

## **CITY PLANNING BOARD**

### **BOARD AGENDA**

Planning Board Regular Meeting - Tuesday, August 9, 2016 - 6:00 p.m.  
City Hall - City Commission Chambers, 100 North U.S. #1, Fort Pierce, Florida

1. **CALL TO ORDER**
2. **PLEDGE OF ALLEGIANCE**
3. **ROLL CALL**
4. **CONSIDERATION OF ABSENCES**
5. **CERTIFICATION OF ALTERNATE MEMBER VOTING STATUS**
6. **APPROVAL OF MINUTES**
  - a. Minutes from the July 12, 2016 meeting
7. **NEW BUSINESS**
  - a. Mayfair at Lawnwood - Request Continuance to the September 13, 2016 Planning Board Meeting
  - b. Site Plan & Design Review - Wawa - 5575 Okeechobee Road
  - c. Conceptual Development Plan - Interstate Business Park - 2496 S. Kings Highway
  - d. Conceptual Development Plan - Kings Highway Business Park - 7001 White Road
8. **BOARD COMMENTS**
9. **ADJOURNMENT**

Any person seeking to appeal any decision by the Planning Board with respect to any matter considered at this meeting is advised that a record of proceedings is required in any such appeal and that such person may need to insure that a verbatim record of the proceedings is made including the testimony and evidence upon which the appeal is to be based.

Persons who require special accommodations under the Americans with Disabilities Act (ADA) should contact (772) 467-3729, at least five (5) days prior to the meeting. Persons who are hearing or speech impaired may use the Florida Relay System by dialing 711.

**Planning Board**

**6. a.**

Meeting Date: 08/09/2016

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Information

REQUESTED ACTION

Minutes from the July 12, 2016 meeting

LOCATION

RESPONSIBLE STAFF

RECOMMENDATION

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Attachments

Planning Board Minutes 7/12/16

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**Form Review**

Form Started By: Alicia Rosenthal

Started On: 07/25/2016 09:02 AM

Final Approval Date: 07/27/2016

# DRAFT



## CITY OF FORT PIERCE PLANNING BOARD

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### Planning Board Minutes

OF THE REGULAR MEETING OF THE FORT PIERCE CITY PLANNING BOARD HELD ON TUESDAY, **JULY 12, 2016**, IN FORT PIERCE CITY HALL, COMMISSION CHAMBERS, 100 NORTH US HIGHWAY 1, FORT PIERCE, FLORIDA.

1. CALL TO ORDER

2. PLEDGE OF ALLEGIANCE

3. ROLL CALL

Present: **Mike Dahan; Steve Weaver; Frank Creyaufmiller; Charles Hayek; Marcia Baker; John George; Eloise Cumings; Al Bernetti; Tim O'Connell; Bob Burdge, Chairman**

Absent: **Robert Poitier; Brian Paul**

Staff Present: **Jim Messer, City Attorney  
Kori Benton, Senior Planner  
Vennis Gilmore, Planning Analyst  
Alicia Rosenthal, Administrative Assistant**

4. CONSIDERATION OF ABSENCES

Mr. Poitier absence is unexcused.

**Motion was made by John George, and seconded by Marcia Baker to excuse Mr. Paul's absence for the June 2016 and July 2016 meetings.**

**AYE: Mike Dahan, Steve Weaver, Frank Creyaufmiller, Charles Hayek, Marcia Baker, John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Chairman Bob Burdge**

Passed

5. CERTIFICATION OF ALTERNATE MEMBER VOTING STATUS

Mr. Bernetti and Mr. Weaver were made voting members for the meeting.

6. APPROVAL OF MINUTES

- a. Minutes from the June 14, 2016 meeting

**Motion was made by John George, and seconded by Marcia Baker to approve the minutes from the June 14, 2016 meeting.**

**AYE: John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Steve Weaver, Frank Creyaufmiller, Charles Hayek, Marcia Baker, Chairman Bob Burdge**  
Passed

**7. NEW BUSINESS**

**a. Mayfair at Lawnwood - Request Continuance to the August 9, 2016 Planning Board Meeting**

Mr. Benton gave an update on Mayfair at Lawnwood and stated the applicant and staff are requesting the Board to continue the item until the August 9, 2016 meeting.

**Motion was made by Marcia Baker, and seconded by John George to move Mayfair at Lawnwood from the table.**

**AYE: Steve Weaver, Frank Creyaufmiller, Charles Hayek, Marcia Baker, John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Chairman Bob Burdge**  
Passed

**Motion was made by Marcia Baker, and seconded by John George to continue Mayfair at Lawnwood until the August 9, 2016 Planning Board meeting.**

**AYE: Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Steve Weaver, Frank Creyaufmiller, Charles Hayek, Marcia Baker, John George, Chairman Bob Burdge**  
Passed

**b. Conditional Use with New Construction - Waste Management Transfer Station Expansion - 3890 Selvitz Road**

Mr. Gilmore gave an overview of the application. No one spoke for or against the application.

**Motion was made by John George, and seconded by Marcia Baker to forward a recommendation to City Commission to approve the request as presented.**

**AYE: Frank Creyaufmiller, Charles Hayek, Marcia Baker, John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Steve Weaver, Chairman Bob Burdge**  
Passed

**c. Conditional Use with No New Construction - JP Custom Cabinets - 103 N. 13th Street**

Mr. Benton gave an overview of the application and answered questions from the Board regarding the traffic pattern, landscaping, irrigation and air quality components of the operation.

David Cleveland, Applicant Representative, provided additional information and answered questions from the Board. Mr. Cleveland stated his clients have no objections of installing some drip lines to the new proposed trees.

**Motion was made by John George, and seconded by Charles Hayek to approve the presented request with the condition that the applicant integrates additional landscaping along the west and north property lines pursuant to City Code Section 22-187(6).**

**AYE: Charles Hayek, Marcia Baker, John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Steve Weaver, Frank Creyaufmiller, Chairman Bob Burdge**  
Passed

**d. Conditional Use - Dock - 1703 Sunset Isles Road – Surfside Canal**

Mr. Benton gave an overview of the application and answered questions from the Board regarding the floating dock.

**Motion was made by Marcia Baker, and seconded by John George to forward a recommendation of approval to the City Commission.**

**AYE: Marcia Baker, John George, Eloise Cumings, Al Bernetti, Tim O'Connell, Mike Dahan, Frank Creyaufmiller, Charles Hayek, Chairman Bob Burdge**  
**Other: Steve Weaver (RECUSE)**  
Passed

**8. BOARD COMMENTS**

**9. ADJOURNMENT**



**APPOINTED OFFICERS (continued)**

- A copy of the form must be provided immediately to the other members of the agency.
- The form must be read publicly at the next meeting after the form is filed.

IF YOU MAKE NO ATTEMPT TO INFLUENCE THE DECISION EXCEPT BY DISCUSSION AT THE MEETING:

- You must disclose orally the nature of your conflict in the measure before participating.
- You must complete the form and file it within 15 days after the vote occurs with the person responsible for recording the minutes of the meeting, who must incorporate the form in the minutes. A copy of the form must be provided immediately to the other members of the agency, and the form must be read publicly at the next meeting after the form is filed.

**DISCLOSURE OF LOCAL OFFICER'S INTEREST**

I, STEVEN M. WEAVER, hereby disclose that on JULY 12, 2015, 20\_\_ :

(a) A measure came or will come before my agency which (check one or more)

- inured to my special private gain or loss;
- inured to the special gain or loss of my business associate, VERA MEYER ;
- inured to the special gain or loss of my relative, \_\_\_\_\_ ;
- inured to the special gain or loss of \_\_\_\_\_, by whom I am retained; or
- inured to the special gain or loss of \_\_\_\_\_, which is the parent subsidiary, or sibling organization or subsidiary of a principal which has retained me.

(b) The measure before my agency and the nature of my conflicting interest in the measure is as follows:

DOCK CONDITIONAL USE.  
APPLICANT IS A BUSINESS ASSOCIATE.

If disclosure of specific information would violate confidentiality or privilege pursuant to law or rules governing attorneys, a public officer, who is also an attorney, may comply with the disclosure requirements of this section by disclosing the nature of the interest in such a way as to provide the public with notice of the conflict.

July 12, 2015  
Date Filed

[Signature]  
Signature

NOTICE: UNDER PROVISIONS OF FLORIDA STATUTES §112.317, A FAILURE TO MAKE ANY REQUIRED DISCLOSURE CONSTITUTES GROUNDS FOR AND MAY BE PUNISHED BY ONE OR MORE OF THE FOLLOWING: IMPEACHMENT, REMOVAL OR SUSPENSION FROM OFFICE OR EMPLOYMENT, DEMOTION, REDUCTION IN SALARY, REPRIMAND, OR A CIVIL PENALTY NOT TO EXCEED \$10,000.

**Planning Board**

**7. a.**

Meeting Date: 08/09/2016

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Information

REQUESTED ACTION

Mayfair at Lawnwood - Request Continuance to the September 13, 2016 Planning Board Meeting

LOCATION

RESPONSIBLE STAFF

RECOMMENDATION

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**Form Review**

Form Started By: Alicia Rosenthal  
Final Approval Date: 08/03/2016

Started On: 08/02/2016 03:40 PM

Information

REQUESTED ACTION

Site Plan & Design Review - Wawa - 5575 Okeechobee Road

LOCATION

5575 Okeechobee Road

RESPONSIBLE STAFF

Vennis Gilmore, Planning Analyst

RECOMMENDATION

Staff recommends the Planning Board forward a recommendation of **approval** with the following conditions:

- 1) Per City Code Section 22-187. (10) Other areas. Areas in Commercial or Industrial zones shall have a site obscuring fence or planted material so as to provide a visual and noise buffer between such areas and the lot in the residential or open space zone. **Please integrate the appropriate buffering along the southern property line accordingly.**
- 2) Per City Code Section 22-31 (c) (4) j. Materials or objects which would detract from the open space character of an uncovered or unenclosed area will not be permitted in such an area. **Please consider an architectural feature such as an awning or canopy to cover the proposed ice container outside of the establishment.**
- 3) **Please consider adjustment of the north-west entry and shared driveway to provide greater clearance from the entry from Okeechobee Road, increase stacking area, and minimization of internal intersection conflicts.**

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Attachments

Staff Report  
Site Aerial  
Zoning Map  
Survey  
Site Plan  
Demo Plan  
Floor Plan  
Exterior Elevation Rendering  
Gas Canopy Rendering

Landscaping Plan  
Traffic Report  
TRC Comments

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### **Form Review**

Form Started By: Vennis Gilmore  
Final Approval Date: 08/03/2016

Started On: 08/02/2016 04:16 PM



# CITY OF FORT PIERCE

## PLANNING DEPARTMENT

REBECCA GROHALL, AICP, PLANNING DIRECTOR AND FPRA DIRECTOR  
COMPREHENSIVE PLANNING ♦ DEVELOPMENT REVIEW  
HISTORIC PRESERVATION ♦ URBAN DESIGN ♦ ZONING

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**TO:** Members of the City of Fort Pierce Planning Board

**THROUGH:** Rebecca Grohall, AICP, Planning Director

**FROM:** Vennis Gilmore, Planning Analyst

**SUBJECT:** **Application for Site Plan and Design Review**  
**Wawa Fueling Station**  
**5575 Okeechobee Road**

**DATE:** August 1, 2016

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### STAFF REPORT

**Owner:** S and S Rentals B, LLC  
400 High Point Dr.  
Suite 500  
Cocoa, FL. 32926

**Representative:** Scott Kerney P.E., Engineer, Wawa, Inc.  
7699 Golf Channel Drive  
Orlando, FL. 32819

**Applicant's Request:** Approval to construct a 6,119 sf fueling station in the General Commercial (C-3) District

**Location:** 5575 Okeechobee Road

**Parcel ID:** 2419-322-0002-000-5, 2419-322-0003-000-2, 2419-322-0004-000-9, & 2419-321-0001-000-5

**Current Zoning:** General Commercial Zone (C-3)

**Future Land Use:** General Commercial (GC)

**Surrounding Zoning:**

North	East	South	West
C-3	R-4	C-3	C-3

**Site Size:** 6.72 acres – Proposed Lease Area

**Utilities:** FPUA

## **Staff Analysis:**

### ***Request***

In accordance with Sections 22-58, 22-59, and 22-73 of the City Code, the applicant is requesting the review and approval of a Site Plan and Design Review to construct a 6,119 sf Fueling Station on a collection of parcels in the C-3 General Commercial district, located at 5575 Okeechobee Road. The site is presently utilized by Elpex Commercial Tire & Service Center acting as a Goodyear Tires satellite location. The site currently supports a 10,246 sf brick stucco building with a tar and gravel roof, a 9,120 sf metal structure with 9 commercial vehicle service bays, built in 1968. The proposed 6,119 sf fueling station to replace the existing facility will include a Wawa convenience store and a canopy for the fuel pumps.

The proposed site improvements include the removal of the building, existing asphalt, and the construction of a 6,119 square foot Wawa Convenience Store and gas station along with associated parking and utilities. A series of storm inlets with exfiltration trench and dry detention area will convey runoff and will provide water quality and water quantity for the site. Additional utilities are required with this development and therefore will require additional utility permitting. In compliance with City Code Section 22-187 General Landscaping Requirements; landscaping improvements are included in the applicant's site plan.

The property is located southeast corner of the Okeechobee Road and Jenkins Road intersection. The site is surrounded by C-3 General Commercial businesses to the north, south, and west. East of the subject property is R-4 Medium Density Residential zoning. The existing 19,366 sf of buildings will be demolished upon site plan and design review approval.

### ***Zoning & Land Use***

The subject site is located within the General Commercial Zone (C-3) and has a General Commercial land use designation. The purpose of this district is intended to provide for a broad variety of business activities including shoppers' goods stores, convenience goods and service establishments, offices and tourist/entertainment facilities. Many public and semi-public uses are also appropriate. Compared to the C-4 zone, this district is more suitable for uses requiring a high degree of accessibility to vehicular traffic, low intensity uses on large tracts of land, most repair services and small warehousing and wholesaling operations. Although this zone should be located along or near arterial or collector streets, it is not the intent of this district to encourage the extension of strip commercial areas. Instead it should promote concentrations of commercial activities.

The General Commercial designation provides for higher intensity commercial developments or horizontal and vertical mixed-use developments. Uses allowed within this designation include multifamily residential, intensive and general commercial, retail, service, offices, tourist/entertainment facilities, hotels/motels, parks and recreation, along with compatible public, quasi-public, and special uses. Development shall include either commercial or mixed uses fronting major roadway corridors with higher intensity near major intersections.

The presented development is consistent with the land development code and the comprehensive plan.

### ***Traffic***

Access to the site is provided via two-way driveways on Okeechobee Road and Jenkins Road. The proposed use will increase traffic generation of the site due to new patrons and frequency of customers. The proposed operation is estimated to generate 5,175 trips daily, of which 3,260 are from existing pass by capture, and 1,915 are new trips. Of the new trips, 250 are designated as AM peak hour, and 312 PM peak hour trips. The increase of the existing use totals approximately 198 peak hour new daily trips, of which 92 are AM peak hour and 106 are PM peak hour. The proposed trips will be met with the active Florida Department of Transportation (FDOT) enhancements to Okeechobee Road, and the intersection with Jenkins. Additionally, the proposed site plan integrates a designated turn lane from Okeechobee to be utilized by Wawa and future development to the east. Staff suggests that the applicant consider adjustment of the north west entry and shared access driveway to provide greater clearance for safety.

### ***Landscaping***

Pursuant to City Code Section 22-187 (7) interior vehicular use areas, the use will have a 74,030 sf vehicular use area; requiring 4,936 sf of landscape area. This interior landscaping will consist of 50 trees; of which trees, 22 will be oaks, 4 palms substitutes, 14 magnolias, and 10 cypresses. Per City Code Section 22-187 (4)(5)(6), 20 trees with a hedge are provided within the landscape strips abutting a right-of-way, via 11 oaks and 9 crape myrtles. Landscape strips abutting adjacent property necessitates 19 trees and the applicant will be providing 2 trees, 16 pines, and 1 oak. Additionally, due to the residential zoning of the property to the south, the applicant will be integrating a landscape hedge to buffer the site. The refuse collection area will be screened with the suggested shrubs by code. Currently, there are no trees to be mitigated for the proposed development.

### ***Design Review***

The city's design review guidelines suggest approaches to design situations that will contribute to the existing and developing character of the city. The intent of the city's design review process is not to stifle innovative architecture but rather to assure respect for and reduce incompatible and adverse impacts on the visual experience throughout the city. This shall be accomplished through respectful interpretation of vernacular building typologies and styles in proposed development. Proposed designs shall take cues from traditional proportioning systems and be synonymous with a chosen style.

Wawa's design presentation is intended to reflect the language and style associated with modern Florida vernacular architecture typified by clapboard siding, a pitched roof element, pastel colors, and front awnings. The design integrates a variety of landscaping to blend the fuel islands, store and parking areas with the surrounding commercial corridor. The fuel stations are presented with trademark "winged" gas canopy, sloping towards the right-of-way to expose the colored metal roof design.

### **Technical Review Committee**

All affected departments have reviewed the proposed Conditional Use with regards requirements of the City Code. Findings from the review by corresponding departments and the associated responses by the applicant are provided for viewing by the Planning Board. The applicant has been asked for a resubmittal of recommended improvements by staff. New information presented to staff prior to the Planning Board meeting will be provided to all Planning Board members for further review.

## Staff Recommendation

The presented Site Plan, Design Review and related new construction adhere to the requirements of the City Land Development Code and guidelines of the City's Comprehensive Plan pertaining to the requested fueling station activity the subject requests. Minor landscaping and driveway revisions have been agreed upon by the applicant after receiving the technical review commentary. The applicant's responses to technical review commentary have been received but no revisions have been submitted. Should staff receive these revisions prior to the Planning Board meeting; the revisions will be provided and acknowledged. Staff recommends the Planning Board forward a recommendation of **approval** with the following conditions:

1) Per City Code Section 22-187. (10) Other areas. Areas in Commercial or Industrial zones shall have a site obscuring fence or planted material so as to provide a visual and noise buffer between such areas and the lot in the residential or open space zone. **Please integrate the appropriate buffering along the southern property line accordingly.**

2) Per City Code Section 22-31 (c) (4) j. Materials or objects which would detract from the open space character of an uncovered or unenclosed area will not be permitted in such an area. **Please consider an architectural feature such as an awning or canopy to cover the proposed ice container outside of the establishment.**

3) **Please consider adjustment of the north-west entry and shared driveway to provide greater clearance from the entry from Okeechobee Road, increase stacking area, and minimization of internal intersection conflicts.**



Cloer Rd

Cloer Rd

Sonic Drive-In

Walgreens

RaceTrac

Coffee House

Okeechobee Rd

S Jenkins Rd

Kerr St

5575 Okeechobee Road

70

70



## ZONING MAP

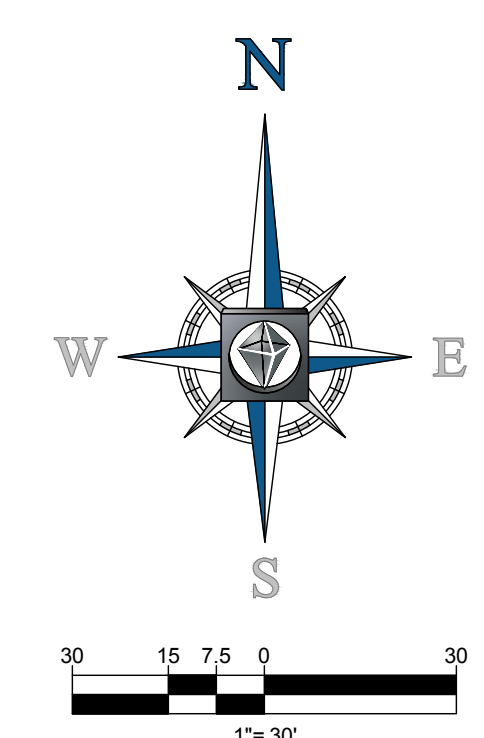
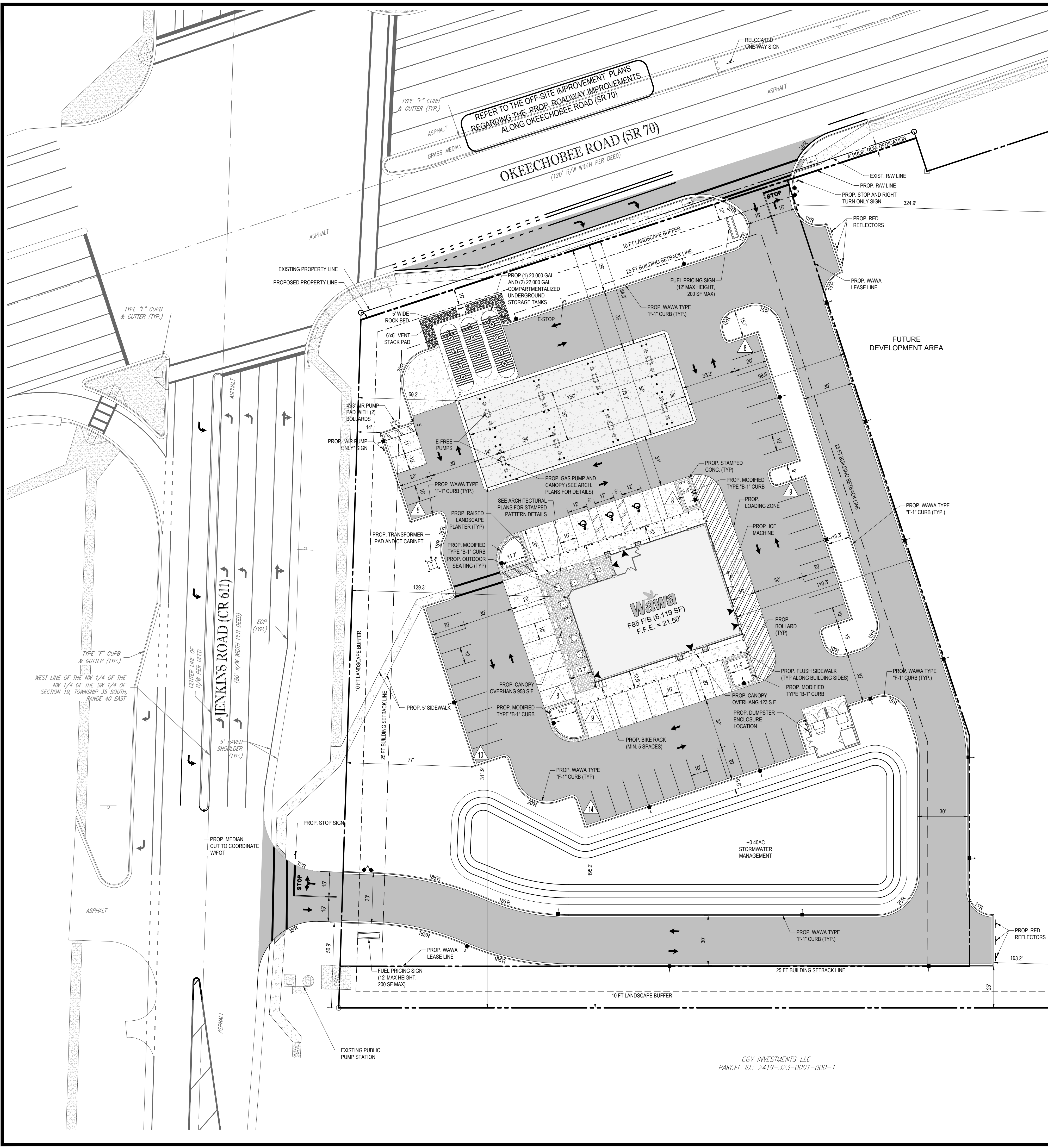
**C-3 General Commercial**



**R-4 Medium Residential**







**LEGEND:**

- PROPERTY LINE
- PROP. ASPHALT PAVEMENT
- PROP. BUILDING AND GAS CANOPY
- PROP. HEAVY DUTY CONCRETE PAVEMENT
- PROP. STANDARD CONCRETE PAVEMENT
- EXISTING CONCRETE SIDEWALK
- PROP. GRAVEL
- PROP. STAMPED CONCRETE
- PROP. PARKING SPACE COUNT
- PROP. GAS PUMPS
- PROP. SIGN
- PROP. VENT STACK
- PROP. AIR MACHINE
- PROP. UNDERGROUND STORAGE TANKS
- PROP. LIGHT POLE
- PROP. TRANSFORMER PAD

**SITE DATA:**

PROJECT NAME: WAWA - SR70 & JENKINS  
 PROJECT USE: CONVENIENCE STORE W/ GAS SALES, 8 PUMPS (16 FUELING POSITIONS)  
 APPLICANT: WAWA  
 ENGINEER: BOHLER ENGINEERING  
 SITE ADDRESS: 5575 OKEECHOBEE ROAD FORT PIERCE, FL 34947  
 PARCEL ID NO.'S: 2419-321-0001-000-5 2419-322-0002-000-5

BUILDING SETBACKS	REQUIRED	PROVIDED
NORTH (ALONG OKEECHOBEE BLVD)	25'	178.2'
EAST (TO LEASE LINE)	15'	110.3'
SOUTH	15'	195.2'
WEST (ALONG JENKINS ROAD)	15'	129.3'

CANOPY SETBACKS	REQUIRED	PROVIDED
NORTH (ALONG OKEECHOBEE BLVD)	25'	64.5'
EAST (TO LEASE LINE)	15'	98.6'
SOUTH	15'	311.9'
WEST (ALONG JENKINS ROAD)	15'	60.2'

**AREA CALCULATIONS**

TOTAL SITE AREA	292,878 SF (6.72 AC)
ROW DEDICATION	1,399 SF (0.03 AC)
NET SITE AREA	291,479 SF (6.69 AC)
WAWA LEASE AREA	145,638 SF (3.34 AC) - 100.00%

**TOTAL IMPERVIOUS AREA**

BUILDING AREA	95,167 SF (2.18 AC) - 65.35%
CANOPY AREA	6,119 SF (0.14 AC) - 4.20%
PAVEMENT / SIDEWALK AREA	7,150 SF (0.16 AC) - 4.91%
	81,898 SF (1.88 AC) - 56.24%

**TOTAL PERVIOUS AREA**

	50,471 SF (1.16 AC) - 34.65%
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EXISTING ZONING: C-3 COMMERCIAL  
 EXISTING LAND USE: CG (GENERAL COMMERCIAL)  
 FEMA FLOOD ZONE: FLOOD ZONE "X" ACCORDING TO THE FLOOD INSURANCE RATE MAP, COMMUNITY NUMBER 120286, PANEL NUMBER 0167, SUFFIX J, MAP NUMBER 12111C0167J, EFFECTIVE FEBRUARY 16, 2012.

**BUILDING HEIGHT:**

	ALLOWABLE	PROPOSED
	65'	23'-10" (TOP OF PARAPET) 33'-4" (TOP OF TOWER)

**LANDSCAPE BUFFERS:**

	REQUIRED	PROVIDED
NORTH	10'	10.0' - 29.0'
EAST	N/A	N/A
SOUTH	10'	25.0' - 50.9'
WEST	10'	14.0' - 27.0'

**PARKING REQUIREMENTS:**  
 CONVENIENCE STORE (6,119 SF) WITH GAS SALES: (5 SPACES / 1000 S.F.) = 30 SPACES  
 TOTAL REQUIRED: 30 SPACES  
 TOTAL PROVIDED PARKING SPACES: 71 SPACES  
 HANDICAP PARKING STALL: 3 (20'X12')  
 STANDARD PARKING STALL: 67 (20'X10', 18'X10')  
 SPECIALTY PARKING STALL: 1 (20'X11')

**PROPOSED ELECTRIC:** FPUA ELECTRIC  
**PROPOSED WATER:** FPUA WATER  
**PROPOSED SEWER:** FPUA SEWER LIFT STATION  
**STORMWATER:** ON-SITE STORMWATER COLLECTION FACILITY  
**REQUIRED PERMITS:** CITY OF FORT PIERCE: SITE PLAN APPROVAL, LAND CLEARING PERMIT, ST. LUCIE COUNTY: RIGHT-WAY PERMIT, FORT PIERCE UTILITIES AUTHORITY: WATER / SEWER PERMIT, S.F.W.A.D.: ENVIRONMENTAL RESOURCE GENERAL PERMIT, F.D.O.T.: DRIVEWAY CONNECTION PERMIT, DRAINAGE CONNECTION PERMIT, F.D.E.P.: NPDES NOTICE OF INTENT, WASTEWATER COLLECTION PERMIT

**SITE NOTES:**

- ALL DIMENSIONS SHOWN ARE SHOWN AT FACE OF CURB, UNLESS OTHERWISE NOTED. B/C INDICATES DIMENSION IS TO BACK OF CURB.
- ALL RADII DIMENSIONS ARE 3' TO FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," LATEST EDITION, FDOT DESIGN STANDARDS LATEST EDITIONS AND THE CITY OF FORT PIERCE LAND DEVELOPMENT CODE.
- FREE STANDING SIGNAGE SHOWN ONLY FOR REPRESENTATIONAL PURPOSES, AND SHALL BE PERMITTED SEPARATELY.
- PROPOSED DUMPSTER ENCLOSURE WILL HAVE A COMPATIBLE FINISH AND COLOR WITH THE RESPECTIVE STRUCTURE.
- ALL IMPACTED RIGHT-OF-WAY AREAS SHALL BE SODDED.
- THE PROPOSED CONVENIENCE STORE WITH GAS SALES IS A 24 HOUR OPERATION.

**ADA ACCESSIBILITY NOTES:**

- ALL HANDICAPPED PARKING SPACES AND ACCESS AISLES ADJACENT TO THE HANDICAP PARKING SPACES SHALL HAVE A MAXIMUM OF 2% SLOPE IN ALL DIRECTIONS (THIS INCLUDES RUNNING SLOPE AND CROSS SLOPE).
- AN ACCESSIBLE ROUTE FROM THE PUBLIC STREET OR SIDEWALK TO ALL BUILDING ENTRANCES MUST BE PROVIDED. THIS ACCESSIBLE ROUTE SHALL BE A MINIMUM OF 60" WIDE. THE RUNNING SLOPE OF AN ACCESSIBLE ROUTE SHALL NOT EXCEED 5% AND THE CROSS SLOPE SHALL NOT EXCEED 2%.
- SLOPES EXCEEDING 5% BUT LESS THAN 8% WILL REQUIRE A RAMP AND MUST CONFORM TO THE REQUIREMENTS FOR RAMP DESIGN (HANDRAILS, CURBS, LANDINGS). NO RAMP SHALL EXCEED AN 8% RUNNING SLOPE OR 2% CROSS SLOPE.
- IN THE CASE THAT A NEW SIDEWALK WILL BE CONSTRUCTED IN THE RW OF A SITE THE RUNNING SLOPE OF THE SIDEWALK SHALL NOT EXCEED 5% AND THE CROSS SLOPE SHALL NOT EXCEED 2%. THIS STANDARD APPLIES TO CROSS WALKS IN THE DRIVEWAY AS WELL AND WILL REQUIRE SPECIAL ATTENTION DURING STAKING TO MAKE SURE THE 2% CROSS SLOPE IS MET IN THE CROSS WALK.
- IT WILL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO ENSURE THAT THE HANDICAP PARKING SPACES, ACCESSIBLE ROUTES, AND SIDEWALKS/CROSSWALKS ARE CONSTRUCTED TO MEET ADA REQUIREMENTS.
- ANY REQUIREMENTS LISTED ABOVE THAT CAN NOT BE MET SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION IMMEDIATELY. ANYTHING NOT BUILT TO THE ABOVE STANDARDS WILL REQUIRE REMOVAL AND REPLACEMENT OF THE NON COMPLIANT AREAS AT THE GENERAL CONTRACTORS COST.

**BOHLER ENGINEERING**  
 SITE PLANNING CONSULTING SERVICES  
 LAND SURVEYING PROGRAM MANAGEMENT LANDSCAPE ARCHITECTURE  
 SUSTAINABLE DESIGN PERMITTING SERVICES TRANSPORTATION SERVICES  
 \* BALTIMORE, MD \* NEW JERSEY \* PHILADELPHIA, PA  
 \* BOSTON, MA \* NEW YORK, NY \* RICHMOND, VA  
 \* CHICAGO, IL \* FORT LAUDERDALE, FL \* MIAMI, FL  
 \* HOUSTON, TX \* JACKSONVILLE, FL \* TAMPA, FL  
 \* KANSAS CITY, MO \* NORTHERN VIRGINIA  
 \* LEHIGH VALLEY, PA \* RALEIGH, NC  
 \* LINCOLN, NE \* LOS ANGELES, CA  
 \* MEMPHIS, TN \* NORTON, VA  
 \* OKLAHOMA CITY, OK \* SOUTH FLORIDA

**REVISIONS**

REV	DATE	COMMENT	BY

**811** KNOW WHAT'S BELOW ALWAYS CALL 811 BEFORE YOU DIG  
 It's fast. It's free. It's the law.  
 www.callsunshine.com

**NOT APPROVED FOR CONSTRUCTION**

PROJECT No.: FLB15009  
 DRAWN BY: SMT  
 DATE: 06/24/16  
 SCALE: 1" = 30'  
 CAD I.D.: SP0

**SR 70 AND JENKINS FOR WAWA**  
 LOCATION OF SITE SEC SR 70 & S JENKINS RD FORT PIERCE, FLORIDA

**BOHLER ENGINEERING**  
 2255 GLADES ROAD, SUITE 305E BOCA RATON, FLORIDA 33431  
 Phone: (561) 571-0280  
 Fax: (561) 571-0281  
 FLORIDA BUSINESS CERT. OF AUTH. NO. 30780  
 LANDSCAPE ARCHITECT BUSINESS LIC. NO. LC2605055

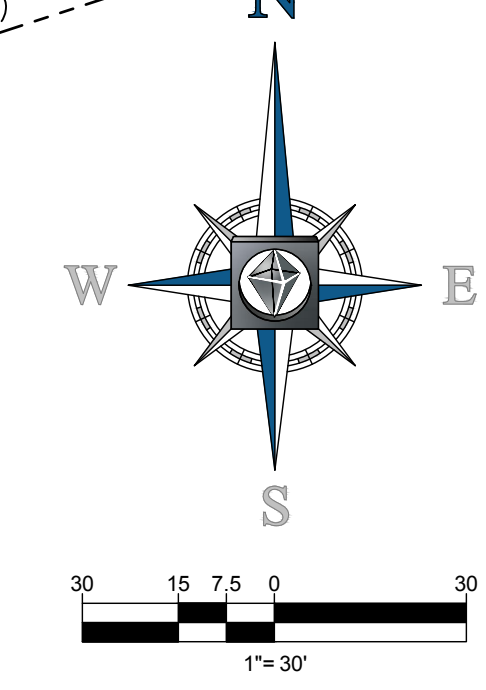
GEORGE BALABAN  
 LICENSE No. 74543  
 STATE OF FLORIDA  
 PROFESSIONAL ENGINEER  
 07/05/16

SHEET TITLE: **SITE LAYOUT PLAN**  
 SHEET NUMBER: **C-5**



### DEMOLITION NOTES:

- BOHLER ENGINEERING FL, LLC IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION. CONTRACTOR IS TO PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL THE OSHA REQUIREMENTS, TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
- ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AS WELL AS ALL FEDERAL, STATE AND LOCAL REGULATIONS. ANY DISCREPANCIES OR DEVIATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR TO BOHLER ENGINEERING FL, LLC IN WRITING FOR RESOLUTION PRIOR TO INITIATION OF SITE ACTIVITY.
- PRIOR TO STARTING ANY DEMOLITION CONTRACTOR IS RESPONSIBLE FOR TO:
  - THE CONTRACTOR SHALL OBTAIN A SUNSHINE STATE ONE CALL LOCATION CERTIFICATION PRIOR TO ANY EXCAVATION OR DEMOLITION. THE NUMBER IS 1-800-432-4770.
  - ENSURING COPIES OF ALL PERMITS AND APPROVALS MUST BE MAINTAINED ON SITE AND AVAILABLE FOR REVIEW.
  - INSTALLING THE REQUIRED SOIL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO SITE DISTURBANCE.
  - LOCATE/APP ALL UTILITIES AND SERVICES, INCLUDING BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE LIMITS OF DISTURBANCE.
  - PROTECTING AND MAINTAINING IN OPERATION, ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED DURING ALL DEMOLITION ACTIVITIES.
  - FAMILIARIZING THE SITES WITH THE APPLICABLE UTILITY SERVICE PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY DEMOLITION REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY REQUIREMENTS.
  - COORDINATION WITH UTILITY COMPANIES REGARDING WORKING "OFF-PEAK" HOURS OR ON WEEKENDS AS MAY BE REQUIRED TO MINIMIZE THE IMPACT ON THE AFFECTED PARTIES.
  - A COMPLETE INSPECTION FOR CONTAMINANTS BY FEDERAL, STATE, AND LOCAL REGULATIONS. ALL ENVIRONMENTAL WORK INCLUDING HAZARDOUS MATERIAL, SOILS, ASBESTOS, OR OTHER REFERENCED OR IMPLIED HEREIN IS THE SOLE RESPONSIBILITY OF THE OWNER'S ENVIRONMENTAL CONSULTANT.
- THE CONTRACTOR SHALL PROVIDE ALL THE "MEANS AND METHODS" NECESSARY TO PREVENT MOVEMENT, SETTLEMENT, OR COLLAPSE OF EXISTING STRUCTURES, AND ANY OTHER IMPROVEMENTS THAT ARE REMAINING ON OR OFF SITE. THE DEMOLITION CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS OF DAMAGE TO ALL ITEMS THAT ARE TO REMAIN AS A RESULT OF HIS ACTIVITIES. ALL REPAIRS SHALL USE NEW MATERIAL. THE REPAIRS SHALL RESTORE THE ITEM TO THE PRE-DEMOLITION CONDITION.
- IN THE ABSENCE OF SPECIFICATIONS, THE CONTRACTOR SHALL PERFORM EARTH MOVEMENT ACTIVITIES, DEMOLITION AND REMOVAL OF ALL FOUNDATION WALLS, FOOTINGS, AND OTHER MATERIALS WITHIN THE LIMITS OF DISTURBANCE IN ACCORDANCE WITH DIRECTION BY OWNERS STRUCTURAL OR GEOTECHNICAL ENGINEER.
- EXPLOSIVES SHALL NOT BE USED WITHOUT PRIOR WRITTEN CONSENT OF BOTH THE OWNER AND APPLICABLE GOVERNMENTAL AUTHORITIES. ALL THE REQUIRED PERMITS AND EXPLOSIVE CONTROL MEASURES THAT ARE REQUIRED BY THE FEDERAL, STATE, AND LOCAL GOVERNMENTS SHALL BE IN PLACE PRIOR TO STARTING AN EXPLOSIVE PROGRAM. THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL INSPECTION AND SEISMIC VIBRATION TESTING THAT IS REQUIRED TO MONITOR THE EFFECTS ON ALL LOCAL STRUCTURES.
- CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND GENERALLY ACCEPTED SAFE PRACTICES IN CONFORMANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL" AS WELL AS FEDERAL, STATE, AND LOCAL REGULATIONS WHEN DEMOLITION RELATED ACTIVITIES IMPACT ROADWAYS OR ROADWAY RIGHTS-OF-WAY.
- CONDUCT DEMOLITION ACTIVITIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, SIDEWALKS, WALKWAYS, AND OTHER ADJACENT FACILITIES. STREET CLOSURE PERMITS MUST BE RECEIVED FROM THE APPROPRIATE GOVERNMENTAL AUTHORITY.
- DEMOLITION ACTIVITIES AND EQUIPMENT SHALL NOT USE AREAS OUTSIDE THE DEFINED PROPERTY LINE WITHOUT WRITTEN PERMISSION OF THE OWNER, AND/OR APPROPRIATE GOVERNMENT AGENCY.
- USE DUST CONTROL MEASURES TO LIMIT AIRBORNE DUST AND DIRT RISING AND SCATTERING IN THE AIR IN ACCORDANCE WITH FEDERAL, STATE, AND/OR LOCAL STANDARDS. AFTER THE DEMOLITION IS COMPLETE, ADJACENT STRUCTURES AND IMPROVEMENTS SHALL BE CLEANED OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL ADJACENT AREAS TO THEIR "PRE-DEMOLITION" CONDITION.
- CONTRACTOR IS RESPONSIBLE TO SAFEGUARD SITE AS NECESSARY TO PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE ENTRY OF UNAUTHORIZED PERSONS AT ANY TIME.
- THIS DEMOLITION PLAN IS INTENDED TO IDENTIFY THOSE EXISTING ITEMS/CONDITIONS WHICH ARE TO BE REMOVED. IT IS NOT INTENDED TO PROVIDE DIRECTION OTHER THAN THAT ALL METHODS AND MEANS ARE TO BE IN ACCORDANCE WITH STATE, FEDERAL, LOCAL, AND JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL OSHA AND OTHER SAFETY PRECAUTIONS NECESSARY TO PROVIDE A SAFE WORK SITE.
- DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL DEMOLITION WASTES AND DEBRIS (SOLID WASTES) SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL CITY, COUNTY, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.
- CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL EXISTING SITE IMPROVEMENTS AND UTILITIES. ALL DISCREPANCIES SHALL BE IDENTIFIED TO THE ENGINEER IN WRITING.
- ALL EXISTING UTILITIES BEING REMOVED SHALL BE DONE SO BY THE CONTRACTOR IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.



