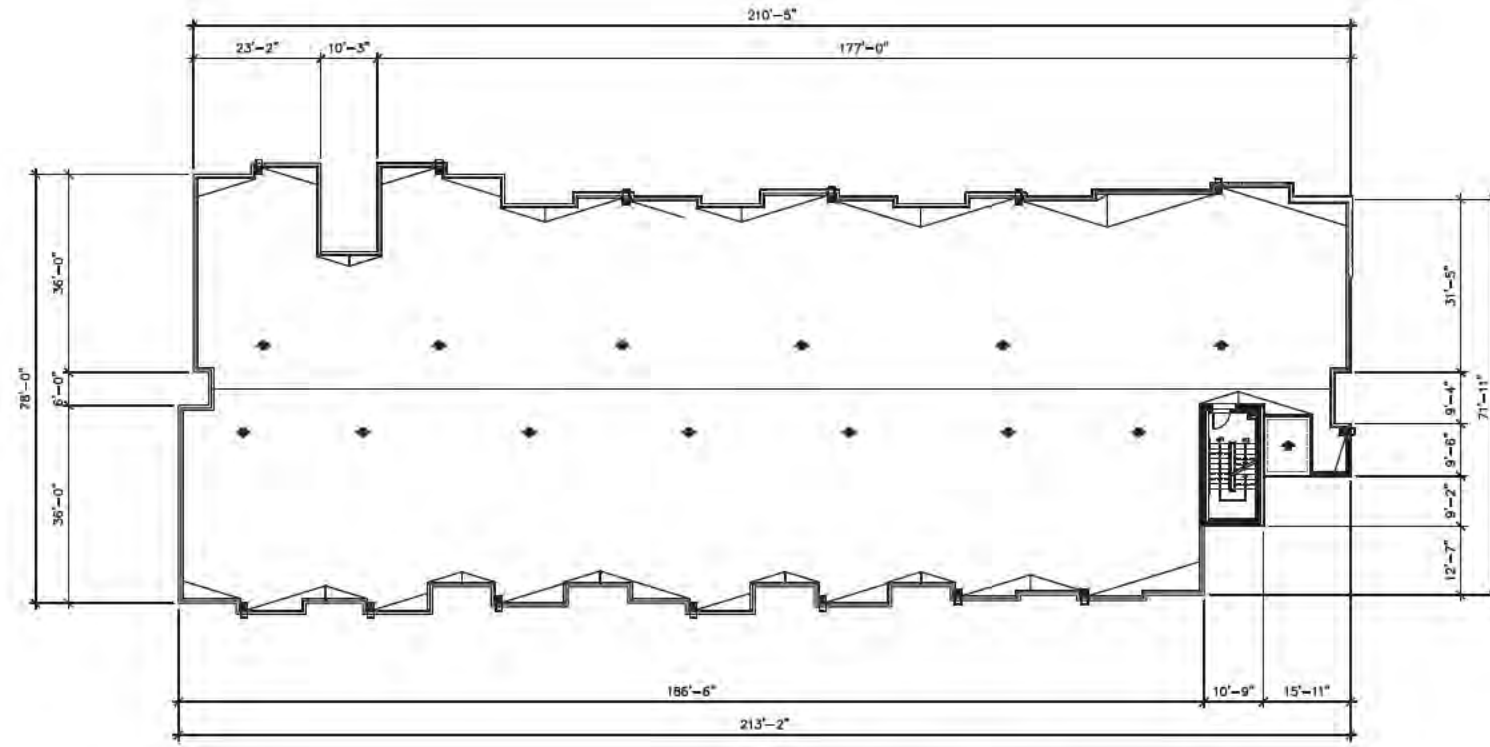
EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS
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CRW: 25423
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A7.2



ROOF PLAN



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Phone 602.285.0200 Fax 602.285.5225

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA

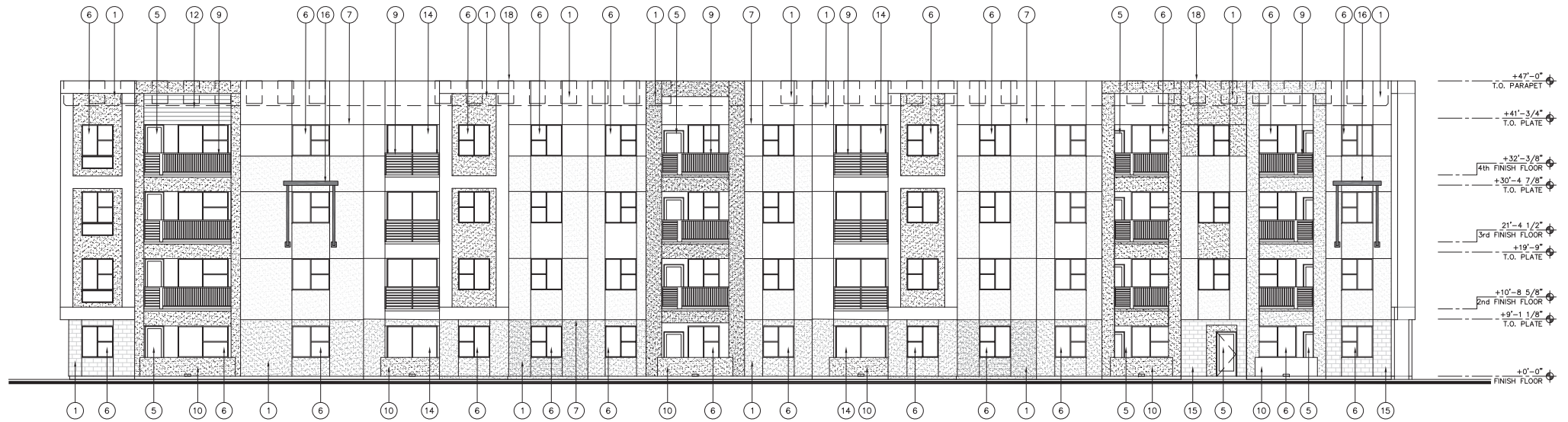
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

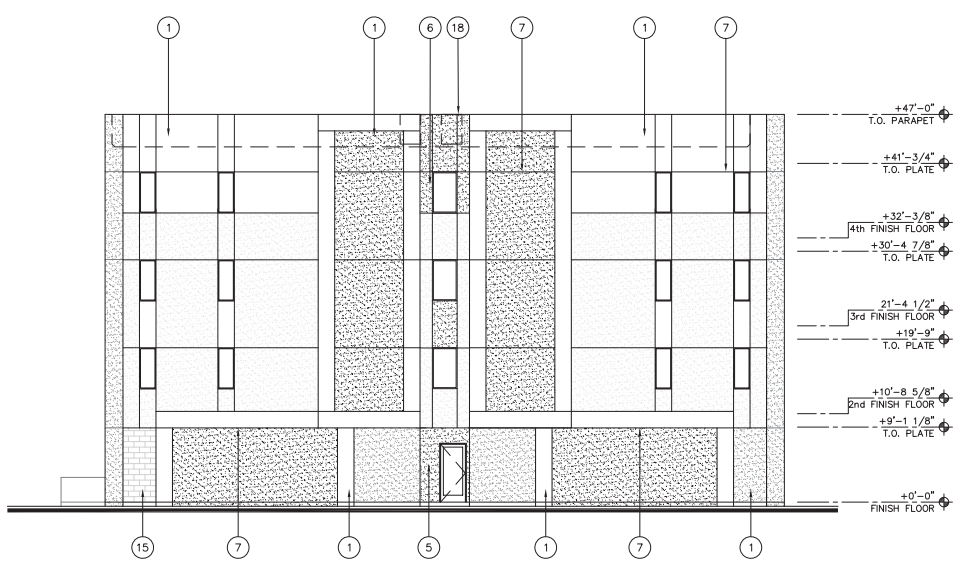
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SCALE: 1/8" = 1'-0"
SHEET NO:

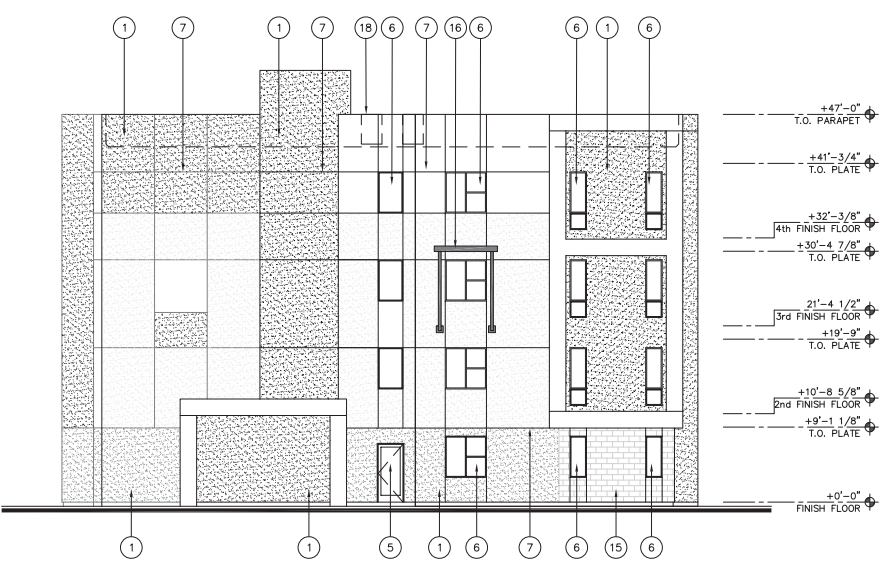
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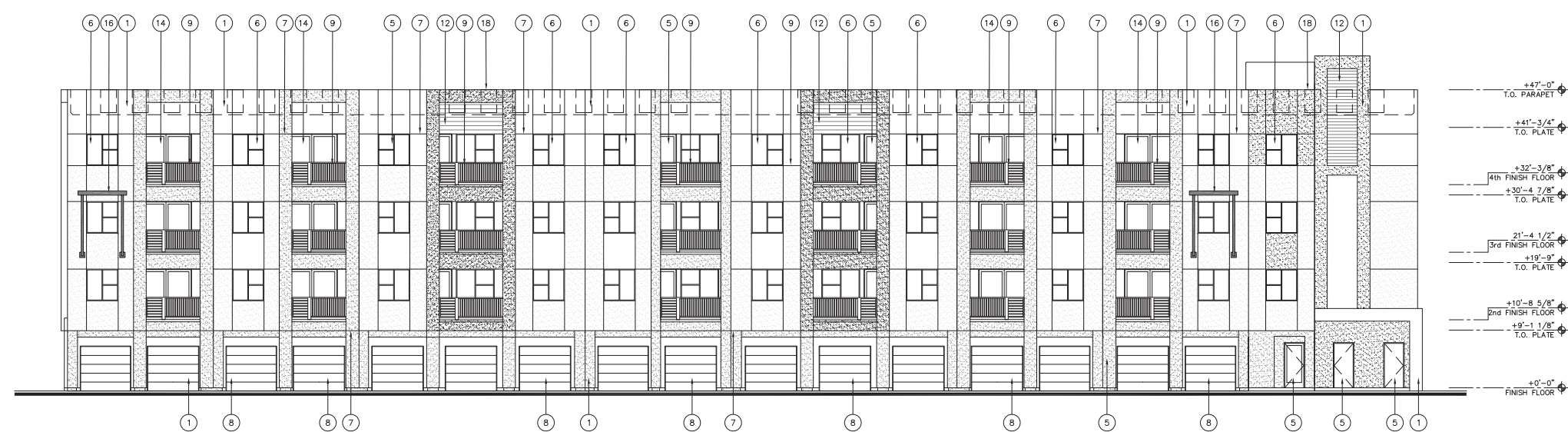
A WEST ELEVATION



C SOUTH ELEVATION



B NORTH ELEVATION



D EAST ELEVATION

- ELEVATION KEY NOTES: #
- EXTERIOR STUCCO SYSTEM
 - METAL PARAPET CAP
 - STOREFRONT SYSTEM
 - FOAM POP-OUT
 - DOOR PER PLAN
 - WINDOW PER PLAN
 - STUCCO CONTROL JOINT
 - GARAGE DOOR
 - METAL PATIO/BALCONY RAIL
 - FRAMED PATIO/BALCONY WALL
 - TOWER ELEMENT
 - HORIZONTAL LAP SIDING
 - DECORATIVE METAL FEATURE
 - SLIDING GLASS DOOR PER PLAN
 - MASONRY VENEER
 - METAL AWNINGS
 - TELESCOPING DOOR SYSTEM
 - MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
 - FIBER CEMENT SIDING

EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER
F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X10"
H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

- GENERAL ELEVATION NOTES:
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EXPIRES 9/30/25

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 Apartment Homes
 CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING RESUBMITTAL

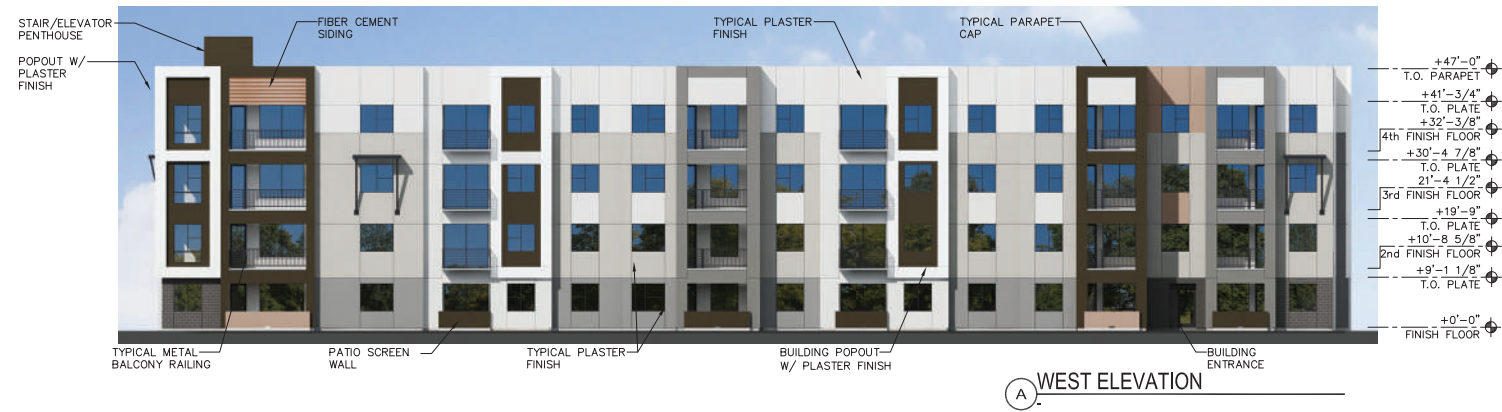
REVISIONS:

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JOB NO: 23-331
 SCALE: 3/32" = 1'-0"
 SHEET NO:

A7.4

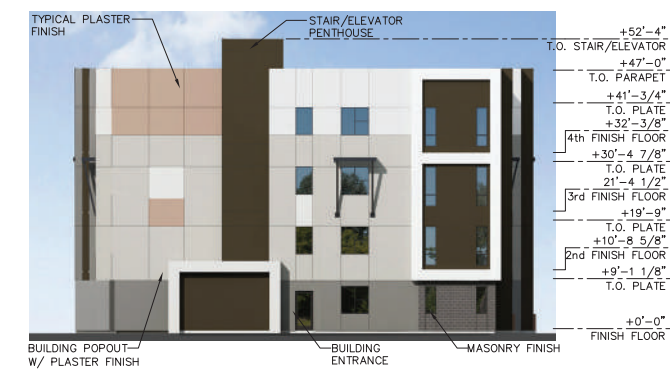
BUILDING 5 - ELEVATIONS



A WEST ELEVATION



C SOUTH ELEVATION



B NORTH ELEVATION



D EAST ELEVATION



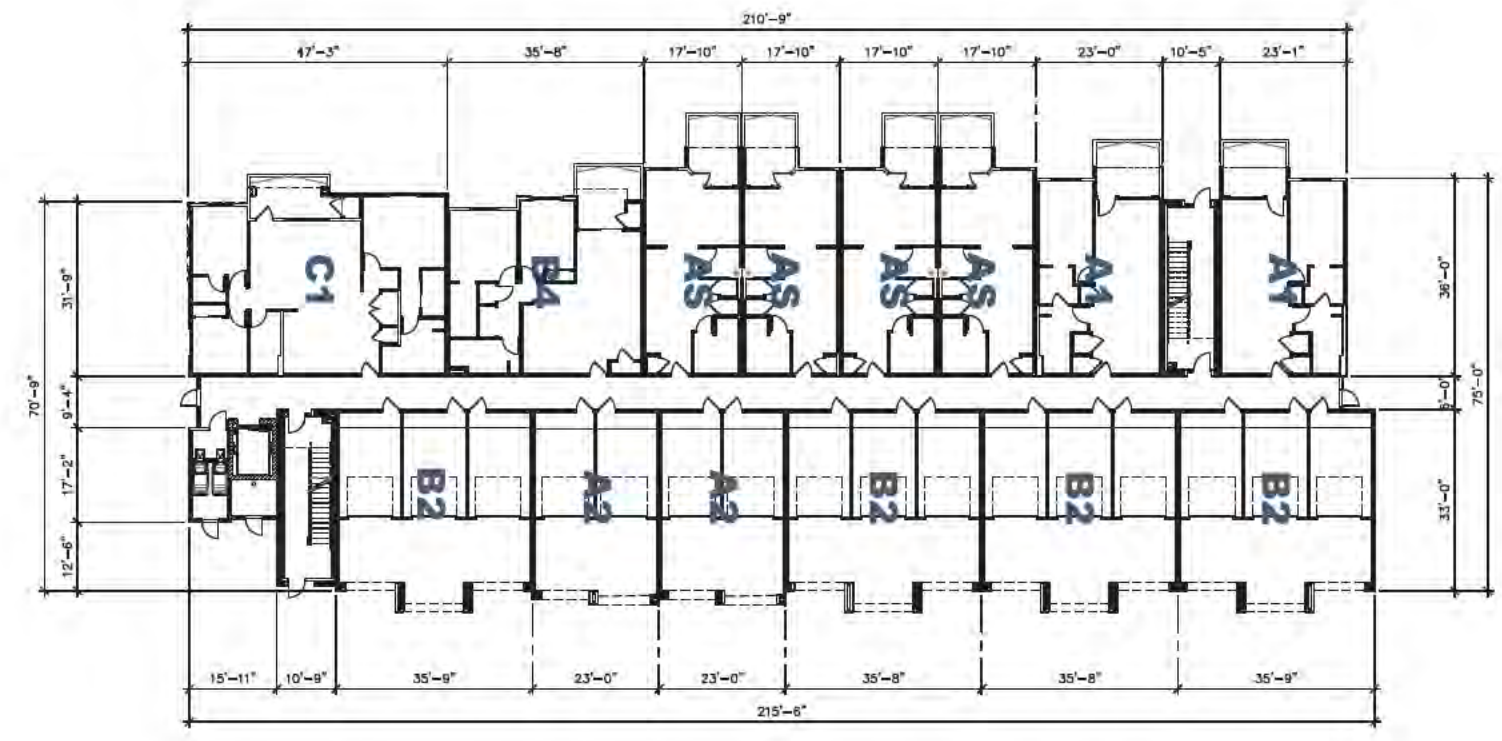
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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

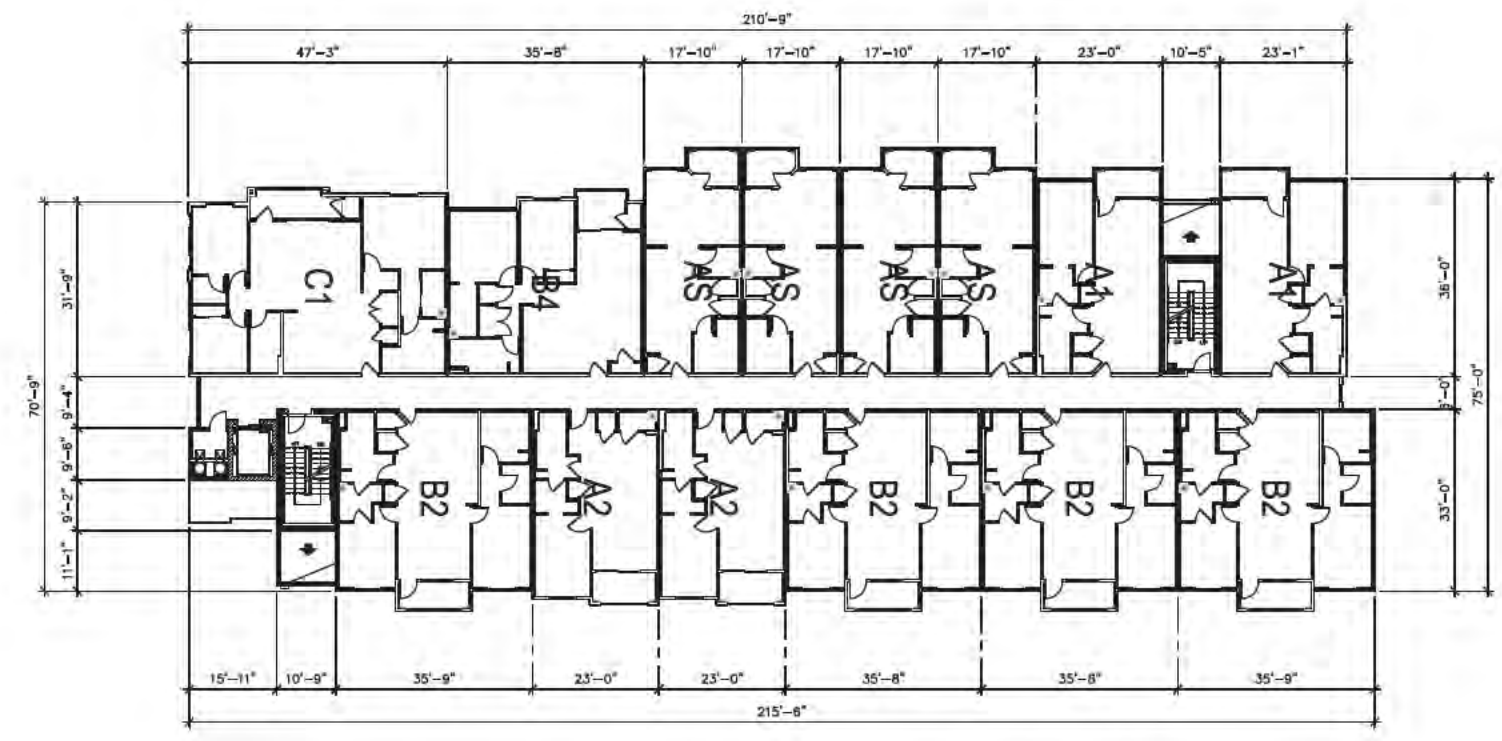
07/29/2024
BUILDING 5
COLOR ELEVATION
N.T.S
preliminary not for construction
EX6



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FIRST FLOOR PLAN



FOURTH FLOOR PLAN

EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

REVISIONS	
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JCB/NC: 2/4/20
 SCALE: 1/8" = 1'-0"
 SHEET NO:

A8.1

BUILDING 6 - FLOOR PLANS



biltform architecture group, inc

11450 north cave creek road, suite 11
duncan, arizona 85020
Phone 602.285.0200 Fax 602.285.0225

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

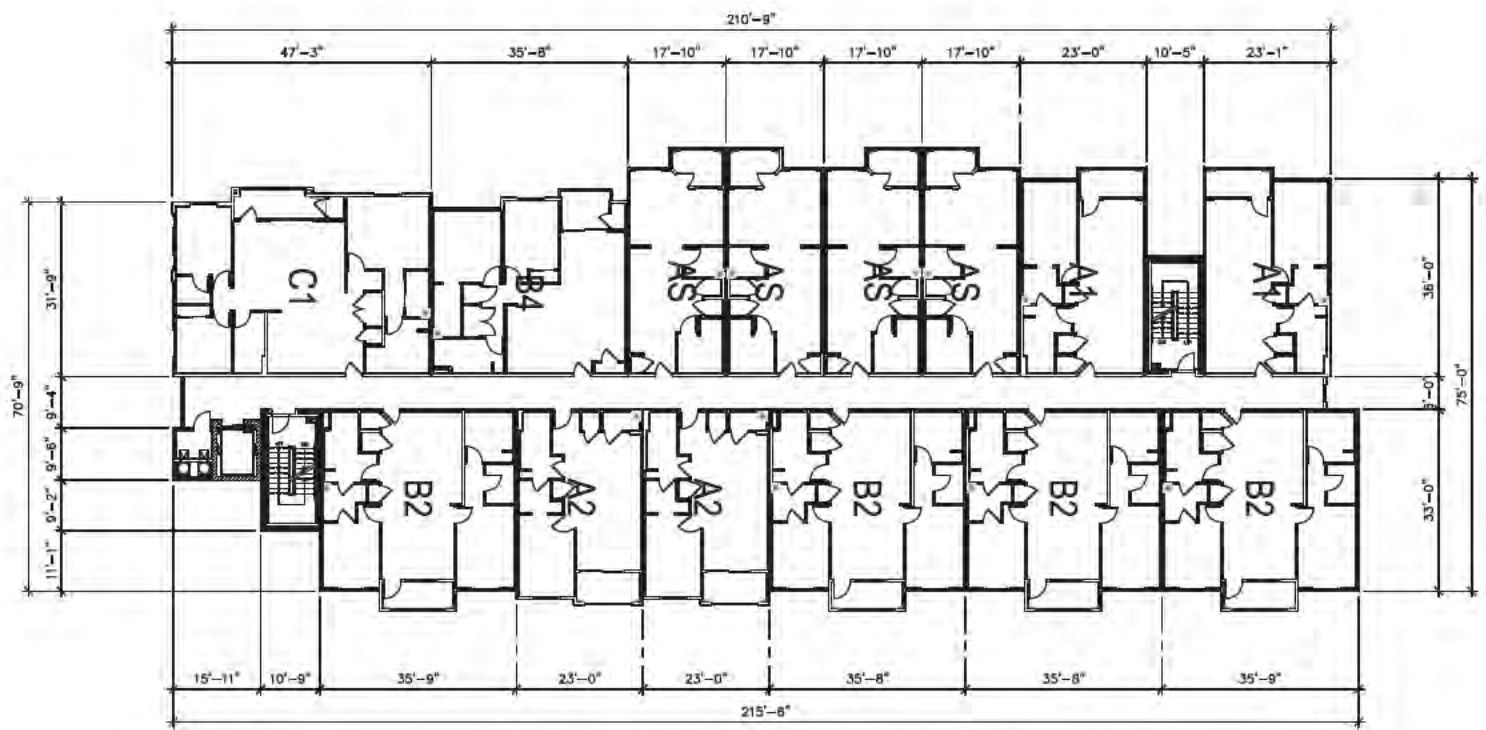
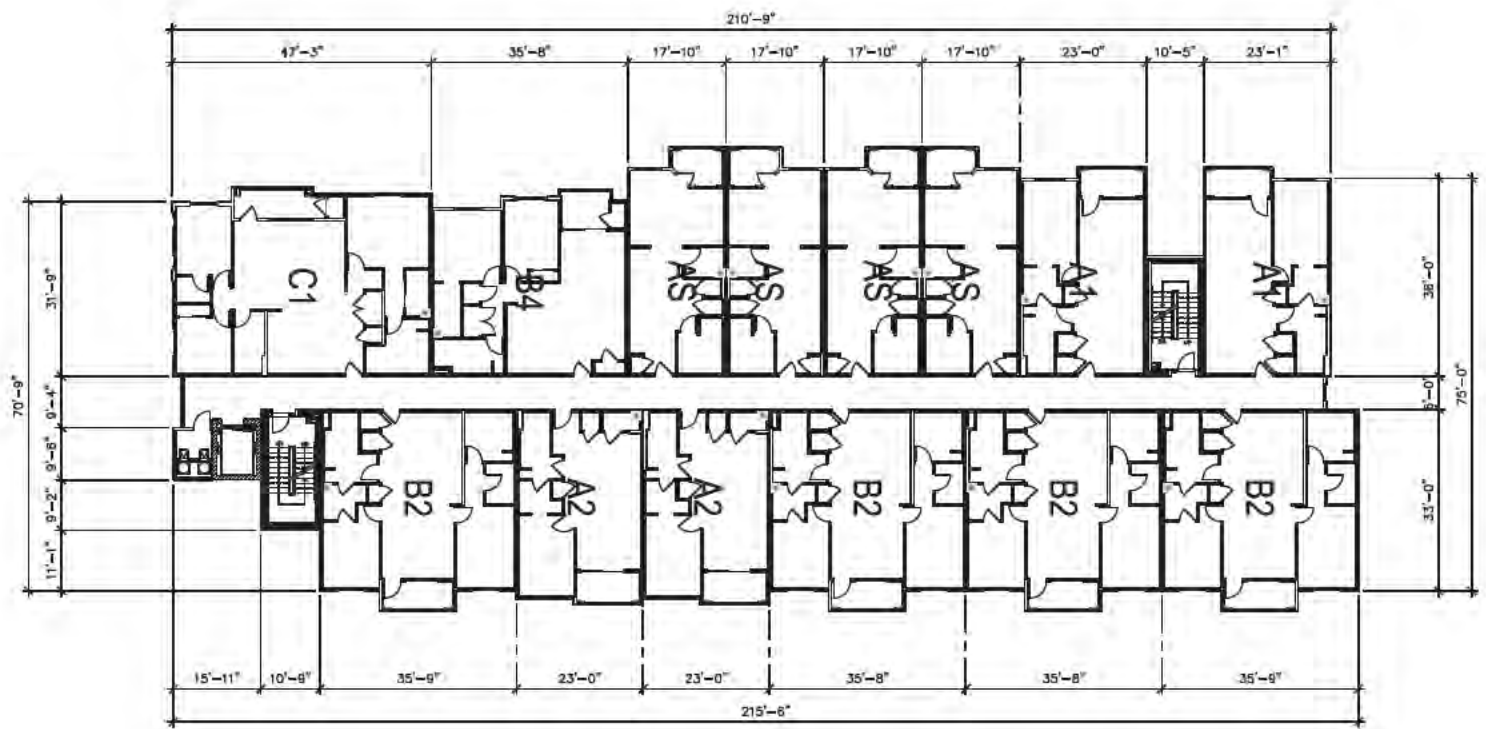
PLANNING
RESUBMITTAL

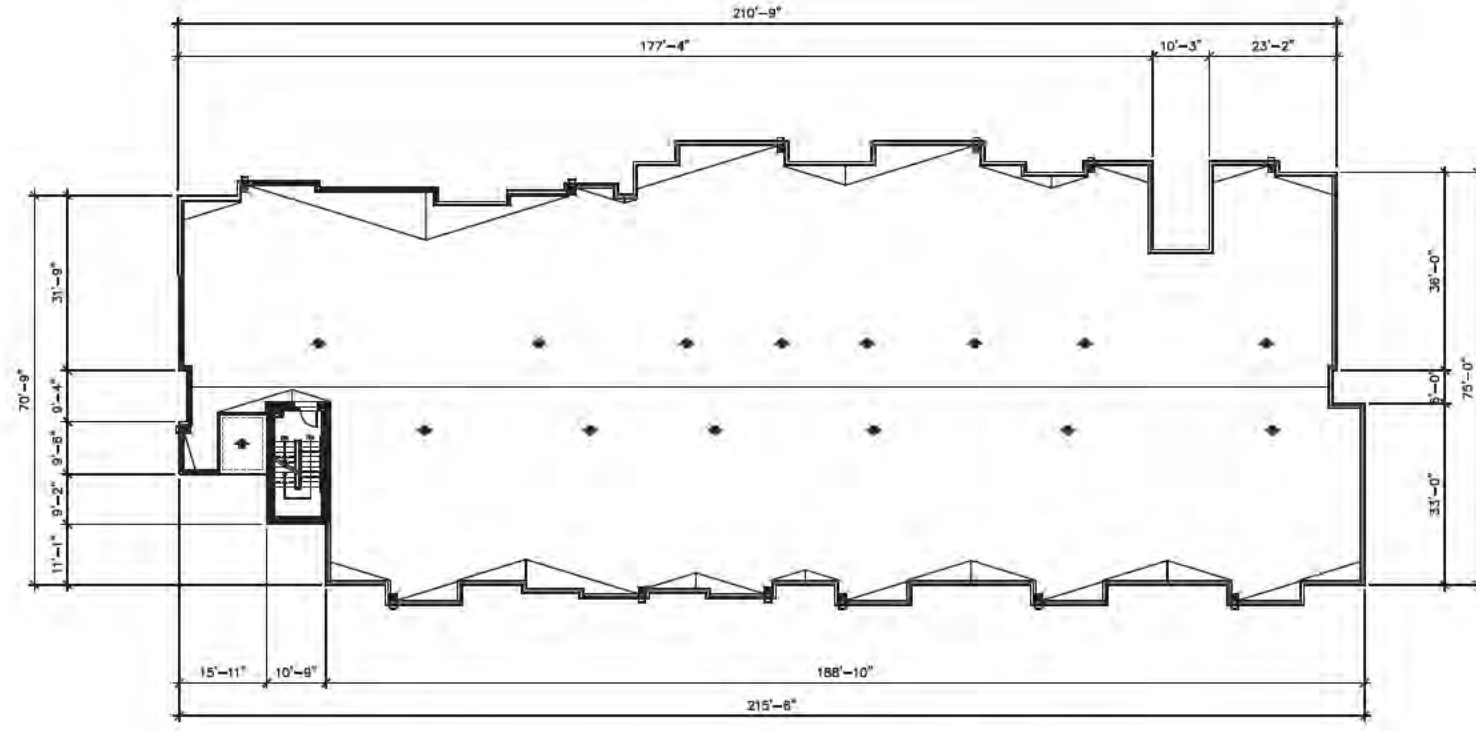
REVISIONS

JCR/NC: 2/4/20
SCALE: 1/8" = 1'-0"
SHEET NO:

A8.2

BUILDING 6 - FLOOR PLANS





ROOF PLAN



biltform architecture group, inc

11450 north cave creek road, suite 11
 phoenix, arizona 85020
 Phone 602.285.0200 Fax 602.285.5225

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA

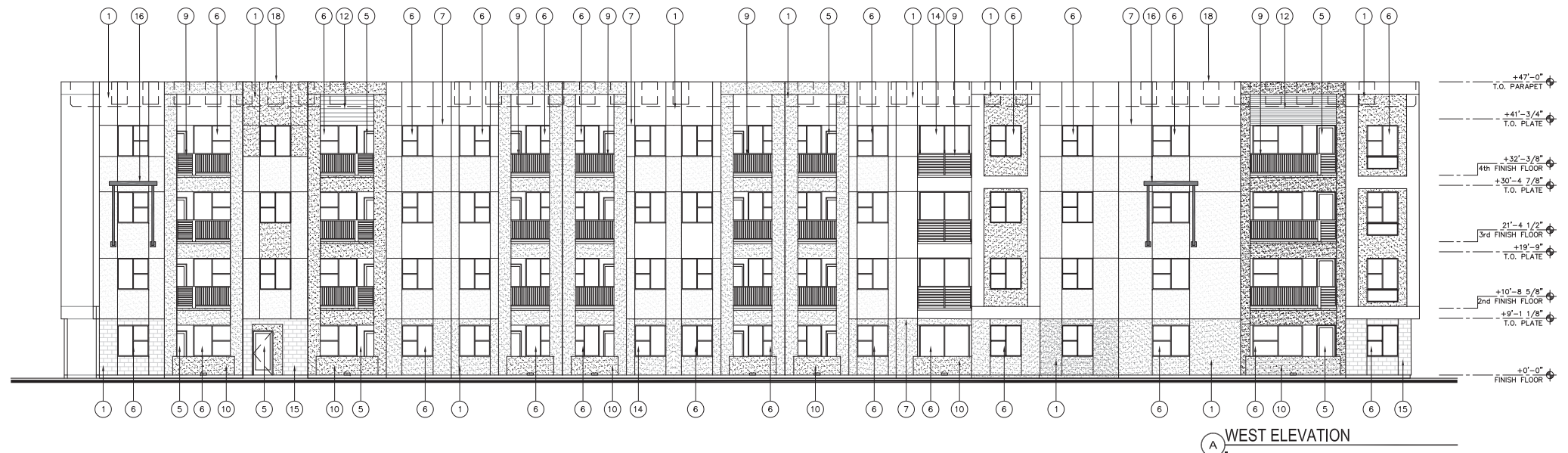
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

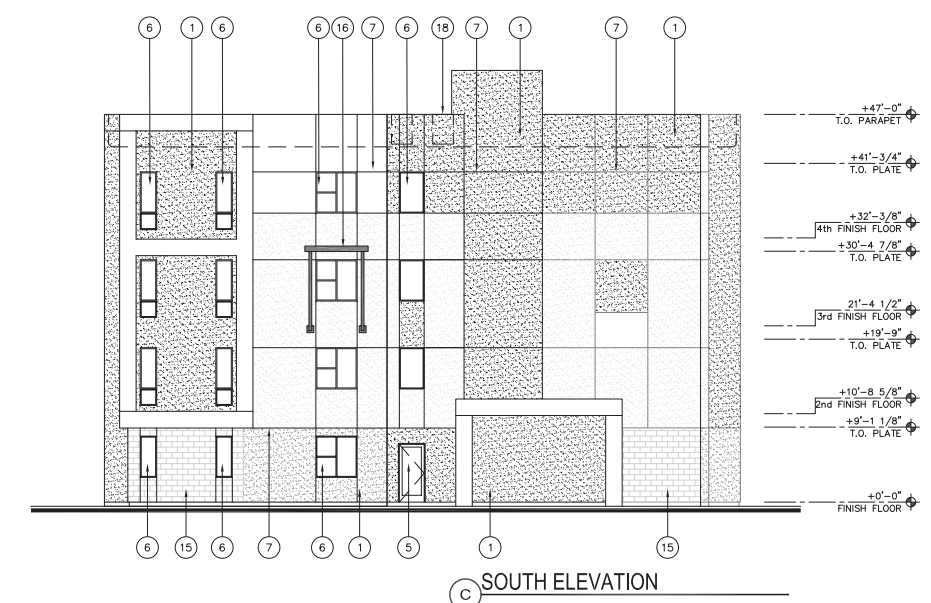
REVISIONS	
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 SHEET NO:

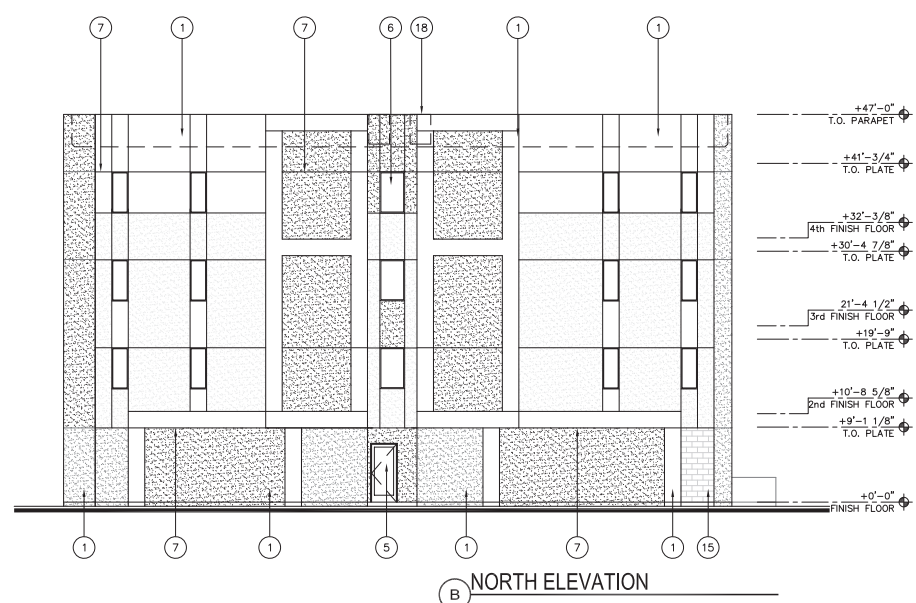
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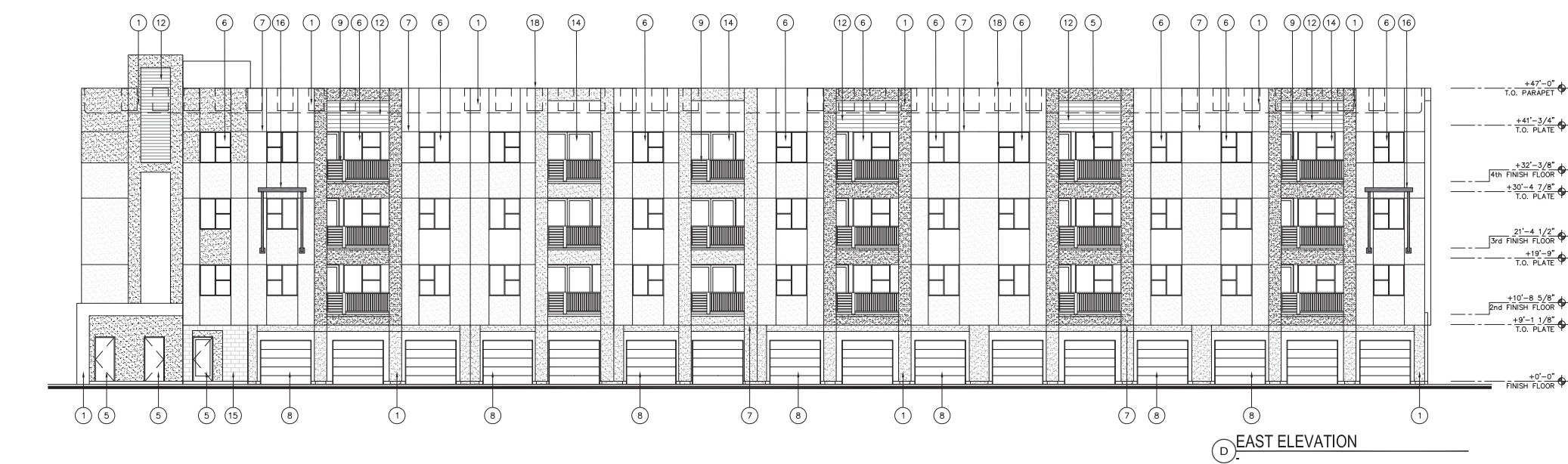
A WEST ELEVATION



C SOUTH ELEVATION



B NORTH ELEVATION



D EAST ELEVATION

- ELEVATION KEY NOTES: #
- EXTERIOR STUCCO SYSTEM
 - METAL PARAPET CAP
 - STOREFRONT SYSTEM
 - FOAM POP-OUT
 - DOOR PER PLAN
 - WINDOW PER PLAN
 - STUCCO CONTROL JOINT
 - GARAGE DOOR
 - METAL PATIO/BALCONY RAIL
 - FRAMED PATIO/BALCONY WALL
 - TOWER ELEMENT
 - HORIZONTAL LAP SIDING
 - DECORATIVE METAL FEATURE
 - SLIDING GLASS DOOR PER PLAN
 - MASONRY VENEER
 - METAL AWNINGS
 - TELESCOPING DOOR SYSTEM
 - MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
 - FIBER CEMENT SIDING

EXTERIOR COLOR SCHEME

Color Key	Body Color	Material / Finish
A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS
B	BODY COLOR 2	DUNN EDWARDS DE6376 LOOKING GLASS
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER
F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X10"
H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

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EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING RESUBMITTAL

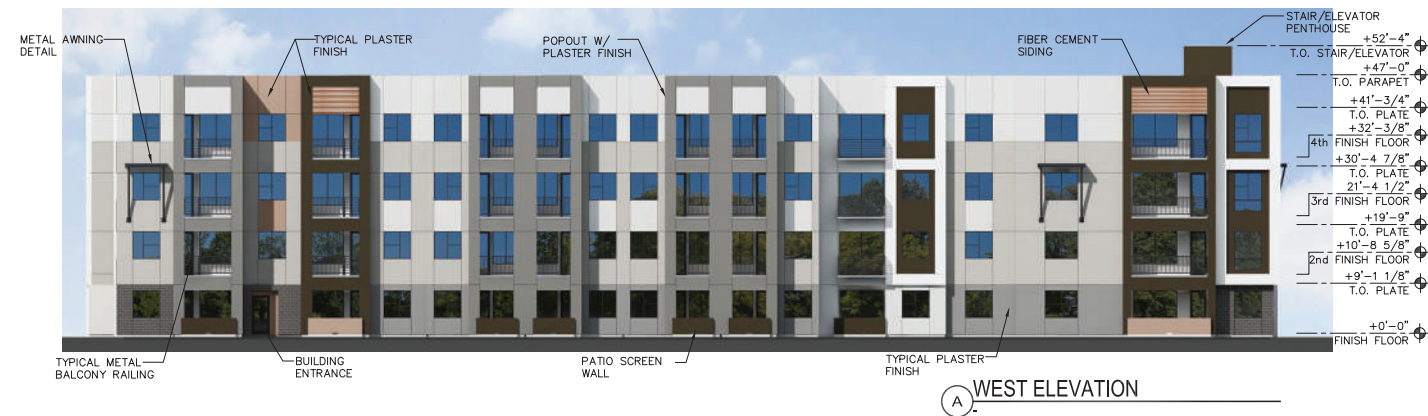
REVISIONS:

NO.	DESCRIPTION
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JOB NO: 23-331
 SCALE: 3/32" = 1'-0"
 SHEET NO:

A8.4

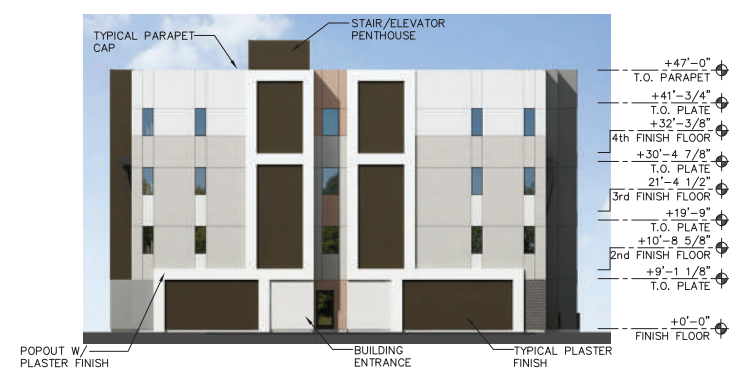
BUILDING 6 - ELEVATIONS



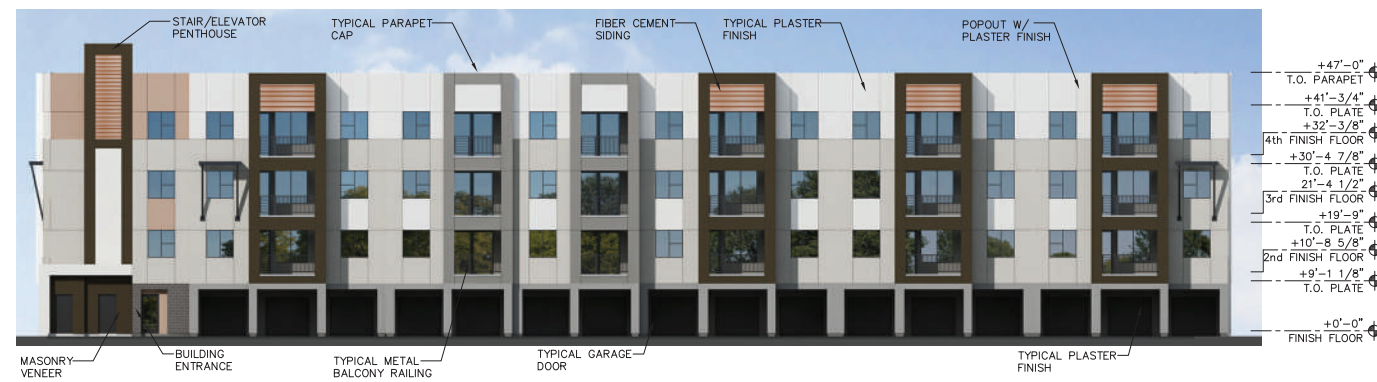
(A) WEST ELEVATION



(C) SOUTH ELEVATION



(B) NORTH ELEVATION



(D) EAST ELEVATION

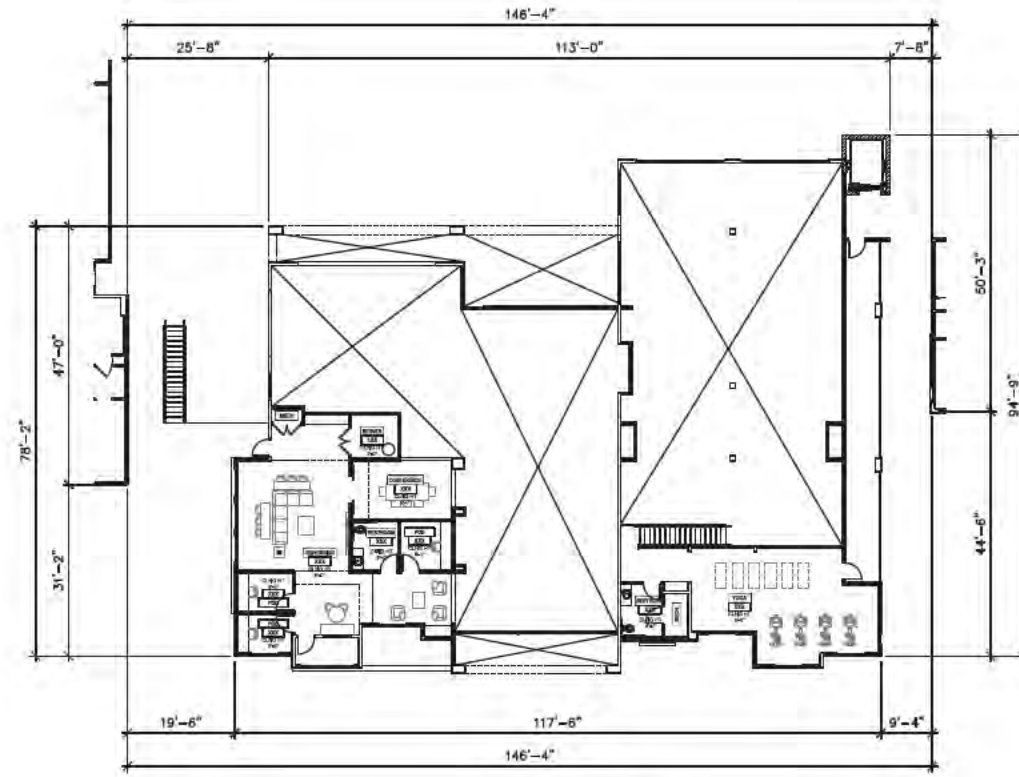


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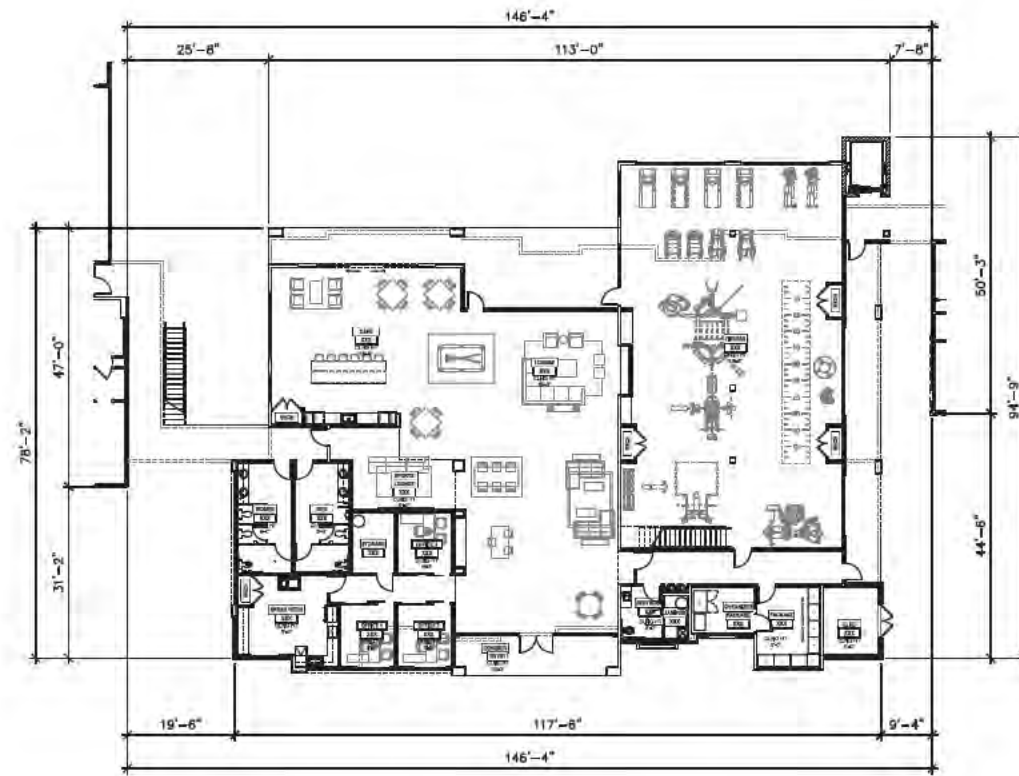
TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

BUILDING 6
COLOR ELEVATION
N.T.S.
preliminary not for construction

07/29/2024
EX7



SECOND FLOOR PLAN



FIRST FLOOR PLAN

CLUBHOUSE
BUILDING - FLOOR PLANS



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EXPIRES 9/30/25

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Apartment Homes
CHANDLER, ARIZONA

TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS	
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JCB/NC: 25-031
SCALE: 1/8" = 1'-0"
SHEET NO:

A9.1



biftform architecture group, inc.

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 phoenix, arizona 85020
 Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25

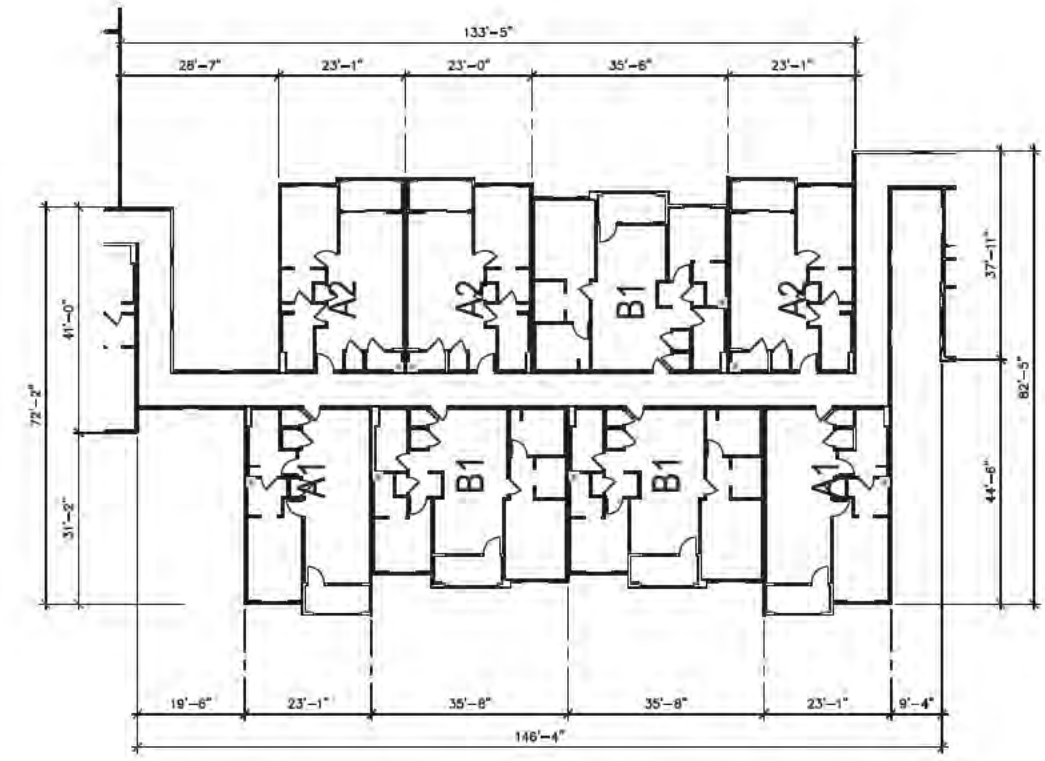
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

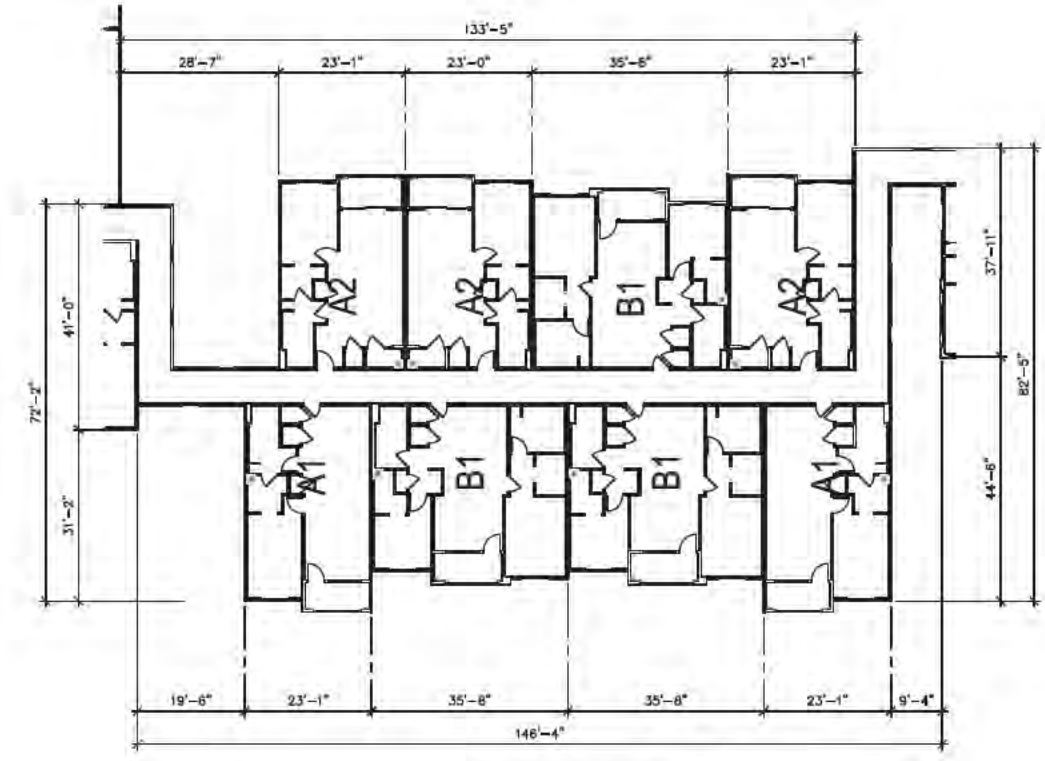
REVISIONS	
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JCB/NC: 25423
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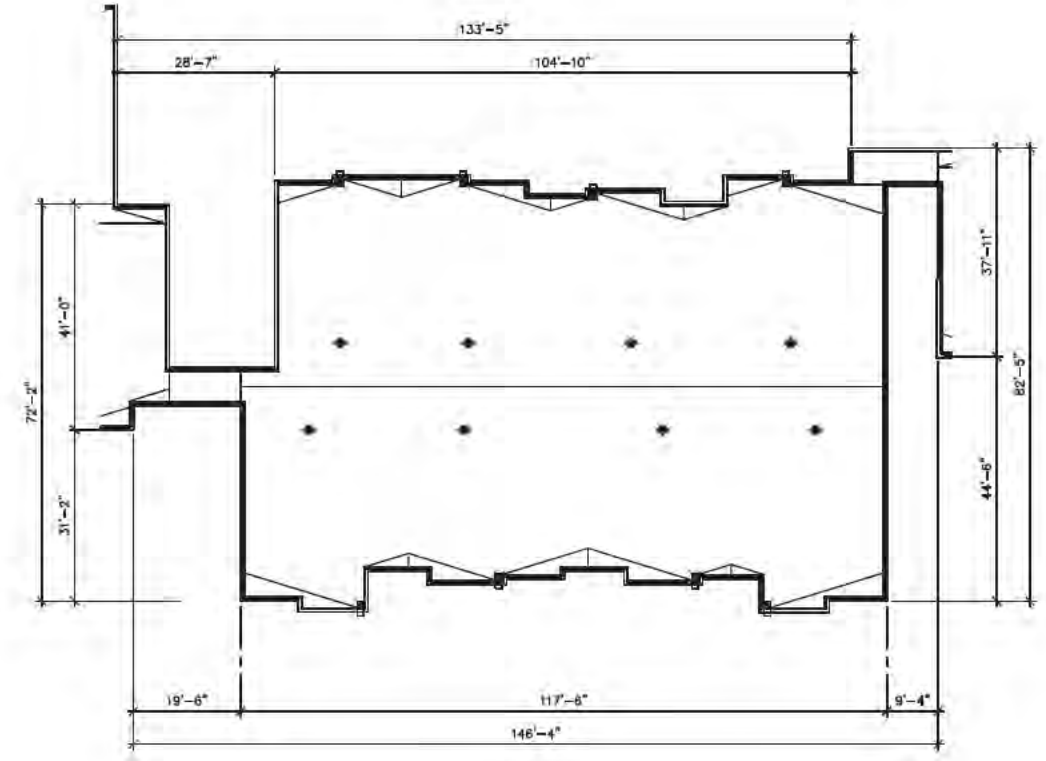
A9.2



FOURTH FLOOR PLAN



THIRD FLOOR PLAN



ROOF PLAN

CLUBHOUSE
 BUILDING - FLOOR PLANS

GENERAL ELEVATION NOTES:

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EXTERIOR COLOR SCHEME

A	BODY COLOR 1	DUNN EDWARDS DEW381 DROPLETS	F	METAL SCREENS	ALUMINUM CLEAR ANODIZED FINISH
B	BODY COLOR 2	DUNN EDWARDS DE6378 LOOKING GLASS	G	MASONRY VENEER	TRENWYTH HIGH POLISH 4107 8"X16"
C	BODY COLOR 3	BENJAMIN MOORE AC-34 CAPE HATTERAS SAND	H	RAILING, DOORS & METAL CANOPIES	SHERWIN WILLIAMS SW6258 TRICON BLACK
D	BODY COLOR 4	BENJAMIN MOORE 2139-10 RIVER ROCK	I	WINDOW / STOREFRONT FRAMES	DARK BRONZE
E	BODY COLOR 5 & LAP SIDING	DUNN EDWARDS DE6105 WEATHERED LEATHER	J	FIBER CEMENT PANELS	BENJAMIN MOORE 2112-70 AMERICAN WHITE

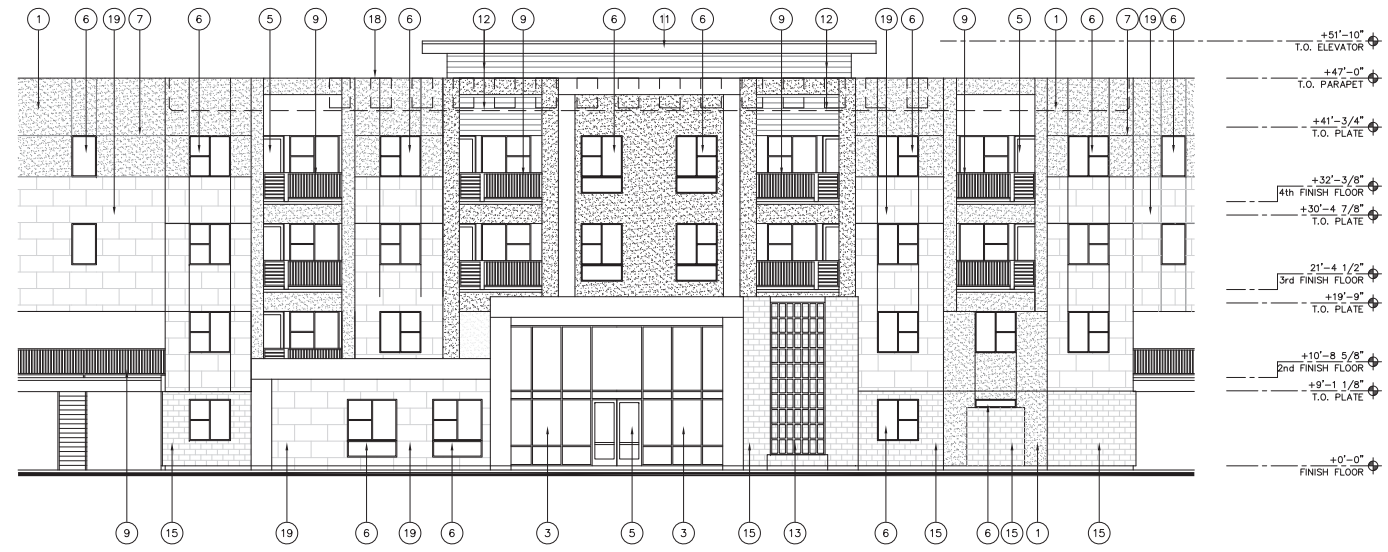
ELEVATION KEY NOTES: #

1. EXTERIOR STUCCO SYSTEM	12. HORIZONTAL LAP SIDING.
2. METAL PARAPET CAP	13. DECORATIVE METAL FEATURE.
3. STOREFRONT SYSTEM	14. SLIDING GLASS DOOR PER PLAN.
4. FOAM POP-OUT	15. MASONRY VENEER
5. DOOR PER PLAN	16. METAL AWNINGS
6. WINDOW PER PLAN	17. TELESCOPING DOOR SYSTEM
7. STUCCO CONTROL JOINT	18. MECHANICAL EQUIPMENT BEYOND, SCREENED BY PARAPET WALLS
8. GARAGE DOOR	19. FIBER CEMENT SIDING
9. METAL PATIO/BALCONY RAILING	
10. FRAMED PATIO/BALCONY WALL	
11. TOWER ELEMENT.	



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phoenix, arizona 85020
Phone 602.285.9200 Fax 602.285.9229



A WEST ELEVATION



B EAST ELEVATION - COURTYARD

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS:

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JOB NO: 23-031
SCALE: 3/32" = 1'-0"
SHEET NO:

CLUBHOUSE
BUILDING - ELEVATIONS

A9.3



A WEST ELEVATION



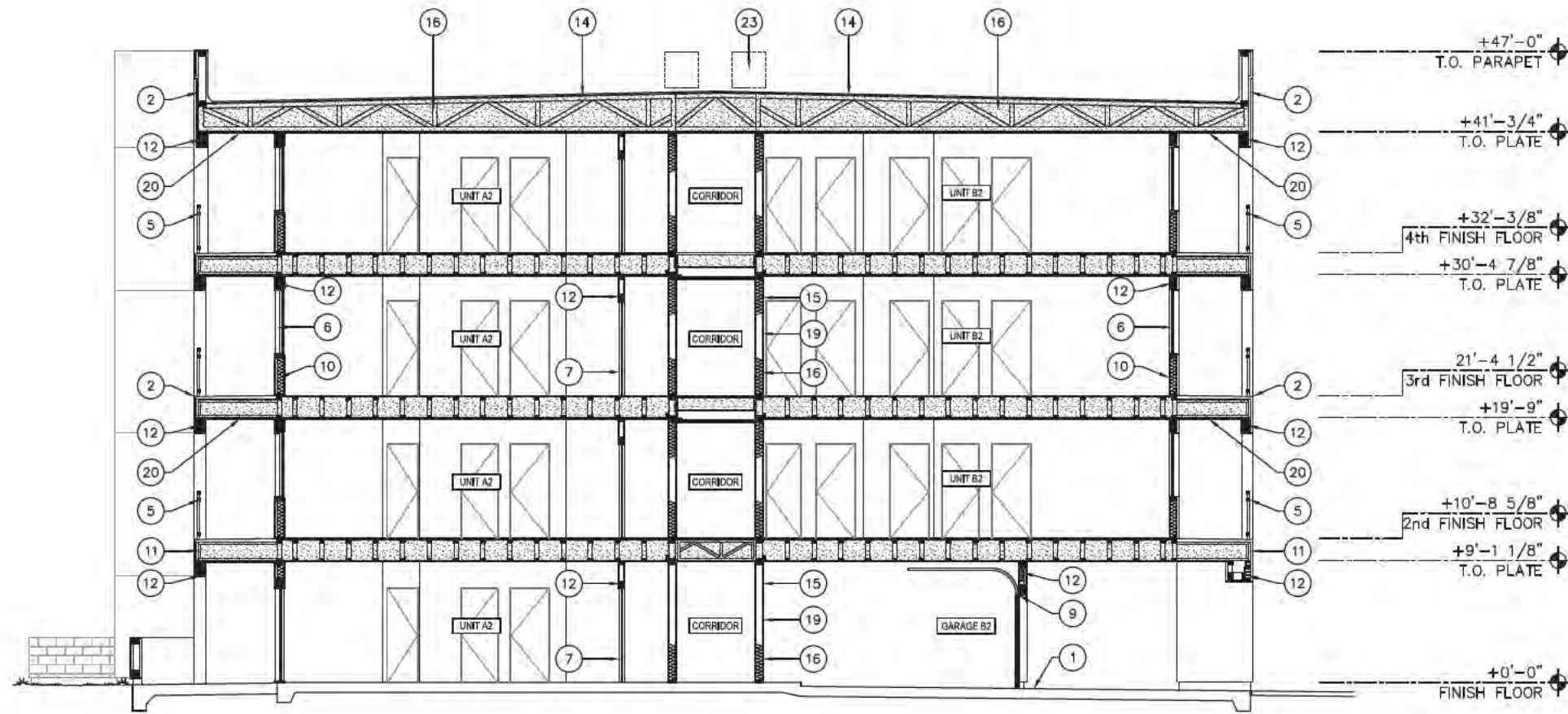
B EAST COURTYARD ELEVATION



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TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

07/29/2024
CLUBHOUSE
COLOR ELEVATION
EX8
N.T.S.
preliminary not for construction



TYPICAL BUILDING SECTION

- KEYNOTE SCHEDULE: (A)
1. CONCRETE FOUNDATION - REFER TO STRUCTURAL DRAWINGS.
 2. EXTERIOR STUCCO SYSTEM OVER WATERPROOFING MEMBRANE.
 3. STONE/BRICK VENEER.
 4. PARAPET CAP - REFER TO ROOF PLAN AND DETAILS.
 5. METAL BALCONY RAILING @ 42" A.F.F.
 6. WINDOW - REFER TO UNIT PLANS.
 7. DOOR - REFER TO UNIT PLANS.
 8. TYPICAL PARAPET BEYOND.
 9. GARAGE DOOR ASSEMBLY.
 10. WOOD STUD FRAMING.
 11. PREFAB WOOD TRUSS.
 12. BEAM/HEADER.
 13. FINISHED GRADE.
 14. 1" CLASS 'B' SPRAY FOAM ROOFING SYSTEM. INSTALL PER SPECIFICATIONS AND MANUFACTURERS INSTRUCTIONS.
 15. GYPSUM BOARD FINISH PER ASSEMBLY TYPE.
 16. INSULATION - REFER TO INSULATION SCHEDULE THIS SHEET.
 17. 2x SILL PLATE.
 18. DOUBLE 2x TOP PLATE.
 19. INTERIOR WALL - 2x FRAMING WITH GYPSUM BOARD FINISH.
 20. EXTERIOR GYPSUM SOFFIT BOARD FINISH.
 21. SITE PAVING - REFER TO SITE PLAN AND CIVIL.
 22. BALCONY - 1 1/2" MIN. HARDROCK CONCRETE OVER PROTECTION BOARD AND WATERPROOFING MEMBRANE. REFER TO SPECIFICATIONS. SLOPE MIN. 1/4" PER FOOT TO DRAIN.
 23. MECHANICAL EQUIPMENT - BELOW HEIGHT OF PARAPET.
 24. 42" FRAMED EXTENDED PATIO WALL.

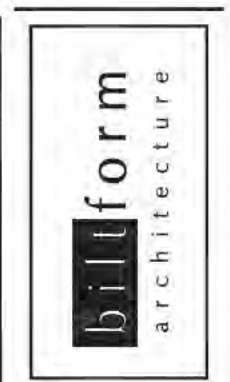
INSULATION SCHEDULE:

EXTERIOR WALLS	R-19 at 2x6 WALLS R-13 at 2x4 WALLS
ROOF	R-38 BATTS TO FILL CAVITY
FLOOR/CEILING	BATT OR BLOWN INSULATION TO FILL CAVITY
GARAGE CEILING	BATT OR BLOWN INSULATION TO FILL CAVITY
PARTY WALLS	3" SOUND BATTS EACH SIDE
CORRIDOR WALLS	3.5" SOUND BATTS

GENERAL INSULATION NOTE:
FILL FLOOR/CEILING AND ROOF/CEILING CAVITY WITH NON-COMBUSTIBLE BATT INSULATION

BUILDING COMPONENT SCHEDULE:

BUILDING ELEMENT	CONSTRUCTION TYPE V-A	DETAIL REFERENCE
EXTERIOR BEARING WALL	1 HOUR RATED	3/FR1
INTERIOR BEARING WALL	1 HOUR RATED	10/FR1
INTERIOR NON-BEARING WALL	NON-RATED	17/FR2
UNIT SEPARATION WALL	1 HOUR RATED	2/FR1
FLOOR/CEILING ASSEMBLY (UNIT SEPARATION)	1 HOUR RATED	4/FR1
FLOOR/CEILING ASSEMBLY (CORRIDORS)	1 HOUR RATED	5/FR1
ROOF/CEILING ASSEMBLY	1 HOUR RATED	7/FR1
STAIR ENCLOSURE CORRIDOR WALL	2 HOUR RATED	19/FR2
STAIR ENCLOSURE UNIT SEPARATION WALL	2 HOUR RATED	19/FR2
STAIR ENCLOSURE EXTERIOR WALL	2 HOUR RATED	22/FR2
ELEVATOR SHAFT	2 HOUR RATED	19/FR2



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EXPIRES 9/30/25

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PLANNING RESUBMITTAL

REVISIONS:

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6		

JOB NO: 254231
SCALE: 1/8" = 1'-0"
SHEET NO:

A10.1

BUILDING SECTIONS



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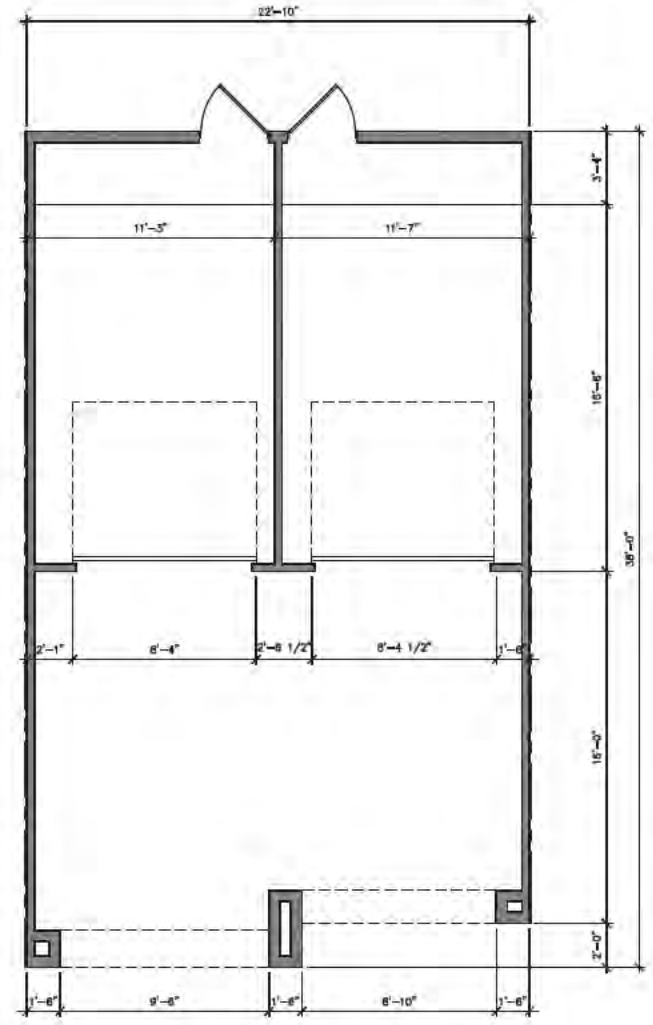
EXPIRES 9/30/25

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PLANNING
 RESUBMITTAL

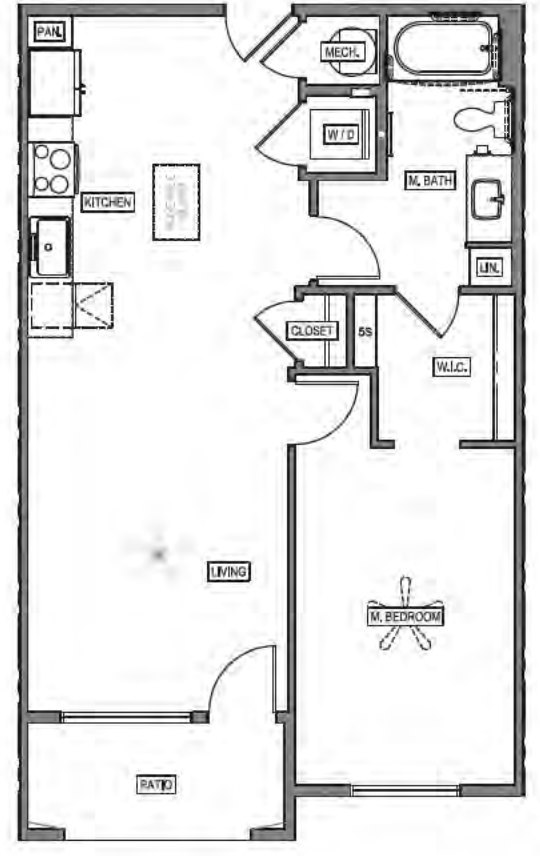
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:



UNIT A1 - GARAGE
 TYPICAL

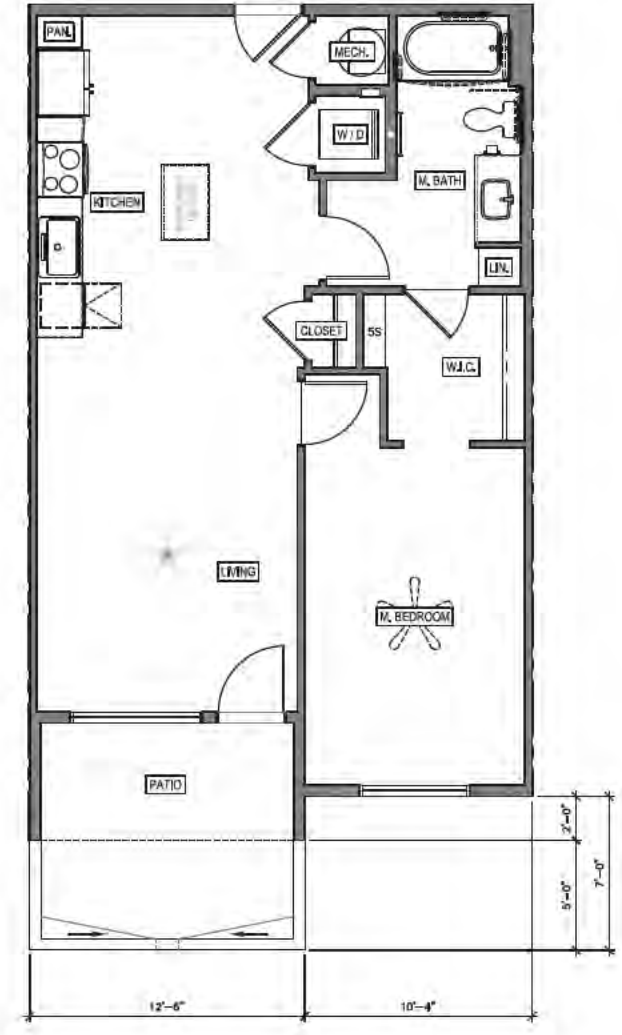
GROSS AREA:	
GARAGE:	459 S.F.
TANDEM PARKING:	393 S.F.
TOTAL:	852 S.F.