### LEGEND:

	EXIST. UTILITY POLE TO BE REMOVED
	EXIST. UTILITY POLE TO REMAIN
	EXIST. OVERHEAD LINE TO BE REMOVED
	EXIST. OVERHEAD LINE TO REMAIN
	EXIST. FENCE TO BE REMOVED
	EXIST. FENCE TO REMAIN
	EXIST. BUILDINGS TO BE REMOVED
	LIMITS OF DISTURBANCE
	EXIST. STORM STRUCTURE TO BE REMOVED
	TO BE REMOVED
	TO REMAIN
	EXIST. LP TO BE REMOVED
	EXIST. BACKFLOW PREVENTION DEVICE TO BE REMOVED
	EXIST. ASPHALT TO BE REMOVED
	EXIST. CONCRETE TO BE REMOVED
	EXIST. GRAVEL TO BE REMOVED

### DATUM NOTE:

CONTROL BENCHMARK: SAINT LUCIE COUNTY BENCHMARK DESCRIBED AS SAINT LUCIE COUNTY 2004 VERTICAL CONTROL DISK IN CONCRETE LOCATED AT THE NORTHEAST CORNER OF JENKINS AND OKEECHOBEE ROADS, 47.9' SOUTHWEST OF A TYPICAL CONCRETE POWER POLE AT THE INTERSECTION OF AN 8" CONCRETE SIDEWALK, 14'2" NORTHWEST OF THE SOUTHWEST CORNER OF THE PARKING LOT PARALLEL TO OKEECHOBEE ROAD.

ELEVATION: 19.13' (NORTH AMERICAN VERTICAL DATUM 1988)

ON-SITE TBM #1: STATION IS DESCRIBED AS A LB-7652 NAIL AND DISK SET ON TOP OF CONCRETE SIDEWALK LOCATED 105.04' NORTH AND 31.69' EAST OF THE NORTHWEST CORNER OF SUBJECT PARCEL. ELEVATION: 20.50' (NORTH AMERICAN VERTICAL DATUM 1988)

ON-SITE TBM #2: STATION IS DESCRIBED AS A LB-7652 NAIL AND DISK SET ON TOP OF GRATE INLET LOCATED 33.19' NORTH AND 299.81' WEST OF THE NORTHEAST CORNER OF SUBJECT PARCEL. ELEVATION: 17.92' (NORTH AMERICAN VERTICAL DATUM 1988)

## BOHLER ENGINEERING

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 FAX: (561) 571-0281  
 FLORIDA BUSINESS CERT. OF AUTH. NO. 38780  
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### REVISIONS

REV	DATE	COMMENT	BY

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CHECKED BY:	GB
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SCALE:	N.T.S.
CAD I.D.:	DPD

### SR 70 AND JENKINS FOR WAWA

LOCATION OF SITE  
 SEC SR 70 & S JENKINS RD  
 FORT PIERCE, FLORIDA

## BOHLER ENGINEERING

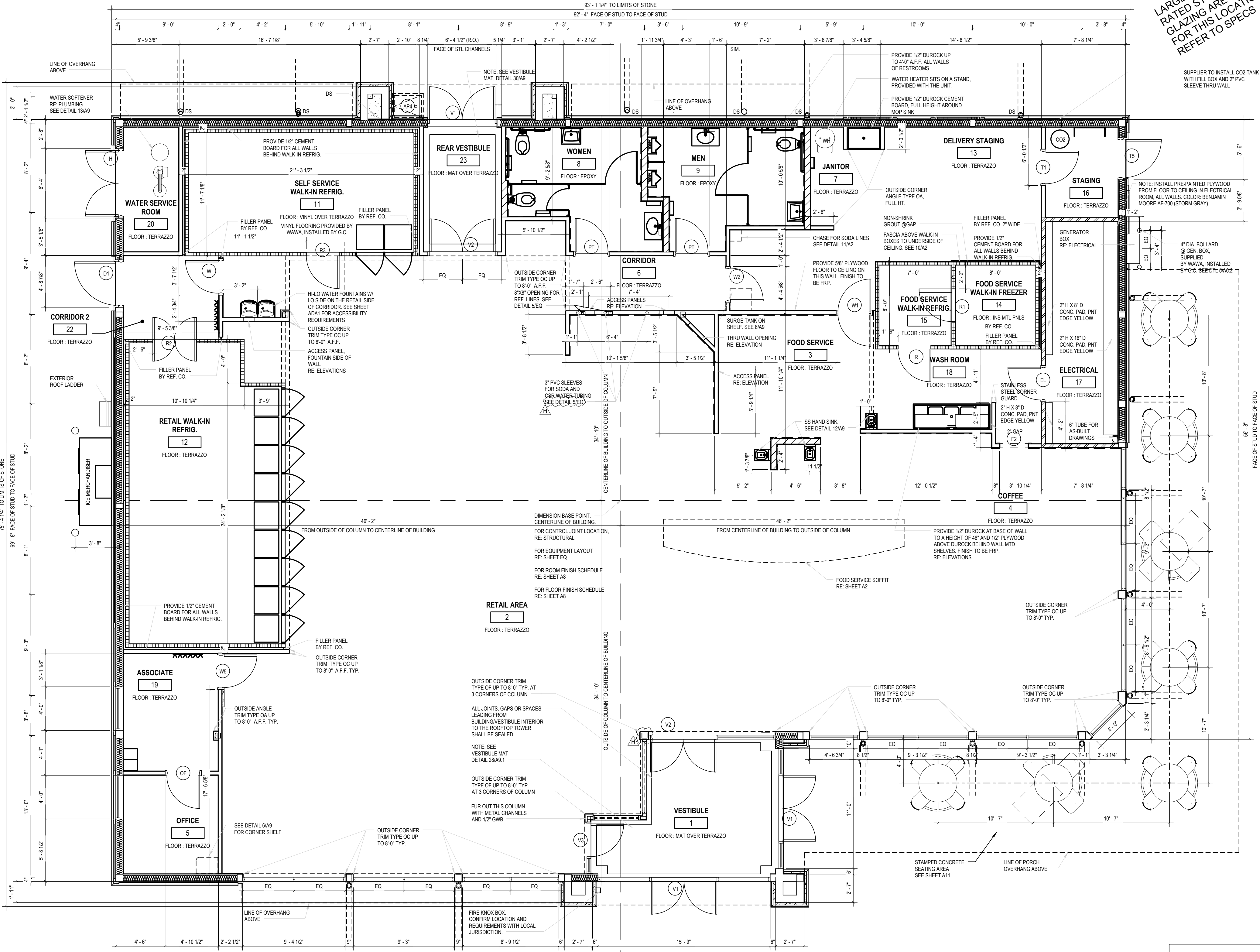
2255 GLADES ROAD, SUITE 305E  
 BOCA RATON, FLORIDA 33431  
 Phone: (561) 571-0280  
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 FLORIDA BUSINESS CERT. OF AUTH. NO. 38780  
 LANDSCAPE ARCHITECT BUSINESS LIC. NO. LC2005005

STATE OF FLORIDA  
 PROFESSIONAL ENGINEER  
 No. 74543  
 07/05/16

SHEET TITLE: **DEMOLITION PLAN**

SHEET NUMBER: **C-3**

**1 FLOOR PLAN**  
3/16" = 1'-0"



**GENERAL NOTES**  
1. SOIL TREATMENT FOR TERMITE CONTROL SHALL BE PER SPECIFICATIONS  
2. REFER TO FLORIDA PRODUCT APPROVAL SCHEDULE ON COVER SHEET FOR APPLICABLE STOREFRONT SPEC. IMPACT GLASS MAY BE REQUIRED.  
3.

**LEGEND**

	8" MTL. STUDS W/ R-30 BATT INSUL. AND EXT. VENEER		DOOR IDENTIFICATION. SEE DOOR SCHEDULE
	INSUL. MTL. PANELS BY REF. CO.		BLOCKING IDENTIFICATION. SEE EQUIPMENT BLOCKING NOTES
	3-5/8" MTL. STUDS @ 16" O.C. W/ 1/2" GWB		WATER HEATER
	3-5/8" MTL. STUDS @ 16" O.C. W/ 1/2" GWB AND 5/8" PLYWOOD ON ELECTRICAL SIDE		FIRE EXTINGUISHER. SEE DETAIL 4/A9. PROVIDE BLOCKING AS REQUIRED & COORDINATE FINAL LOCATION W/ FIRE MARSHALL
	6" MTL. STUDS @ 16" O.C. W/ 1/2" GWB		NOTE: ALL WALLS TO GO TO UNDERSIDE OF ROOF DECK, UNLESS OTHERWISE NOTED. PROVIDE SLOTTED TRACK AT CEILING.
	CONCRETE BLOCK (CMU)		

**EQUIPMENT BLOCKING NOTES**

#	ITEM	BLK MATL	LOCATION AND REMARKS
1	SHELVING @ 3 COMP. SINK	1/2" PLYWOOD	IN LIEU OF GWB. SEE ELEVATION
2	MOP HOLDER	2" X 6"	5'-0"
3	HAND DRYER	2" X 6"	3'-3" AND 4'-0"
5	HAND WASH SINK	2" X 6"	3'-5"
6	URINAL	AS PER MANUF.	
7	GRAB BARS	2" X 6"	2'-10" 1/2" & VERTICAL
8	POWER WASH SHELF	2" X 6"	6'-9" 1/2"
9	TRASH BAG HOLDER	2" X 6"	2'-0" AND 4'-0"
10	TOILET PARTITIONS	AS PER MANUF.	
11	COAT HOOKS	2" X 12"	4'-0"
12	OFFICE SECURITY SHELF	2" X 10"	4'-9" SEE DETAIL ON 6/A9
13	LOCKERS	2" X 10"	5'-6"
14	VEEDER ROOT	2" X 12"	5'-5"
15	SURGE TANK SHELF	2" X 6"	7'-8"
17	WATER FOUNTAIN LOW	2" X 6"	2'-6"
18	WATER FOUNTAIN HIGH	2" X 6"	3'-0"
19	BEER COOLER SIGN	2" X 8"	11'-0" SEE SHEET G1.
20	DIGITAL SIGN (COFFEE)	2" X 8"	8'-0" SEE DETAIL 6/A9.1
22	FIRE EXTINGUISHER	2" X 6"	3'-6" SEE DETAIL 4/A9
23	BEER COOLER SIDE CABINETS	2" X 8"	10'-0" SEE SHEET G1.
25	CSR UNIT	5/8" PLYWD	IN LIEU OF GWB. FLR TO CLG
26	BEVERAGE AREA FASCIA	1/2" PLYWOOD	SHEATH WALLS FLOOR TO CEILING. SEE NOTE 4 AND DETAIL 13/G1 & 16/A9.1
31	DIGITAL SIGN (VERT)	2 X 8	8'-0" SEE DETAIL 7/A9.1

- NOTES:**
- PROVIDE ADDITIONAL BLOCKING AT OTHER LOCATIONS AS REQUIRED
  - ALL HEIGHTS CENTERED UNLESS NOTED OTHERWISE
  - PROVIDE BLOCKING FOR ALL CABINETS. 2X12, CENTERED ON CABINET TOP
  - PROVIDE 1/4" JAMES HARDIE "HARDIBACKER" CEMENT BOARD OVER 1/2" PLYWOOD AT SELF SERVE DRINK AREA. ATTACH CEMENT BOARD TO PLYWOOD ALONG THE WALL STUDS USING CORROSION RESISTANT SCREWS @ 8" TO 8 INCHES O.C. TAPE JOINTS BETWEEN PIECES OF BACKER BOARD WITH FIBERGLASS OR MESH TAPE. FILL SCREW HOLES IN THE BACKER BOARD WITH FILLER COMPOUND.

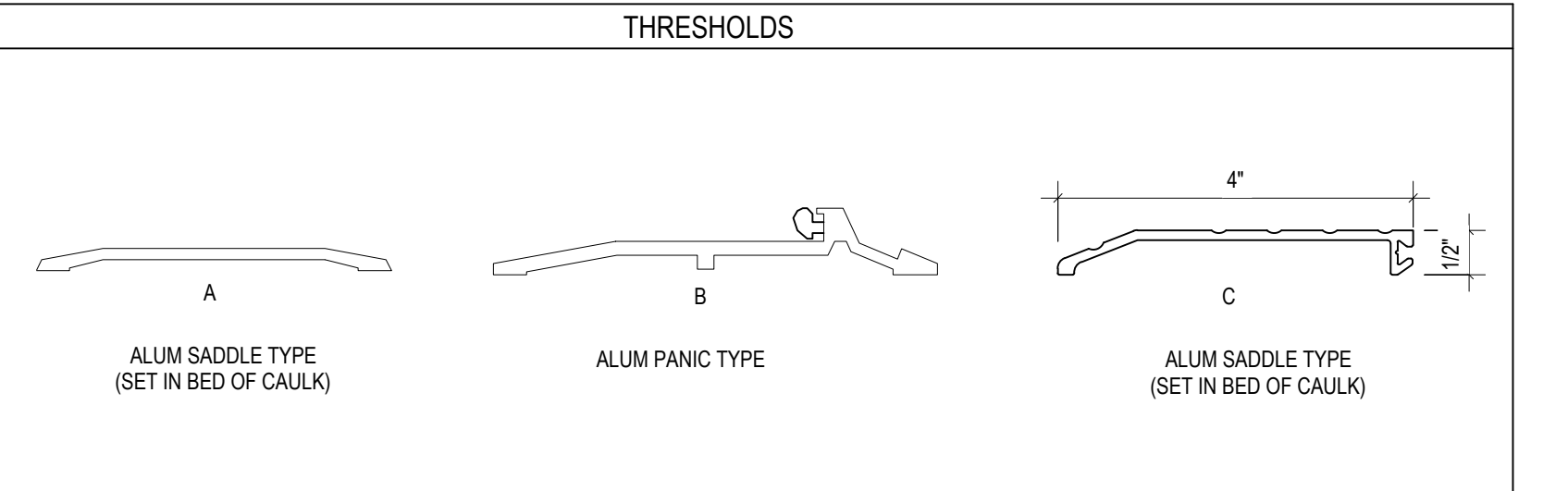
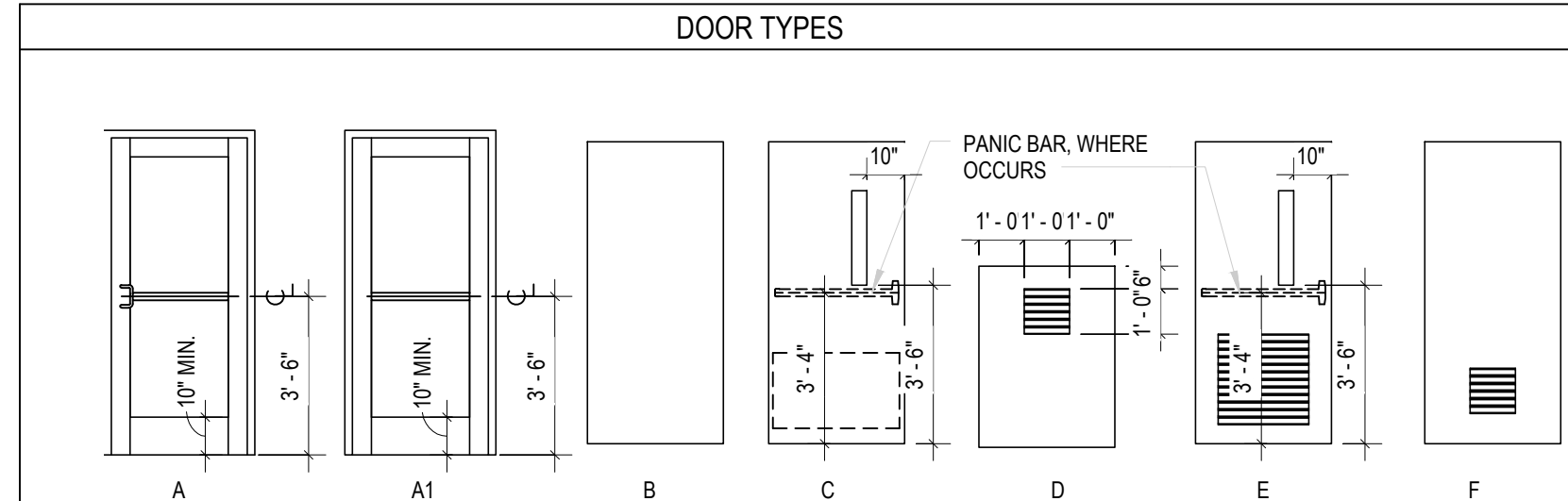
**DOOR NOTES**

- DOOR FRAMES:**  
ALL DOOR FRAMES, INTERIOR AND EXTERIOR TO BE WELDED. NO KNOCK-DOWN FRAMES. ADD WOOD BLOCKING AS REQUIRED.
- DOOR #S SHOWN IN DOOR SCHEDULE ARE TYPICAL FOR WAWA STORES. VERIFY QUANTITIES AND WHICH DOORS ARE REQUIRED AS INDICATED IN FLOOR PLAN.
  - SEE STOREFRONT MANUFACTURER SPECIFICATIONS FOR VESTIBULE DOORS. DOORS ARE TO BE KEYPAD ALIKE WITH THUMB LATCH AT EXTERIOR DOORS AND CLOSER AT ALL DOORS.
  - ALL EXTERIOR HOLLOW METAL DOORS SHALL BE INSULATED DOORS.
  - RUN CONTINUOUS BEAD OF CLEAR SILICONE CAULK AROUND ALL DOOR FRAMES AND ALL INTERIOR TILE CORNERS IN RESTROOMS, COFFEE AREA AND FOOD SERVICE AREA.
  - EXIT DOOR LOCKS, IF PROVIDED ON EGRESS DOORS, SHALL NOT REQUIRE THE USE OF A KEY. A TOOL OR SPECIAL KNOWLEDGE OR EFFORT FOR OPERATION FROM THE EGRESS SIDE IN ACCORDANCE TO SECTION 14.5.2.3 OF FPPC AND SECTION 1008.1.9 OF FBC UNLESS SPECIFICALLY ALLOWED BY CODE.
  - A LATCH OR OTHER FASTENING DEVICE ON AN EGRESS DOOR SHALL BE PROVIDED WITH A KNOB, HANDLE, PANIC BAR, OR OTHER SIMPLE TYPE OF RELEASING DEVICE HAVING AN OBVIOUS METHOD OF OPERATION UNDER ALL LIGHTING CONDITIONS. DOORS SHALL BE OPERABLE WITH NO MORE THAN ONE RELEASING OPERATION PER FPPC 14.5.2.10
  - REFER TO HARDWARE SCHEDULE IN THE OPERATION MANUAL.
  - THRESHOLD 'A' NOT TO EXCEED 1/2" HIGH.
  - DOOR TO REQUIRE HOLE IN SLAB FOR BOLT.
  - ALL DOOR FRAMES, INTERIOR AND EXTERIOR SHOULD BE FULL WELDED. NO KNOCK-DOWN FRAMES. ADD BLOCKING AROUND FRAMES.
  - ALL LAMINATE GLASS NEEDS TO COMPLY WITH SAFETY GLAZING REQUIREMENTS IN SECTION 2406 OF FBCB.

**DOOR SCHEDULE**

#	WIDTH	HEIGHT	THICKNESS	TYPE	DOOR MATERIAL	FRAME TYPE	THRESHOLD	HARDWARE SET	COMMENTS
AP4	2'-6"	2'-0"	-	-	-	STL	-	-	NYSTROM HS
D1	4'-0"	7'-0"	0'-1.334"	C	HM	HM	A	HW 10	4" X 25" LAMINATE GLASS VISION PANEL
EL	3'-0"	6'-8"	0'-1.334"	E	HM	HM	-	HW 13	4" X 25" LAMINATE GLASS VISION PANEL 24" X 24" LOUVER
F2	2'-8"	6'-8"	-	-	-	HM	-	-	CASE OPENING. SEE DETAIL 5/A9
H	3'-6"	7'-0"	0'-1.334"	B/F	HM	HM	B	HW 8	ONE LEAF W/ 12" X 18" LOUVER (DOUBLE DOOR). SEE DS & 28/A1 FOR HEAD & JAMB
OF	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 12	4" X 25" ONE WAY VISION PANEL. UNDERCUT 1"
PT	3'-0"	6'-8"	0'-1.334"	B	HM	HM	-	HW 7	UNDERCUT DOOR 1"
R	3'-0"	6'-9"	0'-2"	REF	-	-	-	-	BY REFRIGERATION COMPANY
R1	3'-0"	6'-9"	0'-4"	REF	-	-	-	-	BY REFRIGERATION COMPANY
R2	4'-0"	6'-9"	0'-2"	REF	-	-	-	-	BY REFRIGERATION COMPANY (DOUBLE DOORS)
R3	3'-0"	6'-9"	0'-2"	REF	-	-	-	-	BY REFRIGERATION COMPANY (GLAZED)
T1	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 11	4" X 25" LAMINATE GLASS VISION PANEL
T5	3'-6"	7'-0"	0'-1.334"	C	HM	HM	B	HW 5.3	4" X 25" LAMINATE GLASS VISION PANEL
TH	3'-0"	4'-0"	0'-1.334"	D	-	HM	-	HW 19	TOWER ACCESS: NYSTROM XTM SERIES INSULATED ACCESS PANEL WITH 12" X 12" LOUVER. SEE 3/A6.
V1	3'-0"	7'-0"	0'-1.334"	A	ALUM	ALUM	C	-	NO HOLD OPENS (DOUBLE DOOR)
V2	3'-0"	7'-0"	0'-1.334"	A	ALUM	ALUM	C	-	NO HOLD OPENS (DOUBLE DOOR)
V3	3'-0"	7'-0"	0'-1.334"	A1	ALUM	ALUM	C	-	NO HOLD OPENS, PUSH ONLY
W	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 3	4" X 25" LAMINATE GLASS VISION PANEL
W1	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 3.1	DOUBLE ACTING W/ 4" X 25" GLASS VISION PANEL
W2	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 3.2	4" X 25" ONE WAY VISION GLASS / THUMB LATCH
W5	3'-0"	6'-8"	0'-1.334"	C	HM	HM	-	HW 3.5	ONE WAY 4" X 25" VISION PANEL. PROVIDE 8" WIDE JAMB W/ EXTENSION ON THE RETAIL SIDE

- NOTES:**
- PROVIDE WOOD BLOCKING AT JAMBS AND HEAD OF ALL INTERIOR DOORS.
  - ALL LAMINATE GLASS NEED TO COMPLY WITH SAFETY GLAZING REQUIREMENTS IN SECTION 2406 OF FBCB.



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**CLIENT NAME**  
WAWA  
260 WEST BALTIMORE PIKE  
WAWA, PENNSYLVANIA 19063

**PROJECT NAME**  
WAWA F85 FB  
Okeechobee Rd. & Jenkins Rd.  
Fl. Pierce, FL

**SHEET TITLE**  
FLOOR PLAN

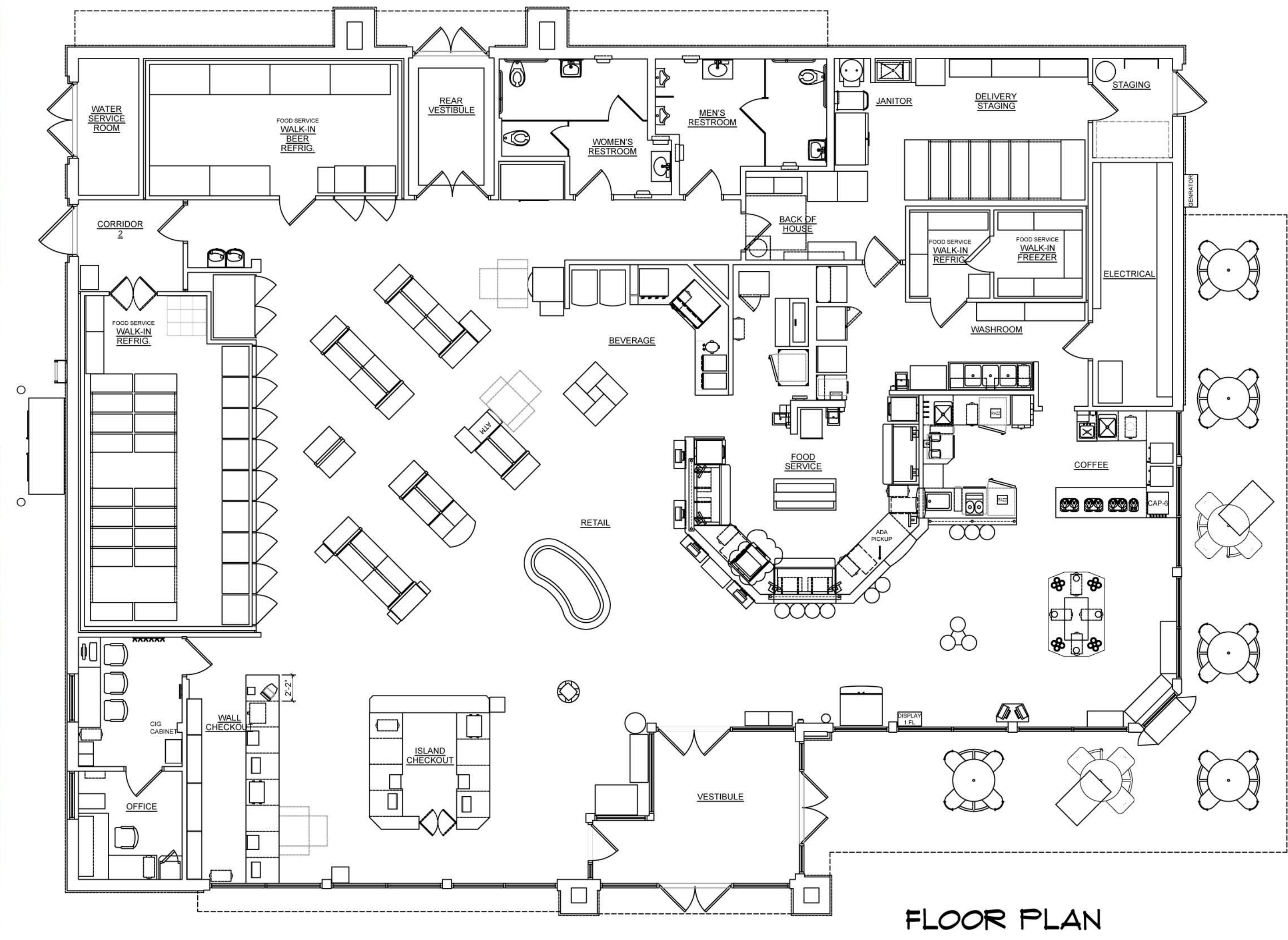
**STATE OF FLORIDA**  
RICK ZANE SMITH  
REGISTERED ARCHITECT  
AR0010527

**Revision Schedule**

No.	DESCRIPTION	Date
1	ZONING	06/21/2016

**PROJECT NO.** 2100290  
**DATE** 04/20/2016  
**DRAWN**  
**CHECKED**

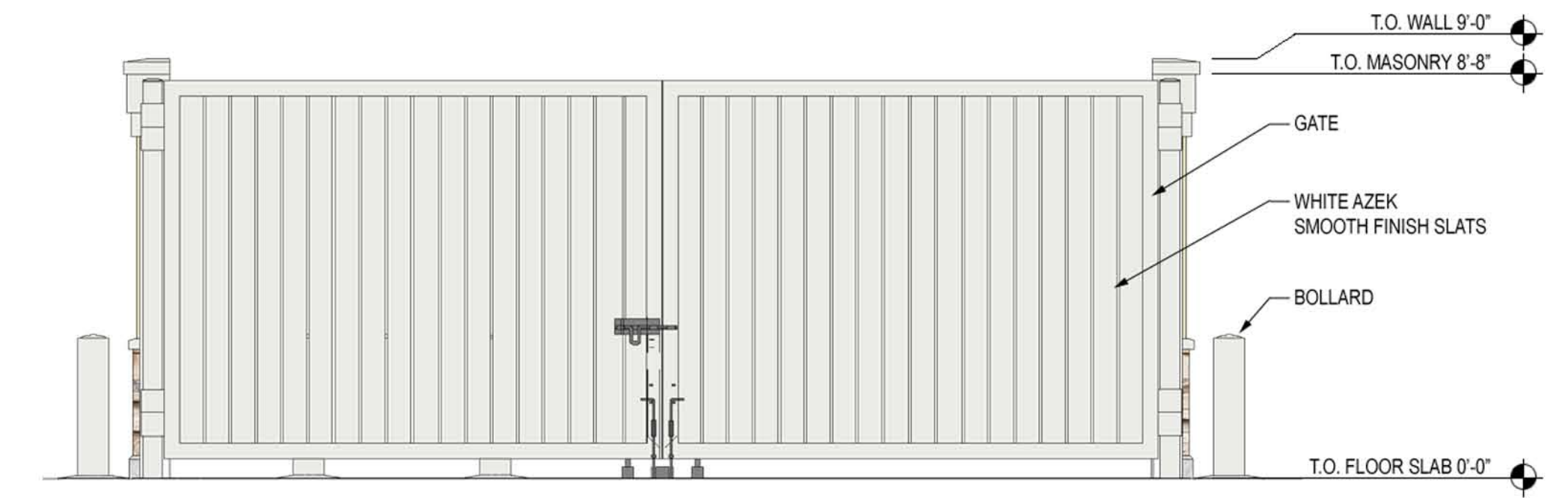
**A1**



Metal Door Benjamin Moore White Diamond BM 2121-60	Metal Canopy Atas #17 Brite Red
Trim James Hardie Arctic White JH10-20	Stucco Senergy Meringue #3085
Fiber Cement Siding James Hardie Harris Cream	Ice Storage Benjamin Moore Natural Wicker OC - 1
Metal Roof Atas #23 23 Coppertone	Exterior Stone Cultured Stone Southwest Blend



NOTE:  
SIGNAGE IS SHOWN FOR REFERENCE ONLY  
SEPARATE PERMIT REQUIRED



FRONT ELEVATION

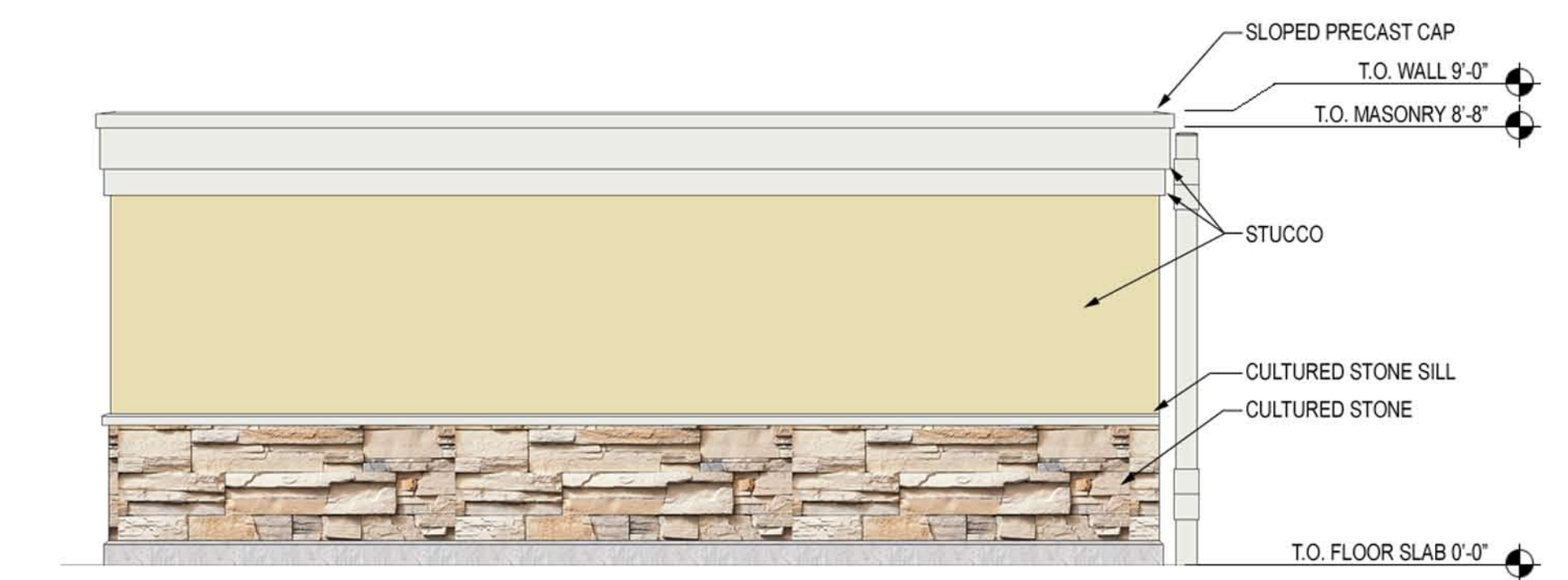


RIGHT ELEVATION

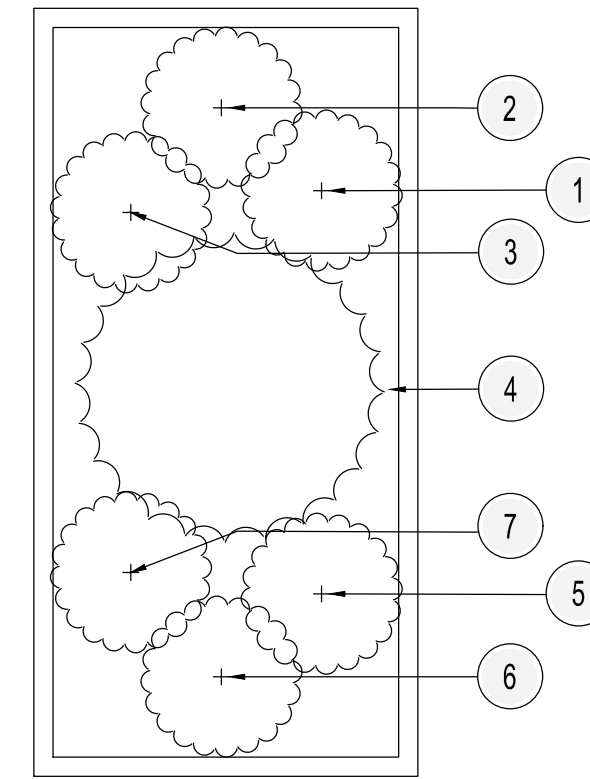
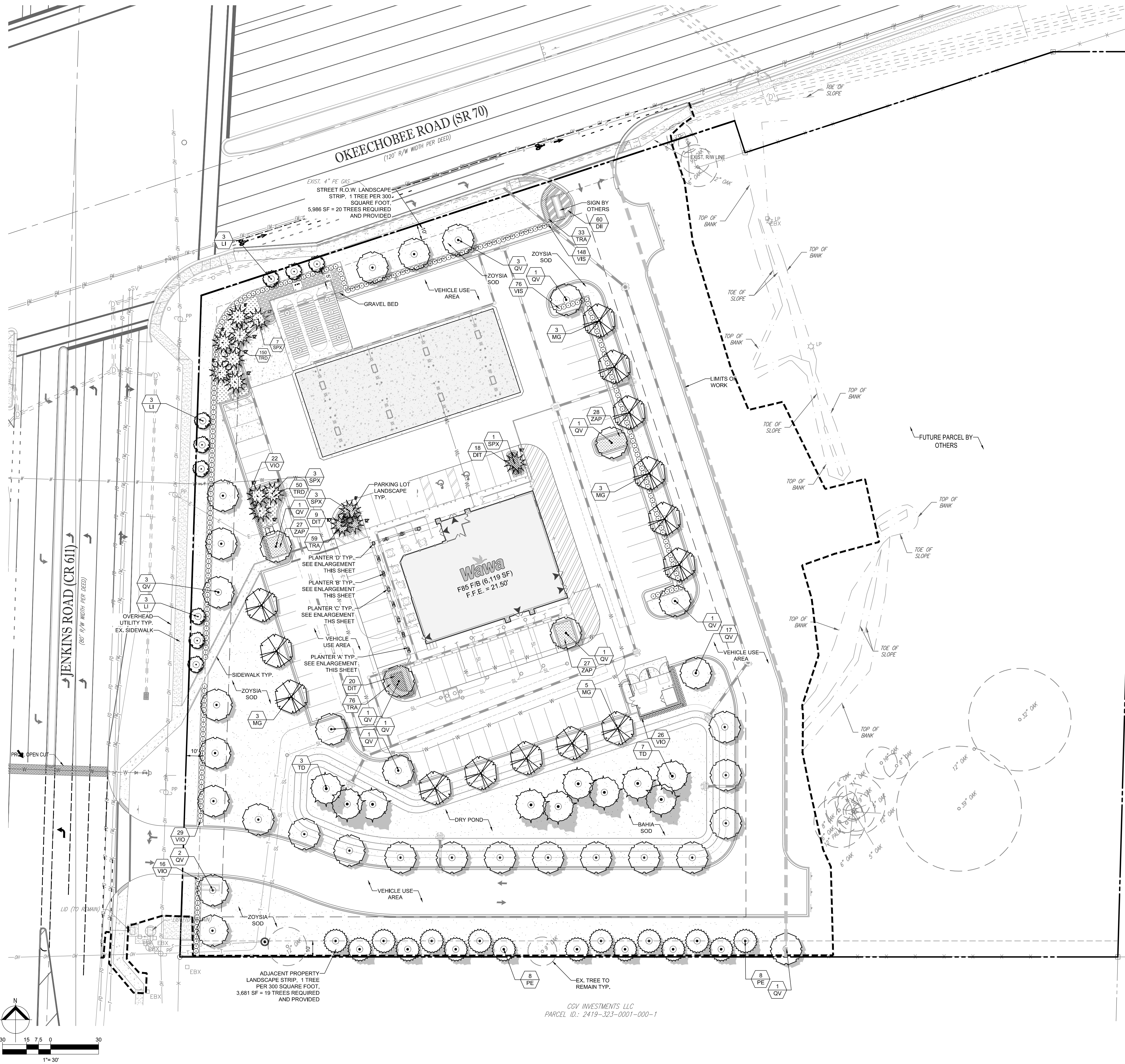


REAR ELEVATION

Metal Door Benjamin Moore White Diamond BM 2121-60	Trim James Hardie Arctic White JH10-20	Stucco Senergy Meringue #3085	Exterior Stone Cultured Stone Southwest Blend
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LEFT ELEVATION



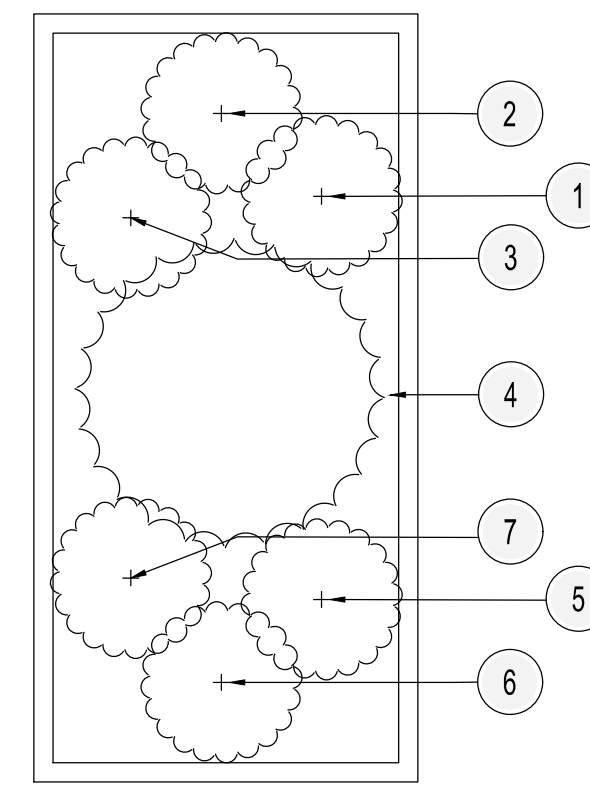
1 PLANTER 'A' ENLARGEMENT  
SCALE: NTS

- SOUTH FLORIDA WARM SEASON PALETTE**
1. RED SALVIA
  2. PINK PORTULACA
  3. YELLOW ZINNIA
  4. DWARF WHITE FOUNTAIN GRASS
  5. WHITE PENTAS
  6. ORANGE GAZANIA
  7. PINK PROTULACA

- SOUTH FLORIDA COOL SEASON PALETTE**
1. POT MARGOLD
  2. PINK PETUNIA
  3. WHITE ALYSSUM
  4. DWARF WHITE FOUNTAIN GRASS
  5. ORANGE ZINNIA
  6. RED PETUNIA
  7. PINK VERBENA

NOTE: ALL PLANT MATERIAL TO BE ON GALLON POT, SEE MATERIALS SCHEDULE FOR MULCH.