UNIT A1.2
 1ST FLOOR - BUILDING 4 DOG YARD

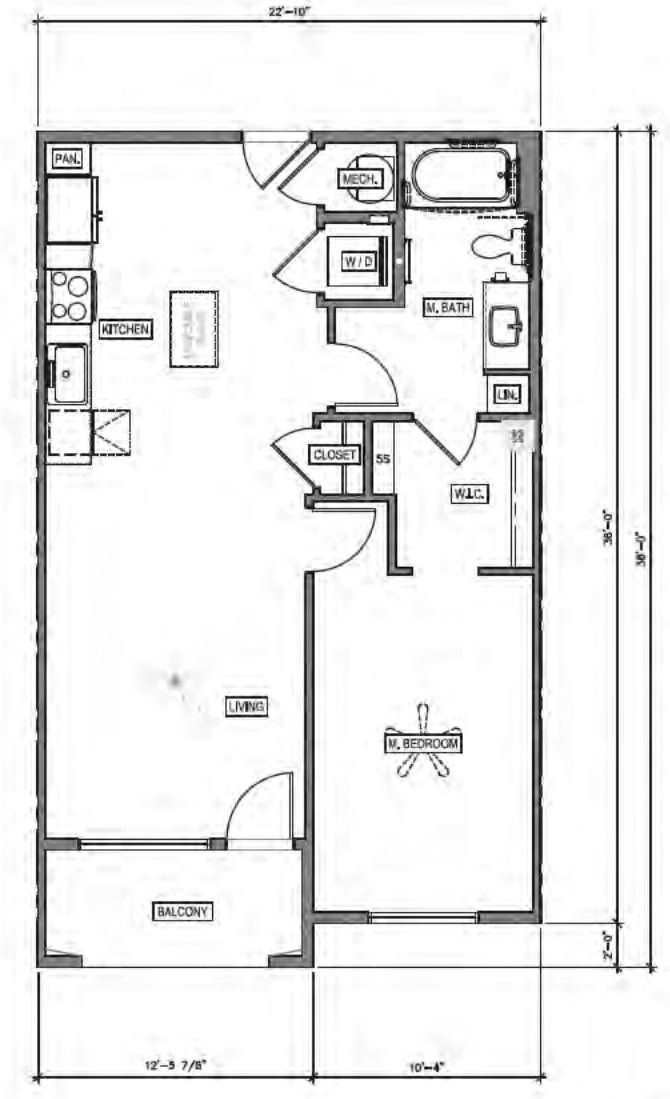
GROSS LIVABLE AREA:	
LIVABLE:	787 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	65 S.F.
TOTAL:	852 S.F.

DOG YARD REFER TO SITE PLAN



UNIT A1 1
 1ST FLOOR

GROSS LIVABLE AREA:	
LIVABLE:	787 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	129 S.F.
TOTAL:	915 S.F.



UNIT A1
 UPPER FLOORS

GROSS LIVABLE AREA:	
LIVABLE:	787 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	65 S.F.
TOTAL:	852 S.F.

FLOOR PLAN
 UNIT A1 & A1 GARAGE

A2.1.0

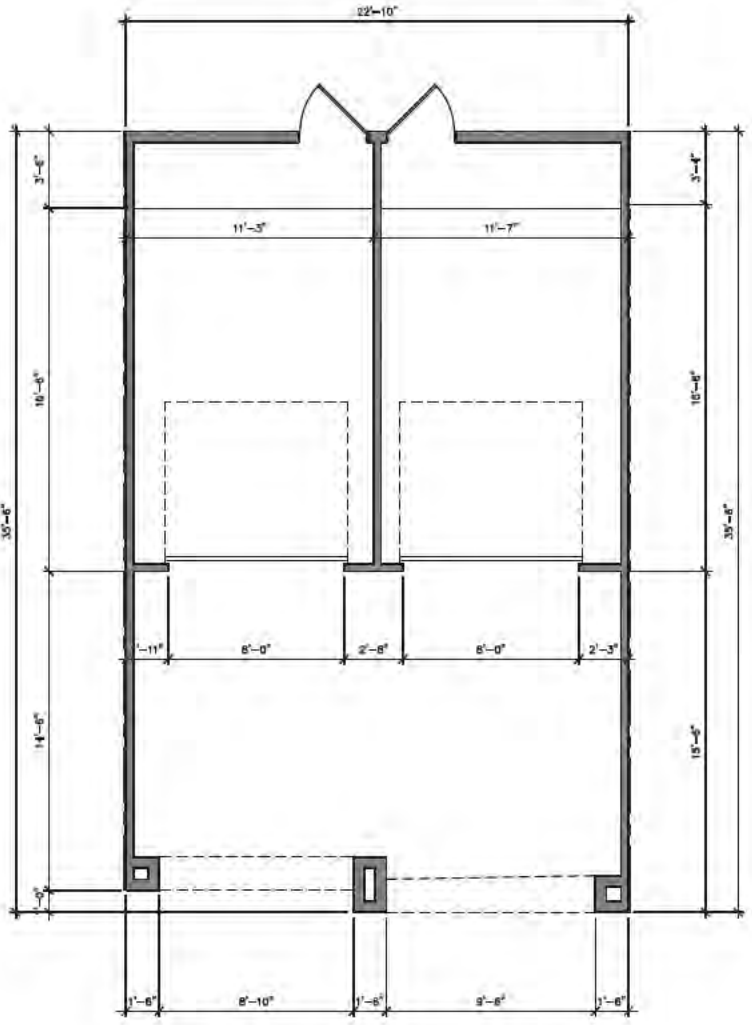
EXPIRES 9/30/25

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PLANNING
RESUBMITTAL

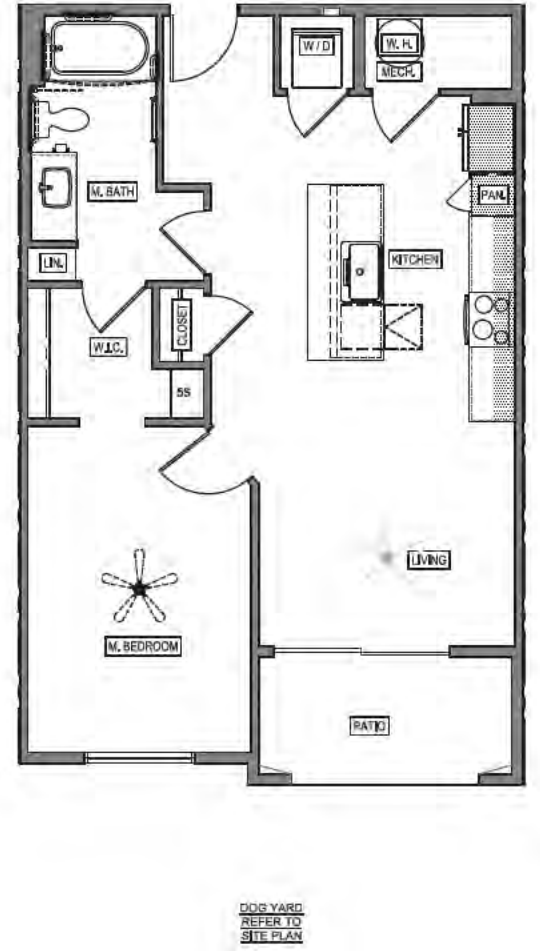
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JOB NO:	24-031
SCALE:	1/4" = 1'-0"
SHEET NO:	



UNIT A2 - GARAGE
TYPICAL

GROSS AREA:	
GARAGE:	480 S.F.
TANDEM PARKING:	345 S.F.
TOTAL:	825 S.F.



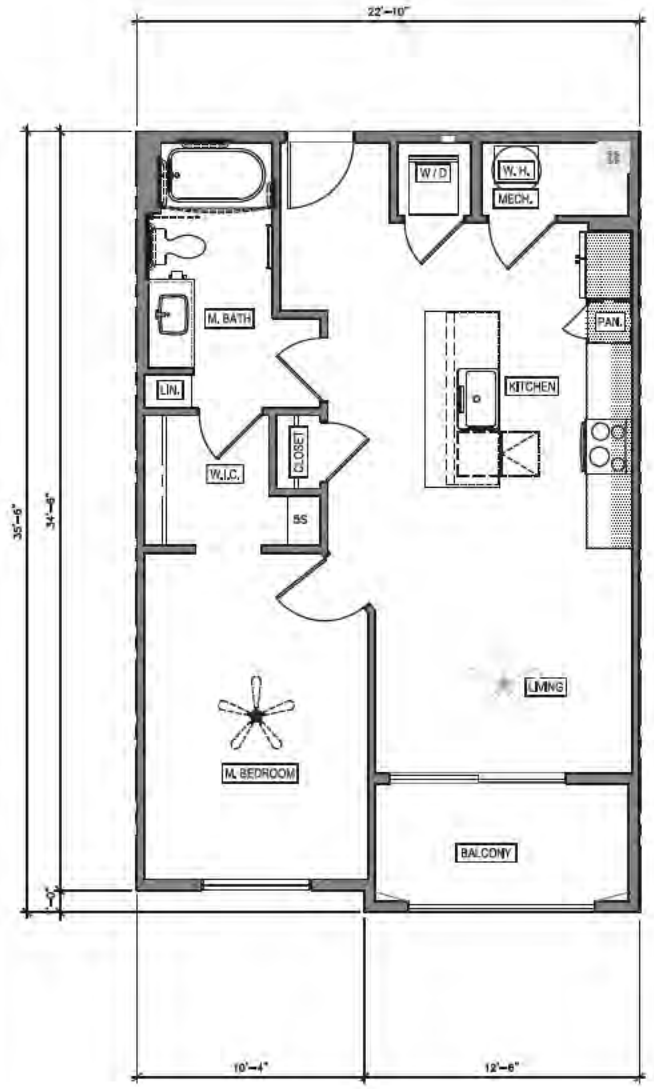
UNIT A2.3
1ST FLOOR - BUILDING 4 DOG YARD

GROSS LIVABLE AREA:	
LIVABLE:	735 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	70 S.F.
TOTAL:	805 S.F.



UNIT A2.1 & A2.2
1ST FLOOR

GROSS LIVABLE AREA:	
LIVABLE:	735 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	129 S.F.
TOTAL:	863 S.F.

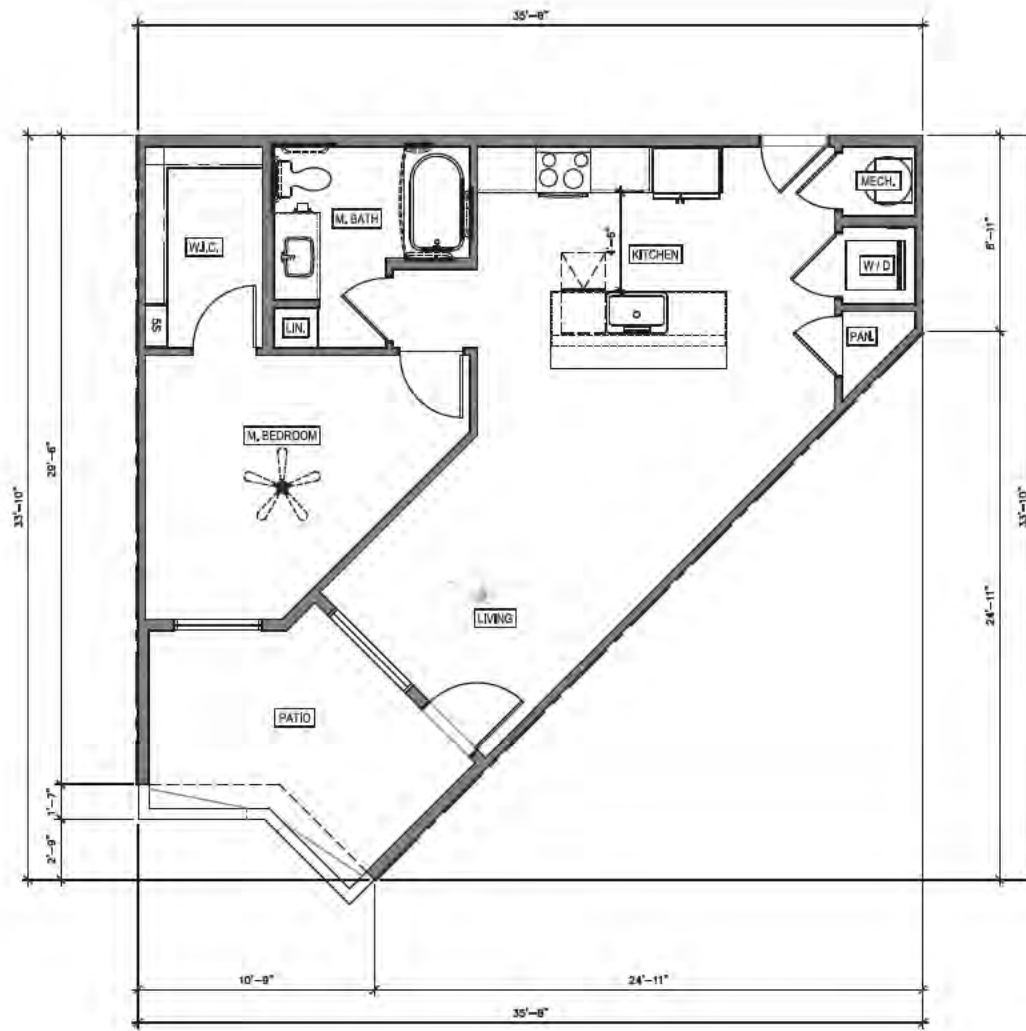


UNIT A2
UPPER FLOORS

GROSS LIVABLE AREA:	
LIVABLE:	735 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	70 S.F.
TOTAL:	805 S.F.

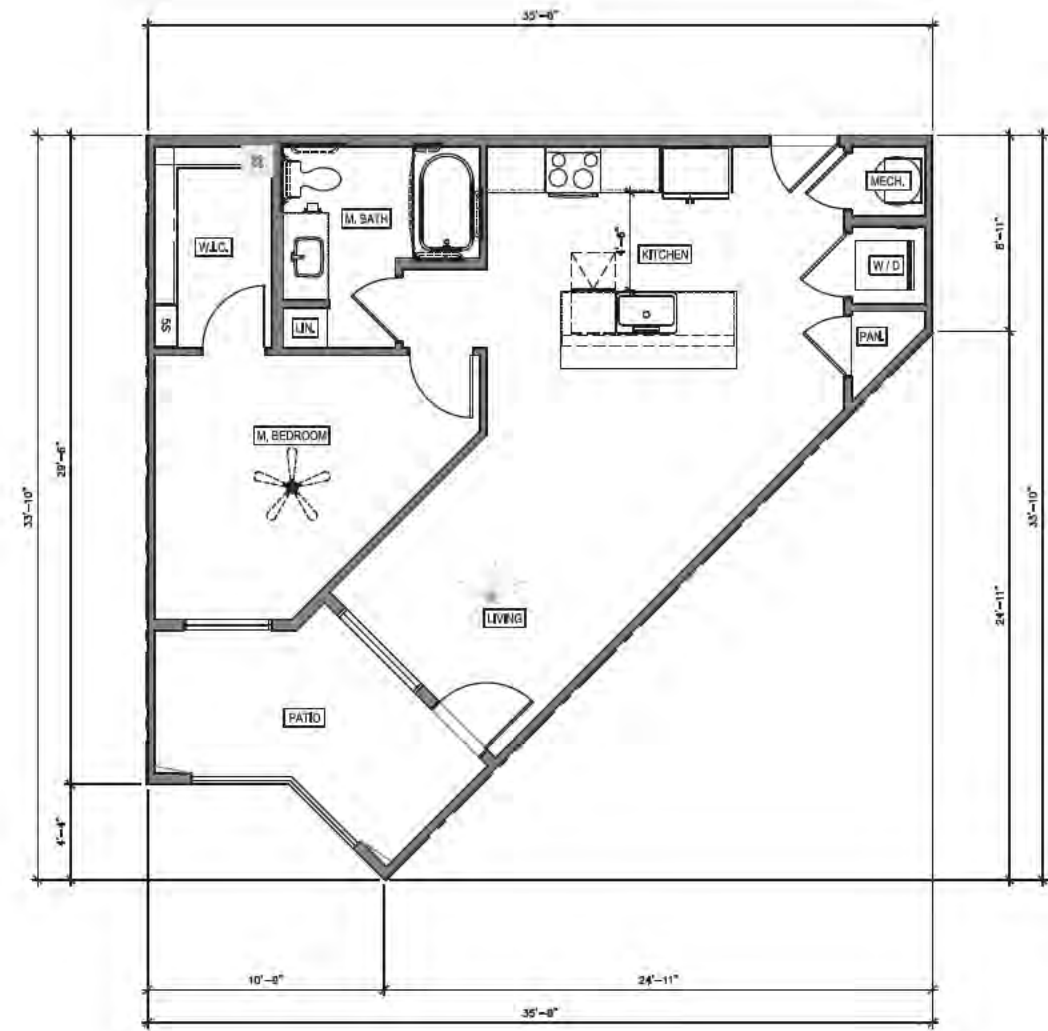
FLOOR PLAN
UNIT A2 & A2 GARAGE

A2.2.0



UNIT A6.1
1ST FLOOR

GROSS LIVABLE AREA:	
LIVABLE:	752 S.F.
STORAGE:	0 S.F.
PATIO/PORCH/CONY:	178 S.F.
TOTAL:	930 S.F.



UNIT A6
UPPER FLOORS

GROSS LIVABLE AREA:	
LIVABLE:	752 S.F.
STORAGE:	0 S.F.
PATIO/PORCH/CONY:	110 S.F.
TOTAL:	862 S.F.

FLOOR PLAN
UNIT A6



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EXPIRES 9/30/25
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PLANNING
RESUBMITTAL

REVISIONS:	
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JOB NO: 24-031
SCALE: 1/4" = 1'-0"
SHEET NO:

A2.3.0



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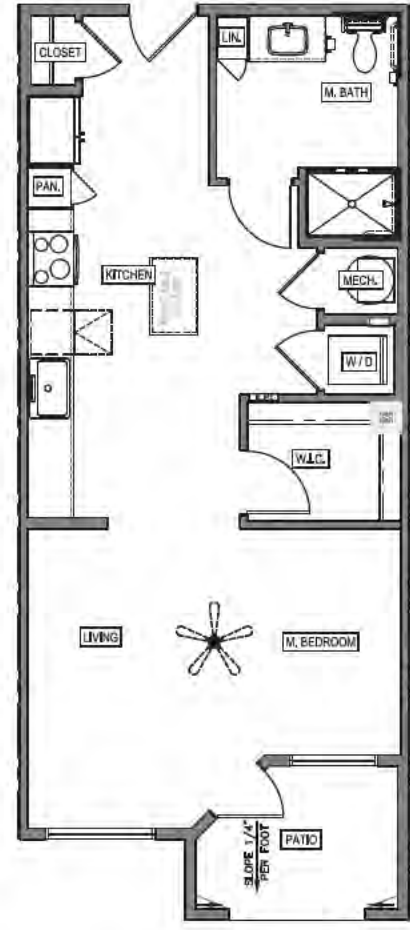
EXPIRES 9/30/25

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PLANNING
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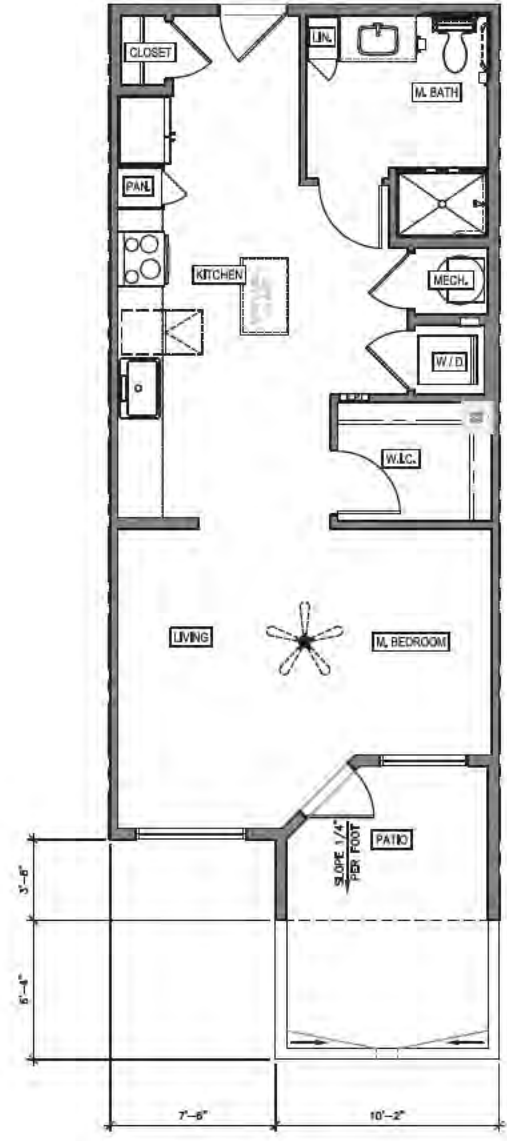
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 SCALE: 1/4" = 1'-0"
 SHEET NO:



DOG YARD
 REFER TO
 SITE PLAN

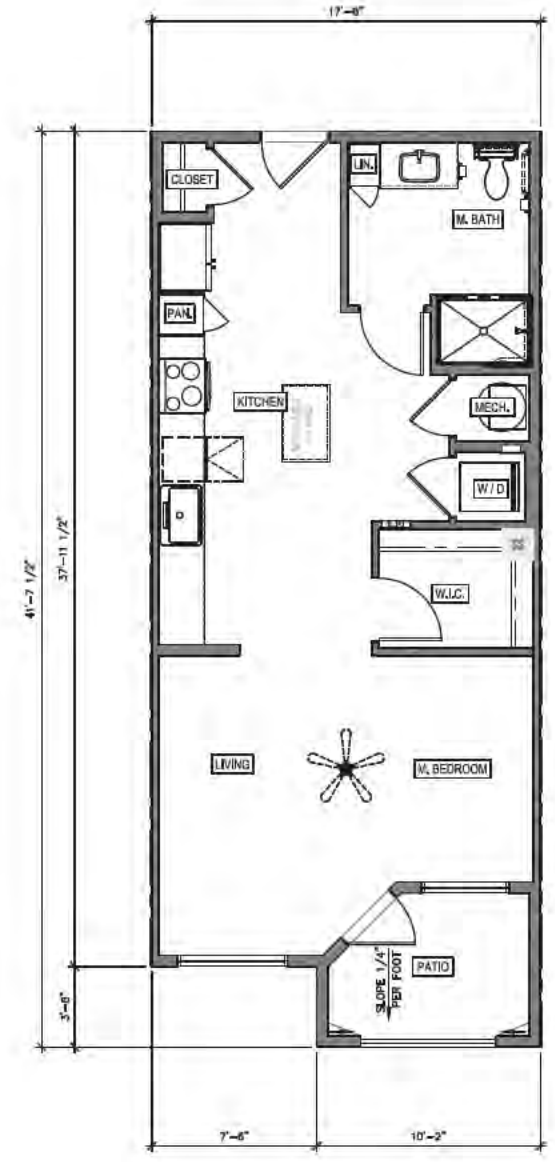
UNIT AS.3
 1ST FLOOR - DOG YARD

GROSS LIVABLE AREA:	
LIVABLE:	650 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	64 S.F.
TOTAL:	714 S.F.



UNIT AS.1 & AS.2
 1ST FLOOR

GROSS LIVABLE AREA:	
LIVABLE:	650 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	128 S.F.
TOTAL:	778 S.F.



UNIT AS
 UPPER FLOORS

GROSS LIVABLE AREA:	
LIVABLE:	650 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	64 S.F.
TOTAL:	714 S.F.

FLOOR PLAN
 UNIT AS

A2.4.0



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EXPIRES 9/30/25

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PLANNING
 RESUBMITTAL

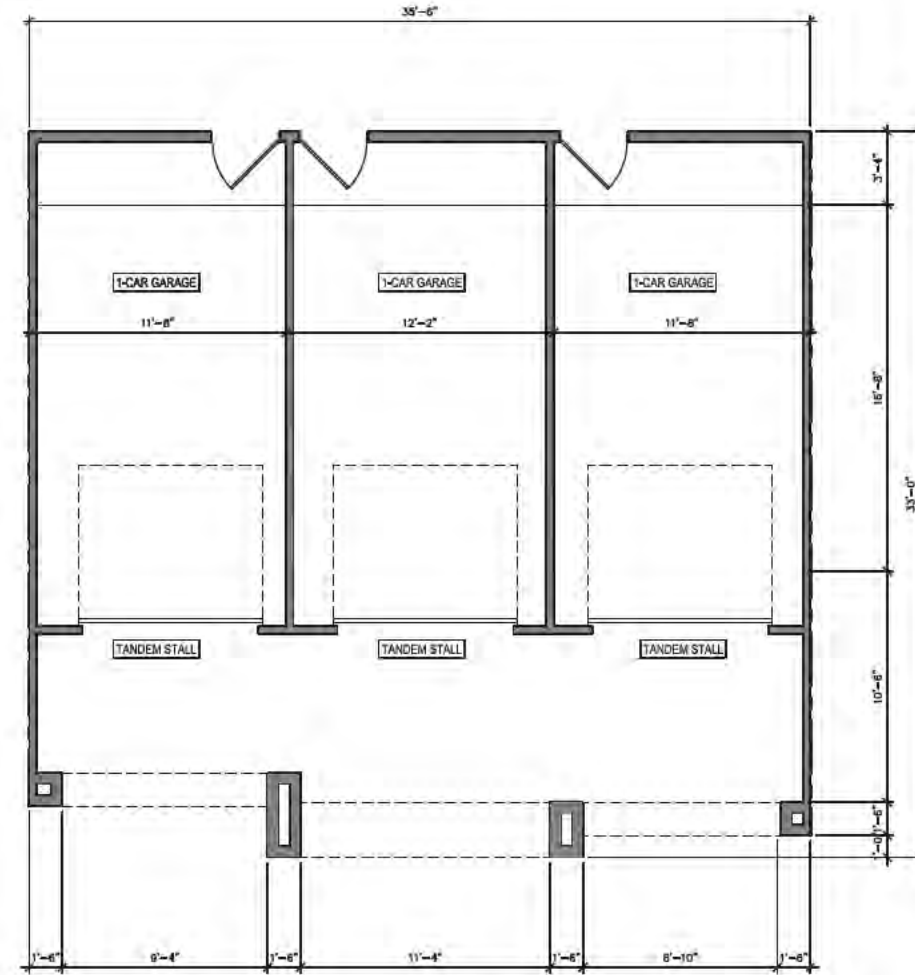
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:



UNIT B1
 UPPER FLOORS

GROSS LIVABLE AREA:	1,068 S.F.
LIVABLE:	9 S.F.
STORAGE:	72 S.F.
PATIO/BALCONY:	1,140 S.F.
TOTAL:	



UNIT B1 - GARAGE
 TYPICAL

GROSS AREA:	713 S.F.
GARAGE:	427 S.F.
TANDEM PARKING:	1,140 S.F.
TOTAL:	

FLOOR PLANS
 UNIT B1 & B1 GARAGE

A2.5.0



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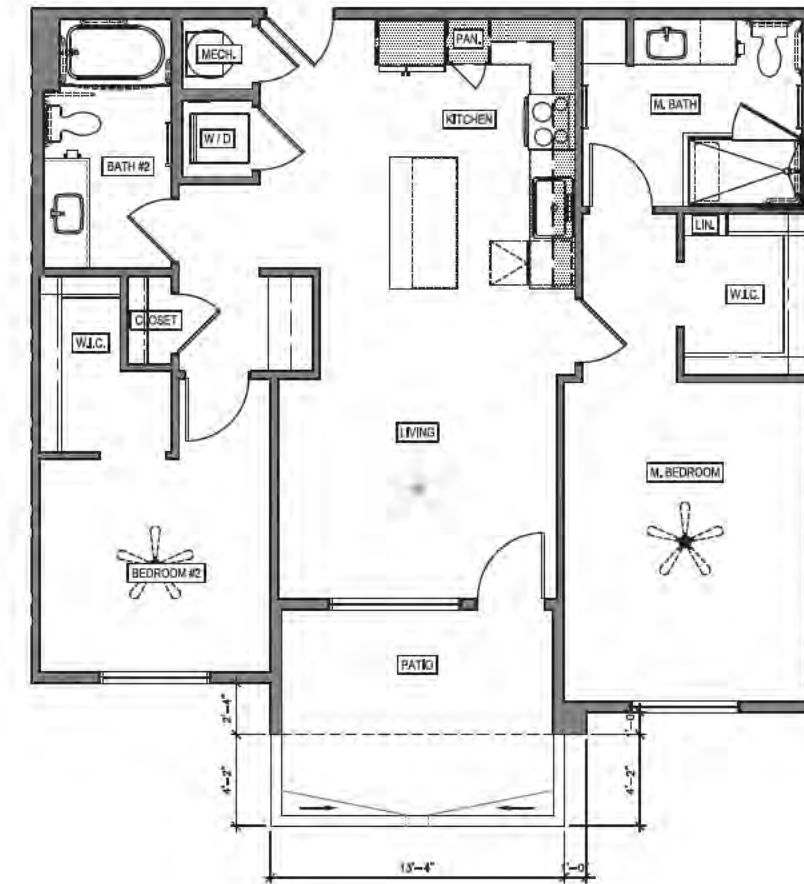
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DOG YARD
 REFER TO
 SITE PLAN

UNIT B1.3
 1ST FLOOR - DOG YARD

GROSS LIVABLE AREA:	1,066 S.F.
LIVABLE:	0 S.F.
STORAGE:	72 S.F.
PATIO/BALCONY:	1,140 S.F.
TOTAL:	



UNIT B1.1 & B1.2
 1ST FLOOR

GROSS LIVABLE AREA:	1,111 S.F.
LIVABLE:	28 S.F.
STORAGE:	155 S.F.
PATIO:	1,291 S.F.
TOTAL:	

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
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PLANNING
 RESUBMITTAL

REVISIONS:	
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:

FLOOR PLANS
 UNIT B1

A2.5.1



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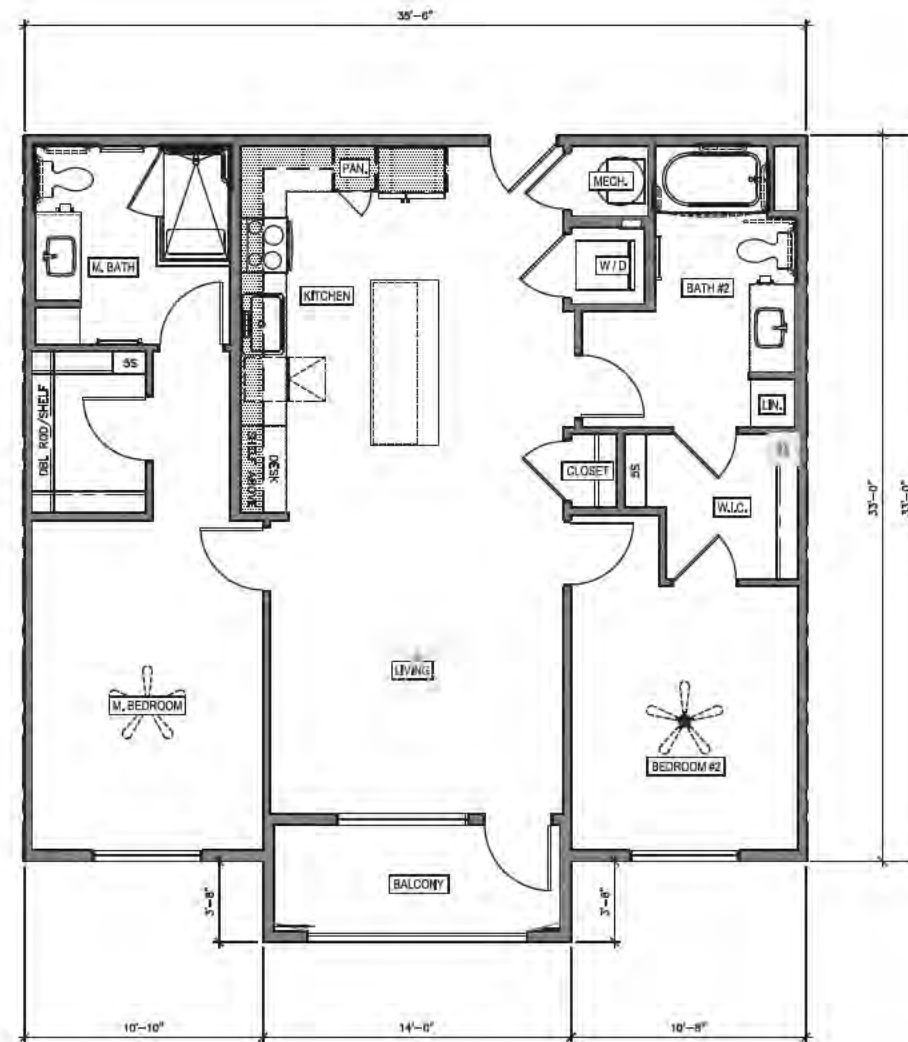
EXPIRES 9/30/25

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TOLL BROTHERS APARTMENT LIVING
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PLANNING
 RESUBMITTAL

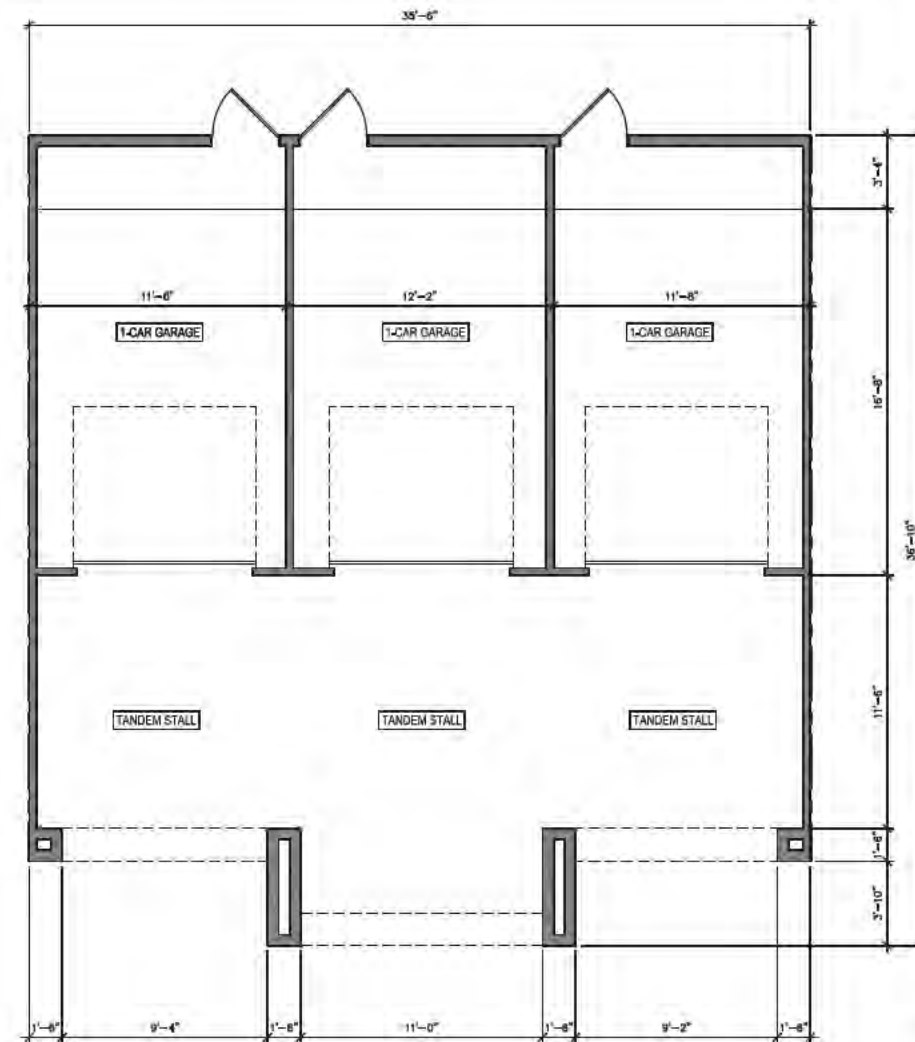
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:



UNIT B2
 UPPER FLOORS

GROSS LIVABLE AREA:	1,153 S.F.
LIVABLE:	9 S.F.
STORAGE:	73 S.F.
PATIO/BALCONY:	1,226 S.F.
TOTAL:	



UNIT B2 - GARAGE
 TYPICAL

GROSS AREA:	711 S.F.
GARAGE:	618 S.F.
TANDEM PARKING:	1,226 S.F.
TOTAL:	

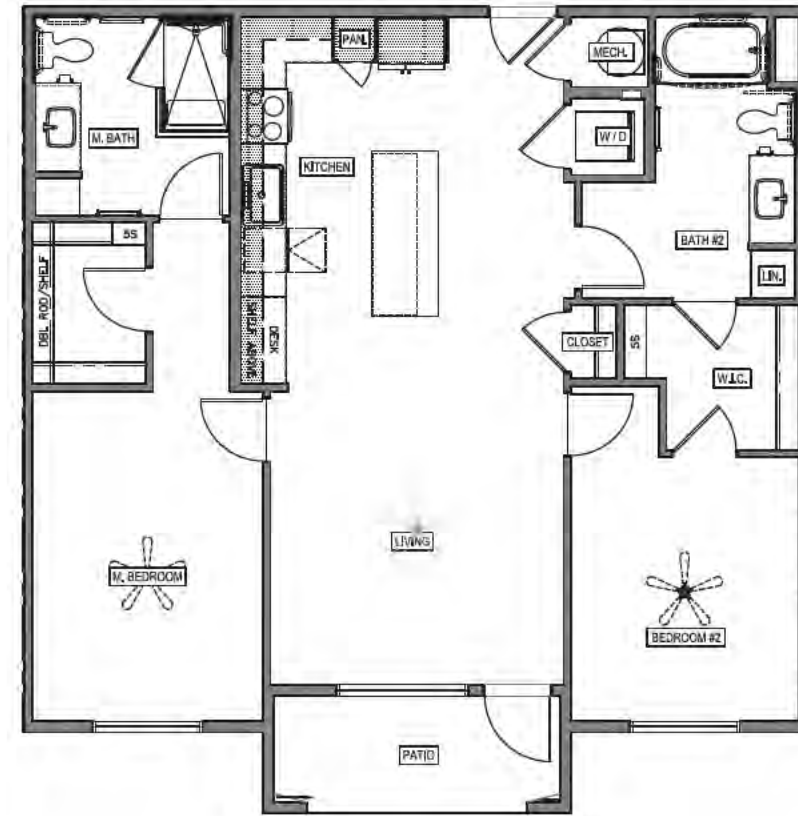
FLOOR PLANS
 UNIT B2 & B2 GARAGE

A2.6.0



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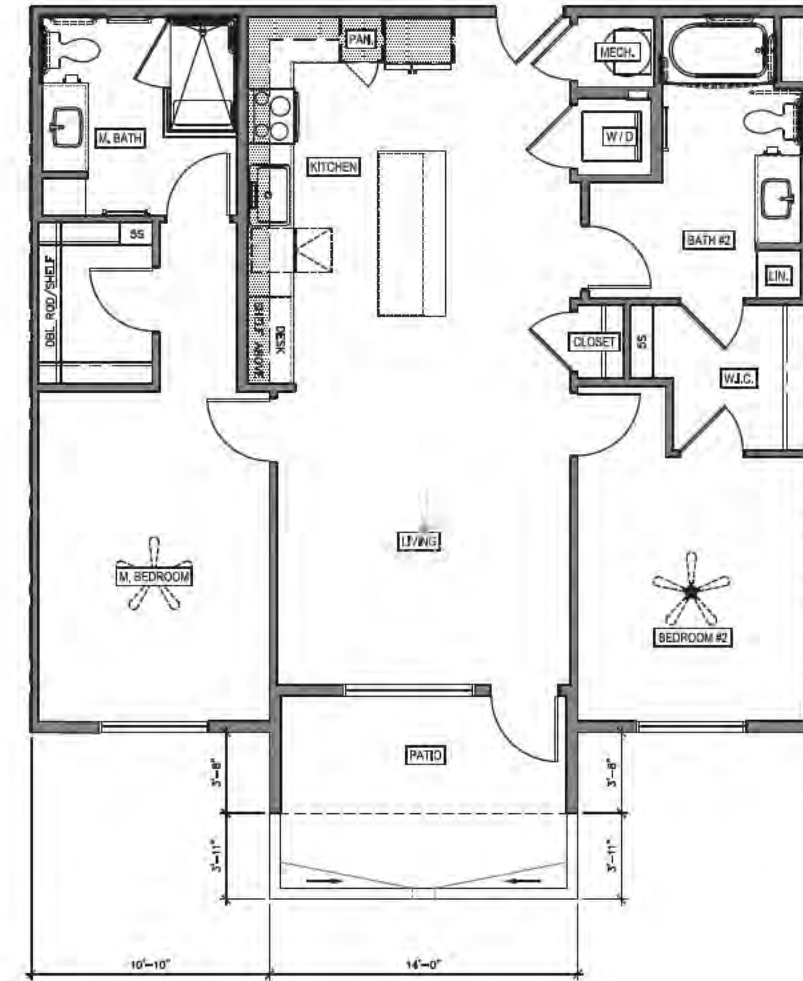
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DOG YARD
 REFER TO
 SITE PLAN

UNIT B2.3
 1ST FLOOR - DOG YARD

GROSS LIVABLE AREA:	1,153 S.F.
LIVABLE:	0 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	73 S.F.
TOTAL:	1,226 S.F.



UNIT B2.1 & B2.2
 1ST FLOOR

GROSS LIVABLE AREA:	1,153 S.F.
LIVABLE:	0 S.F.
STORAGE:	0 S.F.
PATIO/BALCONY:	126 S.F.
TOTAL:	1,281 S.F.

EXPIRES 9/30/25

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PLANNING
 RESUBMITTAL

REVISIONS:	
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JOB NO: 25-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:

FLOOR PLANS
 UNIT B2

A2.6.1

EXPIRES 9/30/25

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PLANNING
RESUBMITTAL

REVISIONS	
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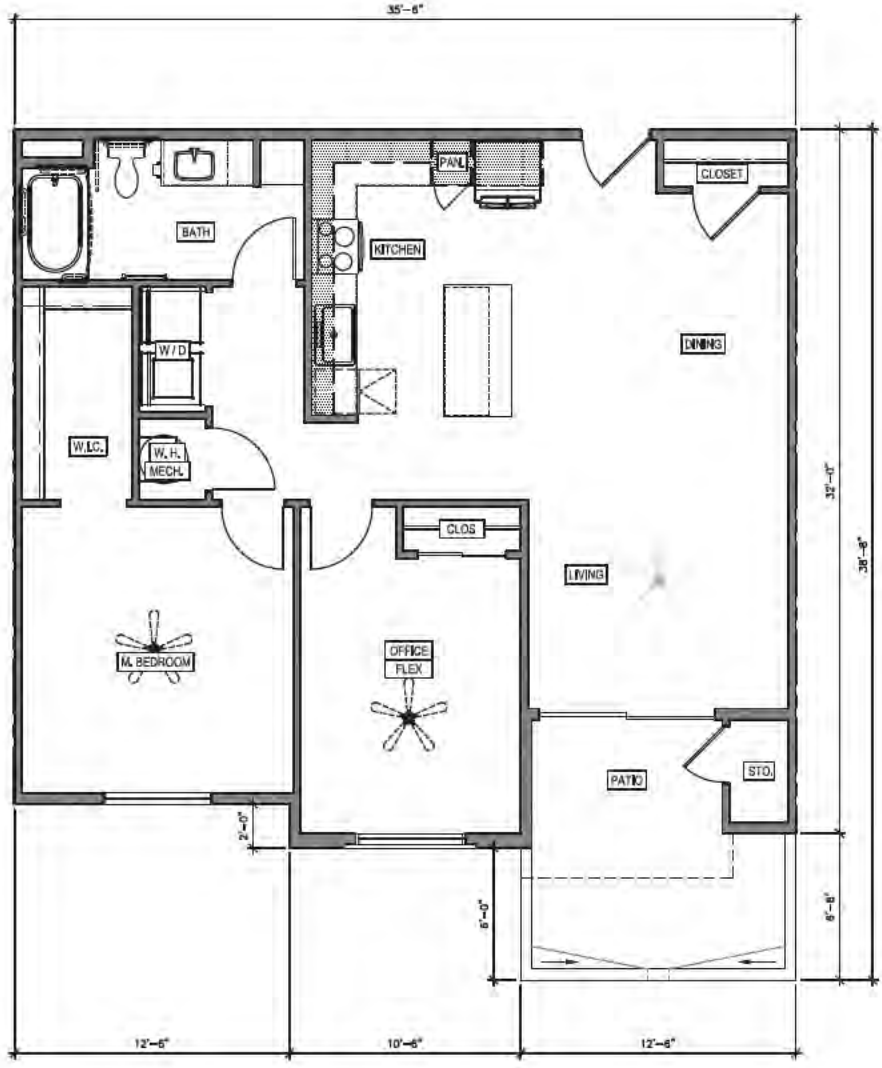
JOB NO: 25-031
SCALE: 1/4" = 1'-0"
SHEET NO:



DOG YARD
REFER TO
SITE PLAN

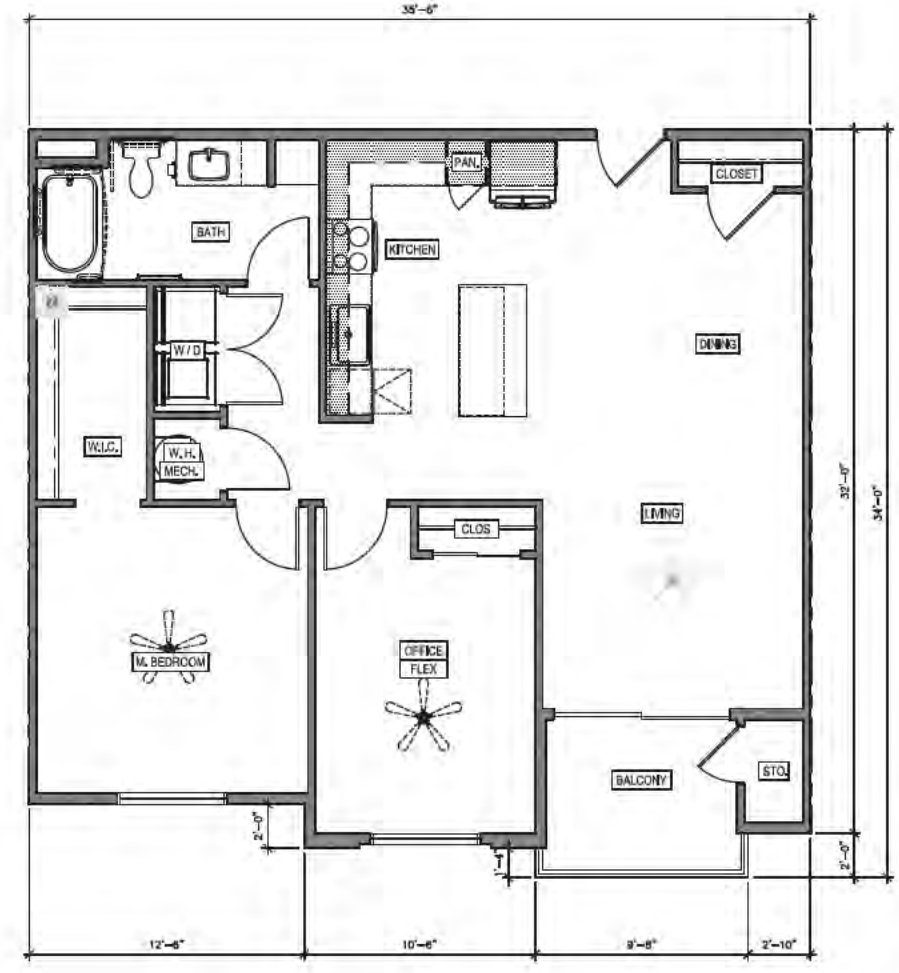
UNIT B4.3
1ST FLOOR - DOG YARD

GROSS LIVABLE AREA:	1,067 S.F.
LIVABLE:	17 S.F.
STORAGE:	64 S.F.
PATIO/BALCONY:	146 S.F.
TOTAL:	1,146 S.F.



UNIT B4.1 & B4.2
1ST FLOOR

GROSS LIVABLE AREA:	1,067 S.F.
LIVABLE:	17 S.F.
STORAGE:	64 S.F.
PATIO/BALCONY:	146 S.F.
TOTAL:	1,212 S.F.



UNIT B4
UPPER FLOORS

GROSS LIVABLE AREA:	1,067 S.F.
LIVABLE:	17 S.F.
STORAGE:	64 S.F.
PATIO/BALCONY:	146 S.F.
TOTAL:	1,146 S.F.

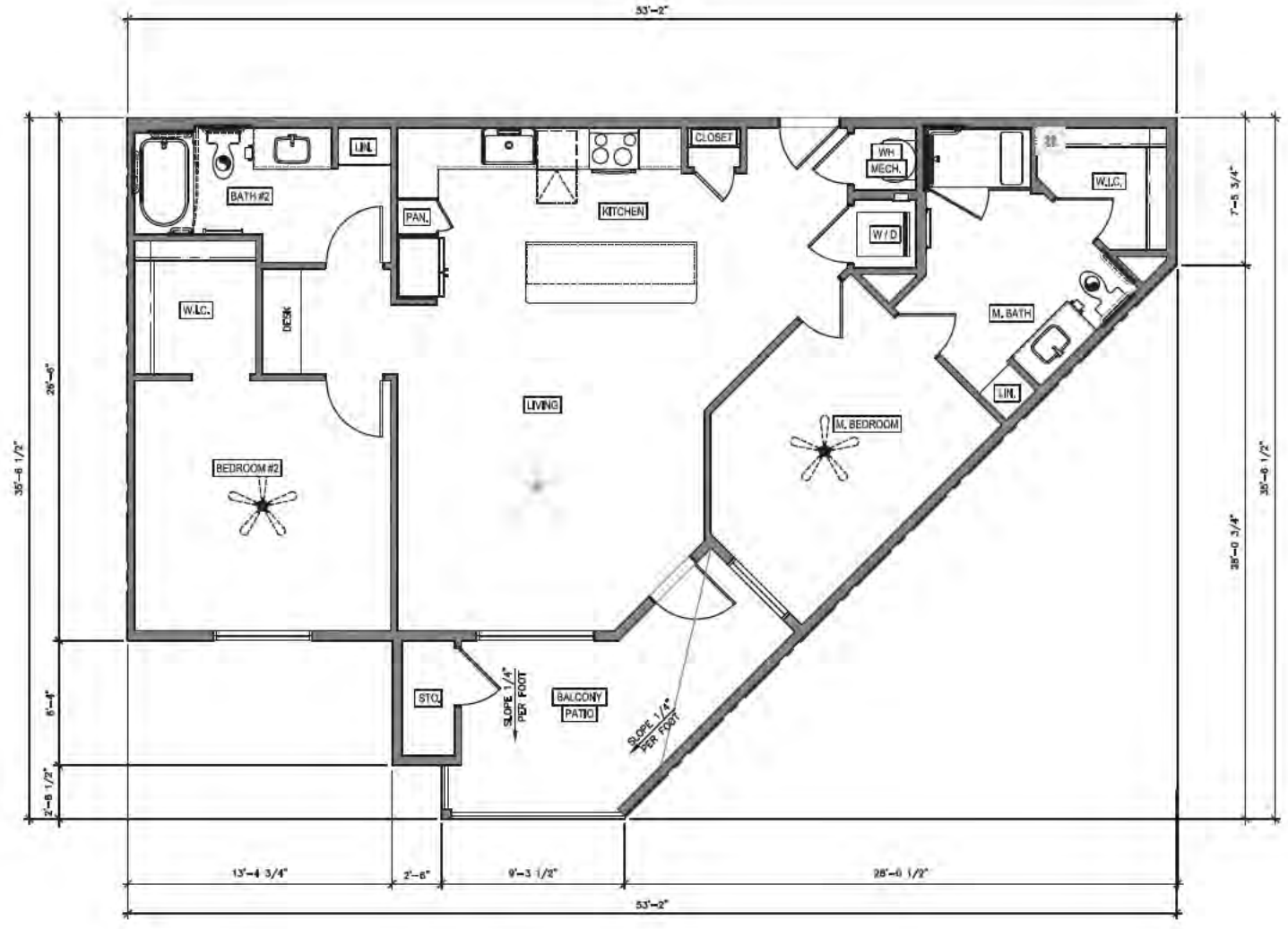
FLOOR PLANS
UNIT B4

A2.7.0



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UNIT B5
 ALL FLOORS

GROSS LIVABLE AREA:	
LIVABLE:	1208 S.F.
STORAGE:	22 S.F.
PATIO/BALCONY:	133 S.F.
TOTAL:	1,363 S.F.

EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA

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PLANNING
 RESUBMITTAL

REVISIONS:	
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:

FLOOR PLAN
 UNIT B5

A2.8.0



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 Phone 602.285.0200 Fax 602.285.5225

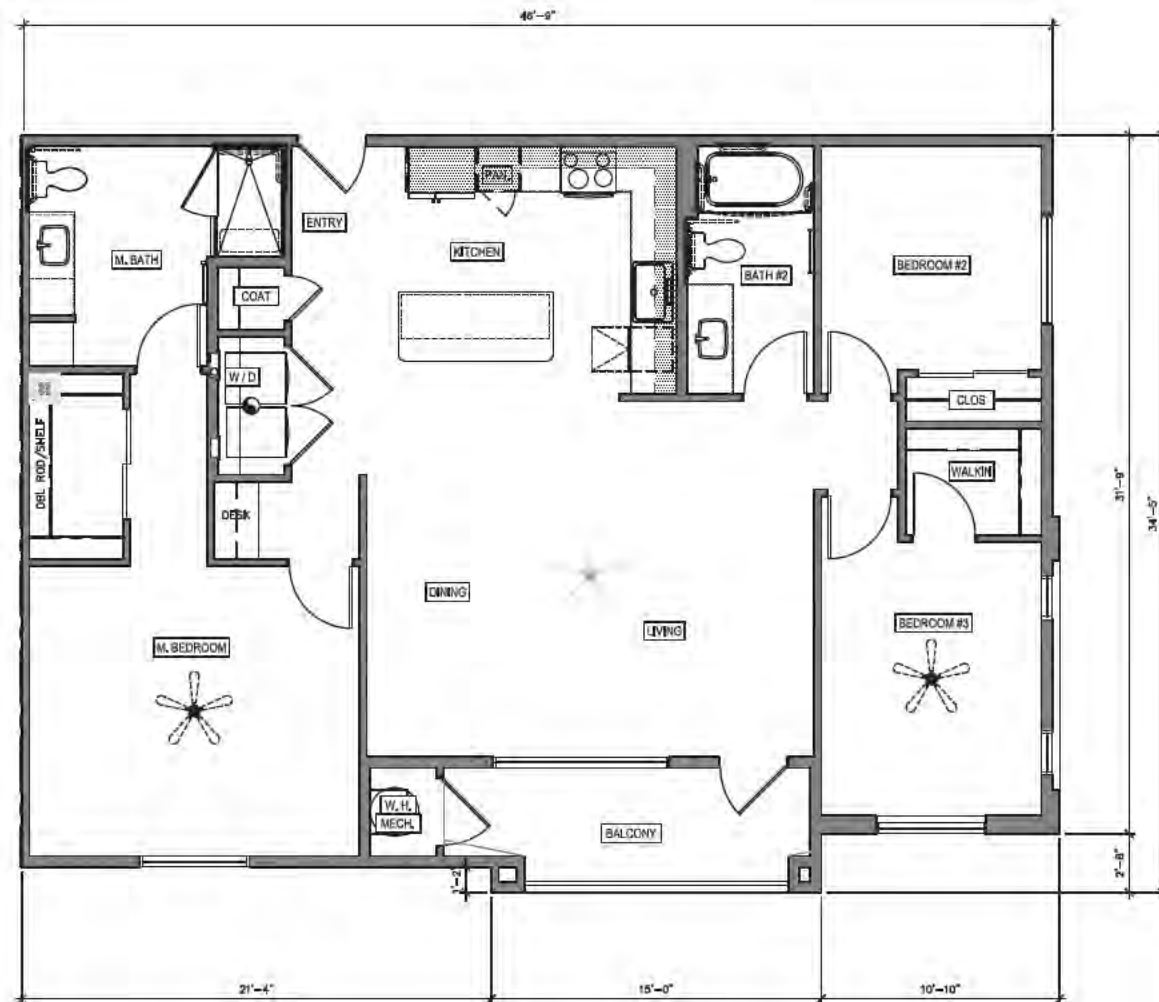
EXPIRES 9/30/25

TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
 RESUBMITTAL

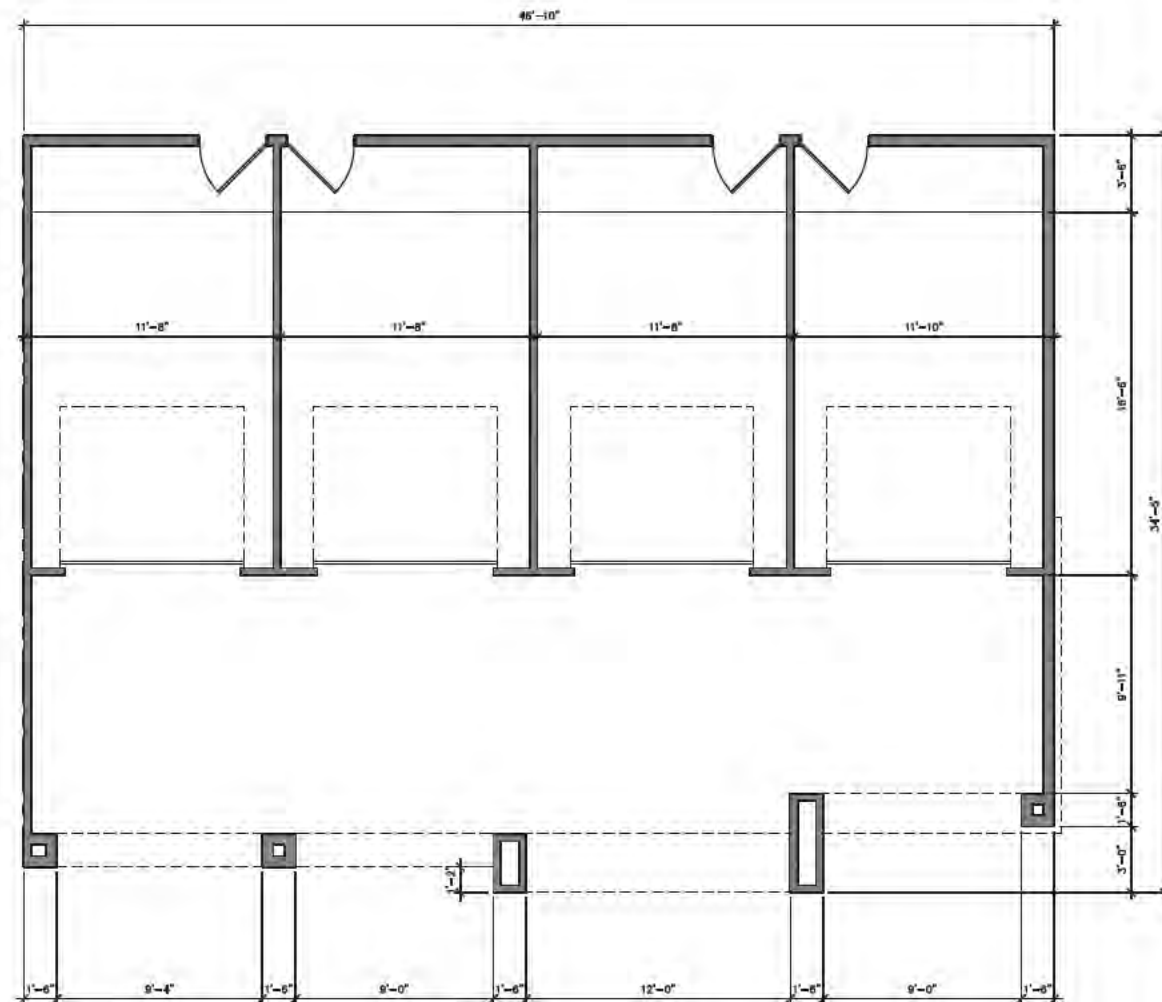
REVISIONS
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JOB NO: 24-031
 SCALE: 1/4" = 1'-0"
 SHEET NO:



UNIT C1
 UPPER FLOORS

GROSS LIVABLE AREA:	1,450 S.F.
LIVABLE:	15 S.F.
STORAGE:	90 S.F.
PATIO/BALCONY:	1,345 S.F.
TOTAL:	1,555 S.F.



UNIT C1 - GARAGE
 TYPICAL

GROSS AREA:	938 S.F.
GARAGE:	817 S.F.
TANDEM PARKING:	1,555 S.F.
TOTAL:	1,555 S.F.

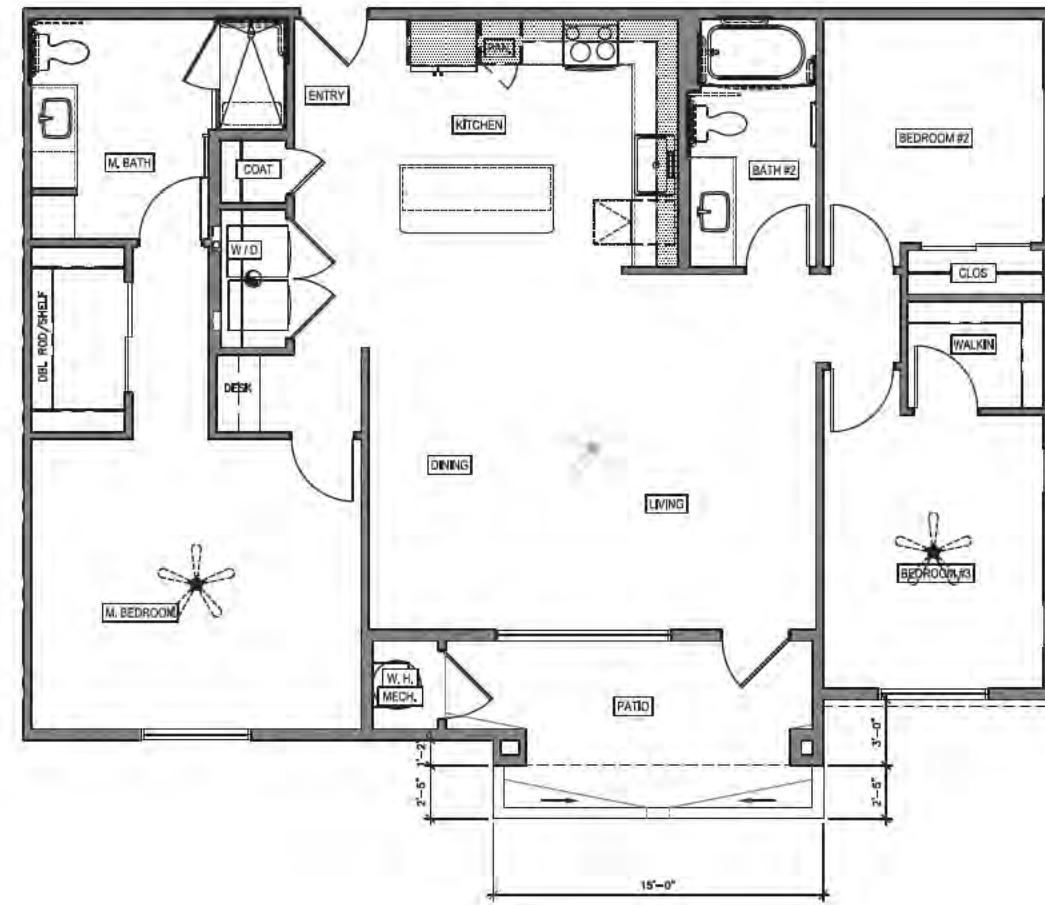
FLOOR PLANS
 UNIT C1 & C1 GARAGE

A2.9.0



biftform architecture
group, inc.

11460 north cave creek road - suite 11
sharahl - phoenix 85020
Phone 602,285,0200 Fax 602,285,5225



UNIT C1.1 & C1.2
1ST FLOOR

GROSS LIVABLE AREA:	1,460 S.F.
LIVABLE:	15 S.F.
STORAGE:	129 S.F.
PATIO/BALCONY:	1,594 S.F.
TOTAL:	1,594 S.F.

EXPIRES 9/30/25

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Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

PLANNING
RESUBMITTAL

REVISIONS	
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JOB NO: 25-031
SCALE: 1/4" = 1'-0"
SHEET NO:

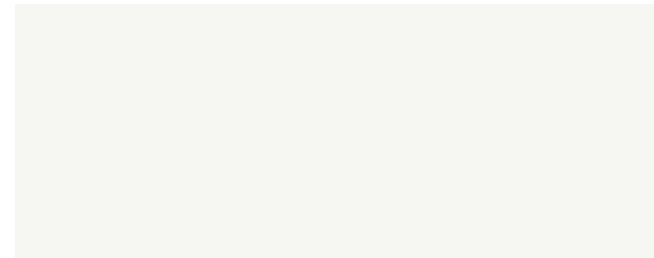
FLOOR PLANS
UNIT C1

A2.9.1

Toll Brothers Uptown

23-031

Dunn Edwards DEW381 Droplets
Body Color 1



**Aluminum Clear
Anodized Finish**
Metal Screens



Dunn Edwards DE6376 Looking Glass
Body Color 2



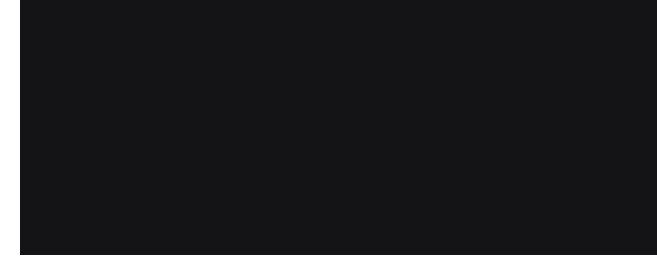
**Trenwyth High
Polish 4107 8"x16"**
Masonry Veneer



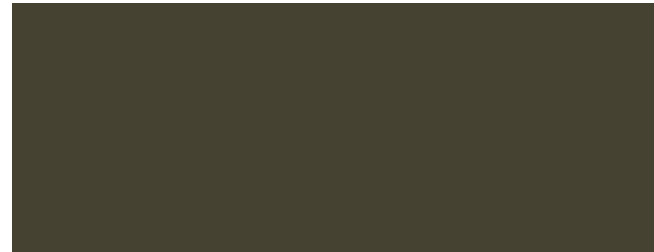
**Benjamin Moore AC-34
Cape Hatteras Sand**
Body Color 3



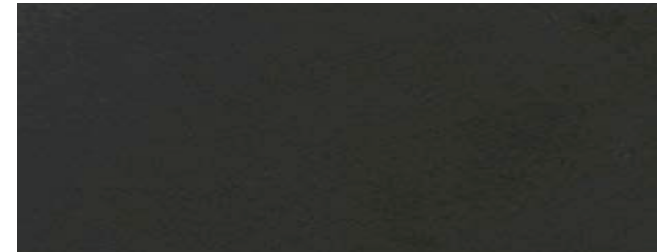
**Railings, Doors &
Metal Canopies**
Sherwin Williams
SW6258 Tricon
Black



**Benjamin Moore 2139-10
River Rock**
Body Color 4



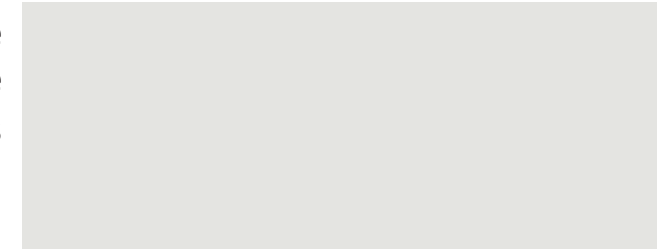
Dark Bronze
Window/
Storefront Frames

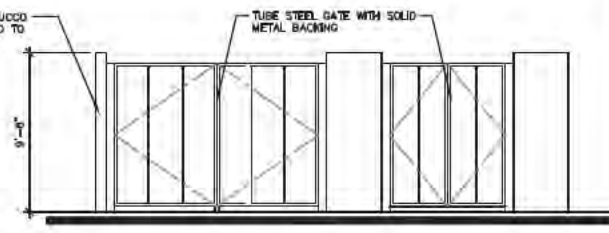
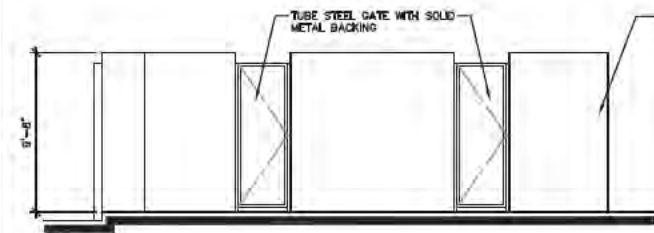
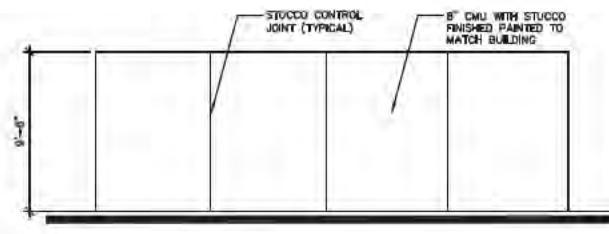
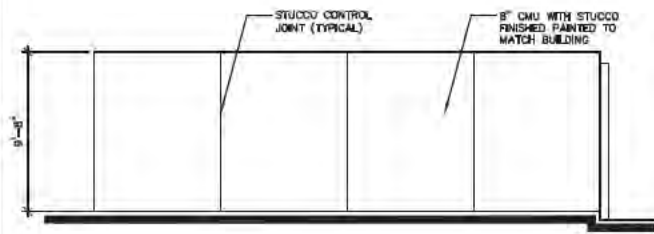


**Dunn Edwards DE6105
Weathered Leather**
Body Color 5 & Lap Siding

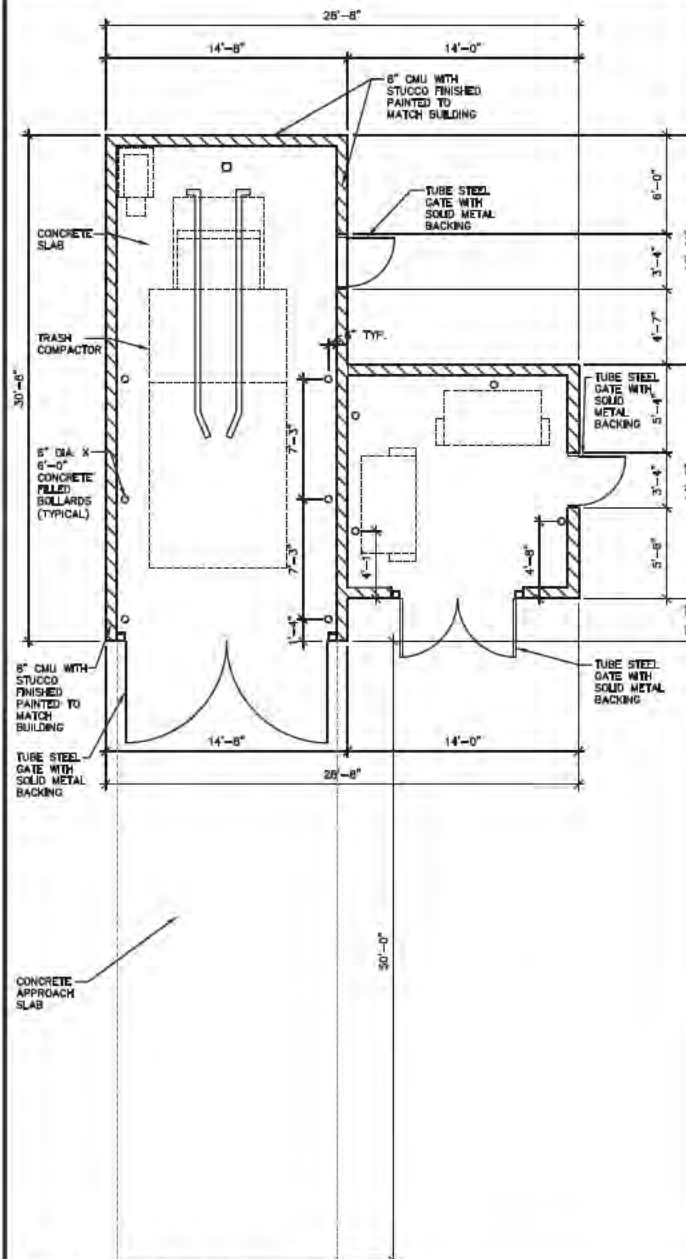


**Benjamin Moore
2112-70 American White**
Fiber Cement Panels

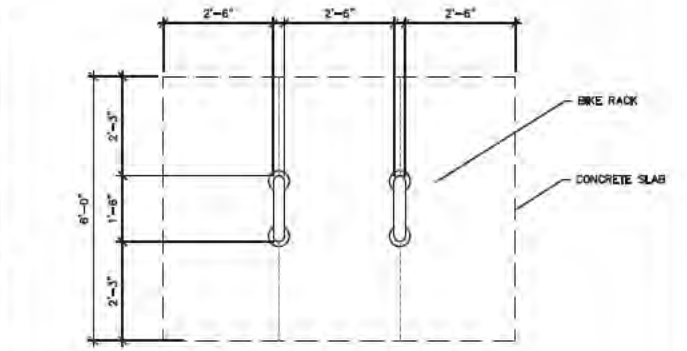




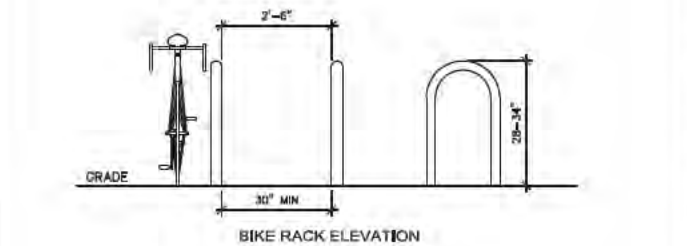
5 REFUSE ENCLOSURE ELEVATION
3/16" = 1'-0"



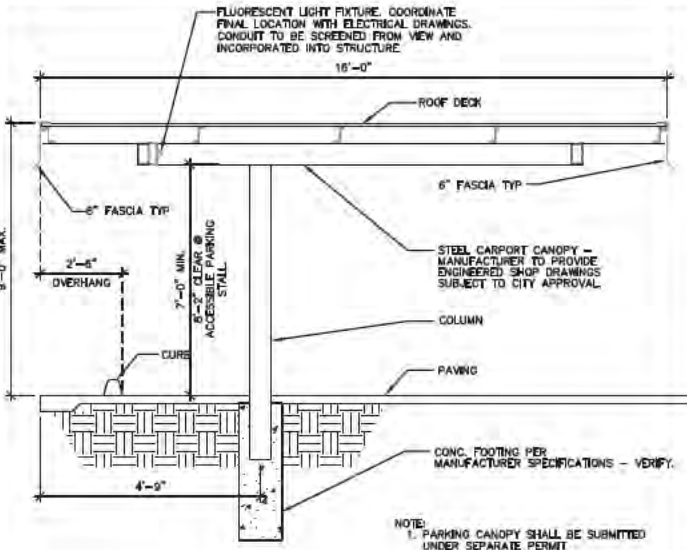
4 REFUSE ENCLOSURE PLAN
3/16" = 1'-0"



1 BIKE RACK - PLAN VIEW
1/2" = 1'-0"



2 BIKE RACK - ELEVATION
1/2" = 1'-0"



3 PARKING CANOPY
3/8" = 1'-0"



biltform architecture group, inc

11400 north cove creek road - suite 111
chandler, arizona 85020
Phone 602.285.0200 Fax 602.285.0225

EXPIRES 9/30/25

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CHANDLER, ARIZONA

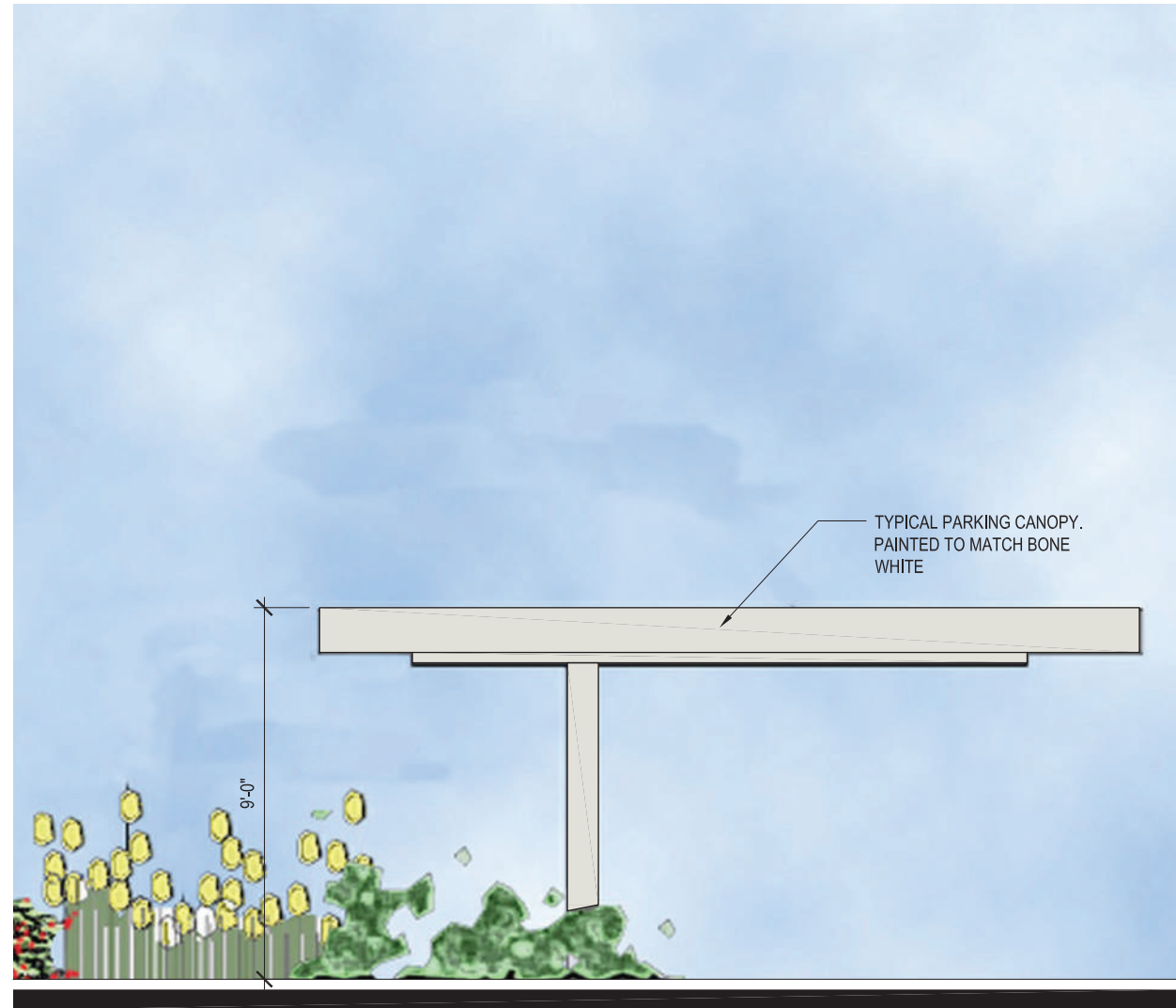
TOLL BROTHERS APARTMENT LIVING
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PLANNING RESUBMITTAL

REVISIONS	
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JOB NO: 23451
SCALE: 3/16" = 1'-0"
SHEET NO:

ASD1



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group, inc.
11460 north cave creek road, suite 11
phoenix, arizona 85020
Phone 602.285.9200
www.biltform.com

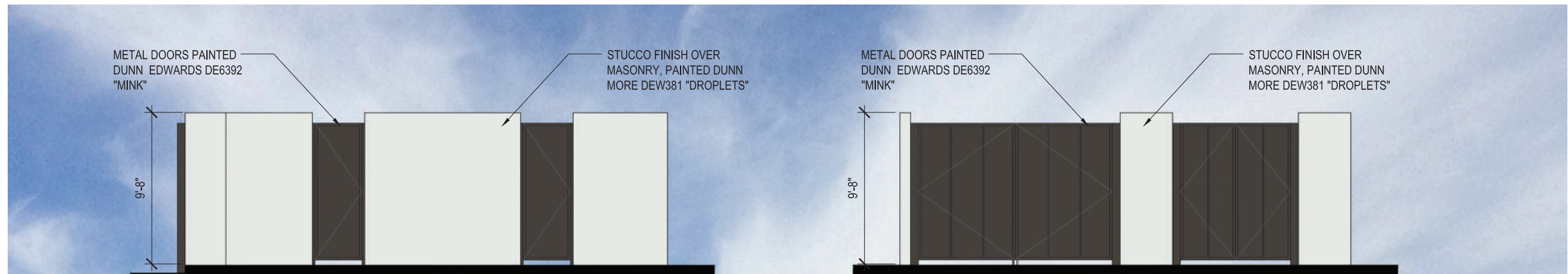
TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

PARKING CANOPY EXHIBIT
COLOR
N.T.S.
preliminary not for construction

05/21/2024

EXA

N.T.S.