**POT SPECIFICATION**  
 NAME: BOULEVARD SERIES PLANTER  
 MANUFACTURER: ALLIED MOLDED PRODUCTS, LLC.  
 MODEL NO.: 1RECLP-482424  
 SIZE: 48" L X 24" W X 24" HT  
 COLOR: BENJAMIN MOOR #OC-1 NATURAL WICKER



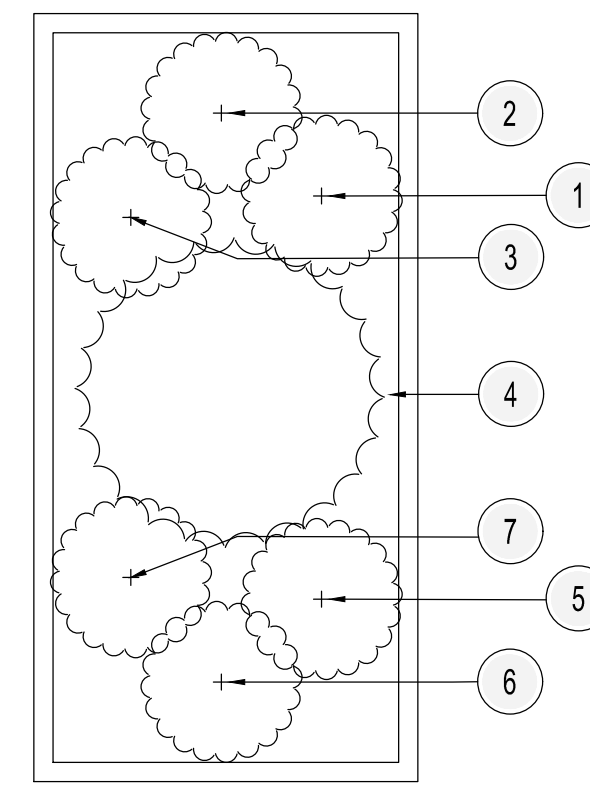
2 PLANTER 'B' ENLARGEMENT  
SCALE: NTS

- SOUTH FLORIDA WARM SEASON PALETTE**
1. RED GAZANIA
  2. YELLOW PURSLANE
  3. ORANGE PLUME CELOSIA
  4. DWARF WHITE FOUNTAIN GRASS
  5. WHITE SALVIA
  6. PINK GAZANIA
  7. RED DIANTHUS

- SOUTH FLORIDA COOL SEASON PALETTE**
1. RED SALVIA
  2. WHITE PETUNIA
  3. YELLOW ALYSSUM
  4. DWARF WHITE FOUNTAIN GRASS
  5. BLUE DAZE
  6. LAVENDER VERBENA
  7. YELLOW TALL MARGOLD

NOTE: ALL PLANT MATERIAL TO BE ON GALLON POT, SEE MATERIALS SCHEDULE FOR MULCH.

**POT SPECIFICATION**  
 NAME: BOULEVARD SERIES PLANTER  
 MANUFACTURER: ALLIED MOLDED PRODUCTS, LLC.  
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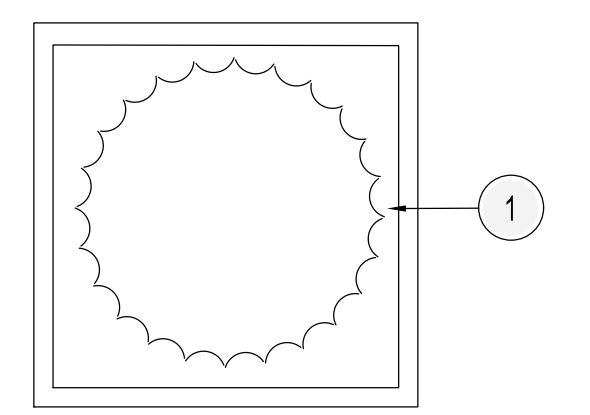
3 PLANTER 'C' ENLARGEMENT  
SCALE: NTS

- SOUTH FLORIDA WARM SEASON PALETTE**
1. TALL PINK ZINNIA
  2. ORANGE PLUME CELOSIA
  3. YELLOW DANIEL DAISY
  4. DWARF WHITE FOUNTAIN GRASS
  5. RED PENTAS
  6. PINK PORTULACA
  7. WHITE BEGONIA

- SOUTH FLORIDA COOL SEASON PALETTE**
1. BLUE DAZE
  2. RED PETUNIA
  3. YELLOW ALYSSUM
  4. DWARF WHITE FOUNTAIN GRASS
  5. PINK DIANTHUS
  6. WHITE VERBENA
  7. YELLOW PLUME CELOSIA

NOTE: ALL PLANT MATERIAL TO BE ON GALLON POT, SEE MATERIALS SCHEDULE FOR MULCH.

**POT SPECIFICATION**  
 NAME: BOULEVARD SERIES PLANTER  
 MANUFACTURER: ALLIED MOLDED PRODUCTS, LLC.  
 MODEL NO.: 1RECLP-482424  
 SIZE: 48" L X 24" W X 24" HT  
 COLOR: BENJAMIN MOOR #OC-1 NATURAL WICKER



4 PLANTER 'C' ENLARGEMENT  
SCALE: NTS

- SOUTH FLORIDA WARM & COOL SEASON PALETTE**
1. DWARF WHITE FOUNTAIN GRASS

NOTE: ALL PLANT MATERIAL TO BE ON GALLON POT, SEE MATERIALS SCHEDULE FOR MULCH.

**POT SPECIFICATION**  
 NAME: BOULEVARD SERIES PLANTER  
 MANUFACTURER: ALLIED MOLDED PRODUCTS, LLC.  
 MODEL NO.: 1SLP-2425  
 SIZE: 24" X 24"  
 COLOR: BENJAMIN MOOR #OC-1 NATURAL WICKER

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PROJECT No.: FLB15009  
 DRAWN BY: KPS  
 CHECKED BY: 0630016  
 DATE: 06/30/16  
 SCALE: 1" = 20'  
 CAD I.D.: LPD

PROJECT: SR 70 AND JENKINS FOR WAWA

LOCATION OF SITE  
 SEC SR 70 & S JENKINS RD  
 ST. LUCIE COUNTY, FLORIDA

**BOHLER ENGINEERING**

2255 GLADES ROAD, SUITE 305E  
 BOCA RATON, FLORIDA 33431

Phone: (561) 571-0280  
 Fax: (561) 571-0281  
 FLORIDA BUSINESS CERT. OF AUTH. NO. 30780  
 LANDSCAPE ARCHITECT BUSINESS LIC. NO. LC26000505

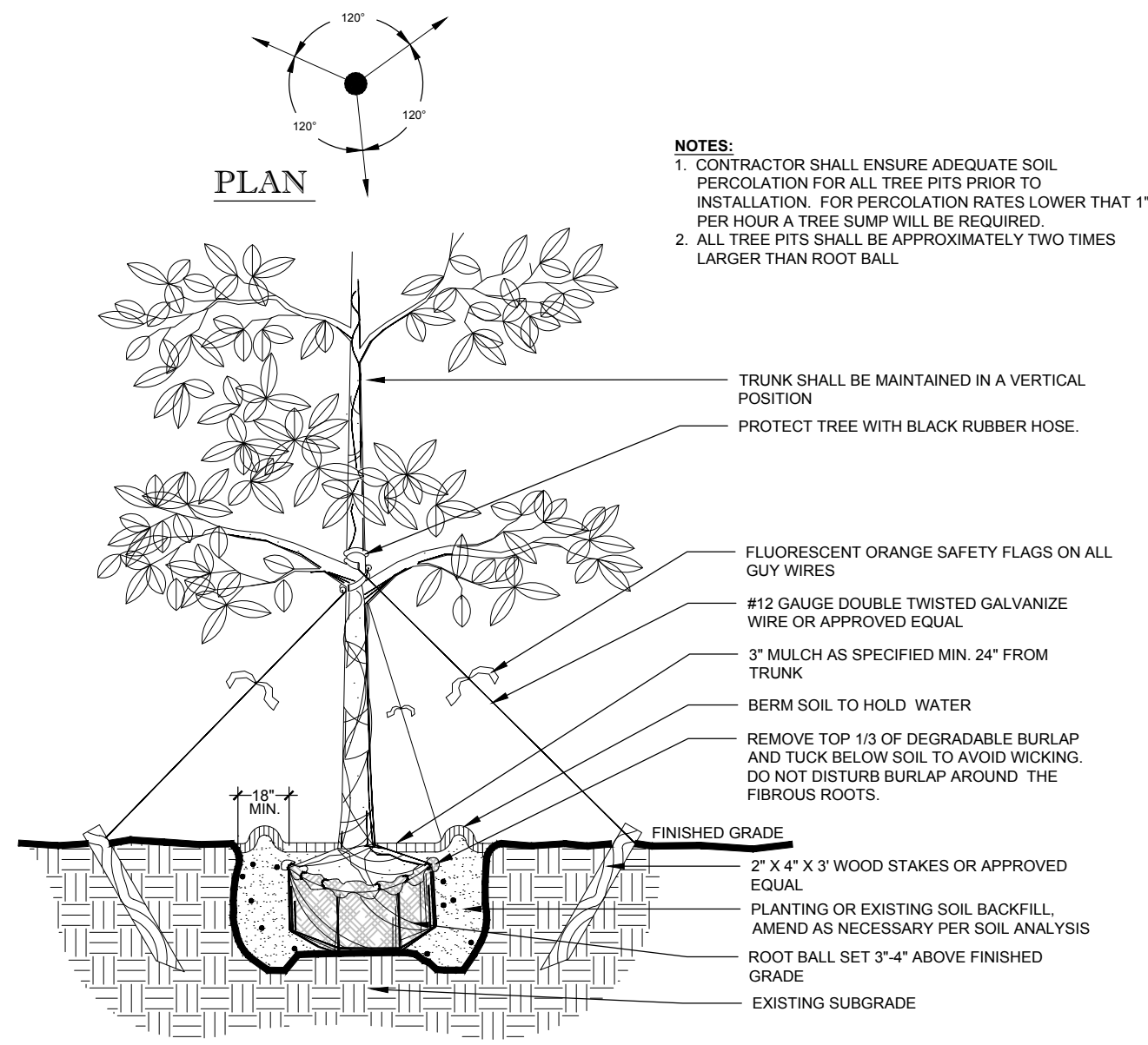
REGISTERED LANDSCAPE ARCHITECT

STATE OF FLORIDA

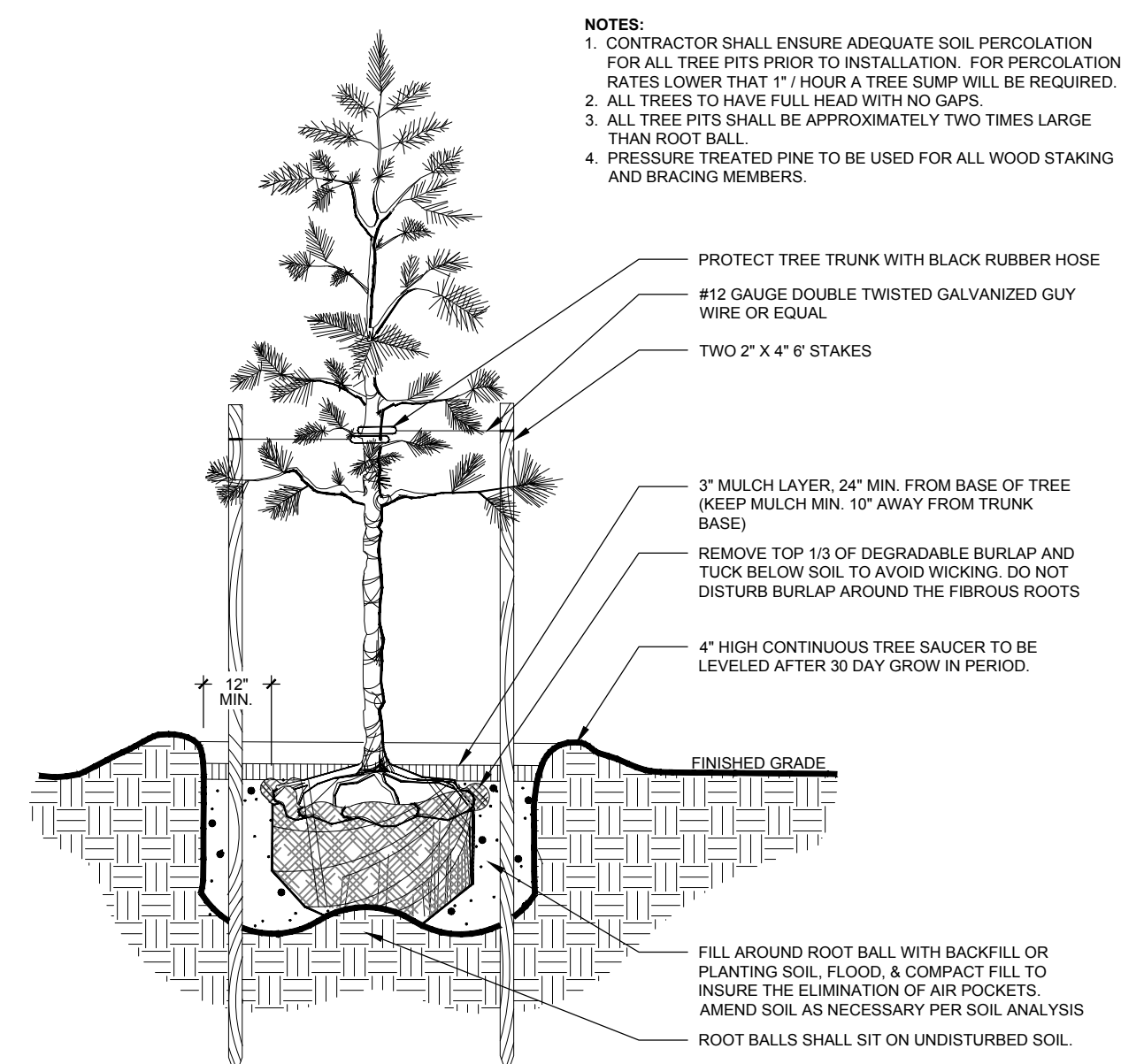
07/01/16

SHEET TITLE: PLANTING PLAN

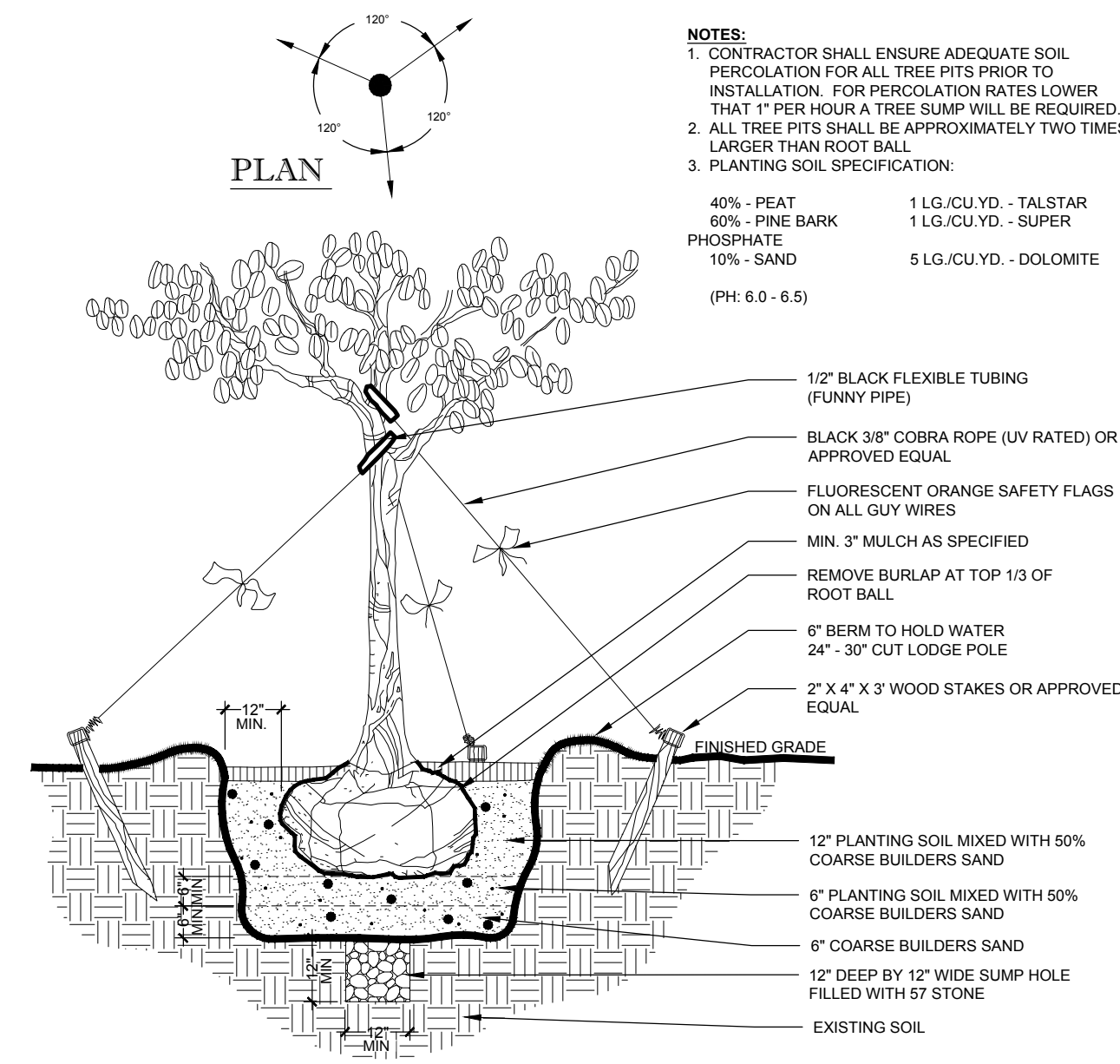
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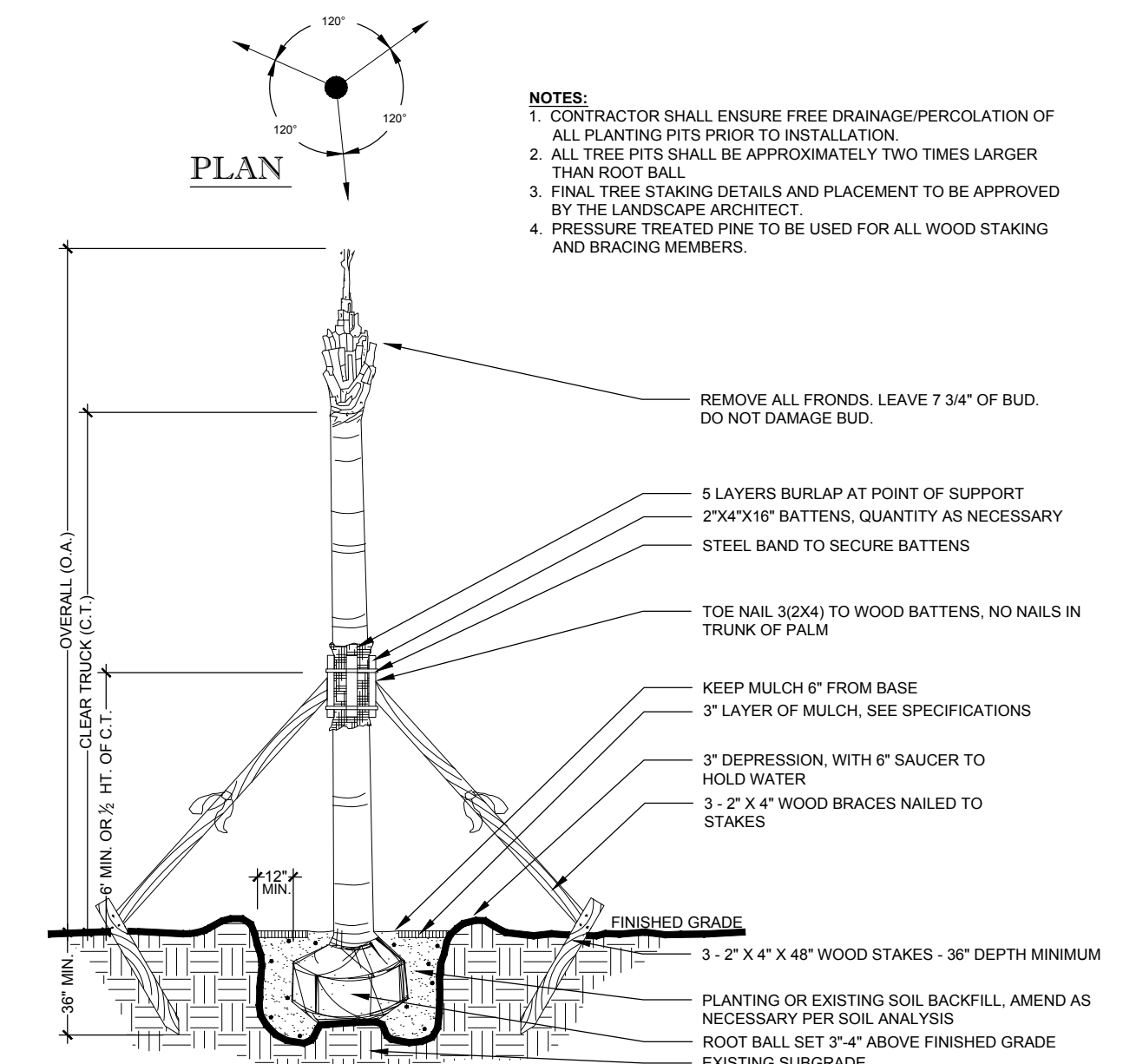
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SCALE: NTS



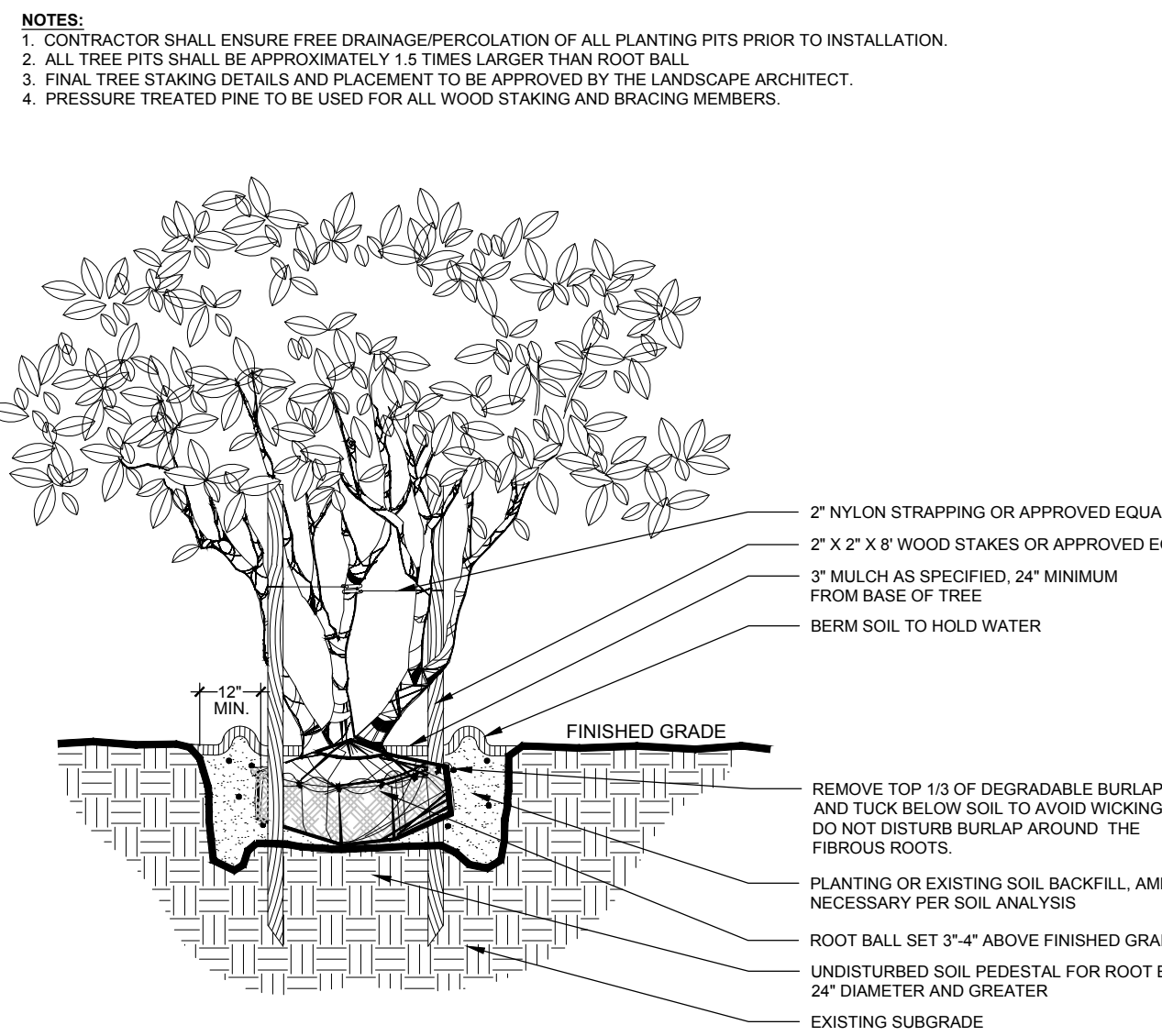
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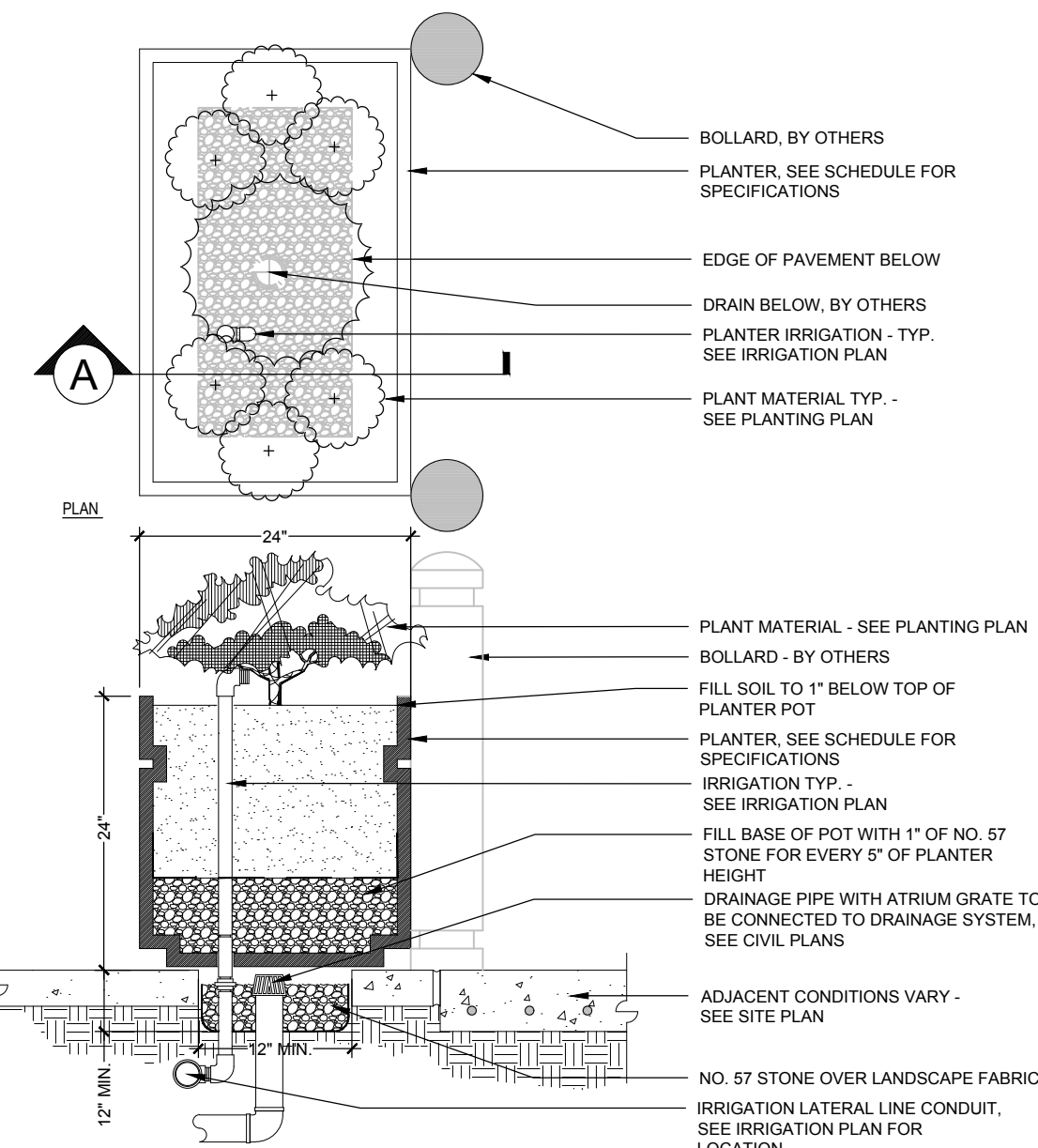
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SCALE: NTS



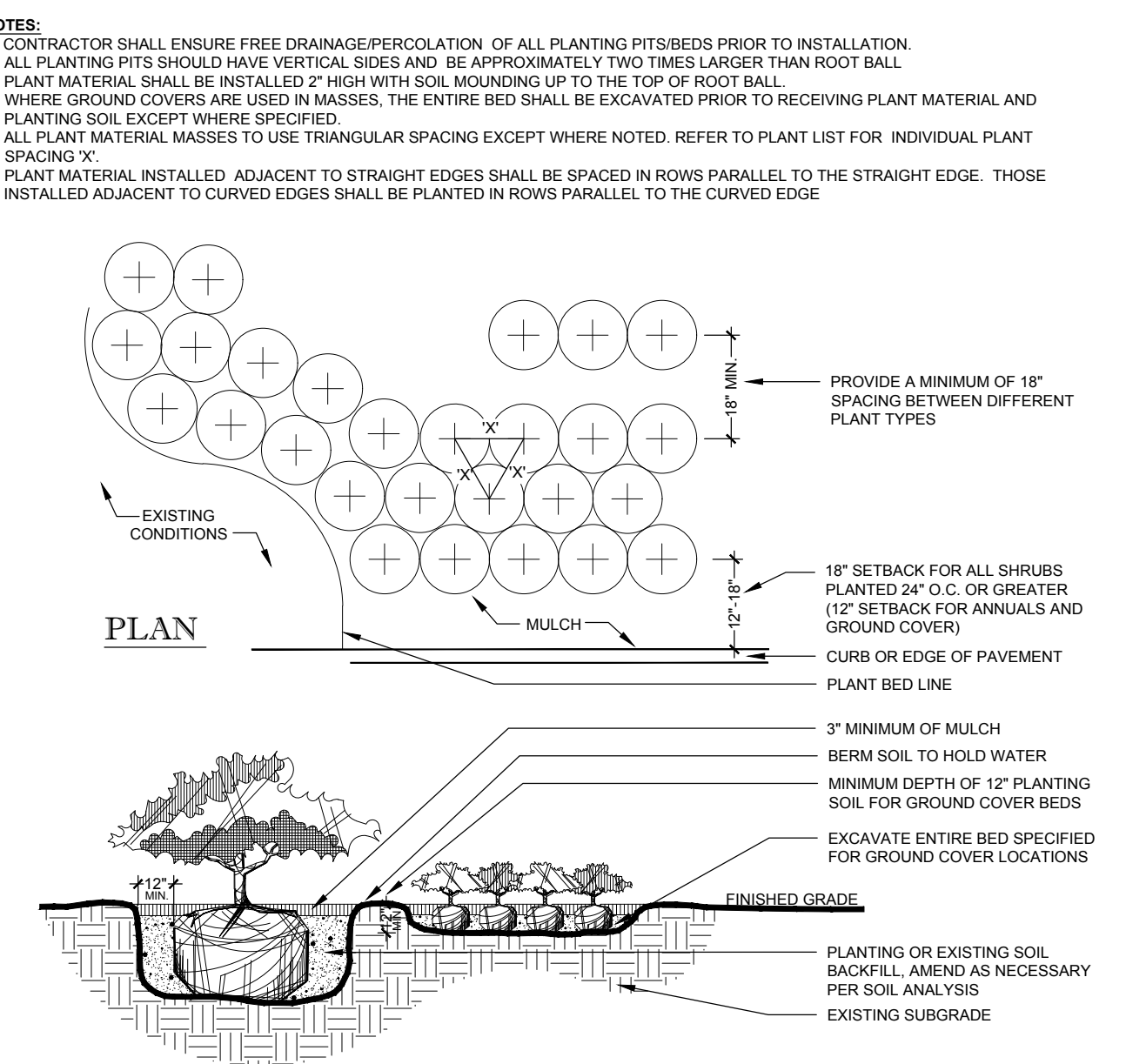
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SCALE: NTS



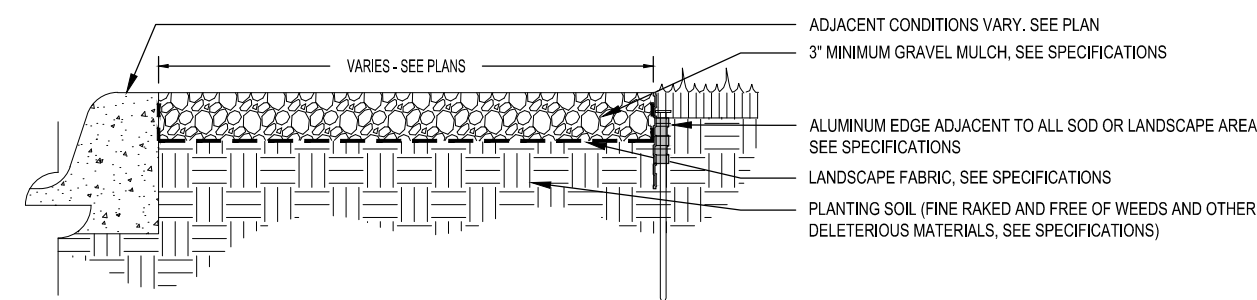
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SCALE: NTS



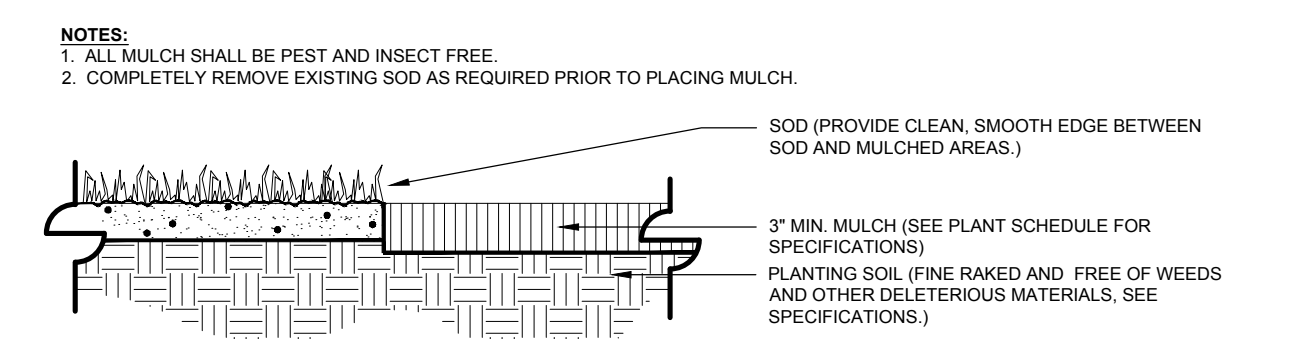
6 TYPICAL RAISED PLANTER DETAIL  
SCALE: NTS



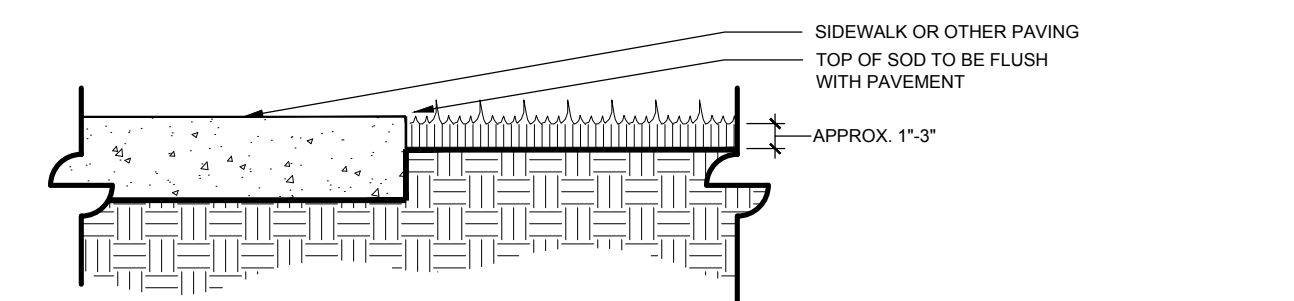
7 SHRUB AND GROUND COVER  
SCALE: NTS



8 GRAVEL APPLICATION  
SCALE: NTS



9 MULCH APPLICATION  
SCALE: NTS



10 SOD APPLICATION  
SCALE: NTS

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SEC SR 70 & S JENKINS RD  
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2255 GLADES ROAD, SUITE 305E  
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 Phone: (561) 571-0280  
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 FLORIDA BUSINESS CERT. OF AUTH. No. 30780  
 LANDSCAPE ARCHITECT BUSINESS LIC. No. LC2600555

REGISTERED LANDSCAPE ARCHITECT

STATE OF FLORIDA

07/01/16

SHEET TITLE:  
**PLANTING DETAILS**

SHEET NUMBER:  
**LP-5.01**





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# TRANSPORTATION ANALYSIS

Wawa - SR 70 and S. Jenkins Road

*Prepared for:*  
Bohler Engineering

# Transportation Analysis

## Wawa – SR 70 and S. Jenkins Road

June 2016

*Prepared for:*

**Bohler Engineering**

*Prepared by:*

**Whitehouse Group Inc.**

6017 E Memorial Highway, Suite E-3

Tampa, FL 33615

Ph: (813) 230-9778

Project No. T0018.00

Vicki L. Castro, P.E.  
P.E. No. 47128

---

Date

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Conceptual Site Plan  
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Turn Lane Warrants  
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## INTRODUCTION

The purpose of this report is to provide the Transportation Analysis to support the proposed driveway connection request to SR 70 and S. Jenkins Road. The proposed development is located south of SR 70 and east of S. Jenkins Road in the City of Ft. Pierce, Florida, as shown in Figure 1.

## PROJECT DESCRIPTION

The project is proposed to have the following uses for the purpose of this analysis:

- 6,119 Square Foot of Convenience Market with Gasoline Pumps

The accesses for the project are proposed to be as follows:

- One (1) right-in/right-out access to SR 70
- One (1) full access to S. Jenkins Road.

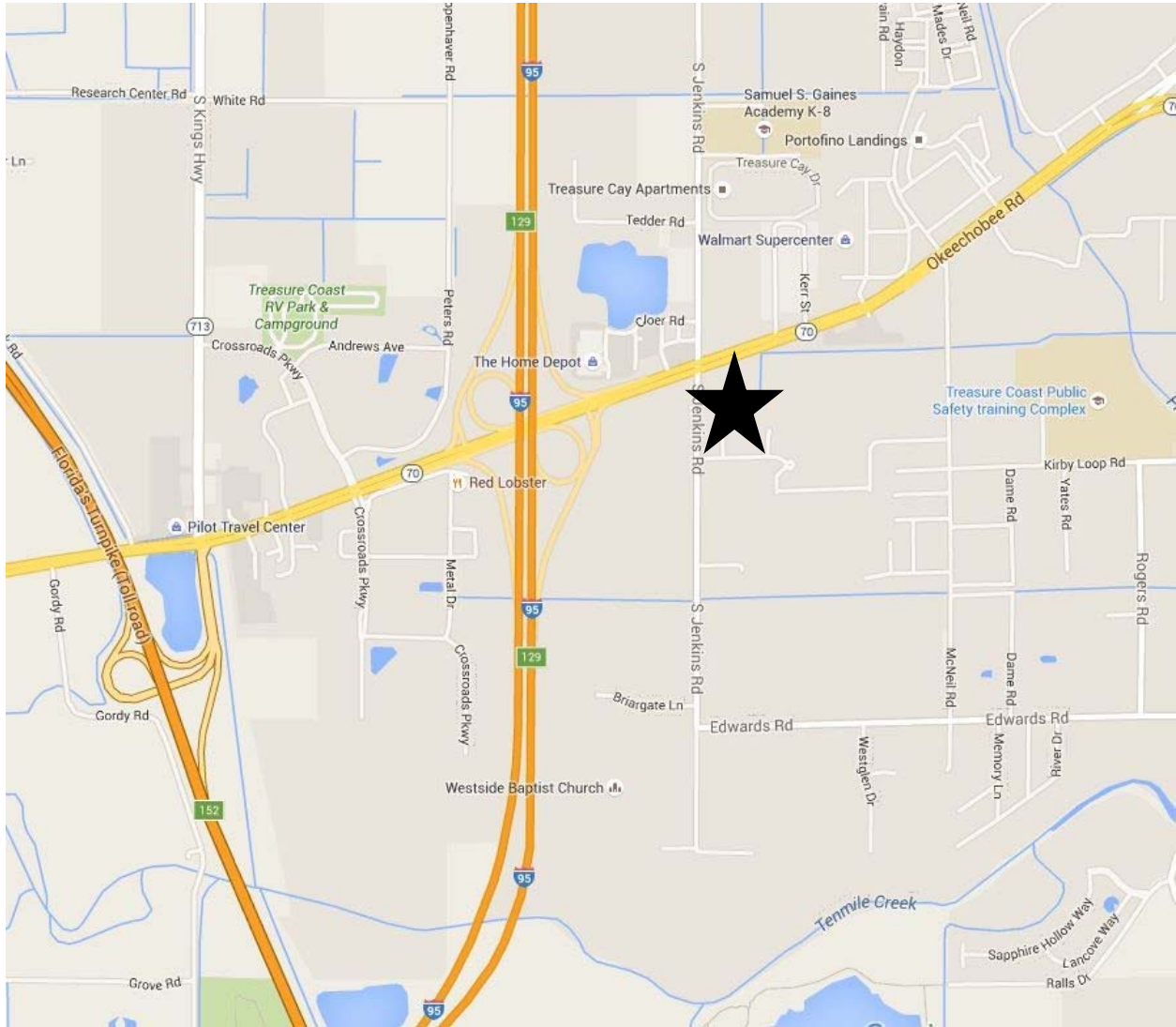
A conceptual site plan is included in the Appendix of this report.

## ESTIMATED DAILY PROJECT TRAFFIC

The trip rates utilized in this report were obtained from the latest computerized version of “OTISS” which utilizes the Institute of Transportation Engineers’ (ITE) Trip Generation, 9<sup>th</sup> Edition, 2012, as its database. Based on these trip rates, it is estimated that the proposed project will attract approximately 5,175 daily trip ends, as shown in Table 1.

Studies contained in the ITE Trip Generation Handbook, 3<sup>rd</sup> Edition, indicate that a percentage of the project trips already exist on the adjacent roadways – passerby capture. Therefore, the new daily trip ends attracted to the proposed project would be 1,915 trip ends, as shown in Table 1.

**Figure 1. Project Location**



**Table 1. Estimated Daily Project Traffic**

<u>Land Use</u>	<u>ITE LUC</u>	<u>Size</u>	<u>Daily Trip Ends (1)</u>	<u>Passerby Capture (2)</u>	<u>New Daily Trip Ends</u>
Convenience Market w Gas	853	6,119 SF	5,175	3,260	1,915

(1) Source: ITE Trip Generation, 9th Edition, 2012.

(2) Source: ITE Trip Generation Handbook, 3rd Edition.

## **ESTIMATED AM PEAK HOUR PROJECT TRAFFIC**

Again, based on data contained in the ITE Trip Generation, 9<sup>th</sup> Edition, the proposed project would attract approximately 250 trip ends during the AM peak hour with 125 inbound and 125 outbound, as shown in Table 2.

As previously stated, studies contained in the ITE Trip Generation Handbook, 3<sup>rd</sup> Edition, indicate that a percentage of the project trips already exist on the adjacent roadways – passerby capture. Therefore, the new AM peak hour trip ends attracted to the proposed project would be 92 trip ends with 46 inbound and 46 outbound, as shown in Table 2.

## **ESTIMATED PM PEAK HOUR PROJECT TRAFFIC**

Again, based on data contained in the ITE Trip Generation, 9<sup>th</sup> Edition, during the PM peak hour, the proposed project would attract approximately 312 trip ends during the PM peak hour with 156 inbound and 156 outbound, as shown in Table 3.

As previously stated, studies contained in the ITE Trip Generation Handbook, 3<sup>rd</sup> Edition, indicate that a percentage of the projects trips already exist on the adjacent roadways – passerby capture. Therefore, the new PM peak hour trip ends attracted to the proposed project would be 106 trip ends with 53 inbound and 53 outbound, as shown in Table 3.

**Table 2. AM Peak Hour Project Traffic**

<u>Land Use</u>	<u>ITE LUC</u>	<u>Size</u>	<u>AM Peak Hour Trip Ends (1)</u>			<u>Passerby Capture (2)</u>			<u>New AM Peak Hour Trip Ends</u>		
			<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>
Convenience Market w Gas	853	6,119 SF	125	125	250	79	79	158	46	46	92

(1) Source: ITE Trip Generation, 9th Edition, 2012.

(2) Source: ITE Trip Generation Handbook, 3rd Edition.

**Table 3. PM Peak Hour Project Traffic**

<u>Land Use</u>	ITE <u>LUC</u>	<u>Size</u>	PM Peak Hour Trip Ends (1)			Passerby Capture (2)			New PM Peak Hour Trip Ends		
			<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>
Convenience Market w Gas	853	6,119 SF	156	156	312	103	103	206	53	53	106

(1) Source: ITE Trip Generation, 9th Edition, 2012.

(2) Source: ITE Trip Generation Handbook, 3rd Edition.

## **ANALYSIS PERIOD**

The study period for this project includes both the AM and PM peak hours.

## **PROJECT TRIP DISTRIBUTION / ASSIGNMENT**

The following distribution of the AM and PM peak hour trip ends was based on the existing traffic and development patterns with hand assignment to the local network:

- 15% to and from the north (via S. Jenkins Road)
- 15% to and from the south (via S. Jenkins Road)
- 20% to and from the east (via SR 70)
- 50% to and from the west (via SR 70)

Table 4 shows the distribution of the new AM and PM peak hour project trip ends. Figure 2 and Figure 3 illustrate the project trip ends on the adjacent roadway network for the AM and PM peak hours, respectively.

## **ADJACENT ROADWAYS**

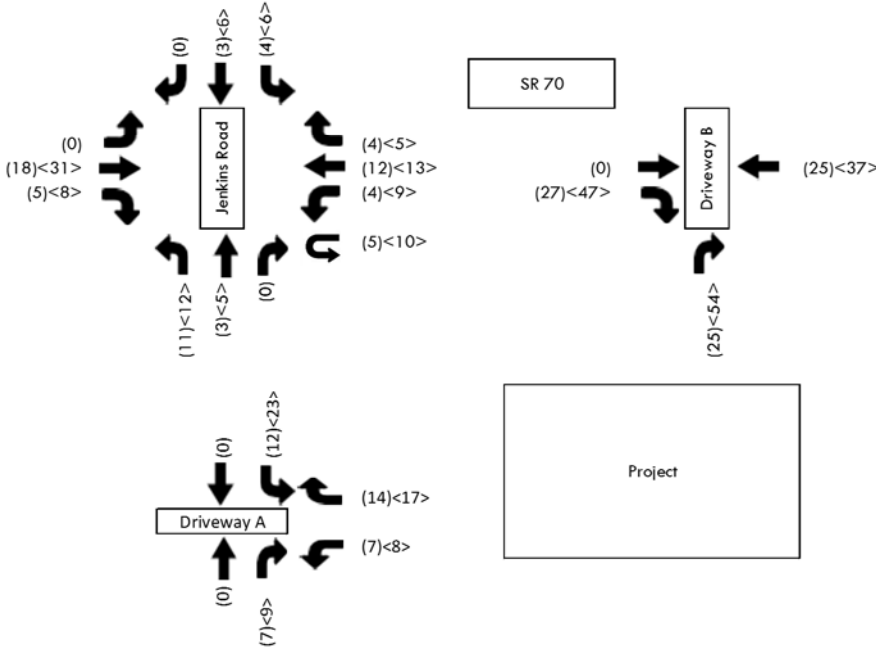
As stated previously, the site is located south of SR 70 and east of S. Jenkins Road. S. Jenkins Road is a two (2) lane roadway and SR 70 is a six (6) lane divided roadway in the vicinity of the project. According to the Florida Department of Transportation (FDOT) Five-Year Work Program, there is a widening project currently under construction that will widen SR 70 to eight (8) lanes with major modifications to the intersection. The new geometry of the intersection will be utilized in the analysis.

**Table 4. Estimated Peak Hour Project Traffic Distribution**

<u>Time Period</u>	<u>North (15%)</u>		<u>South (15%)</u>		<u>East (20%)</u>		<u>West (50%)</u>		<u>Total</u>	
	<u>In</u>	<u>Out</u>	<u>In</u>	<u>Out</u>	<u>In</u>	<u>Out</u>	<u>In</u>	<u>Out</u>	<u>In</u>	<u>Out</u>
AM	7	7	7	7	9	9	23	23	46	46
PM	8	8	8	8	11	11	26	26	53	53

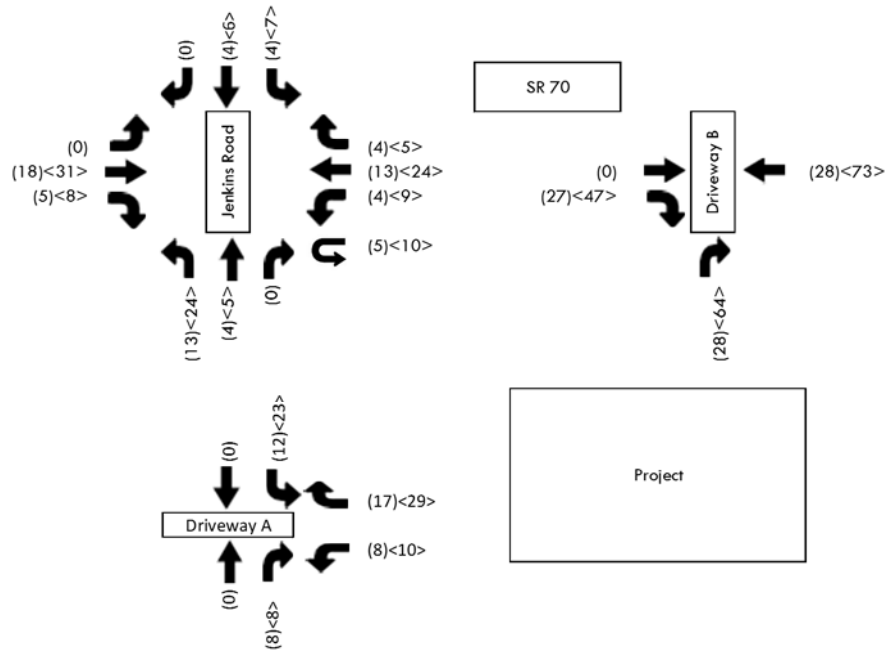
**Figure 2. AM Peak Hour Project Traffic**

**Legend**  
 (1) - New Project Traffic  
 <1> - Passerby Project Traffic



**Figure 3. PM Peak Hour Project Traffic**

  
**Legend**  
 (1) - New Project Traffic  
 <1> - Passerby Project Traffic

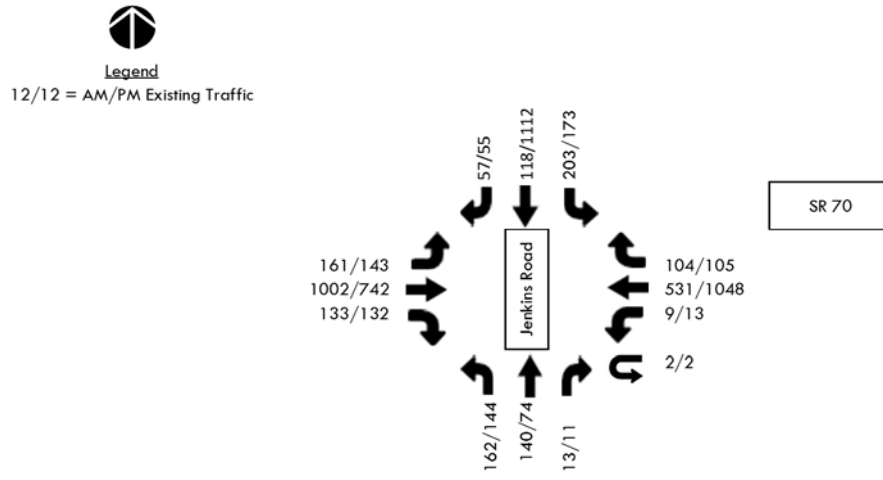


## PEAK SEASON TRAFFIC

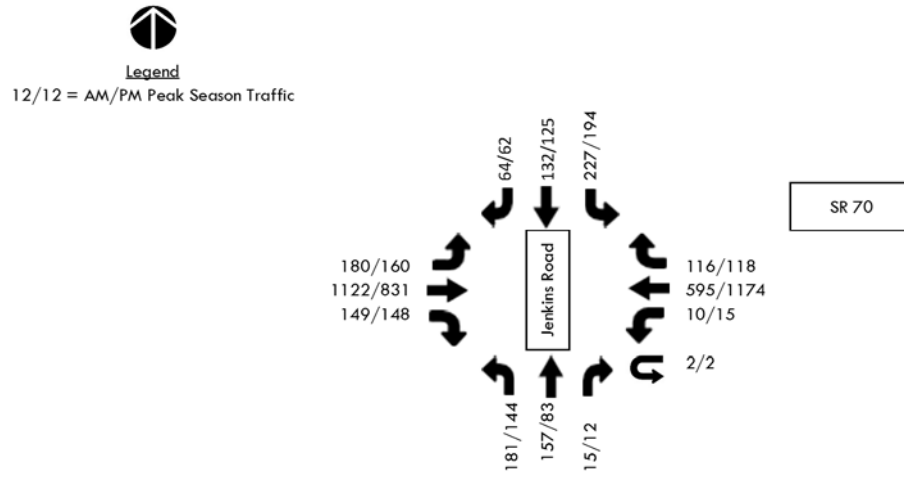
The following methodology was utilized to estimate the existing volumes within the study area:

1. WHITEHOUSE GROUP INC. obtained AM and PM turning movement counts at intersection of SR 70 and S. Jenkins Road.
2. The turning movement counts were adjusted to peak season based on the FDOT 2014 Peak Season Adjustment Factors for St. Lucie County. Figure 4 illustrates the existing traffic, Figure 5 illustrates the peak season traffic, Figure 6 illustrates the AM peak season plus project traffic, and Figure 7 illustrates the PM peak season plus project traffic.

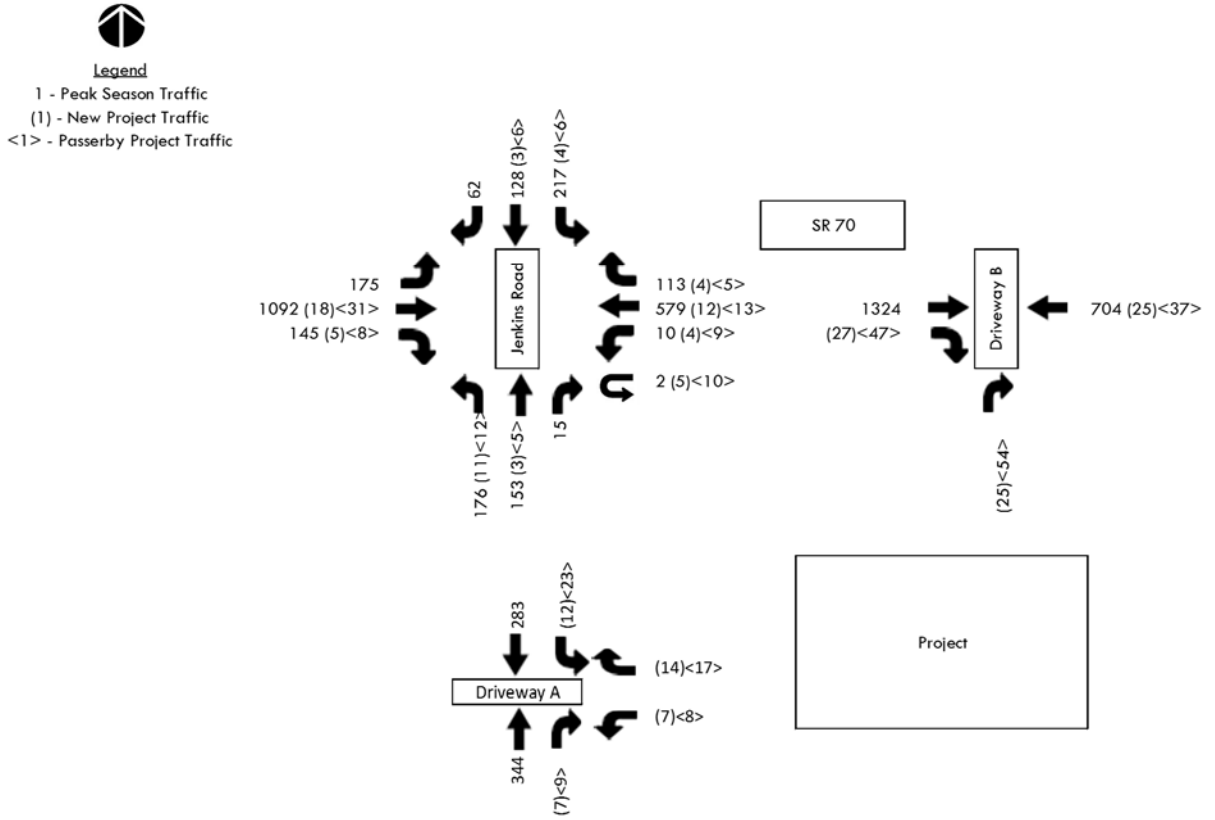
**Figure 4. Existing Traffic**



**Figure 5. Peak Season Traffic**

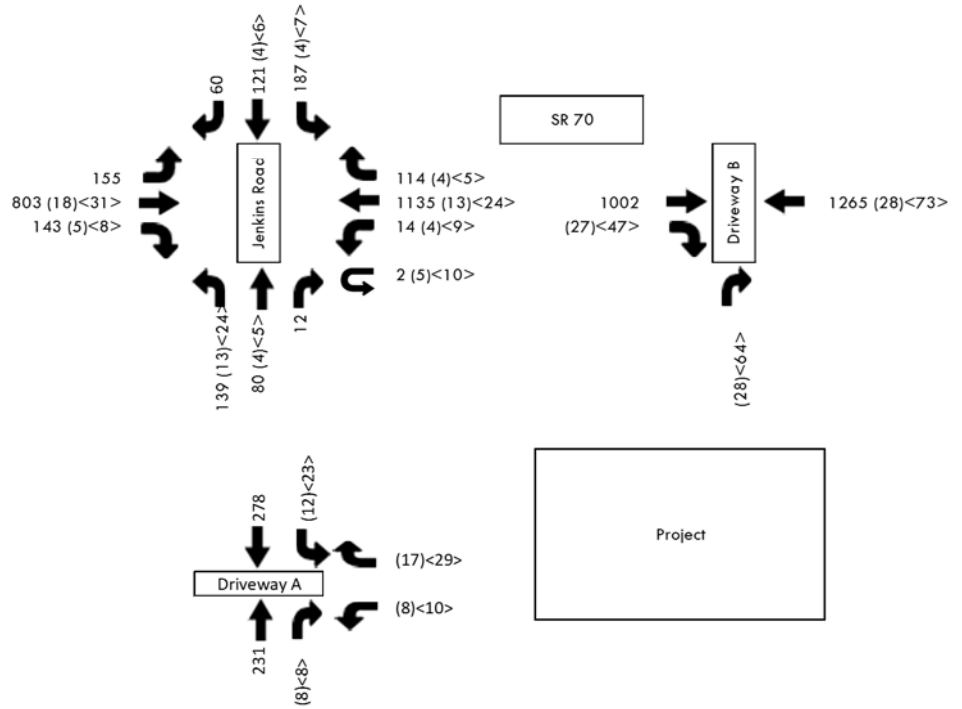


**Figure 6. AM Peak Hour Peak Season + Project Traffic**



**Figure 7. PM Peak Hour Peak Season + Project Traffic**

  
**Legend**  
 1 - Peak Season Traffic  
 (1) - New Project Traffic  
 <1> - Passerby Project Traffic



## INTERSECTION ANALYSIS

Intersection analysis was conducted for the AM and PM peak hours at the following intersections within the study network:

- SR 70 and S. Jenkins Road
- SR 70 and Project Driveway A
- S. Jenkins Road and Project Driveway B

The analysis was based on SYNCHRO for both with and without the proposed project traffic. Table 5 and Table 6 summarize the analysis for the signalized and unsignalized intersections, respectively, and are described in detail in the following paragraphs.

### SR 70 and S. Jenkins Road

This intersection is signalized. Signalized intersection analysis indicates that this intersection should operate at a Level of Service D during the AM and PM peak hours with the peak season plus project traffic, as shown in Table 5.

### SR 70 and Project Driveway

This project driveway is purposed to be unsignalized. Unsignalized intersection analysis indicates that this intersection should operate at a Level of Service B during the AM and PM peak hours with the peak season plus project traffic, as shown in Table 6.

### S. Jenkins Road and Project Driveway

This project driveway is purposed to be unsignalized. Unsignalized intersection analysis indicates that this intersection should operate at a Level of Service A during the AM and PM peak hours with the peak season plus project traffic, as shown in Table 6.

**Table 5. Estimated Intersection Level of Service (Signalized)**

<u>Intersection</u>	<u>Time Period</u>	<u>Peak Season Traffic</u>		<u>Peak Season Plus Project Traffic</u>	
		<u>Delay</u>	<u>LOS</u>	<u>Delay</u>	<u>LOS</u>
SR 70 and Jenkins Road	AM	38.1	D	42.8	D
	PM	39.8	D	41.6	D

**Table 6. Estimated Intersection Level of Service (Unsignalized)**

<u>Intersection</u>	<u>Movement</u>	<u>Peak Season Plus Project Traffic AM Peak Hour</u>			<u>Peak Season Plus Project Traffic PM Peak Hour</u>		
		<u>Left</u>	<u>Through</u>	<u>Right</u>	<u>Left</u>	<u>Through</u>	<u>Right</u>
SR 70 and Driveway A	NB	-	-	B	-	-	B
Jenkins Road and Driveway B	NB	-	*	*	-	*	*
	SB	A	*	-	A	*	-
	WB	B	-	A	B	-	A

\*Free flow movement; therefore, the level of service was not calculated.

## ACCESS RECOMMENDATIONS

The recommendations included in this report are based on a field review of the site, the proposed site plan and the Transportation Analysis. The methodology utilized to determine the need for a left turn lane on S. Jenkins Road was based on the Highway Research Record 211 – Volume Warrants for Left-turn Storage Lanes at Unsignalized Grade Intersections (Harmelink Curves). The FDOT Driveway Guide was utilized to determine the need for right turn lanes on SR 70 and S. Jenkins Road. The access recommendations are summarized in Table 7 and described in the following paragraph:

### SR 70 and Project Driveway A

The project driveway is proposed to have right-in/right-out access to US 27. Based on the estimated project traffic, an eastbound right turn lane is not warranted. However, at the request of the FDOT, an eastbound right turn lane is being constructed. Based on the posted speed limit of SR 70, it is recommended that the turn lane be 240'-foot, which includes a 50-foot taper.

### S. Jenkins Road and Project Driveway B

The project driveway is proposed to have full access to S. Jenkins Road. Based on the estimated project traffic, neither a northbound right turn lane or southbound left turn lane is warranted.

.

**Table 7. Access Recommendations**

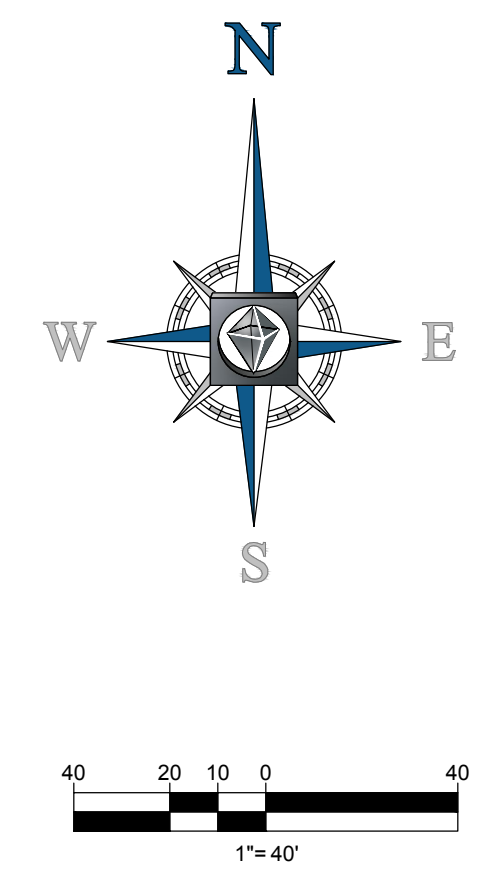
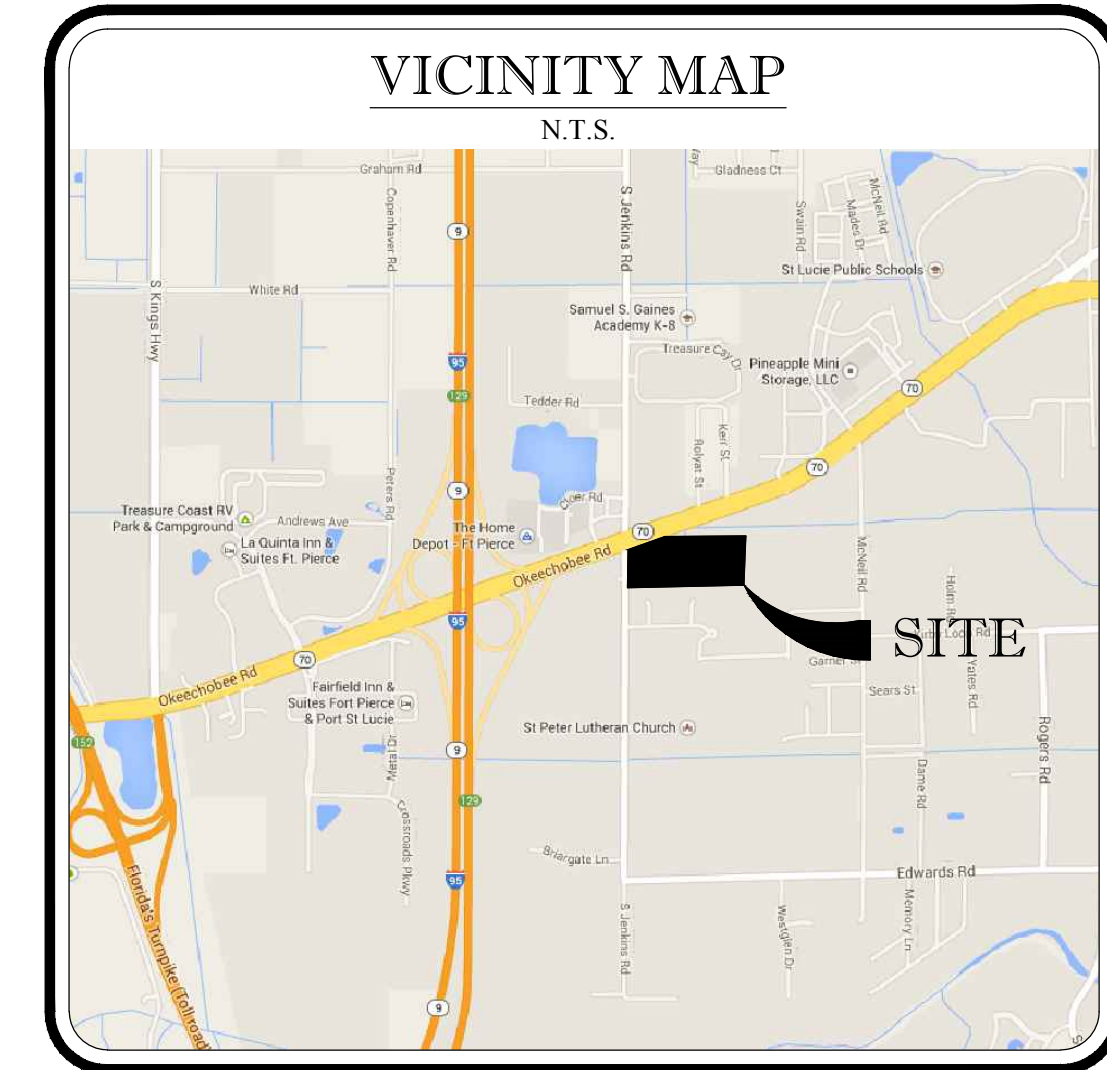
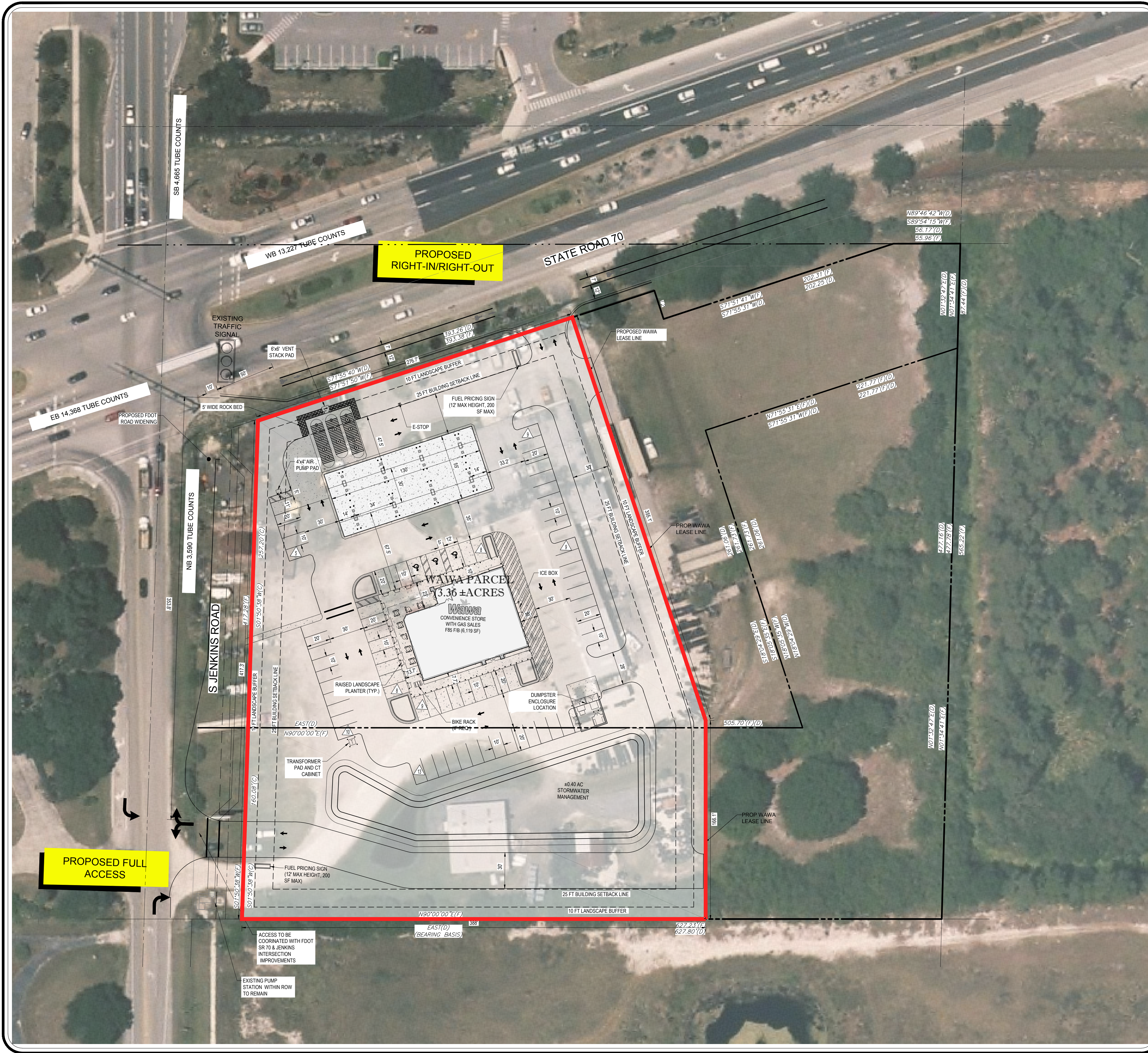
<u>Intersection</u>	<u>Movement</u>	<u>Peak Hour Volume (1)</u>	<u>Turn Lane Warranted?</u>	<u>Queue Storage</u>	<u>Length (2) Length (2)</u>	<u>Length Length</u>
SR 70 and Driveway A	EBR	74/90	N	0'	240'	240'
Jenkins Road and Driveway B	NBR	16/16	N			
	SBL	35/50	N			

(1) See Figures 6 and 7, Peak Hour Peak Season Plus Project Traffic, from the report.