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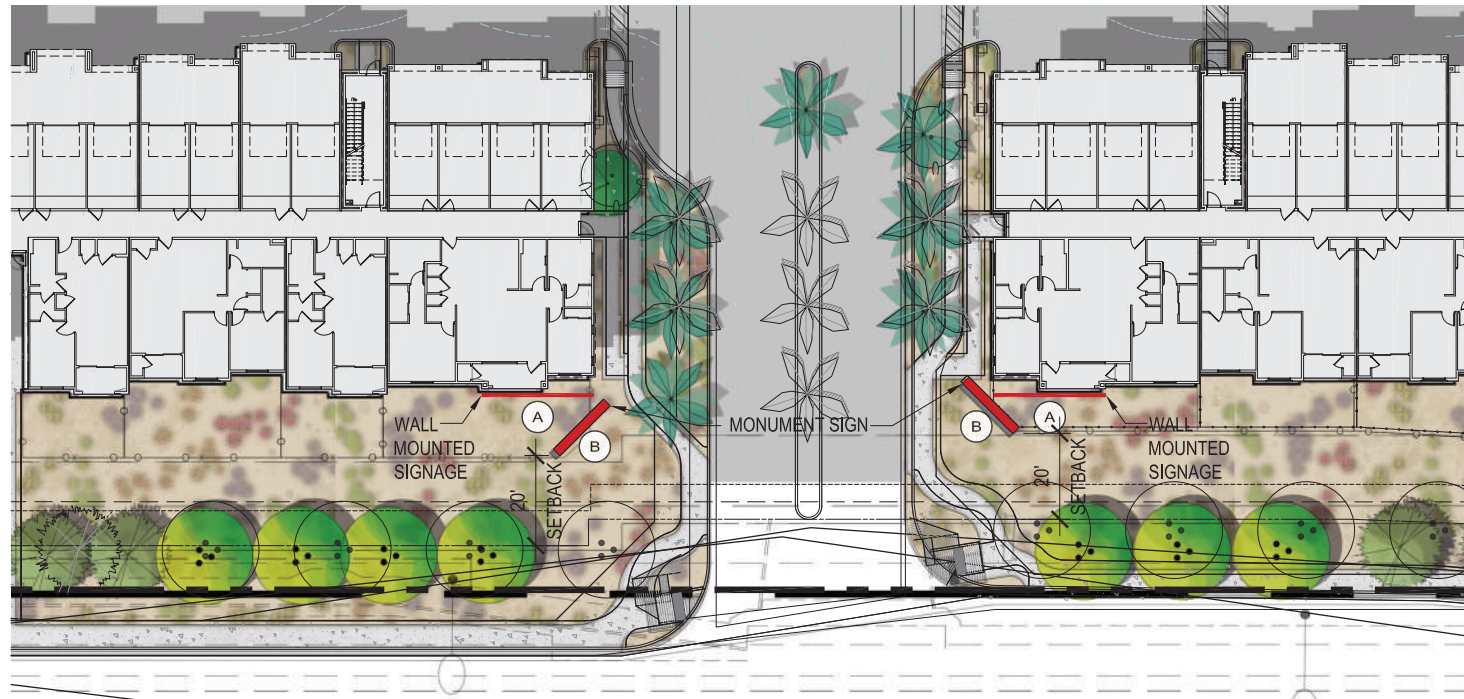
TOLL BROTHERS UPTOWN TOLL BROTHERS APARTMENT LIVING

COMPACTOR EXHIBIT
COLOR
N.T.S.
preliminary not for construction

05/21/2024

EXB

N.T.S.



SIGNAGE TO BE ILLUMINATED AS ALLOWED BY CITY OF CHANDLER SIGN CODE

B MONUMENT SIGNAGE
1/2" = 1'-0"



A BUILDING SIGNAGE
1/4" = 1'-0"

EXHIBITS ARE FOR CONCEPTUAL PURPOSES ONLY, FINAL SIGNAGE DESIGN AND LAYOUT BY OTHERS

BUILDING AND MONUMENT SIGN EXHIBIT

07/29/2024
EXC

N.T.S.
preliminary not for construction

TOLL BROTHERS UPTOWN

TOLL BROTHERS APARTMENT LIVING



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Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25

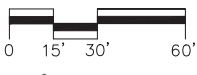
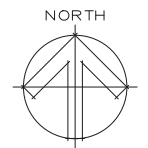
TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

100% SCHEMATIC
DESIGN
02/16/2024

REVISIONS:

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JOB NO: 23-031
SCALE: 1" = 30'-0"
SHEET NO:



**SITE LIGHTING
ELECTRICAL PLAN**



E1.1



biltform architecture group, LLC.
 11460 north cave creek road - suite 11
 phoenix - arizona 85020
 Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25

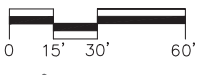
TOLL BROTHERS UPTOWN
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TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

100% SCHEMATIC DESIGN
 02/16/2024

REVISIONS:

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JOB NO: 23-031
 SCALE: 1" = 30'-0"
 SHEET NO:



SITE LIGHTING ELECTRICAL PLAN



E1.2



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group, LLC.
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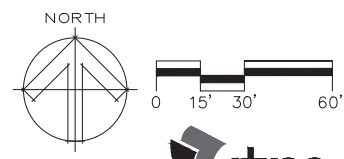
EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
Apartment Homes
CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85268

100% SCHEMATIC
DESIGN
02/16/2024

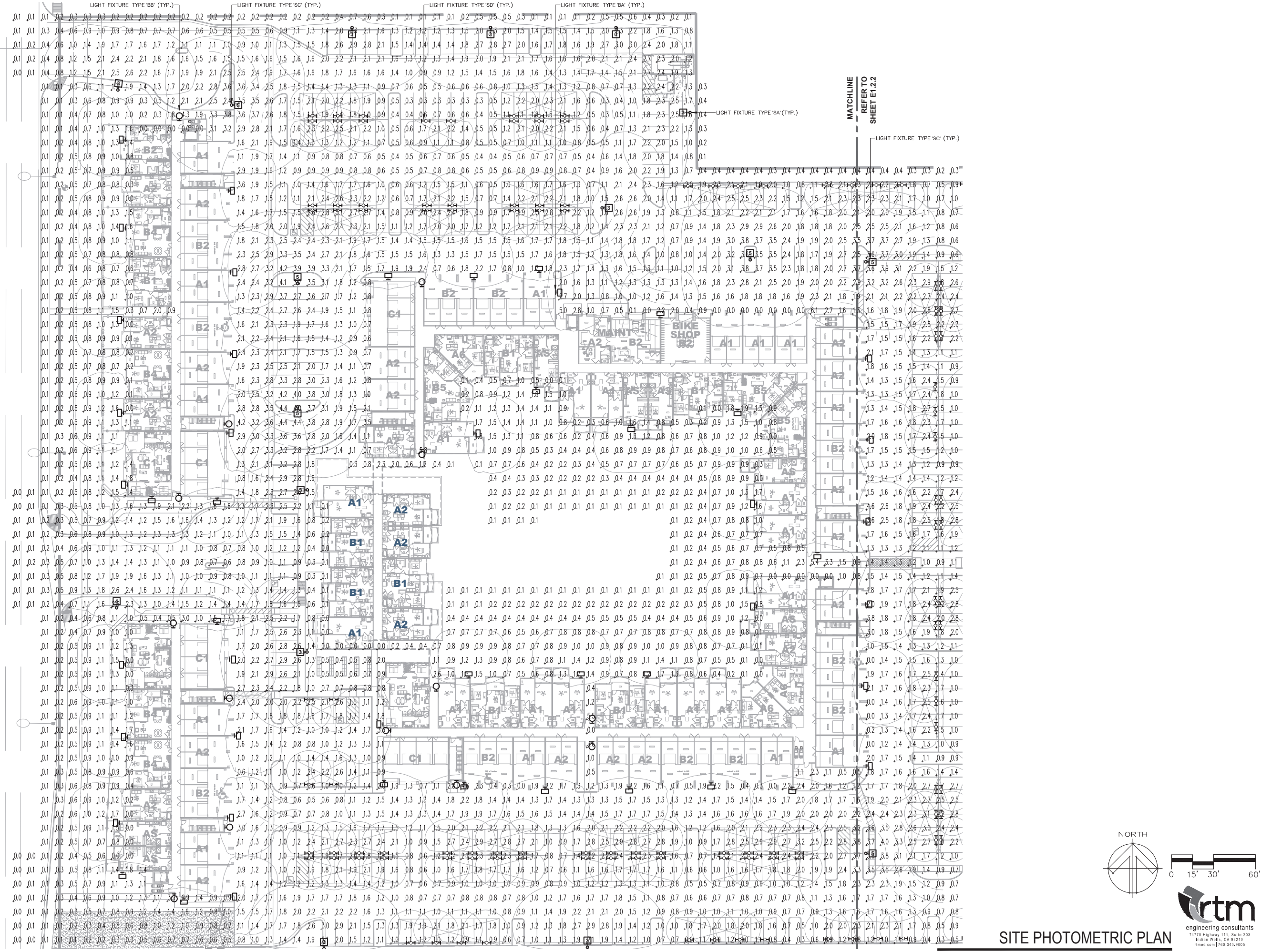
REVISIONS:

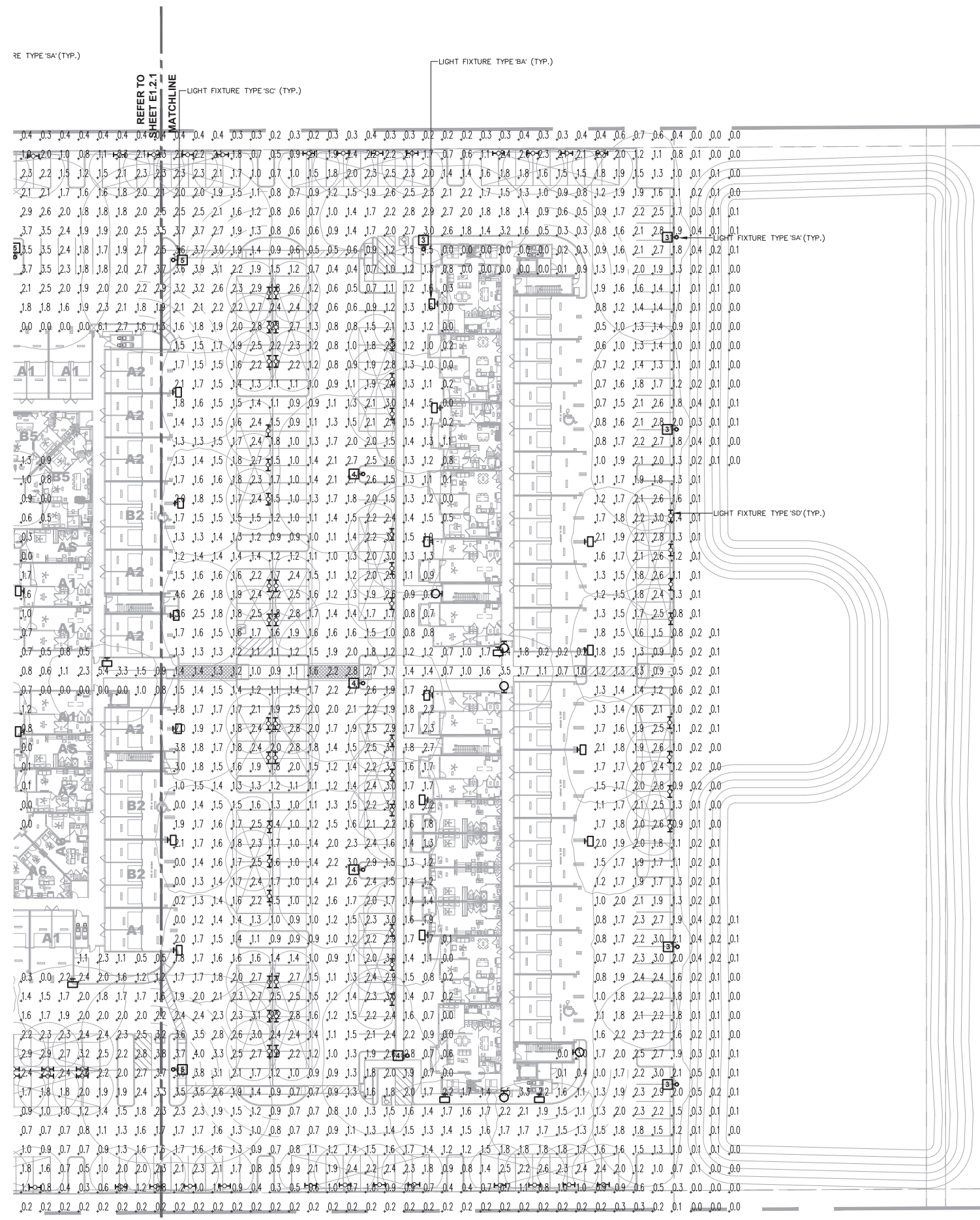
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JOB NO: 23-031
SCALE: 1" = 30'-0"
SHEET NO:
E1.3



SITE PHOTOMETRIC PLAN





RE TYPE 'SA' (TYP.)

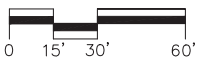
LIGHT FIXTURE TYPE 'BA' (TYP.)

REFER TO SHEET E1.2.1 MATCHLINE

LIGHT FIXTURE TYPE 'SC' (TYP.)

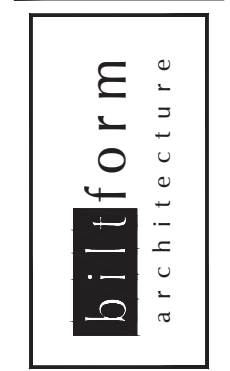
LIGHT FIXTURE TYPE 'SA' (TYP.)

LIGHT FIXTURE TYPE 'SD' (TYP.)



SITE PHOTOMETRIC PLAN

rtm
 engineering consultants
 74770 Highway 111, Suite 203
 Indian Wells, CA 92210
 rtmec.com | 760.346.9005



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 11460 north cave creek road - suite 11
 phoenix - arizona 85020
 Phone 602.285.9200 Fax 602.285.9229

EXPIRES 9/30/25
TOLL BROTHERS UPTOWN
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85288

100% SCHEMATIC DESIGN
 02/16/2024


REVISIONS:

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JOB NO: 23-031
 SCALE: 1" = 30'-0"
 SHEET NO:

E1.4

D-Series Size 1 LED Area Luminaire



Specifications

EPA: 1.00 ft² (0.09 m²)
 Length: 22.71" (582 mm)
 Width: 14.28" (363 mm)
 Height H1: 7.88" (200 mm)
 Height H2: 2.73" (69 mm)
 Weight: 34 lbs (15 kg)

Introduction
 The modern styling of the D-Series features a highly refined aesthetic that blends seamlessly with its environment. The D-Series offers the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. D-Series outstanding photometry aids in reducing the number of poles required in area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

Ordering Information EXAMPLE: DSX1 LED P7 40K 70CRI T3M MVOLT SPA NLTAR2 PIRHN DB8XD


Series	LEDs	Color Temperature	Color Rendering Index	Dimensions	Voltage	Mounting	Shipped/Included
DSX1 LED	P7	30K	90CRI	T3M	120V/277V	SPA	Shipped/Included
	P7	40K	90CRI	T3M	120V/277V	SPA	Shipped/Included
	P7	50K	90CRI	T3M	120V/277V	SPA	Shipped/Included

Accessories

3455: 30' pole
 3456: 36' pole
 3457: 42' pole
 3458: 48' pole
 3459: 54' pole
 3460: 60' pole
 3461: 66' pole
 3462: 72' pole
 3463: 78' pole
 3464: 84' pole
 3465: 90' pole
 3466: 96' pole
 3467: 102' pole
 3468: 108' pole
 3469: 114' pole
 3470: 120' pole

3455 3456 3457 3458 3459 3460 3461 3462 3463 3464 3465 3466 3467 3468 3469 3470
 FIXTURE TYPE 'SA', 'SB' AND 'SC'
 MOUNT @ '18-0" A.F.F.

D-Series Size 1 LED Wall Luminaire



Specifications

Width: 13.39" (340 mm)
 Depth: 10" (254 mm)
 Height: 6.31" (160 mm)

Back Box (BBW, E20WC)

Width: 13.39" (340 mm)
 Depth: 4" (102 mm)
 Height: 6.31" (160 mm)

Introduction
 The D-Series Wall luminaire is a stylish, fully integrated LED solution for building-mount applications. It features a sleek, modern design and is carefully engineered to provide long-lasting, energy-efficient lighting with a variety of optical and control options for customized performance.

With an expected service life of over 20 years of nighttime use and up to 74% in energy savings over comparable 25W metal halide luminaires, the D-Series Wall is a reliable, low-maintenance lighting solution that produces sites that are exceptionally illuminated.

Ordering Information EXAMPLE: DSXW1 LED 20C 1000 40K T3M MVOLT DB8TDX

Series	LEDs	Driver Current	Color Temperature	Dimensions	Voltage	Mounting	Control Options
DSXW1 LED	1000	1000mA	40K	T3M	120V/277V	DB8TDX	None
	1000	1000mA	50K	T3M	120V/277V	DB8TDX	None
	1000	1000mA	60K	T3M	120V/277V	DB8TDX	None

Accessories

3471: 30' pole
 3472: 36' pole
 3473: 42' pole
 3474: 48' pole
 3475: 54' pole
 3476: 60' pole
 3477: 66' pole
 3478: 72' pole
 3479: 78' pole
 3480: 84' pole
 3481: 90' pole
 3482: 96' pole
 3483: 102' pole
 3484: 108' pole
 3485: 114' pole
 3486: 120' pole

3471 3472 3473 3474 3475 3476 3477 3478 3479 3480 3481 3482 3483 3484 3485 3486 3487 3488 3489 3490
 FIXTURE TYPE 'BA'
 MOUNT @ '18-0" A.F.F.

Columbia Lighting LXEN

NARROW FACED, OPEN AND GASKETED LINEAR

FEATURES

- 4' fiberglass housing with F1 weatherability rating, standard
- Narrow housing design for applications with limited space
- IP65 and IP67 as available; IP65 all models with standard (optional) POM latches
- UL Sanitation certified to NSF standards
- Lens firmly secured with captive tamper resistance ready projection latches
- Removable gear tray electrical access
- Includes surface mounting brackets
- NEMA 4X housing
- Operates from -20°C to +50°C as standard

INSTALLATION

- Liter bestie: conduit only
- Electrical: Long life 50,000 hour LEDs at L80 for reduced maintenance
- Driver options include fixed output for on/off function or continuous 0-10V dimming
- QR code label affixed to housing for easy traceability

CERTIFICATIONS

- All luminaires are built to UL 1598 standards, and meet appropriate CSA/UL labels
- CSA certified to UL 924 standards with battery pack or DTS (Dimming Bypass Module) options
- UL Sanitation certified to NSF 2 Food Equipment standard for non-food zones
- Ingress protection IP65 and IP67 standards, IP65 on center recesses

WARRANTY

- 5 year warranty

STOCK GUIDE

Color	UPC	Description	Weight	Part No.	UPC
White	66181	4' LXEN	1.5 lbs	66181	66181

3491 3492 3493 3494 3495 3496 3497 3498 3499 3500
 FIXTURE TYPE 'SD'
 MOUNT AT CARPORT

S9370 Series

FEATURES

- 4' x 12" x 9" (1020mm x 305mm x 229mm)
- 4.5W/4.5W/4.5W (15W total)
- 9370-L12

ORDER AS A COMPLETE UNIT:
 Model No. + Lamp Code + CCT + Finish + Option Code
 S9370-L12 + 27K + PT + Option

FRISHES

- PT Powder Coated Finish
- *Specify Color Code from the list of Powder Coating Finishes (except interior only finishes)

OPTIONS

- E01 Remote 10W Emergency LED Battery Backup (Requires Additional Power Cable)

LIGHT OUTPUT

LX3 e - 25.6 LPW Delivered Lumens (Example: L12= 12W x 23.6LPW = 285.2 Lumens)

FEATURES

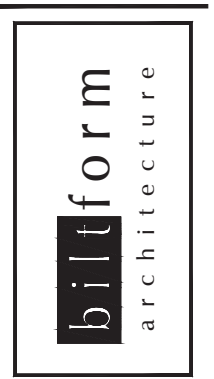
- Solid Aluminum Material
- Solid Top
- Driver Lenses Ground Glass Lens
- UL Listed for Wet Location
- LED 0-10V Dimming Driver

WARRANTY

- 5 year warranty

3501 3502 3503 3504 3505 3506 3507 3508 3509 3510
 FIXTURE TYPE 'BB'
 MOUNT @ 7'-6" A.F.F.

EXTERIOR LIGHTING
 FIXTURE CUTSHEETS



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 11450 north cave creek road suite 11 phoenix arizona 85020
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EXPIRES 9/30/25
 TOLL BROTHERS UPTOWN
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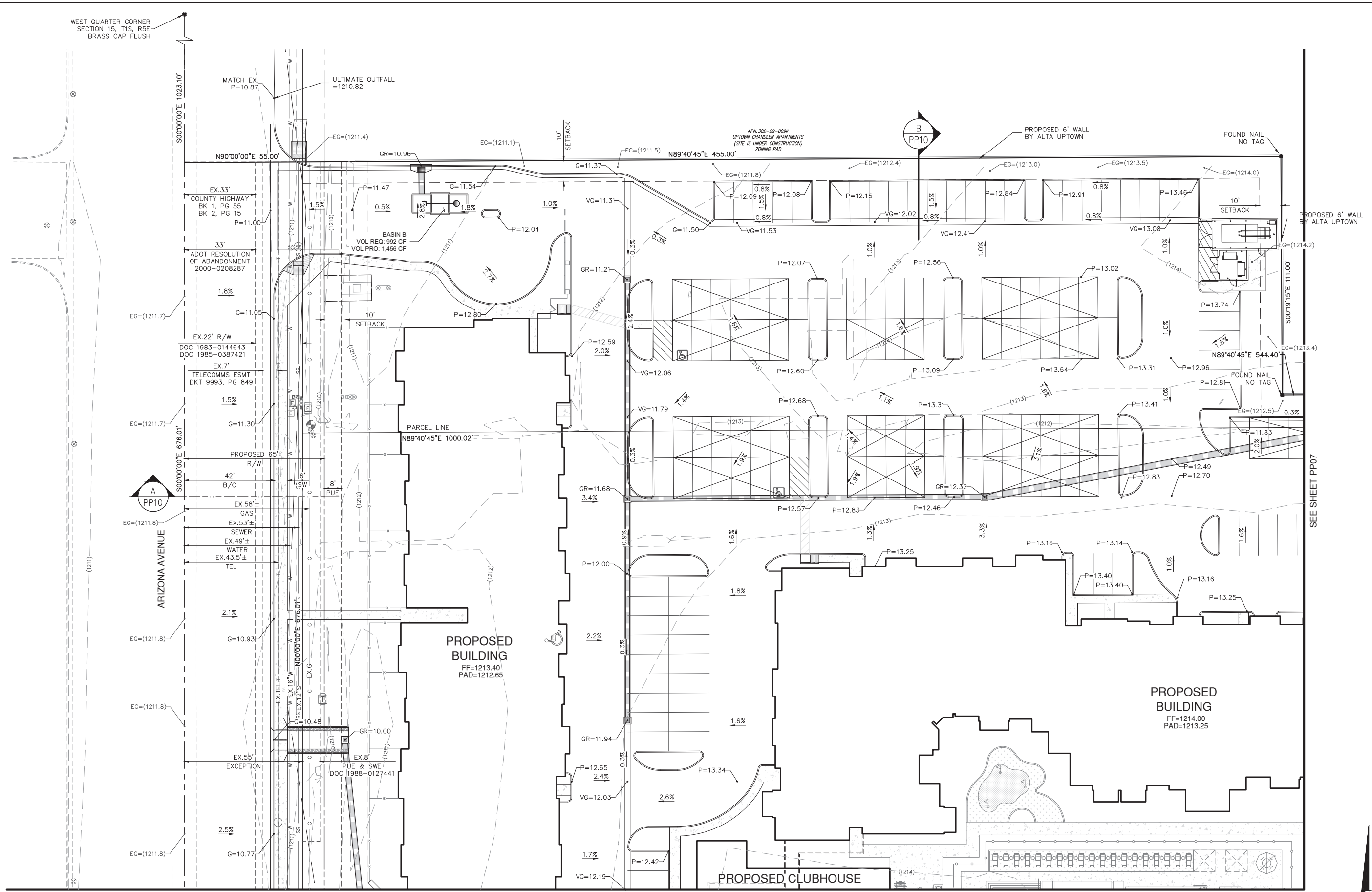
100% SCHEMATIC
 DESIGN
 02/16/2024

REVISONS:

NO.	DATE	DESCRIPTION
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JOB NO: 23-031
 SCALE: N/A
 SHEET NO:

E1.5



Bowman

Bowman Consulting Group Ltd
 2420 S. Power Road, Suite 105
 Mesa, Arizona 85209
 Phone: (480) 628-8830
 www.bowmanfg.com
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PRELIMINARY GRADING & DRAINAGE PLAN
TOLL BROTHERS UPTOWN
 CHANDLER, ARIZONA
 MARICOPA COUNTY

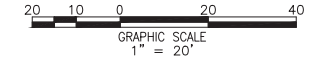
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 PROJECT NUMBER

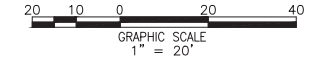
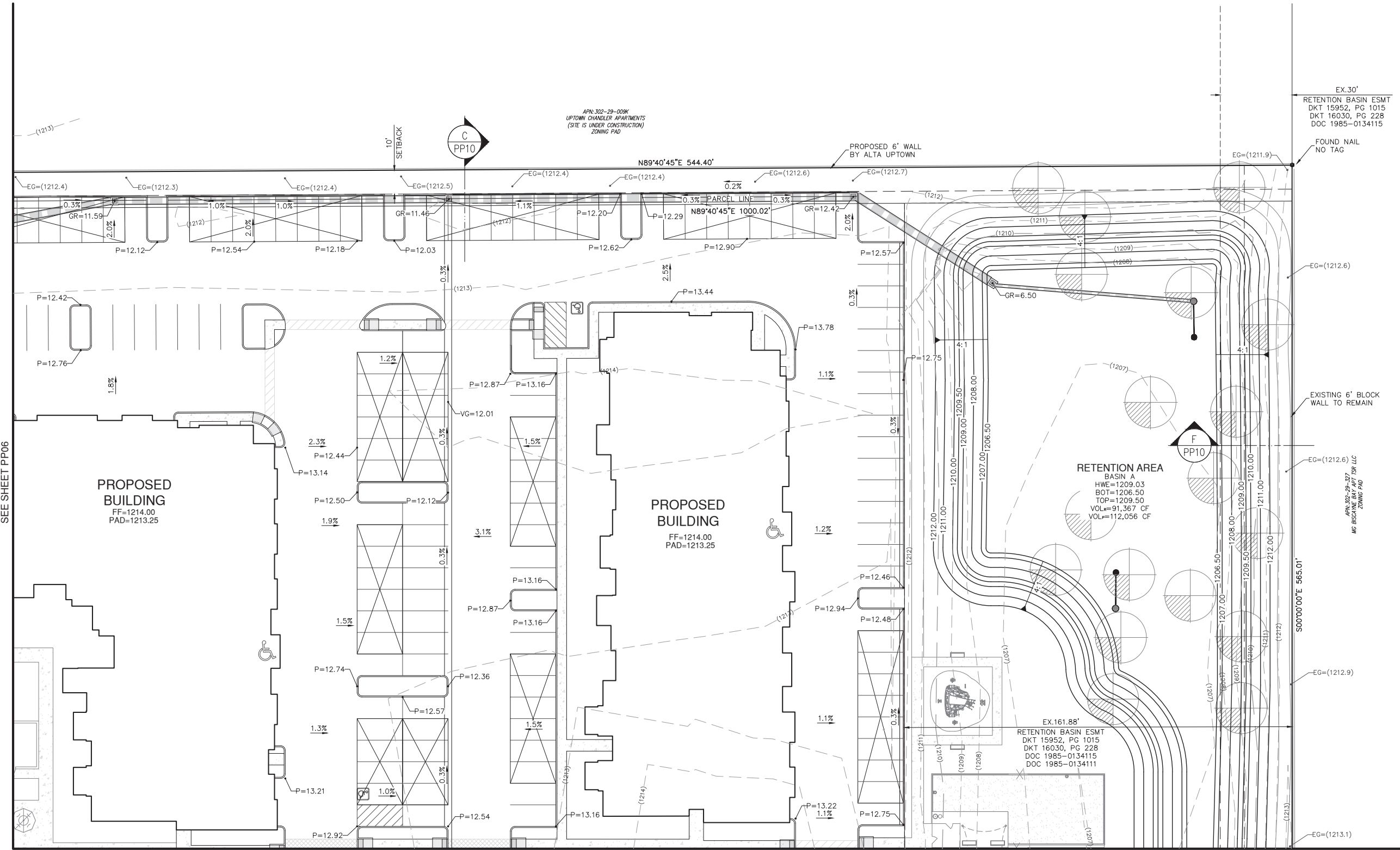


PLAN STATUS

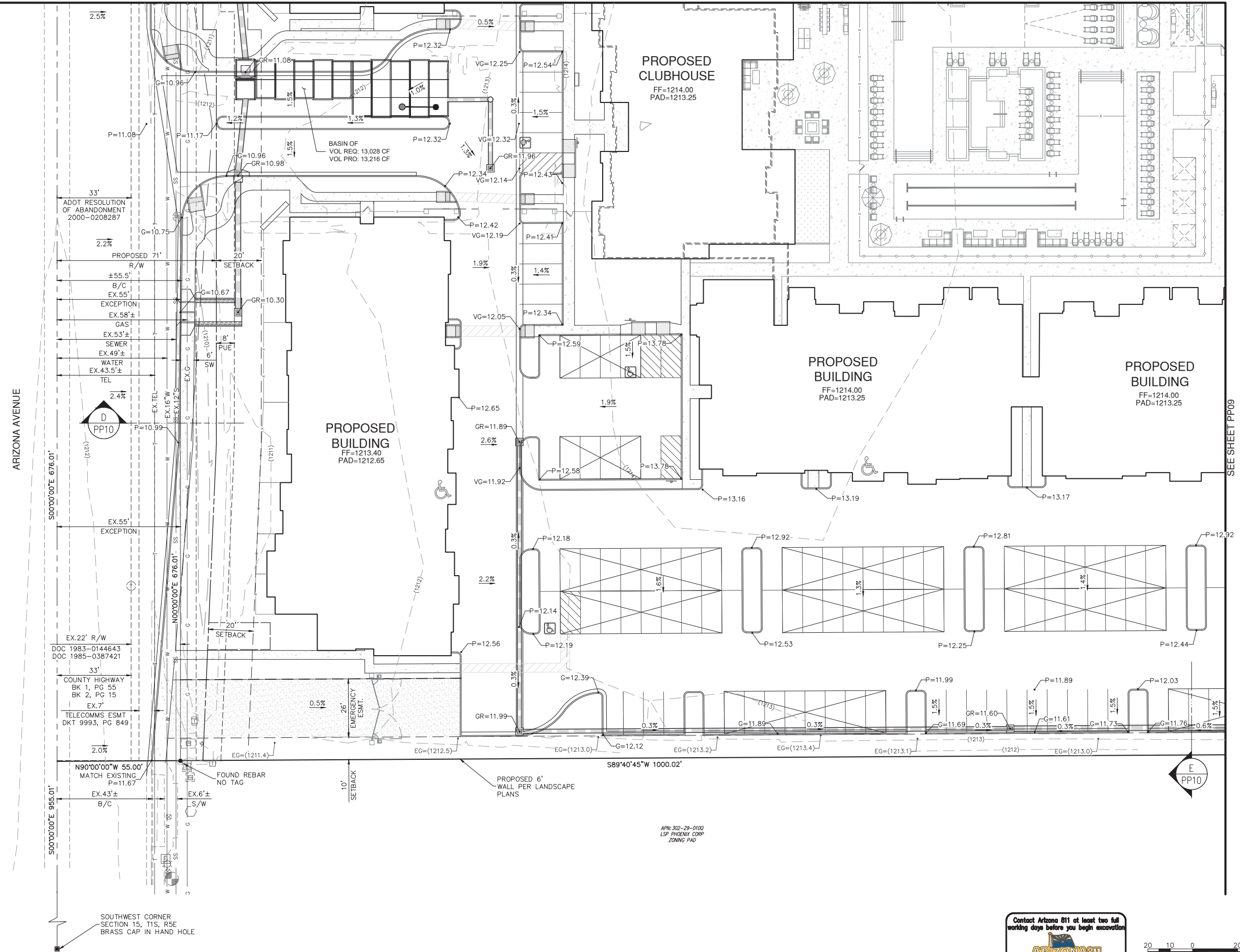
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JDG	AAG NTL
DESIGN	DRAWN CHKD
SCALE	H: 1"=20'
	V: NONE
JOB No.	051622
DATE :	08/01/2024

PP06
 SHEET 06 OF 14



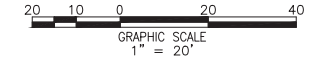
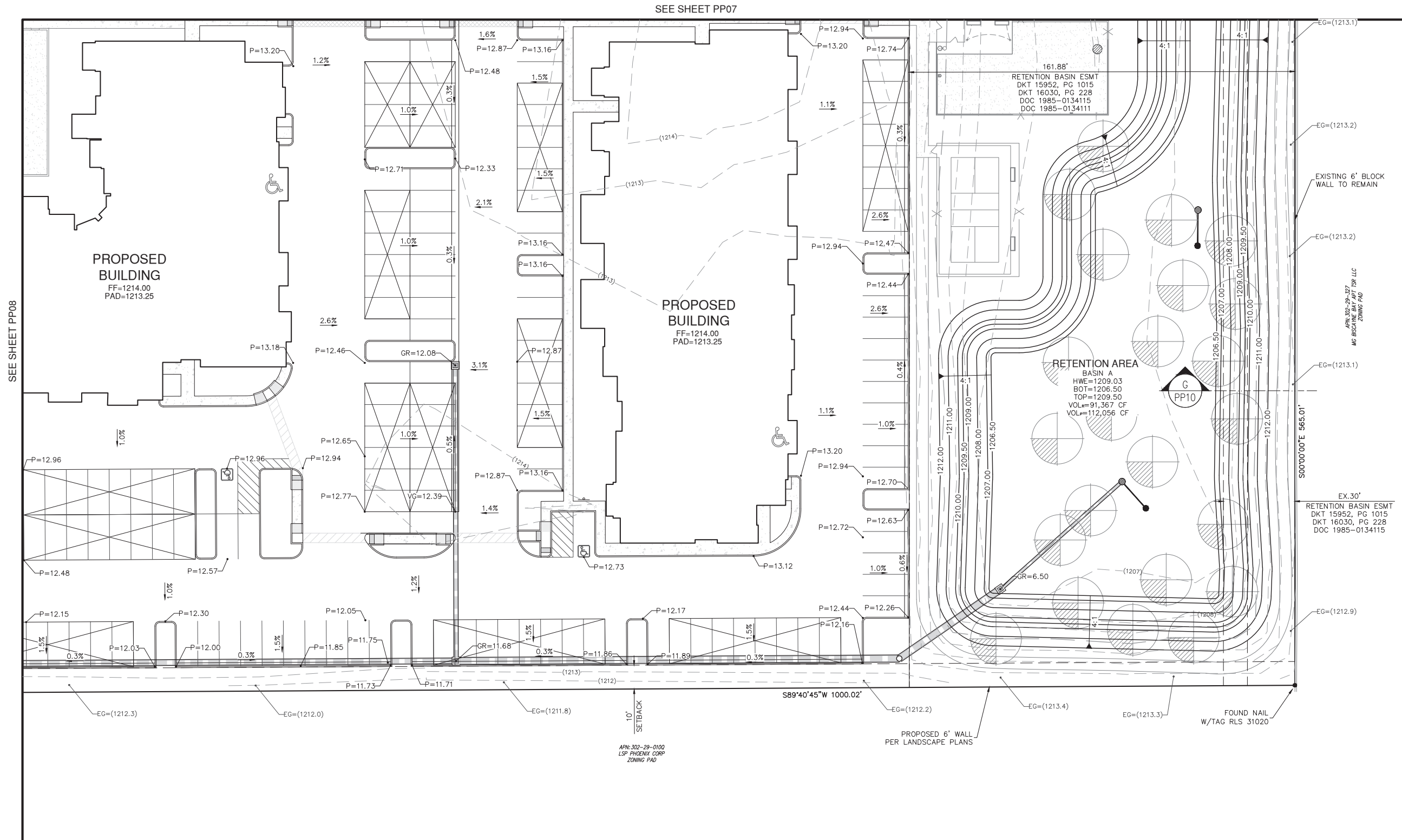


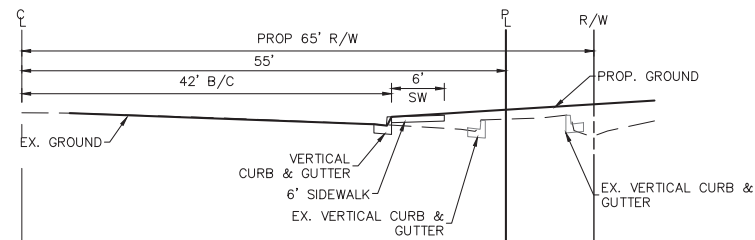
SEE SHEET PP06



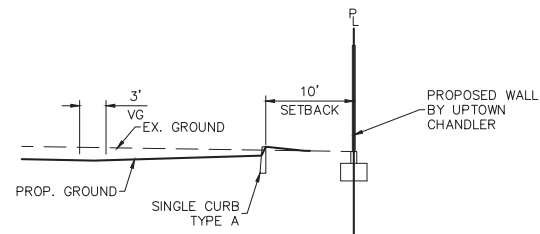


DATE	DESCRIPTION
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DESIGN	DRAWN CHKD
SCALE	H: 1"=20'
	V: NONE
JOB No.	051622
DATE :	08/01/2024

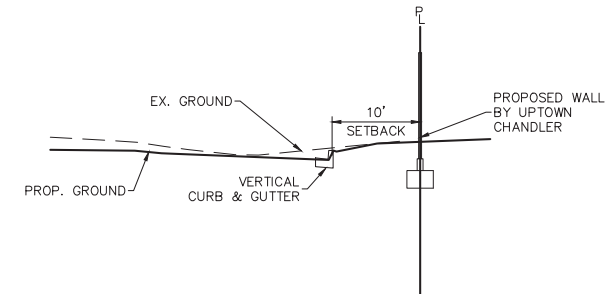




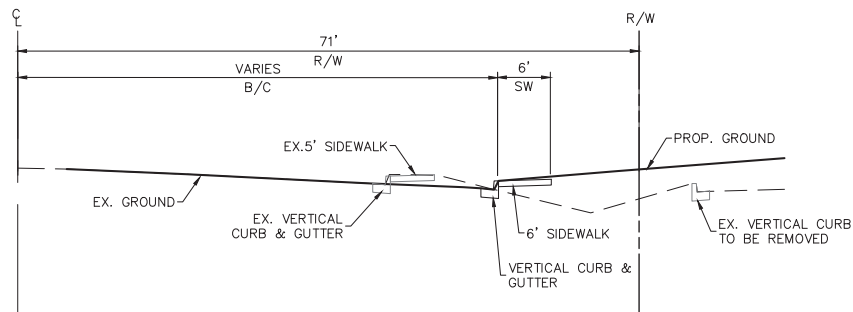
SECTION A
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



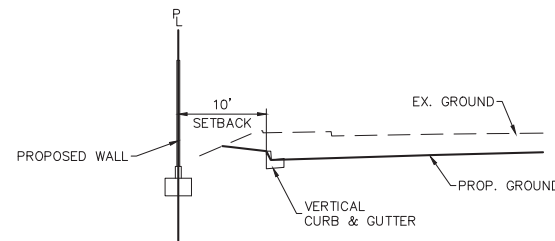
SECTION B
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VERTICAL SCALE: 1" = 5'



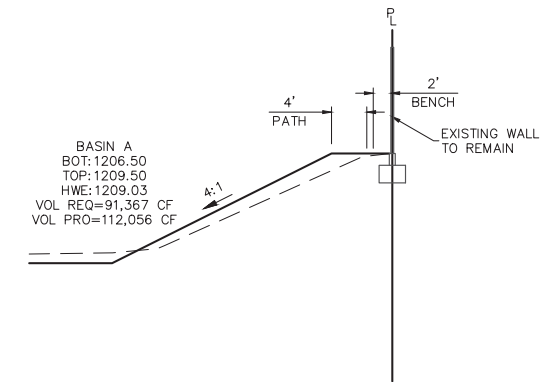
SECTION C
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



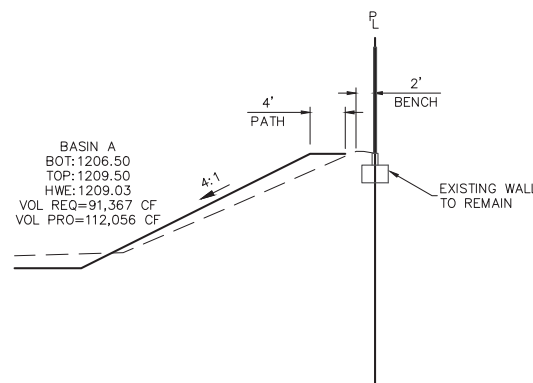
SECTION D
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



SECTION E
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



SECTION F
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



SECTION G
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'

Bowman

Bowman Consulting Group Ltd
2420 S. Power Road, Ste 105
Mesa, Arizona 85209
Phone: (480) 628-8830
www.bowmanfg.com
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PRELIMINARY GRADING SECTIONS
TOLL BROTHERS UPTOWN
CHANDLER, ARIZONA
MARICOPA COUNTY

051622
PROJECT NUMBER



PLAN STATUS

DATE	DESCRIPTION
JDG	AAG NTL
DESIGN	DRAWN CHKD
SCALE	H: 1"=20' V: NONE

JOB No. 051622
DATE : 08/01/2024



PP10
SHEET 10 OF 14

LEGAL DESCRIPTION

ALL THAT CERTAIN REAL PROPERTY IN THE COUNTY OF MARICOPA, STATE OF ARIZONA, DESCRIBED AS FOLLOWS:

PARCEL NO. 1:
THE SOUTH 550 FEET OF THE NORTH 1100 FEET OF THE SOUTH 2055 FEET OF THE WEST 1055 FEET OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA;

EXCEPT THE WEST 33 FEET; AND

EXCEPT THE EAST 161.88 FEET WHICH IS A WATER RETENTION BASIN UNDER AGREEMENT DATED MARCH 5, 1982 AND RE-RECORDED APRIL 12, 1982 IN DOCKET 15952, PAGE 1015, RECORDS OF MARICOPA COUNTY, ARIZONA, AND ALSO

EXCEPT THAT PROPERTY CONVEYED IN QUITCLAIM DEED RECORDED IN RECORDING NO. 85-319670, DESCRIBED AS FOLLOWS:

COMMENCING AT THE WEST QUARTER CORNER OF SECTION 15;

THENCE NORTH 89 DEGREES 45 MINUTES 58 SECONDS EAST ALONG THE EAST-WEST MIDSECTION LINE, A DISTANCE OF 33.00 FEET TO THE POINT OF BEGINNING;

THENCE SOUTH PARALLEL TO AND 33.00 FEET EAST OF THE WEST SECTION LINE OF SAID SECTION 15, A DISTANCE OF 1699.08 FEET;

THENCE NORTH 89 DEGREE 37 MINUTES 20 SECONDS EAST, A DISTANCE OF 22.00 FEET;

THENCE NORTH, A DISTANCE OF 1699.02 FEET;

THENCE SOUTH 89 DEGREES 45 MINUTES 58 SECONDS WEST, A DISTANCE OF 22.00 FEET ALONG THE EASTWEST MIDSECTION LINE TO THE POINT OF BEGINNING.

PARCEL NO. 2:
THE EAST 161.88 FEET OF THE SOUTH 550.00 FEET OF THE NORTH 1100 FEET OF THE SOUTH 2055 FEET OF THE WEST 1055 FEET OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA.

PARCEL NO. 3:
THE NORTH 160.67 FEET OF THE SOUTH 1665.67 FEET OF THE EAST 1022 FEET OF THE WEST 1055.00 FEET OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA;

EXCEPT THAT PROPERTY CONVEYED IN QUITCLAIM DEED RECORDED IN RECORDING NO. 85-319670, DESCRIBED AS FOLLOWS:

COMMENCING AT THE WEST QUARTER CORNER OF SECTION 15;

THENCE NORTH 89 DEGREES 45 MINUTES 58 SECONDS EAST ALONG THE EAST-WEST MIDSECTION LINE, A DISTANCE OF 33.00 FEET TO THE POINT OF BEGINNING;

THENCE SOUTH PARALLEL TO AND 33.00 FEET EAST OF THE WEST SECTION LINE OF SAID SECTION 15, A DISTANCE OF 1699.08 FEET;

THENCE NORTH 89 DEGREE 37 MINUTES 20 SECONDS EAST, A DISTANCE OF 22.00 FEET;

THENCE NORTH, A DISTANCE OF 1699.02 FEET;

THENCE SOUTH 89 DEGREES 45 MINUTES 58 SECONDS WEST, A DISTANCE OF 22.00 FEET ALONG THE EASTWEST MIDSECTION LINE TO THE POINT OF BEGINNING;

EXCEPT ANY PORTION LYING WITHIN THAT PORTION CONVEYED BY SPECIAL WARRANTY DEED RECORDED DECEMBER 23, 2022 IN RECORDING NO. 2022090494, BEING A PORTION OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID SECTION 15, BEING MARKED BY A 1/2" REBAR IN HAND-HOLE, FROM WHICH THE WEST QUARTER CORNER OF SAID SECTION 15, BEING MARKED BY A 2" ARIZONA DEPARTMENT OF TRANSPORTATION ALUMINUM CAP FLUSH BEARS NORTH 0 DEGREES 06 MINUTES 52 SECONDS WEST, 2854.15 FEET;

THENCE ALONG THE WEST LINE OF SAID SOUTHWEST QUARTER, NORTH 0 DEGREES 06 MINUTES 52 SECONDS WEST, 1631.03 FEET;

THENCE ALONG A LINE 1631.00 FEET NORTH OF AND PARALLEL WITH THE SOUTH LINE OF SAID SOUTHWEST QUARTER, NORTH 89 DEGREES 33 MINUTES 47 SECONDS EAST, 55.00 FEET TO THE POINT OF BEGINNING;

THENCE ALONG THE EAST RIGHT-OF-WAY OF ARIZONA AVENUE AND ALSO ALONG A LINE 65.00 FEET EAST OF AND PARALLEL WITH SAID WEST LINE, NORTH 0 DEGREES 06 MINUTES 52 SECONDS WEST, 391.01 FEET;

THENCE ALONG THE SOUTH RIGHT-OF-WAY OF CORPORATE PLACE AND ALSO ALONG A LINE 2022.00 FEET NORTH OF AND PARALLEL WITH THE SOUTH LINE OF SAID SOUTHWEST QUARTER, NORTH 89 DEGREES 33 MINUTES 47 SECONDS EAST, 1000.02 FEET;

THENCE ALONG THE WEST BOUNDARY OF THAT FINAL PLAT OF WESTTECH CORPORATE CENTER PHASE 1 RECORDED IN BOOK 574 OF MAPS, PAGE 24 OF MARICOPA COUNTY RECORDS AND ALSO ALONG THE WEST BOUNDARY OF BISCAYNE BAY APARTMENTS RECORDED IN BOOK 513 OF MAPS, PAGE 23 OF MARICOPA COUNTY RECORDS AND ALSO ALONG A LINE 1055.00 FEET EAST OF AND PARALLEL WITH THE WEST LINE OF SAID SOUTHWEST QUARTER, SOUTH 0 DEGREES 06 MINUTES 52 SECONDS EAST, 502.01 FEET;

THENCE ALONG A LINE 1520.00 FEET NORTH OF AND PARALLEL WITH THE SOUTH LINE OF SAID SOUTHWEST QUARTER, SOUTH 89 DEGREES 33 MINUTES 47 SECONDS WEST, 544.40 FEET;

THENCE NORTH 0 DEGREES 26 MINUTES 13 SECONDS WEST, 111.00 FEET;

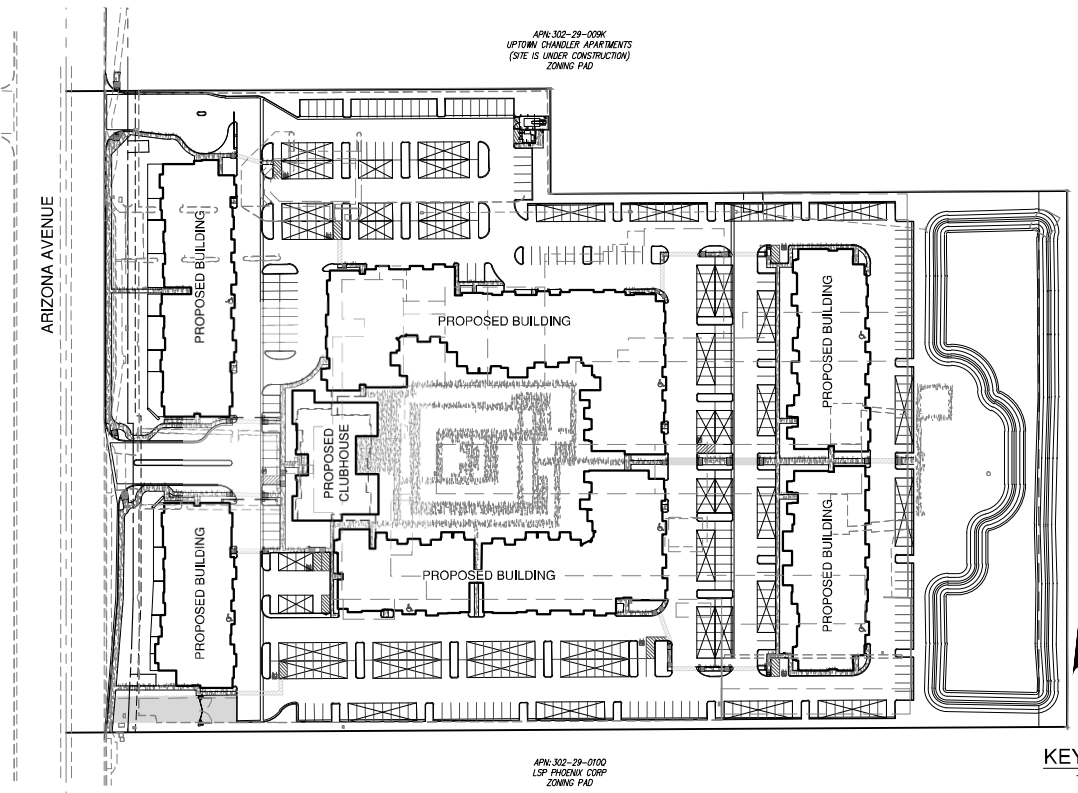
THENCE ALONG A LINE 1631.00 FEET NORTH OF AND PARALLEL WITH THE SOUTH LINE OF SAID SOUTHWEST QUARTER, SOUTH 89 DEGREES 33 MINUTES 47 SECONDS WEST, 455.00 FEET TO THE POINT OF BEGINNING.

LEGEND

●	BRASS CAP FLUSH	— — — — —	STORM DRAIN PIPE
○	MONUMENT AS NOTED	— — — — —	DRYWELL
□	BRASS CAP IN HAND HOLE	— — — — —	EXISTING DRYWELL
○	SEWER MANHOLE	— — — — —	BOUNDARY LINE
○	SEWER CLEANOUT	— — — — —	SECTION LINE
●	SEWER FLOW DIRECTION	— — — — —	PARCEL LINE
●	FIRE HYDRANT	— — — — —	EASEMENT LINE
●	WATER VALVE	R/W	RIGHT OF WAY
●	BACK FLOW PREVENTER	PUE	PUBLIC UTILITY EASEMENT
□	WATER METER VAULT	SWE	SIDEWALK EASEMENT
⊕	EXISTING GAS METER	— — — — —	EXISTING CONTOUR & ELEVATION
⊕	EXISTING SANITARY SEWER MANHOLE	— — — — —	EXISTING WATER
⊕	EXISTING SANITARY SEWER CLEANOUT	— — — — —	EXISTING SEWER
⊕	EXISTING ELECTRICAL PULL BOX	— — — — —	EXISTING GAS
⊕	EXISTING TELEPHONE JUNCTION BOX	— — — — —	PROPOSED SEWER
⊕	EXISTING TELEPHONE MANHOLE	— — — — —	PROPOSED WATER
☆	EXISTING LIGHT	— — — — —	PROPOSED STREET FLOW
⊕	EXISTING TRANSFORMER	①	PROPOSED PARKING COUNT
⊕	EXISTING WATER METER		
⊕	EXISTING FIRE DEPARTMENT CONNECTION		
⊕	EXISTING WATER BACK FLOW PREVENTER		

TOLL BROTHERS UPTOWN PRELIMINARY PLAT CHANDLER, ARIZONA

LOCATED IN THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST, OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA.



CITY OF CHANDLER SITE PLAN NOTES

- ALL SITE IMPROVEMENTS, INCLUDING LANDSCAPE AND SITE CLEANUP, MUST BE COMPLETED PRIOR TO CERTIFICATE OF OCCUPANCY FOR ANY BUILDING WITHIN A PHASE.
- ALL ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY PARAPET WALLS EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT.
- SOLID MASONRY WALLS AND GATES EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT SHALL SCREEN ALL GROUND MOUNTED MECHANICAL EQUIPMENT.
- ALL EXTERNAL LIGHTING SHALL BE LOCATED AND DESIGN TO PREVENT RAYS FROM BEING DIRECTED OFF OF THE PROPERTY UPON WHICH THE LIGHTING IS LOCATED.
- THE FIRE DEPARTMENT DOUBLE CHECK ASSEMBLY SHALL BE PAINTED TO MATCH THE ADJACENT WALL COLOR.
- THERE SHALL BE NO OBSTRUCTION OF SITE SIGNAGE BY LANDSCAPE PLANT MATERIAL, AND THAT SUCH MUST BE RELOCATED/CORRECTED BEFORE THE FIELD INSPECTION WILL ACCEPT/PASS THE SIGN IN THE FIELD OR ISSUE A CERTIFICATE OF OCCUPANCY FOR A PROJECT.
- ALL TRANSFORMER BOXES, METER PANELS AND ELECTRIC EQUIPMENT, BACKFLOW DEVICES OR ANY OTHER UTILITY EQUIPMENT NOT ABLE TO BE OR REQUIRED TO BE SCREENED BY LANDSCAPING OR WALLS, SHALL BE PAINTED TO MATCH THE BUILDING COLOR.
- ALL WALLS OVER 7' IN HEIGHT, SITE LIGHTING, SIGNAGE, RAMADAS AND SHADE STRUCTURES REQUIRE SEPARATE SUBMITTAL AND PERMITS

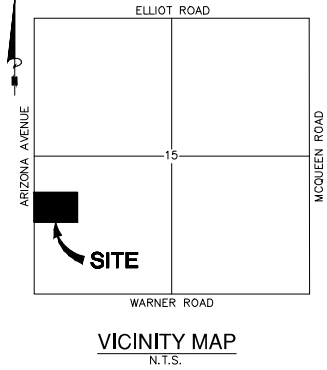
UTILITY SERVICES

GAS	SOUTHWEST GAS
ELECTRIC	SALT RIVER PROJECT
TELEPHONE	CENTURY LINK/COX COMMUNICATIONS
CABLE	CENTURY LINK/COX COMMUNICATIONS
WATER	CITY OF CHANDLER
SEWER	CITY OF CHANDLER

Sheet List Table	
Sheet Number	Sheet Title
01	COVERSHEET
02	PRELIMINARY PLAT

ENGINEER
BOWMAN CONSULTING
2420 S. POWER ROAD
SUITE 105
MESA, ARIZONA 85209
CONTACT: NATHAN LARSON
PH: 480.629.9830
EMAIL: nlarson@bowmancg.com

DEVELOPER
TOLL APARTMENTS, LP
8787 E. VIA DE VENTURA
SUITE 270
SCOTTSDALE, ARIZONA 85258
CONTACT: ANNA PICACHE
PH: 203.598.1807
EMAIL: apicache@tollbrothers.com



BENCHMARK
SECTION 10, T1S, R5E, 2" BRASS CAP IN BASE OF ELECTRIC TRANSMISSION TOWER #115 BETWEEN GUADALUPE RD AND ELLIOT RD, EAST OF ARIZONA AVE, 90' SOUTH OF CANAL. ELEVATION: 1211.75 DATUM: NAVD88

BASIS OF BEARINGS
NORTH 90°00'00" EAST ALONG THE WEST LINE OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 SOUTH, RANGE 5 EAST, OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA

FLOOD ZONE CERTIFICATION

FLOOD INSURANCE MAP NUMBER 04013C2729M
EFFECTIVE ON 11.4.2015
ZONE "X": AREAS OF 0.2% ANNUAL CHANCE FLOOD; AREAS OF 1% ANNUAL CHANCE OF FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE, AND AREAS PROTECTED BY LEVEES FROM 1% ANNUAL CHANCE OF FLOOD.
FLOOD ZONE DESIGNATION PROVIDED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

SITE DATA

ZONING: EXISTING: C-3
PROPOSED: PAD
TOTAL NUMBER OF UNITS: 420 UNITS
GROSS AREA: 14.98 ac
NET AREA: 13.94 ac
NET DENSITY: 30.13 du/ac
RETENTION REQUIRED: 105,387 of 2.42 ac-ft
RETENTION PROVIDED: 126,726 of 2.91 ac-ft
LATITUDE: N33°20'18.73"
LONGITUDE: W111°50'23.78"

NOTE

THE IMPROVEMENTS SHOW ON THIS PLAT WILL NOT BE FULLY APPROVED BY THE CITY AND THE CERTIFICATE OF OCCUPANCY OR ACCEPTANCE WILL NOT BE ISSUED UNTIL THE OVERHEAD UTILITY LINE UNDERGROUNDING REQUIREMENT HAS BEEN SATISFIED



Bowman Consulting Group, LLC
2420 S. Power Road, Ste 105
Mesa, Arizona 85209
Phone: (480) 629-9830
www.bowmancg.com
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COVERSHEET
TOLL BROTHERS UPTOWN
MARICOPA COUNTY
CHANDLER, ARIZONA

051622
PROJECT NUMBER



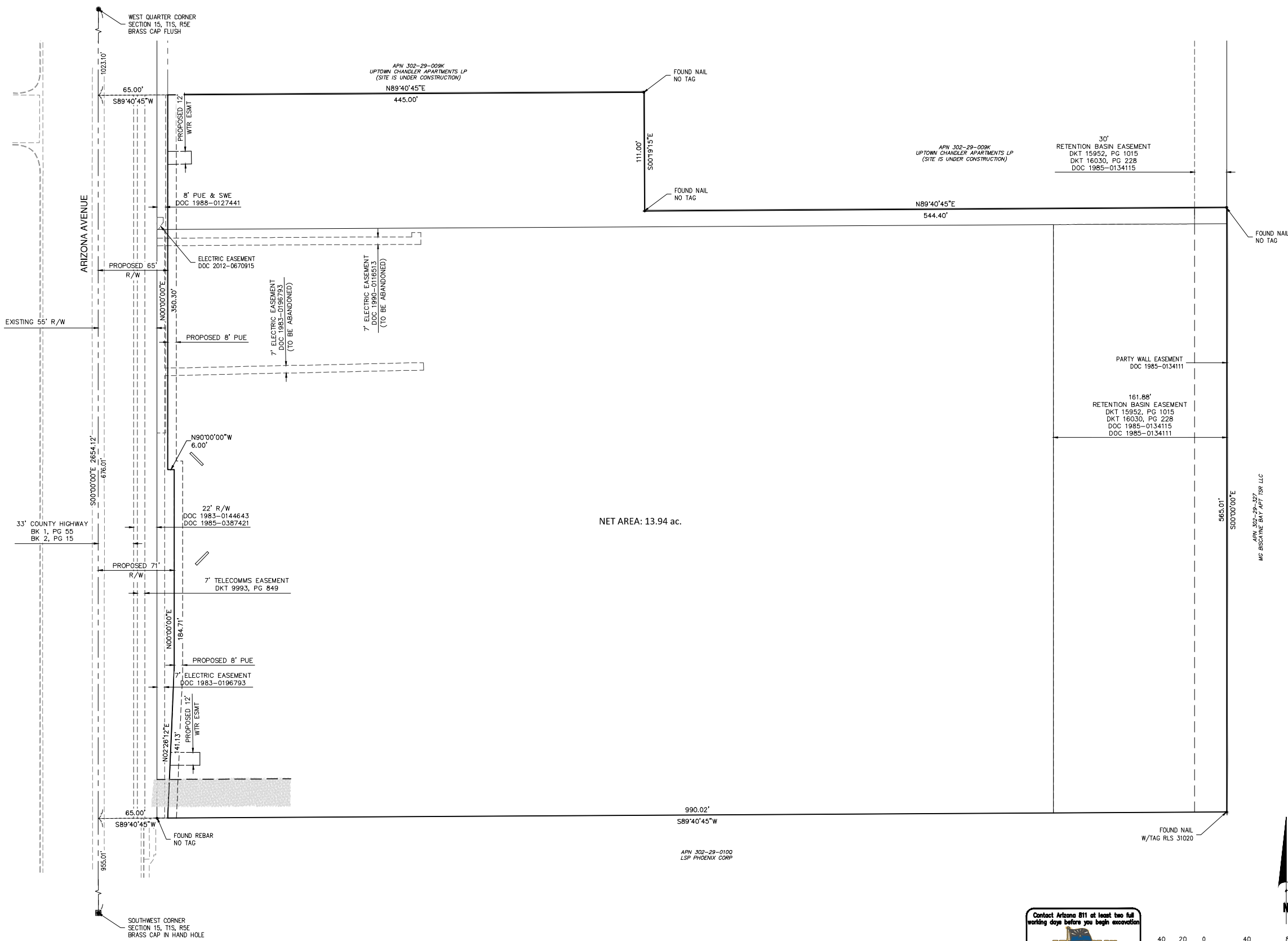
PLAN STATUS

DATE	DESCRIPTION
JDG DESIGN	AAG NTL DRAWN CHKD
SCALE	H: 1" = 80' V: NONE
JOB No.	051622
DATE	09/03/2024

PP01
SHEET 01 OF 14



C.O.C. LOG No. PLT24-0001



NET AREA: 13.94 ac.

Bowman

Bowman Consulting Group Ltd
 4020 S. Power Road, Suite 105
 Mesa, Arizona 85205
 Phone: (480) 628-8830
 www.bowmanllc.com
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PRELIMINARY PLAT
TOLL BROTHERS UPTOWN
 CHANDLER, ARIZONA
 MARICOPA COUNTY

051622
 PROJECT NUMBER

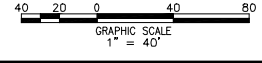


PLAN STATUS

DATE	DESCRIPTION		
JDG DESIGN	AAG DRAWN	NTL	CHKD
SCALE: H: 1"=40'	V: NONE		
JOB No. 051622			
DATE: 08/03/2024			

PP02
 SHEET 02 OF 02

C.O.C.: LOG No. PLT24-0001



City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Casey Jacobsen

Business Name ARENA 23

Date 8/22/24



Arizona's first
INDOOR SPORTS
AMUSEMENT PARK



City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature Wei Xu

Name WEI XU

Business Name Annie's Foot Massage

Date 03/28/2024



City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Mason A. Abbott

Business Name Uptown Jungle Chandler

Date 8-8-2024





07.29.2024

City of Chandler, Planning and Zoning Department

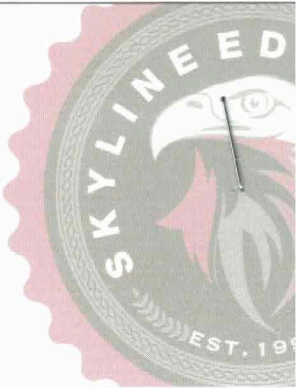
Cc: Withey Morris Bough

Re: Rezoning Case No. PLH24-0001



Swen Anderson, MRED
President
Office (480) 779 - 2010 ext. 2120
Mobile (480) 205 - 8009
sanderson@skylineschools.com
www.skylineschools.com

2020 N. Arizona Ave, Suite 109
Chandler, AZ 85225



Dear Planning and Zoning Department,

I am writing to express our strong support for the rezoning of the Earnhardt Auto Centers property located on the southeast corner of Arizona Ave and Warner, which has been sold to Toll Brothers for the purpose of developing a multi-family apartment building. As the owner of AZ Compass Prep School, a K-12 public charter institution located near the proposed development, we believe this rezoning initiative will bring numerous benefits to our community.

The development of a multi-family apartment building aligns with the growing demand for diverse housing options in our area. This project will provide much-needed housing for families, professionals, and individuals, contributing to the overall vitality and growth of our community. Additionally, the presence of new residents within the Chandler Uptown area can positively impact local businesses and services, fostering a vibrant and thriving neighborhood.

The transformation of the former auto center into a modern residential complex will not only improve the visual landscape of the Chandler Uptown area, but also promote a sense of community and connectivity among residents. As an educational institution, we are particularly excited about the potential benefits for our students and their families. The proximity of new housing options can make our school more accessible to a wider range of families and may create opportunities for collaboration and partnerships between our school and the new residential community, further enriching the educational experience for our students.

We commend Earnhardt Auto Centers for their foresight in facilitating this positive change and Toll Brothers for their commitment to developing high-quality housing. We wholeheartedly support the rezoning of this property and believe that it will have a lasting and positive impact on our community. Thank you for considering our perspective. We look forward to the positive transformation that this development will bring to our neighborhood.

Sincerely,
Swen Anderson
President, AZ Compass Prep School
sanderson@skylineschools.com
480.205.8009

AZ Compass Prep School
2020 N. Arizona Ave. Suite 206
Chandler, AZ. 85225
www.azcompassprep.com

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature Monique Pool

Name Monique Pool

Business Name Arby's

Date 3.28.24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Blake West

Business Name Barro's Pizza

Date 7/17/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name JM Mi Jason Milanowski

Business Name Big 5 Sporting Goods

Date 7/17/24



2000 N. Arizona Avenue
Chandler, AZ 85225
(480) 821-9226

Visit us online:

www.big5sportinggoods.com

 facebook.com/big5sportinggoods

 twitter.com/big5since55

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Brett Bates

Business Name Brewster's ICE cream chandler 398

Date 8/22/2024

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Cinnamon Wells

Business Name DJs Smoke Shop

Date 7/17/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Alicia Jaramillo

Business Name Dunkin'

Date 8/22/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature  _____

Name Jose Gonzalez

Business Name Jack in the box

Date 03/28/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature MARISSA SOTO

Name MARISSA SOTO

Business Name JIMMY JOHNS

Date 3/28/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Mer Stoff

Business Name La bella Paw Spa

Date 8/8/24



City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Michael Downs 

Business Name Day Auto Supply

Date 8/8/24



AUTO & TRUCK PARTS

Mike Downs


Day Auto Supply, Inc
2071 N Arizona Ave Suite #119
Chandler, AZ 85225

Phone: 480-963-6402
Fax: 480-917-3901

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Chris DeNeice 

Business Name PGA TOUR SUPERSTORE

Date 8/22/24



**PGA TOUR
SUPERSTORE**

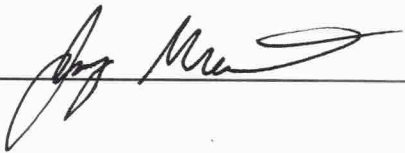
Chris DeNeice
General Manager

2031 N Arizona Ave | Chandler, AZ 85225
O: 480-214-4370 | C: 303-332-5295
cdeneice@pgatss.com

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature 

Name Jorge Moreno

Business Name Royal Fadelz Barbershop # 2 LLC.

Date 03/28/24



City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Larry

Business Name satelite

Date 7/17/24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Signature Erika Herrera

Name Erika Herrera

Business Name Subway

Date 3.28.24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name Julie Bohner manager

Business Name Tom Ryan's

Date 7-17-24

City of Chandler Planning & Zoning Commission and City Council,

As a Chandler business, we want to express our support for the Toll Brothers rezoning case PLH24-0001, north of the Arizona Avenue and Warner intersection. We have reviewed the site plan and images and are excited to bring more Chandler residents to the unused Earnhardt car dealership that is no longer adding value to our community or surrounding businesses. Growth in Maricopa County has exploded, and more quality housing is needed to support our economy and businesses. Toll Brothers has an excellent reputation as a quality builder in Arizona. Thank you for joining us in support of this project and thank you for your service to this community.

Sincerely,

Name ASIYAH JAFARY

Business Name TOP CROWN SMOKE & VAPE

Date 08/08/2024



Wednesday, September 18, 2024

City of Chandler
175 South Arizona Avenue
Chandler, Arizona 85225

Subject: Toll Brothers Uptown Multi-Family Housing Development Rezoning Case PLH24-0001

Dear City Council & Planning and Zoning Commission,

The Chandler Chamber of Commerce would like to extend our support for the Toll Brothers Multi-Family Housing Development rezoning case PLH24-0001 to create additional housing on Warner and Arizona Avenue. The Chandler Chamber of Commerce represents over 1,700 member businesses, representing over 175,000 employees, that stand to benefit from this development. Business and industry continues to boom in Chandler and more housing is needed to sustain this growth.

The proposed multifamily development has strong support from surrounding businesses that recognize its potential to provide much needed workforce housing. By increasing density, it will help accommodate the growing demand for affordable living options while supporting local economic growth. This project will complement the existing community by offering additional housing that aligns with the City's long term housing development goals. Accessible housing benefits both our community by enabling more people who work here to also reside here.

This project aims to expand housing opportunities, allowing more families and businesses to thrive in Chandler. Toll Brothers has an outstanding reputation as a Builder, and community partner. We urge you to support the Toll Brothers Multi-Family Housing Development rezoning case PLH24-0001.

Sincerely,

A handwritten signature in black ink that reads 'Terri Kimble'.

Terri Kimble
President/CEO
Chandler Chamber of Commerce

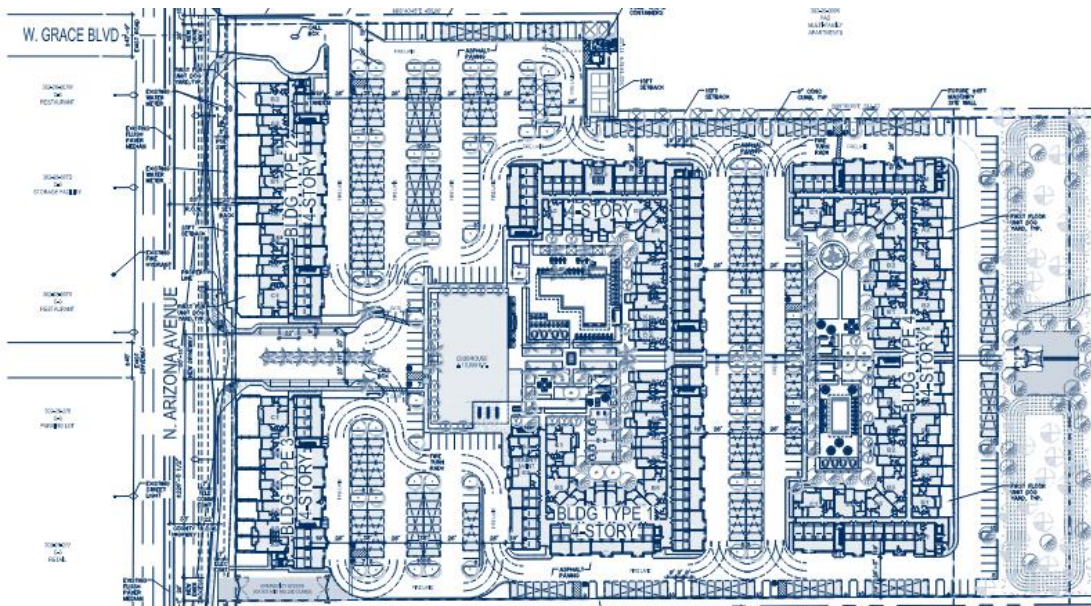
A handwritten signature in black ink that reads 'Carly Wakefield'.

Carly Wakefield
Vice President of Workforce Development &
Government Relations
Chandler Chamber of Commerce

Toll Brothers Uptown

Traffic Impact Study

Category I



Prepared for:



Toll Brothers Apartment Living
8767 E Via De Ventura, Suite 390
Scottsdale, AZ 85258



Shelly Sorensen

Prepared by:



Lokahi, LLC
10555 N. 114th Street, Suite 105
Scottsdale, AZ 85259

Project Number: 23.5569
January 26, 2024



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1. INTRODUCTION AND EXECUTIVE SUMMARY

1.1. PURPOSE OF REPORT AND STUDY OBJECTIVES

Lōkahi, LLC (Lōkahi) was retained by Toll Brothers Apartment Living to complete a Traffic Impact Study (TIS) for the proposed Toll Brothers Uptown residential development. The development is generally located on the southeast corner of Arizona Avenue and Grace Boulevard in the City of Chandler, Arizona.

The proposed development will be comprised of 423 dwelling units. Of which, there will be 40 studio units, 208 one-bedroom units, 137 two-bedroom units, and 38 three-bedroom units.

The objective of this Traffic Impact Study is to analyze the traffic related impacts of the proposed development to the adjacent roadway network. See **Figure 1** for the vicinity map.

This Traffic Impact Analysis includes:

- Level of service analysis of existing conditions for the weekday AM and PM peak hours
- Trip generation for the proposed development
- Level of service analysis for the opening year (2027) weekday AM and PM peak hours
- Level of service analysis for 5 years after the opening year (2032) weekday AM and PM peak hours

The following are the intersections included in this study:

- Arizona Avenue and Grace Boulevard/Driveway A (1) – existing (with future site access)
- Arizona Avenue and Driveway B (2) – existing (with future site access)
- Arizona Avenue and Driveway C (3) – proposed (emergency access only)
- Arizona Avenue and Driveway D (4) – existing
- Arizona Avenue and Driveway E (5) – existing



1.2. EXECUTIVE SUMMARY

This report presents the analyses and the results of a Traffic Impact Study prepared for the proposed Toll Brothers Uptown residential development. The proposed development is generally located on the southeast corner of Arizona Avenue and Grace Boulevard in the City of Chandler, Arizona.

The proposed development will be comprised of 423 dwelling units. Of which, there will be 40 studio units, 208 one-bedroom units, 137 two-bedroom units, and 38 three-bedroom units.

The proposed Toll Brothers Uptown residential development is anticipated to start leasing the first units in 2026. As a conservative approach it was assumed the full build out would occur in the year 2027.

Existing Capacity Analysis

The AM and PM peak hour existing conditions capacity analyses were completed for the existing study intersections. The results of the capacity analyses reveal that all movements operate at a LOS C or better.

Trip Generation

The proposed Toll Brothers Uptown development is anticipated to generate a total of 1,971 daily trips, with 175 trips occurring in the AM peak hour and 165 trips in the PM peak hour.

Trip Generation

Land Use	ITE Code	Qty	Unit	Weekday	AM Peak Hour			PM Peak Hour		
				Total	Total	In	Out	Total	In	Out
Multifamily Housing (Mid-Rise)	221	423	Dwelling Units	1,971	175	40	135	165	104	61

Trip Generation Comparison

A trip generation comparison was completed between the proposed Toll Brothers Uptown development and the existing automobile sales dealership. The proposed development generates 22 fewer weekday trips, with 42 more trips during the AM peak hour and 8 fewer trips during the PM peak hour, when compared to the existing automobile sales dealership.