(2) Based on FDOT Standard Index #301 and a posted speed limit of 45 mph of SR 70.

# **APPENDIX**

**APPENDIX**  
CONCEPTUAL SITE PLAN



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 DRAWN BY: JRT  
 CHECKED BY: CTM  
 DATE: 11/08/2015  
 SCALE: AS NOTED  
 CAD I.D.: EX0

PROJECT: **LEASE EXHIBIT**

FOR **WAWA**

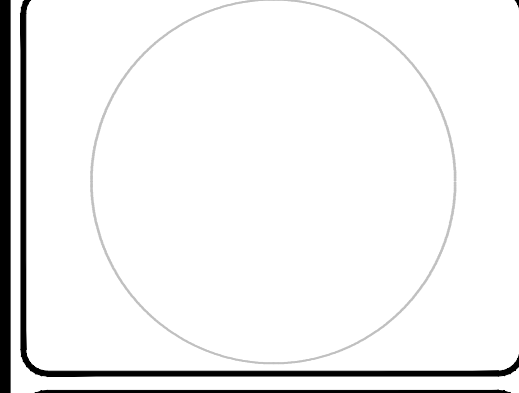
**Wawa**

LOCATION OF SITE

SEC SR 70 & S JENKINS RD  
 ST. LUCIE COUNTY, FLORIDA

**BOHLER ENGINEERING**

3820 NORTHDAL BLVD., SUITE 300B  
 TAMPA, FLORIDA 33624  
 Phone: (813) 812-4100  
 Fax: (813) 812-4101  
 FLORIDA BUSINESS CERT. OF AUTH. No. 30763



SHEET TITLE: **CONCEPT PLAN**

SHEET NUMBER: **A**

**APPENDIX**  
TRIP GENERATION

## PERIOD SETTING

**Analysis Name :** Daily  
**Project Name :** Wawa **No :**  
**Date:** 4/14/2016 **City:**  
**State/Province:** **Zip/Postal Code:**  
**Country:** **Client Name:**  
**Analyst's Name:** **Edition:** ITE-TGM 9th Edition

Land Use	Independent Variable	Size	Time Period	Method	Entry	Exit	Total
853 - Convenience Market with Gasoline Pumps	1000 Sq. Feet Gross Floor Area	6.12 <sup>(0)</sup>	Weekday	Average 845.6	2588 50%	2587 50%	5175

(0) indicates size out of range.

## TRAFFIC REDUCTIONS

Land Use	Entry Reduction	Adjusted Entry	Exit Reduction	Adjusted Exit
853 - Convenience Market with Gasoline Pumps	0 %	2588	0 %	2587

## EXTERNAL TRIPS

Land Use	External Trips	Pass-by%	Pass-by Trips	Non-pass-by Trips
853 - Convenience Market with Gasoline Pumps	5175	63 %	3260	1915

## ITE DEVIATION DETAILS

**Weekday**  
 Landuse No deviations from ITE.  
 Methods No deviations from ITE.  
 External Trips 853 - Convenience Market with Gasoline Pumps  
 ITE does not recommend a particular pass-by% for this case.

## SUMMARY

<b>Total Entering</b>	2588
<b>Total Exiting</b>	2587
<b>Total Entering Reduction</b>	0
<b>Total Exiting Reduction</b>	0
<b>Total Entering Internal Capture Reduction</b>	0
<b>Total Exiting Internal Capture Reduction</b>	0
<b>Total Entering Pass-by Reduction</b>	1630
<b>Total Exiting Pass-by Reduction</b>	1630
<b>Total Entering Non-Pass-by Trips</b>	958
<b>Total Exiting Non-Pass-by Trips</b>	957

### PERIOD SETTING


**Analysis Name :** AM Peak Hour  
**Project Name :** Wawa **No :**  
**Date:** 4/14/2016 **City:**  
**State/Province:** **Zip/Postal Code:**  
**Country:** **Client Name:**  
**Analyst's Name:** **Edition:** ITE-TGM 9th Edition

Land Use	Independent Variable	Size	Time Period	Method	Entry	Exit	Total
853 - Convenience Market with Gasoline Pumps	1000 Sq. Feet Gross Floor Area	6.12	Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m.	Average 40.92	125 50%	125 50%	250

### TRAFFIC REDUCTIONS

Land Use	Entry Reduction	Adjusted Entry	Exit Reduction	Adjusted Exit
853 - Convenience Market with Gasoline Pumps	<input type="text" value="0"/> %	125	<input type="text" value="0"/> %	125

### EXTERNAL TRIPS

Land Use	External Trips	Pass-by%	Pass-by Trips	Non-pass-by Trips
853 - Convenience Market with Gasoline Pumps	250	 <input type="text" value="63"/> %	158	92

### ITE DEVIATION DETAILS

**Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m.**

Landuse No deviations from ITE.

Methods No deviations from ITE.

External Trips No deviations from ITE.

## SUMMARY

<b>Total Entering</b>	125
<b>Total Exiting</b>	125
<b>Total Entering Reduction</b>	0
<b>Total Exiting Reduction</b>	0
<b>Total Entering Internal Capture Reduction</b>	0
<b>Total Exiting Internal Capture Reduction</b>	0
<b>Total Entering Pass-by Reduction</b>	79
<b>Total Exiting Pass-by Reduction</b>	79
<b>Total Entering Non-Pass-by Trips</b>	46
<b>Total Exiting Non-Pass-by Trips</b>	46

### PERIOD SETTING


**Analysis Name :** PM Peak Hour  
**Project Name :** Wawa **No :**  
**Date:** 4/14/2016 **City:**  
**State/Province:** **Zip/Postal Code:**  
**Country:** **Client Name:**  
**Analyst's Name:** **Edition:** ITE-TGM 9th Edition

Land Use	Independent Variable	Size	Time Period	Method	Entry	Exit	Total
853 - Convenience Market with Gasoline Pumps	1000 Sq. Feet Gross Floor Area	6.12	Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 p.m.	Average 50.92	156 50%	156 50%	312

### TRAFFIC REDUCTIONS

Land Use	Entry Reduction	Adjusted Entry	Exit Reduction	Adjusted Exit
853 - Convenience Market with Gasoline Pumps	0 %	156	0 %	156

### EXTERNAL TRIPS

Land Use	External Trips	Pass-by%	Pass-by Trips	Non-pass-by Trips
853 - Convenience Market with Gasoline Pumps	312	 66 %	206	106

### ITE DEVIATION DETAILS

**Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 p.m.**

Landuse No deviations from ITE.

Methods No deviations from ITE.

External Trips No deviations from ITE.

## SUMMARY

<b>Total Entering</b>	156
<b>Total Exiting</b>	156
<b>Total Entering Reduction</b>	0
<b>Total Exiting Reduction</b>	0
<b>Total Entering Internal Capture Reduction</b>	0
<b>Total Exiting Internal Capture Reduction</b>	0
<b>Total Entering Pass-by Reduction</b>	103
<b>Total Exiting Pass-by Reduction</b>	103
<b>Total Entering Non-Pass-by Trips</b>	53
<b>Total Exiting Non-Pass-by Trips</b>	53

**APPENDIX**  
PASSERBY CAPTURE RATES

**Table F.14 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period  
Land Use Code 851—Convenience Market (Open 24 Hours)**

SIZE (1,000 SQ. FT. GFA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
3	Overland Park, KS	Aug. 1987	68	4:30-5:30 p.m.	34	53	13	66	—	—
3	Overland Park, KS	July 1987	68	4:30-5:30 p.m.	28	50	22	72	—	—
~1.9	Billings, MT	1987	461	4:00-6:00 p.m.	62	13	25	38	—	ITE Montana Section Tech Comm
<50.0	Chicago suburbs, IL	1987	72	3:00-6:00 p.m.	28	—	—	72	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	54	3:00-6:00 p.m.	78	—	—	22	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	34	3:00-6:00 p.m.	69	—	—	31	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	100	3:00-6:00 p.m.	63	—	—	37	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	43	3:00-6:00 p.m.	43	—	—	57	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	135	3:00-6:00 p.m.	39	—	—	61	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	74	3:00-6:00 p.m.	53	—	—	47	—	Kenig, O'Hara, Humes, Flock
<50.0	Chicago suburbs, IL	1987	80	3:00-6:00 p.m.	64	—	—	36	—	Kenig, O'Hara, Humes, Flock

Average Pass-By Trip Percentage: 51

“—” means no data were provided

**Table F.15 Pass-By and Non-Pass-By Trips Weekday, AM Peak Period  
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. GFA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	7:00-9:00 a.m.	54	11	35	46	1,240	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	7:00-9:00 a.m.	48	17	35	52	1,210	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	47	7:00-9:00 a.m.	62	19	19	38	1,705	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	—	7:00-9:00 a.m.	72	15	13	28	940	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	49	7:00-9:00 a.m.	66	16	18	34	990	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	62	7:00-9:00 a.m.	74	10	16	26	790	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	58	7:00-9:00 a.m.	64	5	31	36	1,255	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	79	7:00-9:00 a.m.	56	6	38	44	635	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	49	7:00-9:00 a.m.	67	4	29	33	1,985	Barton-Aschman Assoc.

Average Pass-By Trip Percentage: 63

“—” means no data were provided

**Table F.16 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period  
Land Use Code 853—Convenience Market with Gasoline Pumps**

SIZE (1,000 SQ. FT. GFA)	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
						PRIMARY	DIVERTED	TOTAL		
2.8	Louisville area, KY	1993	—	4:00–6:00 p.m.	62	11	27	38	2,875	Barton-Aschman Assoc.
2.4	Louisville area, KY	1993	—	4:00–6:00 p.m.	58	13	29	42	2,655	Barton-Aschman Assoc.
4.2	Louisville area, KY	1993	61	4:00–6:00 p.m.	58	26	16	42	2,300	Barton-Aschman Assoc.
2.6	Crestwood, KY	1993	68	4:00–6:00 p.m.	67	15	18	33	950	Barton-Aschman Assoc.
3.7	Louisville area, KY	1993	70	4:00–6:00 p.m.	61	16	23	39	2,175	Barton-Aschman Assoc.
3.0	New Albany, IN	1993	80	4:00–6:00 p.m.	65	15	20	35	1,165	Barton-Aschman Assoc.
2.3	Louisville, KY	1993	67	4:00–6:00 p.m.	57	16	27	43	1,954	Barton-Aschman Assoc.
2.2	New Albany, IN	1993	115	4:00–6:00 p.m.	48	16	36	52	820	Barton-Aschman Assoc.
3.6	Louisville area, KY	1993	60	4:00–6:00 p.m.	56	17	27	44	2,505	Barton-Aschman Assoc.
2.6	Seminole Co., FL	1989	82	4:00–6:00 p.m.	73	20	7	27	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	98	4:00–6:00 p.m.	81	15	4	19	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	115	4:00–6:00 p.m.	69	16	15	31	—	Tipton Associates Inc.
2.6	Volusia Co., FL	1989	98	4:00–6:00 p.m.	74	15	11	26	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	4:00–6:00 p.m.	74	24	2	26	—	Tipton Associates Inc.
2.7	Volusia Co., FL	1989	82	4:00–6:00 p.m.	87	8	5	13	—	Tipton Associates Inc.
2.6	Seminole Co., FL	1989	99	2:00–4:00 p.m.	64	28	8	36	—	Tipton Associates Inc.
2.4	Volusia Co., FL	1989	38	2:00–4:00 p.m.	68	21	11	32	—	Tipton Associates Inc.

Average Pass-By Trip Percentage: 66

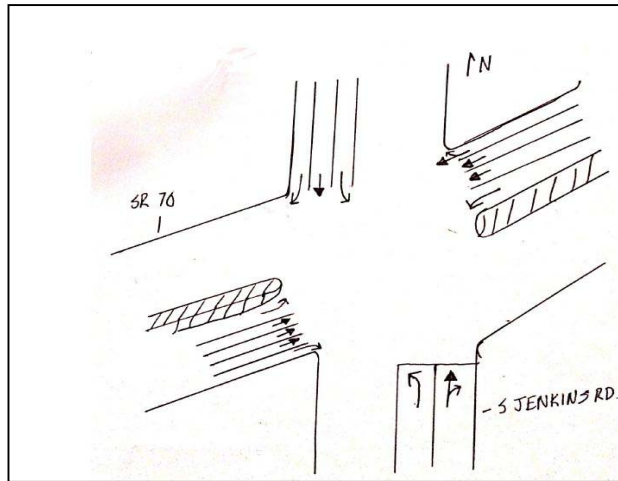
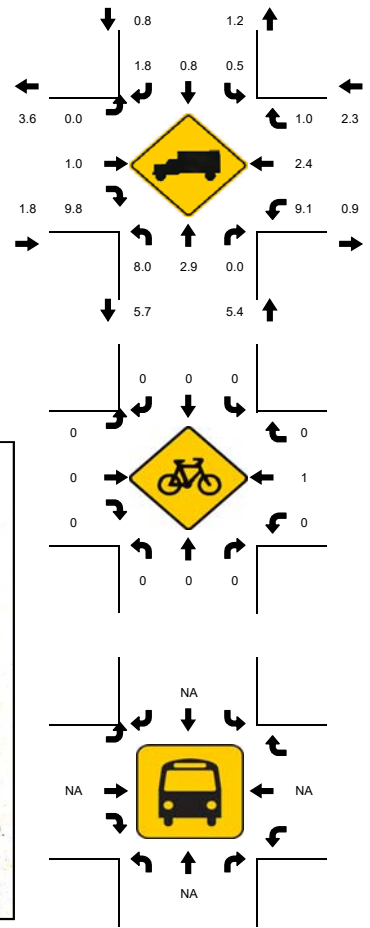
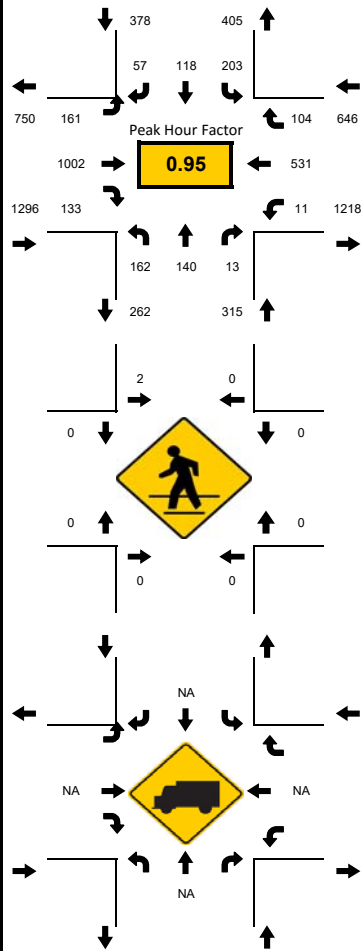
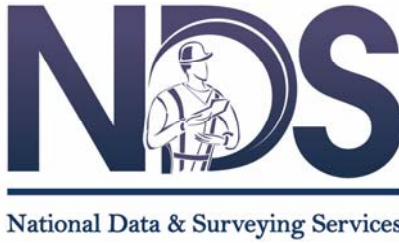
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**APPENDIX**  
TURNING MOVEMENT COUNTS

LOCATION: S. Jenkins Rd & SR 70  
 CITY/STATE: FORT PIERCE

PROJECT ID: 16-3181-001  
 DATE: Thu, Jun 09, 2016

Peak-Hour: 07:30 AM - 08:30 AM  
 Peak 15-Minute: 08:00 AM - 08:15 AM

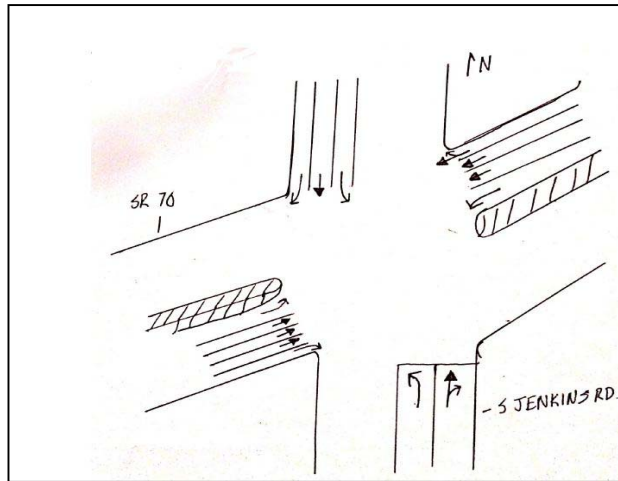
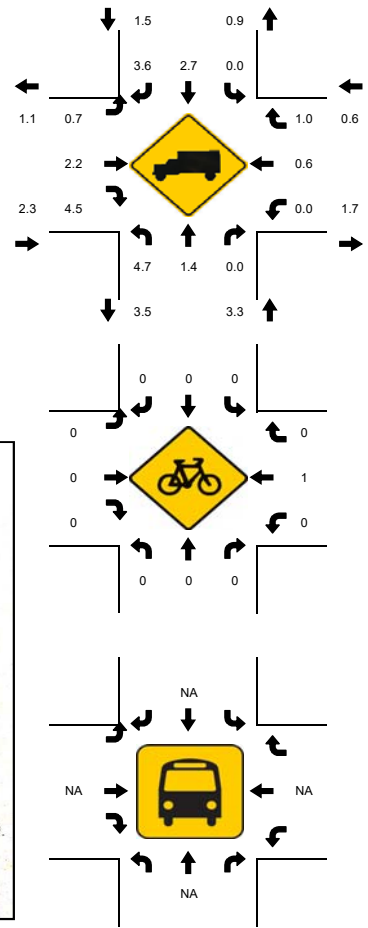
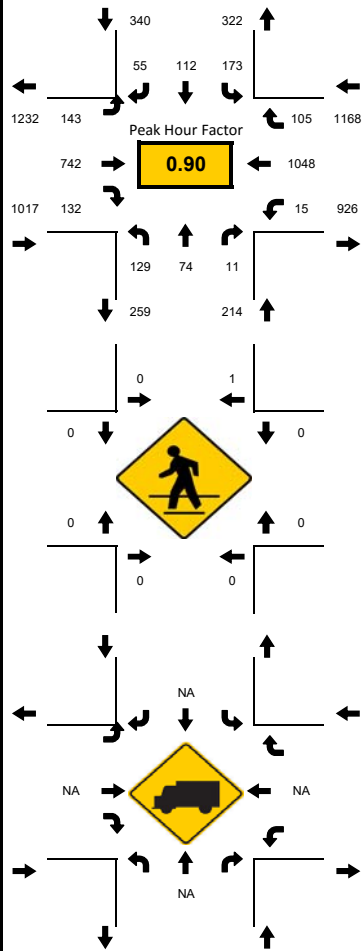
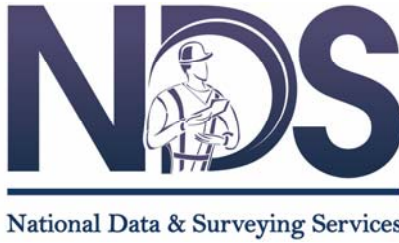


15-Min Count Period Beginning At	S. Jenkins Rd Northbound					S. Jenkins Rd Southbound					SR 70 Eastbound					SR 70 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	39	19	0	0		13	15	13	0		29	163	22	20		2	125	9	0		449	2317
07:15 AM	54	26	0	0		38	29	10	0		32	182	34	19		1	111	13	0		530	2565
07:30 AM	34	33	3	0		50	23	15	0		33	275	30	16		0	143	22	0		661	2635
07:45 AM	58	46	2	0		44	33	10	0		55	269	35	21		2	100	23	0		677	2543
08:00 AM	30	31	5	0		67	35	22	0		35	247	43	19		2	153	27	0		697	2425
08:15 AM	40	30	3	0		42	27	10	0		38	211	25	27		7	135	32	2		600	1728
08:30 AM	46	24	4	0		45	27	15	0		33	179	21	15		4	159	12	0		569	1128
08:45 AM	42	22	5	0		41	20	6	0		37	207	27	20		6	130	16	0		559	559
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
All Vehicles	232	184	20	0		268	140	88	0		220	1100	172	108		28	612	128	8		3192	
Heavy Trucks	20	8	0		4	4	4		0	20	16		4	16	4		100					
Pedestrians	0				8				0				0				8					
Bicycles	0	0	0		0	0	0		0	0	0		0	4	0		4					
Railroad																						
Stopped Buses																						

LOCATION: S. Jenkins Rd & SR 70  
CITY/STATE: FORT PIERCE

PROJECT ID: 16-3181-001  
DATE: Thu, Jun 09, 2016

Peak-Hour: 04:30 PM - 05:30 PM  
Peak 15-Minute: 05:15 PM - 05:30 PM



15-Min Count Period Beginning At	S. Jenkins Rd Northbound					S. Jenkins Rd Southbound					SR 70 Eastbound					SR 70 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	30	17	2	0		27	20	7	0		18	131	31	9		1	161	14	0		459	2450
04:15 PM	40	23	2	0		41	31	9	0		38	203	31	22		1	240	14	0		673	2653
04:30 PM	31	15	0	0		52	33	13	0		29	167	39	10		1	226	26	0		632	2739
04:45 PM	21	17	2	0		36	20	12	0		33	203	32	17		5	274	31	1		686	2728
05:00 PM	41	21	8	0		43	35	14	0		34	167	34	19		4	245	16	1		662	2617
05:15 PM	36	21	1	0		42	24	16	0		47	205	27	28		5	303	32	0		759	1955
05:30 PM	49	22	3	0		55	21	12	0		31	154	32	13		4	221	17	1		621	1196
05:45 PM	30	12	5	0		48	25	10	0		36	154	30	15		6	194	25	0		575	575
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
All Vehicles	164	84	32	0		208	140	64	0		188	820	156	112		20	1212	128	4		3216	
Heavy Trucks	16	4	0		0	8	8		4	20	12		0	12	4		88					
Pedestrians	0	0	0		0	4	0		0	0	0		0	0	0		4					
Bicycles	0	0	0		0	0	0		0	0	0		0	4	0		4					
Railroad																						
Stopped Buses																						

**APPENDIX**  
FDOT SEASONAL ADJUSTMENT FACTORS

MOCF: 0.93

Week	Dates	SF	PSCF
1	01/01/2014 - 01/04/2014	0.99	1.06
2	01/05/2014 - 01/11/2014	0.99	1.06
3	01/12/2014 - 01/18/2014	0.98	1.05
4	01/19/2014 - 01/25/2014	0.97	1.04
* 5	01/26/2014 - 02/01/2014	0.95	1.02
* 6	02/02/2014 - 02/08/2014	0.94	1.01
* 7	02/09/2014 - 02/15/2014	0.92	0.99
* 8	02/16/2014 - 02/22/2014	0.91	0.98
* 9	02/23/2014 - 03/01/2014	0.91	0.98
*10	03/02/2014 - 03/08/2014	0.92	0.99
*11	03/09/2014 - 03/15/2014	0.92	0.99
*12	03/16/2014 - 03/22/2014	0.92	0.99
*13	03/23/2014 - 03/29/2014	0.93	1.00
*14	03/30/2014 - 04/05/2014	0.94	1.01
*15	04/06/2014 - 04/12/2014	0.95	1.02
*16	04/13/2014 - 04/19/2014	0.96	1.03
*17	04/20/2014 - 04/26/2014	0.97	1.04
18	04/27/2014 - 05/03/2014	0.98	1.05
19	05/04/2014 - 05/10/2014	1.00	1.08
20	05/11/2014 - 05/17/2014	1.01	1.09
21	05/18/2014 - 05/24/2014	1.02	1.10
22	05/25/2014 - 05/31/2014	1.03	1.11
23	06/01/2014 - 06/07/2014	1.03	1.11
24	06/08/2014 - 06/14/2014	1.04	1.12
25	06/15/2014 - 06/21/2014	1.04	1.12
26	06/22/2014 - 06/28/2014	1.05	1.13
27	06/29/2014 - 07/05/2014	1.06	1.14
28	07/06/2014 - 07/12/2014	1.06	1.14
29	07/13/2014 - 07/19/2014	1.07	1.15
30	07/20/2014 - 07/26/2014	1.07	1.15
31	07/27/2014 - 08/02/2014	1.06	1.14
32	08/03/2014 - 08/09/2014	1.06	1.14
33	08/10/2014 - 08/16/2014	1.05	1.13
34	08/17/2014 - 08/23/2014	1.05	1.13
35	08/24/2014 - 08/30/2014	1.06	1.14
36	08/31/2014 - 09/06/2014	1.07	1.15
37	09/07/2014 - 09/13/2014	1.08	1.16
38	09/14/2014 - 09/20/2014	1.09	1.17
39	09/21/2014 - 09/27/2014	1.07	1.15
40	09/28/2014 - 10/04/2014	1.06	1.14
41	10/05/2014 - 10/11/2014	1.04	1.12
42	10/12/2014 - 10/18/2014	1.02	1.10
43	10/19/2014 - 10/25/2014	1.02	1.10
44	10/26/2014 - 11/01/2014	1.02	1.10
45	11/02/2014 - 11/08/2014	1.02	1.10
46	11/09/2014 - 11/15/2014	1.02	1.10
47	11/16/2014 - 11/22/2014	1.02	1.10
48	11/23/2014 - 11/29/2014	1.01	1.09
49	11/30/2014 - 12/06/2014	1.01	1.09
50	12/07/2014 - 12/13/2014	1.00	1.08
51	12/14/2014 - 12/20/2014	0.99	1.06
52	12/21/2014 - 12/27/2014	0.99	1.06
53	12/28/2014 - 12/31/2014	0.98	1.05

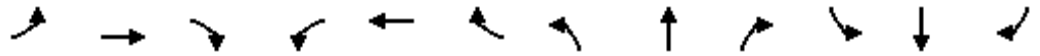
\* Peak Season

**APPENDIX**  
INTERSECTION ANALYSIS

# Timings

## 1: Jenkins Road & SR 70

6/17/2016

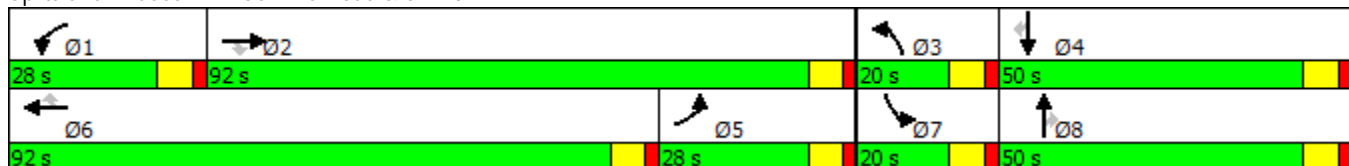


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↑↑↑	↖	↖↗	↑↑↑	↖	↖↗	↑	↖	↖↗	↑↑	↖
Traffic Volume (vph)	180	1122	149	12	595	116	181	157	15	227	132	64
Future Volume (vph)	180	1122	149	12	595	116	181	157	15	227	132	64
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	10.0	15.0	15.0	10.0	15.0	15.0	7.0	10.0	10.0	7.0	10.0	10.0
Minimum Split (s)	17.0	22.0	22.0	17.0	22.0	22.0	14.0	17.0	17.0	14.0	17.0	17.0
Total Split (s)	28.0	92.0	92.0	28.0	92.0	92.0	20.0	50.0	50.0	20.0	50.0	50.0
Total Split (%)	14.7%	48.4%	48.4%	14.7%	48.4%	48.4%	10.5%	26.3%	26.3%	10.5%	26.3%	26.3%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lag	Lag	Lag	Lead	Lead	Lead	Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	None	None	None	None	None	Max	Max	None	Max	Max
Act Effect Green (s)	13.7	36.3	36.3	10.1	18.5	18.5	11.6	43.3	43.3	13.1	44.7	44.7
Actuated g/C Ratio	0.12	0.31	0.31	0.09	0.16	0.16	0.10	0.37	0.37	0.11	0.38	0.38
v/c Ratio	0.46	0.58	0.26	0.04	0.60	0.34	0.55	0.23	0.02	0.61	0.10	0.09
Control Delay	51.8	35.5	6.0	53.3	48.7	10.7	57.7	28.3	0.1	58.2	25.6	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	51.8	35.5	6.0	53.3	48.7	10.7	57.7	28.3	0.1	58.2	25.6	0.3
LOS	D	D	A	D	D	B	E	C	A	E	C	A
Approach Delay		34.5			42.7			42.3			39.3	
Approach LOS		C			D			D			D	

### Intersection Summary

Cycle Length: 190  
 Actuated Cycle Length: 116.7  
 Natural Cycle: 70  
 Control Type: Actuated-Uncoordinated  
 Maximum v/c Ratio: 0.61  
 Intersection Signal Delay: 38.1  
 Intersection LOS: D  
 Intersection Capacity Utilization 62.7%  
 ICU Level of Service B  
 Analysis Period (min) 15

### Splits and Phases: 1: Jenkins Road & SR 70



# Timings

## 1: Jenkins Road & SR 70

6/17/2016

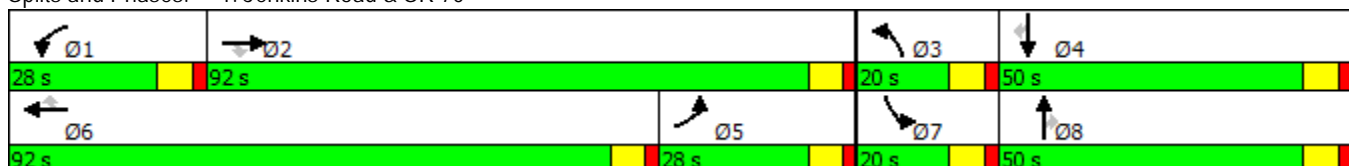


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	160	831	148	17	1174	118	144	83	12	194	125	62
Future Volume (vph)	160	831	148	17	1174	118	144	83	12	194	125	62
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	10.0	15.0	15.0	10.0	15.0	15.0	7.0	10.0	10.0	7.0	10.0	10.0
Minimum Split (s)	17.0	22.0	22.0	17.0	22.0	22.0	14.0	17.0	17.0	14.0	17.0	17.0
Total Split (s)	28.0	92.0	92.0	28.0	92.0	92.0	20.0	50.0	50.0	20.0	50.0	50.0
Total Split (%)	14.7%	48.4%	48.4%	14.7%	48.4%	48.4%	10.5%	26.3%	26.3%	10.5%	26.3%	26.3%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lag	Lag	Lag	Lead	Lead	Lead	Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	None	None	None	None	None	Max	Max	None	Max	Max
Act Effect Green (s)	12.0	47.7	47.7	10.0	35.0	35.0	10.8	43.2	43.2	12.4	44.7	44.7
Actuated g/C Ratio	0.09	0.36	0.36	0.08	0.27	0.27	0.08	0.33	0.33	0.09	0.34	0.34
v/c Ratio	0.52	0.37	0.23	0.07	0.70	0.24	0.52	0.14	0.02	0.62	0.11	0.10
Control Delay	64.0	31.9	5.7	60.1	45.6	7.1	65.5	33.7	0.1	66.9	31.8	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	64.0	31.9	5.7	60.1	45.6	7.1	65.5	33.7	0.1	66.9	31.8	0.3
LOS	E	C	A	E	D	A	E	C	A	E	C	A
Approach Delay		33.0			42.3			51.2			44.5	
Approach LOS		C			D			D			D	

### Intersection Summary

Cycle Length: 190  
 Actuated Cycle Length: 130.7  
 Natural Cycle: 70  
 Control Type: Actuated-Uncoordinated  
 Maximum v/c Ratio: 0.70  
 Intersection Signal Delay: 39.8  
 Intersection LOS: D  
 Intersection Capacity Utilization 62.8%  
 ICU Level of Service B  
 Analysis Period (min) 15

### Splits and Phases: 1: Jenkins Road & SR 70



# Timings

## 1: Jenkins Road & SR 70

6/17/2016

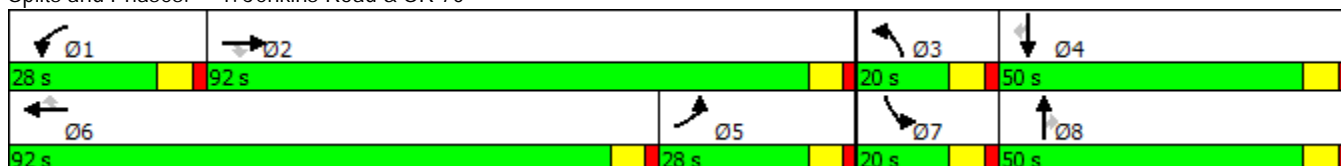


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	175	1141	158	40	604	122	199	161	15	227	137	62
Future Volume (vph)	175	1141	158	40	604	122	199	161	15	227	137	62
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	10.0	15.0	15.0	10.0	15.0	15.0	7.0	10.0	10.0	7.0	10.0	10.0
Minimum Split (s)	17.0	22.0	22.0	17.0	22.0	22.0	14.0	17.0	17.0	14.0	17.0	17.0
Total Split (s)	28.0	92.0	92.0	28.0	92.0	92.0	20.0	50.0	50.0	20.0	50.0	50.0
Total Split (%)	14.7%	48.4%	48.4%	14.7%	48.4%	48.4%	10.5%	26.3%	26.3%	10.5%	26.3%	26.3%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lag	Lag	Lag	Lead	Lead	Lead	Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	None	None	None	None	None	Max	Max	None	Max	Max
Act Effect Green (s)	21.4	34.4	34.4	10.1	19.2	19.2	12.4	43.2	43.2	13.1	43.9	43.9
Actuated g/C Ratio	0.17	0.28	0.28	0.08	0.15	0.15	0.10	0.35	0.35	0.10	0.35	0.35
v/c Ratio	0.31	0.67	0.29	0.15	0.63	0.36	0.60	0.26	0.02	0.65	0.11	0.10
Control Delay	46.6	42.7	6.6	57.8	53.1	10.9	63.3	32.5	0.1	64.6	29.8	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	46.6	42.7	6.6	57.8	53.1	10.9	63.3	32.5	0.1	64.6	29.8	0.3
LOS	D	D	A	E	D	B	E	C	A	E	C	A
Approach Delay		39.3			46.6			47.6			44.0	
Approach LOS		D			D			D			D	

### Intersection Summary

Cycle Length: 190  
 Actuated Cycle Length: 125  
 Natural Cycle: 70  
 Control Type: Actuated-Uncoordinated  
 Maximum v/c Ratio: 0.67  
 Intersection Signal Delay: 42.8  
 Intersection LOS: D  
 Intersection Capacity Utilization 63.2%  
 ICU Level of Service B  
 Analysis Period (min) 15

### Splits and Phases: 1: Jenkins Road & SR 70



# Timings

## 1: Jenkins Road & SR 70

6/17/2016

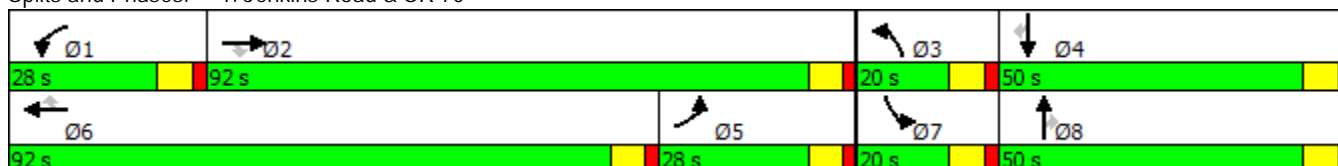


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↑↑↑	↖	↖↗	↑↑↑	↖	↖↗	↑	↖	↖↗	↑↑	↖
Traffic Volume (vph)	155	852	156	44	1172	123	176	89	12	198	131	60
Future Volume (vph)	155	852	156	44	1172	123	176	89	12	198	131	60
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	10.0	15.0	15.0	10.0	15.0	15.0	7.0	10.0	10.0	7.0	10.0	10.0
Minimum Split (s)	17.0	22.0	22.0	17.0	22.0	22.0	14.0	17.0	17.0	14.0	17.0	17.0
Total Split (s)	28.0	92.0	92.0	28.0	92.0	92.0	20.0	50.0	50.0	20.0	50.0	50.0
Total Split (%)	14.7%	48.4%	48.4%	14.7%	48.4%	48.4%	10.5%	26.3%	26.3%	10.5%	26.3%	26.3%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lag	Lag	Lag	Lead	Lead	Lead	Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	None	None	None	None	None	Max	Max	None	Max	Max
Act Effect Green (s)	11.8	40.5	40.5	10.0	35.0	35.0	11.8	43.2	43.2	12.5	43.8	43.8
Actuated g/C Ratio	0.09	0.31	0.31	0.08	0.27	0.27	0.09	0.33	0.33	0.10	0.34	0.34
v/c Ratio	0.52	0.44	0.27	0.17	0.70	0.25	0.58	0.15	0.02	0.62	0.11	0.10
Control Delay	63.9	37.4	5.9	60.8	45.5	7.1	66.1	33.7	0.1	66.9	32.2	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	63.9	37.4	5.9	60.8	45.5	7.1	66.1	33.7	0.1	66.9	32.2	0.3
LOS	E	D	A	E	D	A	E	C	A	E	C	A
Approach Delay		36.7			42.5			52.9			44.9	
Approach LOS		D			D			D			D	

### Intersection Summary

Cycle Length: 190  
 Actuated Cycle Length: 130.5  
 Natural Cycle: 70  
 Control Type: Actuated-Uncoordinated  
 Maximum v/c Ratio: 0.70  
 Intersection Signal Delay: 41.6  
 Intersection LOS: D  
 Intersection Capacity Utilization 62.8%  
 ICU Level of Service B  
 Analysis Period (min) 15

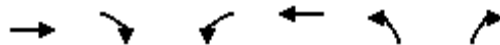
### Splits and Phases: 1: Jenkins Road & SR 70



# HCM Unsignalized Intersection Capacity Analysis

## 6: Driveway A & SR 70

6/17/2016



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗		↑↑		↗
Traffic Volume (veh/h)	1324	74	0	766	0	79
Future Volume (Veh/h)	1324	74	0	766	0	79
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97
Hourly flow rate (vph)	1365	76	0	790	0	81
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)	640					
pX, platoon unblocked			0.81		0.81	0.81
vC, conflicting volume			1441		1760	682
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			1064		1460	122
tC, single (s)			4.1		6.8	6.9
tC, 2 stage (s)						
tF (s)			2.2		3.5	3.3
p0 queue free %			100		100	89
cM capacity (veh/h)			524		97	730
Direction, Lane #	EB 1	EB 2	EB 3	WB 1	WB 2	NB 1
Volume Total	682	682	76	395	395	81
Volume Left	0	0	0	0	0	0
Volume Right	0	0	76	0	0	81
cSH	1700	1700	1700	1700	1700	730
Volume to Capacity	0.40	0.40	0.04	0.23	0.23	0.11
Queue Length 95th (ft)	0	0	0	0	0	9
Control Delay (s)	0.0	0.0	0.0	0.0	0.0	10.5
Lane LOS						
Approach Delay (s)	0.0			0.0		10.5
Approach LOS						
B						
Intersection Summary						
Average Delay			0.4			
Intersection Capacity Utilization			48.2%		ICU Level of Service	
Analysis Period (min)			15			
			A			

# HCM Unsignalized Intersection Capacity Analysis

## 8: Jenkins Road & Driveway B

6/17/2016

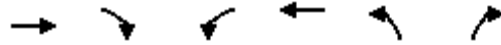


Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	15	31	344	16	35	283
Future Volume (Veh/h)	15	31	344	16	35	283
Sign Control	Stop		Free		Free	
Grade	0%		0%		0%	
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97
Hourly flow rate (vph)	15	32	355	16	36	292
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None		None	
Median storage (veh)						
Upstream signal (ft)						450
pX, platoon unblocked	0.98					
vC, conflicting volume	581	186			371	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	541	186			371	
tC, single (s)	6.8	6.9			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	97	96			97	
cM capacity (veh/h)	450	825			1184	
Direction, Lane #	WB 1	WB 2	NB 1	NB 2	SB 1	SB 2
Volume Total	15	32	237	134	133	195
Volume Left	15	0	0	0	36	0
Volume Right	0	32	0	16	0	0
cSH	450	825	1700	1700	1184	1700
Volume to Capacity	0.03	0.04	0.14	0.08	0.03	0.11
Queue Length 95th (ft)	3	3	0	0	2	0
Control Delay (s)	13.3	9.5	0.0	0.0	2.4	0.0
Lane LOS	B	A			A	
Approach Delay (s)	10.7		0.0		1.0	
Approach LOS	B					
Intersection Summary						
Average Delay			1.1			
Intersection Capacity Utilization			32.2%		ICU Level of Service	
Analysis Period (min)			15			
			A			

# HCM Unsignalized Intersection Capacity Analysis

## 6: Driveway A & SR 70

6/17/2016



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑		↑↑		↑
Traffic Volume (veh/h)	1002	74	0	1366	0	92
Future Volume (Veh/h)	1002	74	0	1366	0	92
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97
Hourly flow rate (vph)	1033	76	0	1408	0	95
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)	640					
pX, platoon unblocked			0.87		0.87	0.87
vC, conflicting volume			1109		1737	516
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			817		1542	132
tC, single (s)			4.1		6.8	6.9
tC, 2 stage (s)						
tF (s)			2.2		3.5	3.3
p0 queue free %			100		100	88
cM capacity (veh/h)			699		92	773
Direction, Lane #	EB 1	EB 2	EB 3	WB 1	WB 2	NB 1
Volume Total	516	516	76	704	704	95
Volume Left	0	0	0	0	0	0
Volume Right	0	0	76	0	0	95
cSH	1700	1700	1700	1700	1700	773
Volume to Capacity	0.30	0.30	0.04	0.41	0.41	0.12
Queue Length 95th (ft)	0	0	0	0	0	10
Control Delay (s)	0.0	0.0	0.0	0.0	0.0	10.3
Lane LOS						B
Approach Delay (s)	0.0				0.0	10.3
Approach LOS						B
Intersection Summary						
Average Delay			0.4			
Intersection Capacity Utilization			41.1%	ICU Level of Service		A
Analysis Period (min)	15					

# HCM Unsignalized Intersection Capacity Analysis

## 8: Jenkins Road & Driveway B

6/17/2016



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	18	46	231	16	50	278
Future Volume (Veh/h)	18	46	231	16	50	278
Sign Control	Stop		Free		Free	
Grade	0%		0%		0%	
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97
Hourly flow rate (vph)	19	47	238	16	52	287
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None		None	
Median storage (veh)						
Upstream signal (ft)						450
pX, platoon unblocked	0.98					
vC, conflicting volume	494	127			254	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	447	127			254	
tC, single (s)	6.8	6.9			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	96	95			96	
cM capacity (veh/h)	509	900			1308	
Direction, Lane #	WB 1	WB 2	NB 1	NB 2	SB 1	SB 2
Volume Total	19	47	159	95	148	191
Volume Left	19	0	0	0	52	0
Volume Right	0	47	0	16	0	0
cSH	509	900	1700	1700	1308	1700
Volume to Capacity	0.04	0.05	0.09	0.06	0.04	0.11
Queue Length 95th (ft)	3	4	0	0	3	0
Control Delay (s)	12.3	9.2	0.0	0.0	3.0	0.0
Lane LOS	B	A			A	
Approach Delay (s)	10.1		0.0		1.3	
Approach LOS	B					
Intersection Summary						
Average Delay			1.7			
Intersection Capacity Utilization			29.4%		ICU Level of Service	
Analysis Period (min)			15			
			A			

**APPENDIX**  
TURN LANE WARRANTS

## 7.2

### WHEN SHOULD WE BUILD RIGHT TURN LANES?

**Exhibit 44**  
Recommended Guidelines  
for Exclusive Right Turn  
Lanes to Unsignalized\*  
Driveway

Roadway Posted Speed Limit	Number of Right Turns Per Hour
45 mph or less	<b>80-125</b> (see note 1)
Over 45 mph	<b>35-55</b> (see note 2)

\*May not be appropriate for signalized locations where signal phasing plays an important role in determining the need for right turn lanes.

1. The lower threshold of 80 right turn vehicles per hour would be most used for higher volume (greater than 600 vehicles per hour, per lane in one direction on the major roadway) or two-lane roads where lateral movement is restricted. The 125 right turn vehicles per hour upper threshold would be most appropriate on lower volume roadways, multilane highways, or driveways with a large entry radius (50 feet or greater).
2. The lower threshold of 35 right turn vehicles per hour would be most appropriately used on higher volume two-lane roadways where lateral movement is restricted. The 55 right turn vehicles per hour upper threshold would be most appropriate on lower volume roadways, multilane highways, or driveways with large entry radius (50 feet or greater).

**Note:** A posted speed limit of 45 mph may be used with these thresholds if the operating speeds are known to be over 45 mph during the time of peak right turn demand.

**Note on Traffic projections:** Projecting turning volumes is, at best, a knowledgeable estimate. Keep this in mind especially if the projections of right turns are close to meeting the guidelines. In that case, consider requiring the turn lane.

## Where The Right Turn Lane Guidelines Came From

These recommendations are primarily based on the research done in ***NCHRP Report 420, Impacts of Access Management Techniques***, Chapter 4 – Unsignalized Access Spacing (Technique 1B), and *Use of Speed Differential as a Measure To Evaluate the Need for Right-Turn Deceleration Lane at Unsignalized Intersections*, by Jan Thakkar, P.E., and Mohammed A. Hadi, Ph.D., P.E.

In the ***NCHRP Report 420***, the observed high-speed roads, 30 to 40 right turn vehicles per hour caused evasive maneuvers on 5 to 10 percent of the following through vehicles. For lower speed roadways, 80 to 110 right turn vehicles caused 15 to 20 percent of the following through vehicles to make evasive maneuvers. The choice of acceptable percentages of through vehicles impacted is a decision based on reasonable expectations of the different roadways.

In the Thakkar-Hadi study, by modeling speed differentials, a better understanding of the impacts of through volume and driveway radius was discovered.

### 7.3

#### IMPACT OF LARGE AND SLOW MOVING VEHICLES TURNING RIGHT



Speed and the volume of right turns should not be the only criteria used to determine the requirement for an exclusive right turn lane at unsignalized intersections. In order to minimize the rear-end collision potential of some situations, a right turn lane may be required where large and slow moving vehicles need to turn right such as;

- Trucking facilities (or locations that have a high volume of large vehicle traffic such as water ports, train stations, etc.)
- Recreational facilities attracting boats, trailers and other large recreation vehicles
- Transit facilities
- Schools

SOUTHBOUND LEFT TURN LANE - PM

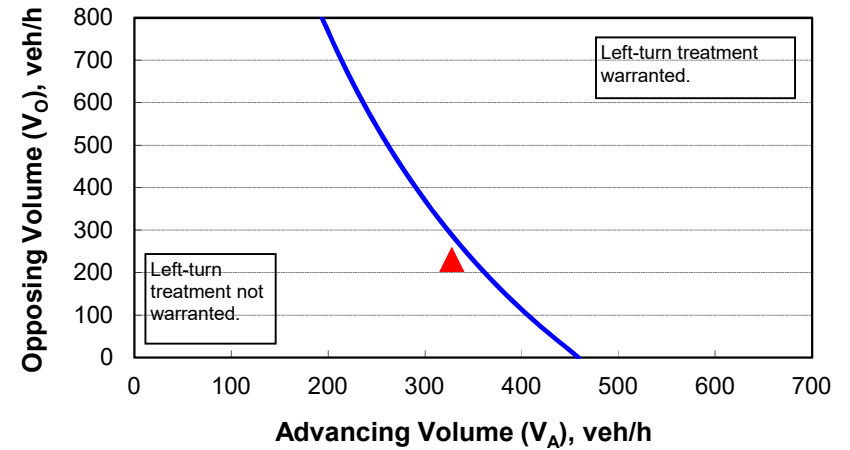
2-lane roadway (English)

INPUT

Variable	Value
85 <sup>th</sup> percentile speed, mph:	45
Percent of left-turns in advancing volume ( $V_A$ ), %:	15%
Advancing volume ( $V_A$ ), veh/h:	328
Opposing volume ( $V_O$ ), veh/h:	231

OUTPUT

Variable	Value
Limiting advancing volume ( $V_A$ ), veh/h:	349
<b>Guidance for determining the need for a major-road left-turn bay:</b>	
<b>Left-turn treatment NOT warranted.</b>	



CALIBRATION CONSTANTS

Variable	Value
Average time for making left-turn, s:	3.0
Critical headway, s:	5.0
Average time for left-turn vehicle to clear the advancing lane, s:	1.9

SOUTHBOUND LEFT TURN LANE - AM

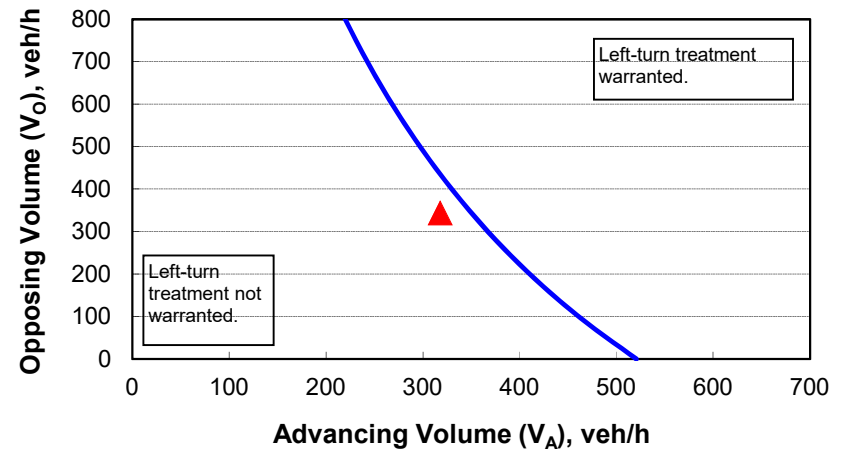
2-lane roadway (English)

INPUT

Variable	Value
85 <sup>th</sup> percentile speed, mph:	45
Percent of left-turns in advancing volume ( $V_A$ ), %:	11%
Advancing volume ( $V_A$ ), veh/h:	318
Opposing volume ( $V_O$ ), veh/h:	344

OUTPUT

Variable	Value
Limiting advancing volume ( $V_A$ ), veh/h:	350
<b>Guidance for determining the need for a major-road left-turn bay:</b>	
<b>Left-turn treatment NOT warranted.</b>	



CALIBRATION CONSTANTS

Variable	Value
Average time for making left-turn, s:	3.0
Critical headway, s:	5.0
Average time for left-turn vehicle to clear the advancing lane, s:	1.9

**APPENDIX**  
FDOT STANDARD INDEX 301

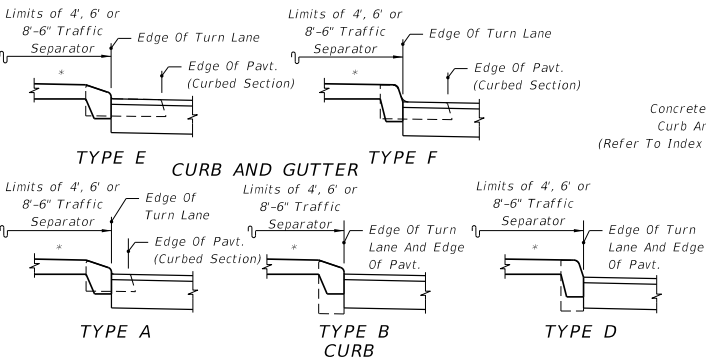
TURN LANES • CURBED AND UNCURBED MEDIANS								
Design Speed (mph)	Entry Speed (mph)	Clearance Distance $L_1$	URBAN CONDITIONS			RURAL CONDITIONS		
			Brake To Stop Distance $L_2$	Total Decel. Distance $L$	Clearance Distance $L_3$	Brake To Stop Distance $L_2$	Total Decel. Distance $L$	Clearance Distance $L_3$
35	25	70'	75'	145'	110'	—	—	—
40	30	80'	75'	155'	120'	—	—	—
45	35	85'	100'	185'	135'	—	—	—
50	40/44	105'	135'	240'	160'	185'	290'	160'
55	48	125'	—	—	—	225'	350'	195'
60	52	145'	—	—	—	260'	405'	230'
65	55	170'	—	—	—	290'	460'	270'

**DESIGN NOTES**

- Basis for turn lane configurations:
  - Informed Driver.
  - Stop condition (With Or Without Stop Control).
  - Wet Pavement.
  - Reaction preceding entry point.
  - Minimum braking distance for urban conditions.
  - 75' min. for  $L_2$ .
  - Comfortable deceleration rates for rural conditions (AASHTO 2001 threshold rate of 11.2 ft./s<sup>2</sup>).

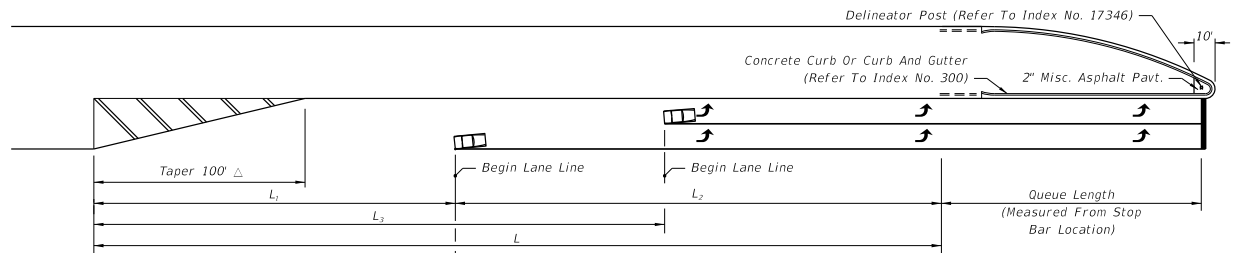
**GENERAL NOTES**

- The plan views shown are for turn lane taper shapes and dimensional purposes only, they do not prescribe the use of curb, curb and gutter, shoulders nor separators specifically to either rural or urban conditions.
- Total deceleration distances must not be reduced except where lesser values are imposed by unrelocatable control points.
- Right turn lane tapers and distances identical to left turn lanes under stop control conditions. Right turn lane tapers and/or distances are site specific under free flow or yield conditions.
- These left turn configurations apply to continuous left turn lanes only where specifically called for in the plans.
- For pavement markings see Index No. 17346.



For Curb And Curb & Gutter Types, See Index No. 300  
 \* Option 1 Separators Shown (Refer To Index No. 302)

**MEDIAN CURB AND TRAFFIC SEPARATOR JUNCTURE DETAILS**

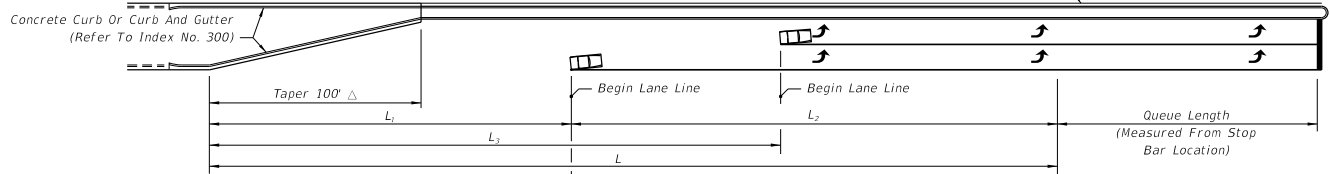


Brakes Applied After Turning Vehicle Clears Through Lane;

**FLUSH AND/OR CURBED SEPARATION**

Entry Speed:  
 10 mph Below Design Speed For Urban Condition  
 Average Running Speed For Rural Condition

Traffic Separator (Refer To Index No. 302)

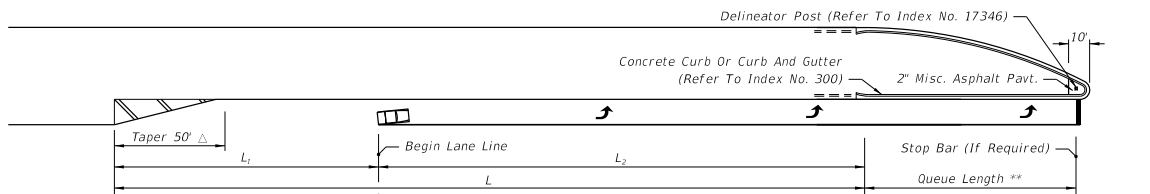


Brakes Applied After Turning Vehicle Clears Through Lane;

**RAISED SEPARATION**

Entry Speed:  
 10 mph Below Design Speed For Urban Condition  
 Average Running Speed For Rural Condition

**DOUBLE LEFT TURNS**



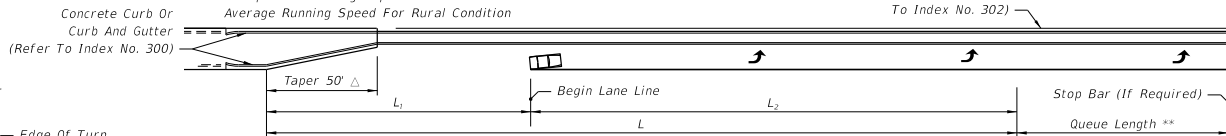
Brakes Applied After Turning Vehicle Clears Through Lane;

**FLUSH AND/OR CURBED SEPARATION**

Entry Speed:  
 10 mph Below Design Speed For Urban Condition  
 Average Running Speed For Rural Condition

Traffic Separator (Refer To Index No. 302)

\*\* Queue Length Is Measured From The Median Nose Radial Point Or, When A Stop Bar Is Required, From The Stop Bar.



Brakes Applied After Turning Vehicle Clears Through Lane;

**RAISED SEPARATION**

Entry Speed:  
 10 mph Below Design Speed For Urban Condition  
 Average Running Speed For Rural Condition

**SINGLE LEFT TURNS**

- Δ The length of taper may be increased to  $L_1$  for single left turns and  $L_2$  for double left turns when:
- Left turn queue vehicles are adequately provided for within the design queue length.
  - Through vehicle queues will not block access to left turn lane.
  - Approved by District Design Engineer.

LAST REVISION 07/01/05	DESCRIPTION:	<p>FDOT 2014 DESIGN STANDARDS</p>	<p>TURN LANES</p>	<p>INDEX NO. 301</p>	<p>SHEET NO. 1 of 1</p>
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**TECHNICAL REVIEW COMMITTEE (TRC) COMMENTS FORM**

**FROM : Marc Meyers**

**RE : WaWa Gas Station – 5575 Okeechobee Rd.**

**DATE : 7/21/16**

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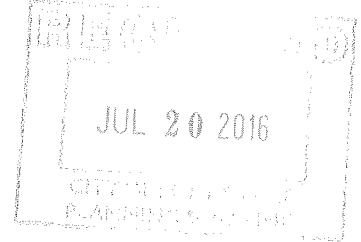
Items needing to be addressed are:

1. Fort Pierce Building and SLC P& Z front page
2. Demo of existing building?
3. Grease trap/interceptor?
4. Fuel tank separate permit
5. Dumpster separate permit – submitted
6. Canopy separate permit – submitted
7. Main building separate permit – submitted
8. Site work

THE SUNRISE CITY  
**FORT PIERCE**  
*Florida*

**Engineering Department**  
100 North US Highway 1  
Fort Pierce, FL 34950  
Tel: 772.467.3774  
Fax: 772.460.6847  
www.cityoffortpierce.com

**To** : Kori Benton, Senior Planner  
**FROM** : John R. Andrews, P.E., City Engineer *JA*  
**RE** : Wawa Gas Station – 5575 Okeechobee Road  
TRC No. 16-07000002  
**DATE** : July 14, 2016



This is to advise you that we have completed the review of the following documents as received by this office on July 7, 2016:

- |   |  |
|---|--|
| <input checked="" type="checkbox"/> Site Plan   | <input type="checkbox"/> Engineering Drawings                                    |
| <input type="checkbox"/> Test Reports & Related Documents                                 | <input type="checkbox"/> Certificate of Completion                               |
| <input type="checkbox"/> Record Drawings  | <input type="checkbox"/> Permits from applicable Local, State & Federal Agencies |
| <input type="checkbox"/> Clearances from all applicable Local, State and Federal Agencies |  |

Based on our reviews and appropriate site final inspection, we  Recommend  Do Not Recommend

- Approval of Site Plan       Building Permit       C/O

Developer, Owner, Engineer, Contractor and other members of the Development Team must be aware, the above recommendation is based only on the construction requirements of the engineering plans and other engineering documentation approved by this department. The Development Team shall be responsible for the compliance with other City department requirements and all approved documents, as well as Local, State and Federal regulations. The development requirements for this project may necessitate additional construction requirements that are not subject to this department's review for approval.

See attached for Site Plan comments to be addressed at time of Building Permit Application



ENGINEERING COMMENTS:

1. Cover Sheet C-1 shall be revised to identify City of Fort Pierce as the governing agency and not St. Lucie County under the headings of Planning and Zoning, Building, and Storm Drainage.
2. The plans shall identify the ADA detectable warning mats, located within the public road right-of-ways, as being brick red in color.
3. The engineering plans shall be revised to reflect all on-site drainage pipe as being comprised of either reinforced concrete pipe or aluminum pipe as stipulated in the City of Fort Pierce Code of Ordinances Section 17-28(b).
4. The Grading, Paving, and Drainage Plans have been conceptually reviewed and will be subjected to a more in-depth review at time of submittal for the Site Work Permit.

JRA/TST



## Planning Department

100 North US Highway 1

Fort Pierce, FL 34950

Tel: 772.467.3000

www.cityoffortpierce.com

Scott Kerney P.E.  
Engineer  
WaWa Inc.  
7699 Golf Channel Drive  
Orlando, FL 32819

**Re: Design & Site Plan Review – Filling Station in C-3 Zoning District  
5575 Okeechobee Road  
TRC No. 16-07000002**

Dear Mr. Kerney,

The following are comments from the Planning Department's review of the application for Design and Site Plan Review of a Filling Station in the C-3 General Commercial District:

- 1) Per City Code Section 22-187. (10) Other areas. When an area other than a vehicular use and retention/detention area of a developed lot in a C-1, C-2, *C-3*, C-4, C-5, I-1, or I-2 zone abuts a lot in an E-1, R-1, R-2, R-3, *R-4*, R-5, OS-1, or OS-2, such area in a **commercial or industrial zone shall have a site obscuring fence or planted material so as to provide a visual and noise buffer between such areas and the lot in the residential or open space zone.** Such fence will be constructed from wood, stone, brick or other suitable material and be a minimum of six (6) feet high. If planted material is used, it shall be planted and maintained so as to form a thirty-six-inch or higher continuous, unbroken solid screen. There shall be at least one shrub, bush or vine planted along the fence for each ten (10) feet of fence for the purpose of beautifying the fence. Slats shall not be put into chain-link fence to obscure the view. The planted material shall be a species which in the county normally grows to a height of six (6) feet or more. **Please integrate the appropriate buffering along the southern property line accordingly.****
- 2) Per City Code Section 22-178. (10) The Landscape Plan shall include tabulations which clearly show relevant statistical information necessary to evaluate compliance with provisions of this chapter. This shall include, but not be limited to, required buffers, **vehicular use landscaping**/screening, non-vehicular landscaping, and such other information as needed. **Specifically, please identify landscape areas included in the tabulation of interior vehicular use area landscaping pursuant to City Code Section 22-187 (7) as referenced in page 3 of the Landscape Plan, which lists 4,936 SF of interior vehicular use landscaping.****
- 3) Per City Code Section 22-58 (d)(2) Site Plans shall include a general location map which shows the approximate location of streets, street signals and vehicular access points to streets along streets abutting the proposed development, rights-of-way, zoning districts, existing land uses and important physical features (including drainage ways) within five hundred**

(500) feet of property proposed for development. **Please update the general location map to clearly display zoning districts, existing land uses, and important physical features (including drainage ways) within five hundred (500) feet of property proposed for development.**

- 4) Per City Code Section 22-59 (g)(4) b. Elevations adjacent to a public right-of-way shall be considered as a building front and treated as such with appropriate entrances, fenestration or detailing & c. Articulation in the facade should give visual relief and be of sufficient depth to provide shade and shadow on adjacent building surfaces. Colors of wall surfaces shall be chosen to complement this facade articulation and colors shall be in keeping with the chosen architectural style and character. **Please consider integrating a minor awning/canopy over the rear door of the right elevation.**
- 5) Per City Code Section 22-31(c) (4) Materials or objects which would detract from the open space character of an uncovered or unenclosed area will not be permitted in such an area. **Please consider an architectural feature such as an awning or canopy to cover the proposed ice container outside of the establishment.**
- 6) Per City Code Section 22-59 (g)(4)j. Exterior building components and all proposed elements of the streetscape shall be painted with a color compatible with the architectural character and style of the proposed development as well as the surrounding buildings. **Please consider an architectural feature to screen the rooftop mechanical components, or consider painting for appropriate assimilation with design.**
- 7) Per City Code Section 22-187 (1)a. Trees used to meet the requirements of this section shall be species which when planted have a height of at least twelve (12) feet and have trunks which can be maintained in a clean condition for over five (5) feet of clear wood. At planting, the trees shall have a diameter of at least two and one-half (2½) inches at a point four and one-half (4½) feet above ground level and a spread of at least five (5) feet (except for **palms which shall have a minimum clear trunk of ten (10) feet**). **Please update the landscape plan to indicate that all palms provided on site will have a minimum of clear trunk height of ten (10) ft.**
- 8) Per City Code Section 22-59 (c)(1)i. the design review submittal shall include an accurate color rendering of proposed signs showing dimensions, type of lettering, materials and actual color samples that demonstrates cohesiveness with the project design. **Please provide an exhibit of the proposed ground sign (s).**

10) Pursuant to City Code Section 22-62(d) *Where required:* Sidewalks and sidewalk linkages shall be installed in the following areas:

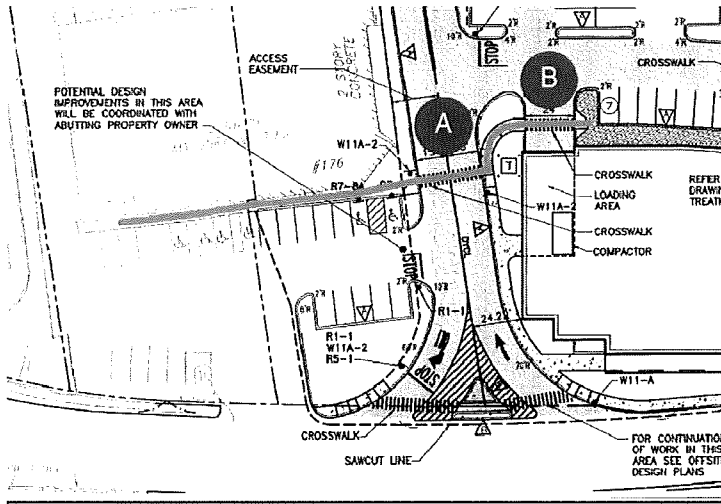
- (2) Safe and efficient sidewalk linkages shall be provided between building entrances and parking areas, and adjacent portions of the development, and adjacent rights-of-way.



Example of connectivity to parking lot ( <b>Partially Provided</b> )	Example of connectivity to the street ( <b>Provided</b> )	Example of connectivity between buildings ( <b>N/A</b> )
--	---	--

Please integrate applicable linkages from parking areas to building entrances, where applicable/not already present.

- Pursuant to City Code Section 22-62(d) (3) In addition to the requirements of subsection (2), internal sidewalk interconnection between adjacent properties shall be provided for properties that front on arterial roads.

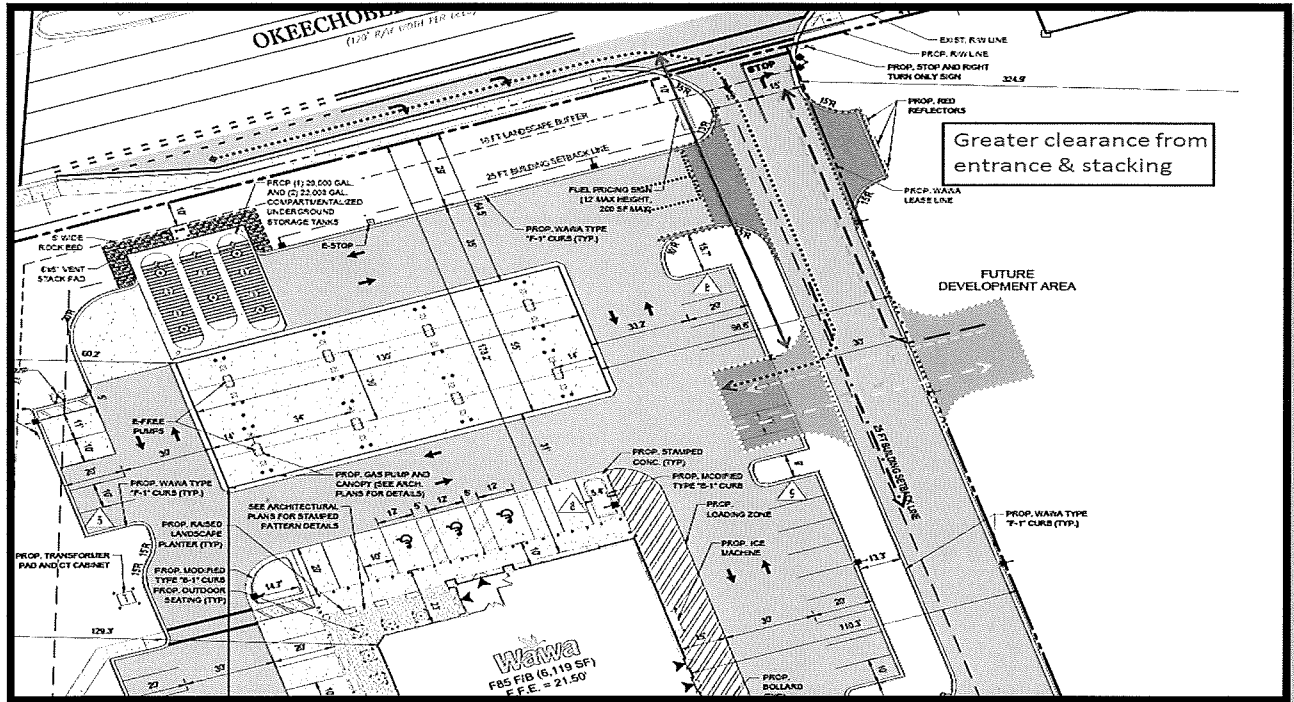


Example of internal sidewalk connecting from property A to property B

Please integrate or ensure a plan to integrate internal sidewalk connection between the site and adjacent "future" development to the east as noted.

**Advisory notes:**

- Please consider adjustment of the north west entry and shared driveway to provide greater clearance from the entry from Okeechobee Road, increase stacking area, and minimization of internal intersection conflicts, as considered below:



- Please consider a design enhancement to the various bollards across the site such as cap elements, or minor lighting.
- Per discussions with the applicable sign representatives, it's noted that a second ground sign will require a variance.
- Please be cautious of sight lines/clear vision areas at intersections of driveways with other internal driveways.
- Per City Code Section 22-60 (j) (4) Outdoor lighting required by this section shall be designed so that any over spill of lighting onto adjacent properties shall not exceed five-tenths (.5) footcandle illumination, vertically and horizontally. The landscaping along the southern property line may account for any overages, but please verify.
- Please note that City Code Section 22-59 (6) guides use of a. Decorative, low-level intensity, non-concealed source lighting which defines vehicular and/or pedestrian ways may be acceptable if not used as general lighting for a development. & b. that exterior architectural, display and decorative lighting visible from all public rights-of-way shall be generated from concealed light source, low-level light fixtures. Color lamps shall not be used.

If deemed necessary, please provide a written response to each comment in order to expedite the review of any subsequent submittals. Please contact me should you have any questions regarding the project at (772) 467-3741 or by e-mail: [vgilmore@city-ftpierce.com](mailto:vgilmore@city-ftpierce.com).

Sincerely,



Vennis Gilmore  
Planning Analyst

**Planning Board**

**7. c.**

Meeting Date: 08/09/2016

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Information

REQUESTED ACTION

Conceptual Development Plan - Interstate Business Park - 2496 S. Kings Highway

LOCATION

2496 S. Kings Highway

RESPONSIBLE STAFF

Kori Benton, Senior Planner

RECOMMENDATION

Approval

---

Attachments

Staff Report

Aerial & Zoning Map

Application Packet - Conceptual Development Plans

TRC Comments

---

**Form Review**

Form Started By: Kori Benton

Started On: 08/02/2016 11:26 AM

Final Approval Date: 08/03/2016



# CITY OF FORT PIERCE

## PLANNING DEPARTMENT

REBECCA GROHALL, AICP, PLANNING DIRECTOR  
COMPREHENSIVE PLANNING ◊ DEVELOPMENT REVIEW  
HISTORIC PRESERVATION ◊ URBAN DESIGN ◊ ZONING

---

TO: Members of the City of Fort Pierce Planning Board

THROUGH: Rebecca Grohall, AICP, Planning Director

FROM: Kori Benton, Senior Planner

RE: Conceptual Development Plan  
Interstate Business Park  
2496 S. Kings Highway

DATE: August 2, 2016

---

### STAFF REPORT

Owner/ Applicant: Kings Highway Commercial South LLC  
340 S US Hwy 1 #607  
Jupiter, FL 33477

Representative: M. Troy Holloway, ASLA, LEED AP, BD&C  
Gentile, Glas, Holloway, O'Mahoney & Associates, Inc.  
1907 Commerce Lane, Suite 101  
Jupiter, Florida 33458

Requested Action: Approvals of a Conceptual Development Plan

Location: 2496 S. Kings Highway

Parcel IDs: 2324-232-0000-000-4

Zoning: C-3, General Commercial

Surrounding Zoning:

North	East	South	West
C-3	RS-2 (SLC)	C-3	R-4

Future Land Use: GC, General Commercial

Parcel Size: ~24.04 acres

Utilities: FPUA Water/Wastewater – Split Electric Service FPUA/FPL

Per Section 22-58(e) of the City Code, the applicant is requesting the review and approval of a Conceptual Development Plan. Section 22-58(e) of the City Code provides:

Prior to, but not in lieu of, submission of a complete application and site plan the applicant may, solely at his own election, adopt the following procedure:

- (1) A conceptual development plan may be submitted to the department of planning and development, which shall schedule a meeting for its consideration by the city planning board. The conceptual development plan shall include a map showing the general layout of the proposed development, statements describing in detail the character and intended use of the development and information bearing on the ownership and control of the proposed use;
- (2) The city planning board shall review the conceptual development plan and make recommendations as appropriate for its approval or disapproval;
- (3) The city commission shall hold a hearing and approve or disapprove the conceptual development plan or approve it with conditions.

#### Staff Analysis:

The applicant is requesting review of a Conceptual Development Plan for a proposed commerce park to be located at 2496 S. Kings Highway. The overall project includes 259,300 square feet of floor space, distributed between 12 buildings with intended use for office, retail, and warehouse purposes.

The undeveloped parcel is located between Kings Highway and Peters Road, south of White Road, containing 24.05 acres. The property is zoned C-3, General Commercial with a Future Land Use of GC, General Commercial. The C-3 zoning district does not allow warehouse establishments without conditional use approval, therefore a subsequent development review request with warehousing or limited industrial users will require a Conditional Use request. The property previously received Site Plan and Conditional Use approval, in 2007, for a similar development proposal.

The property to the north is vacant and zoned C-3, General Commercial. The properties to the east are also predominantly vacant and located in unincorporated St. Lucie County. The property to the southeast of the subject parcel is zoned C-3, General Commercial and occupied by a recreational vehicle park. The vacant parcel to the southwest is vacant and zoned C-3, General Commercial. The property across Kings Highway to the west is Zoned R-4, Medium Density Residential and currently vacant.

The Florida Department of Transportation (FDOT), St. Lucie County, and the City of Fort Pierce are engaged in a collaborative improvement project for Kings Highway which would expand and significantly enhance this abutting roadway network.

Access to the development will consist of two entrances, one located on Kings Highway and the other located on Peters Road. An internal roadway that extends east/west through the development will enable vehicles to access both roads for improved traffic distribution. In addition, dedicated turn lanes are reserved for the development to allow vehicles heading north and south on Kings Highway improved access to the development. The segment of Peters Road near the proposed development is substandard; therefore further consideration of improvements to the affected segment of Peters Road will be explored further with the St. Lucie County Engineering Department as the time of formal development review.

A traffic study will be required for formal development review application; however the scale of the project should generate approximately 2,500-3,500 daily trips upon full build-out. A complete analysis, accounting for revisions to adjacent roadways and concurrency will be required.

The conceptual plan accounts for code required parking, lighting, and landscaping to meet the requirements of the City Code, with the note that additional landscape and buffering will be required along the proposed eastern access road and project boundaries abutting residentially zoned property.

Storm water retention is presented to be managed by a wet-water retention lake, central to the south end of the project.

The project would be subject to compliance with the City's design review guidelines.

All affected Departments have reviewed the submittals and provided comments regarding the proposed conceptual development based on compliance with the requirements of the City Code. The comments generated are attached for review by the Board.

Staff Comments:

The proposed Conceptual Development Plan generally meets the requirements of the City Code, and is conceptually consistent with the City's Comprehensive Plan; therefore Staff recommends that Planning Board forward a recommendation of approval to the City Commission.