Trip Generation Comparison

Land Use	ITE Code	Qty	Unit	Weekday	AM Peak Hour			PM Peak Hour		
				Total	Total	In	Out	Total	In	Out
Existing Development	840	71.6	1000 SF GFA	1,993	133	97	36	173	69	104
Proposed Development	221	423	Dwelling Units	1,971	175	40	135	165	104	61
Difference				-22	42	-57	99	-8	35	-43

Future Conditions

Year 2027 (build out) and year 2032 (5 years after build out) analyses were completed without and with the build out of the proposed Toll Brothers Uptown. Utilizing MAG socioeconomic data for the study area, existing traffic volumes were grown by an annual growth rate of 1.0% to determine the future traffic volumes. Additionally, known surrounding development(s) were included in future traffic volumes.

Year 2027 – Opening Year

A capacity analysis was completed for both the AM and PM peak hours for the year 2027, without and with the build out of the proposed Toll Brothers Uptown development. The year 2027 build capacity analysis resulted in all movements operating at a LOS D or better, with the exception of:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Eastbound left PM peak hour operates at LOS E
- **Arizona Avenue and Driveway B (2)**
 - Westbound left AM peak hour operates at LOS E

Year 2032 – 5 Years After Opening Year

A capacity analysis was completed for both the AM and PM peak hours for the year 2032, without and with the build out of the proposed Toll Brothers Uptown development. The year 2032 build capacity analysis resulted in all movements operating at a LOS D or better, with the exception of:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Eastbound left PM peak hour operates at LOS E
- **Arizona Avenue and Driveway B (2)**
 - Westbound left AM peak hour operates at LOS F

Delays are typical during peak hours for minor-to-major turning movements as well as for movements at stop-controlled intersections. Drivers familiar with the area often choose to use alternate routes



during peak hours or drive at different times to avoid potential delay. Therefore, improvements to mitigate LOS E or F at the above intersections are not recommended as part of this study.

Recommendations

The following are the recommendations with the build out of the proposed Toll Brothers Uptown development:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Buildout of full-access point on the east leg of the intersection

- **Arizona Avenue and Driveway B (2)**
 - Buildout of full-access point on the east leg of the intersection

- **Arizona Avenue and Driveway C (3)**
 - Buildout of an emergency access only driveway.



2. PROPOSED DEVELOPMENT

The proposed development is generally located on the southeast corner of Arizona Avenue and Grace Boulevard in the City of Chandler, Arizona. See **Figure 1** for a vicinity map.

The proposed development will be comprised of 423 dwelling units. Of which, there will be 40 studio units, 208 one-bedroom units, 137 two-bedroom units, and 38 three-bedroom units.

There will be a total of three (3) access points provided for the proposed development:

Arizona Avenue and Grace Boulevard/Driveway A (1) will be a full-access point on the east side of the existing intersection. This driveway will provide full access, allowing all movements into and out of the site.

Arizona Avenue and Driveway B (2) will be a full-access point on the east side of the existing intersection. This driveway will provide full access, allowing all movements into and out of the site.

Arizona Avenue and Driveway C (3) will be an emergency access only along Arizona Avenue, approximately 270-feet south of Driveway B.

See **Figure 2** and **Appendix A** for the proposed site plan.

PROPOSED SITE

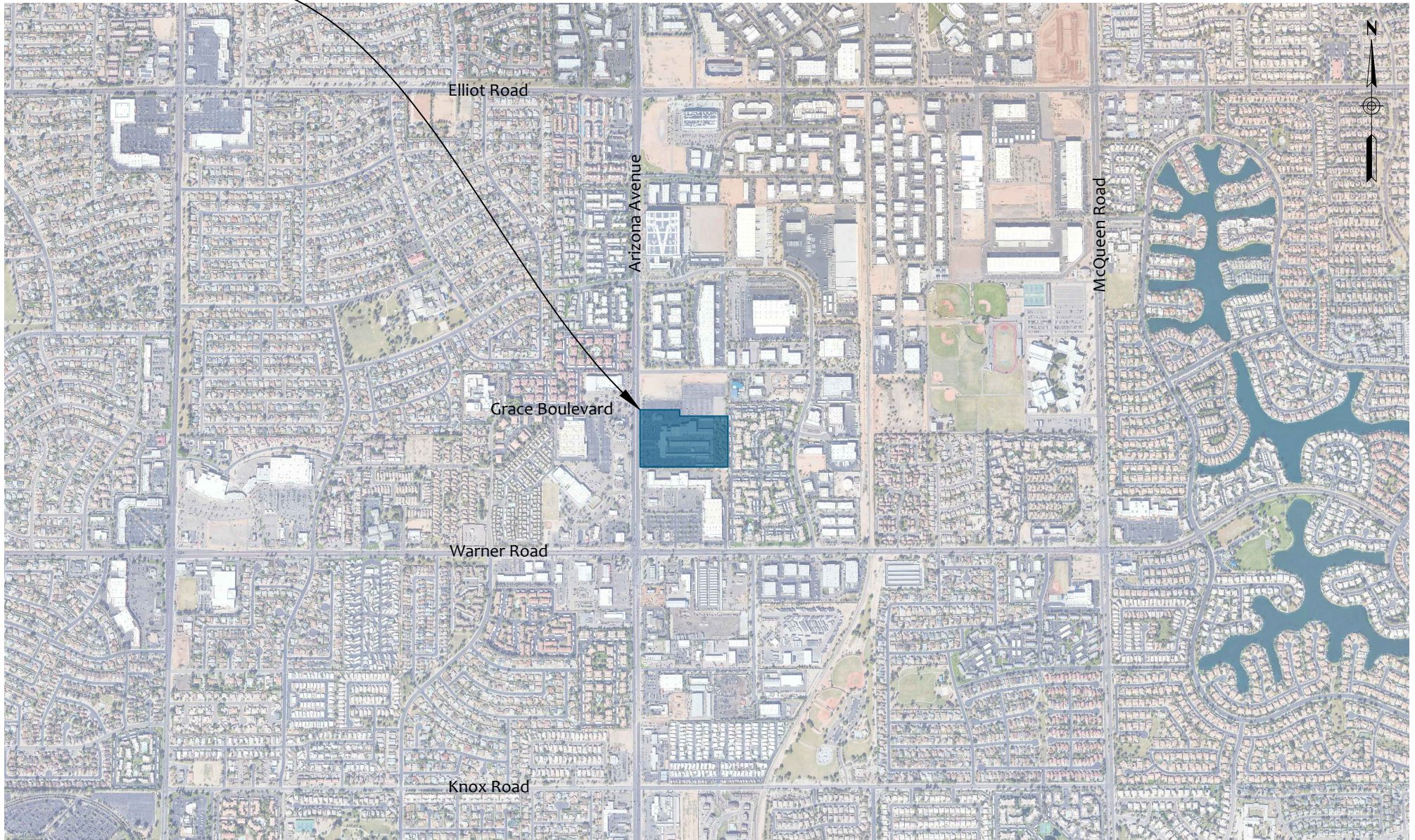


FIGURE 1 | VICINITY MAP

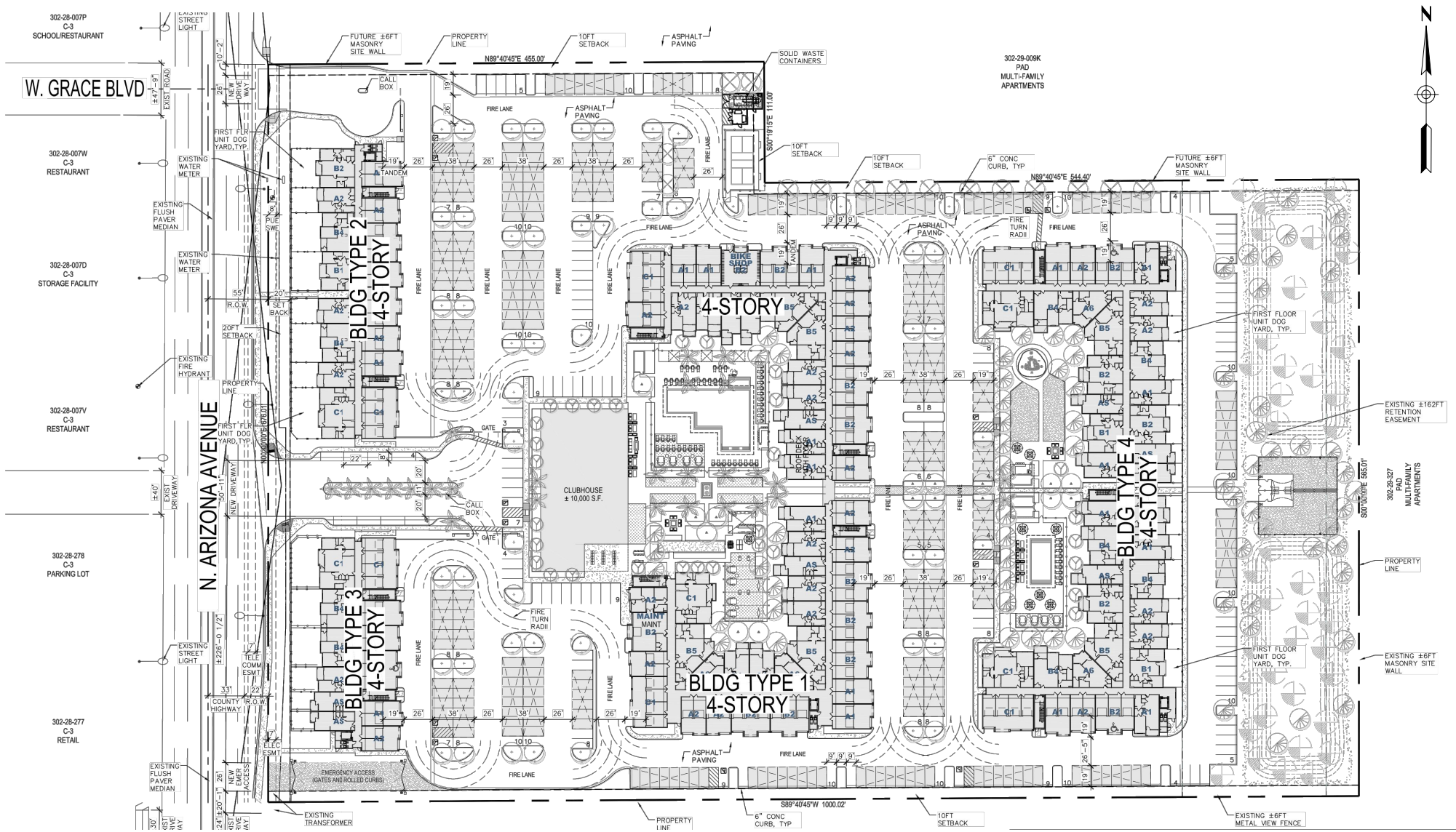


FIGURE 2 | SITE PLAN



3. AREA CONDITIONS

The proposed site is located within the City of Chandler, Arizona. **Sections 3.1** and **3.2** provide detailed descriptions of the study roadway segments and intersections. See **Figure 3** for the study area.

3.1. STUDY ROADWAY SEGMENTS

Arizona Avenue generally runs north-south and provides three (3) travel lanes for each direction of travel, with a center two-way left turn lane. There is a posted speed limit of 45 miles per hour (mph). Per the *City of Chandler's Transportation Master Plan Update*, dated January 2020, Arizona Avenue is classified as a major arterial. According to the City of Chandler 2023 Road Segment Traffic Volumes, Arizona Avenue has an Average Daily Traffic (ADT) of 31,700 vehicles per day (vpd), between Elliot Road and Warner Road.

Grace Boulevard generally runs east-west within the proposed study area and provides one (1) travel lane for each direction of travel. There is an unposted speed limit of 25 mph.

3.2. STUDY INTERSECTIONS

Arizona Avenue and Grace Boulevard (1) currently operates as a one-way stop-controlled T-intersection with stop control on the eastbound approach. The eastbound approach provides approximately 45 feet of unmarked pavement that is assumed to operate as one (1) left turn lane and one (1) right turn lane. The northbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, and three (3) through lanes. The southbound approach provides three (3) through lanes and one (1) dedicated right turn lane.

Arizona Avenue and Driveway B (3) currently operates as a two-way stop-controlled T-intersection with stop control on the eastbound and westbound approaches. The eastbound and westbound approaches are assumed to operate as one (1) shared left-through-right lane. The northbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, two (2) through lanes and one (1) shared through-right turn lane. The southbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, and two (2) through lanes, and one (1) dedicated right turn lane. Driveway B is located approximately 375-feet south of Grace Boulevard (centerline-to-centerline).

Arizona Avenue and Driveway D (4) currently operates as a two-way stop-controlled T-intersection with stop control on the eastbound and westbound approaches. The eastbound and westbound approaches are assumed to operate as one (1) shared left-through-right lane. The northbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, two (2) through lanes and one (1) shared through-right turn lane. The southbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, and two (2) through lanes, and one (1)



dedicated right turn lane. Driveway D is located approximately 675-feet south of Grave Boulevard (centerline-to-centerline).

Arizona Avenue and Driveway E (5) currently operates as a two-way stop-controlled T-intersection with stop control on the eastbound and westbound approaches. The eastbound and westbound approaches are assumed to operate as one (1) shared left-through-right lane. The northbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, two (2) through lanes and one (1) dedicated right turn lane. The southbound approach provides one (1) dedicated left turn lane, via a two-way left turn lane, and two (2) through lanes, and one (1) dedicated right turn lane. Driveway D is located approximately 675-feet south of Grave Boulevard (centerline-to-centerline).

3.3. STUDY AREA LAND USE

The proposed site is generally located on southeast corner of Arizona Avenue and Grace Boulevard in the City of Chandler, Arizona. The immediate surroundings study area primarily consists of residential and commercial land uses. The Chandler Mercado retail center borders the site to the south. The Biscayne Bay Apartments border the site to the west. Directly north is currently a portion of the Earnhardt Chevrolet parking lot and vacant land, however a future multifamily development is currently planned for construction.

3.4. SITE ACCESSIBILITY

Roadway System

The US 60 and SR 202 Freeways run east-west within the vicinity of the study area. Additionally, The SR 101 runs north-south within the vicinity of the study area. The SR 101, US 60, and SR 202 currently run through the cities of Phoenix, Mesa, Chandler, Tempe, and Town of Gilbert. The US 60 and SR 202 can be accessed via Arizona Avenue. SR 101 can be accessed by both Elliot Road and Warner Road.

Pedestrian Facilities

Within the study area, sidewalks are provided along both sides of Arizona Avenue.

Grace Boulevard provides continuous sidewalks on the north side of the road, with intermittent sidewalks on the south side of the road, within the study area.

Bicycle Facilities

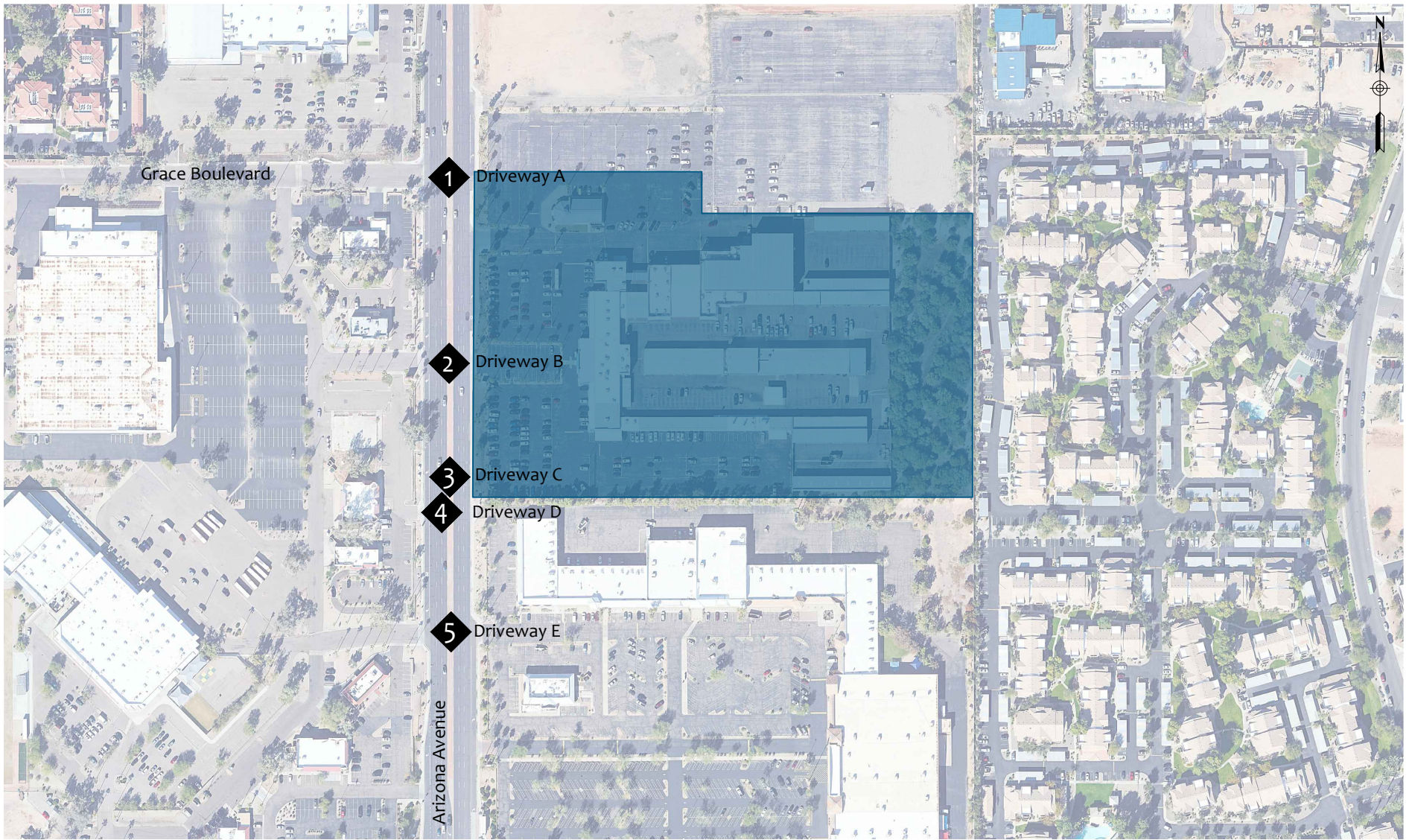
Bike lanes are currently not provided within the proposed study area.

Transit Facilities

Within the immediate study area, Valley Metro Route 112 operates along Arizona Avenue. Valley Metro Route 112 provides an existing shaded bus stop in the northbound direction of travel



approximately 230 feet north of Grace Boulevard. Similarly, Valley Metro Route 112 provides an existing shaded bus stop in the southbound direction of travel approximately 135 feet south of Grace Boulevard.



Legend

 Intersection

FIGURE 3 | STUDY AREA



4. EXISTING CONDITIONS

4.1. EXISTING LAND USE

According to Maricopa County Assessor’s website, the proposed site will occupy three (3) existing parcels, APNs 302-29-009E, 302-29-009G, and 302-29-009L. Currently, these three (3) parcels are currently being utilized by the existing Earnhardt Chevrolet auto dealership. The three (3) existing parcels are zoned for C-3 uses. See **Appendix B** for detailed parcel information.

4.2. EXISTING TRAFFIC COUNTS

A local data collection firm, All Traffic Data Services, LLC, was utilized to collect traffic counts. On Thursday, September 9th, 2023, four hours of typical weekday turning movement counts were obtained during the AM (7:00 to 9:00 am) and PM (4:00 to 6:00 pm) peak hours at the following intersection:

- Arizona Avenue and Grace Boulevard/Driveway A (1)
- Arizona Avenue and Driveway B (2)
- Arizona Avenue and Driveway D (4)
- Arizona Avenue and Driveway E (5)

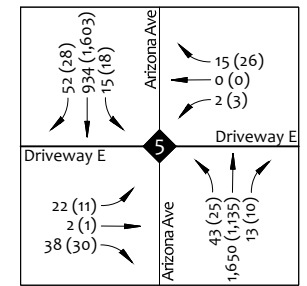
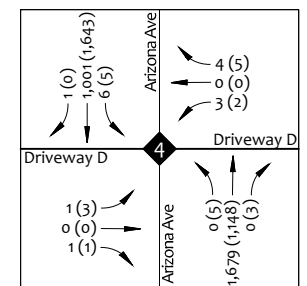
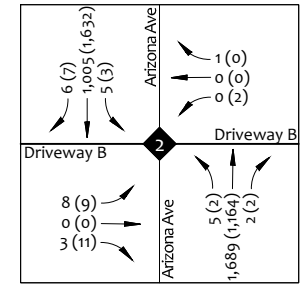
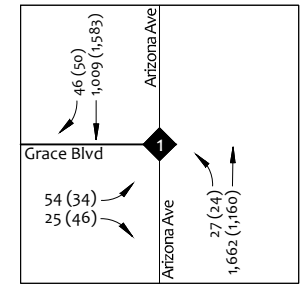
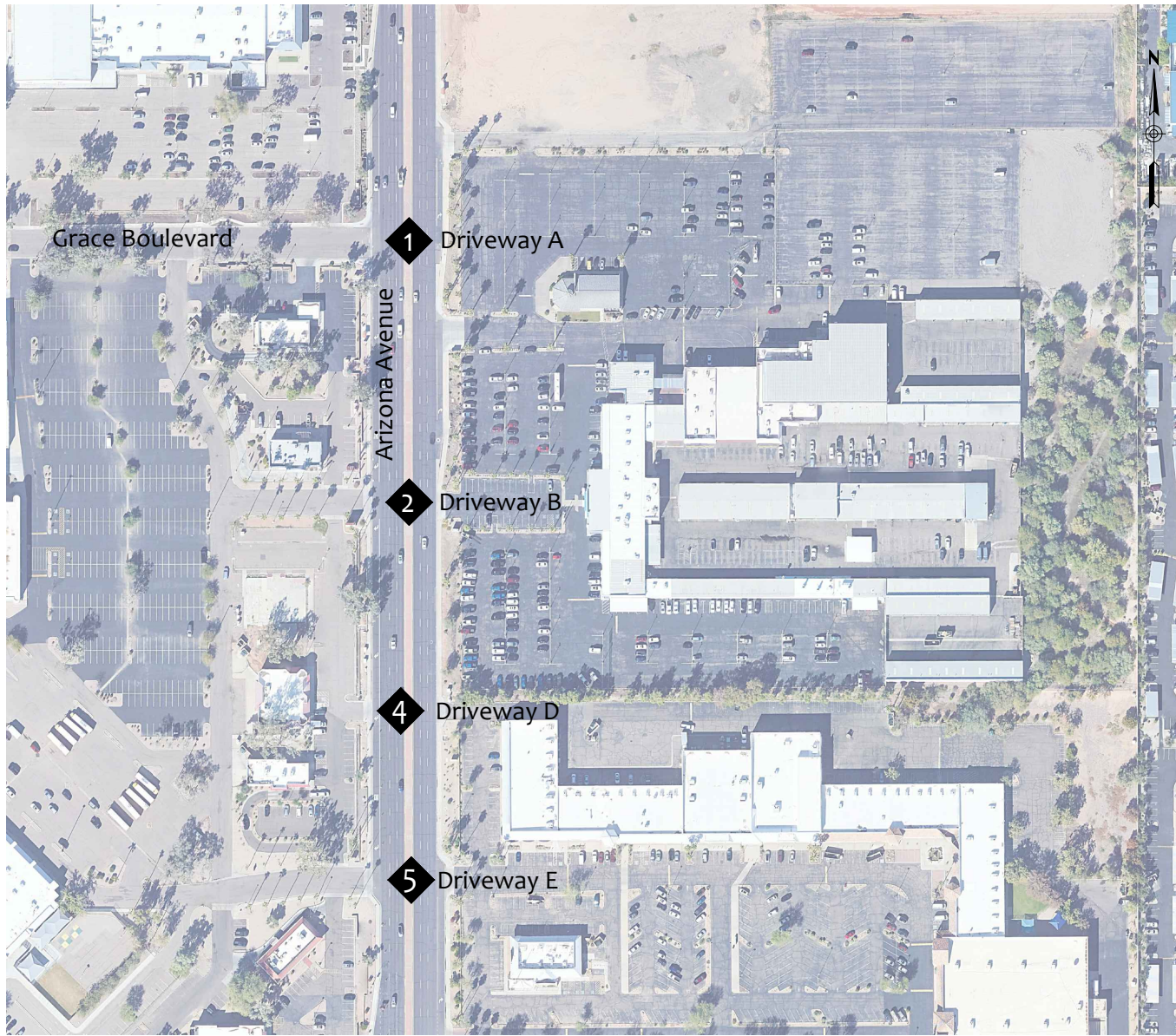
Additionally, on Thursday, September 9th, 2023, bi-directional tube counts for 24-hours in 15-minute intervals were collected along the following roadway segment(s):

- Arizona Avenue, south of Grace Boulevard

The turning movement counts were then analyzed for the highest 1-hour within each time period. The following peak hours were analyzed throughout this study.

AM Peak Hour	7:15 am – 8:15 am
PM Peak Hour	4:45 pm – 5:45 pm

See **Appendix C** for detailed traffic count data. See **Figure 4** for the existing AM and PM peak hour traffic volumes.



Legend

AM (PM) Peak Hour Traffic Volumes

Intersection

FIGURE 4 | EXISTING TRAFFIC VOLUMES



4.3. EXISTING CAPACITY ANALYSIS

The existing conditions capacity analysis was completed for the existing study intersections. The capacity and level of service for the study area intersections were evaluated using the methodology presented in the 7th Edition of the Highway Capacity Manual (HCM). Traffic analysis software, Synchro Version 12, was used to perform the analyses using the existing peak hour factor (PHF) obtained from the traffic counts.

Table 1 is from the 6th Edition of the Highway Capacity Manual Exhibit 19-8 and 20-2, which lists the Level of Service (LOS) thresholds for signalized and unsignalized intersections.

Table 1 – Level of Service Criteria

Level of Service (LOS)	Control Delay per Vehicle (s/veh)	
	Signalized Intersection	Unsignalized Intersection
A	≤ 10	≤ 10
B	> 10-20	> 10-15
C	> 20-35	> 15-25
D	> 35-55	> 25-35
E	> 55-80	> 35-50
F	> 80	> 50

The results of the capacity analyses reveal that all movements operate at a LOS C or better.

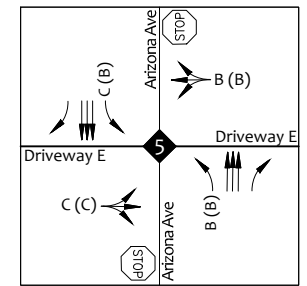
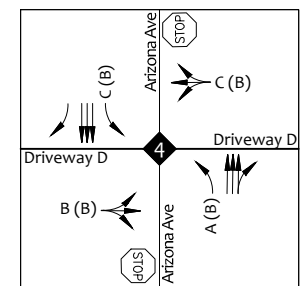
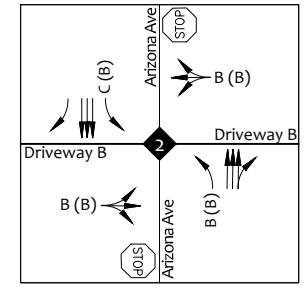
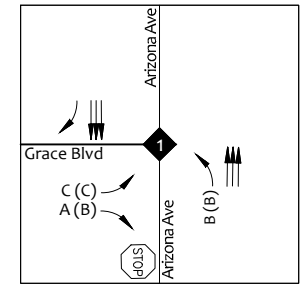
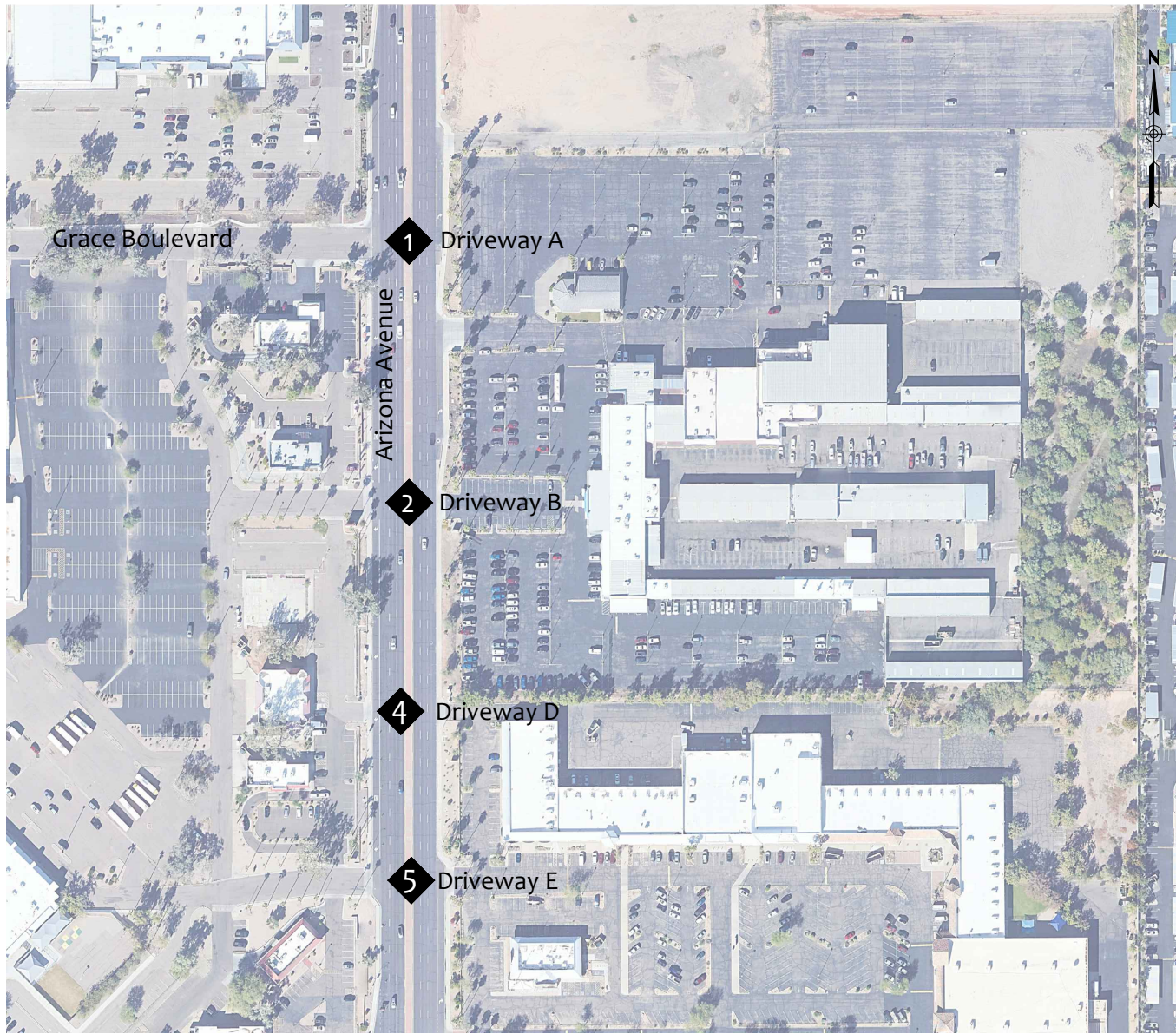
See **Figure 5** for the existing AM and PM peak hour capacity analysis. The existing AM and PM peak hour level of service, delay, and volume-to-capacity ratios for the study intersections are shown in **Table 2**.

The detailed capacity analysis sheets can be found in **Appendix D**.



Table 2 – Existing Level of Service, Delay, and V/C Ratio

Intersection	Existing Conditions					
	AM PEAK			PM PEAK		
	LOS	DELAY	V/C	LOS	DELAY	V/C
Arizona Avenue and Grace Boulevard (1)						
Eastbound Left	C	19.5	0.197	C	23.3	0.154
Eastbound Right	A	9.7	0.035	B	10.8	0.072
Northbound Left	B	12.8	0.062	B	14.8	0.064
Arizona Avenue and Driveway B (2)						
Eastbound Shared Left-Through-Right	B	11.9	0.023	B	12.8	0.044
Westbound Shared Left-Through-Right	B	10.4	0.002	B	12.3	0.004
Northbound Left	B	11.9	0.011	B	13.9	0.005
Southbound Left	C	17.1	0.019	B	11.4	0.006
Arizona Avenue and Driveway D (4)						
Eastbound Shared Left-Through-Right	B	11.6	0.004	B	15.0	0.011
Westbound Shared Left-Through-Right	C	18.7	0.030	B	10.5	0.011
Northbound Left	A	0.0	-	B	14.1	0.013
Southbound Left	C	17.8	0.024	B	11.4	0.009
Arizona Avenue and Driveway E (5)						
Eastbound Shared Left-Through-Right	C	20.1	0.230	C	15.4	0.112
Westbound Shared Left-Through-Right	B	14.4	0.049	B	10.4	0.043
Northbound Left	B	12.6	0.094	B	14.8	0.067
Southbound Left	C	17.9	0.058	B	11.5	0.033



- Legend**
- AM(PM) Peak Hour Capacity Analysis
 - ◆ Intersection
 - ↔ Lane Configuration

FIGURE 5 | EXISTING CAPACITY ANALYSIS



5. PROJECTED TRAFFIC

5.1. TRIP GENERATION

The trip generation for the proposed development was calculated utilizing the Institute of Transportation Engineers (ITE) publication entitled *Trip Generation, 11th Edition*. The ITE rates are based on studies that measured the trip generation characteristics for various types of land uses. The rates are expressed in terms of trips per unit of land use type. This publication is considered to be the standard for the transportation engineering profession.

5.1.1. PROPOSED DEVELOPMENT

The proposed development will be comprised of 423 dwelling units located in four-story buildings. Therefore, the trip generation for the proposed Toll Brothers Uptown development was calculated utilizing the ITE Land Use 840 – Multifamily Housing (Mid-Rise).

The total trip generation for the proposed development is shown in **Table 3**.

Table 3 – Trip Generation – Proposed Development

Land Use	ITE Code	Qty	Unit	Weekday	AM Peak Hour			PM Peak Hour		
				Total	Total	In	Out	Total	In	Out
Multifamily Housing (Mid-Rise)	221	423	Dwelling Units	1,971	175	40	135	165	104	61

The proposed Toll Brothers Uptown development is anticipated to generate a total of 1,971 daily trips, with 175 trips occurring in the AM peak hour and 165 trips in the PM peak hour.

Detailed trip generation calculations can be found in **Appendix E**.

5.1.2. EXISTING DEVELOPMENT

As previously mentioned, the existing site is occupied by the Earnhardt Chevrolet automobile sales dealership. Therefore, the trips generated by 71,582 square foot automobile sales dealership was calculated utilizing ITE Lane Use 840 – Automobile Sales (New). The total trip generation for the existing development is shown in **Table 4**.

Table 4 – Trip Generation – Existing Development

Land Use	ITE Code	Qty	Unit	Weekday	AM Peak Hour			PM Peak Hour		
				Total	Total	In	Out	Total	In	Out
Automobile Sales (New)	840	71.6	1000 SF GFA	1,993	133	97	36	173	69	104



5.2. TRIP GENERATION COMPARISON

A trip generation comparison was completed between the proposed Toll Brothers Uptown development and the existing automobile sales dealership. See **Table 5** below.

Table 5 – Trip Generation Comparison

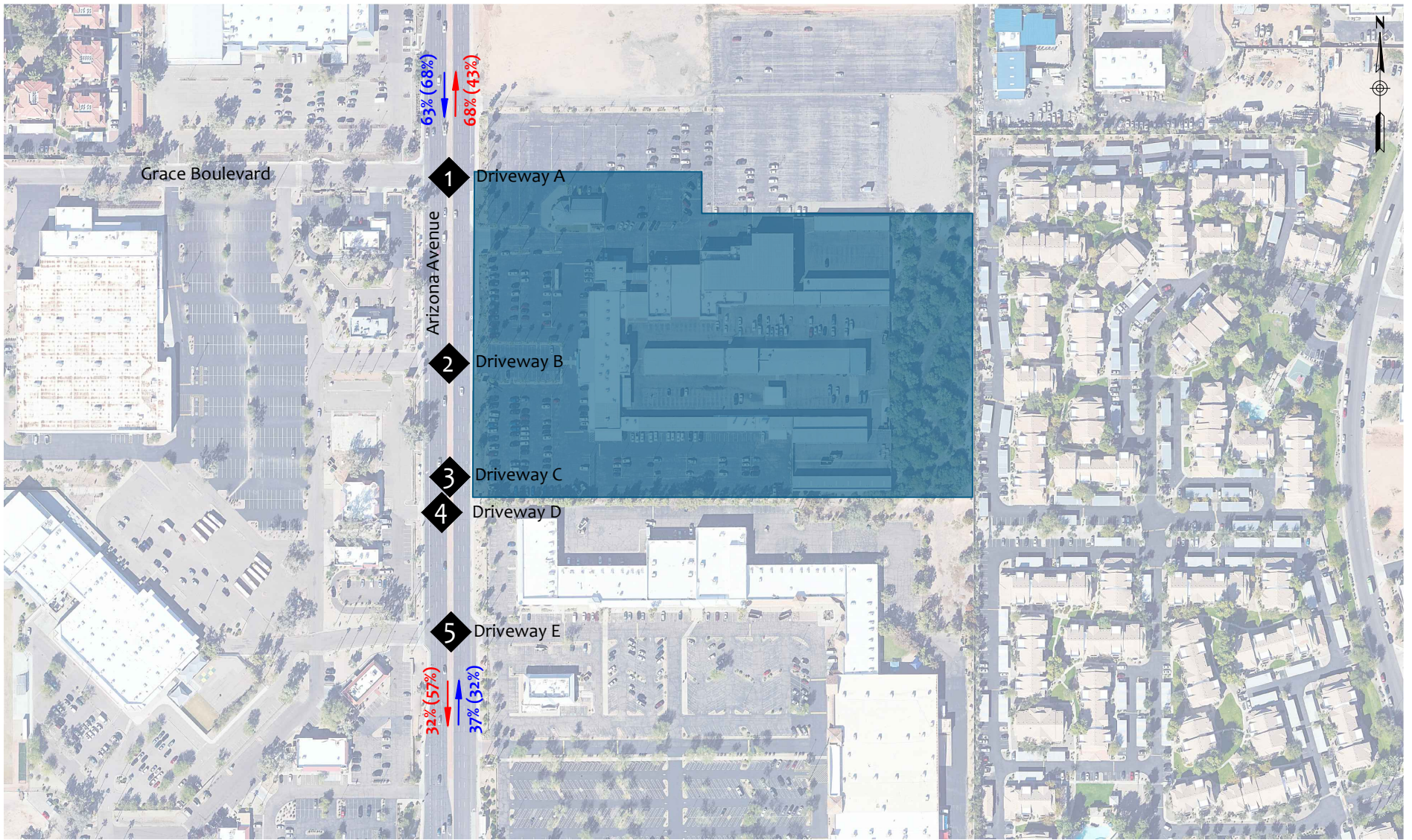
Land Use	ITE Code	Qty	Unit	Weekday	AM Peak Hour			PM Peak Hour		
				Total	Total	In	Out	Total	In	Out
Existing Development	840	71.6	1000 SF GFA	1,993	133	97	36	173	69	104
Proposed Development	221	423	Dwelling Units	1,971	175	40	135	165	104	61
Difference				-22	42	-57	99	-8	35	-43

The proposed development generates 22 fewer weekday trips, with 42 more trips during the AM peak hour and 8 fewer trips during the PM peak hour, when compared to the existing automobile sales dealership.

5.3. TRIP DISTRIBUTION AND ASSIGNMENT

The trip distribution procedure determines the general pattern of travel for vehicles entering and exiting the proposed development. The trip distribution for the proposed development is largely based on the distribution of the existing traffic with adjustments to account for the anticipated changes in traffic patterns along the surrounding roadway network. The trip distribution is shown in **Figure 6**.

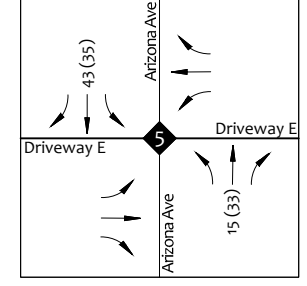
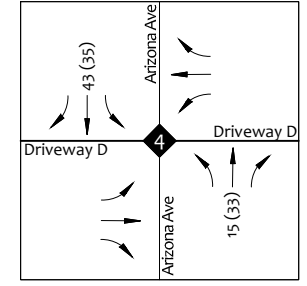
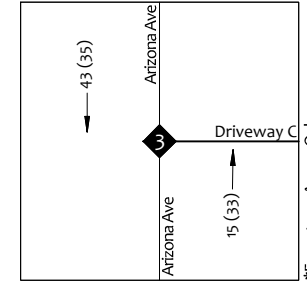
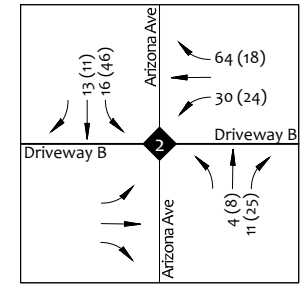
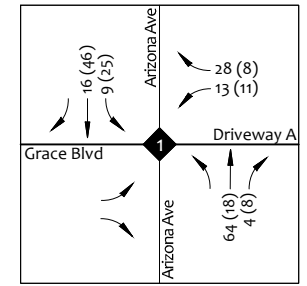
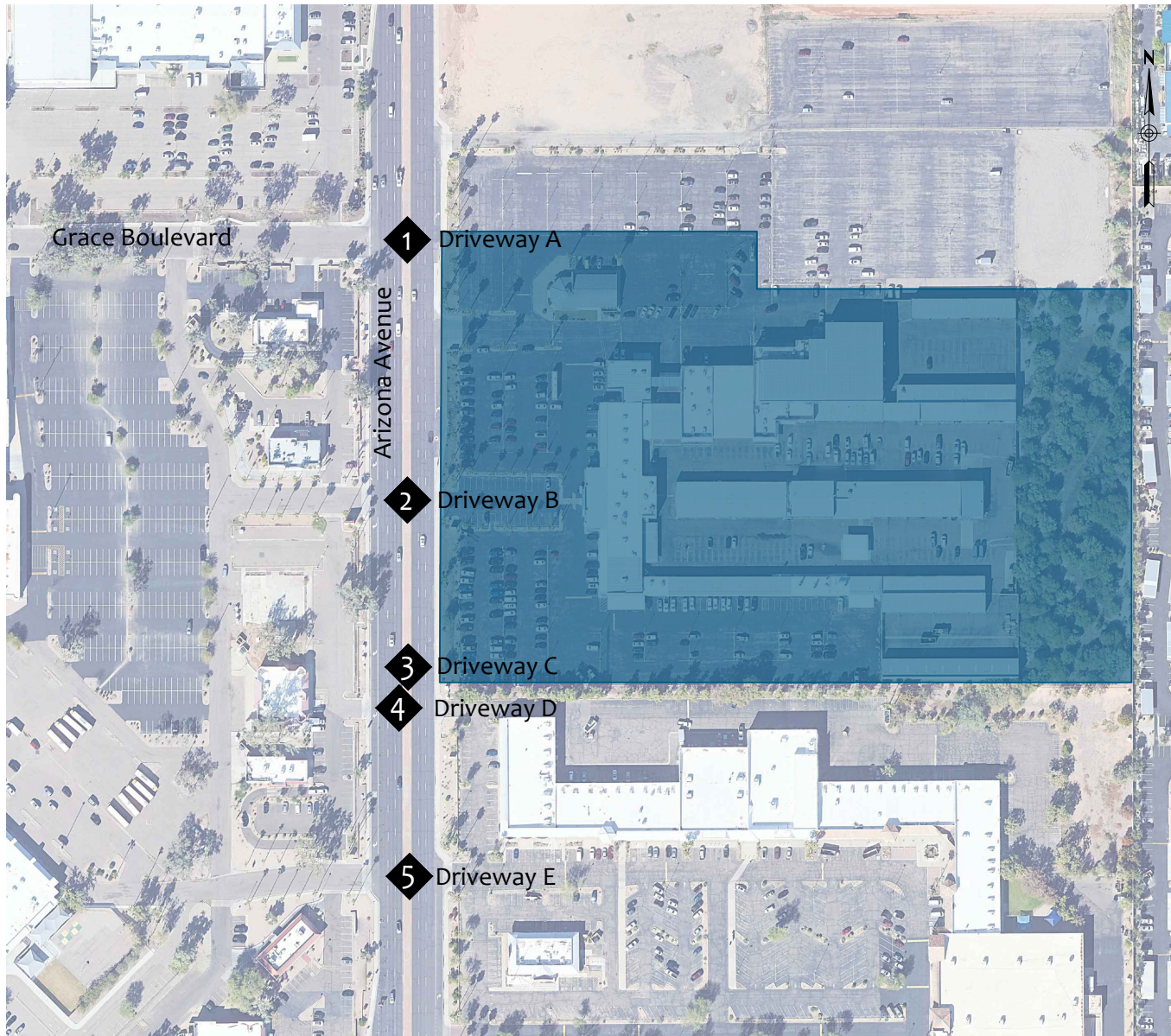
The trip assignment was generally based on proximity of the driveways, permitted turn movements, as well as ease and probability of use respective to each land use within the site. The site generated traffic volumes are shown in **Figure 7**.



Legend

- XX% Inbound Trip Distribution Percentages
- XX% Outbound Trip Distribution Percentages
- X Intersection

FIGURE 6 | TRIP DISTRIBUTION



*Emergency Access Only

Legend

AM (PM) Peak Hour Traffic Volumes

◆ Intersection

FIGURE 7 | SITE GENERATED TRAFFIC VOLUMES



6. FUTURE CONDITIONS (YEAR 2027 – OPENING YEAR)

The Toll Brothers Uptown development is anticipated to be completed and open in the year 2027. Therefore, this section analyzes the effects the proposed development will have on the surrounding roadway network during the year 2027.

6.1. YEAR 2027 BACKGROUND TRAFFIC VOLUMES

According to the 2023 Maricopa Association of Government (MAG) socioeconomic projections within the proposed study area, it is estimated that in the year 2030 the population within the Regional Analysis Zone (RAZ) will be approximately 53,951. MAG estimates that the 202 population of the surrounding area to be 52,605. This results in an approximate annual growth rate of 0.25%. As a conservative approach, a 1.0% annual growth rate was utilized.

This annual growth rate of 1.0% was applied to the existing traffic volumes (**Figure 4**) to estimate the ambient traffic growth in the surrounding area. See **Appendix F** for MAG socioeconomic projections.

Surrounding Developments

Additionally, the following development project within the vicinity of the proposed development was considered when projecting the future background volumes.

Alta Uptown

The proposed Alta Uptown residential development is located on the southeast corner of Arizona Avenue and Corporate Place. The traffic volumes associated with this site were obtained from the *Alta Uptown Traffic Impact Study*, dated December 1, 2021. According to this Traffic Impact Study, the proposed Alta Uptown development will be comprised of 431 multifamily residential units and is anticipated to open in the year 2024. The traffic volumes generated by the Alta Uptown were obtained from this Traffic Impact Study. The Alta Uptown peak hour site traffic volumes are included in the year 2027 background volumes shown in **Figure 8**. See **Appendix G** for excerpts of the *Alta Uptown Traffic Impact Study*, dated December 1, 2021.

See **Figure 8** for the year 2027 background traffic volumes which includes the ambient growth and the surrounding development identified above.

6.2. YEAR 2027 BUILD TRAFFIC VOLUMES

To determine 2027 build traffic volumes, site traffic volumes (**Figure 7**) were added to the 2027 background traffic volumes (**Figure 8**). This represents year 2027 traffic volumes with the build out of the proposed Toll Brothers Uptown. The year 2027 build traffic volumes are shown in **Figure 9**.



6.3. YEAR 2027 NO BUILD CAPACITY ANALYSIS

The capacity and level of service for the study area intersections were evaluated for the year 2027 no build scenario. The corresponding traffic volumes are shown in **Figure 8**. A peak hour factor (PHF) of 0.92 was utilized.

The results of the 2027 no build capacity analysis level of service is shown in **Figure 10**. The results of the capacity analyses reveal that all movements operate at a LOS D or better. The year 2027 AM and PM peak hour level of service and delay for the study intersections are shown in **Table 6**.

The year 2027 no build capacity analysis sheets can be found in **Appendix H**.

6.4. YEAR 2027 BUILD CAPACITY ANALYSIS

The year 2027 build capacity analysis was completed for the study intersections and driveways during the AM and PM peak hour. The analysis for the study area intersections were evaluated using the methodology described in **Section 4.2**. A PHF of 0.92 was utilized.

The following improvements were included in the year 2027 capacity analysis:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Buildout of full-access point on the east leg of the intersection
- **Arizona Avenue and Driveway B (2)**
 - Buildout of full-access point on the east leg of the intersection
- **Arizona Avenue and Driveway C (3)**
 - Buildout of an emergency access only driveway.

The capacity and level of service for the study area intersections were evaluated for the year 2027 build traffic volumes (**Figure 9**). The results of the 2027 build capacity analysis level of are shown in **Figure 11**. The results of the capacity analyses reveal that all movements operate at a LOS D or better, with the exception of:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Eastbound left PM peak hour operates at LOS F

Delays are typical during peak hours for minor-to-major turning movements as well as for movements at stop-controlled intersections. Drivers familiar with the area often choose to use alternate routes during peak hours or drive at different times to avoid potential delay. Therefore, improvements to mitigate LOS F at the above intersections are not recommended as part of this study.

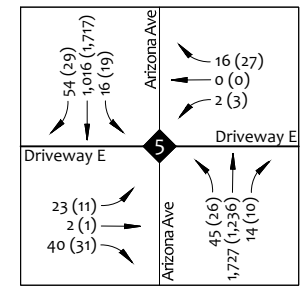
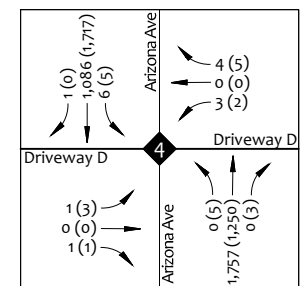
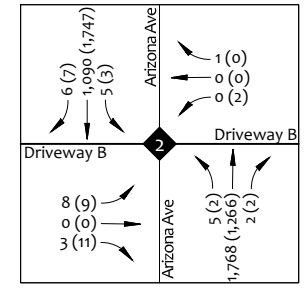
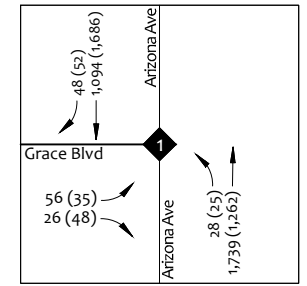
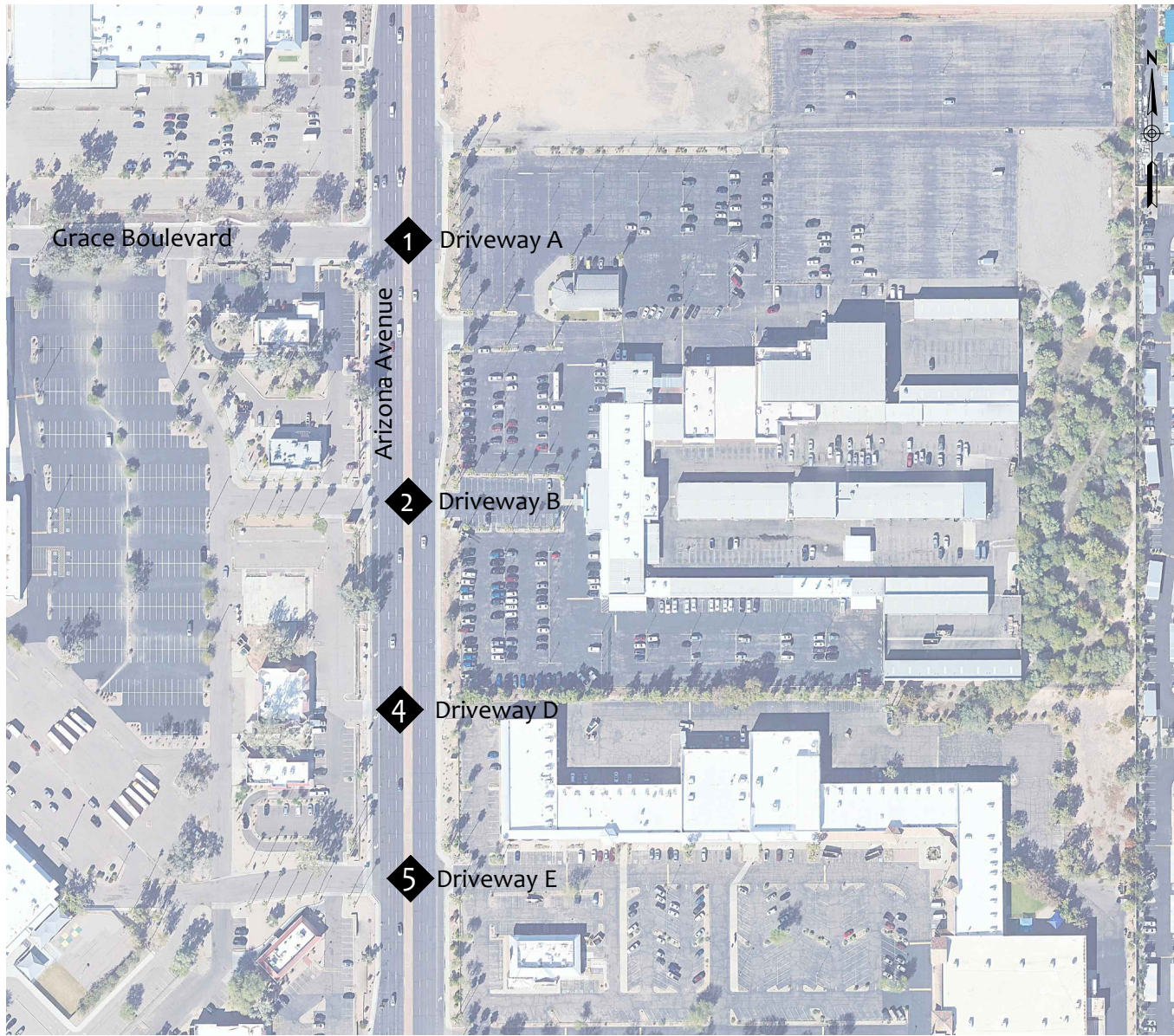


The year 2027 AM and PM peak hour level of service, delay, and volume-to-capacity ratios for the study intersections are shown in **Table 6**. The year 2027 build capacity analysis sheets can be found in **Appendix I**.



Table 6 – Year 2027 Level of Service, Delay, and V/C Ratio

Intersection	Year 2027 No Build						Year 2027 Build					
	AM PEAK			PM PEAK			AM PEAK			PM PEAK		
	LOS	DELAY	V/C	LOS	DELAY	V/C	LOS	DELAY	V/C	LOS	DELAY	V/C
Arizona Avenue and Grace Boulevard/Driveway A (1)												
Eastbound Left	C	17.4	0.173	D	33.4	0.232	D	15.1	0.146	F	129.4	0.613
Eastbound Right	A	10.0	0.038	B	10.8	0.077	A	10.0	0.038	B	10.8	0.077
Westbound Shared Left-Through-Right	-	-	-	-	-	-	C	18.8	0.146	D	29.4	0.123
Northbound Left	B	12.1	0.056	C	18.2	0.090	B	12.2	0.058	C	19.3	0.098
Southbound Left	-	-	-	-	-	-	C	16.4	0.030	B	13.8	0.062
Arizona Avenue and Driveway B (2)												
Eastbound Shared Left-Through-Right	B	11.8	0.022	C	16.8	0.067	B	13.4	0.027	C	21.6	0.091
Westbound Shared Left-Through-Right	B	10.4	0.002	B	13.6	0.005	-	-	-	-	-	-
Westbound Left	-	-	-	-	-	-	D	34.8	0.213	C	19.0	0.092
Westbound Right	-	-	-	-	-	-	B	11.0	0.104	A	10.0	0.026
Northbound Left	B	11.1	0.009	C	16.7	0.007	B	11.2	0.009	C	16.9	0.007
Southbound Left	C	17.2	0.018	B	12.8	0.007	C	18.0	0.059	B	14.2	0.113
Arizona Avenue and Driveway D (4)												
Eastbound Shared Left-Through-Right	B	11.0	0.004	C	22.9	0.021	B	11.0	0.004	D	25.3	0.024
Westbound Shared Left-Through-Right	C	15.8	0.022	B	11.3	0.013	C	16.5	0.024	B	11.7	0.014
Northbound Left	A	0.0	-	C	17.4	0.018	A	0.0	-	C	18.2	0.020
Southbound Left	C	17.4	0.022	B	12.9	0.012	C	17.7	0.023	B	13.3	0.012
Corporate Place and Driveway D (5)												
Eastbound Shared Left-Through-Right	C	20.2	0.230	C	24.7	0.204	C	22.6	0.258	D	28.1	0.232
Westbound Shared Left-Through-Right	B	14.1	0.047	B	11.0	0.051	B	14.6	0.049	B	11.2	0.053
Northbound Left	B	12.9	0.097	C	18.2	0.094	B	13.4	0.103	C	19.0	0.099
Southbound Left	C	17.6	0.057	B	13.1	0.044	C	17.9	0.059	B	13.5	0.047

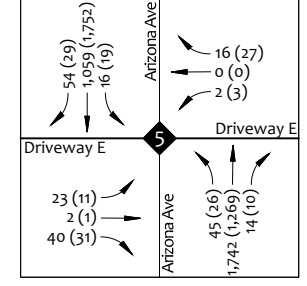
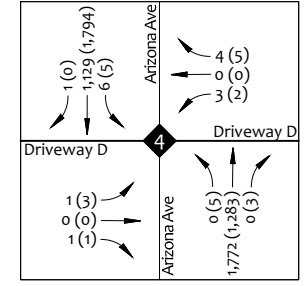
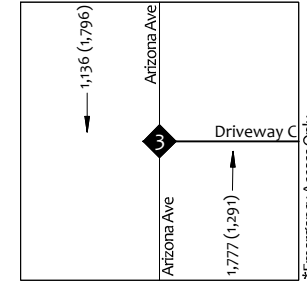
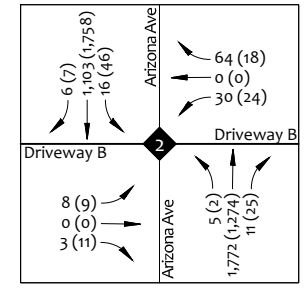
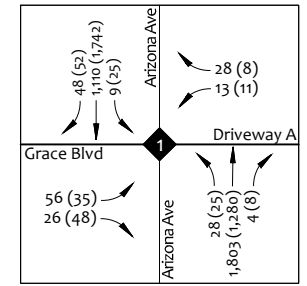
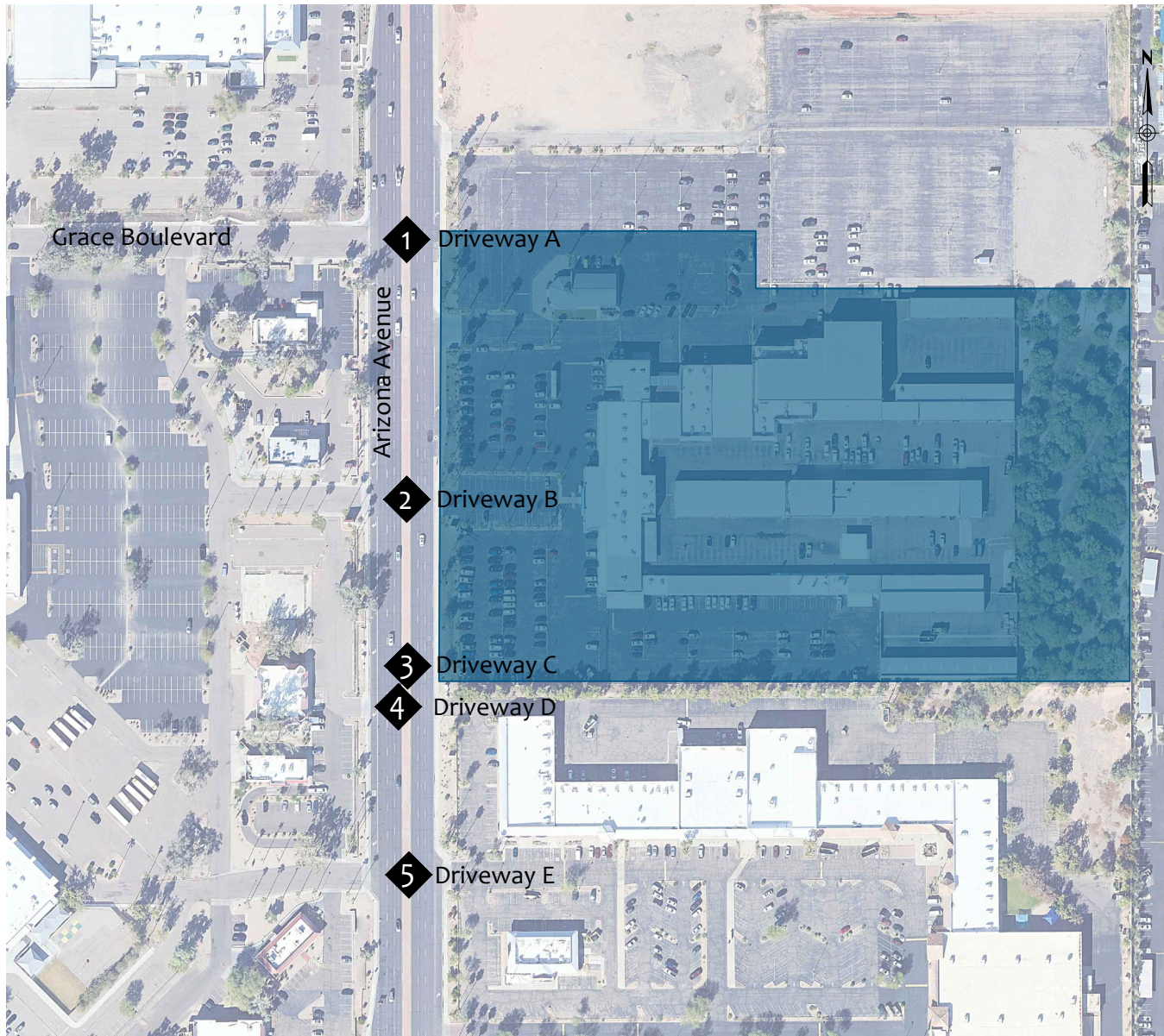


Legend

AM (PM) Peak Hour Traffic Volumes

Intersection

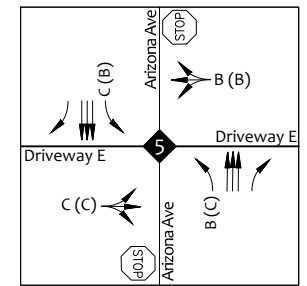
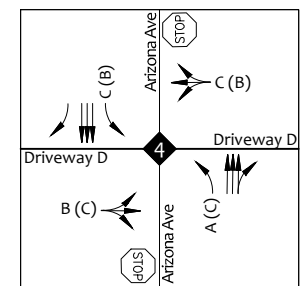
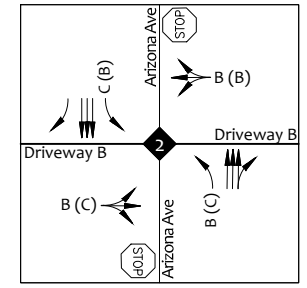
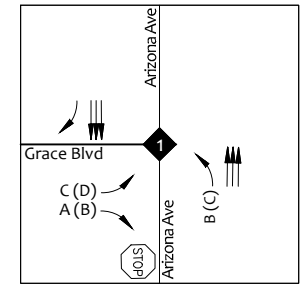
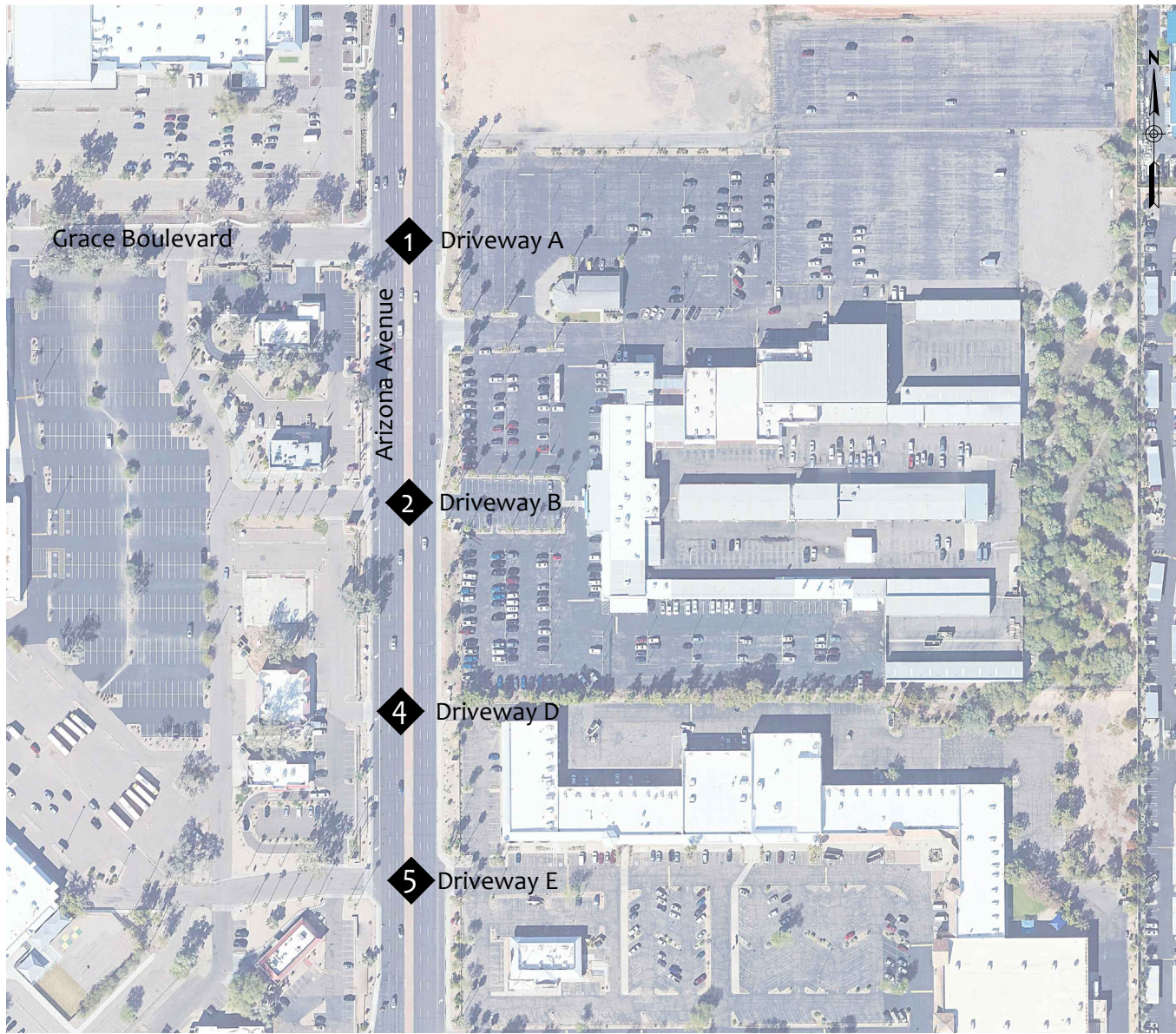
FIGURE 8 | YEAR 2027 BACKGROUND TRAFFIC VOLUMES



*Emergency Access Only

Legend
 AM (PM) Peak Hour Traffic Volumes
 X Intersection

FIGURE 9 | YEAR 2027 BUILD TRAFFIC VOLUMES



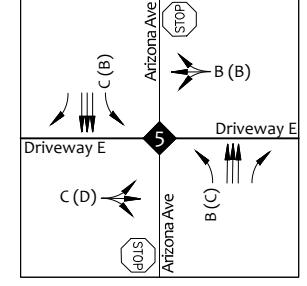
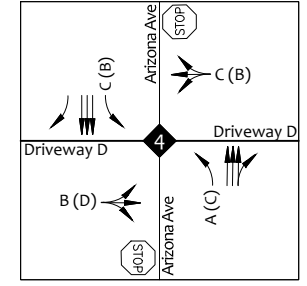
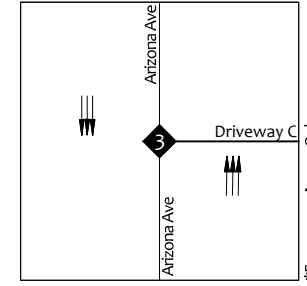
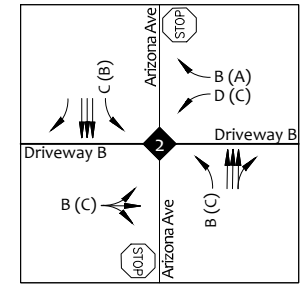
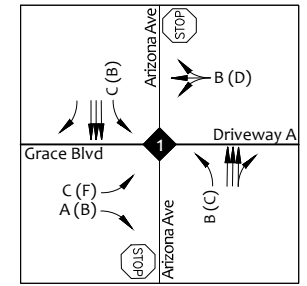
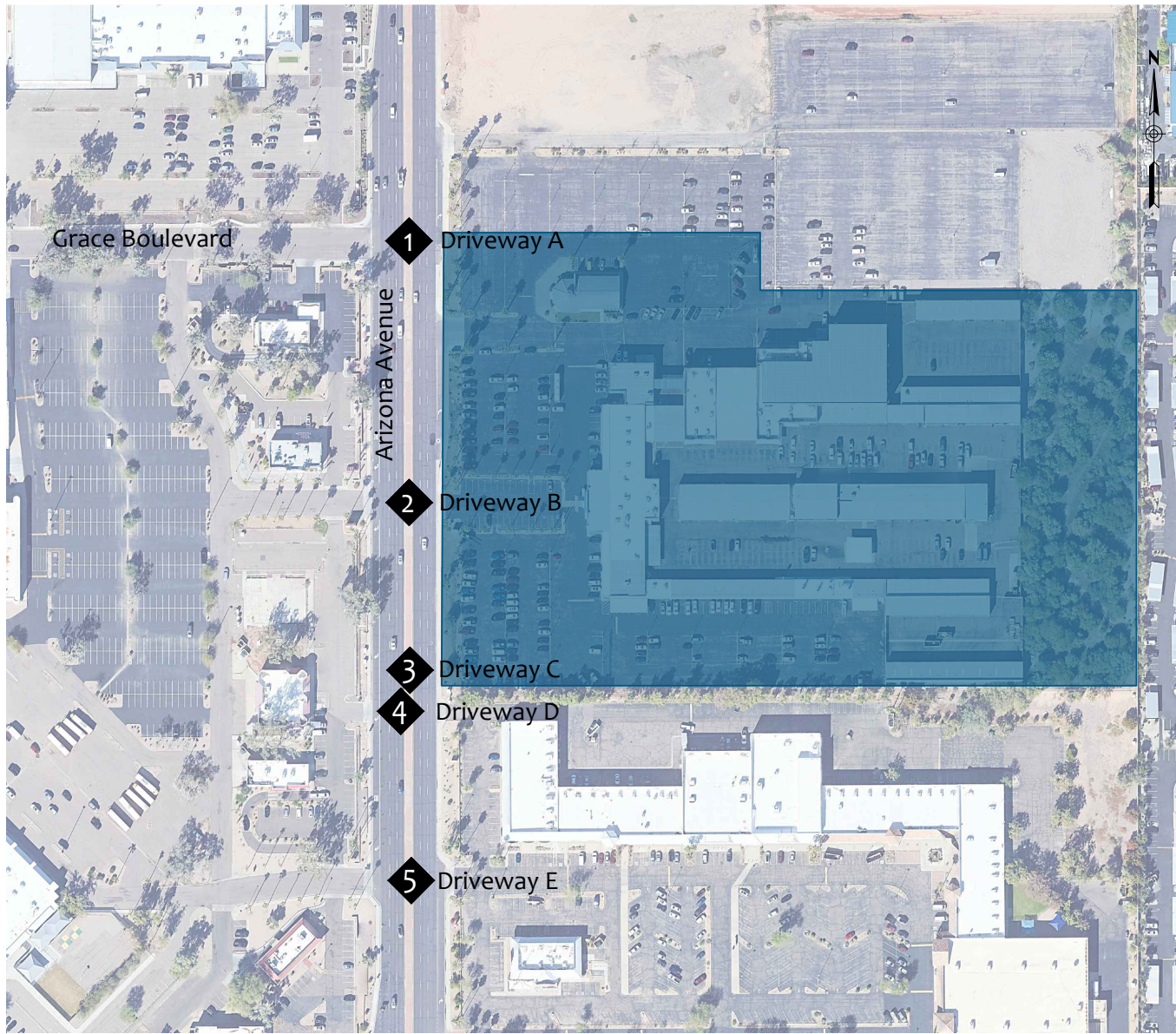
Legend

AM(PM) Peak Hour Capacity Analysis

◆ Intersection

↔ Lane Configuration

FIGURE 10 | YEAR 2027 NO BUILD CAPACITY ANALYSIS



*Emergency Access Only

Legend

AM(PM) Peak Hour Capacity Analysis

◆ Intersection

↔ Lane Configuration

FIGURE 11 | YEAR 2027 BUILD CAPACITY ANALYSIS



7. FUTURE CONDITIONS (YEAR 2032)

This section analyzes the effects the proposed development will have on the surrounding roadway network during the year of 2032, 5 years after the opening year.

7.1. YEAR 2032 BACKGROUND TRAFFIC VOLUMES

Similar to the year 2027 background traffic volumes described in detail in **Section 6.1**, a 1.0% annual growth rate was applied to the existing traffic volumes through the year 2032. Additionally, the surrounding development identified in **Section 6.1** was included in the year 2032 background volumes.

The year 2032 background traffic volumes are shown in **Figure 12**.

7.2. YEAR 2032 BUILD TRAFFIC VOLUMES

When the total site traffic (**Figure 7**) were added to the year 2032 background traffic (**Figure 12**), the result is the 2032 build traffic volumes. This represents the traffic volumes with the build out of the proposed development. The year 2032 build traffic volumes are shown in **Figure 13**.

7.3. YEAR 2032 NO BUILD CAPACITY ANALYSIS

The capacity and level of service for the study area intersections were evaluated for the year 2032 no build scenario. The corresponding traffic volumes are shown in **Figure 12**. A PHF of 0.92 was utilized.

The results of the 2032 no build capacity analysis level of service is shown in **Figure 14**. The results of the capacity analyses reveal that all movements operate at a LOS D or better, with the exception of:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Eastbound left PM peak hour operates at LOS E

The year 2032 AM and PM peak hour level of service and delay for the study intersections are shown in **Table 7**. The year 2032 no build capacity analysis sheets can be found in **Appendix J**.



7.4. YEAR 2032 BUILD CAPACITY ANALYSIS

The year 2032 build capacity analysis was completed for the study intersections and driveways during the AM and PM peak hour. The analysis for the study area intersections were evaluated using the methodology described in [Section 4.2](#). A PHF of 0.92 was utilized.

The improvements identified in [Section 6.4](#) were included in the year 2032 build scenario.

The capacity and level of service for the study area intersections were evaluated for the year 2032 build traffic volumes ([Figure 13](#)). The results of the 2032 build capacity analysis level of service are shown in [Figure 15](#). The results of the capacity analyses reveal that all movements operate at a LOS D or better, with the exception of:

The results of the capacity analyses reveal that all movements operate at a LOS D or better, with the exception of:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Eastbound left PM peak hour operates at LOS F

- **Arizona Avenue and Driveway B (2)**
 - Westbound left AM peak hour operates at LOS E

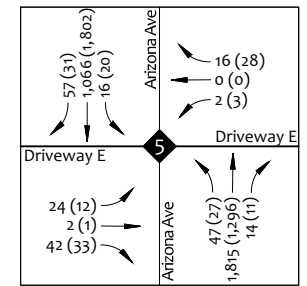
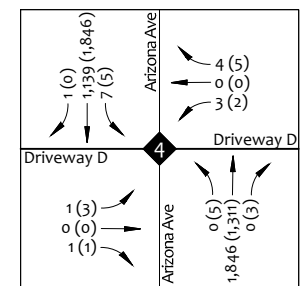
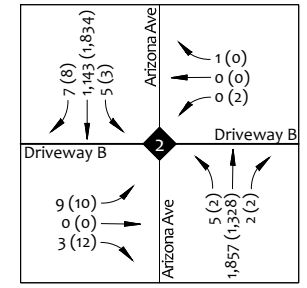
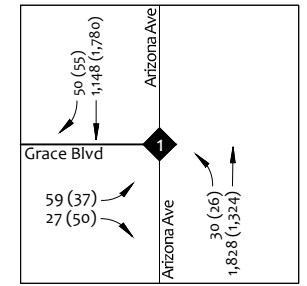
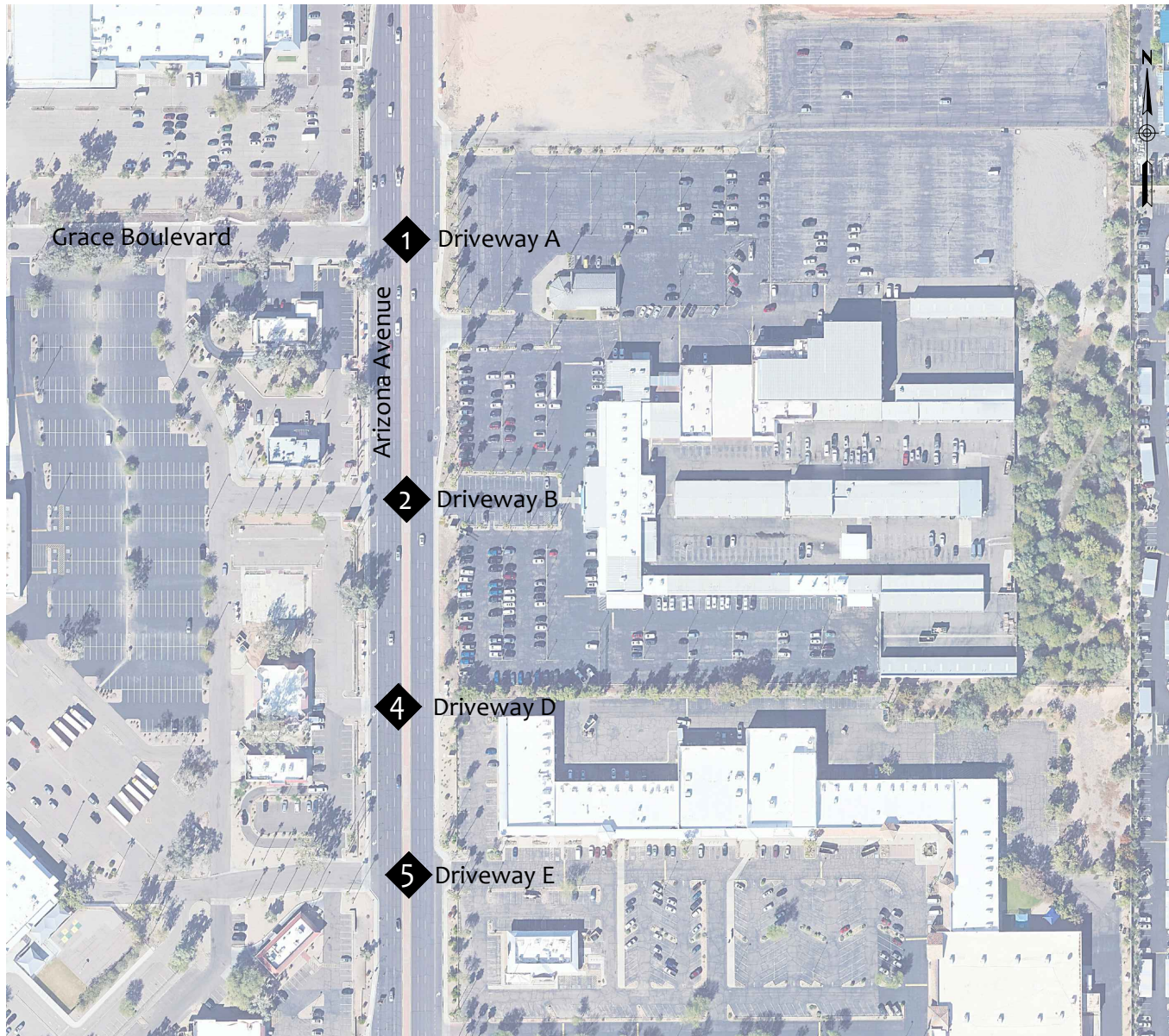
Delays are typical during peak hours for minor-to-major turning movements as well as for movements at stop-controlled intersections. Drivers familiar with the area often choose to use alternate routes during peak hours or drive at different times to avoid potential delay. Therefore, improvements to mitigate LOS E or F at the above intersections are not recommended as part of this study.