Kings Highway

Peters Road

Crossroads Parkway

Ronald Reagan Turnpike

Okeechobee Road

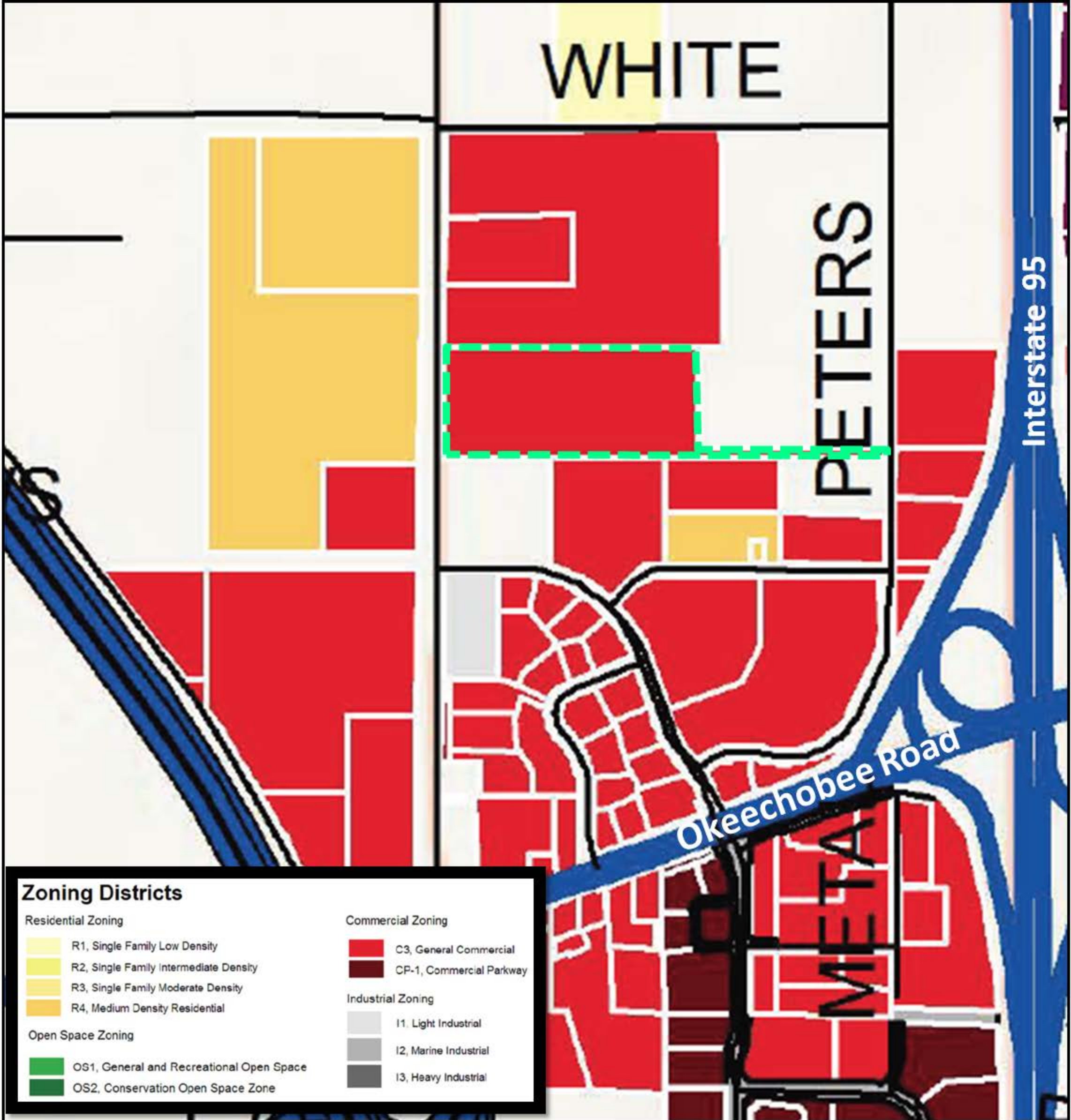
Interstate 95



# 2496 Kings Highway

Site Aerial







# CITY OF FORT PIERCE

## PLANNING DEPARTMENT

COMPREHENSIVE PLANNING ◊ DEVELOPMENT REVIEW  
HISTORIC PRESERVATION ◊ URBAN DESIGN ◊ URBAN REDEVELOPMENT ◊ ZONING

### DEVELOPMENT REVIEW

Property address or Location 2496 S. Kings Highway - Interstate Business Park

Parcel ID #(s) 2324-232-0000-000-4

Project description The Applicant is proposing a conceptual development site plan on 24.05 acres of land in the City of Fort Pierce.

Kings Highway Commercial South LLC

**Property Owner(s)**

340 South U.S. Highway One

**Street Address**

Jupiter FL 3347

**City State Zip**

**Phone Number**

**Email Address**

M. Troy Holloway/Gentile Glas Holloway O'Mahoney & Associates, Inc. (2GHO)

**Applicant/Representative, Title, Company**

1907 Commerce Lane Suite 101

**Street Address**

Jupiter FL 33458

**City State Zip**

561-575-9557

**Phone Number**

troy@2gho.com

**Email Address**

*Property Owner(s) Acknowledgements: - This application will not be considered complete without the signature of all property owners of record, which shall serve as an acknowledgement of the submission of this application. The property owner's signature below shall also authorize the Applicant (if other than the property owner) and/or Representative to act in his/her behalf for the purposes of seeking approval for the application described herein. The undersigned consents to inspection and photographing of the subject property by the Planning staff for purposes of consideration of this Application and/or presentation to the Planning Board and City Commission.*


  
Property Owner(s) Signature(s)

**Edward A. Sasso, Manager**

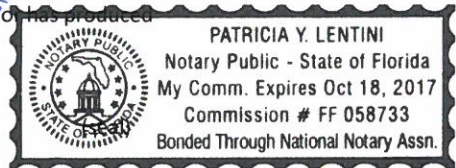
STATE OF FLORIDA -- PALM BEACH COUNTY

The foregoing instrument was acknowledged before me this 28 day of June, 2016, by

Edward A. Sasso who is personally known to me or has produced

  
Signature of Notary

as identification.



**INTAKE MEETINGS ARE REQUIRED FOR ALL SUBMITTALS. CALL (772) 467-3729**

**TO BE COMPLETED BY STAFF**

Zoning	Future Land Use	Total Acres	Historic District	Historic Designation
<b>C-3</b>	<b>CG</b>	<b>24.04</b>	<b>N/A</b>	Contributing Individual Non-Contributing None

Pre-Application Meeting Date December 9, 2015

Fees \_\_\_\_\_ Control # \_\_\_\_\_ B. Permit # \_\_\_\_\_

Intake Planner \_\_\_\_\_

Planner Assigned \_\_\_\_\_

Approved By \_\_\_\_\_ Date \_\_\_\_\_

Comments \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Intake Date Stamp

# DEVELOPMENT REVIEW

## General Information

- Incomplete application packets cannot be accepted.
- Site Plan approval is valid for one (1) year following City Commission approval. In order to maintain site plan approval, vertical improvements, permitted by the Building Department must commence prior to the 12-month expiration date, and building permits must be maintained until site plan is completed, per plans, or approval shall lapse.

Choose Application Type:

Application Type		
<input type="checkbox"/> Site Plan	<input type="checkbox"/> Conditional Use with New Const.	<input type="checkbox"/> Major Amendment
<input checked="" type="checkbox"/> Conceptual Development Plan	<input type="checkbox"/> Minor Amendment	

Site Information:

Non-Residential: Proposed Sq. Ft.: 263,352 s.f. Residential: Proposed Units: \_\_\_\_\_

Surrounding Uses: (i.e. single family home, retail, industrial, etc.)

North	South	East	West
vacant	RV Park	vacant	vacant

## Application Outlook



## Site Plan submittal requirements:

Submit one (1) original & thirteen (13) hard copies and one (1) CD of the following. Additional copies will be required of subsequent submittals.

- Complete notarized application
- Warranty Deed
- SLC Property Record Card
- Statements of ownership & control of proposed development. Statement describing in detail: character & intended use.
- General location map (see Section 22-58.d.2)
- Survey (see Section 22-58.d.3)
- Conceptual** Site Plan (see Section 22-58.d.4)
- Landscaping Plan (see Section 22-187)
- Storm Drainage Plan (see Section 22-58.d.6)
- Environmental Impact Report
- Beach/Dune System protection plan, if applicable (see Section 22-58.d.7)
- Lighting Plan (see Section 22-58.d.8)
- Design Review submittals (see Design Review application)
- Traffic Impact Report
- Concurrency Review submittals (see Concurrency Review application)

June 28, 2016

City of Fort Pierce  
100 N. U.S. 1/P.O. Box 1480  
Fort Pierce, FL 34964-1480

**RE: KINGS HIGHWAY COMMERCIAL SOUTH LLC – INTERSTATE  
BUSINESS PARK  
2GHO JOB#15-1206**

To Whom It May Concern:

This letter is to serve as permission for Gentile Glas Holloway O'Mahoney & Associates, Inc. to act as the agents to prepare and submit all documentation and attend all meetings pertaining to the property as above described, as it relates to the Governmental Application processes through the City of Fort Pierce, Florida.

Sincerely,

Kings Highway Commercial South LLC.

  
Signature

Edward A. Sasso, Manager

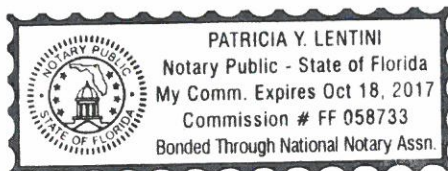
STATE OF FLORIDA  
COUNTY OF PALM BEACH

I hereby certify on the 28<sup>th</sup> day of June, 2016, Edward A. Sasso personally appeared before me and produced identification, or is personally known to me, to be the person described herein and who executed the foregoing instrument.

Witness my hand and official seal in the state and county, the day and year aforesaid.

  
Notary

Commission Expires:





June 28, 2016

Mr. Kori Benton, MPA  
Economic Development Team  
City of Fort Pierce Planning Department  
100 N. U.S. 1/P.O. Box 1480  
Fort Pierce, FL 34964-1480

**RE: KINGS HIGHWAY COMMERCIAL SOUTH LLC – INTERSTATE BUSINESS PARK  
2GHO JOB#15-1206**

Dear Mr. Benton:

Please find enclosed an application for Conceptual Development Review for property located on the east side of Kings Highway (State Road 713) in the City of Fort Pierce, Florida. The subject site is 24.04 acres of land with an existing Land Use Designation of CG – Commercial General and an existing Zoning District of C-3.

The following is a list of the items for the Conceptual Development Review, 5 full size copies and 4 11x17 reduced copies:

- Development Application;
- Preliminary Site Plan and Landscape Plan;
- Survey
- Warranty Deed and Property Card
- Agent Letter
- Filing Fee of \$500.00

It is very important to the project that the application receive conceptual approval at the following dates:

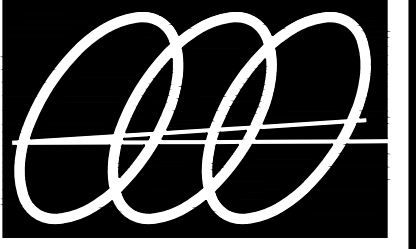
- |                                  |                 |
|----------------------------------|-----------------|
| - Technical Review Committee     | July 21, 2016   |
| - Planning and Zoning Board      | August 9, 2016  |
| - City Commission final approval | August 15, 2016 |

Should you have any questions regarding the submittal, please feel free to contact me at 561-575-9557 or [troy@2gho.com](mailto:troy@2gho.com)

Sincerely,  
Gentile Glas Holloway O'Mahoney & Associates, Inc.

M. Troy Holloway, PLA, ASLA, LEED®AP, BD&C  
Partner

cc: Edward Sasso  
Cynthia Angelos, Esq.



**Gentile Glas  
Holloway  
O'Mahoney  
& Associates, Inc.**  
Landscape Architects  
Planners and  
Environmental Consultants

1907 Commerce Lane  
Suite 101  
Jupiter, Florida 33458  
561-575-9557  
561-575-5260 FAX  
www.2GHO.com

# Interstate Business Park at Kings Highway Ft. Pierce, Florida

Designed: \_\_\_\_\_ MTH  
Drawn: \_\_\_\_\_ MTH  
Approved: \_\_\_\_\_ GGG MTH/EMO  
Date: \_\_\_\_\_ 7/1/16  
Job no. \_\_\_\_\_ 15-0206  
Revisions: \_\_\_\_\_

Scale

L.C.000177

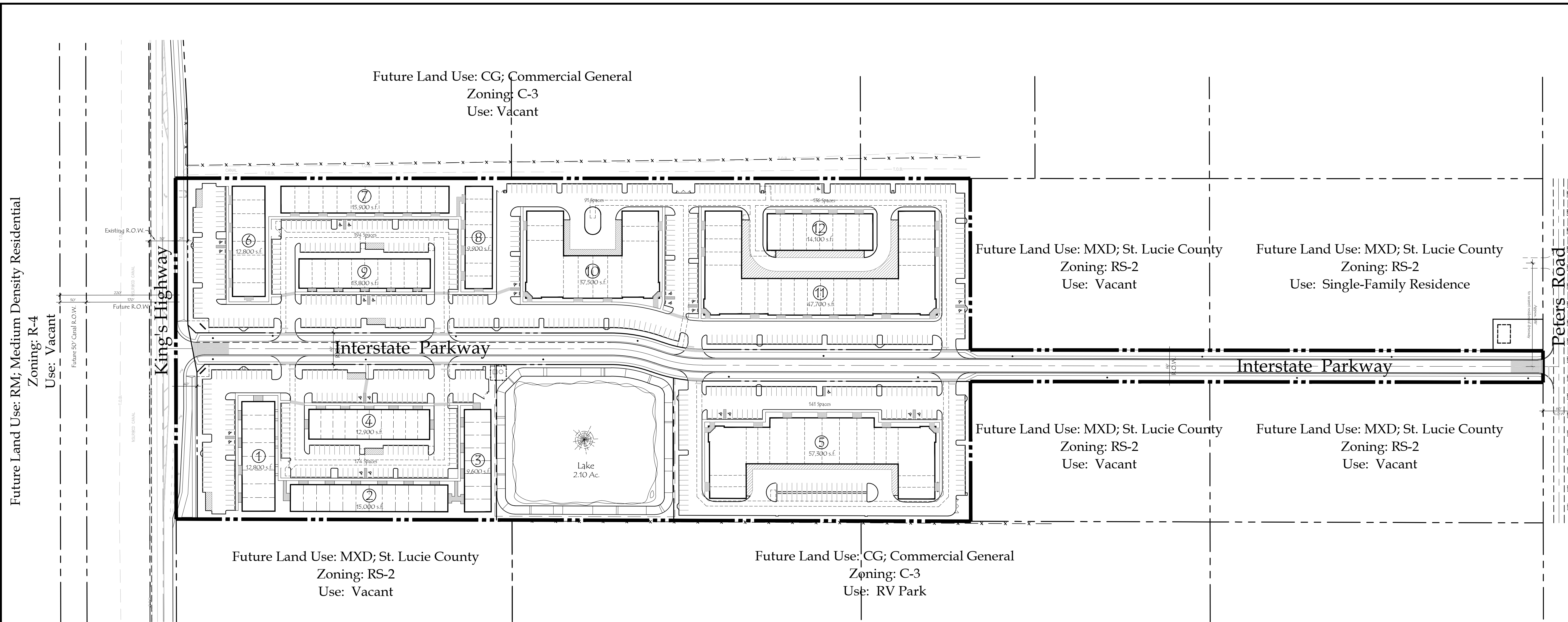
Sheet Title:  
**Master  
Conceptual  
Plan**

Scale: 1" = 100'

Sheet No.

# MC-1

15-0206



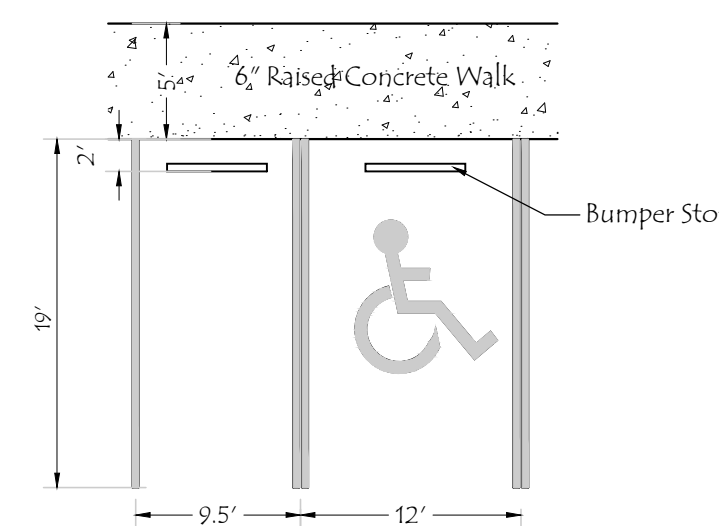
## Site Data

Total Site Area	24.05 Acres; 1,047,550.94 s.f..
Site Area after dedications	23.58 Ac.; 1,027,127.0 s.f.
Existing Zoning	C-3; General Commercial
Existing Land Use	CG; Commercial General
Pervious Area	7.1 Acres; 308,139 s.f. (30.0%)
Impervious Area	16.5 Acres; 718,988 s.f. (70.0%)

## Building Data

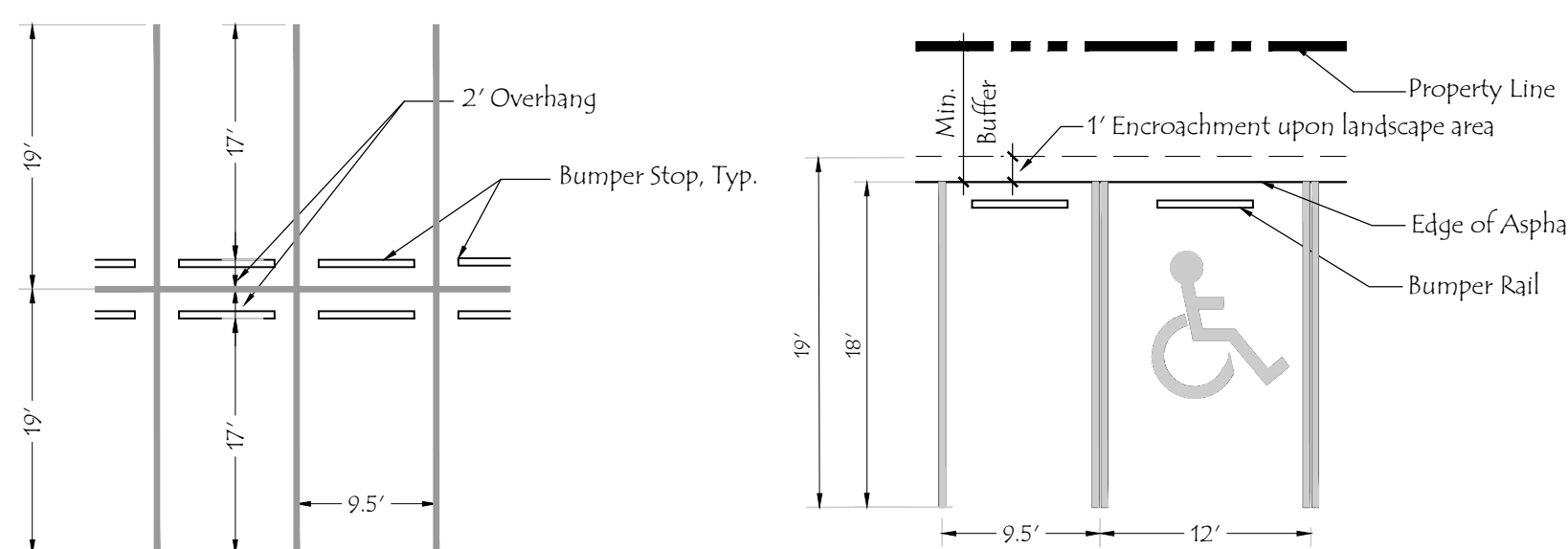
	Gross Flr. Area
Building 1	12,800 s.f.
Building 2	15,000 s.f.
Building 3	9,600 s.f.
Building 4	12,900 s.f.
Building 5	57,300 s.f.
Building 6	12,800 s.f.
Building 7	15,900 s.f.
Building 8	9,900 s.f.
Building 9	13,800 s.f.
Building 10	37,500 s.f.
Building 11	47,700 s.f.
Building 12	14,100 s.f.
<b>Total Gross Flr. Area</b>	<b>259,300 s.f.</b>
<b>Bldg. Lot Coverage</b>	<b>250,000 s.f. 24.3% (60% max.)</b>

"Gross Floor Area" as defined by the zoning code as follows:  
The sum of the gross horizontal areas of the several floors of a building, measured from the exterior faces of the exterior walls excluding public corridors, common restrooms, attic areas with a headroom of less than seven (7) feet, unenclosed stairs or fire escapes, elevator structures, cooling towers, areas devoted to building machinery and equipment (such as air conditioning, ventilating or heating), parking structures and basement space where the ceiling is not more than an average of six (6) feet above the general finished and graded level of the adjacent portion of the lot.



Parking Stall Detail Abutting Walk

N.T.S.



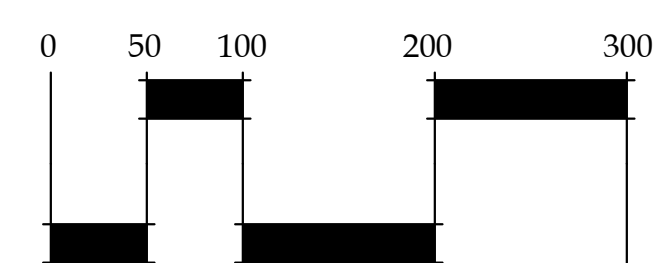
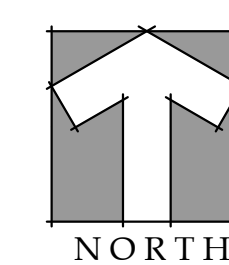
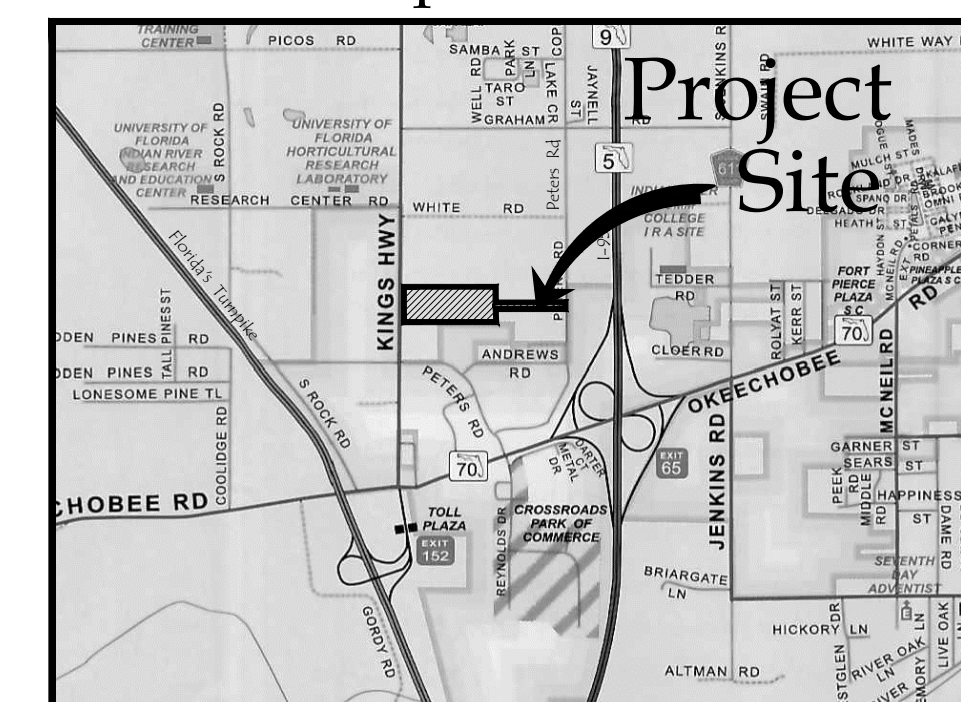
Face to Face Parking Detail

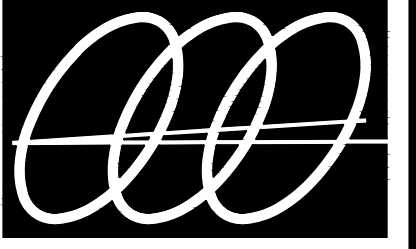
N.T.S.

Parking Stall Detail Abutting Landscape

N.T.S.

## Location Map





**Gentile Glas  
Holloway  
O'Mahoney  
& Associates, Inc.**  
Landscape Architects  
Planners and  
Environmental Consultants

1907 Commerce Lane  
Suite 101  
Jupiter, Florida 33458  
561-575-9557  
561-575-5260 FAX  
www.2GHO.com

# Interstate Business Park at Kings Highway Ft. Pierce, Florida

Designed: MTH  
Drawn: MTH  
Approved: GGG MTH/EMO  
Date: 7/1/16  
Job no. 15-0206  
Revisions:

Seal

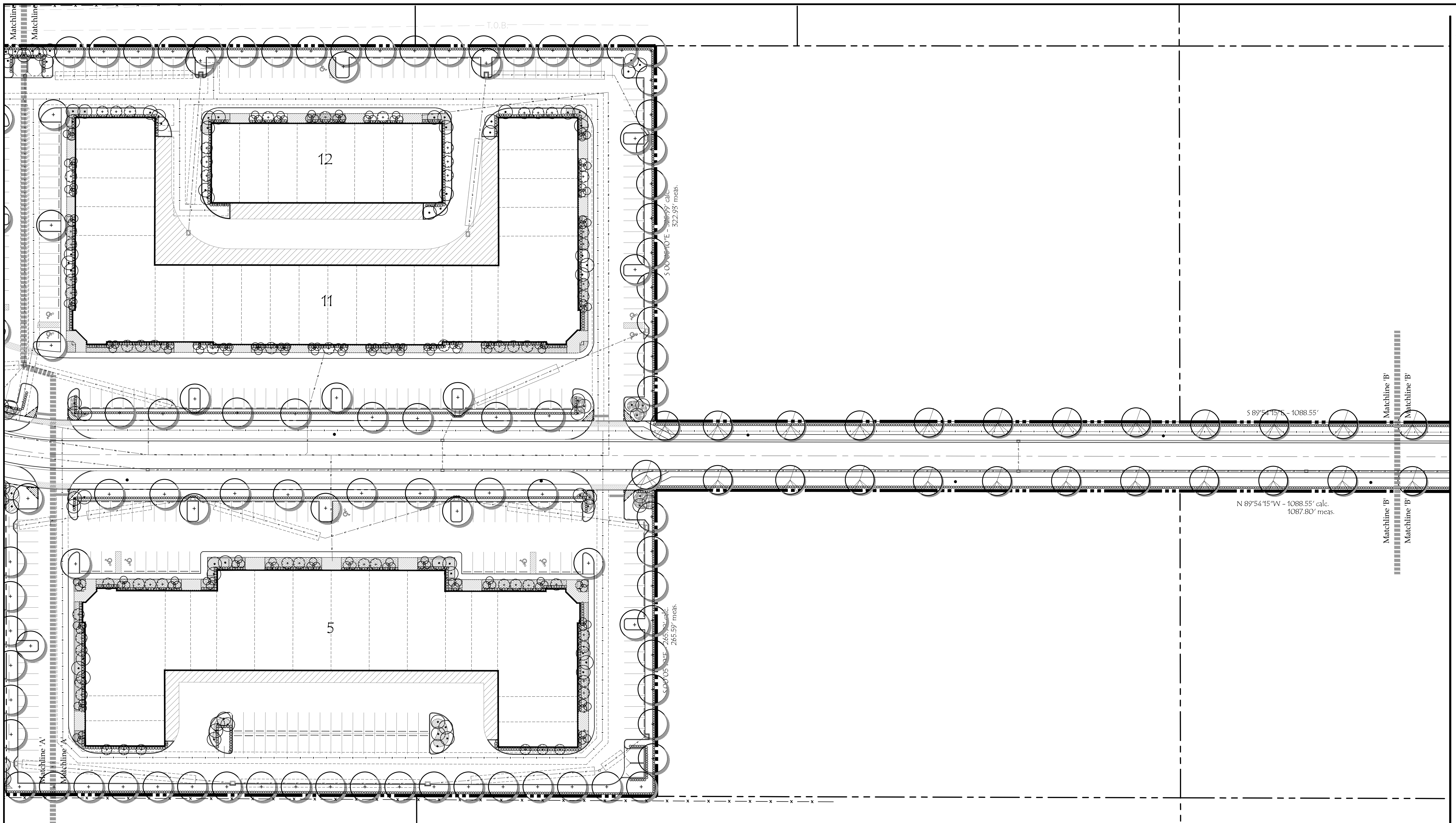
L.C. C000177

Sheet Title:  
**Conceptual  
Landscape  
Dev. Plan**

Scale: 1" = 40'

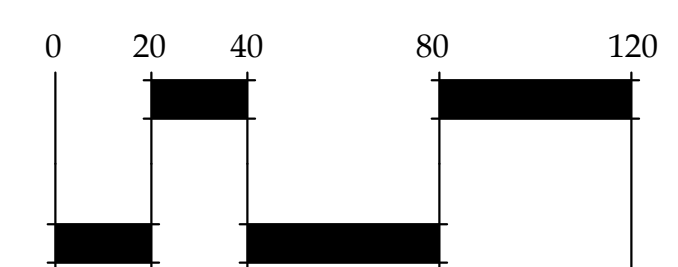
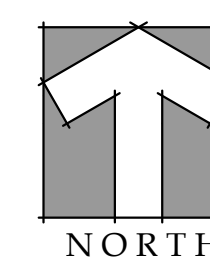
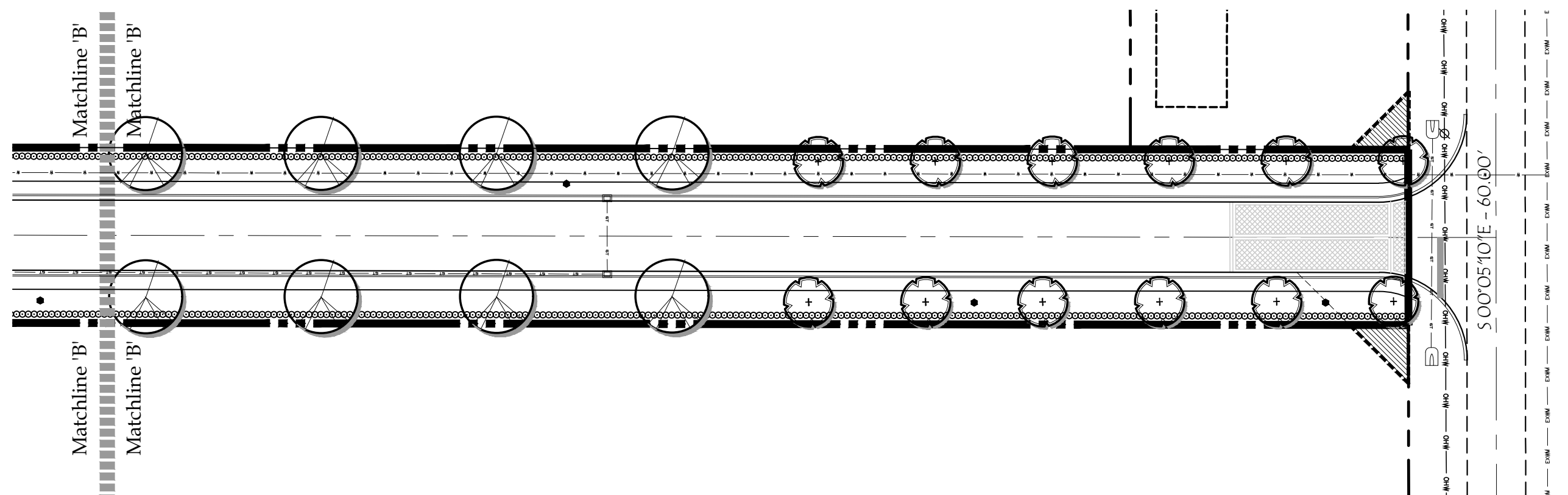
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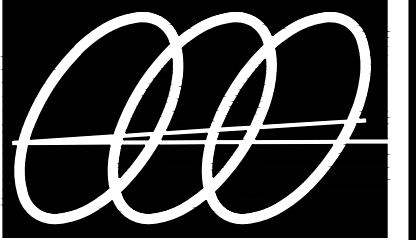
**LP-2**  
15-0206



## LEGEND

- Large Palm: Sabal Palm
- Canopy Tree: Live Oak
- Canopy Tree: Southern Magnolia
- Small Canopy Tree: East Palatka Holly/
- Accent Tree: Nellie R. Stevens
- Specimen Palm: Bizmarkia "Silver"
- Accent Palm/Flowering: Triple Christmas Palm
- Littoral Tree: Cypress
- Large Shrub/Grass: Fakahatchee Grass
- Large Shrub/Grass: Dwarf Fakahatchee Grass
- Groundcover: Jasmine
- Groundcover: Duranta
- Groundcover: Ilex
- Hedge: Coco Plum
- Hedge: Silver Buttonwood





**Gentile Glas  
Holloway  
O'Mahoney  
& Associates, Inc.**  
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Revisions:

Seal

L.C. C000177

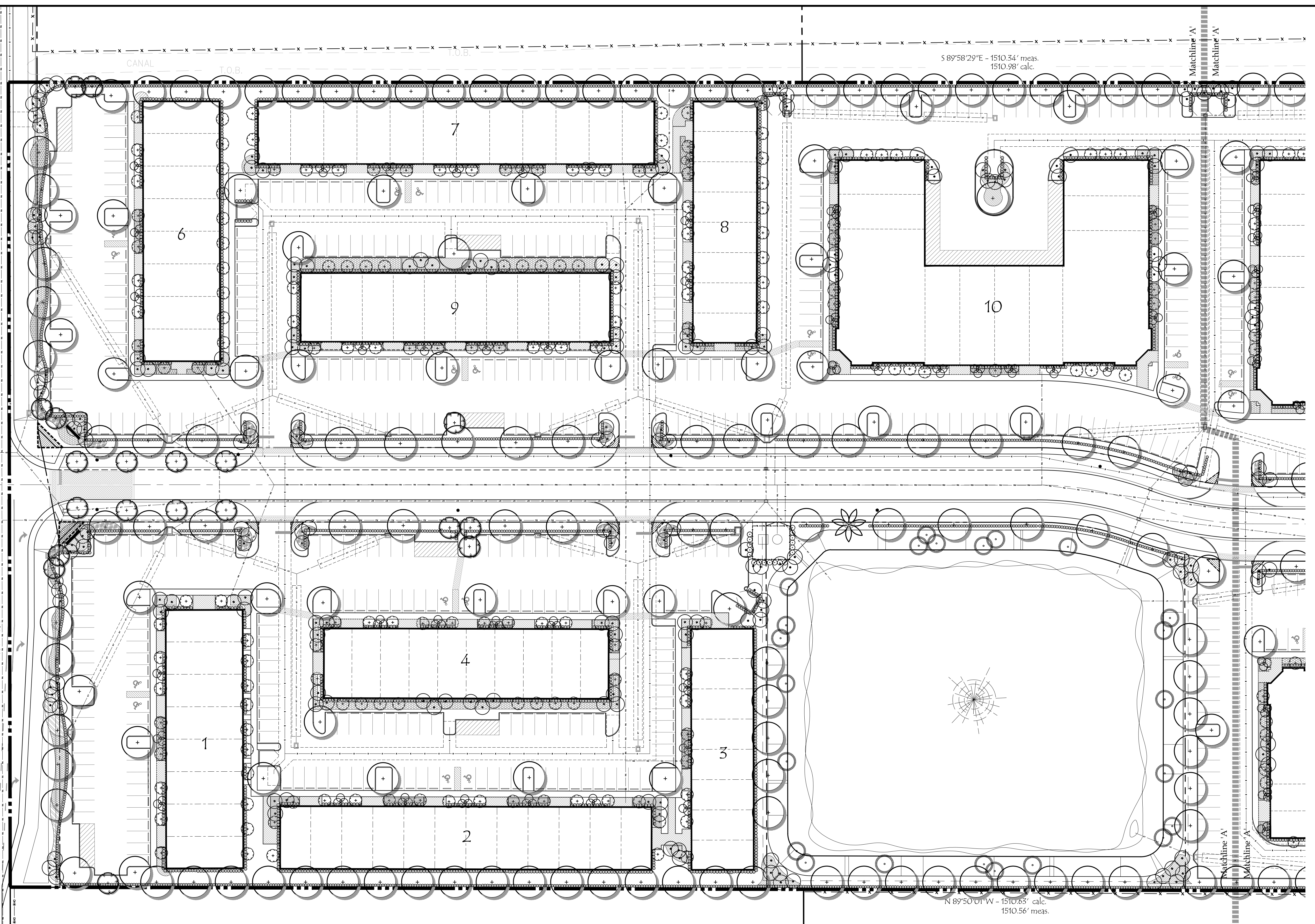
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**Conceptual  
Landscape  
Dev. Plan**

Scale: 1" = 40'

Sheet No.