The year 2032 AM and PM peak hour level of service, delay, and volume-to-capacity ratios for the study intersections are shown in [Table 7](#). The year 2032 build capacity analysis sheets can be found in [Appendix K](#).



Table 7 – Year 2032 Level of Service, Delay, and V/C Ratio

Intersection	Year 2032 No Build						Year 2032 Build					
	AM PEAK			PM PEAK			AM PEAK			PM PEAK		
	LOS	DELAY	V/C	LOS	DELAY	V/C	LOS	DELAY	V/C	LOS	DELAY	V/C
Arizona Avenue and Grace Boulevard/Driveway A (1)												
Eastbound Left	C	18.9	0.199	E	41.5	0.292	C	19.0	0.200	F	165.3	0.723
Eastbound Right	A	10.0	0.039	B	10.8	0.081	A	10.0	0.039	B	11.2	0.086
Westbound Shared Left-Through-Right	-	-	-	-	-	-	C	23.4	0.186	D	30.9	0.130
Northbound Left	B	12.7	0.065	C	20.5	0.109	B	12.9	0.067	C	18.9	0.099
Southbound Left	-	-	-	-	-	-	C	18.4	0.035	B	13.1	0.057
Arizona Avenue and Driveway B (2)												
Eastbound Shared Left-Through-Right	B	12.0	0.025	B	13.9	0.056	C	15.5	0.037	C	17.3	0.076
Westbound Shared Left-Through-Right	B	10.8	0.002	B	13.8	0.005	-	-	-	-	-	-
Westbound Left	-	-	-	-	-	-	E	48.2	0.283	C	15.5	0.071
Westbound Right	-	-	-	-	-	-	B	11.5	0.111	B	10.2	0.028
Northbound Left	B	11.6	0.010	C	16.3	0.007	B	11.7	0.010	C	16.5	0.007
Southbound Left	C	16.8	0.017	B	12.2	0.006	C	17.6	0.057	B	13.3	0.104
Arizona Avenue and Driveway D (4)												
Eastbound Shared Left-Through-Right	B	11.3	0.004	C	22.8	0.021	B	11.3	0.004	C	19.6	0.017
Westbound Shared Left-Through-Right	C	15.8	0.022	B	11.0	0.013	C	16.6	0.024	B	11.1	0.013
Northbound Left	A	0.0	-	C	17.0	0.018	A	0.0	-	C	17.8	0.019
Southbound Left	C	17.1	0.025	B	13.7	0.013	C	17.5	0.026	B	12.7	0.011
Corporate Place and Driveway D (5)												
Eastbound Shared Left-Through-Right	C	21.5	0.254	D	27.9	0.242	C	20.9	0.247	D	26.2	0.228
Westbound Shared Left-Through-Right	B	14.6	0.050	B	10.8	0.052	B	13.7	0.045	B	10.9	0.053
Northbound Left	B	13.6	0.109	C	17.8	0.094	B	12.7	0.098	C	18.7	0.100
Southbound Left	C	17.2	0.055	B	13.9	0.051	C	17.5	0.057	B	12.8	0.045

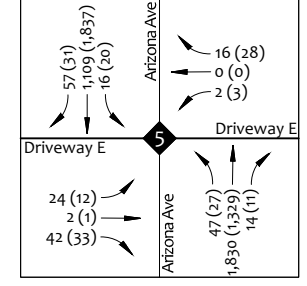
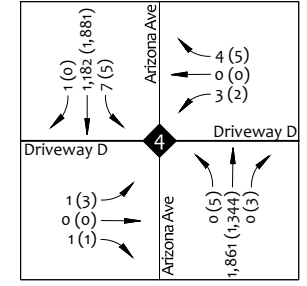
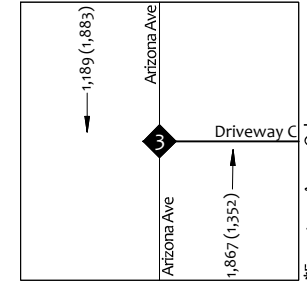
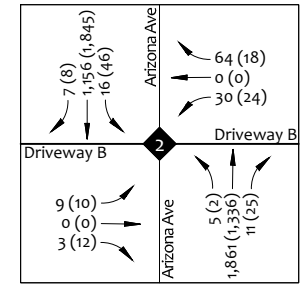
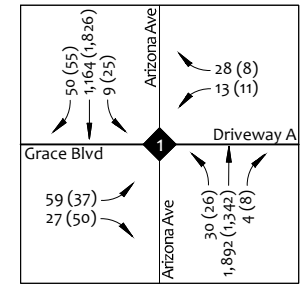
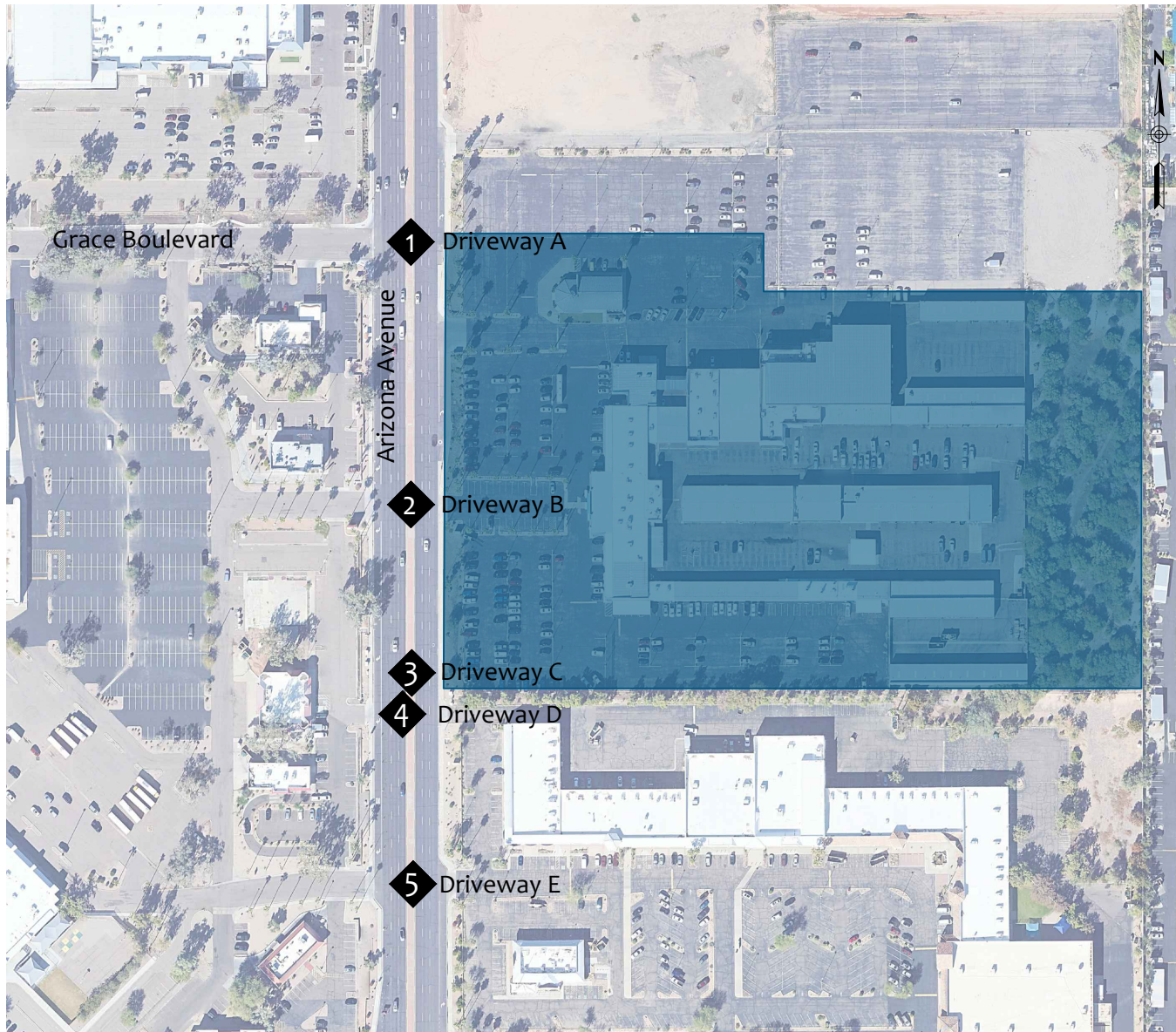


Legend

AM (PM) Peak Hour Traffic Volumes

◆ Intersection

FIGURE 12 | YEAR 2032 BACKGROUND TRAFFIC VOLUMES



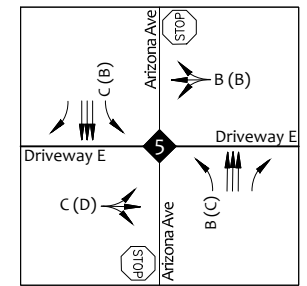
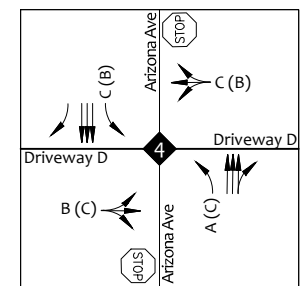
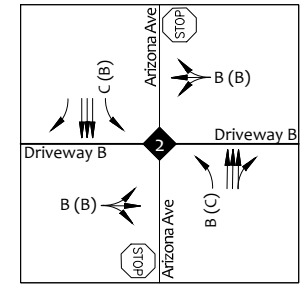
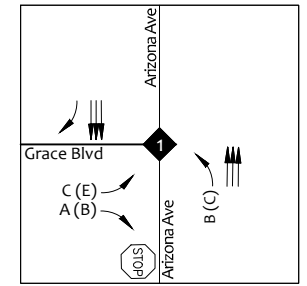
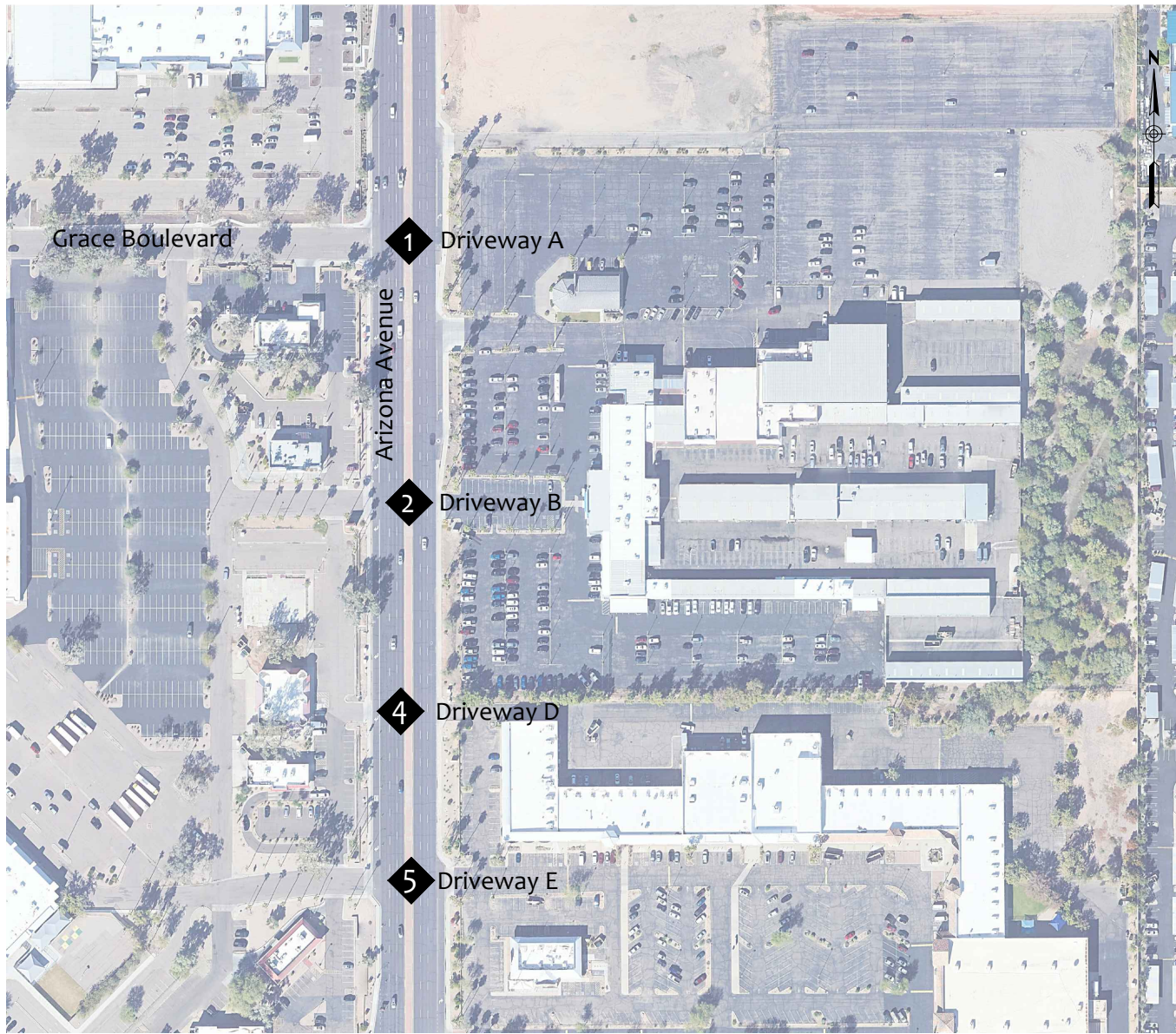
*Emergency Access Only

Legend

AM (PM) Peak Hour Traffic Volumes

X Intersection

FIGURE 13 | YEAR 2032 BUILD TRAFFIC VOLUMES



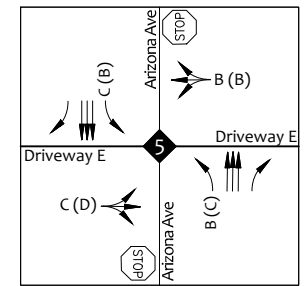
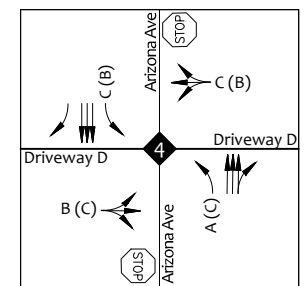
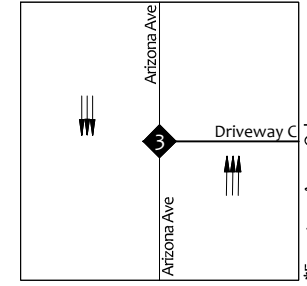
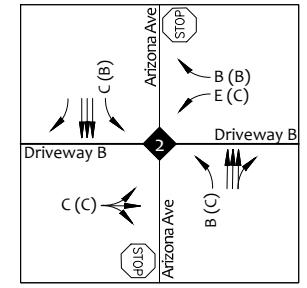
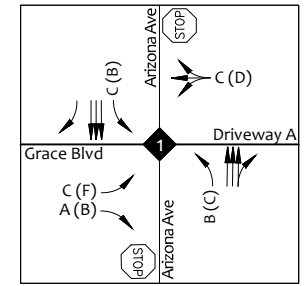
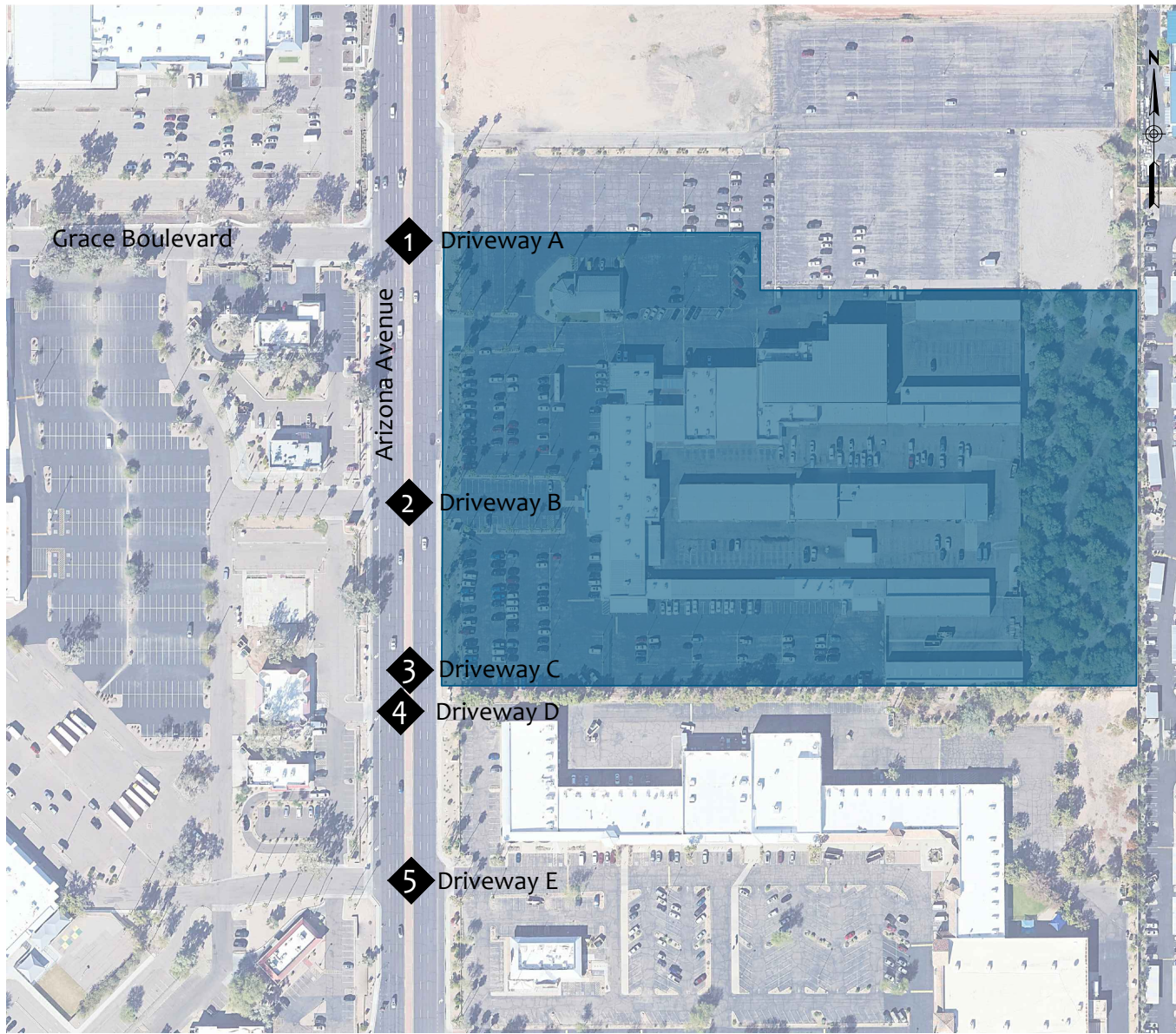
Legend

AM(PM) Peak Hour Capacity Analysis

◆ Intersection

↔ Lane Configuration

FIGURE 14 | YEAR 2032 NO BUILD CAPACITY ANALYSIS



*Emergency Access Only

- Legend**
- AM(PM) Peak Hour Capacity Analysis
 - ◆ Intersection
 - ↔ Lane Configuration

FIGURE 15 | YEAR 2032 BUILD CAPACITY ANALYSIS



8. RIGHT TURN LANE WARRANTS

Turn lanes allow vehicles exiting a roadway to slow to a reduced speed to execute a turn without impeding the main flow of traffic. This section evaluates the need for right turn deceleration lanes. The *City of Chandler Engineering & Design Standards Manual (EDSM)*, 2023 Edition, was utilized to determine the need for right turn deceleration lanes at the proposed site driveways.

Per Section 4.2.10.1 *Right Turn Deceleration Lanes*, right turn deceleration lanes are required at all cross streets and driveways when projected right turn volume exceeds 40 vehicles per hour. As shown in the year 2032 build traffic volumes (**Figure 13**), right turn volumes do not exceed 40 vehicles per hour. Therefore, right turn deceleration lanes are not required per City of Chandler standards.



9. RECOMMENDATIONS AND CONCLUSIONS

The proposed Toll Brothers Uptown development is generally located on the southeast corner of Arizona Avenue and Grace Boulevard in the City of Chandler, Arizona.

The proposed development will be comprised of 423 dwelling units. Of which, there will be 40 studio units, 208 one-bedroom units, 137 two-bedroom units, and 38 three-bedroom units.

The proposed Toll Brothers Uptown development is anticipated to be completed and open in the year 2027.

The following are the recommendations with the build out of the proposed Toll Brothers Uptown development:

- **Arizona Avenue and Grace Boulevard/Driveway A (1)**
 - Buildout of full-access point on the east leg of the intersection

- **Arizona Avenue and Driveway B (2)**
 - Buildout of full-access point on the east leg of the intersection

- **Arizona Avenue and Driveway C (3)**
 - Buildout of an emergency access only driveway.



Appendix A – Proposed Site Plan

BUILDING #4 UNIT MIX:

UNIT TYPE	GROSS LIV	UNITS	TOTAL LIV. AREA
UNIT AS STUDIO/1 BA	650 S.F.	16	10,400 S.F.
UNIT A1 1 BED/1 BA	787 S.F.	30	23,610 S.F.
UNIT A2 1 BED/1 BA	735 S.F.	22	16,170 S.F.
UNIT A6 1 BED/1 BA	752 S.F.	8	6,016 S.F.
UNIT B1 2 BED/2 BA	1,068 S.F.	8	8,544 S.F.
UNIT B2 2 BED/2 BA	1,153 S.F.	22	25,366 S.F.
UNIT B4 2 BD (DEN) 1 BA	1,067 S.F.	20	21,340 S.F.
UNIT B5 2 BED/2 BA	1,208 S.F.	8	9,664 S.F.
UNIT C1 3 BED/2 BA	1,450 S.F.	14	20,300 S.F.
TOTAL:		148 D.U.	141,410 S.F.
AVG. S.F.			955 S.F.
BUILDING NET RENTABLE S.F.			±132,402 S.F.
BUILDING GROSS S.F.			±190,281 S.F.

BUILDING #3 UNIT MIX:

UNIT TYPE	GROSS LIV	UNITS	TOTAL LIV. AREA
UNIT AS STUDIO/1 BA	650 S.F.	8	5,200 S.F.
UNIT A1 1 BED/1 BA	787 S.F.	6	4,722 S.F.
UNIT A2 1 BED/1 BA	735 S.F.	10	7,350 S.F.
UNIT A6 1 BED/1 BA	752 S.F.	0	0 S.F.
UNIT B1 2 BED/2 BA	1,068 S.F.	0	0 S.F.
UNIT B2 2 BED/2 BA	1,153 S.F.	3	3,459 S.F.
UNIT B4 2 BD (DEN) 1 BA	1,067 S.F.	8	8,536 S.F.
UNIT B5 2 BED/2 BA	1,208 S.F.	0	0 S.F.
UNIT C1 3 BED/2 BA	1,450 S.F.	7	10,150 S.F.
TOTAL:		42 D.U.	39,417 S.F.
AVG. S.F.			939 S.F.
BUILDING NET RENTABLE S.F.			±36,892 S.F.
BUILDING GROSS S.F.			±57,042 S.F.

BUILDING #2 UNIT MIX:

UNIT TYPE	GROSS LIV	UNITS	TOTAL LIV. AREA
UNIT AS STUDIO/1 BA	650 S.F.	0	0 S.F.
UNIT A1 1 BED/1 BA	787 S.F.	9	7,083 S.F.
UNIT A2 1 BED/1 BA	735 S.F.	18	13,230 S.F.
UNIT A6 1 BED/1 BA	752 S.F.	0	0 S.F.
UNIT B1 2 BED/2 BA	1,068 S.F.	4	4,272 S.F.
UNIT B2 2 BED/2 BA	1,153 S.F.	10	11,530 S.F.
UNIT B4 2 BD (DEN) 1 BA	1,067 S.F.	8	8,536 S.F.
UNIT B5 2 BED/2 BA	1,208 S.F.	0	0 S.F.
UNIT C1 3 BED/2 BA	1,450 S.F.	7	10,150 S.F.
TOTAL:		56 D.U.	54,801 S.F.
AVG. S.F.			979 S.F.
BUILDING NET RENTABLE S.F.			±51,323 S.F.
BUILDING GROSS S.F.			±77,978 S.F.

BUILDING #1 UNIT MIX:

UNIT TYPE	GROSS LIV	UNITS	TOTAL LIV. AREA
UNIT AS STUDIO/1 BA	650 S.F.	16	10,400 S.F.
UNIT A1 1 BED/1 BA	787 S.F.	33	25,971 S.F.
UNIT A2 1 BED/1 BA	735 S.F.	64	47,040 S.F.
UNIT A6 1 BED/1 BA	752 S.F.	8	6,016 S.F.
UNIT B1 2 BED/2 BA	1,068 S.F.	0	0 S.F.
UNIT B2 2 BED/2 BA	1,153 S.F.	30	34,590 S.F.
UNIT B4 2 BD (DEN) 1 BA	1,067 S.F.	0	0 S.F.
UNIT B5 2 BED/2 BA	1,208 S.F.	16	19,328 S.F.
UNIT C1 3 BED/2 BA	1,450 S.F.	10	14,500 S.F.
TOTAL:		177 D.U.	157,845 S.F.
AVG. S.F.			891 S.F.
BUILDING NET RENTABLE S.F.			±147,466 S.F.
BUILDING GROSS S.F.			±238,816 S.F.

PROJECT DATA:

SITE AREA: ±14.1 NET ACRES (±615,548 S.F.)
 ±15.0 GROSS ACRES (±652,729 S.F.)

ZONING: EXISTING: C-3
 PROPOSED: C-3 XX

APN NO.'S: MULTI-FAMILY

PROPOSED USE: MULTI-FAMILY

MAXIMUM DENSITY ALLOWED: 31 DU/NET AC

PROPOSED DENSITY: 31 DU/NET AC

MAXIMUM BUILDING HEIGHT: XXX

PROPOSED BUILDING HEIGHT: 4 STORY

48FT/51FT PARAPETS AND 54FT STAIR & ELEVATOR SHAFTS

LOT COVERAGE: 158,557 S.F. / 615,548 S.F. = 25.8%

UNIT MIX:

UNIT TYPE	GROSS LIV	UNITS	% TOTAL LIV. AREA
UNIT AS STUDIO/1 BA	650 S.F.	40	9.4%
UNIT A1 1 BED/1 BA	787 S.F.	78	18.4%
UNIT A2 1 BED/1 BA	735 S.F.	114	27.0%
UNIT A6 1 BED/1 BA	752 S.F.	16	3.8%
UNIT B1 2 BED/2 BA	1,068 S.F.	12	2.8%
UNIT B2 2 BED/2 BA	1,153 S.F.	65	15.4%
UNIT B4 2 BD (DEN) 1 BA	1,067 S.F.	36	8.5%
UNIT B5 2 BED/2 BA	1,208 S.F.	24	5.7%
UNIT C1 3 BED/2 BA	1,450 S.F.	38	9.0%
TOTAL:		423 D.U.	393,473 S.F.
AVG. S.F.			930 S.F.

PARKING:

REQUIRED:

STUDIO (1 PER UNIT)	40 P.S.
ONE BED UNIT (1.5 PER UNIT)	312 P.S.
TWO BED UNIT (2 PER UNIT)	274 P.S.
THREE BED UNIT (2.25 PER UNIT)	86 P.S.
GUEST PARKING (0.25 PER UNIT)	106 P.S.
TOTAL PROVIDED:	818 P.S.

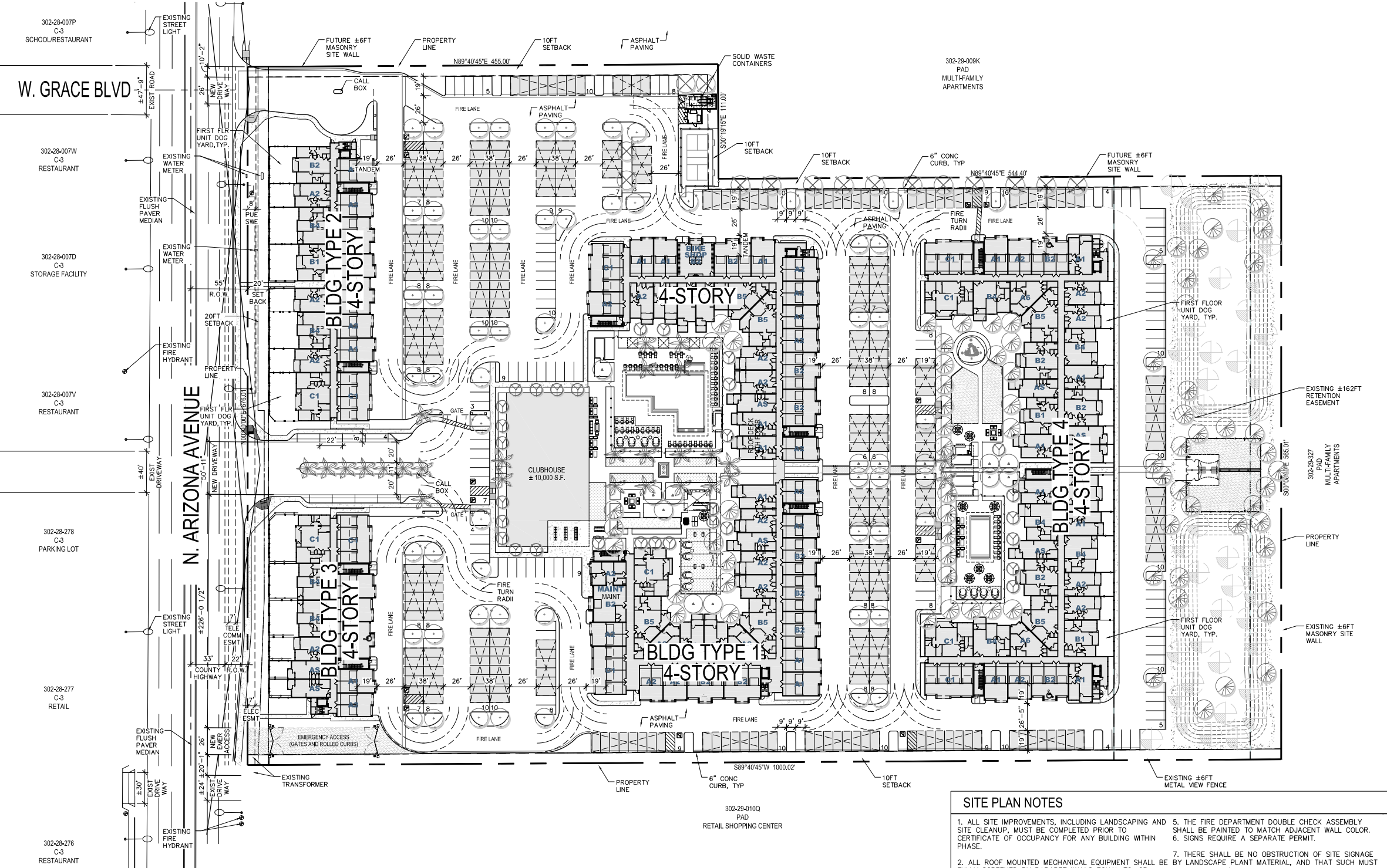
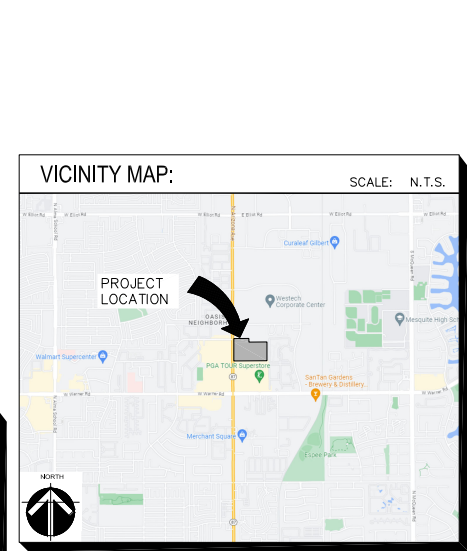
PROVIDED:

GARAGE PARKING	121 P.S.
TANDEM PARKING	121 P.S.
COVERED PARKING	302 P.S.
OPEN PARKING	203 P.S.
TOTAL PROVIDED:	747 P.S.

ACCESSIBLE PARKING: (14 STANDARD AND 2 VAN STALLS) 16 P.S.

SYMBOL SCHEDULE

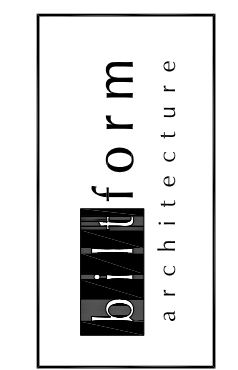
- 28" INSIDE TURNING RADIUS
- 48" OUTSIDE TURNING RADIUS
- IN ACCORDANCE WITH FIRE DEPT STANDARDS
- ACCESSIBLE PARKING SPACE
- PEDESTRIAN CROSSWALK
- FIRE HYDRANT



- SITE PLAN NOTES**
- ALL SITE IMPROVEMENTS, INCLUDING LANDSCAPING AND SITE CLEANUP, MUST BE COMPLETED PRIOR TO CERTIFICATE OF OCCUPANCY FOR ANY BUILDING WITHIN PHASE.
 - ALL ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY PARAPET WALLS EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT.
 - SOLID MASONRY WALLS AND GATES EQUAL TO, OR GREATER THAN, THE HIGHEST POINT ON THE MECHANICAL EQUIPMENT SHALL SCREEN ALL GROUND MOUNTED MECHANICAL EQUIPMENT.
 - ALL EXTERNAL LIGHTING SHALL BE LOCATED AND DESIGNED TO PREVENT RAYS FROM BEING DIRECTED OFF THE PROPERTY UPON WHICH THE LIGHTING IS LOCATED.
 - THE FIRE DEPARTMENT DOUBLE CHECK ASSEMBLY SHALL BE PAINTED TO MATCH ADJACENT WALL COLOR. SIGNS REQUIRE A SEPARATE PERMIT.
 - THERE SHALL BE NO OBSTRUCTION OF SITE SIGNAGE BY LANDSCAPE PLANT MATERIAL, AND THAT SUCH MUST BE RELOCATED/CORRECTED BEFORE THE FIELD INSPECTION WILL ACCEPT/PASS THE SIGN IN THE FIELD OR ISSUE A CERTIFICATE OF OCCUPANCY FOR THE PROJECT.
 - ALL TRANSFORMER BOXES, METER PANELS AND ELECTRIC EQUIPMENT, BACK-FLOW DEVICES AND ANY OTHER UTILITY EQUIPMENT, NOT ABLE TO BE SCREENED BY LANDSCAPING OR WALLS SHALL BE PAINTED TO MATCH THE BUILDING COLOR.
 - ALL WALLS OVER 7FT IN HEIGHT, SITE LIGHTING, SIGNAGE, RAMADAS AND SHADE STRUCTURES REQUIRE SEPARATE SUBMITTAL AND PERMITS.

SITE PLAN "L"
 SCALE: 1' = 0" = 50'

NORTH



biltform architecture group, inc.
 11460 north cave creek road, suite 11
 phoenix, arizona 85020
 Phone 602.285.9200 Fax 602.285.9229

PRELIMINARY NOT FOR CONSTRUCTION
 EXPIRES 9/30/22

CORPORATE PLACE ON AZ AVE
 Apartment Homes
 CHANDLER, ARIZONA
TOLL BROTHERS APARTMENT LIVING
 8767 E. VIA DE VENTURA, SUITE 390, SCOTTSDALE, AZ. 85258

REVISIONS:

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

JOB NO: 21-040
 DATE: JUNE 09, 2021
 SCALE:
 SHEET NO:

A1.0
 © BILTFORM ARCHITECTURE GROUP, INC.



Appendix B – Parcel Information

302-29-009E

Commercial Parcel

This is a Commercial parcel located at [2121 N ARIZONA AVE CHANDLER 85225](#). The current owner is EARNHARDT ARIZONA PROPERTIES LLC. Its current year full cash value is \$4,964,764.

MAPS PICTOMETRY \$ VIEW/PAY TAX BILL DEED

OWNER VALUATIONS ADDITIONAL INFO MAP FERRET

SIMILAR PARCELS REGISTER RENTAL PRINT DETAILS

PROPERTY INFORMATION



[2121 N ARIZONA AVE CHANDLER 85225](#)

MCR #

Description

S 550F OF N 1100F OF S 2055F OF W 1055F SW4 SEC 15 EX E 161.88F TH/OF & EX W 55F RD

Lat/Long

|

Lot Size

460,952 sq ft.

Zoning

C-3

Lot #

High School District

GILBERT UNIFIED #41

Elementary School District

GILBERT UNIFIED SCHOOL DISTRICT

Local Jurisdiction

CHANDLER

S/T/R

15 1S 5E

Market

01/012

Area/Neighborhood

**Subdivision (0
Parcels)**

OWNER INFORMATION



[EARNHARDT ARIZONA PROPERTIES LLC](#)

Mailing Address 7300 W ORCHID LN, CHANDLER, AZ 85226
Deed Number
Last Deed Date
Sale Date n/a
Sale Price n/a

VALUATION INFORMATION



We provide valuation information for the past 5 years. For mobile display, we only show 1 year of valuation information. Should you need more data, please look at our [data sales](#).

The Valuation Information displayed below may not reflect the taxable value used on the tax bill due to any special valuation relief program. [CLICK HERE TO PAY YOUR TAXES OR VIEW YOUR TAX BILL](#)

Tax Year	2024	2023	2022	2021
Full Cash Value ⓘ	\$4,964,764	\$6,799,400	\$5,831,800	\$5,852,100
Limited Value ⓘ	\$4,964,764	\$5,079,047	\$4,837,188	\$4,606,846
Legal Class	1.12	1.12	1.12	1.12
Description	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P
Assessment Ratio	16.5%	17%	17.5%	18%
Assessed LPV	\$819,186	\$863,438	\$846,508	\$829,232
Property Use Code	1820	1820	1820	1820
PU Description	Auto Sales and Service	Auto Sales and Service	Auto Sales and Service	Auto Sales and Service
Tax Area Code	410300	410300	410300	410300
Valuation Source	Notice	Notice	Notice	Notice

ADDITIONAL PROPERTY INFORMATION



Additional property data.

Description	Imp #	Occupancy	Rank	CCI	Age	Sq Ft.
Automobile Showroom	000101	303	2	C	40	18,263
Office Building	000102	344	2	C	37	2,320
Service Repair Garage	000103	528	2	S	36	10,920
Service Repair Garage	000201	528	2	S	40	5,568
Service Repair Garage	000202	528	2	S	27	3,874

Description	Imp #	Occupancy	Rank	CCI	Age	Sq Ft.
Service Repair Garage	000301	528	2	S	37	20,600
Car Wash - Drive Thru	000302	435	2	C	24	1,600
Automobile Showroom	000401	303	2	C	35	3,289
Site Improvements	000501	163	2	D	37	1
Service Repair Garage	000601	528	2	S	21	5,148
Golf Starter Booth	000701	183	2	S	40	120

MAP FERRET MAPS



Mapferret maps, also known as MapId maps, pdf maps, or output maps are now available here without having to search.

▶ [Parcel Maps \(2\)](#)

▶ [Book/Map Maps \(14\)](#)

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The Assessor's Office has compiled information on this website that it uses to identify, classify, and value real and personal property. Please contact the Maricopa County S.T.A.R. Center at (602) 506-3406 if you believe any information is incomplete, out of date, or incorrect so that appropriate corrections can be addressed. Please note that a statutory process is also available to correct errors pursuant to Arizona Revised Statutes 42-16254.

The Assessor does not guarantee that any information provided on this website is accurate, complete, or current. In many instances, the Assessor has gathered information from independent sources and made it available on this site, and the original information may have contained errors and omissions. Errors and omissions may also have occurred in the process of gathering, interpreting, and reporting the information. Information on the website is not updated in "real time". In addition, users are cautioned that the process used on this site to illustrate the boundaries of the adjacent parcels is not always consistent with the recorded documents for such parcels. The parcel boundaries depicted on this site are for illustrative purposes only, and the exact relationship of adjacent parcels should be independently researched and verified. The information provided on this site is not the equivalent of a title report or a real estate survey. Users should independently research, investigate and verify all information before relying on it or in the preparation of legal documents.

By using this website, you acknowledge having read the above and waive any right you may have to claim against Maricopa County, its officers, employees, and contractors arising out of my reliance on or the use of the information provided on this website.

302-29-009G

Commercial Parcel

This is a Commercial parcel located at [2121 N ARIZONA AVE CHANDLE CHANDLER 85225](#). The current owner is EARNHARDT ARIZONA PROPERTIES LLC. Its current year full cash value is \$26.

 MAPS

 PICTOMETRY

 VIEW/PAY TAX BILL

 DEED

 OWNER

 VALUATIONS

 MAP FERRET

 SIMILAR PARCELS

 REGISTER RENTAL

 PRINT DETAILS

PROPERTY INFORMATION



[2121 N ARIZONA AVE CHANDLE CHANDLER 85225](#)

MCR

Description

E 161.88F OF S 550F OF N 1100F OF S 2055F OF W 1055F SW4 SEC 15

Lat/Long

|

Lot Size

81,941 sq ft.

Zoning

C-3

Lot

High School District

GILBERT UNIFIED #41

Elementary School District

GILBERT UNIFIED SCHOOL DISTRICT

Local Jurisdiction

CHANDLER

S/T/R

15 1S 5E

Market

01/012

Area/Neighborhood

Subdivision (0 Parcels)

OWNER INFORMATION



[EARNHARDT ARIZONA PROPERTIES LLC](#)

Mailing Address

7300 W ORCHID LN, CHANDLER, AZ 85226

Deed Number
Last Deed Date
Sale Date n/a
Sale Price n/a

VALUATION INFORMATION



We provide valuation information for the past 5 years. For mobile display, we only show 1 year of valuation information. Should you need more data, please look at our [data sales](#).

The Valuation Information displayed below may not reflect the taxable value used on the tax bill due to any special valuation relief program. [CLICK HERE TO PAY YOUR TAXES OR VIEW YOUR TAX BILL](#)

Tax Year	2024	2023	2022	2021
Full Cash Value [?]	\$26	\$458,400	\$346,100	\$346,100
Limited Value [?]	\$26	\$226,816	\$216,015	\$205,729
Legal Class	1.12	1.12	1.12	1.12
Description	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P
Assessment Ratio	16.5%	17%	17.5%	18%
Assessed LPV	\$4	\$38,559	\$37,803	\$37,031
Property Use Code	1074	1074	1074	1074
PU Description	Misc. Commercial	Misc. Commercial	Misc. Commercial	Misc. Commercial
Tax Area Code	410300	410300	410300	410300
Valuation Source	Notice	Notice	Notice	Notice

MAP FERRET MAPS



Mapferret maps, also known as Mapld maps, pdf maps, or output maps are now available here without having to search.

[▶ Parcel Maps \(2\)](#)

[▶ Book/Map Maps \(14\)](#)

CAUTION! USERS SHOULD INDEPENDENTLY RESEARCH AND VERIFY INFORMATION ON THIS WEBSITE BEFORE RELYING ON IT.

The Assessor's Office has compiled information on this website that it uses to identify, classify, and value real and personal property. Please contact the Maricopa County S.T.A.R. Center at (602) 506-3406 if you believe any information is incomplete, out of date, or incorrect so that appropriate corrections can be addressed. Please note that a statutory process is also available to correct errors pursuant to Arizona Revised Statutes 42-16254.

The Assessor does not guarantee that any information provided on this website is accurate, complete, or current. In many instances, the Assessor has gathered information from independent sources and made it available on this site, and the original information may have contained errors and omissions. Errors and omissions may also have occurred in the process of gathering, interpreting, and reporting the information. Information on the website is not updated in "real time". In addition, users are cautioned that the process used on this site to illustrate the boundaries of the adjacent parcels is not always consistent with the recorded documents for such parcels. The parcel boundaries depicted on this site are for illustrative purposes only, and the exact relationship of adjacent parcels should be independently researched and verified. The information provided on this site is not the equivalent of a title report or a real estate survey. Users should independently research, investigate and verify all information before relying on it or in the preparation of legal documents.

By using this website, you acknowledge having read the above and waive any right you may have to claim against Maricopa County, its officers, employees, and contractors arising out of my reliance on or the use of the information provided on this website.

302-29-009L

Commercial Parcel

This is a Commercial parcel The current owner is EARNHARDT ARIZONA PROPERTIES LLC. Its current year full cash value is \$368,700.

- MAPS
- PICTOMETRY
- VIEW/PAY TAX BILL
- DEED
- OWNER
- VALUATIONS
- MAP FERRET
- SIMILAR PARCELS
- REGISTER RENTAL
- PRINT DETAILS

PROPERTY INFORMATION



MCR #

Description

N 160.67F OF S 1665.67F OF E 1022F OF W 1055F OF SW4 SEC 15 P/F 16-0448116 EX W 22F RD P/F 85-319670 AND EX POR SW4 SEC 15 DAF COM SW COR SD SEC TH N 1631.03F TH E 55F POB TH N 391.01F TH E 1000.02F TH S 502.01F TH W 544.4F TH N 111F W 455F P/F 22-0909494 PER VESTING 16-0448116

Lat/Long

|

Lot Size

101,568 sq ft.

Zoning

C-3

Lot #

High School District

GILBERT UNIFIED #41

Elementary School District

GILBERT UNIFIED SCHOOL DISTRICT

Local Jurisdiction

CHANDLER

S/T/R

15 1S 5E

Market

01/012

Area/Neighborhood

Subdivision (0 Parcels)

OWNER INFORMATION



[EARNHARDT ARIZONA PROPERTIES LLC](#)

Mailing Address 7300 W ORCHID LN, CHANDLER, AZ 85226

Deed Number

Last Deed Date

Sale Date n/a

Sale Price n/a

VALUATION INFORMATION



We provide valuation information for the past 5 years. For mobile display, we only show 1 year of valuation information. Should you need more data, please look at our [data sales](#).

The Valuation Information displayed below may not reflect the taxable value used on the tax bill due to any special valuation relief program. [CLICK HERE TO PAY YOUR TAXES OR VIEW YOUR TAX BILL](#)

Tax Year	2024	2023
Full Cash Value ?	\$368,700	\$292,434
Limited Value ?	\$202,785	\$173,144
Legal Class	1.12	1.12
Description	COMMERCIAL / OTHER R/P	COMMERCIAL / OTHER R/P
Assessment Ratio	16.5%	17%
Assessed LPV	\$33,459	\$29,434
Property Use Code	1074	1074
PU Description	Misc. Commercial	Misc. Commercial
Tax Area Code	410300	410300
Valuation Source	Notice	Notice

MAP FERRET MAPS



Mapferret maps, also known as MapId maps, pdf maps, or output maps are now available here without having to search.

[Parcel Maps \(1\)](#)

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Appendix C – Traffic Count Data

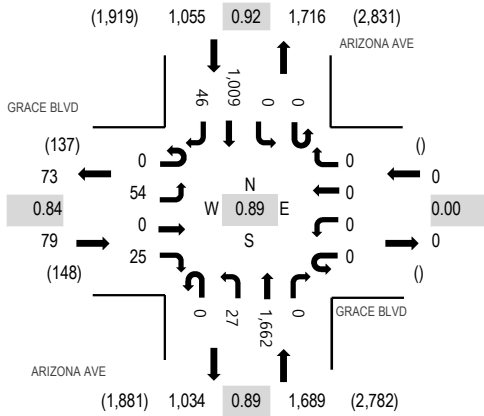
Location: 1 ARIZONA AVE & GRACE BLVD AM

Date: Thursday, September 7, 2023

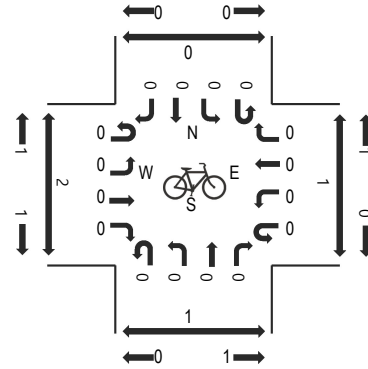
Peak Hour: 07:15 AM - 08:15 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

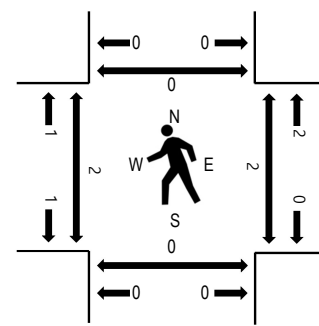
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

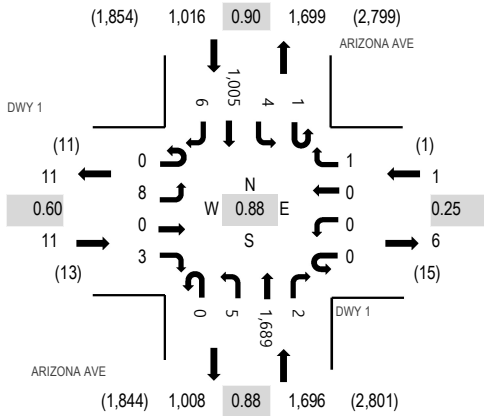


Note: Total study counts contained in parentheses.

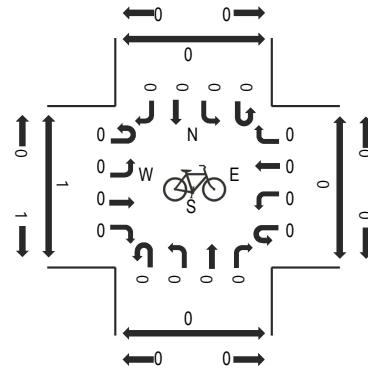
Traffic Counts - Motorized Vehicles

Interval Start Time	GRACE BLVD Eastbound				GRACE BLVD Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	21	0	6	0	0	0	0	0	8	242	0	0	0	210	5	492	2,607	0	1	0	0
7:15 AM	0	10	0	5	0	0	0	0	0	3	360	0	0	0	234	15	627	2,823	1	0	0	0
7:30 AM	0	16	0	6	0	0	0	0	0	9	396	0	0	0	265	6	698	2,731	0	2	0	0
7:45 AM	0	20	0	7	0	0	0	0	0	6	469	0	0	0	283	5	790	2,566	0	0	0	0
8:00 AM	0	8	0	7	0	0	0	0	0	9	437	0	0	0	227	20	708	2,242	1	0	0	0
8:15 AM	0	13	0	3	0	0	0	0	0	4	315	0	0	0	192	8	535		0	1	0	0
8:30 AM	0	10	0	8	0	0	0	0	0	9	268	0	0	0	230	8	533		0	3	0	0
8:45 AM	0	5	0	3	0	0	0	0	0	6	241	0	0	0	195	16	466		0	1	0	0
Count Total	0	103	0	45	0	0	0	0	0	54	2,728	0	0	0	1,836	83	4,849		2	8	0	0
Peak Hour	0	54	0	25	0	0	0	0	0	27	1,662	0	0	0	1,009	46	2,823		2	2	0	0

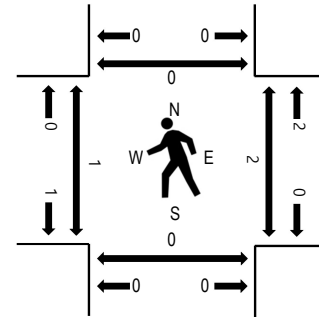
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

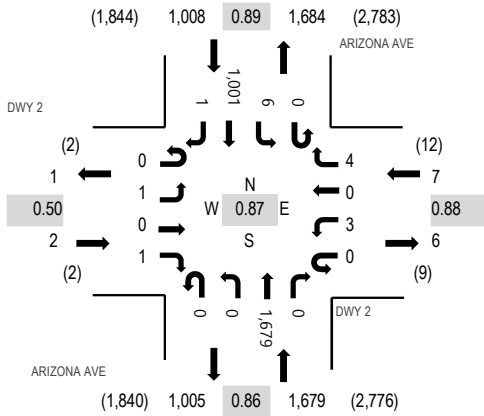


Note: Total study counts contained in parentheses.

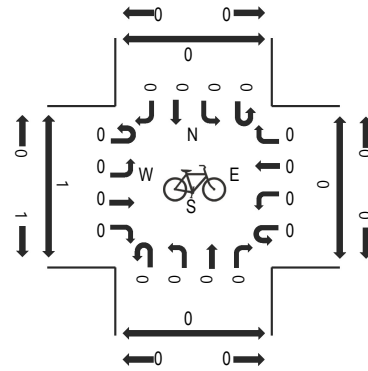
Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 1 Eastbound				DWY 1 Westbound				ARIZONA AVE Northbound			ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings					
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru			Right	West	East	South	North	
7:00 AM	0	0	0	0	0	0	0	0	0	0	254	0	0	0	0	214	0	468	2,515	0	1	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	1	371	0	1	0	0	234	2	609	2,724	0	0	0	0
7:30 AM	0	3	0	0	0	0	0	0	0	2	396	1	0	1	0	263	0	666	2,635	0	1	0	0
7:45 AM	0	4	0	1	0	0	0	0	0	1	482	1	0	1	0	280	2	772	2,480	0	1	0	0
8:00 AM	0	1	0	2	0	0	0	1	0	1	440	0	0	2	0	228	2	677	2,154	1	0	0	0
8:15 AM	0	0	0	1	0	0	0	0	0	0	320	2	0	2	0	195	0	520		0	0	0	0
8:30 AM	0	1	0	0	0	0	0	0	0	0	275	1	0	1	0	233	0	511		0	2	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	250	3	0	0	0	193	0	446		0	0	0	0
Count Total	0	9	0	4	0	0	0	1	0	5	2,788	8	1	7	1,840	6	4,669		1	5	0	0	
Peak Hour	0	8	0	3	0	0	0	1	0	5	1,689	2	1	4	1,005	6	2,724		1	2	0	0	

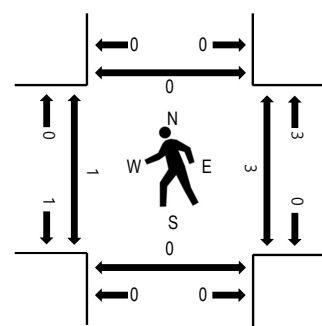
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

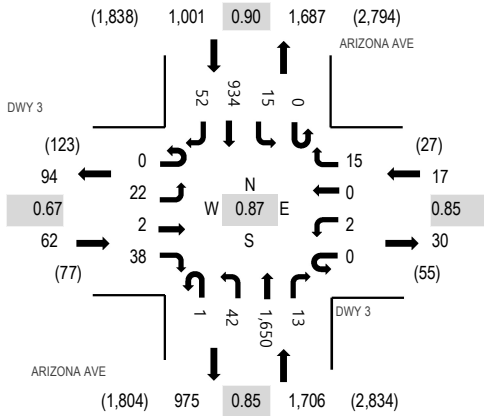


Note: Total study counts contained in parentheses.

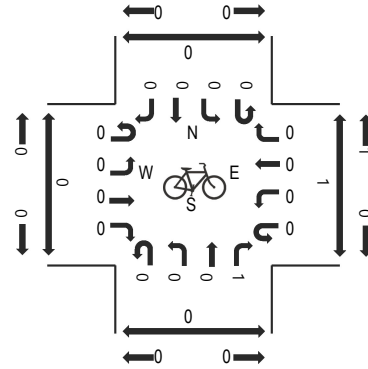
Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 2 Eastbound				DWY 2 Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	0	0	0	1	0	1	0	0	254	0	0	0	208	0	464	2,507	0	2	0	0
7:15 AM	0	0	0	1	0	0	0	1	0	0	373	0	0	0	235	0	610	2,696	0	0	0	0
7:30 AM	0	0	0	0	0	1	0	1	0	0	393	0	0	1	266	0	662	2,605	0	2	0	0
7:45 AM	0	0	0	0	0	1	0	1	0	0	487	0	0	5	276	1	771	2,451	0	1	0	0
8:00 AM	0	1	0	0	0	1	0	1	0	0	426	0	0	0	224	0	653	2,127	1	0	0	0
8:15 AM	0	0	0	0	0	0	0	1	0	1	316	0	0	1	200	0	519		0	0	0	0
8:30 AM	0	0	0	0	0	0	0	1	0	0	275	0	0	2	230	0	508		0	0	0	0
8:45 AM	0	0	0	0	0	1	0	0	0	0	251	0	0	0	195	0	447		0	0	0	0
Count Total	0	1	0	1	0	5	0	7	0	1	2,775	0	0	9	1,834	1	4,634		1	5	0	0
Peak Hour	0	1	0	1	0	3	0	4	0	0	1,679	0	0	6	1,001	1	2,696		1	3	0	0

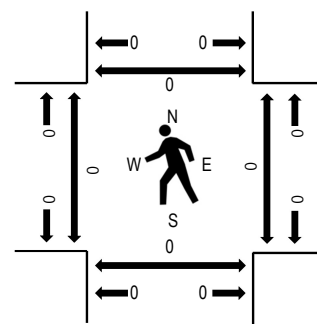
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 3 Eastbound				DWY 3 Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	0	2	0	0	0	3	0	2	247	3	0	3	203	4	467	2,587	0	1	0	0
7:15 AM	0	2	0	6	0	1	0	3	0	6	366	4	0	3	225	10	626	2,786	0	0	0	0
7:30 AM	0	7	1	15	0	0	0	5	0	14	381	2	0	5	247	13	690	2,698	0	0	0	0
7:45 AM	0	9	1	10	0	1	0	2	1	17	481	5	0	4	255	18	804	2,526	0	0	0	0
8:00 AM	0	4	0	7	0	0	0	5	0	5	422	2	0	3	207	11	666	2,189	0	0	0	0
8:15 AM	0	1	0	4	0	0	0	1	0	6	323	1	0	5	193	4	538		0	0	0	0
8:30 AM	0	0	0	4	0	0	0	2	0	3	272	5	0	0	229	3	518		0	0	0	0
8:45 AM	0	0	0	4	0	2	0	2	0	4	256	6	0	2	188	3	467		0	0	0	0
Count Total	0	23	2	52	0	4	0	23	1	57	2,748	28	0	25	1,747	66	4,776		0	1	0	0
Peak Hour	0	22	2	38	0	2	0	15	1	42	1,650	13	0	15	934	52	2,786		0	0	0	0

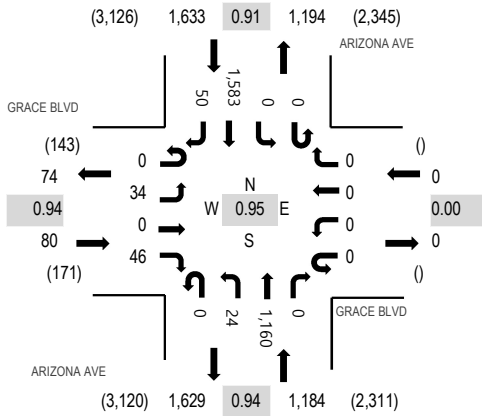
Location: 1 ARIZONA AVE & GRACE BLVD PM

Date: Thursday, September 7, 2023

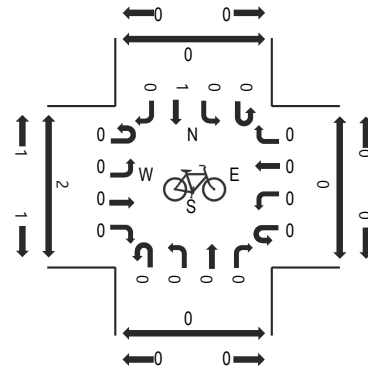
Peak Hour: 04:45 PM - 05:45 PM

Peak 15-Minutes: 05:15 PM - 05:30 PM

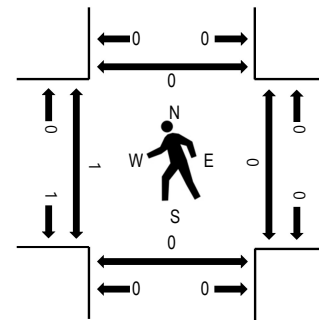
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

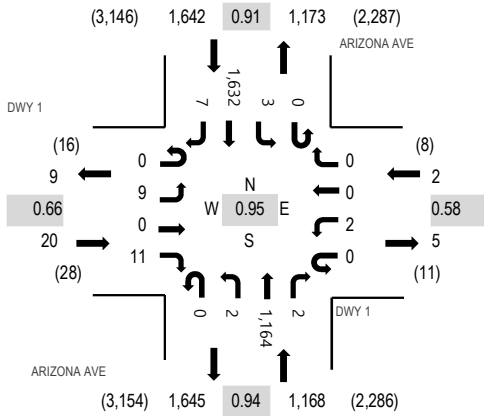


Note: Total study counts contained in parentheses.

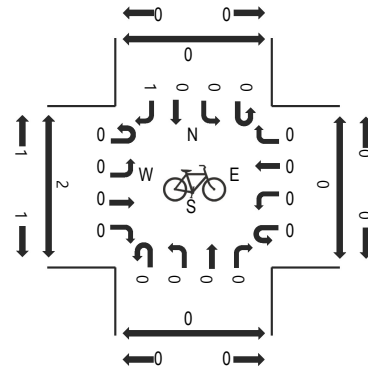
Traffic Counts - Motorized Vehicles

Interval Start Time	GRACE BLVD Eastbound				GRACE BLVD Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	13	0	11	0	0	0	0	1	4	277	0	0	0	400	12	718	2,784	0	3	0	0
4:15 PM	0	10	0	9	0	0	0	0	0	7	288	0	0	0	390	10	714	2,816	0	1	0	0
4:30 PM	0	9	0	15	0	0	0	0	0	2	277	0	0	0	350	14	667	2,864	1	1	0	0
4:45 PM	0	10	0	4	0	0	0	0	0	5	312	0	0	0	340	14	685	2,897	0	0	0	0
5:00 PM	0	10	0	14	0	0	0	0	0	4	295	0	0	0	419	8	750	2,824	0	0	0	0
5:15 PM	0	8	0	12	0	0	0	0	0	8	287	0	0	0	434	13	762		0	0	0	0
5:30 PM	0	6	0	16	0	0	0	0	0	7	266	0	0	0	390	15	700		1	0	0	0
5:45 PM	0	14	0	10	0	0	0	0	0	8	263	0	0	0	305	12	612		0	0	0	0
Count Total	0	80	0	91	0	0	0	0	1	45	2,265	0	0	0	3,028	98	5,608		2	5	0	0
Peak Hour	0	34	0	46	0	0	0	0	0	24	1,160	0	0	0	1,583	50	2,897		1	0	0	0

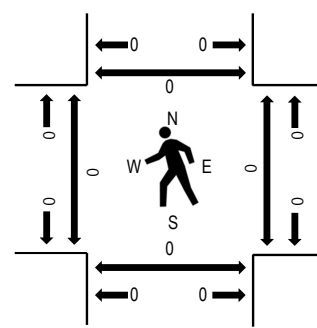
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 1 Eastbound				DWY 1 Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	0	0	0	1	0	1	0	0	277	2	0	0	407	1	689	2,706	0	0	0	0
4:15 PM	0	1	0	2	0	2	0	1	1	1	291	0	0	0	410	1	710	2,757	0	0	0	0
4:30 PM	0	0	0	2	0	1	0	0	0	0	276	3	0	1	361	1	645	2,789	1	1	0	0
4:45 PM	0	0	0	2	0	1	0	0	0	0	314	0	0	1	343	1	662	2,832	0	0	0	0
5:00 PM	0	3	0	5	0	0	0	0	0	2	294	1	0	1	432	2	740	2,762	0	0	0	0
5:15 PM	0	4	0	2	0	1	0	0	0	0	285	0	0	0	447	3	742	2,832	0	0	0	0
5:30 PM	0	2	0	2	0	0	0	0	0	0	271	1	0	1	410	1	688	2,762	0	0	0	0
5:45 PM	0	1	0	2	0	0	0	0	0	1	266	0	0	0	320	2	592	2,762	0	0	0	0
Count Total	0	11	0	17	0	6	0	2	1	4	2,274	7	0	4	3,130	12	5,468	2,832	1	1	0	0
Peak Hour	0	9	0	11	0	2	0	0	0	2	1,164	2	0	3	1,632	7	2,832	2,832	0	0	0	0

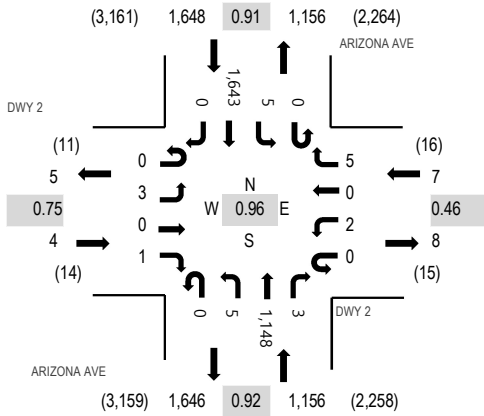
Location: 3 ARIZONA AVE & DWY 2 PM

Date: Thursday, September 7, 2023

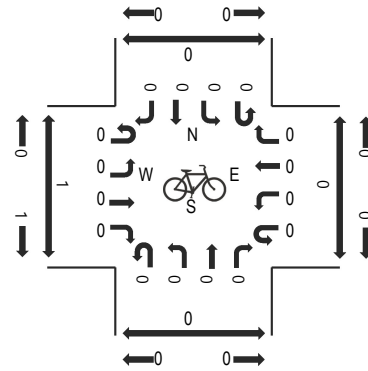
Peak Hour: 04:45 PM - 05:45 PM

Peak 15-Minutes: 05:15 PM - 05:30 PM

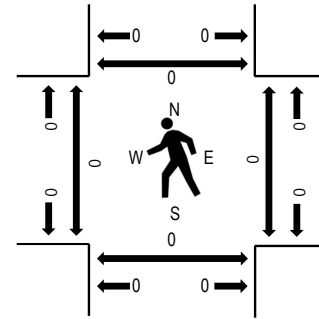
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 2 Eastbound				DWY 2 Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	1	0	1	0	3	0	3	0	1	269	1	0	1	408	1	689	2,706	0	0	0	0
4:15 PM	0	0	0	3	0	1	0	1	0	0	286	0	1	2	413	0	707	2,742	1	0	0	0
4:30 PM	0	2	0	1	0	0	0	1	0	0	276	0	0	3	359	2	644	2,771	0	1	0	0
4:45 PM	0	1	0	0	0	0	0	2	0	1	314	1	0	1	346	0	666	2,815	0	0	0	0
5:00 PM	0	2	0	0	0	0	0	2	0	0	284	0	0	1	436	0	725	2,743	0	0	0	0
5:15 PM	0	0	0	1	0	0	0	1	0	1	279	1	0	2	451	0	736	2,815	0	0	0	0
5:30 PM	0	0	0	0	0	2	0	0	0	3	271	1	0	1	410	0	688	2,743	0	0	0	0
5:45 PM	0	1	0	1	0	0	0	0	0	2	267	0	0	0	323	0	594	2,771	0	0	0	0
Count Total	0	7	0	7	0	6	0	10	0	8	2,246	4	1	11	3,146	3	5,449	27,771	1	1	0	0
Peak Hour	0	3	0	1	0	2	0	5	0	5	1,148	3	0	5	1,643	0	2,815	2,815	0	0	0	0

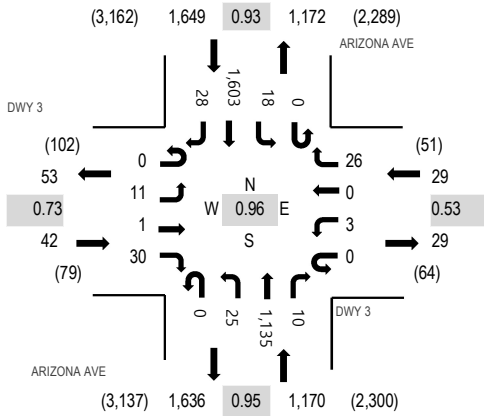
Location: 4 ARIZONA AVE & DWY 3 PM

Date: Thursday, September 7, 2023

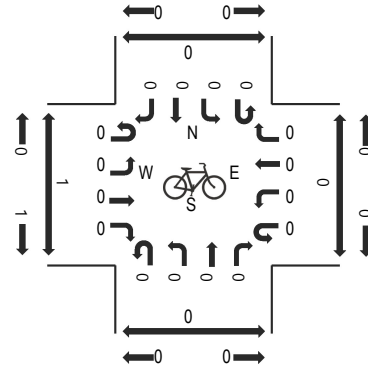
Peak Hour: 04:45 PM - 05:45 PM

Peak 15-Minutes: 05:00 PM - 05:15 PM

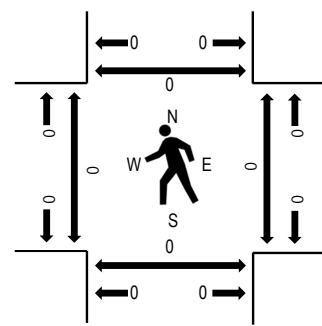
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	DWY 3 Eastbound				DWY 3 Westbound				ARIZONA AVE Northbound				ARIZONA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	3	1	3	0	2	0	3	2	5	268	6	0	3	400	5	701	2,775	0	0	0	0
4:15 PM	0	6	0	8	0	3	0	4	0	5	278	3	0	4	403	11	725	2,829	1	0	0	0
4:30 PM	0	2	1	6	0	1	0	3	1	10	278	6	0	3	352	7	670	2,854	0	1	0	0
4:45 PM	0	5	0	6	0	1	0	5	0	8	302	4	0	3	338	7	679	2,890	0	0	0	0
5:00 PM	0	2	0	7	0	2	0	13	0	10	278	4	0	5	424	10	755	2,817	0	0	0	0
5:15 PM	0	1	1	13	0	0	0	6	0	3	281	0	0	5	437	3	750		0	0	0	0
5:30 PM	0	3	0	4	0	0	0	2	0	4	274	2	0	5	404	8	706		0	0	0	0
5:45 PM	0	2	0	5	0	2	0	4	0	1	266	1	0	7	313	5	606		0	0	0	0
Count Total	0	24	3	52	0	11	0	40	3	46	2,225	26	0	35	3,071	56	5,592		1	1	0	0
Peak Hour	0	11	1	30	0	3	0	26	0	25	1,135	10	0	18	1,603	28	2,890		0	0	0	0

Site Code: 5
ARIZONA AVE S.O GRACE BLVD

Start Time	07-Sep-23 Thu	NB	SB	Total
12:00 AM		75	85	160
01:00		62	70	132
02:00		62	76	138
03:00		81	85	166
04:00		233	190	423
05:00		430	319	749
06:00		663	502	1165
07:00		1504	998	2502
08:00		1288	856	2144
09:00		813	706	1519
10:00		717	753	1470
11:00		869	888	1757
12:00 PM		931	974	1905
01:00		845	946	1791
02:00		910	1103	2013
03:00		956	1152	2108
04:00		1161	1527	2688
05:00		1126	1619	2745
06:00		884	952	1836
07:00		627	692	1319
08:00		507	472	979
09:00		369	367	736
10:00		230	194	424
11:00		133	120	253
Total		15476	15646	31122
Percent		49.7%	50.3%	
AM Peak	-	07:00	07:00	07:00
Vol.	-	1504	998	2502
PM Peak	-	16:00	17:00	17:00
Vol.	-	1161	1619	2745
Grand Total		15476	15646	31122
Percent		49.7%	50.3%	
ADT		ADT 31,122	AADT 31,122	



Appendix D – Existing Capacity Analysis

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↵	↶	↵	↑↑↑	↑↑↑	↶
Traffic Vol, veh/h	54	25	27	1662	1009	46
Future Vol, veh/h	54	25	27	1662	1009	46
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	61	28	30	1867	1134	52

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1941	567	1185	0	-	0
Stage 1	1134	-	-	-	-	-
Stage 2	808	-	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-	-
Pot Cap-1 Maneuver	*399	*793	492	-	-	-
Stage 1	*364	-	-	-	-	-
Stage 2	*692	-	-	-	-	-
Platoon blocked, %	0	0	0	-	-	-
Mov Cap-1 Maneuver	*374	*793	492	-	-	-
Mov Cap-2 Maneuver	*308	-	-	-	-	-
Stage 1	*341	-	-	-	-	-
Stage 2	*692	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v	16.41	0.2	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	492	-	308	793	-	-
HCM Lane V/C Ratio	0.062	-	0.197	0.035	-	-
HCM Control Delay (s/veh)	12.8	-	19.5	9.7	-	-
HCM Lane LOS	B	-	C	A	-	-
HCM 95th %tile Q(veh)	0.2	-	0.7	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	8	0	3	0	0	1	5	1689	2	5	1005	6
Future Vol, veh/h	8	0	3	0	0	1	5	1689	2	5	1005	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	88	88	88	88	88	88	88	88	88	88	88	88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	9	0	3	0	0	1	6	1919	2	6	1142	7

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1933	3086	571	2400	3092	961	1149	0	0	1922	0	0
Stage 1	1153	1153	-	1932	1932	-	-	-	-	-	-	-
Stage 2	779	1933	-	468	1160	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*494	31	*786	*173	31	*666	528	-	-	302	-	-
Stage 1	*325	422	-	*154	242	-	-	-	-	-	-	-
Stage 2	*683	242	-	*807	418	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*479	30	*786	*167	30	*666	528	-	-	302	-	-
Mov Cap-2 Maneuver	*479	30	-	*167	30	-	-	-	-	-	-	-
Stage 1	*319	414	-	*153	240	-	-	-	-	-	-	-
Stage 2	*675	239	-	*788	410	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.88		10.42	0.04	0.08
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	528	-	-	536	666	302	-
HCM Lane V/C Ratio	0.011	-	-	0.023	0.002	0.019	-
HCM Control Delay (s/veh)	11.9	-	-	11.9	10.4	17.1	-
HCM Lane LOS	B	-	-	B	B	C	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0.1	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	1	0	1	3	0	4	0	1679	0	6	1001	1
Future Vol, veh/h	1	0	1	3	0	4	0	1679	0	6	1001	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	87	87	87	87	87	87	87	87	87	87	87	87
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	1	3	0	5	0	1930	0	7	1151	1

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1936	3094	575	2404	3095	965	1152	0	0	1930	0	0
Stage 1	1164	1164	-	1930	1930	-	-	-	-	-	-	-
Stage 2	772	1930	-	474	1166	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*432	29	*793	*154	28	*674	513	-	-	287	-	-
Stage 1	*306	406	-	*146	234	-	-	-	-	-	-	-
Stage 2	*692	234	-	*813	406	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*419	28	*793	*151	28	*674	513	-	-	287	-	-
Mov Cap-2 Maneuver	*419	28	-	*151	28	-	-	-	-	-	-	-
Stage 1	*299	396	-	*146	234	-	-	-	-	-	-	-
Stage 2	*687	234	-	*793	396	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s/v	11.6		18.7		0		0.11	
HCM LOS	B		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	513	-	-	548	271	287	-	-
HCM Lane V/C Ratio	-	-	-	0.004	0.03	0.024	-	-
HCM Control Delay (s/veh)	0	-	-	11.6	18.7	17.8	-	-
HCM Lane LOS	A	-	-	B	C	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

5: Arizona Avenue & Driveway E

09/28/2023

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	22	2	38	2	0	15	43	1650	13	15	934	52
Future Vol, veh/h	22	2	38	2	0	15	43	1650	13	15	934	52
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	87	87	87	87	87	87	87	87	87	87	87	87
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	25	2	44	2	0	17	49	1897	15	17	1074	60

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1966	3118	537	2460	3163	948	1133	0	0	1911	0	0
Stage 1	1108	1108	-	1995	1995	-	-	-	-	-	-	-
Stage 2	857	2010	-	465	1168	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*406	27	*793	*136	24	*674	525	-	-	295	-	-
Stage 1	*339	435	-	*127	213	-	-	-	-	-	-	-
Stage 2	*692	208	-	*813	404	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*337	23	*793	*100	21	*674	525	-	-	295	-	-
Mov Cap-2 Maneuver	*337	23	-	*100	21	-	-	-	-	-	-	-
Stage 1	*319	409	-	*115	193	-	-	-	-	-	-	-
Stage 2	*611	188	-	*720	381	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v20.07		14.4	0.32	0.27
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	525	-	-	310	403	295	-	-
HCM Lane V/C Ratio	0.094	-	-	0.23	0.049	0.058	-	-
HCM Control Delay (s/veh)	12.6	-	-	20.1	14.4	17.9	-	-
HCM Lane LOS	B	-	-	C	B	C	-	-
HCM 95th %tile Q(veh)	0.3	-	-	0.9	0.2	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↙	↗	↙	↑↑↑	↑↑↑	↗
Traffic Vol, veh/h	34	46	24	1160	1583	50
Future Vol, veh/h	34	46	24	1160	1583	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	36	48	25	1221	1666	53

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	2205	833	1719	0	0
Stage 1	1666	-	-	-	-
Stage 2	539	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-
Pot Cap-1 Maneuver	*166	*674	394	-	-
Stage 1	*292	-	-	-	-
Stage 2	*773	-	-	-	-
Platoon blocked, %	0	0	0	-	-
Mov Cap-1 Maneuver	*156	*674	394	-	-
Mov Cap-2 Maneuver	*232	-	-	-	-
Stage 1	*274	-	-	-	-
Stage 2	*773	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v16.08		0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	394	-	232	674	-	-
HCM Lane V/C Ratio	0.064	-	0.154	0.072	-	-
HCM Control Delay (s/veh)	14.8	-	23.3	10.8	-	-
HCM Lane LOS	B	-	C	B	-	-
HCM 95th %tile Q(veh)	0.2	-	0.5	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	9	0	11	2	0	0	2	1164	2	3	1632	7
Future Vol, veh/h	9	0	11	2	0	0	2	1164	2	3	1632	7
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	9	0	12	2	0	0	2	1225	2	3	1718	7

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2219	2956	859	1924	2962	614	1725	0	0	1227	0	0
Stage 1	1724	1724	-	1231	1231	-	-	-	-	-	-	-
Stage 2	494	1232	-	693	1732	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*367	54	*666	*506	53	*746	407	-	-	567	-	-
Stage 1	*243	328	-	*370	451	-	-	-	-	-	-	-
Stage 2	*766	450	-	*683	325	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*363	54	*666	*492	53	*746	407	-	-	567	-	-
Mov Cap-2 Maneuver	*363	54	-	*492	53	-	-	-	-	-	-	-
Stage 1	*242	327	-	*368	449	-	-	-	-	-	-	-
Stage 2	*762	448	-	*668	323	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v12.78		12.34	0.02	0.02
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	407	-	-	484	492	567	-	-
HCM Lane V/C Ratio	0.005	-	-	0.044	0.004	0.006	-	-
HCM Control Delay (s/veh)	13.9	-	-	12.8	12.3	11.4	-	-
HCM Lane LOS	B	-	-	B	B	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	3	0	1	2	0	5	5	1148	3	5	1643	0
Future Vol, veh/h	3	0	1	2	0	5	5	1148	3	5	1643	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	0	1	2	0	5	5	1196	3	5	1711	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2211	2931	856	1903	2930	599	1711	0	0	1199	0	0
Stage 1	1722	1722	-	1208	1208	-	-	-	-	-	-	-
Stage 2	489	1209	-	695	1722	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*325	53	*674	*523	53	*753	399	-	-	571	-	-
Stage 1	*229	317	-	*367	451	-	-	-	-	-	-	-
Stage 2	*773	450	-	*692	317	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*316	51	*674	*510	52	*753	399	-	-	571	-	-
Mov Cap-2 Maneuver	*316	51	-	*510	52	-	-	-	-	-	-	-
Stage 1	*227	314	-	*363	445	-	-	-	-	-	-	-
Stage 2	*758	444	-	*685	314	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	15	10.49	0.06	0.03
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	399	-	-	364	663	571	-	-
HCM Lane V/C Ratio	0.013	-	-	0.011	0.011	0.009	-	-
HCM Control Delay (s/veh)	14.1	-	-	15	10.5	11.4	-	-
HCM Lane LOS	B	-	-	B	B	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	11	1	30	3	0	26	25	1135	10	18	1603	52
Future Vol, veh/h	11	1	30	3	0	26	25	1135	10	18	1603	52
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	1	31	3	0	27	26	1182	10	19	1670	54

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2232	2952	835	1940	2996	591	1724	0	0	1193	0	0
Stage 1	1707	1707	-	1234	1234	-	-	-	-	-	-	-
Stage 2	525	1245	-	706	1761	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*309	50	*674	*523	45	*753	391	-	-	575	-	-
Stage 1	*236	324	-	*349	436	-	-	-	-	-	-	-
Stage 2	*773	430	-	*692	299	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*269	45	*674	*441	40	*753	391	-	-	575	-	-
Mov Cap-2 Maneuver	*269	45	-	*441	40	-	-	-	-	-	-	-
Stage 1	*229	313	-	*326	407	-	-	-	-	-	-	-
Stage 2	*696	401	-	*636	290	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v15.38		10.36	0.32	0.12
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	391	-	-	391	702	575	-	-
HCM Lane V/C Ratio	0.067	-	-	0.112	0.043	0.033	-	-
HCM Control Delay (s/veh)	14.8	-	-	15.4	10.4	11.5	-	-
HCM Lane LOS	B	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.2	-	-	0.4	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



Appendix E – Trip Generation



Appendix F – MAG Socioeconomic Projections

Socioeconomic Projections

Population and Employment

by Municipal Planning Area, Jurisdiction, and Regional Analysis Zone

June 2023

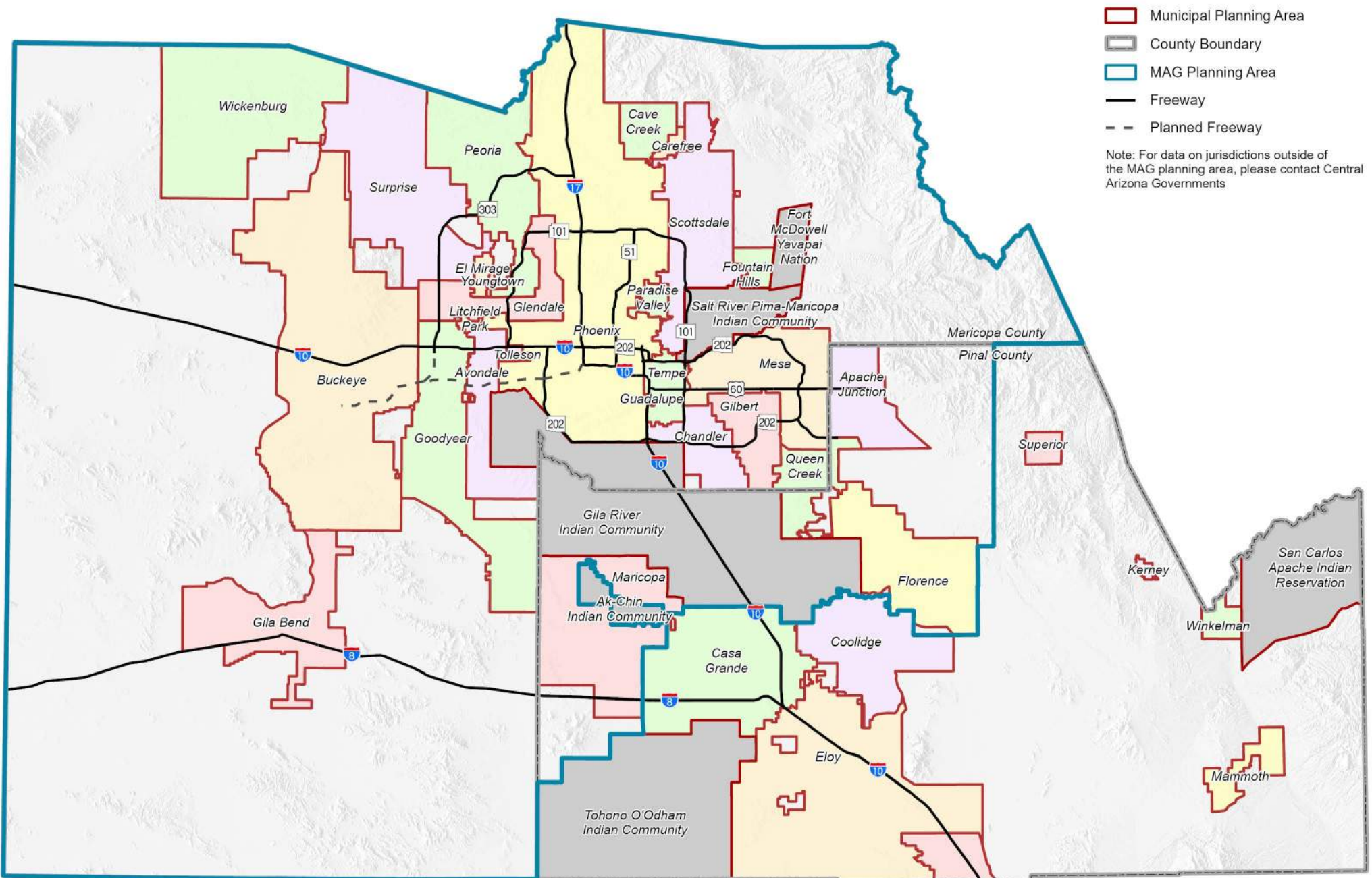


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Municipal Planning Areas (2023) Maricopa and Pinal Counties, Arizona



0 5 10 15 20 25 30 35 40 Miles

Date: May 2023
Source: MAG and the MAG Member Agencies, CAG and the CAG Member Agencies

While every effort has been made to ensure the accuracy of this information, the Maricopa Association of Governments makes no warranty, expressed or implied, as to its accuracy and expressly disclaims liability for the accuracy thereof.

Maricopa Association of Governments

**Table 1: Total Population by Municipal Planning Area
July 1, 2020, 2022 and Projections July 1, 2030 to July 1, 2060**

Municipal Planning Area	Total Population					
	2020	2022	2030	2040	2050	2060
Apache Junction	52,700	53,700	67,100	84,200	100,800	127,600
Avondale	91,100	94,000	117,800	130,900	135,400	137,700
Buckeye	109,700	124,400	194,400	296,200	397,800	491,000
Carefree	3,700	3,700	4,100	4,100	4,200	4,200
Cave Creek	5,000	5,300	5,600	5,900	6,100	6,200
Chandler	286,100	292,000	304,300	315,900	336,500	337,600
El Mirage	35,900	36,300	37,800	38,400	38,500	38,600
Florence	76,600	78,100	106,700	154,000	197,500	242,900
Fort McDowell Yavapai Native Nation	1,200	1,200	1,200	1,200	1,200	1,200
Fountain Hills	23,900	24,000	25,200	25,700	26,000	26,100
Gila Bend	2,100	2,100	2,500	2,500	2,500	2,500
Gila River Indian Native Nation	14,100	14,100	14,100	14,100	14,100	14,100
Gilbert	272,400	281,200	301,600	311,600	323,900	325,100
Glendale	282,200	291,000	323,600	345,300	353,200	354,900
Goodyear	99,900	109,200	165,100	210,000	241,200	276,700
Guadalupe	5,300	5,300	5,400	5,600	5,700	5,700
Litchfield Park	13,300	13,400	14,200	14,300	14,300	14,300
Maricopa	64,800	71,500	100,500	122,200	141,400	159,800
Mesa	550,300	561,500	589,900	624,200	645,500	661,200
Paradise Valley	12,700	12,700	13,300	13,300	13,400	13,400
Peoria	207,400	215,600	244,500	288,900	318,100	346,800
Phoenix	1,665,200	1,711,800	1,867,300	2,007,800	2,101,500	2,184,600
Queen Creek	72,700	84,700	121,000	135,700	149,400	154,900
Salt River Pima-Maricopa Native Nation	6,300	6,400	5,500	5,900	6,000	6,000
Scottsdale	241,800	245,100	275,700	286,700	301,300	307,300
Surprise	155,600	167,500	255,700	342,300	402,900	446,500
Tempe	181,600	187,400	219,300	247,500	258,000	269,700
Tolleson	7,300	7,300	8,100	8,900	9,600	10,300
Unincorporated Maricopa County	99,700	101,600	107,100	110,800	120,400	136,200
Unincorporated Pinal County	66,400	69,100	76,500	87,500	99,600	117,900
Wickenburg	7,800	8,000	8,600	8,600	8,700	8,700
Youngtown	7,100	7,100	7,500	7,700	7,800	7,800

Notes: Numbers rounded to the nearest 100. These projections include both the Maricopa County and Pinal County portions for Apache Junction, Queen Creek, and the Gila River Indian Community. Peoria and Wickenburg include only the Maricopa County portion. Unincorporated Pinal County only includes portions in the MAG planning area.

Source: Maricopa Association of Governments (MAG) Socioeconomic Projections of Population and Employment by Municipal Planning Area (MPA) and Regional Analysis Zone (RAZ), June 2023

For explanation of variables and complete notation on this series, please refer to the Notes and Caveats in Appendix A.

For data on jurisdictions outside of the MAG planning area, please contact Central Arizona Governments.

Maricopa Association of Governments
Table 2: Total Employment by Municipal Planning Area
July 1, 2020, 2022 and Projections July 1, 2030 to July 1, 2060

Municipal Planning Area	Total Employment					
	2020	2022	2030	2040	2050	2060
Apache Junction	7,900	9,200	15,600	19,000	22,800	27,500
Avondale	24,900	28,100	33,600	39,600	42,400	46,800
Buckeye	25,900	26,200	50,100	92,400	126,800	183,600
Carefree	1,600	1,600	1,700	1,700	1,800	1,900
Cave Creek	2,400	2,600	2,800	3,000	3,300	3,500
Chandler	157,400	166,400	189,400	204,500	216,000	224,700
El Mirage	5,700	6,300	10,200	13,600	15,300	16,400
Florence	9,700	9,900	12,100	18,500	26,600	35,500
Fort McDowell Yavapai Native Nation	900	900	900	900	900	900
Fountain Hills	7,800	8,200	9,200	10,100	10,700	11,200
Gila Bend	1,000	1,000	1,100	1,200	1,300	2,000
Gila River Indian Native Nation	9,000	9,000	12,300	14,000	14,800	17,500
Gilbert	98,100	108,000	122,700	136,400	143,900	151,300
Glendale	103,300	111,900	136,400	155,800	171,900	183,900
Goodyear	46,000	59,200	87,200	110,600	124,100	142,800
Guadalupe	1,200	1,100	1,200	1,300	1,300	1,400
Litchfield Park	3,700	3,500	4,900	5,300	5,600	6,000
Maricopa	7,400	8,400	11,100	17,700	24,700	32,700
Mesa	197,400	208,200	237,500	277,300	308,900	331,500
Paradise Valley	4,700	4,700	5,100	5,500	5,500	5,800
Peoria	60,400	62,700	71,700	89,200	98,200	109,600
Phoenix	881,000	933,700	1,048,500	1,149,000	1,216,700	1,272,600
Queen Creek	21,100	22,300	30,700	35,000	39,700	42,900
Salt River Pima-Maricopa Native Nation	18,100	21,900	31,200	34,800	36,600	38,600
Scottsdale	188,900	207,100	230,500	246,500	255,000	264,000
Surprise	38,600	43,500	74,200	92,600	108,800	130,000
Tempe	192,400	207,900	233,700	253,400	264,400	273,000
Tolleson	17,600	19,000	20,600	21,500	22,000	22,400
Unincorporated Maricopa County	28,800	31,100	36,300	41,000	48,100	56,100
Unincorporated Pinal County	4,500	5,000	6,000	8,200	11,900	17,700
Wickenburg	4,300	4,200	4,400	4,500	4,500	4,600
Youngtown	1,600	1,600	1,700	2,200	2,300	2,500

Notes: Numbers rounded to the nearest 100. These projections include both the Maricopa County and Pinal County portions for Apache Junction, Queen Creek, and the Gila River Indian Community. Peoria and Wickenburg include only the Maricopa County portion. Unincorporated Pinal County only includes portions in the MAG planning area.

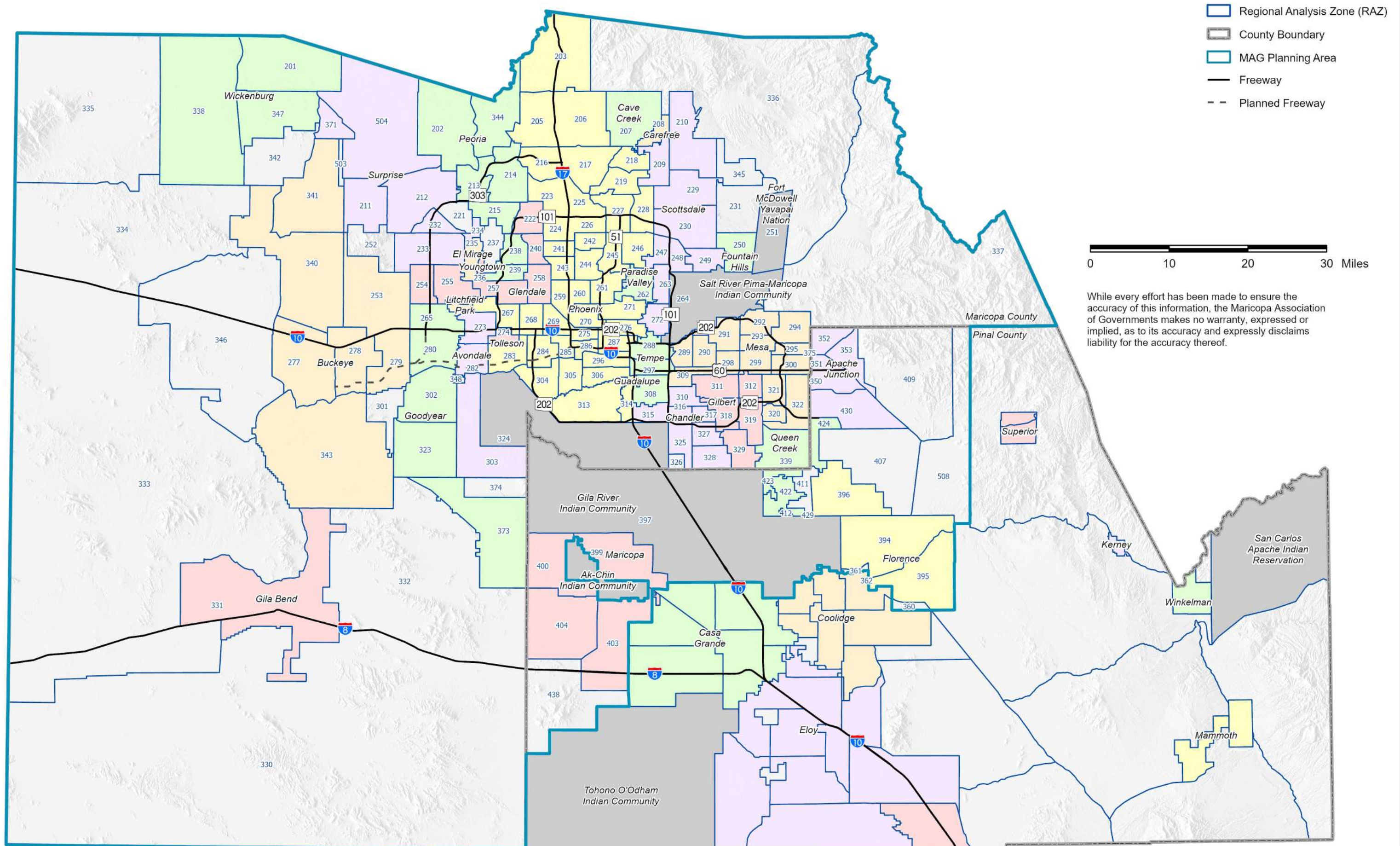
Source: Maricopa Association of Governments (MAG) Socioeconomic Projections of Population and Employment by Municipal Planning Area (MPA) and Regional Analysis Zone (RAZ), June 2023

For explanation of variables and complete notation on this series, please refer to the Notes and Caveats in Appendix A.

For data on jurisdictions outside of the MAG planning area, please contact Central Arizona Governments.

Regional Analysis Zones (RAZ), 2023

Maricopa and Pinal Counties, Arizona



- Regional Analysis Zone (RAZ)
- County Boundary
- MAG Planning Area
- Freeway
- Planned Freeway



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Maricopa Association of Governments
Table 4: Population by Regional Analysis Zone (RAZ) by MPA
July 1, 2020, 2022 and Projections July 1, 2030 to July 1, 2060

RAZ	County	Total Population					
		2020	2022	2030	2040	2050	2060
Chandler MPA							
310	Maricopa	52,605	53,151	53,951	55,245	67,304	68,080
315	Maricopa	40,816	41,024	41,126	42,128	42,486	42,620
316	Maricopa	37,089	37,366	42,624	46,383	48,689	48,730
317	Maricopa	33,314	33,627	34,815	38,544	43,787	43,818
325	Maricopa	47,238	49,471	50,721	51,342	51,796	51,860
327	Maricopa	27,143	28,146	29,766	30,180	30,253	30,253
328	Maricopa	47,865	49,184	51,261	52,110	52,212	52,237
	Total	286,070	291,969	304,264	315,932	336,527	337,598
El Mirage MPA							
235	Maricopa	35,927	36,275	37,825	38,412	38,539	38,649
	Total	35,927	36,275	37,825	38,412	38,539	38,649
Florence MPA							
394	Pinal	24,189	24,585	40,293	60,963	76,838	94,450
395	Pinal	11,112	9,895	10,092	13,181	20,578	30,315
396	Pinal	41,252	43,666	56,358	79,855	100,044	118,095
	Total	76,553	78,146	106,743	153,999	197,460	242,860
Fort McDowell Yavapai Native Nation MPA							
251	Maricopa	1,152	1,153	1,154	1,154	1,154	1,154
	Total	1,152	1,153	1,154	1,154	1,154	1,154
Fountain Hills MPA							
250	Maricopa	23,857	23,972	25,185	25,739	25,967	26,076
	Total	23,857	23,972	25,185	25,739	25,967	26,076
Gila Bend MPA							
331	Maricopa	2,133	2,134	2,501	2,501	2,513	2,513
	Total	2,133	2,134	2,501	2,501	2,513	2,513
Gila River Indian Native Nation MPA							
324	Maricopa	3,583	3,585	3,588	3,589	3,590	3,591
397	Pinal	10,471	10,472	10,472	10,472	10,472	10,472
	Total	14,054	14,057	14,060	14,061	14,062	14,063

Notes: These projections include both the Maricopa County and Pinal County portions for Apache Junction, Queen Creek, and the Gila River Indian Community. Peoria and Wickenburg include only the Maricopa County portion. Unincorporated Pinal County only includes portions in the MAG planning area.

Source: Maricopa Association of Governments (MAG) Socioeconomic Projections of Population and Employment by Municipal Planning Area (MPA) and Regional Analysis Zone (RAZ), June 2023

For explanation of variables and complete notation on this series, please refer to the Notes and Caveats in Appendix A.

For data on jurisdictions outside of the MAG planning area, please contact Central Arizona Governments.

Maricopa Association of Governments
Table 5: Employment by Regional Analysis Zone (RAZ) by MPA
July 1, 2020, 2022 and Projections July 1, 2030 to July 1, 2060

RAZ	County	Total Employment					
		2020	2022	2030	2040	2050	2060
Chandler MPA							
310	Maricopa	21,336	21,647	22,852	24,773	29,165	31,402
315	Maricopa	40,905	42,057	44,289	46,475	47,570	48,430
316	Maricopa	28,818	28,354	31,693	34,155	35,131	36,130
317	Maricopa	8,446	8,590	9,707	11,486	12,571	14,044
325	Maricopa	33,588	38,276	48,197	51,538	53,785	55,244
327	Maricopa	14,410	17,261	21,978	24,436	25,762	26,985
328	Maricopa	9,898	10,249	10,702	11,589	12,022	12,488
	Total	157,401	166,434	189,418	204,452	216,006	224,723
El Mirage MPA							
235	Maricopa	5,664	6,338	10,238	13,573	15,318	16,450
	Total	5,664	6,338	10,238	13,573	15,318	16,450
Florence MPA							
394	Pinal	4,953	5,022	6,089	8,847	12,588	16,675
395	Pinal	1,953	1,575	1,901	3,038	4,270	5,551
396	Pinal	2,747	3,279	4,148	6,616	9,776	13,251
	Total	9,653	9,876	12,138	18,501	26,634	35,477
Fort McDowell Yavapai Native Nation MPA							
251	Maricopa	917	881	879	879	881	874
	Total	917	881	879	879	881	874
Fountain Hills MPA							
250	Maricopa	7,850	8,248	9,206	10,083	10,706	11,217
	Total	7,850	8,248	9,206	10,083	10,706	11,217
Gila Bend MPA							
331	Maricopa	952	1,047	1,107	1,224	1,304	2,016
	Total	952	1,047	1,107	1,224	1,304	2,016
Gila River Indian Native Nation MPA							
324	Maricopa	6,857	6,956	9,476	11,136	11,785	14,310
397	Pinal	2,115	2,011	2,801	2,910	3,038	3,174
	Total	8,972	8,967	12,277	14,046	14,823	17,484

Notes: These projections include both the Maricopa County and Pinal County portions for Apache Junction, Queen Creek, and the Gila River Indian Community. Peoria and Wickenburg include only the Maricopa County portion. Unincorporated Pinal County only includes portions in the MAG planning area.

Source: Maricopa Association of Governments (MAG) Socioeconomic Projections of Population and Employment by Municipal Planning Area (MPA) and Regional Analysis Zone (RAZ), June 2023

For explanation of variables and complete notation on this series, please refer to the Notes and Caveats in Appendix A.

For data on jurisdictions outside of the MAG planning area, please contact Central Arizona Governments.

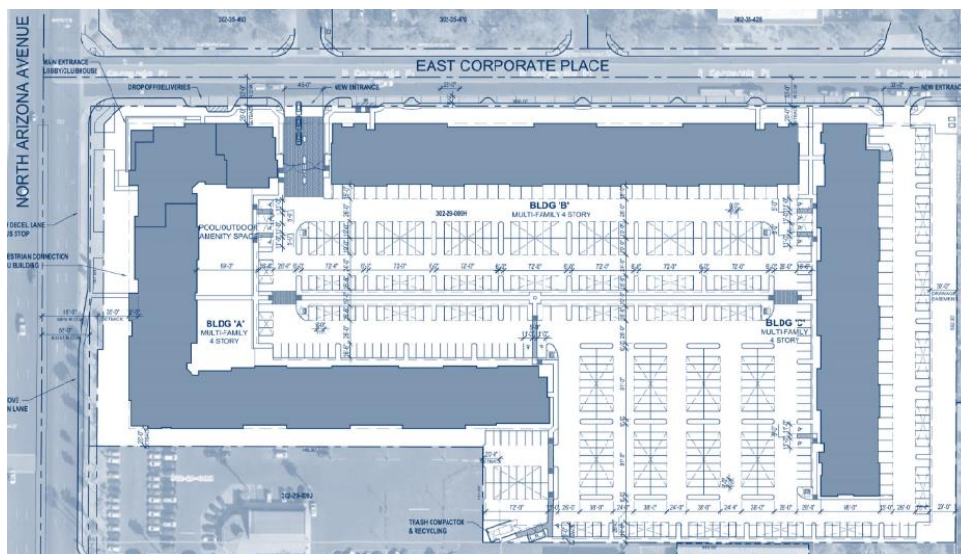


Appendix G – Surrounding Development(s)



Alta Uptown

Wood Partners



Prepared for:



Wood Partners
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Project Number: 21.5261
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Legend

AM(PM) Peak Hour Traffic Volumes
 X Intersection

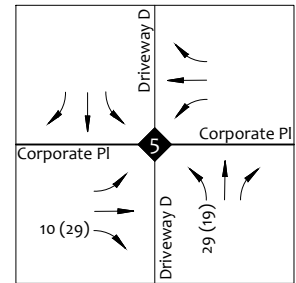
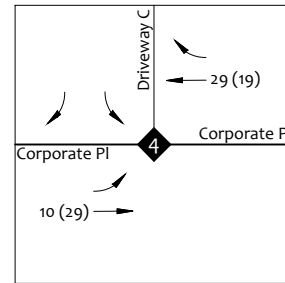
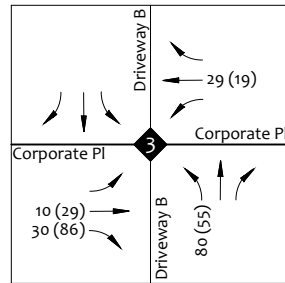
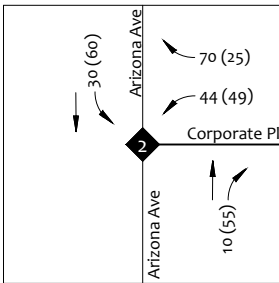
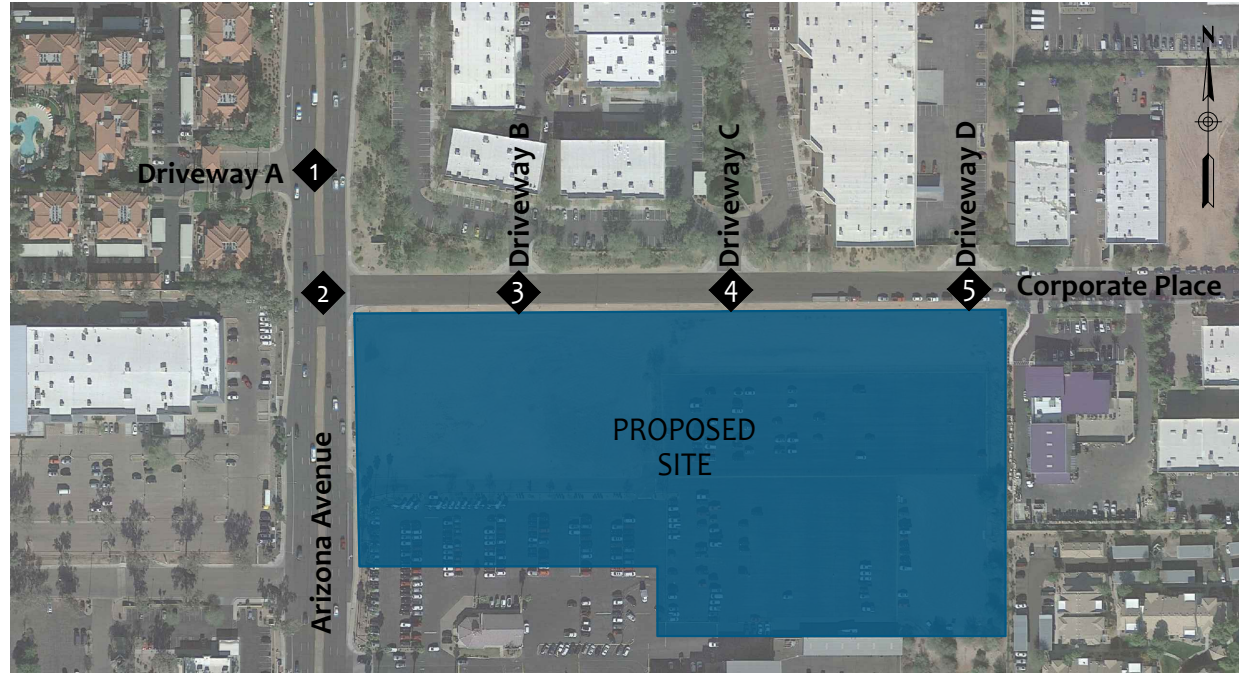
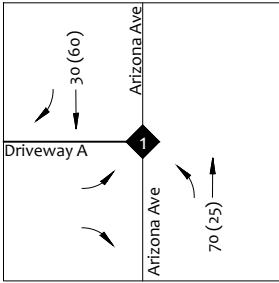


FIGURE 7 | SITE TRAFFIC VOLUMES



Appendix H – Year 2027 No Build Capacity Analysis

Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↵	↶	↵	↑↑↑	↑↑↑	↶
Traffic Vol, veh/h	56	26	28	1739	1094	48
Future Vol, veh/h	56	26	28	1739	1094	48
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	61	28	30	1890	1189	52

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	2006	595	1241	0	0
Stage 1	1189	-	-	-	-
Stage 2	817	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-
Pot Cap-1 Maneuver	*355	*753	540	-	-
Stage 1	*416	-	-	-	-
Stage 2	*692	-	-	-	-
Platoon blocked, %	0	0	0	-	-
Mov Cap-1 Maneuver	*335	*753	540	-	-
Mov Cap-2 Maneuver	*351	-	-	-	-
Stage 1	*393	-	-	-	-
Stage 2	*692	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v15.03		0.19	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	540	-	351	753	-	-
HCM Lane V/C Ratio	0.056	-	0.173	0.038	-	-
HCM Control Delay (s/veh)	12.1	-	17.4	10	-	-
HCM Lane LOS	B	-	C	A	-	-
HCM 95th %tile Q(veh)	0.2	-	0.6	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	8	0	3	0	0	1	5	1768	2	5	1090	6
Future Vol, veh/h	8	0	3	0	0	1	5	1768	2	5	1090	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	9	0	3	0	0	1	5	1922	2	5	1185	7

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1975	3130	592	2418	3136	962	1191	0	0	1924	0	0
Stage 1	1196	1196	-	1934	1934	-	-	-	-	-	-	-
Stage 2	780	1935	-	485	1202	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*506	34	*746	*226	34	*666	595	-	-	301	-	-
Stage 1	*395	471	-	*154	242	-	-	-	-	-	-	-
Stage 2	*683	241	-	*766	468	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*492	33	*746	*219	33	*666	595	-	-	301	-	-
Mov Cap-2 Maneuver	*492	33	-	*219	33	-	-	-	-	-	-	-
Stage 1	*388	463	-	*152	239	-	-	-	-	-	-	-
Stage 2	*676	239	-	*749	459	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	11.79	10.42	0.03	0.08
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	595	-	-	542	666	301	-	-
HCM Lane V/C Ratio	0.009	-	-	0.022	0.002	0.018	-	-
HCM Control Delay (s/veh)	11.1	-	-	11.8	10.4	17.2	-	-
HCM Lane LOS	B	-	-	B	B	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	1	0	1	3	0	4	0	1757	0	6	1086	1
Future Vol, veh/h	1	0	1	3	0	4	0	1757	0	6	1086	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	1	3	0	4	0	1910	0	7	1180	1

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1957	3103	590	2395	3104	955	1182	0	0	1910	0	0
Stage 1	1193	1193	-	1910	1910	-	-	-	-	-	-	-
Stage 2	764	1910	-	485	1195	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*523	34	*753	*211	34	*674	584	-	-	296	-	-
Stage 1	*377	459	-	*153	241	-	-	-	-	-	-	-
Stage 2	*692	241	-	*773	458	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*508	33	*753	*206	33	*674	584	-	-	296	-	-
Mov Cap-2 Maneuver	*508	33	-	*206	33	-	-	-	-	-	-	-
Stage 1	*369	449	-	*153	241	-	-	-	-	-	-	-
Stage 2	*688	241	-	*755	448	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.96		15.79	0	0.1
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	584	-	-	607	341	296	-	-
HCM Lane V/C Ratio	-	-	-	0.004	0.022	0.022	-	-
HCM Control Delay (s/veh)	0	-	-	11	15.8	17.4	-	-
HCM Lane LOS	A	-	-	B	C	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

5: Arizona Avenue & Driveway E

09/28/2023

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	23	2	40	2	0	16	45	1727	14	16	1016	54
Future Vol, veh/h	23	2	40	2	0	16	45	1727	14	16	1016	54
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	25	2	43	2	0	17	49	1877	15	17	1104	59

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1988	3129	552	2453	3173	939	1163	0	0	1892	0	0
Stage 1	1139	1139	-	1975	1975	-	-	-	-	-	-	-
Stage 2	849	1990	-	478	1198	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*387	26	*793	*138	24	*674	506	-	-	304	-	-
Stage 1	*321	419	-	*132	219	-	-	-	-	-	-	-
Stage 2	*692	214	-	*813	390	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*321	22	*793	*102	20	*674	506	-	-	304	-	-
Mov Cap-2 Maneuver	*321	22	-	*102	20	-	-	-	-	-	-	-
Stage 1	*302	395	-	*120	198	-	-	-	-	-	-	-
Stage 2	*609	194	-	*721	368	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	20.2	14.09	0.32	0.26
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	506	-	-	307	415	304	-	-
HCM Lane V/C Ratio	0.097	-	-	0.23	0.047	0.057	-	-
HCM Control Delay (s/veh)	12.9	-	-	20.2	14.1	17.6	-	-
HCM Lane LOS	B	-	-	C	B	C	-	-
HCM 95th %tile Q(veh)	0.3	-	-	0.9	0.1	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	0.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↙	↗	↙	↑↑↑	↑↑↑	↗
Traffic Vol, veh/h	35	48	25	1262	1696	52
Future Vol, veh/h	35	48	25	1262	1696	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	38	52	27	1372	1843	57

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2447	922	1900	0	-	0
Stage 1	1843	-	-	-	-	-
Stage 2	603	-	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-	-
Pot Cap-1 Maneuver	*111	*674	301	-	-	-
Stage 1	*209	-	-	-	-	-
Stage 2	*773	-	-	-	-	-
Platoon blocked, %	0	0	0	-	-	-
Mov Cap-1 Maneuver	*101	*674	301	-	-	-
Mov Cap-2 Maneuver	*164	-	-	-	-	-
Stage 1	*190	-	-	-	-	-
Stage 2	*773	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v20.34		0.35	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	301	-	164	674	-	-
HCM Lane V/C Ratio	0.09	-	0.232	0.077	-	-
HCM Control Delay (s/veh)	18.2	-	33.4	10.8	-	-
HCM Lane LOS	C	-	D	B	-	-
HCM 95th %tile Q(veh)	0.3	-	0.9	0.3	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	9	0	11	2	0	0	2	1266	2	3	1747	7
Future Vol, veh/h	9	0	11	2	0	0	2	1266	2	3	1747	7
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	0	12	2	0	0	2	1376	2	3	1899	8

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2460	3288	949	2148	3295	689	1907	0	0	1378	0	0
Stage 1	1905	1905	-	1382	1382	-	-	-	-	-	-	-
Stage 2	555	1383	-	766	1913	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*204	22	*666	*434	22	*746	309	-	-	464	-	-
Stage 1	*164	252	-	*277	372	-	-	-	-	-	-	-
Stage 2	*766	371	-	*683	249	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*201	22	*666	*420	22	*746	309	-	-	464	-	-
Mov Cap-2 Maneuver	*201	22	-	*420	22	-	-	-	-	-	-	-
Stage 1	*163	250	-	*275	369	-	-	-	-	-	-	-
Stage 2	*760	369	-	*666	247	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v16.81		13.61	0.03	0.02
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	309	-	-	326	420	464	-	-
HCM Lane V/C Ratio	0.007	-	-	0.067	0.005	0.007	-	-
HCM Control Delay (s/veh)	16.7	-	-	16.8	13.6	12.8	-	-
HCM Lane LOS	C	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0.2	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	3	0	1	2	0	5	5	1250	3	5	1759	0
Future Vol, veh/h	3	0	1	2	0	5	5	1250	3	5	1759	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	0	1	2	0	5	5	1359	3	5	1912	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2477	3296	956	2147	3294	681	1912	0	0	1362	0	0
Stage 1	1923	1923	-	1371	1371	-	-	-	-	-	-	-
Stage 2	554	1373	-	776	1923	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*173	20	*674	*377	20	*753	295	-	-	460	-	-
Stage 1	*148	237	-	*269	366	-	-	-	-	-	-	-
Stage 2	*773	365	-	*692	237	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*166	20	*674	*365	20	*753	295	-	-	460	-	-
Mov Cap-2 Maneuver	*166	20	-	*365	20	-	-	-	-	-	-	-
Stage 1	*147	234	-	*264	359	-	-	-	-	-	-	-
Stage 2	*753	359	-	*683	234	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v22.93		11.31	0.07	0.04
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	295	-	-	205	578	460	-	-
HCM Lane V/C Ratio	0.018	-	-	0.021	0.013	0.012	-	-
HCM Control Delay (s/veh)	17.4	-	-	22.9	11.3	12.9	-	-
HCM Lane LOS	C	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

5: Arizona Avenue & Driveway E

09/28/2023

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	11	1	31	3	0	27	26	1236	10	19	1717	29
Future Vol, veh/h	11	1	31	3	0	27	26	1236	10	19	1717	29
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	12	1	34	3	0	29	28	1343	11	21	1866	32

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2502	3318	933	2188	3339	672	1898	0	0	1354	0	0
Stage 1	1908	1908	-	1400	1400	-	-	-	-	-	-	-
Stage 2	594	1411	-	788	1939	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*163	19	*674	*343	18	*753	302	-	-	465	-	-
Stage 1	*153	242	-	*255	353	-	-	-	-	-	-	-
Stage 2	*773	348	-	*692	231	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*136	17	*674	*266	16	*753	302	-	-	465	-	-
Mov Cap-2 Maneuver	*136	17	-	*266	16	-	-	-	-	-	-	-
Stage 1	*147	231	-	*231	320	-	-	-	-	-	-	-
Stage 2	*673	315	-	*625	221	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v24.67		10.96	0.37	0.14
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	302	-	-	229	636	465	-	-
HCM Lane V/C Ratio	0.094	-	-	0.204	0.051	0.044	-	-
HCM Control Delay (s/veh)	18.2	-	-	24.7	11	13.1	-	-
HCM Lane LOS	C	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	0.7	0.2	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



Appendix I – Year 2027 Build Capacity Analysis

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵		↵		↕		↵	↑↑↑	↵	↵	↑↑↑	↵
Traffic Vol, veh/h	56	0	26	13	0	28	28	1803	4	9	1110	48
Future Vol, veh/h	56	0	26	13	0	28	28	1803	4	9	1110	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	0	-	-	-	50	-	-	-	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	61	0	28	14	0	30	30	1960	4	10	1207	52

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	2071	-	603	2523	3299	980	1259	0	0	1964	0	0
Stage 1	1226	-	-	2021	2021	-	-	-	-	-	-	-
Stage 2	845	-	-	502	1278	-	-	-	-	-	-	-
Critical Hdwy	6.44	-	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	-	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	-	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	-	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*481	0	*753	*164	21	*635	527	-	-	326	-	-
Stage 1	*355	0	-	*158	242	-	-	-	-	-	-	-
Stage 2	*652	0	-	*773	412	-	-	-	-	-	-	-
Platoon blocked, %	0		0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*418	-	*753	*144	19	*635	527	-	-	326	-	-
Mov Cap-2 Maneuver	*418	-	-	*144	19	-	-	-	-	-	-	-
Stage 1	*344	-	-	*149	228	-	-	-	-	-	-	-
Stage 2	*585	-	-	*722	400	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v13.45		18.8	0.19	0.13
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	527	-	-	418	753	305	326	-	-
HCM Lane V/C Ratio	0.058	-	-	0.146	0.038	0.146	0.03	-	-
HCM Control Delay (s/veh)	12.2	-	-	15.1	10	18.8	16.4	-	-
HCM Lane LOS	B	-	-	C	A	C	C	-	-
HCM 95th %tile Q(veh)	0.2	-	-	0.5	0.1	0.5	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

2: Arizona Avenue & Driveway B

01/10/2024

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↕↕		↕	↕↕↕	↕
Traffic Vol, veh/h	8	0	3	30	0	64	5	1772	11	16	1103	6
Future Vol, veh/h	8	0	3	30	0	64	5	1772	11	16	1103	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	9	0	3	33	0	70	5	1926	12	17	1199	7

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2015	3183	599	2457	3183	969	1205	0	0	1938	0	0
Stage 1	1234	1234	-	1943	1943	-	-	-	-	-	-	-
Stage 2	781	1949	-	514	1240	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*455	26	*746	*165	26	*666	584	-	-	295	-	-
Stage 1	*367	449	-	*151	238	-	-	-	-	-	-	-
Stage 2	*683	236	-	*766	446	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*380	24	*746	*153	24	*666	584	-	-	295	-	-
Mov Cap-2 Maneuver	*380	24	-	*153	24	-	-	-	-	-	-	-
Stage 1	*346	423	-	*149	236	-	-	-	-	-	-	-
Stage 2	*606	234	-	*717	419	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v13.43		18.63	0.03	0.26
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	584	-	-	439	153	666	295	-	-
HCM Lane V/C Ratio	0.009	-	-	0.027	0.213	0.104	0.059	-	-
HCM Control Delay (s/veh)	11.2	-	-	13.4	34.8	11	18	-	-
HCM Lane LOS	B	-	-	B	D	B	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0.8	0.3	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

01/10/2024

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	1	0	1	3	0	4	0	1772	0	6	1129	1
Future Vol, veh/h	1	0	1	3	0	4	0	1772	0	6	1129	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	1	3	0	4	0	1926	0	7	1227	1

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2011	3166	614	2430	3167	963	1228	0	0	1926	0	0
Stage 1	1240	1240	-	1926	1926	-	-	-	-	-	-	-
Stage 2	770	1926	-	504	1241	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*515	29	*753	*194	29	*674	549	-	-	289	-	-
Stage 1	*345	433	-	*147	236	-	-	-	-	-	-	-
Stage 2	*692	236	-	*773	432	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*501	28	*753	*189	28	*674	549	-	-	289	-	-
Mov Cap-2 Maneuver	*501	28	-	*189	28	-	-	-	-	-	-	-
Stage 1	*338	423	-	*147	236	-	-	-	-	-	-	-
Stage 2	*688	236	-	*754	422	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.01		16.49	0	0.09
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	549	-	-	601	321	289	-	-
HCM Lane V/C Ratio	-	-	-	0.004	0.024	0.023	-	-
HCM Control Delay (s/veh)	0	-	-	11	16.5	17.7	-	-
HCM Lane LOS	A	-	-	B	C	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	23	2	40	2	0	16	45	1742	14	16	1059	54
Future Vol, veh/h	23	2	40	2	0	16	45	1742	14	16	1059	54
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	25	2	43	2	0	17	49	1893	15	17	1151	59

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2041	3192	576	2488	3236	947	1210	0	0	1909	0	0
Stage 1	1186	1186	-	1991	1991	-	-	-	-	-	-	-
Stage 2	855	2007	-	496	1245	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*345	22	*793	*128	20	*674	477	-	-	297	-	-
Stage 1	*295	396	-	*128	214	-	-	-	-	-	-	-
Stage 2	*692	209	-	*813	368	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*284	19	*793	*92	17	*674	477	-	-	297	-	-
Mov Cap-2 Maneuver	*284	19	-	*92	17	-	-	-	-	-	-	-
Stage 1	*277	372	-	*115	192	-	-	-	-	-	-	-
Stage 2	*605	188	-	*719	347	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v22.63		14.57	0.34	0.25
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	477	-	-	274	396	297	-
HCM Lane V/C Ratio	0.103	-	-	0.258	0.049	0.059	-
HCM Control Delay (s/veh)	13.4	-	-	22.6	14.6	17.9	-
HCM Lane LOS	B	-	-	C	B	C	-
HCM 95th %tile Q(veh)	0.3	-	-	1	0.2	0.2	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵		↶		↕		↵	↑↑↑		↑↑↑		↶
Traffic Vol, veh/h	35	0	48	11	0	8	25	1280	8	25	1742	52
Future Vol, veh/h	35	0	48	11	0	8	25	1280	8	25	1742	52
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	0	-	-	-	50	-	-	-	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	38	0	52	12	0	9	27	1391	9	27	1893	57

Major/Minor	Minor2	Minor1		Major1			Major2					
Conflicting Flow All	2559	-	947	2262	3454	700	1950	0	0	1400	0	0
Stage 1	1948	-	-	1450	1450	-	-	-	-	-	-	-
Stage 2	611	-	-	812	2004	-	-	-	-	-	-	-
Critical Hdwy	6.44	-	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	-	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	-	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	-	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*77	0	*674	*143	10	*753	279	-	-	437	-	-
Stage 1	*140	0	-	*231	331	-	-	-	-	-	-	-
Stage 2	*773	0	-	*692	210	-	-	-	-	-	-	-
Platoon blocked, %	0		0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*62	-	*674	*108	8	*753	279	-	-	437	-	-
Mov Cap-2 Maneuver	*62	-	-	*108	8	-	-	-	-	-	-	-
Stage 1	*127	-	-	*209	299	-	-	-	-	-	-	-
Stage 2	*689	-	-	*577	189	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v60.79		29.36	0.37	0.19
HCM LOS	F	D		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	279	-	-	62	674	168	437	-	-
HCM Lane V/C Ratio	0.098	-	-	0.613	0.077	0.123	0.062	-	-
HCM Control Delay (s/veh)	19.3	-	-	129.4	10.8	29.4	13.8	-	-
HCM Lane LOS	C	-	-	F	B	D	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	2.6	0.3	0.4	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

2: Arizona Avenue & Driveway B

01/10/2024

Intersection												
Int Delay, s/veh	0.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↕↕		↕	↕↕↕	↕
Traffic Vol, veh/h	9	0	11	24	0	18	2	1274	25	46	1758	7
Future Vol, veh/h	9	0	11	24	0	18	2	1274	25	46	1758	7
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	0	12	26	0	20	2	1385	27	50	1911	8

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2569	3427	955	2267	3421	706	1918	0	0	1412	0	0
Stage 1	2011	2011	-	1403	1403	-	-	-	-	-	-	-
Stage 2	558	1416	-	864	2018	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*156	15	*666	*326	15	*746	304	-	-	443	-	-
Stage 1	*130	215	-	*265	362	-	-	-	-	-	-	-
Stage 2	*766	355	-	*683	213	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*134	13	*666	*282	14	*746	304	-	-	443	-	-
Mov Cap-2 Maneuver	*134	13	-	*282	14	-	-	-	-	-	-	-
Stage 1	*115	191	-	*264	359	-	-	-	-	-	-	-
Stage 2	*740	353	-	*595	189	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	21.61	15.15	0.03	0.36
HCM LOS	C	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	304	-	-	238	282	746	443	-	-
HCM Lane V/C Ratio	0.007	-	-	0.091	0.092	0.026	0.113	-	-
HCM Control Delay (s/veh)	16.9	-	-	21.6	19	10	14.2	-	-
HCM Lane LOS	C	-	-	C	C	A	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0.3	0.3	0.1	0.4	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

01/10/2024

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	3	0	1	2	0	5	5	1283	3	5	1794	0
Future Vol, veh/h	3	0	1	2	0	5	5	1283	3	5	1794	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	0	1	2	0	5	5	1395	3	5	1950	0

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	2530	3370	975	2198	3368	699	1950	0	0	1398	0	0
Stage 1	1961	1961	-	1407	1407	-	-	-	-	-	-	-
Stage 2	569	1409	-	791	1961	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*152	17	*674	*335	17	*753	279	-	-	438	-	-
Stage 1	*137	224	-	*251	350	-	-	-	-	-	-	-
Stage 2	*773	349	-	*692	224	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*146	16	*674	*324	16	*753	279	-	-	438	-	-
Mov Cap-2 Maneuver	*146	16	-	*324	16	-	-	-	-	-	-	-
Stage 1	*135	221	-	*246	343	-	-	-	-	-	-	-
Stage 2	*752	342	-	*682	221	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v25.26		11.68	0.07	0.04
HCM LOS	D	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	279	-	-	182	546	438	-
HCM Lane V/C Ratio	0.02	-	-	0.024	0.014	0.012	-
HCM Control Delay (s/veh)	18.2	-	-	25.3	11.7	13.3	-
HCM Lane LOS	C	-	-	D	B	B	-
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0	0	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↗	↑↑↑	↗	↗	↑↑↑	↗
Traffic Vol, veh/h	11	1	31	3	0	27	26	1269	10	19	1752	29
Future Vol, veh/h	11	1	31	3	0	27	26	1269	10	19	1752	29
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	12	1	34	3	0	29	28	1379	11	21	1904	32

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2554	3392	952	2239	3413	690	1936	0	0	1390	0	0
Stage 1	1946	1946	-	1436	1436	-	-	-	-	-	-	-
Stage 2	608	1447	-	804	1977	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*144	16	*674	*304	15	*753	285	-	-	443	-	-
Stage 1	*141	229	-	*238	337	-	-	-	-	-	-	-
Stage 2	*773	332	-	*692	218	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*118	13	*674	*231	13	*753	285	-	-	443	-	-
Mov Cap-2 Maneuver	*118	13	-	*231	13	-	-	-	-	-	-	-
Stage 1	*135	218	-	*214	304	-	-	-	-	-	-	-
Stage 2	*669	299	-	*624	208	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v28.14		11.19	0.38	0.14
HCM LOS	D	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	285	-	-	202	614	443	-	-
HCM Lane V/C Ratio	0.099	-	-	0.232	0.053	0.047	-	-
HCM Control Delay (s/veh)	19	-	-	28.1	11.2	13.5	-	-
HCM Lane LOS	C	-	-	D	B	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	0.9	0.2	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



Appendix J – Year 2032 No Build Capacity Analysis

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↙	↗	↙	↑↑↑	↑↑↑	↗
Traffic Vol, veh/h	59	27	30	1828	1148	50
Future Vol, veh/h	59	27	30	1828	1148	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	64	29	33	1987	1248	54

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2108	624	1302	0	-	0
Stage 1	1248	-	-	-	-	-
Stage 2	860	-	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-	-
Pot Cap-1 Maneuver	*379	*753	498	-	-	-
Stage 1	*378	-	-	-	-	-
Stage 2	*652	-	-	-	-	-
Platoon blocked, %	0	0	0	-	-	-
Mov Cap-1 Maneuver	*354	*753	498	-	-	-
Mov Cap-2 Maneuver	*322	-	-	-	-	-
Stage 1	*353	-	-	-	-	-
Stage 2	*652	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v16.11	16.11	0.21	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	498	-	322	753	-	-
HCM Lane V/C Ratio	0.065	-	0.199	0.039	-	-
HCM Control Delay (s/veh)	12.7	-	18.9	10	-	-
HCM Lane LOS	B	-	C	A	-	-
HCM 95th %tile Q(veh)	0.2	-	0.7	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	9	0	3	0	0	1	5	1857	2	5	1143	7
Future Vol, veh/h	9	0	3	0	0	1	5	1857	2	5	1143	7
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	0	3	0	0	1	5	2018	2	5	1242	8

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	2072	3285	621	2538	3291	1010	1250	0	0	2021	0	0
Stage 1	1253	1253	-	2030	2030	-	-	-	-	-	-	-
Stage 2	818	2032	-	508	1261	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*498	23	*746	*180	23	*626	550	-	-	311	-	-
Stage 1	*354	438	-	*166	249	-	-	-	-	-	-	-
Stage 2	*642	248	-	*766	434	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*483	23	*746	*174	22	*626	550	-	-	311	-	-
Mov Cap-2 Maneuver	*483	23	-	*174	22	-	-	-	-	-	-	-
Stage 1	*348	431	-	*164	246	-	-	-	-	-	-	-
Stage 2	*635	246	-	*749	426	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	11.96	10.76	0.03	0.07
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	550	-	-	530	626	311	-	-
HCM Lane V/C Ratio	0.01	-	-	0.025	0.002	0.017	-	-
HCM Control Delay (s/veh)	11.6	-	-	12	10.8	16.8	-	-
HCM Lane LOS	B	-	-	B	B	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	1	0	1	3	0	4	0	1846	0	7	1139	1
Future Vol, veh/h	1	0	1	3	0	4	0	1846	0	7	1139	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	1	3	0	4	0	2007	0	8	1238	1

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2056	3260	619	2517	3261	1003	1239	0	0	2007	0	0
Stage 1	1253	1253	-	2007	2007	-	-	-	-	-	-	-
Stage 2	803	2007	-	510	1254	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*482	27	*753	*216	27	*635	541	-	-	304	-	-
Stage 1	*337	425	-	*163	247	-	-	-	-	-	-	-
Stage 2	*652	247	-	*773	425	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*467	26	*753	*210	26	*635	541	-	-	304	-	-
Mov Cap-2 Maneuver	*467	26	-	*210	26	-	-	-	-	-	-	-
Stage 1	*329	415	-	*163	247	-	-	-	-	-	-	-
Stage 2	*647	247	-	*752	414	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.27		15.82	0	0.1
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	541	-	-	576	340	304	-	-
HCM Lane V/C Ratio	-	-	-	0.004	0.022	0.025	-	-
HCM Control Delay (s/veh)	0	-	-	11.3	15.8	17.1	-	-
HCM Lane LOS	A	-	-	B	C	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

5: Arizona Avenue & Driveway E

09/28/2023

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	24	2	42	2	0	16	47	1815	14	16	1066	57
Future Vol, veh/h	24	2	42	2	0	16	47	1815	14	16	1066	57
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	26	2	46	2	0	17	51	1973	15	17	1159	62

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2085	3284	579	2574	3330	986	1221	0	0	1988	0	0
Stage 1	1193	1193	-	2075	2075	-	-	-	-	-	-	-
Stage 2	891	2090	-	499	1255	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*435	21	*793	*137	19	*635	470	-	-	313	-	-
Stage 1	*291	392	-	*139	222	-	-	-	-	-	-	-
Stage 2	*652	217	-	*813	364	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*356	18	*793	*97	16	*635	470	-	-	313	-	-
Mov Cap-2 Maneuver	*356	18	-	*97	16	-	-	-	-	-	-	-
Stage 1	*274	370	-	*124	198	-	-	-	-	-	-	-
Stage 2	*565	193	-	*720	343	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v21.53		14.65	0.34	0.24
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	470	-	-	291	393	313	-	-
HCM Lane V/C Ratio	0.109	-	-	0.254	0.05	0.055	-	-
HCM Control Delay (s/veh)	13.6	-	-	21.5	14.6	17.2	-	-
HCM Lane LOS	B	-	-	C	B	C	-	-
HCM 95th %tile Q(veh)	0.4	-	-	1	0.2	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	0.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↙	↗	↙	↑↑↑	↑↑↑	↗
Traffic Vol, veh/h	37	50	26	1324	1780	55
Future Vol, veh/h	37	50	26	1324	1780	55
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	150	0	50	-	-	195
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	54	28	1439	1935	60

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2567	967	1995	0	-	0
Stage 1	1935	-	-	-	-	-
Stage 2	632	-	-	-	-	-
Critical Hdwy	5.74	7.14	5.34	-	-	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	3.12	-	-	-
Pot Cap-1 Maneuver	*105	*674	260	-	-	-
Stage 1	*176	-	-	-	-	-
Stage 2	*733	-	-	-	-	-
Platoon blocked, %	0	0	0	-	-	-
Mov Cap-1 Maneuver	*93	*674	260	-	-	-
Mov Cap-2 Maneuver	*138	-	-	-	-	-
Stage 1	*157	-	-	-	-	-
Stage 2	*733	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/v	23.87	0.39	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	260	-	138	674	-	-
HCM Lane V/C Ratio	0.109	-	0.292	0.081	-	-
HCM Control Delay (s/veh)	20.5	-	41.5	10.8	-	-
HCM Lane LOS	C	-	E	B	-	-
HCM 95th %tile Q(veh)	0.4	-	1.1	0.3	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	10	0	12	2	0	0	2	1328	2	3	1834	8
Future Vol, veh/h	10	0	12	2	0	0	2	1328	2	3	1834	8
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	0	13	2	0	0	2	1443	2	3	1993	9

Major/Minor	Minor2		Minor1			Major1		Major2				
Conflicting Flow All	2582	3450	997	2253	3458	723	2002	0	0	1446	0	0
Stage 1	2000	2000	-	1449	1449	-	-	-	-	-	-	-
Stage 2	582	1450	-	804	2009	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*314	21	*626	*424	20	*706	321	-	-	506	-	-
Stage 1	*178	261	-	*321	403	-	-	-	-	-	-	-
Stage 2	*724	403	-	*642	257	-	-	-	-	-	-	-
Platoon blocked, %	1	1	0	1	1	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*310	21	*626	*410	20	*706	321	-	-	506	-	-
Mov Cap-2 Maneuver	*310	21	-	*410	20	-	-	-	-	-	-	-
Stage 1	*177	259	-	*319	400	-	-	-	-	-	-	-
Stage 2	*720	400	-	*625	256	-	-	-	-	-	-	-

Approach	EB		WB			NB		SB		
HCM Control Delay, s/v13.92			13.83			0.02		0.02		
HCM LOS	B		B							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	321	-	-	428	410	506	-	-
HCM Lane V/C Ratio	0.007	-	-	0.056	0.005	0.006	-	-
HCM Control Delay (s/veh)	16.3	-	-	13.9	13.8	12.2	-	-
HCM Lane LOS	C	-	-	B	B	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0.2	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

09/28/2023

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	3	0	1	2	0	5	5	1311	3	5	1846	0
Future Vol, veh/h	3	0	1	2	0	5	5	1311	3	5	1846	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	0	1	2	0	5	5	1425	3	5	2007	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2598	3457	1003	2251	3455	714	2007	0	0	1428	0	0
Stage 1	2017	2017	-	1438	1438	-	-	-	-	-	-	-
Stage 2	581	1439	-	813	2017	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*175	15	*635	*422	15	*753	304	-	-	421	-	-
Stage 1	*159	243	-	*237	336	-	-	-	-	-	-	-
Stage 2	*773	336	-	*652	243	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*168	15	*635	*409	15	*753	304	-	-	421	-	-
Mov Cap-2 Maneuver	*168	15	-	*409	15	-	-	-	-	-	-	-
Stage 1	*157	240	-	*233	330	-	-	-	-	-	-	-
Stage 2	*754	330	-	*642	240	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v22.84		11	0.06	0.04
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	304	-	-	206	607	421	-	-
HCM Lane V/C Ratio	0.018	-	-	0.021	0.013	0.013	-	-
HCM Control Delay (s/veh)	17	-	-	22.8	11	13.7	-	-
HCM Lane LOS	C	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

5: Arizona Avenue & Driveway E

09/28/2023

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑		↕	↑↑↑	↕
Traffic Vol, veh/h	12	1	33	3	0	28	27	1296	11	20	1802	31
Future Vol, veh/h	12	1	33	3	0	28	27	1296	11	20	1802	31
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	13	1	36	3	0	30	29	1409	12	22	1959	34

Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	2624	3482	979	2295	3503	704	1992	0	0	1421	0	0
Stage 1	2002	2002	-	1467	1467	-	-	-	-	-	-	-
Stage 2	622	1479	-	828	2036	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*163	14	*635	*379	13	*753	311	-	-	425	-	-
Stage 1	*165	249	-	*224	323	-	-	-	-	-	-	-
Stage 2	*773	319	-	*652	236	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*135	12	*635	*284	12	*753	311	-	-	425	-	-
Mov Cap-2 Maneuver	*135	12	-	*284	12	-	-	-	-	-	-	-
Stage 1	*156	236	-	*203	293	-	-	-	-	-	-	-
Stage 2	*672	288	-	*581	224	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v27.85		10.85	0.36	0.15
HCM LOS	D	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	311	-	-	207	649	425	-	-
HCM Lane V/C Ratio	0.094	-	-	0.242	0.052	0.051	-	-
HCM Control Delay (s/veh)	17.8	-	-	27.9	10.8	13.9	-	-
HCM Lane LOS	C	-	-	D	B	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	0.9	0.2	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



Appendix K – Year 2032 Build Capacity Analysis

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘		↗		↕		↘	↑↑↑		↑↑↑	↗	
Traffic Vol, veh/h	59	0	27	13	0	28	30	1892	4	9	1164	50
Future Vol, veh/h	59	0	27	13	0	28	30	1892	4	9	1164	50
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	0	-	-	-	50	-	-	-	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	64	0	29	14	0	30	33	2057	4	10	1265	54

Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	2173	-	633	2650	3463	1030	1320	0	0	2061	0	0
Stage 1	1285	-	-	2124	2124	-	-	-	-	-	-	-
Stage 2	888	-	-	526	1339	-	-	-	-	-	-	-
Critical Hdwy	6.44	-	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	-	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	-	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	-	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*379	0	*753	*120	13	*635	487	-	-	279	-	-
Stage 1	*317	0	-	*124	206	-	-	-	-	-	-	-
Stage 2	*652	0	-	*773	382	-	-	-	-	-	-	-
Platoon blocked, %	0		0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*321	-	*753	*102	12	*635	487	-	-	279	-	-
Mov Cap-2 Maneuver	*321	-	-	*102	12	-	-	-	-	-	-	-
Stage 1	*303	-	-	*116	192	-	-	-	-	-	-	-
Stage 2	*579	-	-	*708	364	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v16.15		23.42	0.2	0.14
HCM LOS	C	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	487	-	-	321	753	240	279	-	-
HCM Lane V/C Ratio	0.067	-	-	0.2	0.039	0.186	0.035	-	-
HCM Control Delay (s/veh)	12.9	-	-	19	10	23.4	18.4	-	-
HCM Lane LOS	B	-	-	C	A	C	C	-	-
HCM 95th %tile Q(veh)	0.2	-	-	0.7	0.1	0.7	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

2: Arizona Avenue & Driveway B

01/10/2024

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↕↕		↕	↕↕↕	↕
Traffic Vol, veh/h	9	0	3	30	0	64	5	1861	11	16	1156	7
Future Vol, veh/h	9	0	3	30	0	64	5	1861	11	16	1156	7
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	0	3	33	0	70	5	2023	12	17	1257	8

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2111	3337	628	2577	3339	1017	1264	0	0	2035	0	0
Stage 1	1291	1291	-	2040	2040	-	-	-	-	-	-	-
Stage 2	820	2046	-	537	1299	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*367	17	*746	*124	17	*626	540	-	-	304	-	-
Stage 1	*329	417	-	*162	245	-	-	-	-	-	-	-
Stage 2	*642	243	-	*766	413	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*304	16	*746	*115	16	*626	540	-	-	304	-	-
Mov Cap-2 Maneuver	*304	16	-	*115	16	-	-	-	-	-	-	-
Stage 1	*310	394	-	*160	243	-	-	-	-	-	-	-
Stage 2	*565	240	-	*719	390	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v15.47		23.19	0.03	0.24
HCM LOS	C	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	540	-	-	357	115	626	304	-	-
HCM Lane V/C Ratio	0.01	-	-	0.037	0.283	0.111	0.057	-	-
HCM Control Delay (s/veh)	11.7	-	-	15.5	48.2	11.5	17.6	-	-
HCM Lane LOS	B	-	-	C	E	B	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1	1.1	0.4	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

01/10/2024

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	1	0	1	3	0	4	0	1861	0	7	1182	1
Future Vol, veh/h	1	0	1	3	0	4	0	1861	0	7	1182	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	1	3	0	4	0	2023	0	8	1285	1

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2109	3323	642	2552	3324	1011	1286	0	0	2023	0	0
Stage 1	1300	1300	-	2023	2023	-	-	-	-	-	-	-
Stage 2	809	2023	-	529	1301	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*482	23	*753	*197	23	*635	509	-	-	296	-	-
Stage 1	*308	401	-	*157	241	-	-	-	-	-	-	-
Stage 2	*652	241	-	*773	400	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*467	22	*753	*192	22	*635	509	-	-	296	-	-
Mov Cap-2 Maneuver	*467	22	-	*192	22	-	-	-	-	-	-	-
Stage 1	*300	391	-	*157	241	-	-	-	-	-	-	-
Stage 2	*647	241	-	*752	390	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.27		16.56	0	0.1
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	509	-	-	576	319	296	-	-
HCM Lane V/C Ratio	-	-	-	0.004	0.024	0.026	-	-
HCM Control Delay (s/veh)	0	-	-	11.3	16.6	17.5	-	-
HCM Lane LOS	A	-	-	B	C	C	-	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↗	↑↑↑	↗	↗	↑↑↑	↗
Traffic Vol, veh/h	24	2	42	2	0	16	47	1830	14	16	1109	57
Future Vol, veh/h	24	2	42	2	0	16	47	1830	14	16	1109	57
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	26	2	46	2	0	17	51	1989	15	17	1205	62

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2138	3347	603	2609	3393	995	1267	0	0	2004	0	0
Stage 1	1240	1240	-	2091	2091	-	-	-	-	-	-	-
Stage 2	898	2107	-	518	1302	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*482	21	*753	*170	18	*635	521	-	-	305	-	-
Stage 1	*345	433	-	*134	217	-	-	-	-	-	-	-
Stage 2	*652	212	-	*773	400	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*399	18	*753	*122	16	*635	521	-	-	305	-	-
Mov Cap-2 Maneuver	*399	18	-	*122	16	-	-	-	-	-	-	-
Stage 1	*326	408	-	*121	195	-	-	-	-	-	-	-
Stage 2	*572	191	-	*681	377	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v20.92		13.72	0.31	0.24
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	521	-	-	299	432	305	-
HCM Lane V/C Ratio	0.098	-	-	0.247	0.045	0.057	-
HCM Control Delay (s/veh)	12.7	-	-	20.9	13.7	17.5	-
HCM Lane LOS	B	-	-	C	B	C	-
HCM 95th %tile Q(veh)	0.3	-	-	1	0.1	0.2	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	2.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵		↶		↕		↵	↑↑↑		↑↑↑		↶
Traffic Vol, veh/h	37	0	50	11	0	8	26	1342	8	25	1826	55
Future Vol, veh/h	37	0	50	11	0	8	26	1342	8	25	1826	55
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	0	-	-	-	50	-	-	-	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	40	0	54	12	0	9	28	1459	9	27	1985	60

Major/Minor	Minor2	Minor1		Major1		Major2						
Conflicting Flow All	2679	-	992	2368	3618	734	2045	0	0	1467	0	0
Stage 1	2039	-	-	1520	1520	-	-	-	-	-	-	-
Stage 2	640	-	-	848	2099	-	-	-	-	-	-	-
Critical Hdwy	6.44	-	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	-	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	-	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	-	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*69	0	*635	*136	7	*714	286	-	-	473	-	-
Stage 1	*151	0	-	*263	354	-	-	-	-	-	-	-
Stage 2	*733	0	-	*652	214	-	-	-	-	-	-	-
Platoon blocked, %	0		0	0	0	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*56	-	*635	*102	6	*714	286	-	-	473	-	-
Mov Cap-2 Maneuver	*56	-	-	*102	6	-	-	-	-	-	-	-
Stage 1	*137	-	-	*237	319	-	-	-	-	-	-	-
Stage 2	*652	-	-	*541	194	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v76.73		30.95	0.36	0.17
HCM LOS	F	D		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	286	-	-	56	635	159	473	-	-
HCM Lane V/C Ratio	0.099	-	-	0.723	0.086	0.13	0.057	-	-
HCM Control Delay (s/veh)	18.9	-	-	165.3	11.2	30.9	13.1	-	-
HCM Lane LOS	C	-	-	F	B	D	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	3	0.3	0.4	0.2	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

2: Arizona Avenue & Driveway B

01/10/2024

Intersection												
Int Delay, s/veh	0.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↕↕		↕	↕↕↕	↕
Traffic Vol, veh/h	10	0	12	24	0	18	2	1336	25	46	1845	8
Future Vol, veh/h	10	0	12	24	0	18	2	1336	25	46	1845	8
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	195
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	0	13	26	0	20	2	1452	27	50	2005	9

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2691	3589	1003	2372	3584	740	2014	0	0	1479	0	0
Stage 1	2105	2105	-	1470	1470	-	-	-	-	-	-	-
Stage 2	585	1484	-	902	2114	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*230	13	*626	*424	13	*706	315	-	-	482	-	-
Stage 1	*139	221	-	*308	392	-	-	-	-	-	-	-
Stage 2	*724	385	-	*642	218	-	-	-	-	-	-	-
Platoon blocked, %	1	1	0	1	1	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*199	12	*626	*370	12	*706	315	-	-	482	-	-
Mov Cap-2 Maneuver	*199	12	-	*370	12	-	-	-	-	-	-	-
Stage 1	*125	198	-	*305	389	-	-	-	-	-	-	-
Stage 2	*699	382	-	*563	195	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v17.29		13.24	0.02	0.32
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	315	-	-	317	370	706	482	-	-
HCM Lane V/C Ratio	0.007	-	-	0.076	0.071	0.028	0.104	-	-
HCM Control Delay (s/veh)	16.5	-	-	17.3	15.5	10.2	13.3	-	-
HCM Lane LOS	C	-	-	C	C	B	B	-	-
HCM 95th %tile Q(veh)	0	-	-	0.2	0.2	0.1	0.3	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

4: Arizona Avenue & Driveway D

01/10/2024

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		↕
Traffic Vol, veh/h	3	0	1	2	0	5	5	1344	3	5	1881	0
Future Vol, veh/h	3	0	1	2	0	5	5	1344	3	5	1881	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	-	50	-	265
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	0	1	2	0	5	5	1461	3	5	2045	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2651	3530	1022	2302	3529	732	2045	0	0	1464	0	0
Stage 1	2055	2055	-	1473	1473	-	-	-	-	-	-	-
Stage 2	595	1475	-	829	2055	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*217	15	*635	*442	15	*714	286	-	-	475	-	-
Stage 1	*146	229	-	*289	377	-	-	-	-	-	-	-
Stage 2	*733	376	-	*652	229	-	-	-	-	-	-	-
Platoon blocked, %	1	1	0	1	1	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*208	14	*635	*428	14	*714	286	-	-	475	-	-
Mov Cap-2 Maneuver	*208	14	-	*428	14	-	-	-	-	-	-	-
Stage 1	*144	227	-	*283	370	-	-	-	-	-	-	-
Stage 2	*713	369	-	*643	227	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s/v19.63			11.08		0.07		0.03	
HCM LOS	C		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	286	-	-	250	599	475	-	-
HCM Lane V/C Ratio	0.019	-	-	0.017	0.013	0.011	-	-
HCM Control Delay (s/veh)	17.8	-	-	19.6	11.1	12.7	-	-
HCM Lane LOS	C	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑↑	↕	↕	↑↑↑	↕
Traffic Vol, veh/h	12	1	33	3	0	28	27	1329	11	20	1837	31
Future Vol, veh/h	12	1	33	3	0	28	27	1329	11	20	1837	31
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	160	50	-	200
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	13	1	36	3	0	30	29	1445	12	22	1997	34

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2677	3555	998	2346	3577	722	2030	0	0	1457	0	0
Stage 1	2040	2040	-	1503	1503	-	-	-	-	-	-	-
Stage 2	637	1515	-	843	2074	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	*201	14	*635	*442	13	*714	293	-	-	480	-	-
Stage 1	*151	235	-	*272	362	-	-	-	-	-	-	-
Stage 2	*733	356	-	*652	223	-	-	-	-	-	-	-
Platoon blocked, %	1	1	0	1	1	0	0	-	-	0	-	-
Mov Cap-1 Maneuver	*166	12	*635	*329	11	*714	293	-	-	480	-	-
Mov Cap-2 Maneuver	*166	12	-	*329	11	-	-	-	-	-	-	-
Stage 1	*144	224	-	*245	326	-	-	-	-	-	-	-
Stage 2	*631	321	-	*584	213	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v26.23		10.92	0.37	0.14
HCM LOS	D	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	293	-	-	219	641	480	-
HCM Lane V/C Ratio	0.1	-	-	0.228	0.053	0.045	-
HCM Control Delay (s/veh)	18.7	-	-	26.2	10.9	12.8	-
HCM Lane LOS	C	-	-	D	B	B	-
HCM 95th %tile Q(veh)	0.3	-	-	0.9	0.2	0.1	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



City Council Memorandum Development Services Memo No. 24-055FA

Date: November 04, 2024
To: Mayor and Council
Thru: Joshua H. Wright, City Manager
Andy Bass, Deputy City Manager
Kevin Snyder, Development Services Director
From: Lauren Schumann, Planning Senior Program Manager
Subject: PLH24-0023 The District Downtown
Final Adoption of Ordinance No. 5112
Request: Rezoning from Planned Area Development (PAD) for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) to PAD for mixed use including multi-family, office, commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet
Location: Northwest corner Arizona Avenue and the SanTan 202 Freeway
Applicant: Brennan Ray; Burch & Cracchiolo, P.A.

Proposed Motion:

Rezoning

Move City Council adopt Ordinance No. 5112 approving PLH24-0023 The District Downtown, Rezoning from PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) to PAD for mixed use including multi-family, office, and commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet, subject to the conditions as recommended by Planning and Zoning Commission.

Background Data:

- Site is 44.7 acres and vacant
- Subject site received PAD zoning for office, retail, hotel with convention center, and mixed use commercial/residential in 1987 as part of a larger master plan that included the adjacent Stonebridge Ranch Apartment Complex
- Zoning extended in 1989 and vested when the apartments were constructed in 2000
- Zoning amended in 2007 to allow a Mid-Rise overlay with heights up to 120 feet for the entire site
- Time extension was approved in 2010
- A rezoning to add multi-family residential within the mixed use development occurred in 2022 with stipulations pertaining to phasing of development
- The applicant requests rezoning to add automobile and truck sales to permitted uses within the mixed-use development, eliminate phasing stipulations, and revised the westernmost multi-family phase from a wrapped parking garage to surface parked development
- Proposed PDP establishes where uses can occur and conceptual building renderings are provided to establish quality of design; a Mid-Rise Overlay is permitted over the entire site allowing building heights up to 120 feet
- If approved, mixed uses include two multi-family developments totaling 800 units, two office buildings, two hotels, two automotive dealerships, and supporting commercial retail at the street intersection

Surrounding Land Use Data:

North	Pecos Road, then multi-family	South	SanTan Freeway, then commercial shopping center
East	Arizona Avenue, then commercial shopping center	West	Multi-family

General Plan and Area Plan Designations:

Plan	Existing	Proposed
General Plan	Neighborhoods within Downtown Chandler Growth Area	No Change

Proposed Development

Proposed Land Use

Parcel 1 9.19 acres	Two automobile and truck dealerships
Parcel 2 8.93 acres	Commercial Uses including retail, restaurants, maximum of three drive-thru pads, and common outdoor community space Approximately 45,115 square feet commercial uses
Parcel 3 4.73 acres	2 two-story office buildings with outdoor plaza and parking Approximately 72,800 square feet of office
Parcel 4 6.02 acres	Multi-family residential 400 units; density 66.4 du/ac Units wrap parking garage
Parcel 5 12.35 acre	Multi-family residential 400 units; density 32.4 du/ac Surface parked
Parcel 6 & 7 4.01 acres	Two hotels each with 98 rooms; totaling 196 hotel rooms

Review and Recommendation

The subject site has been zoned for mixed-use development since 1987 and a Mid-Rise Overlay allowing for heights up to 120 feet since 2007. The site is the last undeveloped quadrant of the San Tan Freeway/Arizona Avenue interchange and occupies all vacant land bounded by Arizona Avenue, Pecos Road, existing multi-family to the west, and the Loop 202 San Tan Freeway. The other three interchange quadrants have been developed as retail power centers anchored by large box retailers. The applicant proposes to complete this intersection with a mixed-use development including high-density residential, office, and commercial uses.

Within the 2022 zoning case, stipulations were added to ensure more than one use would develop. Stipulations stated a portion of commercial must develop prior to the first phase of multi-family and all commercial buildings along Arizona Avenue were required to be built before the second phase of multi-family could occur. The developer has asked the phasing stipulations be removed to maintain flexibility while attracting potential tenant(s) and allow the property to develop under market demand.

The developer has provided a phasing plan; within the first phase, all landscaping

along Arizona Avenue and Pecos Road will be installed and the internal 'main street' will be built. The main internal drive has been revised to a curvilinear design with a focal point of open space between the different uses and tree-lined sidewalks will be provided throughout the development for connectivity. The second phase would include the commercial corner at Arizona Avenue and Pecos Road, one hotel, and the multi-family located along the western property line. The proposed multi-family development under phase two is surface parked and compatible with the existing surfaced parked multi-family to the immediate west. The new design shifts the buildings further to the east fronting onto the internal main street. The surface parked multi-family development will encompass the entire western property.

The applicant requests to add automobile and truck sales to the permitted uses, which are permitted by right within properties zoned Regional Commercial (C-3) district. The proposed dealerships are adjacent to the SanTan Freeway and could accommodate potentially two different dealerships. Staff is proposing a stipulation allowing only franchise dealerships, which have exclusive rights to sell new vehicles to the public for a specific manufacturer or brand. Franchise dealerships provide a larger selection of new vehicles and provide some used vehicles. The proposed dealerships request a digital sign along Arizona Avenue, which is consistent with signage permitted at other auto dealerships along the freeway within Chandler.

The applicant has included conceptual renderings and exhibits to establish the required level of quality and design for buildings, hardscape, and landscaping. Upon identifying the tenant and respective building needs, the applicant will submit for Administrative Design Review for site and building design. Staff will review the administrative submittal for consistency with the renderings, conceptual imagery, narrative, stipulations, and City Code. The conceptual Development Booklet and stipulations will result in a high-quality mixed-use development to complete the intersection and entrance to the downtown area.

Planning staff has reviewed and supports the request, citing consistency with the General Plan and goals of the Downtown Chandler Growth Area. The District Downtown will create a sense of place through site layout by providing a main street boulevard with tree-lined sidewalks, building awnings, patio areas, public outdoor plaza areas, landscaping, and cohesive architecture. Since 1987, the subject site has been intended to develop with employment uses and supporting commercial uses. The request will continue this vision and improve by adding a residential component of 800 units to further supplement the commercial during the non-office hours. Furthermore, the subject site has allowed for building heights up to 120 feet since 2007. Staff continues to support the request for Mid-Rise

Overlay allowing building heights up to 120 feet as the property abuts the freeway and a major intersection.

Planning and Zoning Commission recommends approval subject to conditions.

Traffic Analysis

A Traffic Impact Study was submitted to the City's Transportation Engineer and accepted. The developer will install a new signal along Pecos Road at Palm Lane to allow better access to the site and circulation within and around the development.

Public / Neighborhood Notification

- This request was noticed in accordance with the requirements of the Chandler Zoning Code.
- A neighborhood meeting sign was posted on the site and on social media via NextDoor.
- The neighborhood meeting was held on July 10, 2024. Four residents attended, asking general questions of the project including traffic, proposed uses, and construction timelines.
- As of the writing of this memo, Planning staff is not aware of any concerns or opposition to the request.

Planning and Zoning Commission Vote Report

Planning and Zoning Commission meeting September 18, 2024

Motion to Approve

In Favor: 5 Opposed: 1 (Heumann) Absent: 1 (Lopez)

The item was moved to the action agenda, as requested by Chair Heumann due to concerns with the proposed automobile dealership use within a mixed-use development, the proposed phasing plan, and the subject site being the southern gateway to Chandler's Downtown. The Chair expressed concerns stating dealerships tend to locate within an area with other dealerships and questioned the viability of two independent dealerships at this location. Furthermore, Chair Heumann questioned if dealerships were the right use for the gateway into the Downtown. The Commission also modified PDP stipulation #2, ensuring that non-residential development would occur prior to reviewing a PDP application for multi-family on Parcel 4.

Other commissioners had concerns with the proposed phasing plan and

thought the western multi-family would be constructed with limited commercial uses. The applicant proposed to amend the phasing plan stating 75% of the commercial retail buildings within Parcel 2 shall be constructed and receive a Certificate of Completion before a Preliminary Development Plan for the second multi-family can be filed. PDP stipulation number two has been amended to reflect the modified stipulation.

Recommended Conditions of Approval

Ordinance No. 5112 was introduced and tentatively adopted on October 17, 2024.

Rezoning

Planning and Zoning Commission recommends the City Council approve the Rezoning from PAD for mixed use development including multi-family, office, and commercial uses permitted under Community Commercial (C-2) to PAD for mixed use including multi-family, office, commercial uses permitted under the Community Commercial District (C-2) and automobile and truck sales with a Mid-Rise Overlay allowing for heights up to 120 feet, subject to the following conditions:

1. Development shall be in substantial conformance with the conceptual plans included in the Development Booklet, entitled, "The District Downtown" and kept on file in the City of Chandler Planning Division, in File No. PLH24-0023, modified by such conditions included at the time the Booklet was approved by the Chandler City Council and/or as thereafter amended, modified, or supplemented by Chandler City Council.
2. Building heights shall not exceed 120 (one hundred and twenty) feet in height as measured to the top of parapet of the building façade.
3. Residential density shall be permitted up to a maximum density of forty (40) dwelling units per acre for Parcel 5 and seventy (70) dwelling units per acre for Parcel 4.
4. Uses permitted within the office buildings include general office and medical office. Medical office shall be contingent upon compliance with minimum parking requirements per Zoning Code section 35-1804 Parking Schedule.
5. Users for the automobile and truck sales shall be limited to franchise dealerships only. Franchise dealerships are those dealerships that (a) meet the definition of a "franchisee" under A.R.S. 28-4301, (b) have received a New Motor Vehicle Dealer license from the Arizona Motor Vehicle Division to

sell new vehicles, and (c) have the legal right to sell new vehicles to the public for a specific manufacturer or brand.

6. Completion of the construction of all required off-site street improvements including but not limited to paving, landscaping, curb, gutter and sidewalks, median improvements and street lighting, to achieve conformance with City codes, standard details, and design manuals.
7. Right-of-way dedications to achieve full half-widths, including turn lanes and deceleration lanes, per the standards of the Chandler Transportation Plan.
8. The developer shall be required to install landscaping in the arterial street median(s) adjoining this project. In the event that the landscaping already exists within such median(s) the developer shall be required to upgrade such landscaping to meet current City Standards.
9. The landscaping and all other improvements in all open-spaces shall be maintained by the property owner or property owners' association and shall be maintained at a level consistent with or better than at the time of planting.
10. The landscaping in all rights-of-way shall be maintained by the adjacent property owner or property owners' association.
11. Minimum building setbacks shall be as follows:

Property Line Location	Minimum Building Setback
Arizona Avenue	30 feet
Pecos Road	30 feet
West	20 feet
South	10 feet

12. The developer shall pull a building permit on a hotel or a minimum of 25,000 square feet of non-residential uses within 18 months of the effective date of the Rezoning Ordinance. If this condition is not satisfied, then the developer shall be subject to a daily penalty of \$100 until a building permit is pulled.

Attachments

Ordinance 5112

Vicinity Maps

Development Booklet

2022 Approved Site Plan

ORDINANCE NO. 5112

AN ORDINANCE OF THE CITY OF CHANDLER, ARIZONA, AMENDING THE ZONING CODE AND MAP ATTACHED THERETO, BY REZONING A PARCEL FROM PLANNED AREA DEVELOPMENT (PAD) FOR MIXED USE DEVELOPMENT INCLUDING MULTI-FAMILY, OFFICE, AND COMMERCIAL USES AS PERMITTED UNDER COMMUNITY COMMERCIAL (C-2) TYPE USES TO PAD FOR MIXED USE DEVELOPMENT INCLUDING MULTI-FAMILY, OFFICE, AND COMMERCIAL USES AS PERMITTED UNDER COMMUNITY COMMERCIAL (C-2) USES AND AUTOMOBILE AND TRUCK SALES WITH MID-RISE OVERLAY ALLOWING HEIGHTS UP TO ONE-HUNDRED AND TWENTY (120) FEET IN CASE PLH24-0023 (THE DISTRICT DOWNTOWN) LOCATED NORTHWEST CORNER OF ARIZONA AVENUE AND SANTAN 202 FREEWAY WITHIN THE CORPORATE LIMITS OF THE CITY OF CHANDLER, ARIZONA; PROVIDING FOR THE REPEAL OF CONFLICTING ORDINANCES; AND PROVIDING FOR PENALTIES.

WHEREAS, an application for rezoning certain property within the corporate limits of Chandler, Arizona, has been filed in accordance with Article XXVI of the Chandler Zoning Code; and

WHEREAS, the application has been published in a local newspaper with general circulation in the City of Chandler, giving fifteen (15) days' notice of the time, place, and date of public hearing; and

WHEREAS, a notice of such hearing was posted on the property at least seven (7) days prior to the public hearing; and

WHEREAS, a public hearing was held by the Planning and Zoning Commission as required by the Zoning Code.

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Chandler, Arizona, as follows:

Section 1. Legal Description of Property:

EXHIBIT 'A'

Said parcel is hereby rezoned from PAD for office, retail, and commercial uses as permitted under Community Commercial (C-2) type uses to PAD for mixed use development including multi-family, office, and commercial uses as permitted under Community Commercial (C-2) uses and automobile and truck sales a with Mid-Rise Overlay allowing for building heights up to 120 feet, subject to the following conditions:

1. Development shall be in substantial conformance with the Development Booklet, entitled, "The District Downtown" and kept on file in the City of Chandler Planning Division, in File No. PLH24-0023, modified by such conditions included at the time the Booklet was approved by the Chandler City Council and/or as thereafter amended, modified or supplemented by Chandler Council.
2. Building heights shall not exceed 120 (one hundred and twenty) feet in height as measured to the top of parapet of the building façade.
3. Residential density shall be permitted up to a maximum density of forty (40) dwelling units per acre for Parcel 5 and seventy (70) dwelling units per acre for Parcel 4.
4. Uses permitted within the office buildings include general office and medical office. Medical office shall be contingent upon compliance with minimum parking requirements per Zoning Code section 35-1804 Parking Schedule.
5. Users for the automobile and truck sales shall be limited to franchise dealerships only. Franchise dealerships are those dealerships that (a) meet the definition of a "franchisee" under A.R.S. 28-4301, (b) have received a New Motor Vehicle Dealer license from the Arizona Motor Vehicle Division to sell new vehicles, and (c) have the legal right to sell new vehicles to the public for a specific manufacturer or brand.
6. Completion of the construction of all required off-site street improvements including but not limited to paving, landscaping, curb, gutter and sidewalks, median improvements and street lighting, to achieve conformance with City codes, standard details, and design manuals.
7. Right-of-way dedications to achieve full half-widths, including turn lanes and deceleration lanes, per the standards of the Chandler Transportation Plan.
8. The developer shall be required to install landscaping in the arterial street median(s) adjoining this project. In the event that the landscaping already exists within such median(s) the developer shall be required to upgrade such landscaping to meet current City Standards.
9. The landscaping and all other improvements in all open-spaces shall be maintained by the property owner or property owners' association and shall be maintained at a level consistent with or better than at the time of planting.
10. The landscaping in all rights-of-way shall be maintained by the adjacent property owner or property owners' association.
11. Minimum building setbacks shall be as follows:

Property Line Location	Minimum Building Setback
Arizona Avenue	30 feet
Pecos Road	30 feet
West	20 feet
South	10 feet

12. The developer shall pull a building permit on a hotel or a minimum of 25,000 square feet of non-residential uses within 18 months of the effective date of the Rezoning Ordinance. If this condition is not satisfied, then the developer shall be subject to a daily penalty of \$100 until a building permit is pulled.

Section 2. The Planning Division of the City of Chandler is hereby directed to enter such changes and amendments as may be necessary upon the Zoning Map of said Zoning Code in compliance with this Ordinance.

Section 3. All ordinances or parts of ordinances in conflict with the provisions of this Ordinance, or any parts hereof, are hereby repealed.

Section 4. In any case, where any building, structure, or land is used in violation of this Ordinance, the Planning Division of the City of Chandler may institute an injunction or any other appropriate action in proceeding to prevent the use of such building, structure, or land.

Section 5. If any section, subsection, sentence, clause, phrase or portion of this Ordinance is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, then this entire ordinance is invalid and shall have no force or effect.

Section 6. A violation of this Ordinance shall be a Class 1 misdemeanor subject to the enforcement and penalty provisions set forth in Section 1-8.3 of the Chandler City Code. Each day a violation continues, or the failure to perform any act or duty required by this Ordinance or the Zoning Code, shall constitute a separate offense.

INTRODUCED AND TENTATIVELY APPROVED by the City Council of the City of Chandler, Arizona, this ____ day of _____, 2024.

ATTEST:

CITY CLERK

MAYOR

PASSED AND ADOPTED by the City Council of the City of Chandler, Arizona, this ____ day of _____, 2024.

ATTEST:

CITY CLERK

MAYOR

CERTIFICATION

I HEREBY CERTIFY that the above and foregoing Ordinance No. 5112 was duly passed and adopted by the City Council of the City of Chandler, Arizona, at a regular meeting held on the ____ day of _____, 2024, and that a quorum was present thereat.

CITY CLERK

APPROVED AS TO FORM:

CITY ATTORNEY TA

Published:

“Exhibit A”

LEGAL DESCRIPTION

REMAINDER PARCEL

THAT PORTION OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 4, TOWNSHIP 2 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 4, FROM WHICH THE NORTH QUARTER CORNER OF SAID SECTION 4 BEARS SOUTH 89°35'07" WEST, A DISTANCE OF 2652.03 FEET;

THENCE ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER SOUTH 89°35'07" WEST, A DISTANCE OF 1761.37 FEET;

THENCE SOUTH 0°24'53" EAST, A DISTANCE OF 70.00 FEET TO A POINT ON THE SOUTH RIGHT OF WAY LINE OF PECOS ROAD;

THENCE ALONG SAID SOUTH RIGHT OF WAY LINE SOUTH 89°16'00" EAST, A DISTANCE OF 1012.29 FEET TO THE POINT OF BEGINNING;

THENCE CONTINUING ALONG SAID SOUTH RIGHT OF WAY LINE THE NEXT FOUR (4) CALLS;

SOUTH 89°16'00" EAST, A DISTANCE OF 185.93 FEET;

NORTH 89°35'07" EAST, A DISTANCE OF 149.80 FEET;

SOUTH 0°24'55" EAST, A DISTANCE OF 10.00 FEET;

NORTH 89°35'07" EAST, A DISTANCE OF 307.63 FEET;

THENCE SOUTH 41°04'53" EAST, A DISTANCE OF 24.58 FEET TO A POINT ON THE WEST RIGHT OF WAY LINE OF ARIZONA AVENUE;

THENCE ALONG SAID WEST RIGHT OF WAY LINE THE NEXT TWELVE (12) CALLS;

SOUTH 08°15'39" WEST, A DISTANCE OF 39.88 FEET;

SOUTH 0°38'48" WEST, A DISTANCE OF 13.94 FEET;

SOUTH 89°35'07" WEST, A DISTANCE OF 4.50 FEET;

SOUTH 0°38'37" WEST, A DISTANCE OF 48.51 FEET;

SOUTH 89°35'07" WEST, A DISTANCE OF 4.50 FEET;

SOUTH 0°38'48" WEST, A DISTANCE OF 12.55 FEET;

SOUTH 14°43'45" EAST, A DISTANCE OF 41.48 FEET;

SOUTH 0°38'48" WEST, A DISTANCE OF 236.06 FEET;

SOUTH 89°21'12" EAST, A DISTANCE OF 16.99 FEET;



Nicholas W. Jarrett

SOUTH 0°38'48" WEST, A DISTANCE OF 75.56 FEET TO THE BEGINNING OF A CURVE TO THE LEFT HAVING A RADIUS OF 3901.72 FEET, AND A CENTRAL ANGLE OF 5°00'15";

THENCE ALONG THE ARC OF SAID CURVE A DISTANCE OF 340.77 FEET;

SOUTH 04°21'27" EAST, A DISTANCE OF 206.09 FEET;

THENCE SOUTH 39°46'29" WEST, A DISTANCE OF 88.25 FEET TO THE NORTH RIGHT OF WAY LINE OF ARIZONA STATE ROUTE 202;

THENCE ALONG SAID NORTH RIGHT OF WAY FOR THE NEXT SIX(6) CALLS;

THENCE SOUTH 84°46'17" WEST, A DISTANCE OF 1169.47 FEET;

THENCE NORTH 65°15'21" WEST, A DISTANCE OF 12.62 FEET;

THENCE NORTH 65°15'21" WEST, A DISTANCE OF 15.23 FEET;

THENCE SOUTH 84°46'07" WEST, A DISTANCE OF 23.25 FEET;

THENCE SOUTH 53°46'56" WEST, A DISTANCE OF 27.03 FEET;

THENCE SOUTH 84°46'17" WEST, A DISTANCE OF 371.92 FEET;

THENCE NORTH 0°54'51" WEST, A DISTANCE OF 331.74 FEET;

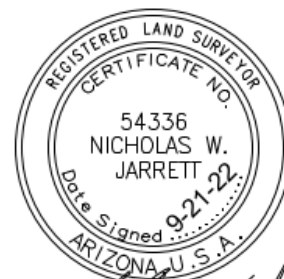
THENCE NORTH 89°05'09" EAST, A DISTANCE OF 431.22 FEET;

THENCE NORTH 0°38'39" WEST, A DISTANCE OF 343.87 FEET;

THENCE NORTH 89°35'05" EAST, A DISTANCE OF 574.11 FEET;

THENCE NORTH 0°24'55" WEST, A DISTANCE OF 567.64 FEET TO THE POINT OF BEGINNING.

CONTAINS 1,221,858 SQUARE FEET +/- OR 28.050 ACRES +/-.



Nicholas W. Jarrett

LEGAL DESCRIPTION

PARCEL 4

THAT PORTION OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 4, TOWNSHIP 2 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 4, FROM WHICH THE NORTH QUARTER CORNER OF SAID SECTION 4 BEARS SOUTH 89°35'07" WEST, A DISTANCE OF 2652.03 FEET;

THENCE ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER SOUTH 89°35'07" WEST, A DISTANCE OF 1761.37 FEET;

THENCE SOUTH 0°24'53" EAST, A DISTANCE OF 70.00 FEET TO A POINT ON THE SOUTH RIGHT OF WAY LINE OF PECOS ROAD;

THENCE ALONG SAID SOUTH RIGHT OF WAY LINE SOUTH 89°16'00" EAST, A DISTANCE OF 435.75 FEET TO THE POINT OF BEGINNING;

THENCE CONTINUING SOUTH 89°16'00" EAST, A DISTANCE OF 576.54 FEET;

THENCE SOUTH 0°24'55" EAST, A DISTANCE OF 567.64 FEET;

THENCE SOUTH 89°35'05" WEST, A DISTANCE OF 574.11 FEET;

THENCE NORTH 0°38'39" WEST, A DISTANCE OF 579.21 FEET TO THE POINT OF BEGINNING.

CONTAINS 329,862 +/- SQUARE FEET OR 7.572 ACRES +/-.



A handwritten signature in black ink that reads "Nicholas W. Jarrett".

LEGAL DESCRIPTION

PARCEL 5

THAT PORTION OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 4, TOWNSHIP 2 SOUTH, RANGE 5 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 4, FROM WHICH THE NORTH QUARTER CORNER OF SAID SECTION 4 BEARS SOUTH 89°35'07" WEST, A DISTANCE OF 2652.03 FEET;

THENCE ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER SOUTH 89°35'07" WEST, A DISTANCE OF 1761.37 FEET;

THENCE SOUTH 0°24'53" EAST, A DISTANCE OF 70.00 FEET TO A POINT ON THE SOUTH RIGHT OF WAY LINE OF PECOS ROAD AND TO THE POINT OF BEGINNING;

THENCE ALONG SAID SOUTH RIGHT OF WAY SOUTH 89°16'00" EAST, A DISTANCE OF 435.75 FEET;

THENCE SOUTH 0°38'39" EAST, A DISTANCE OF 923.14 FEET;

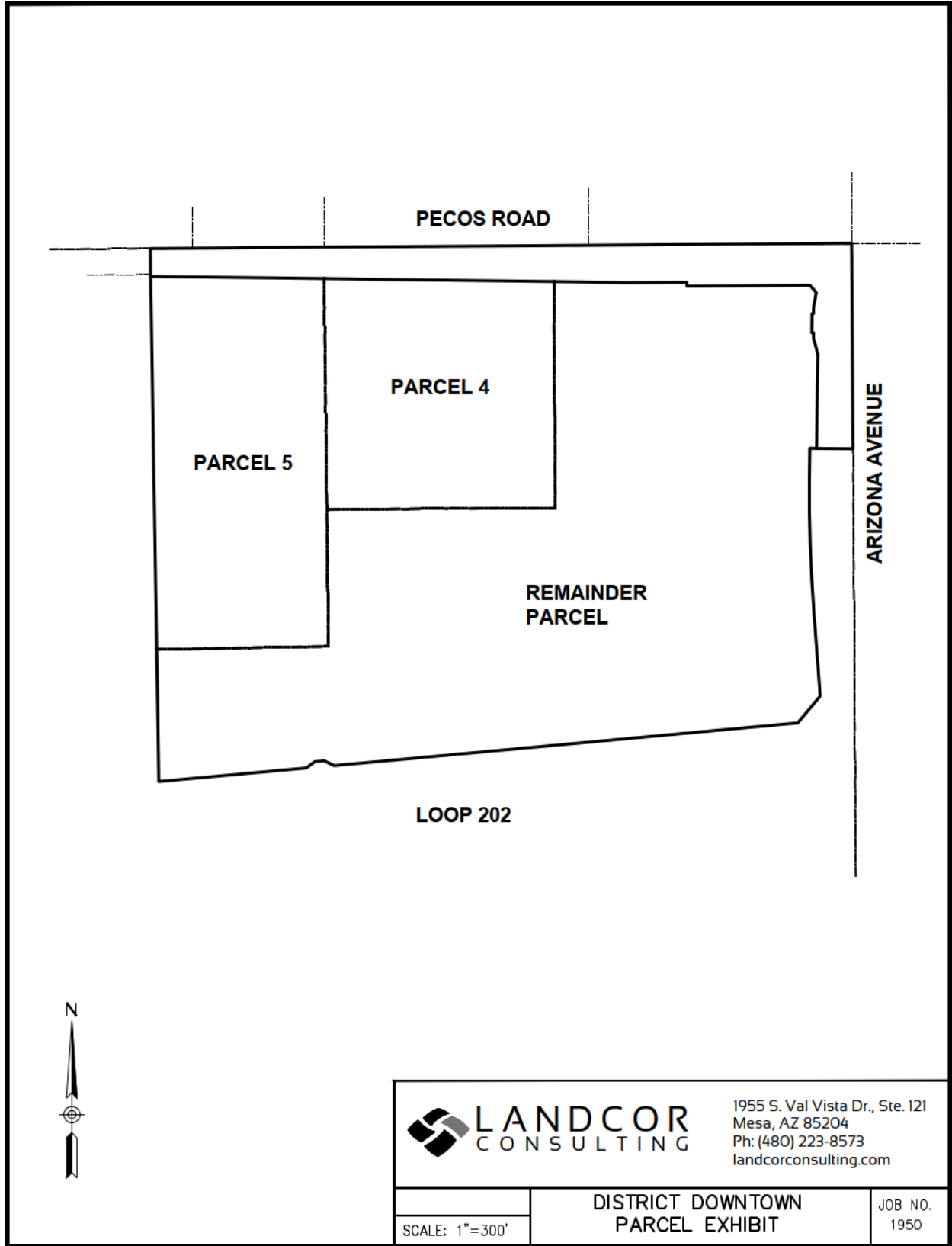
THENCE SOUTH 89°05'09" WEST, A DISTANCE OF 431.22 FEET;

THENCE NORTH 0°54'51" WEST, A DISTANCE OF 935.66 FEET TO THE POINT OF BEGINNING.

CONTAINS 402,810 SQUARE FEET +/- OR 9.247 ACRES +/-.



Nicholas W. Jarrett



PECOS ROAD

PARCEL 4

PARCEL 5

REMAINDER
PARCEL

LOOP 202

ARIZONA AVENUE



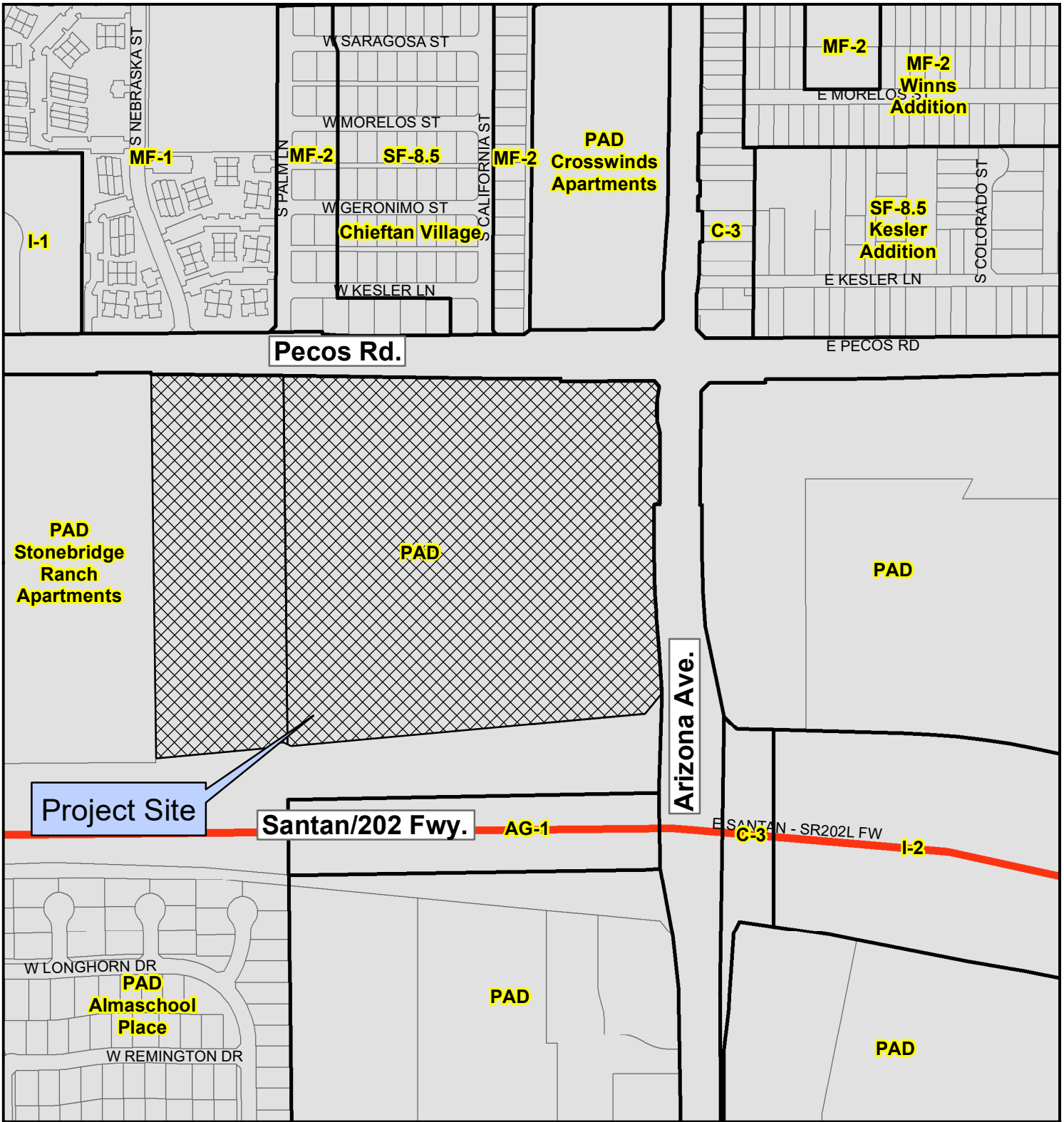
LANDCOR
CONSULTING

1955 S. Val Vista Dr., Ste. 121
Mesa, AZ 85204
Ph: (480) 223-8573
landcorconsulting.com

SCALE: 1"=300'

**DISTRICT DOWNTOWN
PARCEL EXHIBIT**

JOB NO.
1950



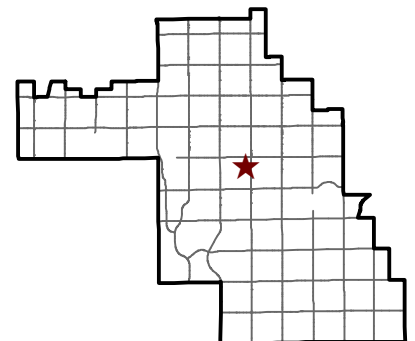
PLH24-0023 The District Downtown



Project Details
REZONE & PDP
 44.7 Acres
 Mixed-Use Multi-Family, Office,
 Community Commercial, and automobile
 and truck sales with a Mid-Rise Overlay
 allowing for heights up to 120 feet



City of Chandler Planning Division
chandleraz.gov/planning
 For more information visit:
<https://gis.chandleraz.gov/planning>





Project Site

Santan/202 Fwy.

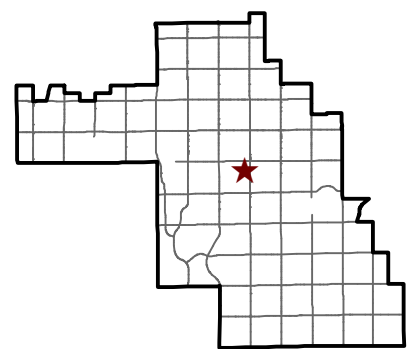
Arizona Ave.

 **PLH24-0023 The District Downtown**

Project Details
REZONE & PDP
 44.7 Acres
 Mixed-Use Multi-Family, Office,
 Community Commercial, and automobile
 and truck sales with a Mid-Rise Overlay
 allowing for heights up to 120 feet



City of Chandler Planning Division
chandleraz.gov/planning
 For more information visit:
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Planned Area Development
and
Preliminary Development Plan

The District Downtown

**Northwest Corner of
Arizona Avenue and Loop 202**

by:

Meridian West AZ/202, LLC

Case No: PLH24-0023

Submitted: June 4, 2024
Updated: September 4, 2024
Updated: September 11, 2024

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TABLE OF EXHIBITS

Exhibit No.	Description
1	Aerial & Vicinity Map
2	Conceptual Site Plan
3	Non-Residential Building Perspectives
4	Multi-Family Residential Concept
5	Colors & Materials Palette
6	Landscape & Open Space Character
7	Conceptual Master Plan – Plant Palette
8	Comprehensive Sign Plan
9	Conceptual Phasing Plan
10	Conceptual Grading & Drainage

THE DISTRICT DOWNTOWN

I. INTRODUCTION

Meridian West AZ/202, LLC (“Meridian West”) is the owner of The District Downtown, an exciting mixed-use development on approximately 49.30± gross (44.73± net) acres (the “Site”) at the northwest corner of Arizona Avenue and the Loop 202 Freeway. *See Exhibit 1, Aerial & Vicinity Map.* The District Downtown will be a high-quality, dynamic mixed-used development containing a mix of office, commercial, and multi-family uses. The design, layout, and theming create a high-quality development that will be a recognizable presence and strong, viable development for the City of Chandler (the “City”) given its location proximate to the Loop 202, the Price Road Corridor, and Chandler Airport. The District Downtown creates shopping and employment opportunities in the immediate area and is anticipated to improve the City’s jobs-to-population ratio while at the same time provide much needed housing to the City and immediate area. To achieve this, Meridian West seeks rezone the Site to Planned Area Development (PAD) and Preliminary Development Plan (PDP) approval.

II. SITE, SURROUNDING AREA, AND EXISTING ZONING

The Site is designated on the City’s General Plan as Neighborhoods and lies within the Downtown Chandler Growth Area. Within the Neighborhood designation, mixed-use development containing residential, commercial, and/or office can be considered at the intersection of major arterials, freeway interchanges with arterial streets, commercial areas, Downtown, and high-capacity transit corridors. Urban residential exceeding 18 du/ac. can be considered in Downtown, in regional commercial areas, and within designated high-capacity transit corridors. The Site is zoned PAD Mixed-Use for a range of uses including commercial, office, and multi-family residential.

Arizona Avenue is designated as a High-Capacity Transit Corridor. High-capacity transit corridors are appropriate locations for the development of high-intensity, mixed-land uses. Such developments would create opportunities for living within walking distance of schools, stores, and restaurants. For some people this means less travel time, cost savings and convenience, and better quality of life.

The Downtown Chandler Growth Area includes the historic Downtown square and extends south to the Loop 202. This Growth Area encourages a vibrant live, work, and play atmosphere. It additionally encourages developing the Growth Area with higher densities, mixed-uses, and transit-oriented development.

The proposed high-quality, mixed-used development is consistent with the General Plan, High-Capacity Transit Corridor, and the Downtown Chandler Growth Area.

III. PROPOSED PLANNED AREA DEVELOPMENT

Meridian West requests the Site be rezoned from PAD Mixed-Use to PAD Mixed-Use for a range of uses including **commercial** (Any use or similar use in the C-2 District of the City’s Zoning Ordinance, including automobile and truck sales); **office** (professional, business, administrative, executive, and other offices); and **multi-family residential**.

IV. PROPOSED PRELIMINARY DEVELOPMENT PLAN

A. *Site Layout*

The District Downtown is designed as a high-quality, mixed-use development that will create vital employment, retail, and housing opportunities at the gateway to Downtown Chandler,

adjacent to major transportation corridors (Arizona Avenue/State Route 87 and the Loop 202), and near the City's primary employment areas (Price Road Corridor and the Chandler Airpark). *See Exhibit 2, Conceptual Site Plan.*

The District Downtown is organized around a uniform street network which connects to a centralized main street that terminates at a central park and open space area, then continues throughout the Site in an appropriate manner that provides clear, distinctive routes for both vehicles and pedestrians to the planned uses. The centralized main street links to Arizona Avenue which compresses as it enters the Site to slow traffic down and create a pedestrian friendly environment. Thoughtfully planned pedestrian experiences are placed throughout the site using organic and structural shade elements. This provides opportunities for shaded seating, where enhanced decorative paving and patterned landscape invite patrons, residents, or employees to enjoy the urban environment. Pedestrian connections are planned between the different uses through defined paths, further emphasizing the mixed-use nature of the development. Unique to the development is an open space/park area near the west end of the main street, near the commercial retail uses. This area may contain design elements and features such as a shaded seating, urban street furniture, patterned landscape, and decorative lighting to attract individuals, couples, and families to the development.

Commercial uses are conveniently located on the eastern most portion of the Site with additional access points provided along Pecos Road. Retail commercial buildings are planned to be oriented along the central main street as well as Arizona Avenue, locating the majority of the parking internal to the Site. In the south-central portion of the site, hotel uses are planned. This use is located near the freeway corridor for higher visibility and scale. Two, 2-story offices are planned along Pecos Road, with an additional multi-family parcel centrally located on the Site, south of the offices. The intentional layout allows for transitions in land use from the west to the east. Specific details on proposed development, uses, etc. are found in *Exhibit 2*.

This PDP depicts the general layout ("Site Plan"), general appearance ("Elevations"), and theming ("Theming") of The District Downtown, but it does not restrict uses or buildings to the building locations or square footages as depicted on the Site Plan or appearance as depicted by the Elevations or Theming. Adjustments in Site Plan configuration to accommodate change in uses, alternative building design or geometry, layout of buildings within the Site, pedestrian and vehicular circulation within the Site, parking or other Site specific criteria as well as, adjustments in buildings floor area, and changes in the building architecture and theming will be allowed as long as each final building or buildings and individual lots and theming within the Site: (1) meets the general intent of the proposed Site Plan; (2) meets the level of quality and finish represented in the proposed building elevations of the Elevations and Theming; (3) comply with the below Design Guidelines; (4) use the colors and materials identified on the Color and Materials Palette or use compatible colors and materials as identified on the Color and Materials Palette; (5) provide code required parking stalls or demonstrate conformance with the City's shared parking requirements; and (6) meet building setbacks as required by the City's Zoning Ordinance or as otherwise identified in this PAD. Actual building footprints, layout within a lot or lots' developable area, and theming shall be determined administratively through the Administrative Design Review approval processes.

Notwithstanding the preceding, a separate Preliminary Development Plan must be submitted and approved by the City Council for the proposed multi-family development on Parcel 4 as shown on *Exhibit 2, Conceptual Site Plan.*

B. Architecture

Non-Residential

The overall theme of the non-residential buildings is an interpretation of a modern southwest look. *See Exhibit 3, Non-Residential Building Perspective.* The buildings incorporate large overhangs for sun screening and to create shaded areas for pedestrians and occupants. The 2-story office buildings feature roof top decks with shaded outdoor areas. Landscape boxes are utilized to drape planting materials over the building façade at the balcony locations and at the upper pedestrian walkway connecting the two buildings. A significant amount of glass and steel is utilized in the building facades to give a modern tech look while still maintaining a design theme consistent with surrounding structures. The commercial/retail buildings are designed to be pedestrian friendly with seating areas and shading for protection from the direct sun. Certain elements from the site sign design package are utilized and incorporated into the building design as well. This can be seen in areas such as the exterior column details on the office and commercial buildings. This helps to create a cohesive and comprehensive overall development plan.

Building materials are similar and consistent. Window sizes reflect a more appropriate sizing for the intended use and incorporate overhangs for sun screening. Building materials include natural stone, composite metal panels, Hardie panels and EIFS where appropriate, and a generous use of glass and aluminum storefront windows on the commercial buildings and curtain walls on the office buildings. Colors range from light and dark earth tones to incorporate the surrounding Sonoran Desert background, to modern metallic blues and greys to signify a new state-of-the-art development. *See Exhibit 5, Color & Materials Palette.*

Residential

The building architecture is anticipated to include varied massing, varying roof parapets, architectural features, stoops, materials, and façade detailing found in many modern, contemporary luxury multi-family developments found in the Phoenix Metro-area and Southwest region. *See Exhibit 4, Multi-Family Residential Concept.* The building massing includes a series of towers and recessed facades and patios. The massing and detailing emphasize the promotion of pedestrian activity via lighting, trees, and shade structures (such as awnings and roof overhangs). The color scheme is a blend of shades of brown, white, and tan with some complementary accent colors found within the Sonoran Desert color palette and is consistent and appropriate with the surrounding building context.

Architectural Design Guidelines

To ensure the future building elevations are designed consistent with the expectations set forth in the PDP, Meridian West proposes the following design guidelines. These design guidelines are intended to create a cohesive, attractive, and appropriate architectural statement. The guidelines provided in this document are not absolute, but are general statements aimed at setting forth the design expectations:

- All building elevations should maintain the same visual integrity, cohesiveness, and design detail.
- All building elevations should consider the use of multiple heights, wall planes, masses, and exterior materials along with natural elements, and complementary colors and texture to other building elevations within the Site.
- All building elevations should incorporate elements of the building elevations approved in this PDP. Where building elevations may expand in scale, simple relief may be used by including various architectural features such as an EIFS relief, a wainscot system, change in material or color on the surface that will be expanded

(where one building plane meets another, a change in material or color will add interest).

- Building elevations should be designed to respond to the harsh southwest climate and oriented to recognize the value of appropriate landscaping and cooling requirements in harmony with the environment.
- Human scale massing and proportions should complement the building elevation function and the design should be harmonious with adjoining developments.
- Canopies, arcades, and overhangs should be designed to create places of refuge for pedestrians and to create interest within the building design.
- Landscape buffers will be designed for the street edges and to emphasize the points of entry into the development.
- Identify internal crosswalks with changes in paving materials, signs, or paint striping.

The design of the future building elevations and theming may be administratively approved by Staff upon a finding that the proposed building elevations and theming are substantially consistent with the *Non-Residential Building Perspectives (Exhibit 3)*, *Multi-Family Residential Concept (Exhibit 4)*, and these Architectural Design Guidelines.

C. Landscaping Theme and Design Considerations

The overall landscape theme incorporates low water use, regionally appropriate plant material that is suitable for the Sonoran Desert to create an attractive experience for those who work, shop, and live at The District Downtown as well as those travelling along the Loop 202, Arizona Avenue, and Pecos Road. The landscaping theme draws from urban forms aimed to complement the scale and texture of the buildings. *See Exhibit 6, Landscape & Open Space Character*. The design is intended to include striking patterns and layering of plant material at entries and pedestrian spaces to emphasize key pedestrian and vehicular routes. Trees will be placed along the streets in formal patterns to provide much needed shade on sidewalks to create a unique aesthetic with contrasting canopies and understory planting. The overall cohesive design will establish The District Downtown as a destination within the south downtown area of Chandler.

All of the plant species proposed adhere to the Arizona Department of Water Resources, “Low Water Use Plant List” in order to incorporate native and hybrid arid region vegetation into the landscape. *See Exhibit 7, Conceptual Master Plan – Plant Palette*. All of the landscape within the boundary of the Site will be watered on an automatic drip irrigation system.

D. Entry Monumentation and Screening

Entry monumentation and signage for the Site will be designed to complement the overall building architecture. Scale, both horizontal and vertical, will be used, where appropriate, to emphasize primary access points and landmark features. Colors and materials proposed will highlight the quality and level of finish of the building elevations.

Proper screening of the parking areas will be accomplished through earthen berms and decorative walls that have been designed to complement the building and utilizing varying colors and textures arranged in an attractive design.

E. Development Standards

Meridian West is proposing the following development standards:

Regulation	Development Standard
Building Height (max.)	120 ft., inclusive of parapet walls, mechanical equipment, and screening
Building Setback (min.)	
- Front (Arizona Ave.)	30 ft.
- Side (Pecos Rd.)	30 ft.
- Side (Loop 202)	10 ft.
- Rear (west)	20 ft.
Landscape Setback (min.)	
- Front (Arizona Ave.)	20 ft.
- Side (Pecos Rd.)	20 ft.
- Side (Loop 202)	10 ft.
- Rear (west)	20 ft.
Lot Coverage ¹ (max.)	60 %
Residential Density ¹	Parcel 4 – 67 du/ac Parcel 5 – 33 du/ac
Perimeter Parking Screen Wall Height	3 ft.

F. Multi-Family Design Standards

The City’s Zoning Ordinance sets forth additional requirements for multi-family development to encourage creative and innovative design techniques, quality, and merit. The District Downtown provides the following:

1. Open Space

The requirements for common and private open space are based on traditional suburban development and do not consider developing in an urban-like setting. Great care has been taken to ensure the usable common open space far exceeds what the City’s Code would otherwise require. The anticipated amenities throughout the development and within the common open spaces in this urban-like setting provide better opportunities for residents and guests to interact with each other. Usable common open space and recreation area at the rate of 125 square feet per bedroom at a minimum width of 12 feet shall be provided. Private open space shall be a minimum of 4 feet in diameter for patios and balconies. This area will be an average of 4’x12’ for a total of 48 sq. ft. per patio/balcony.

2. Site Circulation and Parking

Safe and convenient pedestrian circulation to and from parking lots throughout the development must be provided. Code-required parking spaces and ADA van accessible spaces shall be provided. Parking canopy covers must be architecturally integrated with the surrounding structures, i.e., color, materials, location, and 10-inch minimum fascia (all four sides).

3. Amenity Options

Six amenities are required to be provided in accordance with the following schedule: 260 units or larger. Because of the urban, in-fill nature of the Site and proposed community, the amount and quality of amenities shall strive to satisfy the spirit and intent of needing to provide a second pool, ramada, etc.

4. Interface with Single-Family Areas

¹ Calculated on the overall Site and not on an individual parcel.

There are no single-family areas adjacent to the Site.

5. Specific design attention areas

The design of courtyards and pedestrian areas must relate to the “human scale.” Large unvaried building facades shall be avoided. Common open spaces, rather than parking lots, shall be used as central features.

6. Energy conservation

The residential development shall provide shading for the buildings through overhangs and/or trees and shade trees along drives, building perimeters, and where appropriate. Additional building shading may be provided through the use of inset patios and balconies, metal canopies to shade windows, and the use of landscaping.

7. Landscaping

The amount of landscaping shall be of sufficient intensity to create a pleasant and comfortable living environment. Special attention shall be given to the areas that are highly visible to the public to create an upscale landscape experience.

8. Building Standards

Mechanical equipment shall be fully screened through the use of parapet walls on the building elevations.

9. Lighting

Lighting will comply with the City’s Zoning Code. External lighting will be appropriately located and designed to prevent light from spilling onto adjacent properties.

10. Signage

See the discussion in Section V.A for details concerning signage.

G. Circulation

The District Downtown has been planned to ensure efficient on-site circulation and appropriate access to the public street system surrounding the Site. Access to Site will occur as follows: full access will be provided on Pecos Road at the alignment of Palm Lane, along Arizona Avenue at the alignment of the Walmart driveway (a signalized intersection). Limited access will be provided at three additional driveways for the proposed development; two on Pecos Road and one on Arizona Avenue.

H. Parking

A total of 2,178 parking spaces are required and 2,183 spaces are provided. For commercial uses, the minimum parking ratio shall be 5.5 spaces per 1,000.

V. MISCELLANEOUS

A. Comprehensive Sign Plan

The ***Comprehensive Sign Plan*** is attached as ***Exhibit 8***. The Comprehensive Sign Plan addresses both permanent identification and temporary marketing requirements and has been designed to complement the quality of the employment, commercial, and residential components. To the extent the proposed comprehensive sign plan conflicts with the City’s Sign Code, we request the Comprehensive Sign Plan set forth in this booklet be followed.

B. Phasing

The District Downtown is preliminarily anticipating developing in multiple phases as identified on *Exhibit 9, Conceptual Phasing Plan*, and as follows:

- Phase 1 will be the off-site perimeter improvements along Pecos Road and Arizona Avenue and the main access drive from Arizona Avenue and Pecos Road.
- Phase 2 will be the commercial development along Arizona Avenue and Pecos Road in Parcel 2, one of the two hotels in Parcels 6 and 7, and the multi-family development in Parcel 5.
- A Preliminary Development Plan must be submitted and approved by the City Council for the proposed multi-family development on Parcel 4.
- The balance of the development will be developed according to market demand.
- Any changes to the Conceptual Phasing Plan must be approved by the City Council.

C. Utilities

The onsite water, fire and sewer lines will be private except for a public water loop connecting existing mains in Arizona Avenue, Pecos Road, and the existing apartments to the west. The City has requested that the connection to the apartment be completed as part of this project to enhance the existing water pressures in the complex. The public water loop will comply with the City's requirements.

D. Grading and Drainage

The *Conceptual Grading and Drainage Plan* is attached as *Exhibit 10*. Development of the Site will provide retention for the 100-year, 2-hour storm event plus 10% in accordance with City of Chandler standards for site development.

VI. PROJECT TEAM

Owner/Developer:

Meridian West AZ/202, LLC

Attn: Greg Gienko
PO Box 15270
Phoenix, AZ 85060

Architect (Non-Residential):

Neri Architects

Attn: Guido Neri
Michi Mho
6400 N Northwest Hwy, Suite 4
Chicago, IL 60631

Planning/Landscape:

ABLA

Attn: Andy Baron
Jim Beckman
310 E. Rio Salado Parkway
Tempe, AZ 85281

Traffic Engineer:

Lokahi

Attn: Jamie Blakeman
10555 N. 114th Street, Suite 105,
Scottsdale, AZ 85259

Zoning:

Burch & Cracchiolo, P.A.

Attn: Brennan Ray

Madison Leake
1850 North Central Avenue, Suite 1700
Phoenix, Arizona 85004
Phone: (602) 234-8794

VII. CONCLUSION

The District Downtown is a high-quality, exciting employment, commercial, and residential mixed-use development that will complement the surrounding area and provide a recognizable presence in the rapidly developing employment area. The development presents the opportunity to create a dynamic mixed-used development with a variety of compatible and supportive uses. We request your approval of this PAD and PDP amendment.

Meridian West AZ/202, LLC

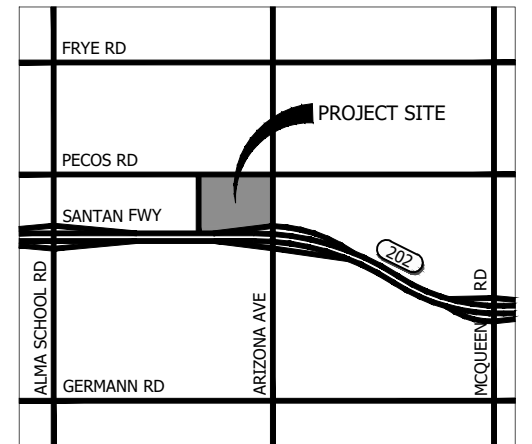
Exhibit 1



PECOS ROAD

ARIZONA AVENUE

LOOP 202



VICINITY MAP

NOT TO SCALE

DISTRICT DOWNTOWN

Aerial

Plan Scale 1:20' Date: 06-16-2022

ABLASTUDIO.COM









Exhibit 2



SITE DATA											
DESCRIPTION		ACREAGE									
GROSS ACREAGE		49.30 AC.									
NET ACREAGE		44.73 AC.									
PARCELS											
PARCEL NUMBER	ACREAGE	USE	FLOOR AREA RATIO	FLOOR AREA	KEYS	UNITS	Density	PARKING REQUIREMENTS			
								PARKING RATIO	REQUIRED	PROVIDED	
PARCEL 1											
BLDG 1-A		AUTO		26,575 S.F.							
BLDG 1-B		AUTO		25,000 S.F.							
PARCEL 1 TOTAL	9.19 AC.		0.13	51,575 S.F.							
PARCEL 2											
BLDG 2-A		RET		2,315 S.F.				250 S.F.	9		
BLDG 2-B		REST		5,000 S.F.				100 S.F.	50		
BLDG 2-C		REST		5,400 S.F.				100 S.F.	54		
BLDG 2-D		REST		2,500 S.F.				100 S.F.	25		
BLDG 2-E		REST		2,900 S.F.				250 S.F.	12		
BLDG 2-F		REST		5,800 S.F.				250 S.F.	23		
BLDG 2-G		REST		13,500 S.F.				250 S.F.	54		
BLDG 2-H		REST		7,700 S.F.				250 S.F.	31		
PARCEL 2 TOTAL	8.93 AC.		0.12	45,115 S.F.						258	352
PARCEL 3											
BLDG 3-A		OFFICE		36,400 S.F.				200 S.F.	182		
BLDG 3-A		OFFICE		36,400 S.F.				200 S.F.	182		
PARCEL 3 TOTAL	4.37 AC.		0.38	72,800 S.F.						364	275
PARCEL 4											
STUDIO		MF				120 UNITS	19.9 DU/AC	1.0 PER UNIT	120		
1 BEDROOMS		MF				200 UNITS		1.5 PER UNIT	300		
2 BEDROOMS		MF				80 UNITS		2.0 PER UNIT	160		
GUEST		MF						0.25 PER UNIT	100		
PARCEL 4 TOTAL	6.02 AC.					400 UNITS	66.4 DU/AC			680	680
PARCEL 5											
STUDIO		MF				120 UNITS	32.4 DU/AC	1.0 PER UNIT	120		
1 BEDROOMS		MF				200 UNITS		1.5 PER UNIT	300		
2 BEDROOMS		MF						2.0 PER UNIT	160		
GUEST		MF						0.3 PER UNIT	100		
PARCEL 5 TOTAL	12.35 AC.					400 UNITS	32.4 DU/AC			680	680
PARCEL 6											
BLDG 6-A		HOTEL		60,000 S.F.	98 KEYS			1/KEY	98		
PARCEL 6 TOTAL	2.28 AC.		0.60	60,000 S.F.	98 KEYS				98	98	
PARCEL 7											
BLDG 7-A		HOTEL		43,089 S.F.	98 KEYS			1/KEY	98		
PARCEL 7 TOTAL	1.73 AC.		0.57	43,089 S.F.	98 KEYS				98	98	
TOTAL											
MULTI-FAMILY		MF				800 UNITS					
RETAIL		RET		32,215 S.F.							
RESTAURANT		REST		12,900 S.F.							
AUTO		AUTO		51,575 S.F.							
HOTEL		HOTEL		103,089 S.F.	196 KEYS						
OFFICE		OFFICE		72,800 S.F.							
TOTAL	44.87 AC.	COMM	0.24	272,579 S.F.	196 KEYS	800 UNITS				2,178	2,183

LEGEND

-  EXISTING POWER POLE
-  EXISTING STREET LIGHT
-  EXISTING TRAFFIC SIGNAL
-  PROPOSED TRAFFIC SIGNAL
-  FULL TRAFFIC MOVEMENT
-  RIGHT IN / RIGHT OUT ONLY

NOTE: GRAPHIC MATERIALS SHOWN ARE PRELIMINARY AND SUBJECT TO CHANGE. ALL SITE PLAN AND REPRESENTATIVE IMAGERY SHALL BE CONSIDERED CONCEPTUAL. FINAL SITE DESIGN MATERIALS TO BE PROVIDED AT THE TIME OF FINAL IMPROVEMENT PLANS.

Exhibit 3



2221
8/17/2022 4:58:41 PM
AC-6.5.5

Typical Retail/Restaurant



2221
8/17/2022 4:58:41 PM
AC-6.5.6

Typical Retail/Restaurant



2221

8/17/2022 4:58:41 PM

AC-6.5.3

Typical Retail/Restaurant



2221

8/17/2022 4:58:41 PM

AC-6.5.4

Typical Retail/Restaurant



2221

8/17/2022 4:58:41 PM

AC-6.5.8

Typical Retail/Restaurant Courtyard



2221
8/17/2022 4:58:41 PM
AC-6.5.2

Typical Retail



2221
8/17/2022 4:58:41 PM
AC-6.5.1

Typical Retail



2221
6/17/2022 4:30:16 PM
AC-6.6.1

Typical Office





2221
6/17/2022 4:30:16 PM
AC-6.6.2

Typical Office



2221
6/17/2022 4:30:17 PM
AC-6.6.3

Typical Office



2221

6/17/2022 4:30:17 PM

AC-6.6.4

Typical Office

Exhibit 4





OMNIPLAN

MULTIFAMILY RESIDENTIAL CONCEPT

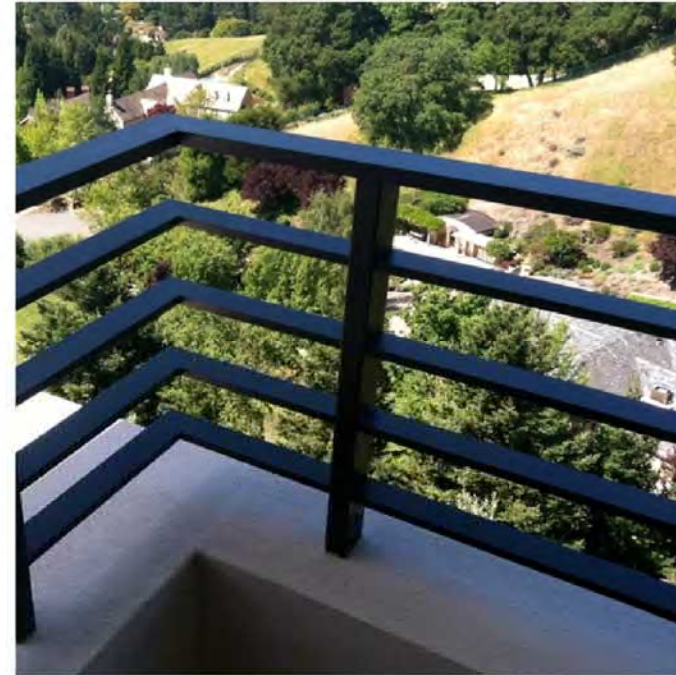


DISTRICT DOWNTOWN
MULTIFAMILY RESIDENTIAL
CHANDLER AZ | 05.20.2024

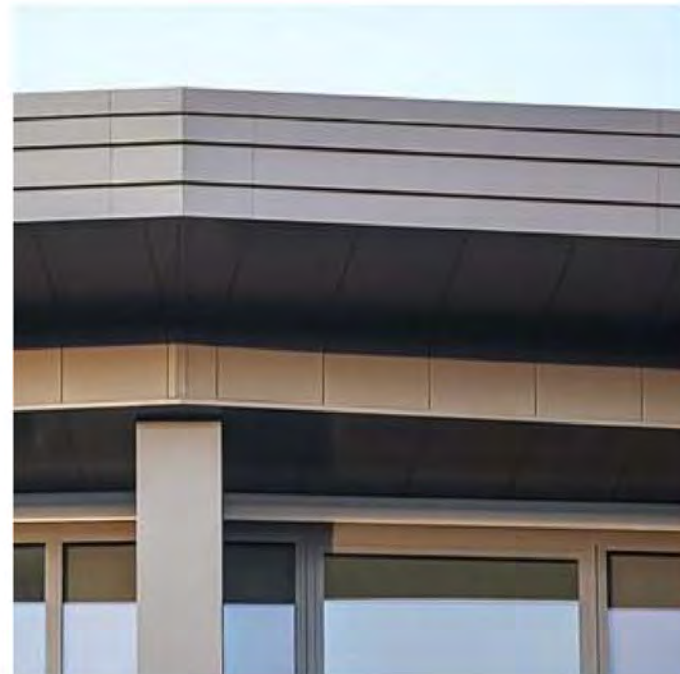
Exhibit 5



ALUMINUM CANOPY



ALUMINUM RAILING



ALUMINUM CLAD ROOF



METAL PANELING

REPRESENTATIVE EXAMPLES FOR COLOR PALETTE AND MATERIALS

Exhibit 6

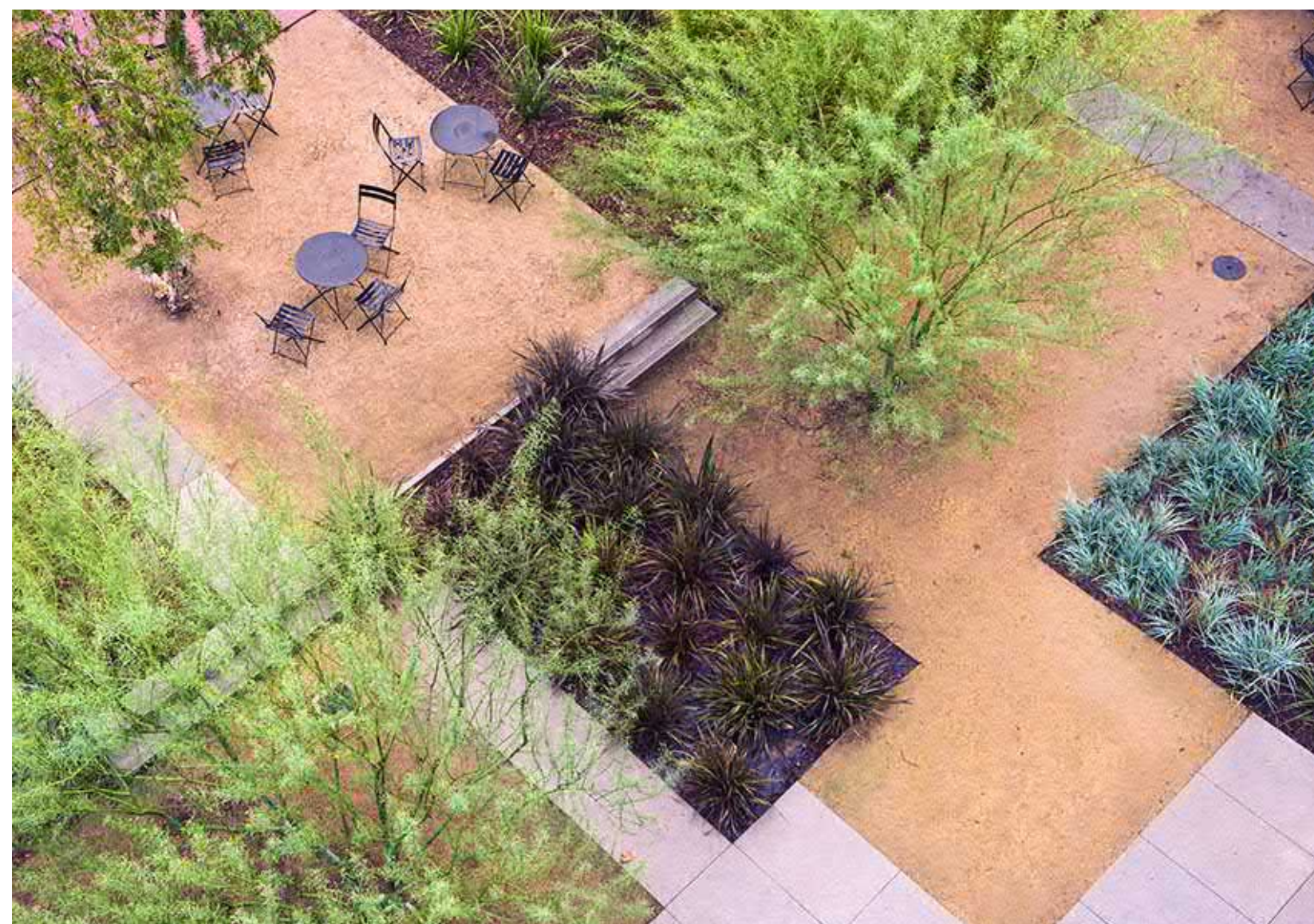
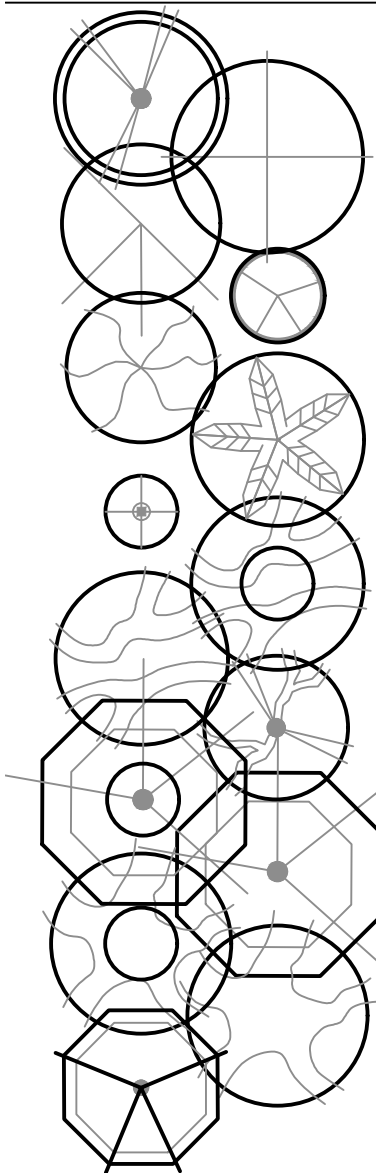


Exhibit 7

PLANT MATERIALS PALETTE



Trees	Size	Qty
Acacia aneura Mulga Acacia	24" Box	
Caesalpinia cacalaco Casalote	24" Box	
Chitalpa tashkinensis Chitalpa	24" Box	
Fraxinus greggii Little Leaf Ash	24" Box	
Olea europea 'Willsonii' Olive	24" Box Multi	
Phoenix dactylifera Date Palm	20' to pineapple	
Pistacia lentiscus Mastic Tree	24" Box	
Pistacia chinensis Chinese Pistache	15 gal.	
Pistacia chinensis Chinese Pistache	24" Box	
Pithecellobium flexicaule Texas Ebony	15 gal.	
Quercus virginiana 'Cathedral' Oak	15 gal.	
Quercus virginiana 'Cathedral' Oak	24" Box	
Ulmus parvifolia 'Sempervirens' Elm	15 gal.	
Ulmus parvifolia 'Sempervirens' Elm	24" Box	
Salvage Tree Mesquite species	36" Box	

Extra Large Shrubs	Size	Qty
Bougainvillea 'California Gold' Orange Bougainvillea	5 gal.	
Caesalpinia gillesii Yellow Bird of Paradise	5 gal.	
Cordia Boissieri Anachuita	5 gal.	
Dodonea viscosa Hopbush	5 gal.	
Tecoma sp. 'Sparky' Sparky	5 gal.	
Vauquelinia californica Arizona Rosewood	5 gal.	
Large Shrubs	Size	Qty
Leucophyllum frutescens 'Green Cloud' Green Cloud Sage	5 gal.	
Leucophyllum langmaniae 'Lynn's Legacy' Lynn's Legacy Sage	5 gal.	
Ligustrum japonicum Japanese Privet	5 gal.	
Salvia clevelandii Chaparral Sage	5 gal.	
Medium Shrubs	Size	Qty
Eremophila glabra spp. caranosa Winter Blaze	5 gal.	
Eremophila hygrophana Blue Bells	5 gal.	
Leucophyllum frutescens 'compacta' Compact Texas Sage	5 gal.	
Teucrium fruticans Bush Germander	5 gal.	

Small Shrubs	Size	Qty
Buxus microphylla japonica Green Beauty Boxwood	5 gal.	
Guara lindheimeri Pink Guara	5 gal.	
Russelia equistiformis Coral Fountain	5 gal.	
Groundcovers	Size	Qty
Chrysactinia mexicana Damianita	1 gal.	
Eremophila prostrata Outback Sunrise Eremophila	1 gal.	
Gazania rigens 'Sun Gold' Trailing Yellow Gazania	1 gal.	
Lantana montevidensis Purple Trailing Lantana	1 gal.	
Lantana sp. 'New Gold' Lantana	1 gal.	
Rosmarinus officinalis 'Prostratus' Trailing Rosemary	1 gal.	
Teucrium chamaedrys 'prostratum' Prostrate Germander	1 gal.	
Vines	Size	Qty
Bougainvillea 'California Gold' Orange Bougainvillea	5 gal.	
Ficus pumila Creeping Fig	5 gal.	

Cacti/ Accents	Size	Qty
Agave bovicornuta Cow's Horn	5 gal.	
Agave salmiana x ferox Large Leaf Agave	5 gal.	
Bouteloua gracilis Blond Ambition	1 gal.	
Dasyliroon acrotrichum Green Desert Spoon	5 gal.	
Euphorbia antisiphilitica Candelilla	1 gal.	
Hesperaloe sp. Pink Parade Pink Parade Hesperaloe	3 gal.	
Hesperaloe parviflora 'Brakelights' Red Yucca	3 gal.	
Muhlenbergia lindheimeri 'Autumn Glow'	5 gal.	
Muhlenbergia rigens Deer Grass	1 gal.	
Nolina matapensis Beargrass	5 gal.	
Opuntia sp. Kelly's Choice Kelly's Choice Prickly Pear	5 gal.	
Pachycereus marginatus Mexican Fence Post cactus	5 gal.	
Portulacaria afra minima Dwarf Elephant Food	5 gal.	
Yucca pallida Pale Leaf Yucca	5 gal.	
Yucca rupicola Twisted Leaf Yucca	5 gal.	

Exhibit 8

THE DISTRICT

D O W N T O W N

COMPREHENSIVE SIGN PROGRAM

May 20, 2024

Prepared for:

MERIDIAN WEST DEVELOPMENT, LLC
P.O. Box 15270, Phoenix, AZ 85060

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Introduction

The THE DISTRICT DOWNTOWN is located on the southwest corner of Arizona Avenue and Pecos Road, directly adjacent to the 202 Freeway, in Chandler, Arizona. The project is a mixed-use development with a variety of commercial uses including office, hotel, restaurant and retail.

Properly adhered to, the criteria contained herein will ensure that project and tenant identification is consistent in quality, and is compatible with the overall architectural character of the center and surrounding area.

Owner:

MERIDIAN WEST DEVELOPMENT, LLC
P.O. Box 15270
Phoenix, AZ 85060
480.231.9238

Contacts:

Greg Gienko

Logo Standard

There is a new name with a new Brand being developed - that will fit within the context of signage areas, materials and colors of the project.

Theme

The intent of this sign criteria is to establish and maintain guidelines consistent with the signage policies and ordinances of the City of Chandler. It is also meant to assure a standard conformance and quality for the design, size, placement and materials used for all business identification for the THE DISTRICT DOWNTOWN.

"Additional Quality Standards" (as required by the City of Chandler) have been met in this package through the following items:

- Freestanding monument sign bases of stone/brick or any material matching buildings.

- Wall signage letter height shall be limited to 15% of the overall building height or 80% of vertical measurement, whichever is shorter.
- All office wall signage shall be reverse pan-channel letters/logos, consistent in color/finish.
- All office wall signage is to be located on the upper floor only.

Project Colors and Materials

Colors and materials used throughout the sign system are derived from the approved architectural color and materials palette.



Dunn Edwards DE6329
Cover of Night



Dunn Edwards DE6328
Anchor Gray



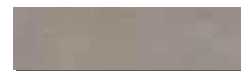
Dunn Edwards DE6374
Silver Polish



Dunn Edwards DE164
Autumn Bark



Dunn Edwards DE5485
Russet Green



Davis Concrete
Mesquite











Background panels on freestanding retail monuments will be fabricated with perforated metal panel construction for mounting of individual letters.



Sign Location Plan



SIGNAGE HIERARCHY KEY

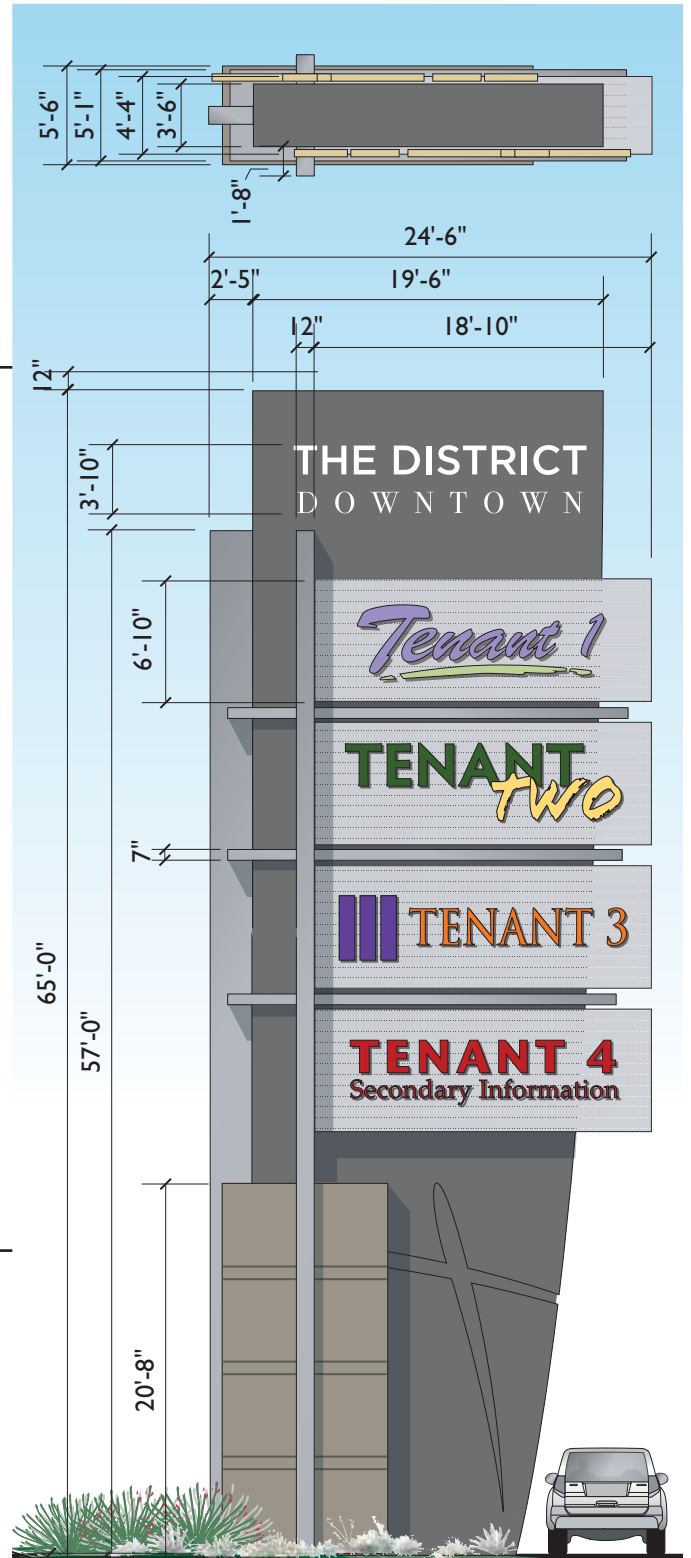
-  Sign Type 1 - Freeway Pylon
-  Sign Type 2 - Corner Project ID Feature
-  Sign Type 3a - Commercial Project ID/Entry Feature
-  Sign Type 3b - Residential Project ID/Entry Feature
-  Sign Type 4a - Primary Tenant / Multi-Tenant
-  Sign Type 4b - Secondary Multi-Tenant
-  Sign Type 4c - Special Use Tenant Monument
-  Sign Type 5 - Parcel ID Monument
-  Sign Type 6a/b - Vehicular Directionals
-  Landmark Water Feature

[1] Freeway Pylon

There shall be two (2) freeway pylons along the southern border of the project, bordering the north side of the 202 Freeway. This sign type will serve to identify the overall project, as well major tenants and amenities within the site such as hotels.

At a height of sixty-five (65) feet, these stylish pylons will have excellent visibility from all lanes of the freeway.

Sign Type	1
Function	Project and major tenant identification.
Height	65'-0"
Sq. Ft.	ID Lettering - 69 s.f. 4 Tenant Panels at 120.2 s.f. each - 480.8 s.f. Total Sign Area - 549.8 s.f.
Quantity	Two (2)
Illum.	Project ID - halo lit Tenants internal face lit
Materials	Project ID of Reverse Pan Channels on aluminum cabinet. Tenant lettering to be Pan Channels with Plexiglass faces. Aluminum architectural accents. Tenant background panels of perforated metal, painted.
Colors	Structure: DE6328 Anchor Grey Base: Painted to match Davis Colors "Mesquite" concrete Accents: DE 6374 Silver Polish Tenant Panels: DE6374 Silver Polish Tenant ID: Corporate Standards Project ID: White



Scale: 3/32" = 1' - 0"

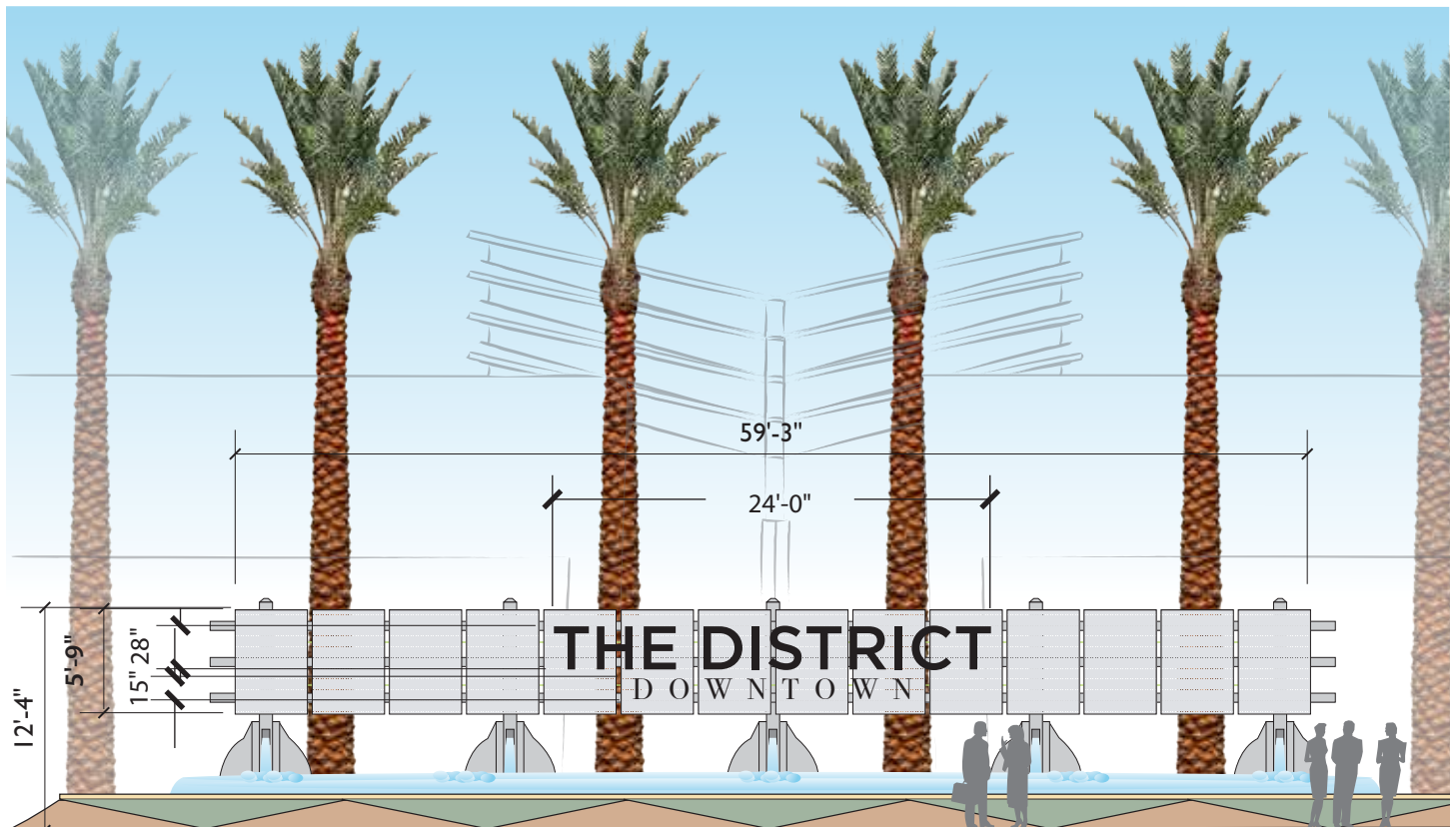
[2] Project ID / Corner Feature

There shall be one (1) corner feature at the southwestern corner of the Arizona Avenue and Pecos Road. This sign is only to identify the project. It will be physically and graphically incorporated into a water feature (by others), enhancing the experience of both vehicular and pedestrian traffic at the intersection

At a height of just over twelve (12) feet, the corner feature will have good visibility without being overpowered by the surrounding architecture.

Project name of edge lit, dimensional letters mounted on perforated panels, backed by matte acrylic panels internally illuminated to create a soft backlit glow.

Sign Type	2
Function	Project identification.
Height	12'-4"
Sq. Ft.	Lettering - 85 s.f.
Quantity	One (1)
Illum.	Project ID - halo lit
Materials	Perforated metal panels on aluminum architectural structure. Project lettering of Reverse Pan Channels. Additional lighting behind perforated panels with acrylic back up.
Colors	Project ID: <i>DE6328 Anchor Grey</i> Panels: <i>DE6374 Silver Polish</i>



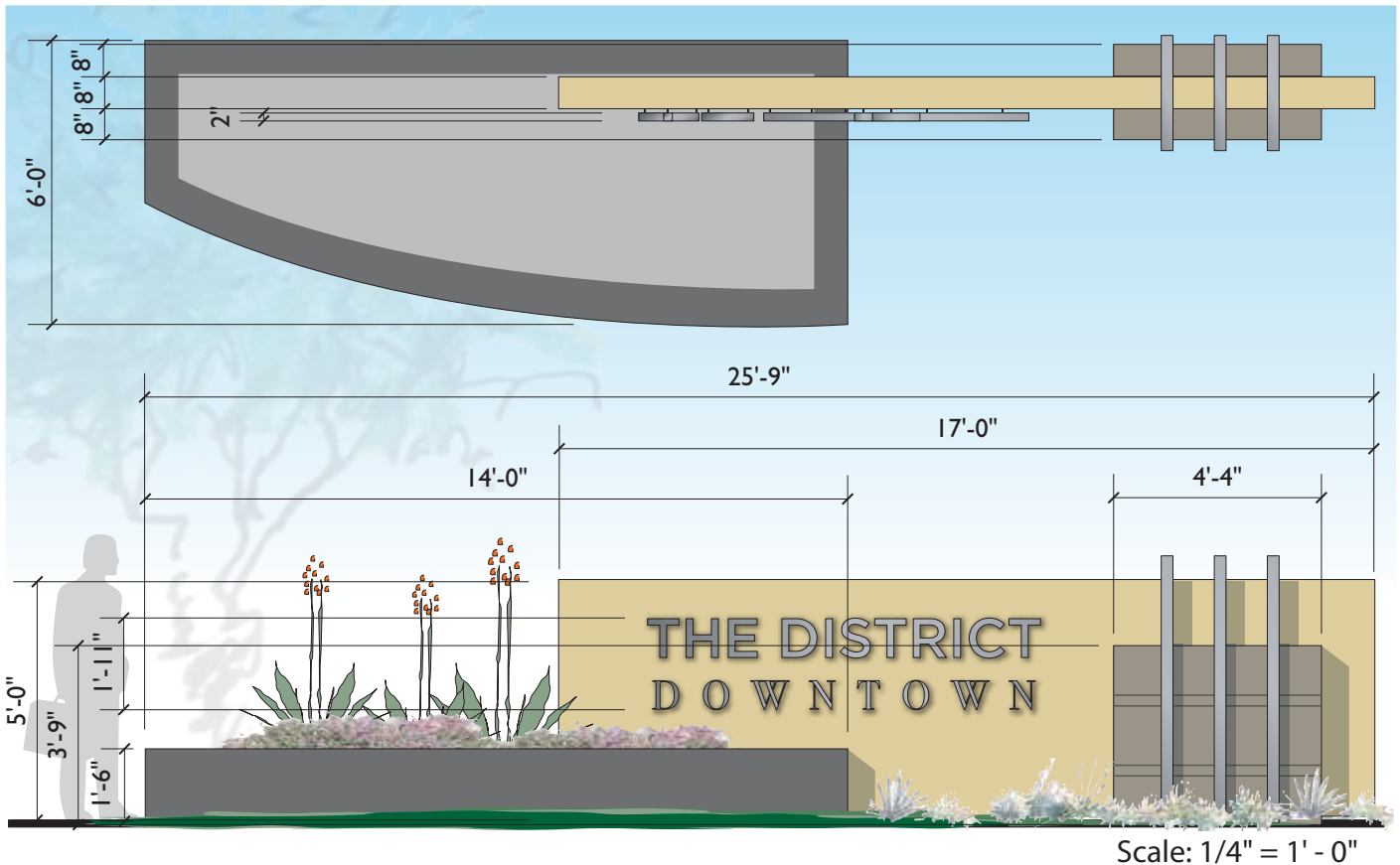
Scale: 3/32" = 1' - 0"

[3] Project ID / Entry Feature

There shall be six (6) low profile entry monuments at the primary entrances on both Pecos Road and Arizona Avenue, mainly where future traffic signals are planned. These signs will display the project identification.

At five (5) feet, the sign and information are below the maximum height stipulation of six (6) feet.

Sign Type	3
Function	Project identification.
Height	5'-0"
Sq. Ft.	Lettering - Approx. 32 s.f.
Quantity	Five(5)
Illum.	Halo and/or ground up-lit
Materials	Masonry/concrete wall, planter and base. Reverse Pan Channel metal letters/logo. Aluminum architectural accents. Painted wall and structure.
Colors	Wall: <i>DE5485 Russet Green</i> Base: <i>Davis Concrete "Mesquite"</i> Accents: <i>DE 6374 Silver Polish</i> Project ID: <i>DE6374 Silver Polish</i> Planter: <i>DE6328 Anchor Grey</i>

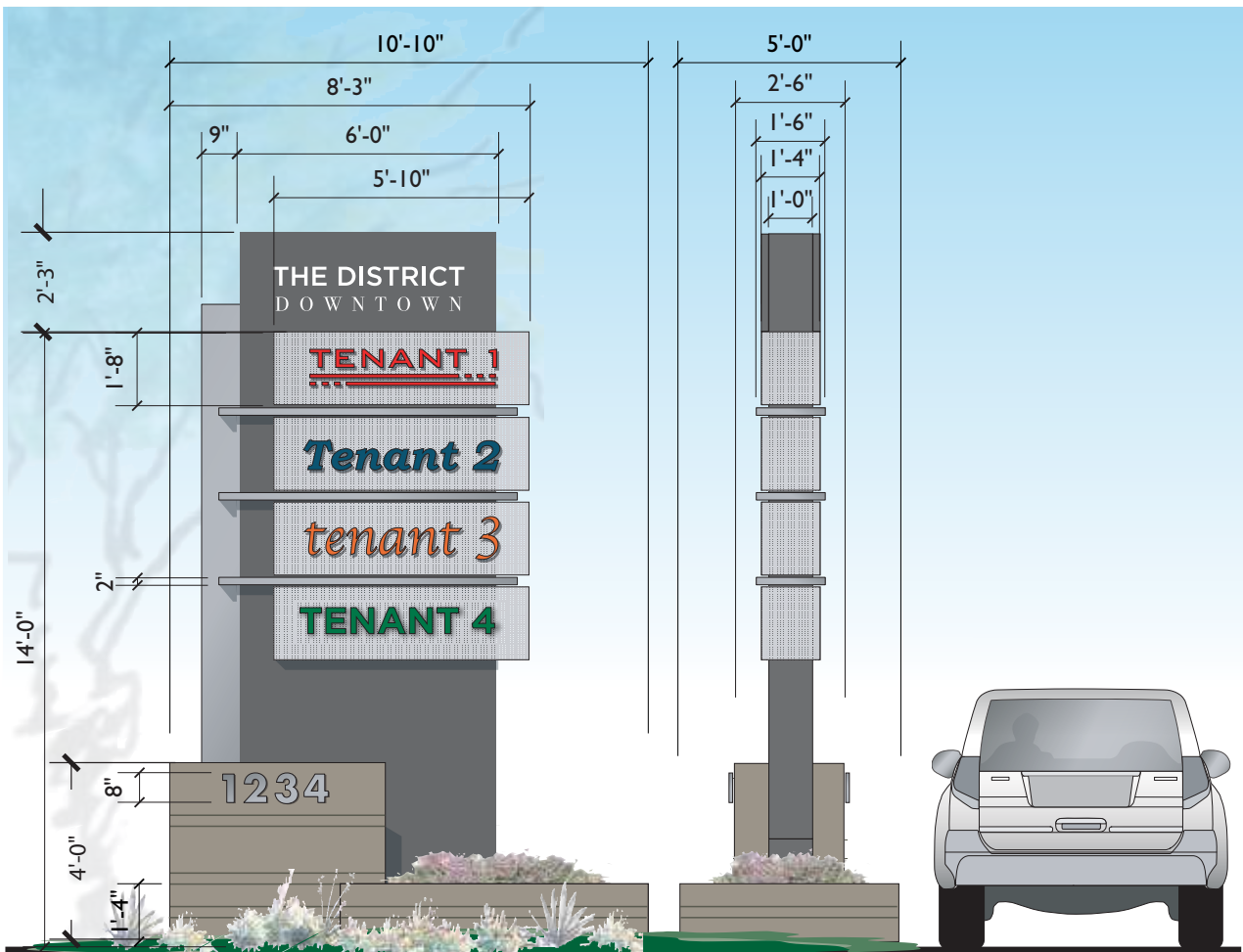


[4a] Multi-Tenant Monument

The primary multi-tenant monument is located near the entries closest to the intersection along Pecos Road and Arizona Avenue. Per code, this sign is at a maximum height of 14'-0", and will display up to four (4) tenants.

These signs pull elements from the architecture and will be incorporated into the surrounding landscaping.

Sign Type	4a
Function	Development identification with four (4) retail/commercial tenant identification panels.
Height	14'-0" to top of tenant sign area
Sq. Ft.	Project ID - 6.3 s.f. Tenants - 4 at 9.7 s.f. each Total 45.2 s.f.
Quantity	Three (3)
Illum.	Combination of face lit and halo.
Materials	Aluminum fabrication with masonry base. Project lettering of Reverse Pan Channels on aluminum cabinet. Tenant lettering to be Pan Channels with Plexiglass faces. Aluminum architectural accents. Tenant background panels of perforated metal, painted.
Colors	Structure: <i>DE6328 Anchor Grey</i> Base/Planter: <i>Davis Concrete "Mesquite"</i> Tenant ID/Accents: <i>DE 6374 Silver Polish</i> Tenant ID: <i>Corporate Standards</i> Project ID: <i>White</i>

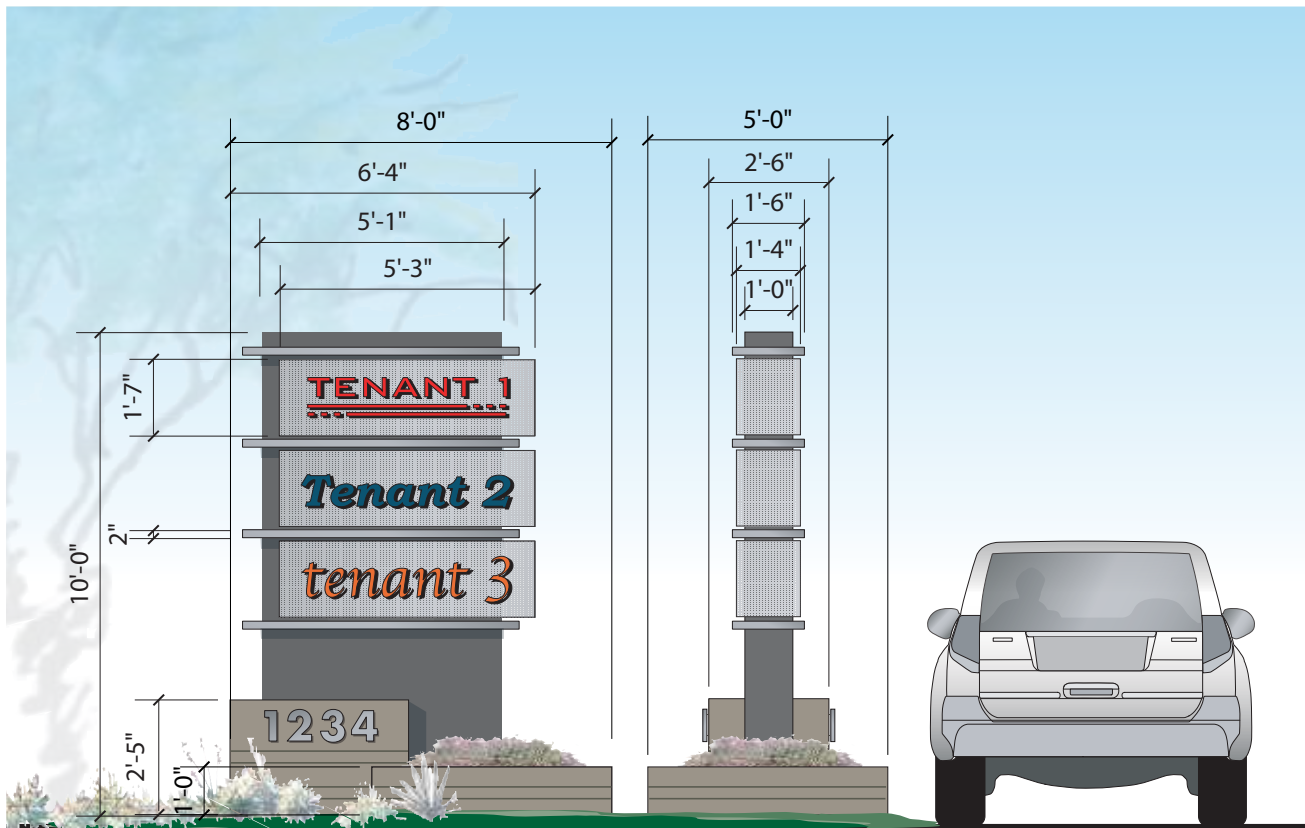


[4b] Secondary Multi-Tenant Monument

The secondary multi-tenant monuments are located along Pecos Road and Arizona Avenue. Per code, this sign is at a maximum height of 10'-0". There will be no project identification on this sign. However, project addressing and up to three (3) tenants will be displayed.

This sign pulls elements from the architecture and will be incorporated into the surrounding landscaping.

Sign Type	4b
Function	Maximum of three (3) retail/commercial tenant identification panels.
Height	10'-0" to top of sign area
Sq. Ft.	Tenants - 3 at 8.3 s.f. each Total - 24.9 s.f.
Quantity	Two (2)
Illum.	Combination of face lit and halo.
Materials	Aluminum fabrication with masonry base. Project lettering of Reverse Pan Channels on aluminum cabinet. Tenant lettering to be Pan Channels with Plexiglass faces. Aluminum architectural accents. Tenant background panels of perforated metal, painted.
Colors	Structure: <i>DE6328 Anchor Grey</i> Base/Planter: <i>Davis Concrete "Mesquite"</i> Tenant ID/Accents: <i>DE 6374 Silver Polish</i> Tenant ID: <i>Corporate Standards</i>



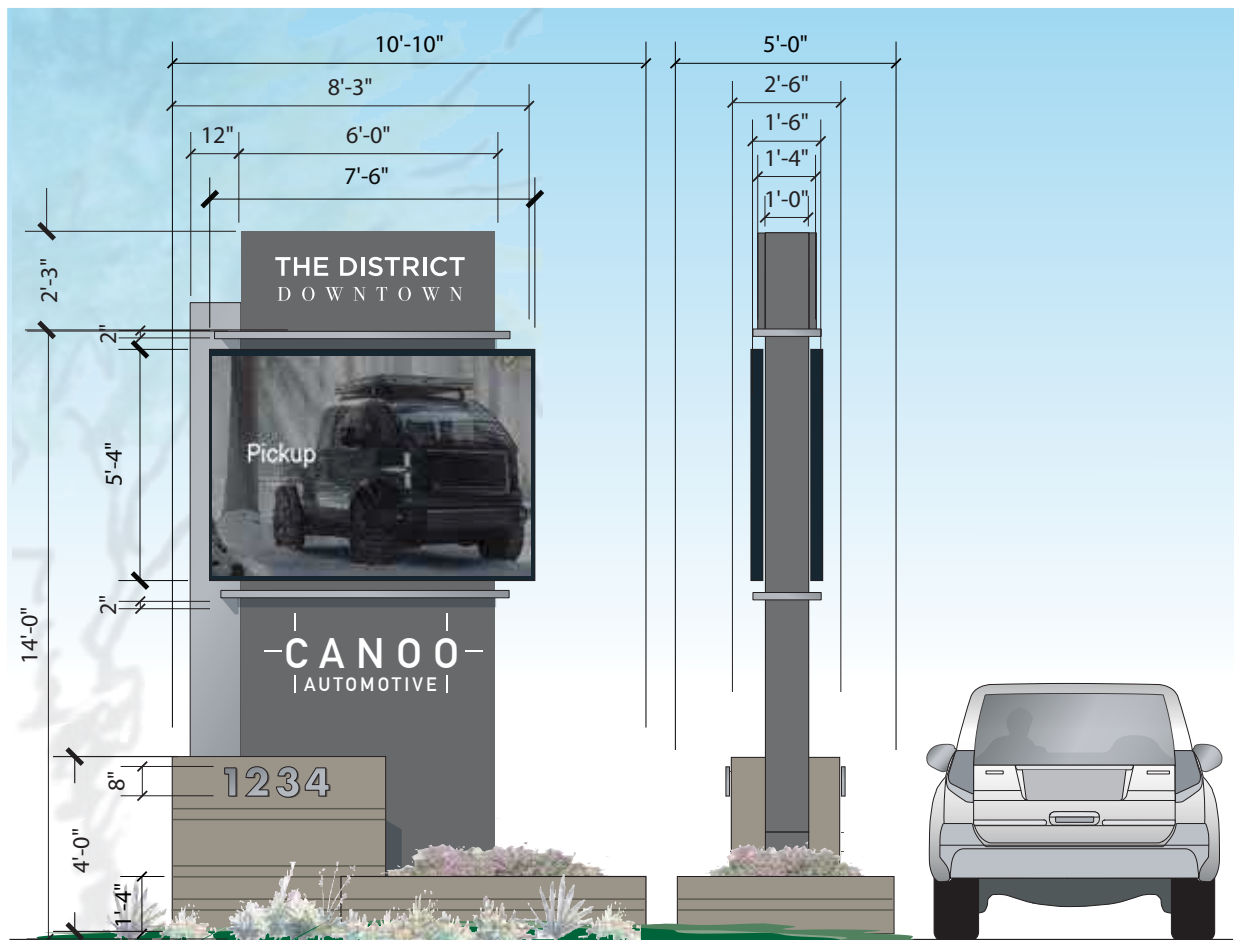
Scale: 1/4" = 1' - 0"

[4c] Special Use Tenant Monument

The Special Use Tenant Monument would be located at the south corner of the project and would be dedicated to a potential auto/dealership use only. This sign would identify the dealership with a standard sized electronic display board for multiple product names/brands/product images.

This sign will use design elements from the dealership's architecture along with the District elements, and will be incorporated into the surrounding landscaping.

Sign Type	4a
Function	Dealership identification with electronic message board.
Height	14'-0" to top of tenant sign area
Sq. Ft.	Project ID - 6.3 s.f. Tenants - Electronic board - 40 s.f. Dealership branding - 10 s.f. Total 56.3 s.f.±
Quantity	One (1)
Illum.	Combination of face lit and halo.
Materials	Aluminum fabrication with masonry base. Project lettering of Reverse Pan Channels on aluminum cabinet. Dealership lettering to be Pan Channels with Plexiglass faces. Aluminum architectural accents. Tenant background panels of perforated metal, painted.
Colors	Structure: <i>DE6328 Anchor Grey</i> Base/Planter: <i>Davis Concrete "Mesquite"</i> Tenant ID/Accents: <i>DE 6374 Silver Polish</i> Tenant ID: <i>Corporate Standards</i> Project ID: <i>White</i>

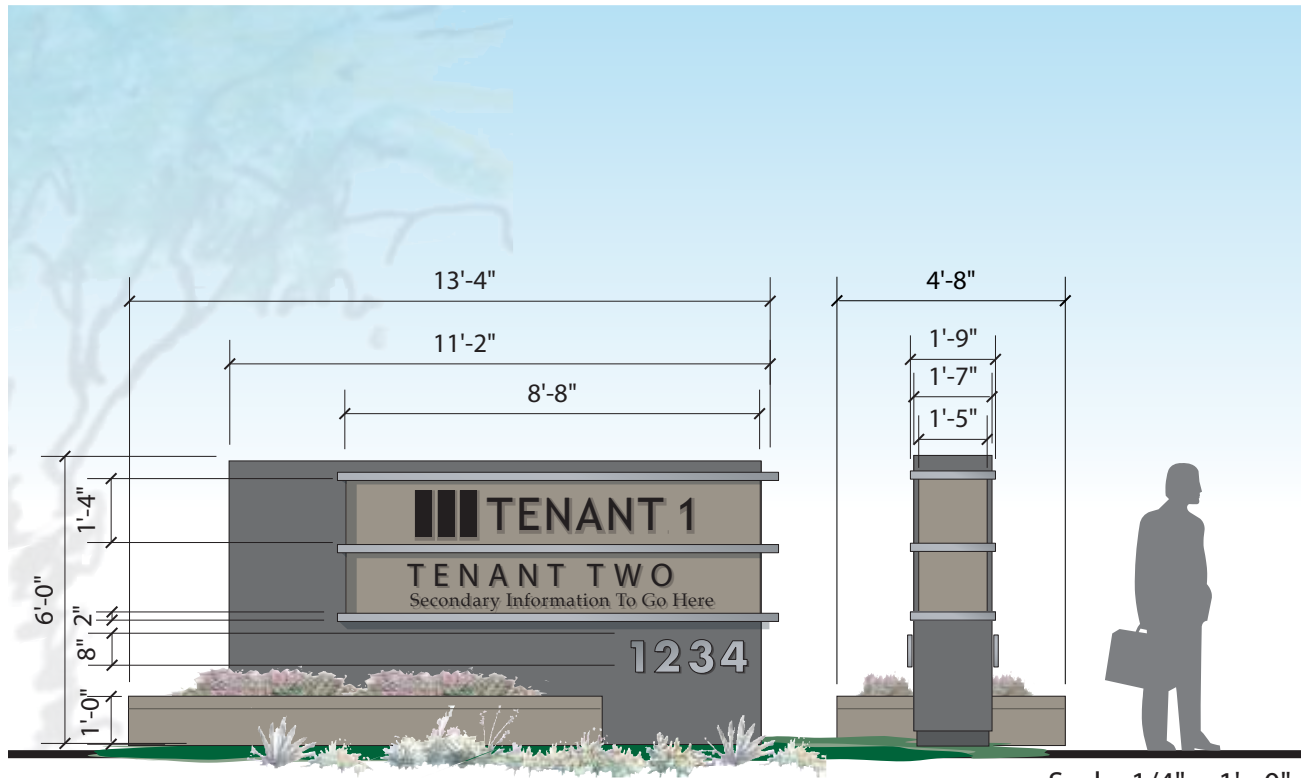


[5] Office / Hotel / Multi-Family Monument

There shall be five (5) low profile monuments along the major thoroughfare of the site to provide a tie with the overall development identification monument for the various mixed use components on the site. The sign will be a maximum height of 6'-0", and contain up to two (2) tenant identifications. For a single use, a single panel area may be used.

This sign pulls elements from the architecture and will be incorporated into the surrounding landscaping.

Sign Type	5
Function	Identification of one (1) corporate tenant/hotel.
Height	6'-0" to top of sign area
Sq. Ft.	ID - 2 at 11.6 s.f. each Total - 23.2 s.f.
Quantity	Five (5)
Illum.	Internal. RPC halo (addressing).
Materials	Aluminum fabrication with masonry base. 1/2" thick push-thru acrylic tenant copy with applied metal face (for halo illumination). Addressing of flat cut alut aluminum addressing. Paint. Hotel may be face illuminated.
Colors	Structure: <i>DE6328 Anchor Grey</i> Base/Planter: <i>Davis Concrete "Mesquite"</i> Tenant Area: <i>Painted to match "Mesquite"</i> Accents: <i>DE 6374 Silver Polish</i> Tenant ID: <i>DE6329 Cover of Night - Hotels may use corporate standards for ID.</i>

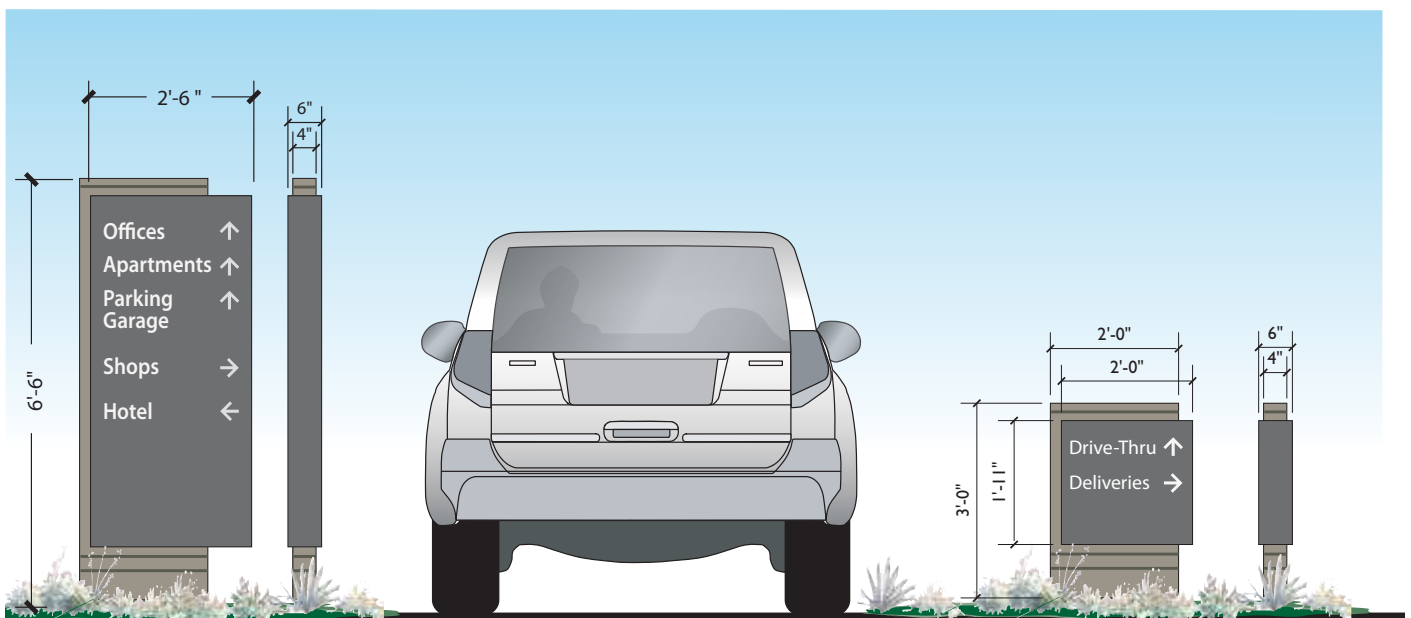


[6] Vehicular Directional

These signs are located at primary traffic decision points to assist traffic flow utilizing two (2) sizes. Along the private interior roadways, the 6'-6" size will be used where traffic is moving faster and with numerous destination options. The smaller, 3'-0" directionals are used within specific project sites, parking lots and driveways, where traffic is slower and only a few destination options exist.

These signs incorporate the architectural and wall details in the

Sign Type	6a	6b
Function	Direct vehicular traffic through the site.	
Height	6'-6" max	3'-0" max
Sq. Ft.	8 s.f.	4 s.f.
Quantity	Three (3)	As needed
Illum.	Non-illuminated	Non-illuminated
Materials	Aluminum structure with applied reflective vinyl letters and symbols. Painted background.	
Colors	Panel: <i>DE6328 Anchor Grey</i> Base: <i>Painted to match "Mesquite"</i> Letters/Arrows: <i>White reflective Vinyl to match</i>	



Scale: 3/8" = 1' - 0"

Building Wall Sign Matrix

BUILDING TYPE	MAXIMUM QUANTITY	LOCATION	SIGN AREA	
Retail Shops	Two (2) per tenant	Signs to be located directly over or adjacent to tenant entry and rear elevations. One (1) per elevation.	Two (2) square feet per each lineal foot of leased frontage.	
Retail Pads	One (1) per elevation that offers vehicular readability from a street, internal thoroughfare, or parking area. Maximum of four (4) per building	Primary sign to be located directly over or adjacent to tenant entry All others per tenant signage program - signage area cannot be transferred to other elevations	Two (2) square feet per each lineal foot of designated frontage.	
Office	Two (2) per tenant	Owner to designate tenants and signage locations which may not necessarily be over their leased space. Locations to be on upper level only.	Two (2) square feet per each lineal foot of leased suite space. No one sign shall exceed 250 square feet.	
Automotive	Two (2) per brand being sold on site		Two (2) square feet per each lineal foot of leased suite space. No one sign shall exceed 250 square feet.	

	MAXIMUM LETTER HEIGHT	ILLUMINATION STYLE	SIGN TYPE/ DESIGN / COLOR
	<p>Thirty-six (36) inches for any individual letter. No layout shall exceed 80% of the vertical measurement of the architectural fascia.</p> <p>Letter height not to exceed 15% of the building height.</p>	Internal, halo or combination thereof.	<p>Pan Channel, Reverse Pan-Channel or Custom Cabinet Letters/Logo. No exposed raceways.</p> <p>Logos, logotype and colors may be per the tenant's national/corporate standards.</p> <p>Tenant Identification signage must incorporate a name for the business. "Modifiers" as the identification will not be allowed.</p>
	<p>Thirty-six (36) inches for any individual letter. Symbols and Iconic elements are exempt from the letter height restrictions. No layout shall exceed 80% of the vertical measurement of the architectural fascia.</p> <p>Letter height not to exceed 15% of the building height or 80% of vertical measurement whichever is shorter.</p>	Internal, halo or combination thereof.	<p>Pan Channel, Reverse Pan-Channel or Custom Cabinet Letters/Logo. No exposed raceways.</p> <p>Logos, logotype and colors may be per the tenant's national/corporate standards.</p> <p>Tenant Identification signage must incorporate a name for the business. "Modifiers" as the identification will not be allowed.</p>
	<p>Forty-eight (48) inch Capital Height</p> <p>Letter height not to exceed 15% of the building height or 80% of vertical measurement whichever is shorter.</p>	Halo	<p>Reverse Pan-Channel Letters/Logos. No exposed raceways.</p> <p>Logo and logotype may be per the tenant national/corporate standards.</p> <p>All signage shall be brushed aluminum finish.</p>
	<p>Forty-eight (48) inch Capital Height.</p> <p>Letter height not to exceed 15% of the building height or 80% of vertical measurement whichever is shorter.</p>	Internal, halo or combination thereof.	<p>Reverse Pan-Channel Letters/Logos. No exposed raceways.</p> <p>Logo and logotype may be per the tenant national/corporate standards.</p> <p>All signage shall be brushed aluminum finish.</p>

[7] Retail Tenant Wall Signage Standards

All retail tenants must have storefront signs fabricated from either internally illuminated individual pan-channel, reverse pan-channel letters and logos, and/or "custom 3-D panels".

All sign designs and layouts will be approved on an individual basis through the Developer's discretion. Creative designs, forms, and the use of "custom" 3-D panel signs are encouraged. Unless as part of a nationally registered and/or trademarked logo, rectangular shaped custom cabinets are strictly prohibited.

Layout/Design

Copy and/or logos utilized shall be Tenant's choice, subject to the approval of Developer and/or Developer's agents and the City of Chandler.

All national retailers shall be permitted to utilize their standard corporate identification program subject to sign area limitations contained in the approved Comprehensive Signage Package (CSP).

Tenant Identification signage must incorporate a name for the business. "Modifiers" as the identification will not be allowed.



Sign Area

For maximum aggregate building sign area per Tenant, refer to matrix.

Calculation of sign area: Where a sign consists only of individual letters, numerals, symbols, or other similar components, the total area of the sign shall be the area of the square or rectangle surrounding all components of the sign.

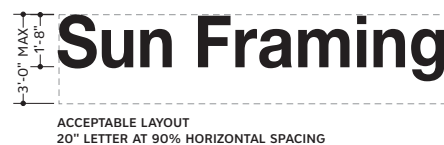
A maximum of one (1) rectangle will be allowed.



Letter Spacing/Kerning

To fit within layout standards, lettering and/or logo may not be condensed more than 90% of horizontal letter width of original design.

Graphic below illustrates a name that is allowed a maximum letter height, and maximum sign length and width. The graphics show what is allowed for fitting the lettering within that space, based on the tenants registered trademark/logo.



Sign Locations / Restrictions

Tenant signage shall be installed in accordance with the typical elevations which follow.

Tenant signs shall be centered horizontally and vertically within the architectural frontage and/or directly over the doorway if space permits. Signs must be located within Tenant's leased elevation. Freestanding Pad Tenants and Major Tenant buildings may have signs on all elevations.

Height and Length of Sign on Wall Surface

In no event shall any sign exceed eighty (80%) percent of the building elevation and/or wall surface upon which it is placed. The available surface area of the architectural sign area shall regulate letter height not exceeding 70% vertical height.

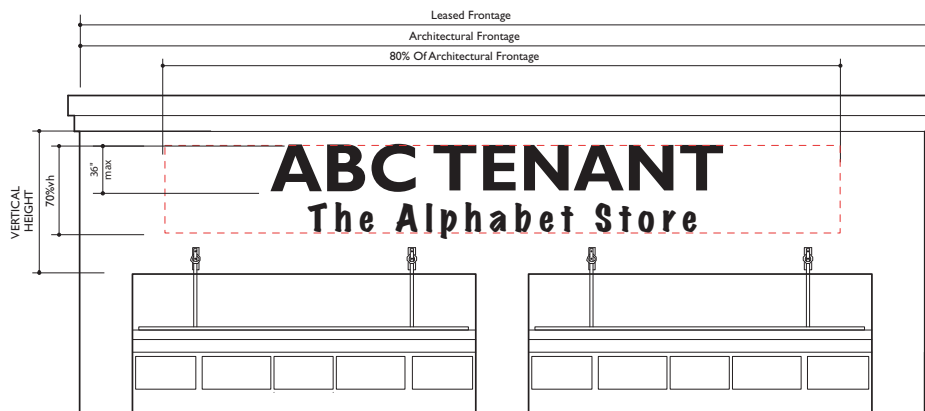
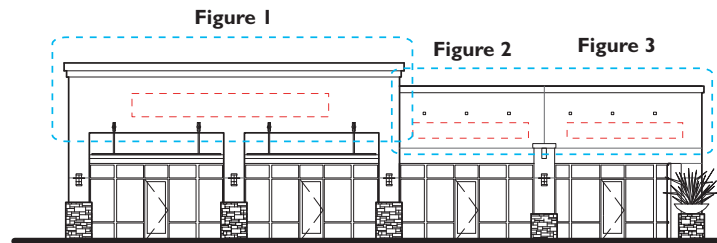


Figure 1

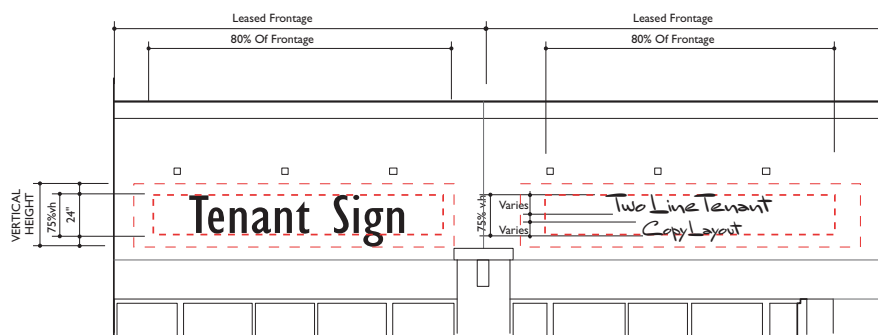


Figure 2

Figure 3

[7] Retail Tenant Wall Signage - Individual Letters

Individual illuminated letters and logos and may include pan channel metal letters with acrylic sign faces, reverse pan channel “backlit” illuminated letters, or any combination thereof as outlined in the matrix per tenant type.

All signage shall be reviewed and approved by the Developer and/or Developer’s agents and shall be appropriate to the surrounding building features, environment, and architectural thematic design. Developer and/or Developer’s agents and the City of Chandler shall have discretion in varying any provision of these specifications.

Construction

Individual pan channel letters and logos must be constructed of minimum of .063 aluminum returns. Pan channel letter faces must be a minimum 1/8" Plexiglas, Acrylic or Lexan.

Reverse pan channel letter faces must be a minimum of 1/8" thick aluminum.

No “Channelume,” “Letteredge,” or similar material will be allowed. Exposed raceways, conduits, fasteners, tubing or transformers will not be permitted. All inductors, transformers, or other equipment will be concealed in a water tight condition.

Colors

Letter face colors are per Tenant's corporate standards with the exception that no fluorescent colors will be allowed. Retainers for pan channels must match letter face or return color. For National Tenants, returns and retainers may be per corporate standards - however, no gold, silver, copper or brass will be allowed.

Illumination

Tenant building signage may be internally illuminated (LED), backlit to create a silhouette combination of lighting methods mentioned herein. No exposed neon and/or clear faced internal neon illumination will be allowed.



[7] Retail Tenant Wall Signage -3D Panel Details

Alternative signage options to the standard individual letters is the use of "Custom 3-D Panels". These are multiple layered in design with a mix of opaque and translucent backgrounds, various fabrication and illumination methods of lettering, built around a single unit. This type of design and fabrication method provides for ease of installation by connecting to one single J-box on the fascia and protects the building from repeated drilling and wiring for individual letter installations.

All designs will be approved on an individual basis through Developer's discretion. Creative designs and forms, and "Custom" Panel signs are encouraged. Unless as part of a nationally registered and/or trademarked logo, rectangular shaped cabinets are prohibited.

Colors

Letter face colors are per Tenant's corporate standards with the exception that no fluorescent colors will be allowed. Retainers for pan channels must match letter face or return color. For National Tenants, returns and retainers may be per corporate standards - however, no gold, silver, copper or brass will be allowed.

Illumination

Tenant building signage may be internally illuminated (LED), backlit to create a silhouette combination of lighting methods mentioned herein. No exposed neon and/or clear faced internal neon illumination will be allowed.



[7] Tenant Wall Signage -Canopy Signs

Typically located on the ground floor, Standing Canopy Signs, Canopy Fascia Signs and Suspended Canopy Signs should be placed centered on the tenant's frontage and centered on the canopy structure, if possible.

The Landlord can approve exceptions for each of these types of canopy signs due to variations in architectural features on the building, variations in the canopy structure and the tenant corporate brand standards.

For **Standing** and **Fascia Canopy** signs, all raceways shall be concealed within the canopy structure or behind the canopy front face outside of the line-of-sight when viewed from below or in-front of the canopy structure. Posts and beams are allowed to be visible if signage is integral with the design.

The sign shall limited to individual characters. Raceways shall be concealed with in the canopy or lattice. Backer panels are allowed.

If **Suspended Canopy** signage is applicable to the building architecture, support structure, "cabinet backers", lettering application and illumination shall be designed to be consistent in appearance, with the exception of the tenants corporate brand standards type and colors. Posts and beams are allowed to be visible if signage is integral with the design. Standing Canopy Signs can be either face-lit, halo-lit, or indirect-lit with LEDs or "neon". These sign shall have a minimum clearance of 84" (7'-0").

Illumination

Indirect sign lighting shall be shielded so that the illumination source is not visible.

Office signage may be non-illuminated, but all retail should be illuminated per the guidelines.

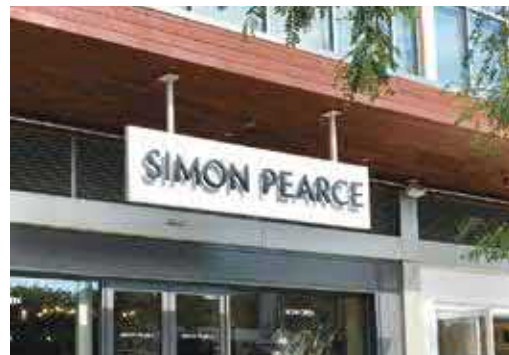
Where illuminated, LED's are recommended. All Canopy signs can be either face-lit, halo-lit, or indirect-lit with LEDs or "neon".



Standing Canopy Sign is a sign mounted to the top of the leading edge of a canopy located above a storefront parallel to the building façade on which it is mounted.



Canopy Fascia Sign is a sign attached to the vertical front face of a canopy, roof overhang, covered walkway, covered porch, or purlin of an open lattice structure.



Suspended Canopy Sign is a sign suspended under a canopy, roof overhang, covered walkway, covered porch, or open lattice walkway and parallel to storefront.

[7] Tenant Wall Signage -Projecting Signs

A Projecting (Blade) Sign is a business identification sign that is mounted perpendicular to the face or corner of a building.

Unless separated by a structure and not visible from an abutting public street, Projecting Signs shall not be placed within one-hundred fifty (150) feet from an abutting public street.

One (1) Projecting Sign is permitted per each fifty (50) lineal feet of store frontage.

Such sign shall not be located within twenty (20) feet in any direction of another Projecting Sign on the same building wall.

A Projecting Sign shall be placed on the tenant's leased frontage and shall not encroach onto another tenant suite adjacent to or above the tenant suite in which the Projecting Sign identifies with approval of the landlord.

Maximum Projection

Sixty (60) inches from the wall of the building to the outside edge of the sign and a minimum clearance from adjacent grade of 10'-0".

Illumination

Internal Illumination can be Face, Halo Backlighted, Dual-Illumination from exterior and interior sources or Non-Illuminated.

Indirect Illumination shall be a shield light sources

Where illuminated, LED's are recommended.



[8] Storefront Graphics -Arcade Signs

Where there are covered walkway areas, Arcade signs hang or flag mount off tenant storefronts oriented to the pedestrian located at the primary public accessible entry.

Within the Commercial Plaza, support structures, "cabinet backers", lettering application and illumination shall be designed to be consistent in appearance. Design of the graphics may unique within the context of their storefront's architecture, services/ products provided and their recognized corporate identity and design theme, subject to approval of the Landlord.

All arcade type signs are oriented perpendicular to the tenant store front with a minimum 84" (7'-0") clearance from walkway.

Structure

Armature should be constructed as a rigid element, not allowing the sign to swing. Suspended Canopy Signs should display the Tenant's unique identity (logo) while evoking the sense of contemporary high craftsmanship and design sensitivity

Illumination

Arcade/Shingle Signs can be either face- lit, halo-lit, or indirect-lit with LEDs or neon.

Indirect sign lighting shall be shielded so that the illumination source is not visible.

Non-illumination allowed.



[8] Storefront Window Graphics (retail & office)

Window Signs

These are fabricated small cabinet or panels signs suspended behind the store front glazing and can identify the tenant or be open/closed signs.

Can be internally illuminated.

Sign may be displayed within 3.75 feet of a window pane.



Window Graphics

No more than 25% of front glass area may have signage/graphic, applied to, or within 3 feet behind the window area. This includes temporary signage applications for sales, promotions and/or any permanent identification signage.



Addressing

Individual tenant suites shall have six (6) inch white 3M vinyl letter addressing centered on the window above the door.

Colors and Layouts

The tenant may display information such as: the suite number or address; tenant contact information (phone number or website); hours of operation; goods or services provided.

Signage applied to storefront glass can be Vinyl, Gold or silver leaf, Professionally hand painted lettering and/or graphics done by a professional sign painter and approved by Landlord prior to application.

All window graphic layouts shall be approved in writing by the Association for quality and consistency with the criteria package for The THE DISTRICT DOWNTOWN prior to submittal to the City of Chandler for approval.

Restrictions

Window Signs or Graphics may not be pre-fabricated neon elements of any kind.

[9] Building Addressing

Building address numerals shall be Futura typeface, with a minimum height of twelve inches (12").

Construction and Colors

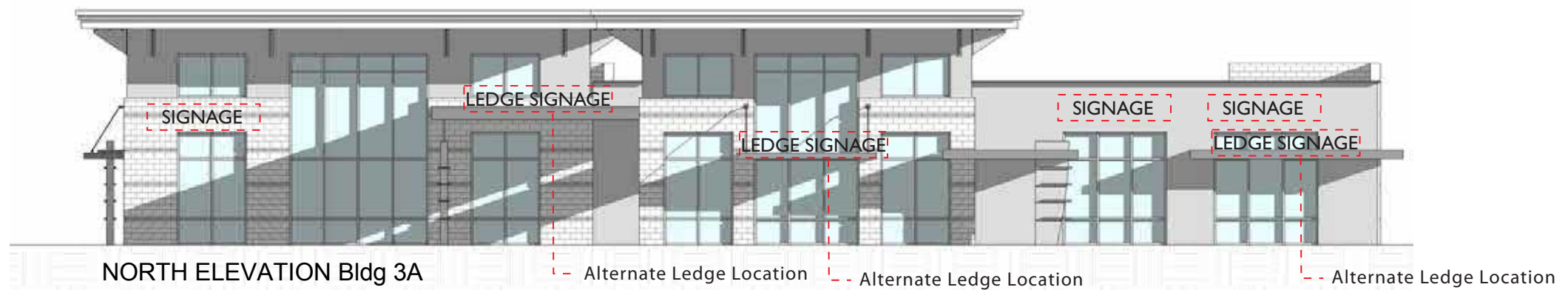
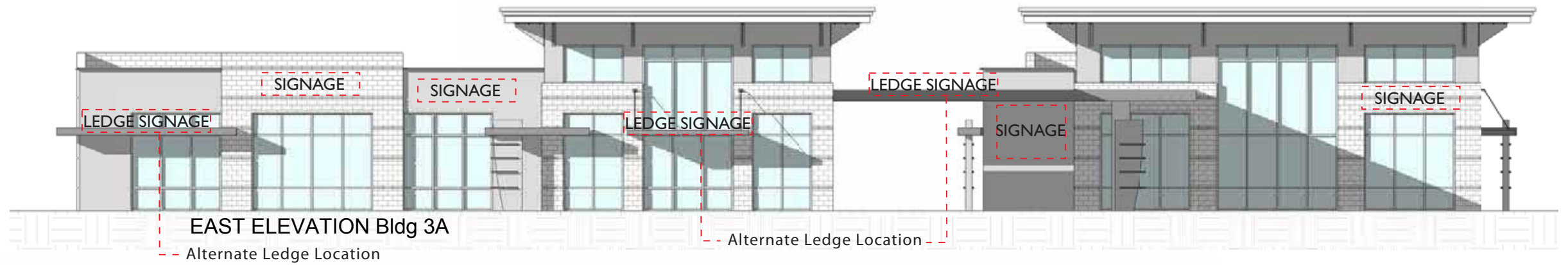
Individual reverse pan-channel numbers are to be used. Addressing can be either halo illuminated clip mounted off the wall, or non illuminated flush mounted.

Numeral locations of darker colors, brushed aluminum finish should be used. Locations with light colored backgrounds should use dark charcoal or black finishes.



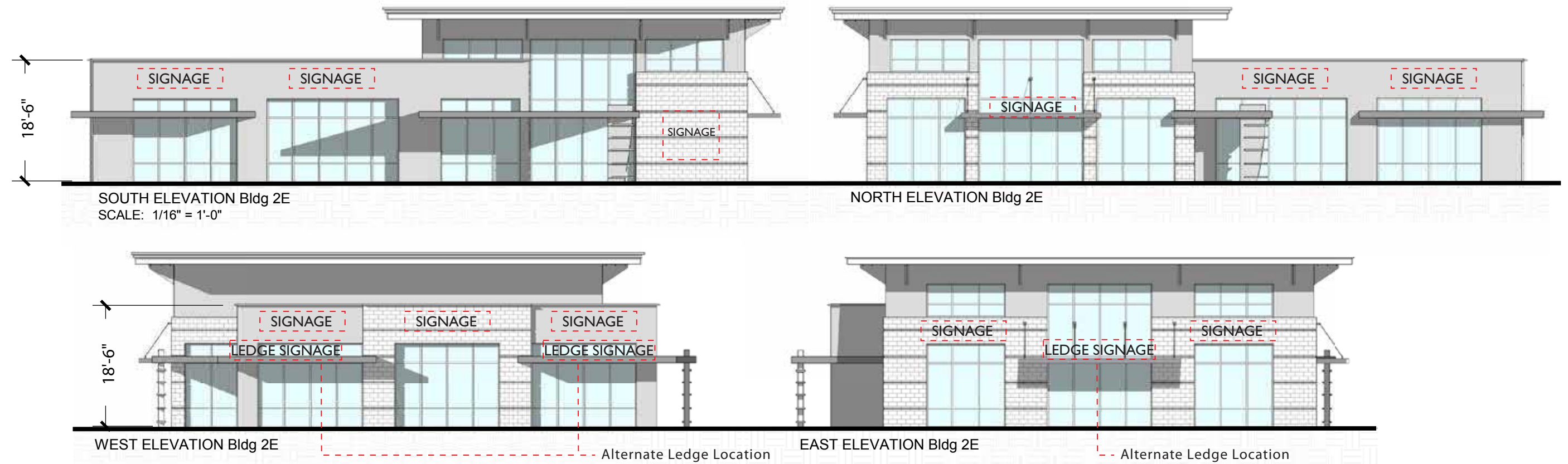
Wall Sign Locations -Typical Retail - Building 3A

Note - Sign locations shown are merely possible locations and mounting methods. Not all signage locations shown will be used. Final locations to be determined upon leasing.



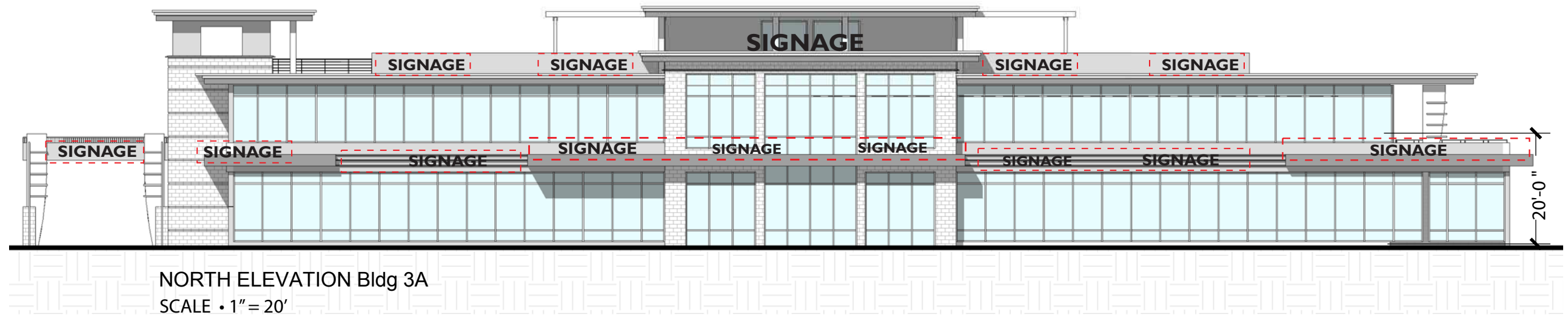
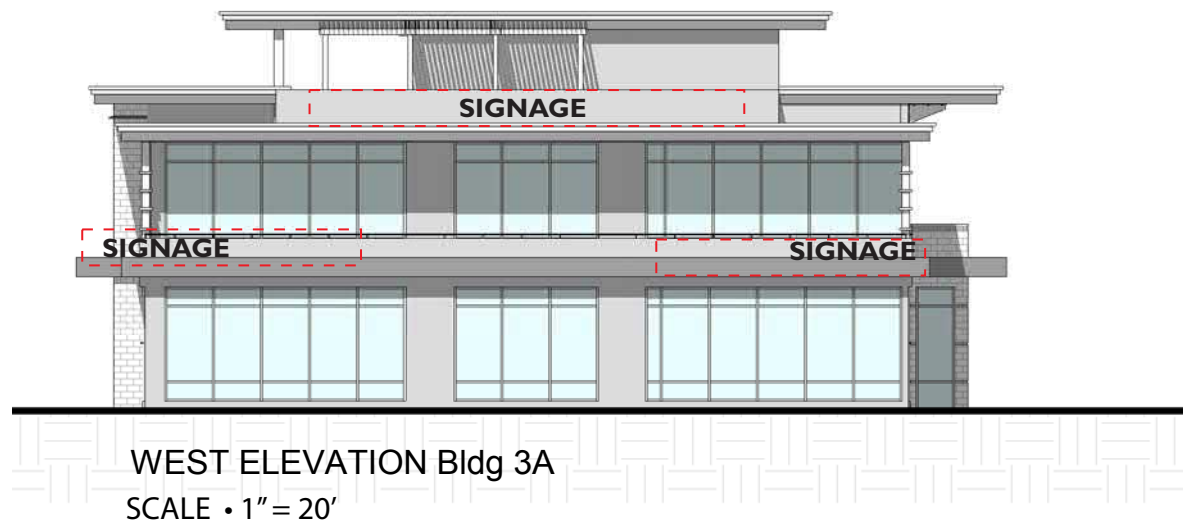
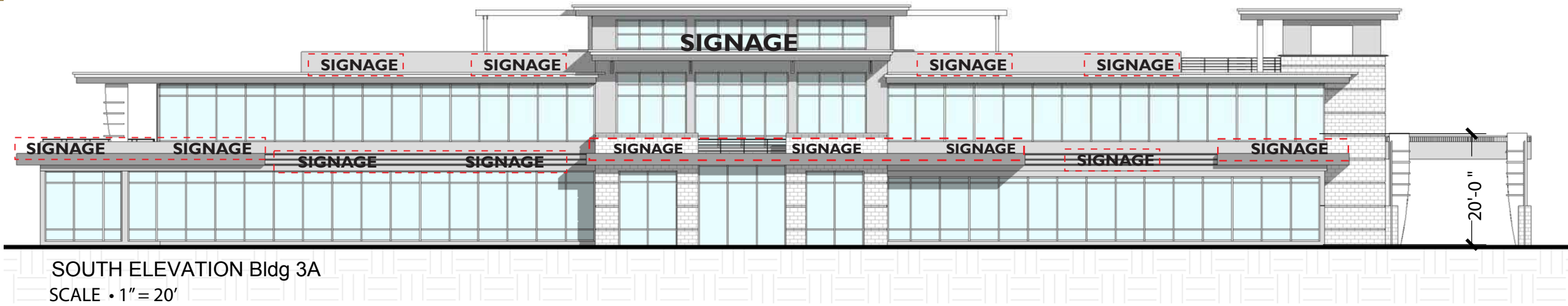
Wall Sign Locations - Typical Retail 1B & 2E

Note - Sign locations shown are merely possible locations and mounting methods. Not all signage locations shown will be used. Final locations to be determined upon leasing.



Wall Sign Locations - 2 Story Office

Note - Sign locations shown are merely possible locations. Final locations to be determined by Developer. Maximum of three (3) signs per North and South elevations.



General Performance Requirements

All signs at The District Downtown must be compatible with the standards outlined in this Master Sign Program and in conformance with City of Chandler Sign Requirements. The purpose of the sign standards is to ensure an attractive commercial office environment and to protect the interests of the surrounding neighborhood, Developer, and Owners/Occupants of all suites within The District. Conformance will be strictly enforced, and any installed nonconforming or unapproved signs will be brought into conformance at the sole expense of the Owner/Occupant.

Meridian West Development LLC herein referred to as "Developer", and The District Downtown Unit Owner's Association herein referred to as "Association" shall administer and interpret the criteria. Before designing signs, all The District Downtown Owners/Occupants will receive a copy of these signage standards. Sign plans submitted to the Developer/Association for approval must conform to these standards. The Developer/Association will administer and interpret the criteria. All signs must be approved in writing by the Developer/Association prior to permit application and installation.

1 Signage proposal

Prior to preparation of signage drawings and specifications, the Owner's/Occupant's sign contractor must review all architectural, structural, and electrical documents as they relate to the building wall and/or storefront at the proposed signage location. In addition, the sign contractor should visit the project site to become familiar with as-built conditions and verify all dimensions.

Each Owner/Occupant must submit to the Developer/Association four (4) sets of detailed shop drawings showing locations, sizes, design, colors, materials, lettering, graphics, conduits, junction boxes, sleeves, methods of illumination and other mounting apparatus of all proposed wall, window, and rear door signs. This submittal must be made at least fifteen (15) days prior to submittal to the City for permits.

2 Developer/Association Approval

After review of the signage proposal, the Developer/Association will return one of the three sets of drawings to the Owner/Occupant, marked either "Approved", "Approved as Noted," or "Revise and Resubmit."

An approval from Developer/Association does not guarantee City approval. A sign permit must be obtained and issued prior to fabrication. For City permits, approved sign drawings from the Developer/Association along with the required documentation for City for review.

- **"Approved"**

If drawings are marked "Approved," the Owner/Occupant is allowed to proceed with obtaining review and approval from the City.

- **"Approved as Noted"**

If drawings are marked "Approved as Noted," the Owner/Occupant is allowed to proceed with City review, provided that any modifications noted are incorporated into the design and proceed

An applicant that takes exception to the noted modifications may revise and resubmit, as explained below.

- **"Revise and Resubmit"**

If drawings are marked "Revise and Resubmit," the plans will be returned to the Owner/Occupant with comments. The drawings shall be revised and resubmitted for Developer/Association approval.

3 Openings in building walls

Locations of all openings for conduit and sleeves in building walls must be indicated by the sign contractor on the drawings submitted. The contractor shall install the sign in accordance with the approved drawings.

4 Messages

Except for tenants leasing more than 5,000 square feet of space; Sign Band signs are restricted to advertising either (a) the person, firm, company, or corporation operating the use conducted on the site, or (b) the products sold therein, but not both.

5 Owner/Occupant responsibilities for other regulations

The Developer/Association's approval of an Owner's/Occupant's signage plan does not constitute an implication, representation, or certification by the Developer/Association that those plans are in compliance with applicable statutes, codes, ordinances, or other regulations. Compliance with other regulations is the sole responsibility of the Owner/Occupant for all work performed on the premises by or for the Owner/Occupant.

6 Prohibited signs

- Cabinet (Typical) Acrylic, Plexiglas, or plastic-faced panels with surface or second-surface applied or painted graphics, injection molded panels with integral color - internally backlit cabinet construction.
- No signs, advertisements, notices, or other lettering shall be displayed, exhibited, inscribed, painted, or affixed on any part of the buildings visible from outside the premises, except as specifically approved by the Developer/ Association.
- Rude, obscene, offensive, animated, flashing, blinking, rotating, moving, or audible signs.
- Placards; posters; playbills; postings; paintings; flags; signs in public right-of-way; and fixed balloons are not permitted in any location, whether or not visible from outside the premises.
- Change-panel signs are prohibited.
- "A" frames and portable signs.
- Signs that are installed without written approval from the Developer/Association, or that are inconsistent with approved drawings, may be subject to removal and reinstallation by the Developer at the Owner's Occupant's expense.
- Painted or hand lettered signs on storefronts.
- Flashing, moving or audible signs.
- Luminous vacuum formed type plastic letters
- Inflatable signs or graphic devices.
- Freestanding attraction boards, posters or other permanent advertising devices.
- Paper, cardboard or Styrofoam signs.
- Signs with exposed neon tubing or raceways.
- Signs with gold or silver plastic trim caps that contrast with letter returns.

7 Illuminated signs

The City of Chandler requires permits for all signs and electrical permits for all signs that are illuminated. It is the Owner's/Occupant's sole responsibility to secure these and any other permits that may be required.

8 Size limitation

Each Owner/Occupant must limit the area of its sign in accordance with the area allocated for signage. Maximum letter height and length varies according to storefront, but it must not exceed the area allocated for signage. Each Owner/Occupant will be granted a minimum of one sign. Owners/Occupants with more than one elevation wall may have a sign on each elevation.

9 Labels

No labels are permitted on the exposed surface of signs, except those required by local ordinance. Sets of individual letters shall have one label on an end letter only. These are to be installed in an inconspicuous location.

10 Freestanding signs

All freestanding signs must meet applicable setbacks, and their installation must comply with all local building and electrical codes.

11 Upkeep and maintenance

Each Owner/Occupant is fully responsible for the upkeep and maintenance of its sign(s), including any individual pylon or monument signage, and Owners/Occupants are to repair any sign defects within five (5) days of notification. If an Owner/Occupant does not repair said sign(s), the Developer, at the Owner/Occupant sole cost and expense, may repair and/or replace sign(s).

A penalty of 100% of the Developer's cost to repair said signage, in addition to the cost of the repair, may be assessed to the Owner/Occupant if the Developer is required to provide the necessary maintenance due to the Owner's/Occupant's noncompliance following notification.

12 Illumination timer

Power to illuminate the Owner's/Occupant's sign is to be from Owner's/Occupant's electricity meter, switched through time clock, set in accordance with schedules determined by the Developer.

13 Sealing of building penetrations

All penetrations of the building structure required for sign installation shall be neatly sealed in a watertight fashion.

14 Damage caused by or during installation

The sign contractor and/or Owner/Occupant will pay for any damage to a building's fascia, canopy,

structure, roof, or flashing caused by sign installation. Owner/Occupant shall be fully responsible for the operations of Owner's/Occupant's sign contractor and shall indemnify, defend, and hold Developer/ Association/Developer harmless for, from, and against damages or liabilities of account thereof.

15 Required insurance for sign contractors

All sign contractors must carry workers' compensation and commercial liability insurance against all damages suffered or done to any and all persons and/or property while engaged in the construction or installation of signs, with a combined single limit in an amount not less than two million and no/100 dollars (\$2,000,000.00) per occurrence. Every sign contractor must hold a current contractor's license in the State of Arizona. Developer must be named as additionally insured in the workers' compensation and commercial liability insurance.

16 Sign Permits

Owner/Occupant is responsible for obtaining all necessary sign permits prior to sign installation.

17 Developer/Association's right to modify requirements

The Developer/Association has the right to modify

Definitions

The following definitions are used throughout the comprehensive signage plan and are exclusive to this document.

Aggregate Sign Area

The total area of all permitted signs pertaining to any one Owner/Occupant, which includes Street-front signage, Storefront signage (arcade and window signage/graphics).

Architectural Frontage

The portion of the building frontage of the leased space which has been delineated through use of reveals, patterning, materials, finishes, column breaks, etc that creates a special area for signage.

Architectural Awnings

Elements which are constructed of permanent type of materials and are an integral part of the building elevation design.

Building Leased Frontage

The overall lineal foot frontage of a leased space.

Cabinet (Typical)

Acrylic, Plexiglas, or plastic-faced panels with surface or second-surface applied or painted graphics, internally backlit in a box construction.

Custom Cabinet

Cabinet designed and fabricated in multiple planes, colors, finishes and unique shapes and forms. Typically with dimensional illuminated letters and opaque backgrounds.

Committee

Made up of the consenting owners as defined in the Construction, Operation and Reciprocal Easement Agreement.

Graphics

Lettering, symbols, and logos used for name identification (primary identification), and for identification of product and services (secondary identification or modifiers).

Sign Area

The aggregate area of the smallest rectangles that encloses individual elements of a sign's lettering and logos.

Sign Envelope

The overall height and length allowances of sign area designated for Owner/Occupant sign placement on a building elevation.

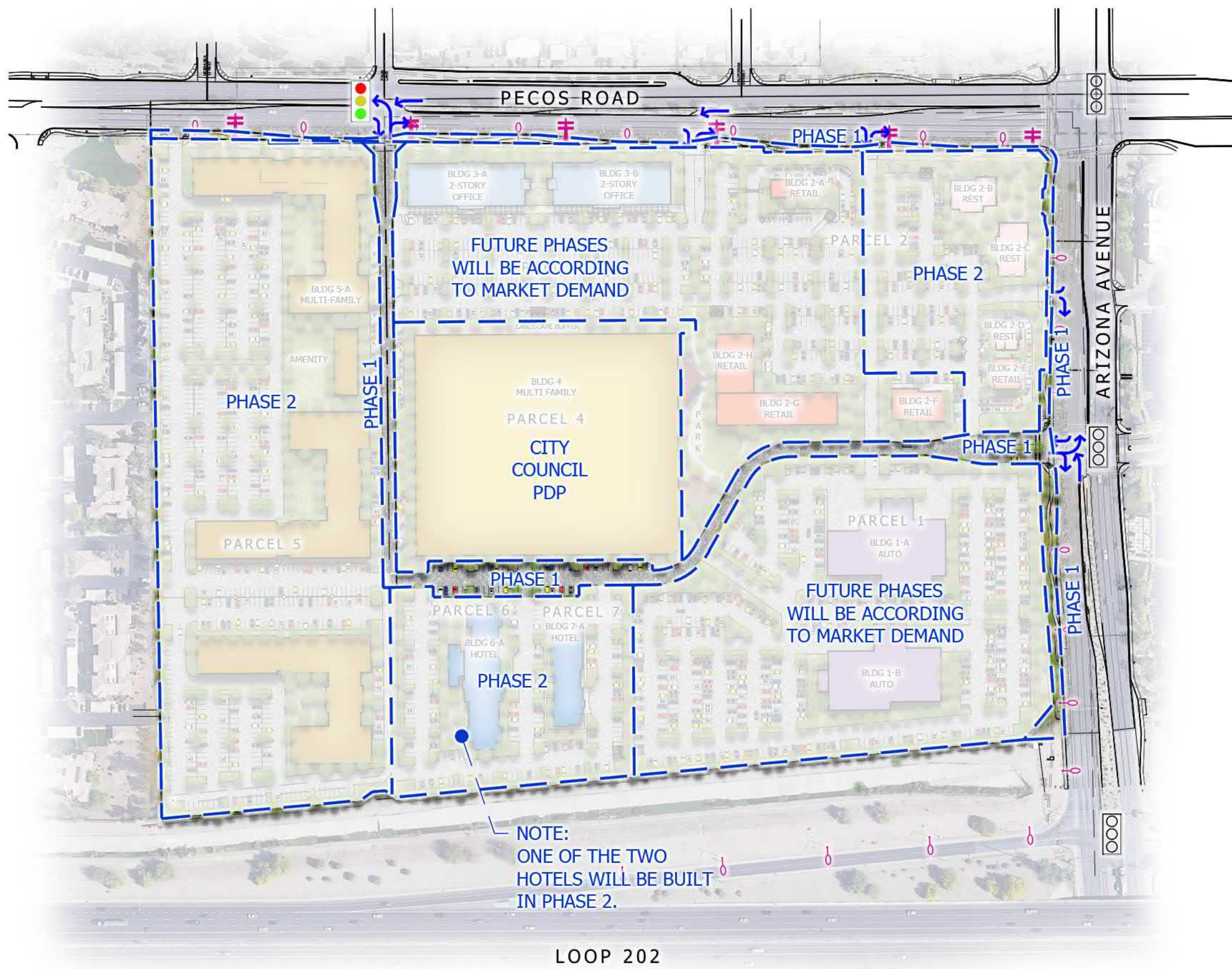
Street Front Signage

Signage installed parallel to the building fascia, typically located along the front of the building on parapet, fascia or building wall intended for the viewing of vehicular traffic.








Storefront Signage & Graphics- Permanent

This is the signage located along the storefront portion, oriented to pedestrian. It includes the transparent portion of storefront (windows) and/or solid wall areas used for merchandise display and permanent graphics. This includes awnings, tenant suite number, logo and name identification, secondary name modifiers, hours of operation, services or name brand marketing, menu cabinets, etc.

Exhibit 9



LEGEND

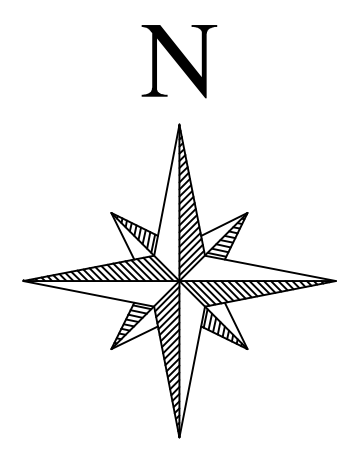
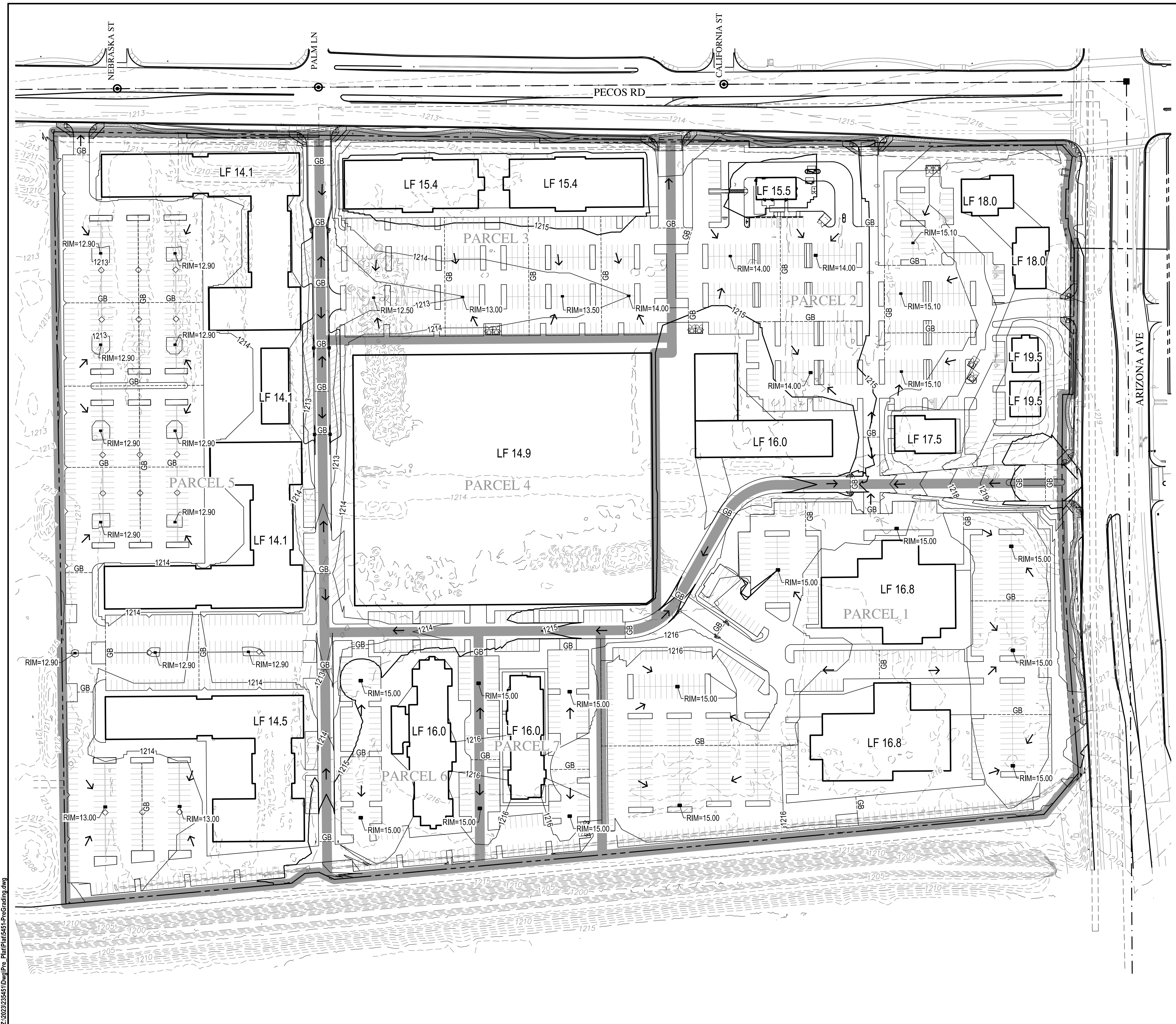
-  EXISTING POWER POLE
-  EXISTING STREET LIGHT
-  EXISTING TRAFFIC SIGNAL
-  PROPOSED TRAFFIC SIGNAL
-  FULL TRAFFIC MOVEMENT
-  RIGHT IN / RIGHT OUT ONLY
-  PHASE LINE

NOTE: GRAPHIC MATERIALS SHOWN ARE PRELIMINARY AND SUBJECT TO CHANGE. ALL SITE PLAN AND REPRESENTATIVE IMAGERY SHALL BE CONSIDERED CONCEPTUAL. FINAL SITE DESIGN MATERIALS TO BE PROVIDED AT THE TIME OF FINAL IMPROVEMENT PLANS.

NOTE:
ONE OF THE TWO
HOTELS WILL BE BUILT
IN PHASE 2.

LOOP 202

Exhibit 10



Horz. 1 in. = 80 ft.

LEGEND

- LF LOWEST FINISH FLOOR
- GB CHANGE IN GRADE
- FLOW ARROW
- PROPOSED CATCH BASIN
- PARCEL BOUNDARY



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DISTRICT DOWNTOWN
PRELIMINARY GRADING AND DRAINAGE PLAN
 SWC ARIZONA AVE AND PECOS RD
 CHANDLER, AZ

RETENTION CALCULATIONS

$V_r = C \times (P/12) \times A \times 1.1$
 DESIGN STORM: 100-YEAR, 2-HOUR
 RAINFALL DEPTH, P: 2.20 INCHES

RETENTION VOLUMES

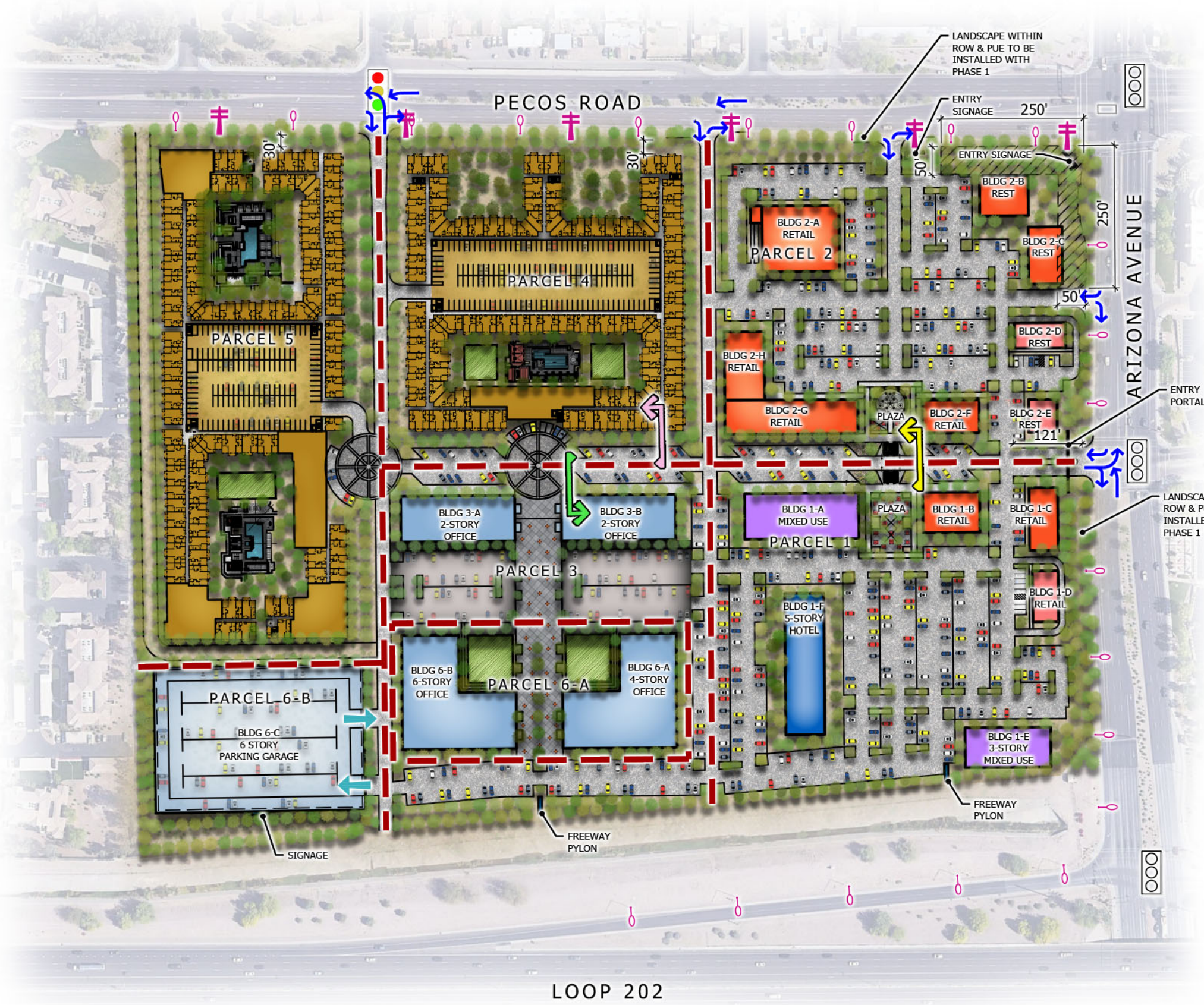
BASIN ID	AREA (SF/AC)	C	VOLUME REQUIRED ¹ , V _r (CF/AF)	DRYWELLS REQUIRED ² (0.1 CFS / DW)
PARCEL 1	399,533 / 9.17	0.9	72,515 / 1.66	6
PARCEL 2	389,164 / 8.93		70,833 / 1.62	6
PARCEL 3	190,468 / 4.37		34,570 / 0.79	3
PARCEL 4	262,289 / 6.02		47,605 / 1.09	4
PARCEL 5	538,027 / 12.35		97,652 / 2.24	8
PARCEL 6	99,545 / 2.29		18,067 / 0.41	2
PARCEL 7	75,531 / 1.73		13,709 / 0.31	2
O-1 (PECOS)	118,487 / 2.72	22,700 / 0.52	2	
O-2 (AZ-87)	82,673 / 1.90	15,839 / 0.36	2	
TOTAL			393,291 / 9.03	35

- NOTES
- REQUIRED RETENTION SHALL PROVIDE 110% OF THE VOLUME REQUIRED (V_r) PER THE CITY OF CHANDLER ENGINEERING & DESIGN STANDARDS MANUAL, 2023 EDITION.
 - FOR DESIGN AND PERMITTING PURPOSES, EACH DRYWELL SHALL HAVE A PRESUMPTIVE CAPACITY OF 0.1 CFS (12,960 CUBIC FEET IN 36 HOURS). CREDIT MAY BE ALLOWED TOWARDS THE TOTAL REMAINING DRYWELLS TO BE CONSTRUCTED AT ONE-HALF OF THE TESTED CAPACITY, LIMITED TO 0.5 CFS.

DATE	DESCRIPTION	REV

NOT FOR CONSTRUCTION OR RECORDING

SCALE (HORIZ.) 1" = 80'
 SCALE (VERT.) N/A
 DATE 5/08/2024
 JOB NUMBER 235451
 SHEET 1 OF 1



SITE DATA	
DESCRIPTION	ACREAGE
GROSS ACREAGE	49.30 AC.
NET ACREAGE	44.73 AC.

PARCEL NUMBER	ACREAGE	USE	FLOOR AREA RATIO	FLOOR AREA	KEYS	UNITS	Density	PARKING REQUIREMENTS	
								PARKING RATIO	REQUIRED
PARCEL 1									
BLDG 1-A		MU		16,000 S.F.				250 S.F.	64
BLDG 1-B		RET		7,000 S.F.				250 S.F.	28
BLDG 1-C		REST		5,500 S.F.					69
BLDG 1-D		REST		4,000 S.F.					35
BLDG 1-E		MU		31,500 S.F.				200 S.F.	158
BLDG 1-F		HOTEL		88,900 S.F.	180 KEYS			1/KEY, 1/100 PUBLIC	210
PARCEL 1 TOTAL	8.42 AC.		0.42	152,900 S.F.	180 KEYS				493

PARCEL 2									
BLDG 2-A		RET		15,500 S.F.				250 S.F.	62
BLDG 2-B		REST		6,000 S.F.					75
BLDG 2-C		REST		6,000 S.F.					75
BLDG 2-D		REST		4,000 S.F.					35
BLDG 2-E		REST		2,900 S.F.					36
BLDG 2-F		REST		5,800 S.F.				250 S.F.	23
BLDG 2-G		RET		14,000 S.F.				250 S.F.	56
BLDG 2-H		RET		8,400 S.F.				250 S.F.	34
PARCEL 2 TOTAL	8.38 AC.		0.17	62,600 S.F.					396

PARCEL 3									
BLDG 3-A		OFFICE		32,000 S.F.				200 S.F.	160
BLDG 3-B		OFFICE		32,000 S.F.				200 S.F.	160
PARCEL 3 TOTAL	8.19 AC.		0.18	64,000 S.F.					320

PARCEL 4									
BLDG 4-A		MF				395 UNITS	52.0 DU/AC	1.8 PER UNIT	716
PARCEL 4 TOTAL	7.60 AC.					395 UNITS	52.0 DU/AC		725

PARCEL 5									
BLDG 5-A		MF				400 UNITS	43.5 DU/AC	1.8 PER UNIT	725
PARCEL 5 TOTAL	9.20 AC.					400 UNITS	43.5 DU/AC		725

PARCELS 6A & 6B									
BLDG 6-A		OFFICE		120,000 S.F.				200 S.F.	600
BLDG 6-B		OFFICE		180,000 S.F.				200 S.F.	900
BLDG 6-B		OFFICE		0 S.F.					
PARCEL 6 TOTAL	3.08 AC.		2.24	300,000 S.F.					1500

TOTAL	
MULTI-FAMILY	795 UNITS
RETAIL	
RESTAURANT	
MIXED USE	
HOTEL	180 KEYS
OFFICE	
TOTAL	44.87 AC. COMM 0.30 579,500 S.F. 180 KEYS 795 UNITS

LEGEND

- EXISTING POWER POLE
- EXISTING STREET LIGHT
- EXISTING TRAFFIC SIGNAL
- PROPOSED TRAFFIC SIGNAL
- FULL TRAFFIC MOVEMENT
- RIGHT IN / RIGHT OUT ONLY

SECTION VIEWS

- TYPICAL MAIN STREET AT RETAIL
- TYPICAL MAIN STREET AT RESIDENTIAL
- TYPICAL MAIN STREET AT OFFICE

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