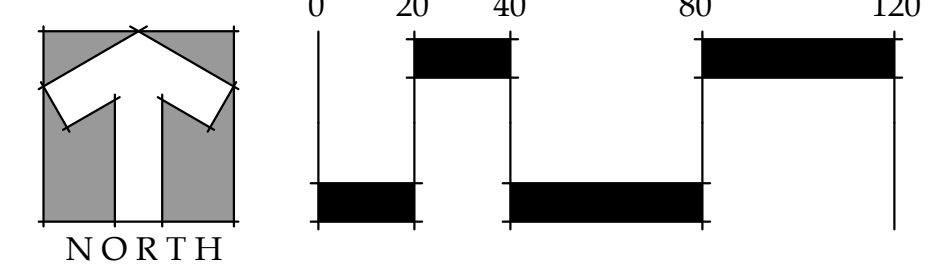
# LP-1

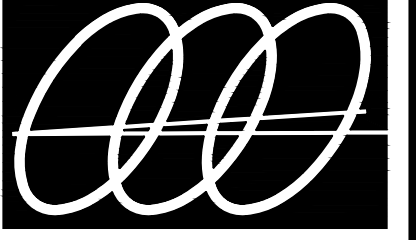
15-0206



## LEGEND

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# Interstate Business Park at Kings Highway Ft. Pierce, Florida

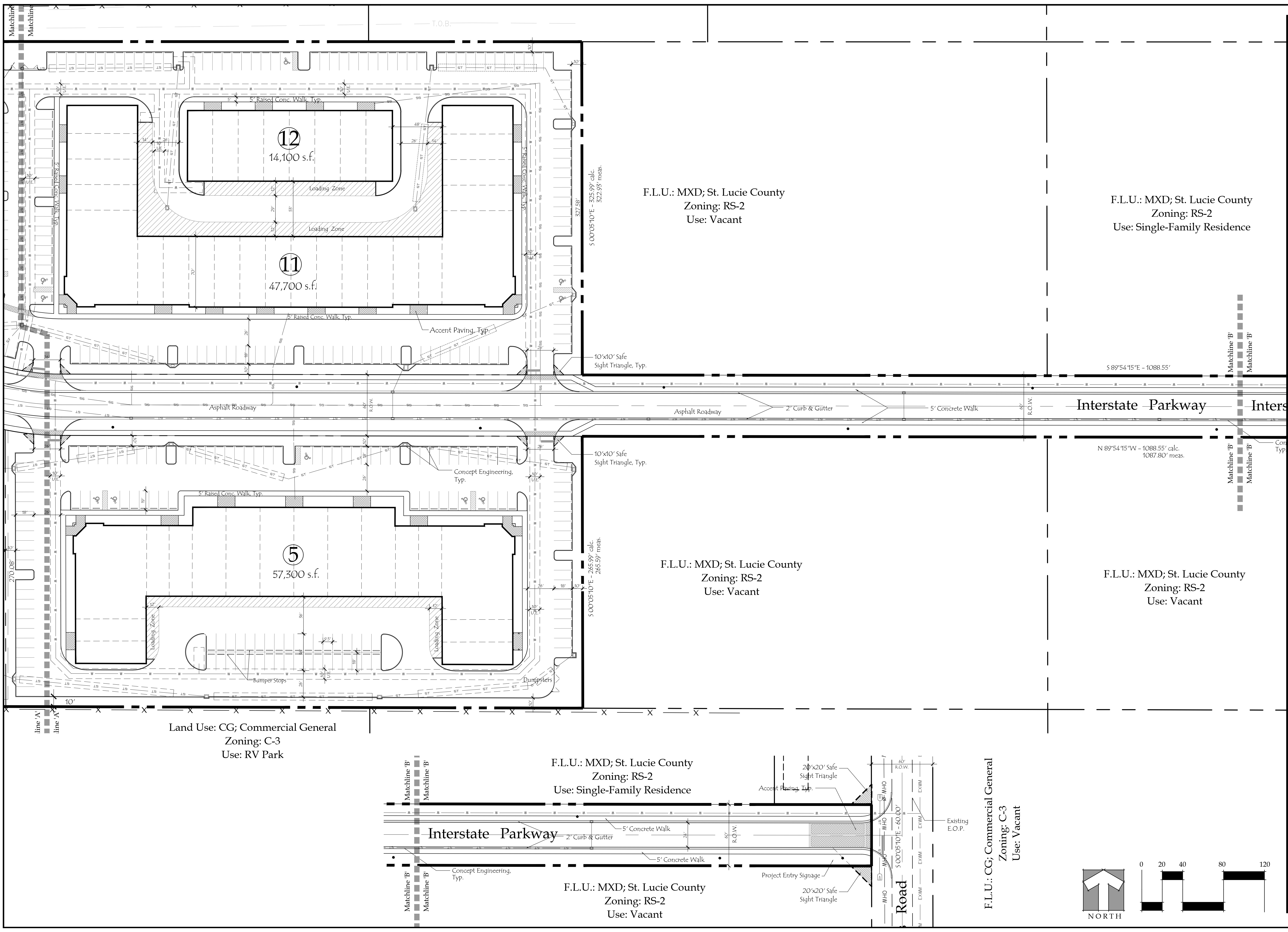
Designed: MTH  
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Date: 7/1/16  
Job no. 15-0206  
Revisions:

Seal

L.C.000177  
Sheet Title:  
**Conceptual  
Development  
Plan**

Scale: 1" = 40'

Sheet No.  
**C-3**  
15-0206



F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Vacant

F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Single-Family Residence

F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Vacant

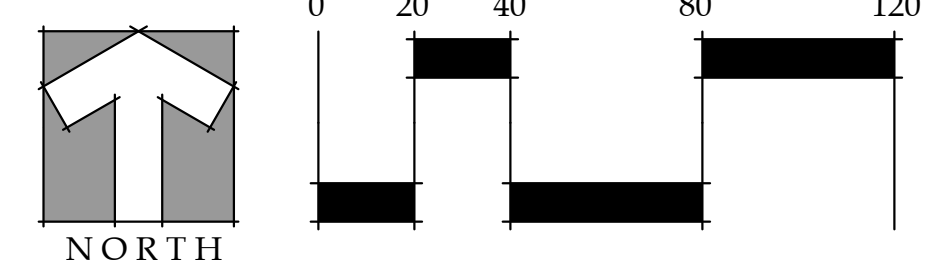
F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Vacant

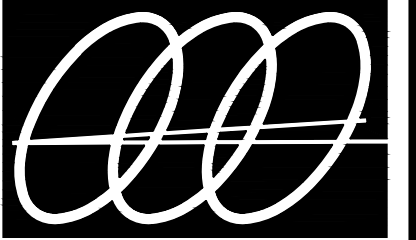
F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Single-Family Residence

F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Vacant

F.L.U.: CG; Commercial General  
Zoning: C-3  
Use: Vacant

Land Use: CG; Commercial General  
Zoning: C-3  
Use: RV Park





**Gentile Glas  
Holloway  
O'Mahoney  
& Associates, Inc.**  
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Planners and  
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# Interstate Business Park at Kings Highway Florida

Designed: MTH  
Drawn: MTH  
Approved: GGG MTH/EMO  
Date: 7/1/16  
Job no. 15-0206  
Revisions:

Seal

L.C.000177  
Sheet Title:  
**Conceptual  
Development  
Plan**

Scale: 1" = 40'

Sheet No.  
**C-2**  
15-0206

F.L.U.: CG; Commercial General  
Zoning: C-3  
Use: Vacant

S 89°58'29"E - 1510.34' meas.  
1510.98' calc.

F.L.U.: CG; Commercial General  
Zoning: C-3  
Use: RV Park

F.L.U.: MXD; St. Lucie County  
Zoning: RS-2  
Use: Vacant

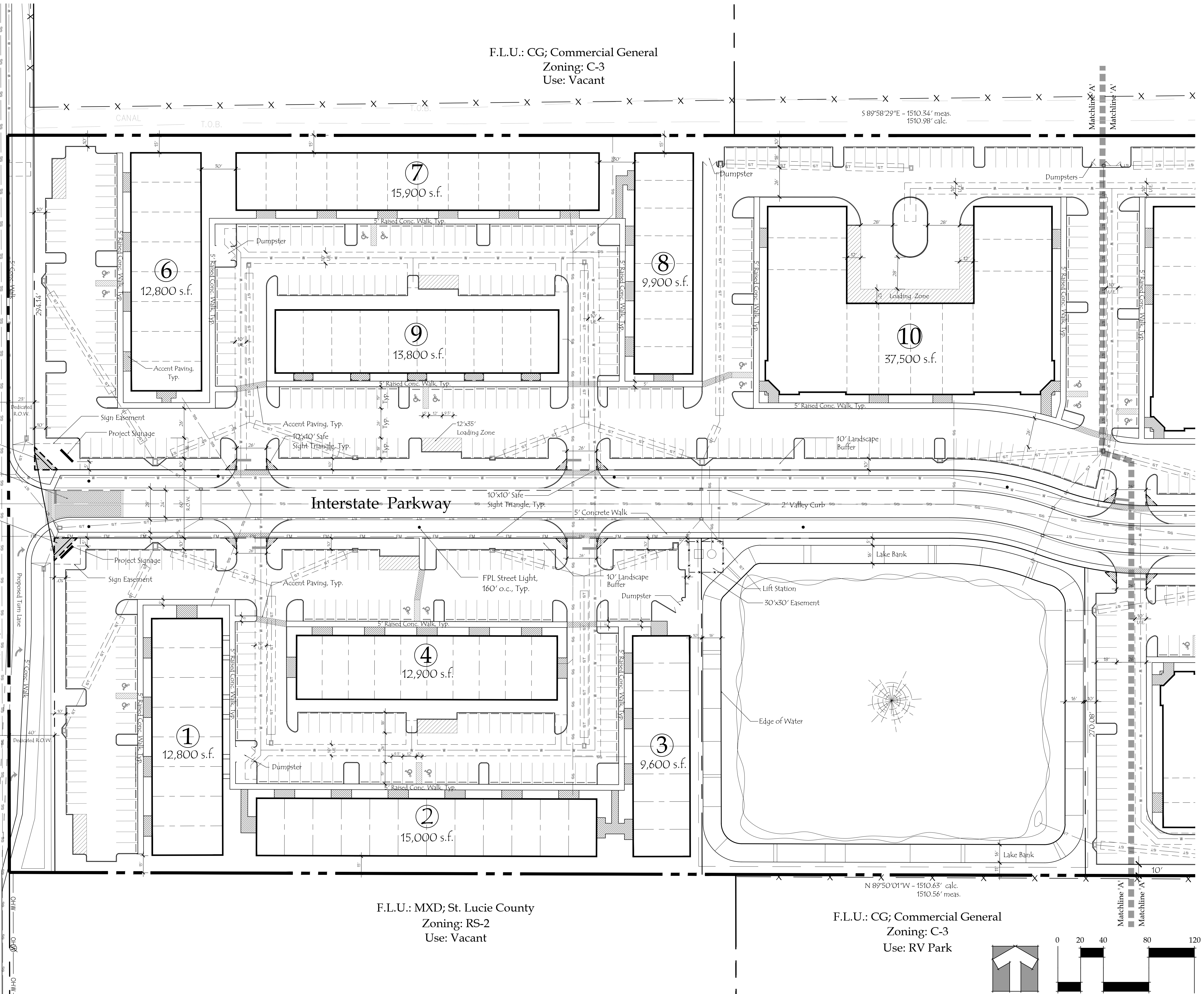
**Kings Highway**

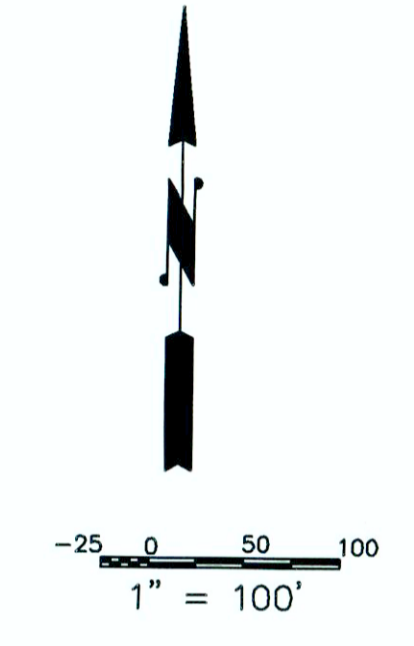
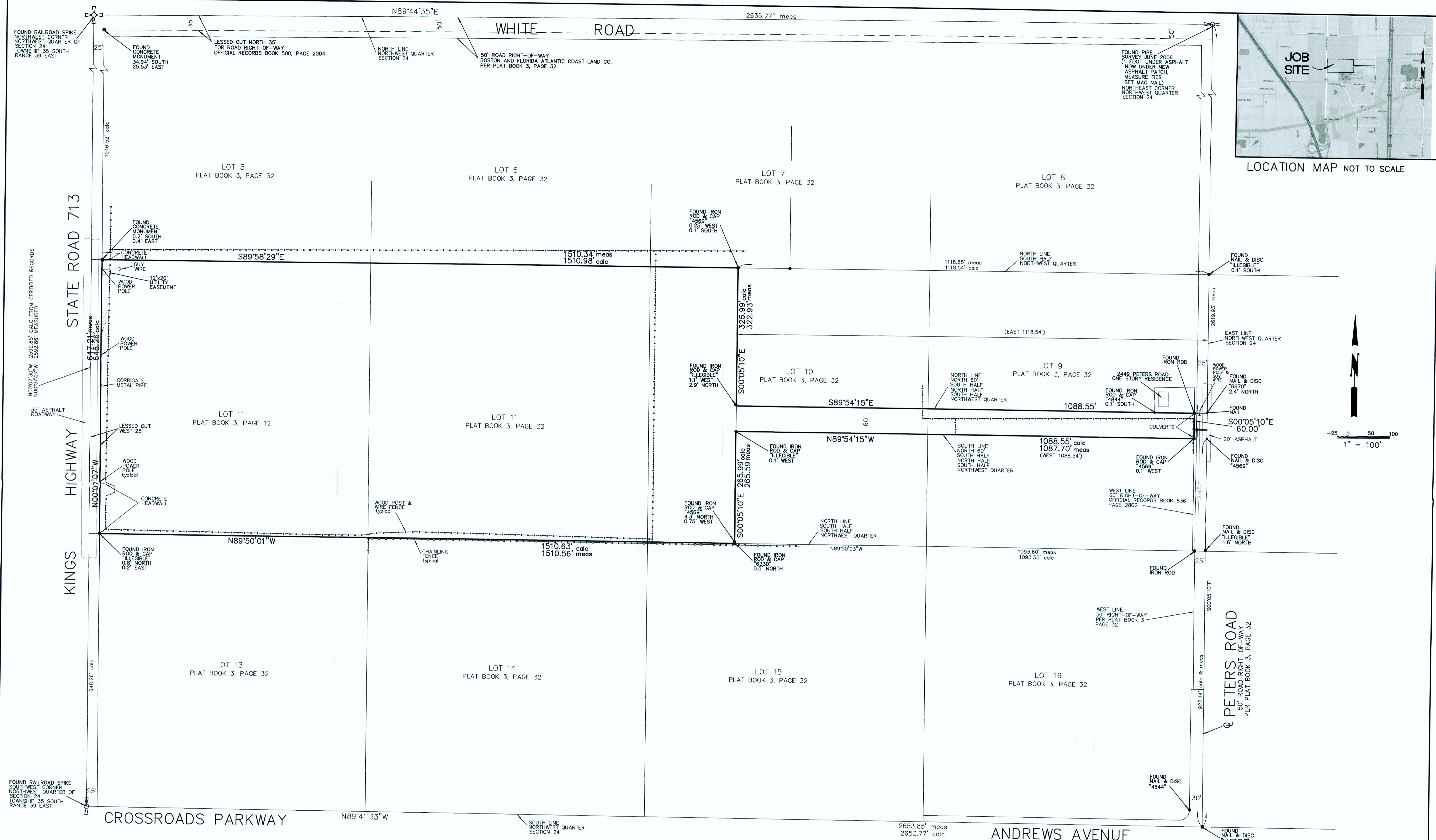
**Interstate Parkway**

**NSLRWCD CANAL**

**NSLRWCD CANAL**

F.L.U.: RM; Medium Density Residential  
Zoning: R-4  
Use: Vacant





THE PROCEDURES AND OR NETWORK DESIGN MEET THE GEODETIC ACCURACY STANDARDS AND SPECIFICATIONS FOR USING GPS RELATED POSITIONING AS SET FORTH BY THE FEDERAL GEODETIC CONTROL SUB-COMMITTEE IN THEIR MOST CURRENT PUBLICATION FOR 3RD ORDER CLASS 1 HORIZONTAL CONTROL SURVEY OR PROVIDE THE HORIZONTAL ACCURACY FOR ALL NEW POSITIONS ESTABLISHED AS A POSITIONAL TOLERANCE.

COORDINATES INFORMATION  
STATE PLANE COORDINATES ARE BASED ON THE TRAVERSE MERCATOR PROJECTION FOR THE EAST ZONE OF FLORIDA AND REFERENCED TO NORTH AMERICAN DATUM OF 1927.

SCALE FACTOR = 0.9999961

GENERAL STATEMENT OF ACCURACY  
NO ADJUSTMENTS MADE TO CONTROL  
EAST-WEST ERROR 0.01'  
NORTH-SOUTH ERROR 0.01'  
OR 0.02' PER 5000'

**SURVEYORS NOTES:**

- THIS SURVEY COMPLIES WITH CHAPTER 5J-17, FLORIDA ADMINISTRATION CODE.
- NO SEARCH OF THE PUBLIC RECORDS HAS BEEN MADE BY THIS OFFICE.
- LEGAL DESCRIPTIONS SUPPLIED BY CLIENT OR CLIENTS AGENT.
- WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.
- SOME TEXT SHOWN HEREIN ARE FOR GRAPHIC AND INFORMATIONAL PURPOSES ONLY.
- VISIBLE ENCROACHMENTS, IF ANY ARE SHOWN HEREON.
- THIS SURVEY FOR CONVEYANCE PURPOSES ONLY AND NOT INTENDED FOR CONSTRUCTION PURPOSES WITHOUT PERMISSION OF THIS SURVEYOR.
- ELEVATIONS HEREON BASED ON NATIONAL GEODETIC VERTICAL DATUM (N.G.V.D. 1929)
- NO UNDERGROUND UTILITIES LOCATED EXCEPT WHERE SHOWN HEREON.
- THIS IS NOT AN ENVIRONMENTAL SURVEY.
- THIS SURVEY DOES NOT GUARANTEE OWNERSHIP OR RIPARIAN RIGHTS BEYOND MEAN HIGH WATER LINE.
- BEARINGS HEREON BASED ON AN GRID BEARING ALONG THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 24 AND TAKEN TO BEAR N000707°W AND ALL OTHER BEARINGS ARE RELATIVE THERETO.
- ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.
- THIS SURVEY IS NOT VALID WITHOUT SURVEYORS SIGNATURE & EMBOSSED SEAL.

**LEGEND:**

⊙ = CENTERLINE  
calc = CALCULATED  
meos = MEASURED  
ORB = OFFICIAL RECORDS BOOK

PROPERTY SHOWN HEREON IS LOCATED IN FLOOD ZONE X PER FLOOD INSURANCE RATE MAP 120286 0167 J DATED FEBRUARY 16, 2012

NOTE:  
THIS SURVEY PERFORMED WITHOUT BENEFIT OF TITLE.

**LEGAL DESCRIPTION** ORB 3487, PAGE 76

THE SOUTH HALF OF THE SOUTH HALF OF THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA, LESS THE EAST 118.54 FEET AND THE WEST 25 FEET THEREOF;  
AND  
THE NORTH 60 FEET OF THE WEST 1088.54 FEET OF THE EAST 1118.54 FEET OF THE SOUTH HALF OF THE NORTH HALF OF THE SOUTH HALF OF THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA, CONTAINING 24.047 ACRES MORE OR LESS.

**MAGELLAN SURVEYING & MAPPING, INC.**  
PROFESSIONAL LAND SURVEYORS  
LICENSED BUSINESS 7575  
450 S. OLD DOES HIGHWAY, SUITE 10  
JUPITER, FLORIDA 33458  
561-746-8745 FAX 561-746-9632 E-mail info@magellansurveying.com

**Boundary Survey**

Prepared For: **KINGS HIGHWAY KIRCO**

Drawn by RES	Date Of Survey	Scale
Field: BM-TFF.B.109 Pg.39	MARCH 23, 2016	1" = 100'
REVISIONS:		Drawing Number
		16-107

Ronald E. Stotler  
PROFESSIONAL SURVEYOR AND MAPPER  
CERTIFICATE NO. 5026  
STATE OF FLORIDA



# CITY OF FORT PIERCE TECHNICAL REVIEW COMMITTEE

---

## A. **Conceptual Site Plan** – Kings Hwy Interstate Business Park– 2496 Kings Hwy.

Mr. Benton: This conceptual development plan is presented pursuant to a previous site plan and conditional use approval that the City Commission provided around 2007-2008 across 24 acres of general commercial property that extends from Peters Road to Kings Highway. The plan is somewhat consistent with the previously considered facility with updates, distribution and variety of different sized and shaped commercial business park structures. They are seeking guidance as far as the City Commission's view and acceptance of this type of project. Cumulatively it represents ¼ million square feet of commercial light industrial and business park type space. The conceptual plan does not present any specific phasing or specific usage however a specific site plan, plan development application or a site plan and conditional use application, which would subsequently be filed, would need to identify those components.

### **FP Planning:**

The C-3 district is intended to provide for a broad variety of business activities including shoppers' goods stores, convenience goods and service establishments, offices and tourist/entertainment facilities. Many public and semi-public uses are also appropriate. Compared to the C-4 zone, this district is more suitable for uses requiring a high degree of accessibility to vehicular traffic, low intensity uses on large tracts of land, most repair services and small warehousing and wholesaling operations. Although this zone should be located along or near arterial or collector streets, it is not the intent of this district to encourage the extension of strip commercial areas. Instead it should promote concentrations of commercial activities. Eligible permitted and conditional uses are presented in the City's Use Table located within Section 22-22, with further consideration of consistency with the City's Comprehensive Plan/ GC- General Commercial Land Use designation.

- Pursuant to City Code Section 22-31 (c)(1) & 22-58, Site plan review shall be required for uses which have buildings with more than four thousand (4,000) square feet of floor area. Furthermore any development upon lots abutting Kings Highway or above 4,000 sq. ft. will be subject to the city's design review guidelines.
- Lot Coverage/Open Space requirements will be considered upon a lot-by-lot basis upon formal subdivision.
- Pursuant to City Code Section 22-179. - Permit applications, an application for tree removal and land clearing shall be filed concurrent with any formal subdivision applications. The property owner is encouraged to preserve native trees where possible.
- The applicant is guided to City Code Chapter 18 regulating formal application for subdivision for specific requirements as the application for subdivision approaches.

- Further review of traffic generation for individual developments will be reviewed for consideration of impacts to Kings Highway, White Road, and Peters Road. Further analysis will guide the potential for designated turn/decel lanes on Kings Highway.
- It's noted that White Road and Peters Road are substandard at the current time.
- Additional landscaping requirements are provided for commercially zoned developments, which abut residentially zoned property. The applicant may wish to consider buffering techniques to provide appropriate separation and safeguard strategies from adjacent residential properties.
- It's noted that a collection of the proposed lots may be eligible for "An adult establishment" based upon the established setback criteria of the City's Land Development Code, due to exclusion of the specific residential (RS-2 / E-2) zoning districts adjacent to the site from said regulations

**FP Building:**

Utilities will be coming in from Kings Highway?

Will Interstate Parkway always remain a private road?

They are going to develop the site and then will it be phased construction?

Mr. Benton: The conceptual site development plan presents Interstate Parkway as right-of-way and I assume, dedicated to the City of Ft. Pierce. Not certain whether we are interested in obtaining that. I think it could go either way, but that is further discussion for the specific site plan application.

It is our anticipation that this will be a phased development over a specific time line. The specific phasing plan has not been provided by the developer at this time.

**FPUA Electric:**

FPUA has 3 phase electric available to the site. For details about the electric service please contact Jason Mittler (772) 466-1600 x 6306. FPUA has gas available to the site. For details please contact Ana Johnson (772) 466-1600 x4705

**FPUA Water/Wastewater:**

Approved as noted City approval does **not** constitute final acceptance by Fort Pierce Utilities Authority. Completed utility drawings need to be submitted for final approved by Fort Pierce Utilities Authority. Please contact James Carnes of Fort Pierce Utilities Authority Water/Wastewater Engineering Department at (772) 466-1600 x 3472 to discuss detailed utility requirements, fees and other administrative items.

**FP Engineering:**

At time of Site Plan submittal the applicant shall revise the Landscape Plan to eliminate all plantings currently depicted within the limits of the Kings Highway Right-of-Way.

**SLC Engineering:**

1. Advisory Comment: The applicant is advised that a Roadway Improvement Agreement (RIA) together with the appropriate surety will be required for the construction of all improvements within County right of way. Please contact Rod Reed, County Surveyor at 462-1721 for additional information. Please be advised that a right of way permit will not be issued until the RIA and surety have been submitted.
2. Advisory Comment: Please be advised that a right of way permit shall be required for the construction activities within the County road right of way. Please contact Selena Griffett, P.E. at 462-2097 for additional information.
3. Advisory Comment: Please contact FDOT as major roadway and drainage improvements are proposed along King's highway adjacent to your site.
4. Advisory Comment: The site is located inside the Urban Service Boundary and additional requirements may need to be met.

I wanted to look at the right-of-way being taken and how it will affect your conceptual plan for which you proposed to do. I had some concerns if that was not taken into consideration it could impact your conceptual design quite a bit because if the right-of-way moves farther to the east, it could impact your building location, parking scheme, and right down the line.

**SLC Fire District:** Show water/fire lines plus show all hydrants within 1000' of property.

The following departments had no comments at the meeting: FP Code and SLC TPO

**Troy Holloway from Gentile Glas Holloway O'Mahoney & Associates:** The owners have had several meetings with DOT for right-of-way taking and drainage requirements. We understand that line is still kind of gray as far as where that right-of-way line is going to sit and there may be some additional properties that may be lost and that is okay because it is conceptual lot layout and we can easily adjust those. As far as that right-of-way, I think it was initially intended when it was done in 2005, the city did want that as right-of-way but it really does not matter to the owner, whether it goes as right-of-way or stays within the project. We will make any adjustments as needed and get the plans re-submitted.

**Planning Board**

**7. d.**

Meeting Date: 08/09/2016

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Information

REQUESTED ACTION

Conceptual Development Plan - Kings Highway Business Park - 7001 White Road

LOCATION

7001 White Road

RESPONSIBLE STAFF

Kori Benton, Senior Planner

RECOMMENDATION

Approval

---

Attachments

Staff Report

Aerial & Zoning Map

Application & Conceptual Plan

TRC Review Comments

---

**Form Review**

Form Started By: Kori Benton

Started On: 08/02/2016 02:35 PM

Final Approval Date: 08/03/2016



# CITY OF FORT PIERCE

## PLANNING DEPARTMENT

REBECCA GROHALL, AICP, PLANNING DIRECTOR  
COMPREHENSIVE PLANNING ♦ DEVELOPMENT REVIEW  
HISTORIC PRESERVATION ♦ URBAN DESIGN ♦ ZONING

---

TO: Members of the City of Fort Pierce Planning Board

THROUGH: Rebecca Grohall, AICP, Planning Director

FROM: Kori Benton, Senior Planner

RE: Conceptual Development Plan  
Kings Highway Business Park  
7001 White Road

DATE: August 2, 2016

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### STAFF REPORT

Owner/Applicant: Kings Highway Commercial Park LLC  
340 S US Hwy 1 #607  
Jupiter, FL 33477

Representative: M. Troy Holloway, ASLA, LEED AP, BD&C  
Gentile, Glas, Holloway, O'Mahoney & Associates, Inc.  
1907 Commerce Lane, Suite 101  
Jupiter, Florida 33458

Requested Action: Approvals of a Conceptual Development Plan

Location: 2496 S. Kings Highway

Parcel IDs: 2324-210-000-000-6

Zoning: C-3, General Commercial

Surrounding Zoning:

North	East	South	West
C-3	RS-2 (SLC)	C-3	R-4

Future Land Use: GC, General Commercial

Parcel Size: 40.59 acres

Utilities: FPUA Water/Wastewater –Electric Service FPL

Per Section 22-58(e) of the City Code, the applicant is requesting the review and approval of a Conceptual Development Plan. Section 22-58(e) of the City Code provides:

Prior to, but not in lieu of, submission of a complete application and site plan the applicant may, solely at his own election, adopt the following procedure:

(1) A conceptual development plan may be submitted to the department of planning and development, which shall schedule a meeting for its consideration by the city planning board. The conceptual development plan shall include a map showing the general layout of the proposed development, statements describing in detail the character and intended use of the development and information bearing on the ownership and control of the proposed use;

(2) The city planning board shall review the conceptual development plan and make recommendations as appropriate for its approval or disapproval;

(3) The city commission shall hold a hearing and approve or disapprove the conceptual development plan or approve it with conditions.

#### Staff Analysis:

The applicant is requesting review of a Conceptual Development Plan for a proposed business park subdivision to be located at 7001 White Road. The presented project presents the potential subdivision of the subject site into 17 lots and a joint water management tract in the southeast corner of the subdivision. The proposal seeks to provide foundation infrastructure for future construction of commercial offices, retail, warehouse or other commerce related purposes.

The undeveloped parcel is located along Kings Highway, south of White Road, containing 40.59 acres. The property is zoned C-3, General Commercial with a Future Land Use of GC, General Commercial. The General Commercial districts provide opportunity to consider development of intensive and general commercial, retail, service, offices, tourist/entertainment facilities, hotels/motels, parks and recreation, along with compatible public, quasi-public, and special uses. The formal application for development review for future uses may require Conditional Use consideration. A preliminary plat of the subject site was previously considered in 2007; however the project was never initiated.

The properties across White Road to the north are vacant agricultural with a portion zoned AG-1 by St. Lucie County and R-1, in the City. The properties to the east are also predominantly vacant and located in unincorporated St. Lucie County with a zoning of RS-2. The property to the southeast of the subject parcel is zoned C-3, General Commercial and occupied by a recreational vehicle park. The vacant parcel to the south is vacant and zoned C-3, General Commercial. The property across Kings Highway to the west is Zoned R-4, Medium Density Residential and currently vacant.

The Florida Department of Transportation (FDOT), St. Lucie County, and the City of Fort Pierce are engaged in a collaborative improvement project for Kings Highway which would expand and significantly enhance this abutting roadway network.

The presented lots range in size from 1.42 acres to 4.26 acres. Development of each lot would require Site Plan and Design Review in accordance with Section 22-58 of the City Code if the principal structure of the development exceeds 4,000 square feet in size.

A new private road is proposed to support the development, extending from Kings Highway to White Road. Review of sufficient right-of-ways for Kings Highway and White Road will be considered at the time of formal development application.

Six-foot wide concrete sidewalks are presented along Kings Highway and White Road, and with the width reduced to five-feet along both sides of the private road. Storm water retention will be provided via connections to the water management tract reserved for the future developments conceptualized.

All affected Departments have reviewed the submittals and provided comments regarding the proposed conceptual development based on compliance with the requirements of the City Code. The comments generated are attached for review by the Board.

**Staff Comments:**

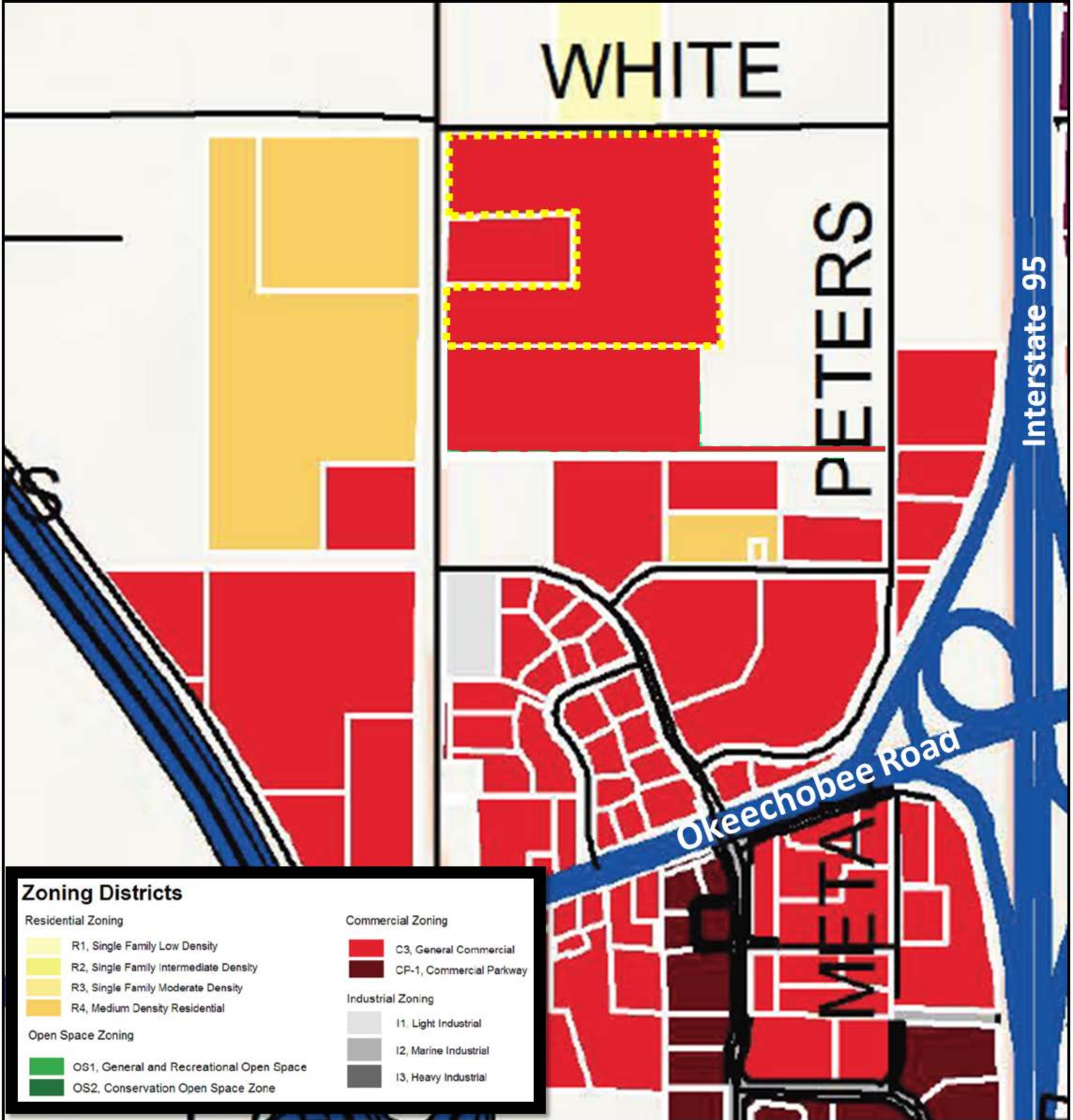
The proposed Conceptual Development Plan generally meets the requirements of the City Code, and is conceptually consistent with the City's Comprehensive Plan; therefore Staff recommends that Planning Board forward a recommendation of approval to the City Commission.



# 7001 White Road

Site Aerial







# CITY OF FORT PIERCE

## PLANNING DEPARTMENT

COMPREHENSIVE PLANNING ◊ DEVELOPMENT REVIEW  
HISTORIC PRESERVATION ◊ URBAN DESIGN ◊ URBAN REDEVELOPMENT ◊ ZONING

### DEVELOPMENT REVIEW

Property address or Location 7001 White Road - Kings Highway Business Park

Parcel ID #(s) 2324-210-0000-000-6

Project description The Applicant is submitting a conceptual development plan on 40.596 acres of land in the City of Fort Pierce.

Kings Highway Commerce Park LLC

**Property Owner(s)**

340 South U.S. Highway One

**Street Address**

Jupiter FL 3347

City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

**Phone Number**

**Email Address**

M. Troy Holloway/Gentile Glas Holloway O'Mahoney & Associates, Inc. (2GHO)

**Applicant/Representative, Title, Company**

1907 Commerce Lane Suite 101

**Street Address**

Jupiter FL 33458

City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

561-575-9557

**Phone Number**

troy@2gho.com

**Email Address**

*Property Owner(s) Acknowledgements: - This application will not be considered complete without the signature of all property owners of record, which shall serve as an acknowledgement of the submission of this application. The property owner's signature below shall also authorize the Applicant (if other than the property owner) and/or Representative to act in his/her behalf for the purposes of seeking approval for the application described herein. The undersigned consents to inspection and photographing of the subject property by the Planning staff for purposes of consideration of this Application and/or presentation to the Planning Board and City Commission.*

**Edward A. Sasso, Manager**

Property Owner(s) Signature(s)

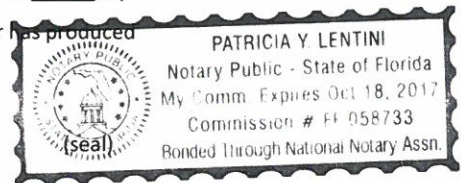
STATE OF FLORIDA -- PALM BEACH COUNTY

The foregoing instrument was acknowledged before me this 28 day of June, 2016, by

Edward A. Sasso who is personally known to me or has produced

[Signature] as identification.

Signature of Notary



**INTAKE MEETINGS ARE REQUIRED FOR ALL SUBMITTALS. CALL (772) 467-3729**

**TO BE COMPLETED BY STAFF**

Zoning	Future Land Use	Total Acres	Historic District	Historic Designation	
C-3	CG	40.5	N/A	Contributing	Individual
				Non-Contributing	None

Pre-Application Meeting Date December 9, 2015

Fees \_\_\_\_\_ Control # \_\_\_\_\_ B. Permit # \_\_\_\_\_

Intake Planner \_\_\_\_\_

Planner Assigned \_\_\_\_\_

Approved By \_\_\_\_\_ Date \_\_\_\_\_

Comments \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Intake Date Stamp

# DEVELOPMENT REVIEW

## General Information

- Incomplete application packets cannot be accepted.
- Site Plan approval is valid for one (1) year following City Commission approval. In order to maintain site plan approval, vertical improvements, permitted by the Building Department must commence prior to the 12-month expiration date, and building permits must be maintained until site plan is completed, per plans, or approval shall lapse.

Choose Application Type:

Application Type		
<input type="checkbox"/> Site Plan	<input type="checkbox"/> Conditional Use with New Const.	<input type="checkbox"/> Major Amendment
<input checked="" type="checkbox"/> Conceptual Development Plan	<input type="checkbox"/> Minor Amendment	

Site Information:

Non-Residential: Proposed Sq. Ft.: \_\_\_\_\_ Residential: Proposed Units: \_\_\_\_\_

Surrounding Uses: (i.e. single family home, retail, industrial, etc.)

North	South	East	West
vacant	vacant	vacant	vacant

## Application Outlook



## Site Plan submittal requirements:

Submit one (1) original & thirteen (13) hard copies and one (1) CD of the following. Additional copies will be required of subsequent submittals.

- Complete notarized application
- Warranty Deed
- SLC Property Record Card
- Statements of ownership & control of proposed development. Statement describing in detail: character & intended use.
- General location map (see Section 22-58.d.2)
- Survey (see Section 22-58.d.3)
- Site Plan (see Section 22-58.d.4) *conceptual*
- Landscaping Plan (see Section 22-187)
- Storm Drainage Plan (see Section 22-58.d.6)
- Environmental Impact Report
- Beach/Dune System protection plan, if applicable (see Section 22-58.d.7)
- Lighting Plan (see Section 22-58.d.8)
- Design Review submittals (see Design Review application)
- Traffic Impact Report
- Concurrency Review submittals (see Concurrency Review application)



June 28, 2016

Mr. Kori Benton, MPA  
Economic Development Team  
City of Fort Pierce Planning Department  
100 N. U.S. 1/P.O. Box 1480  
Fort Pierce, FL 34964-1480

**RE: KINGS HIGHWAY COMMERCE PARK LLC – KINGS HIGHWAY BUSINESS PARK  
2GHO JOB#15-1207**

Dear Mr. Benton:

Please find enclosed an application for Conceptual Development Review for property located on the east side of Kings Highway (State Road 713) in the City of Fort Pierce, Florida. The subject site is 40.596 acres of land with an existing Land Use Designation of CG – Commercial General and an existing Zoning District of C-3.

The following is a list of the items for the Conceptual Development Review, 5 full size copies and 4 11x17 reduced copies:

- Development Application;
- Preliminary Site Plan;
- Survey
- Warranty Deed and Property Card
- Agent Letter
- Filing Fee of \$500.00

It is very important to the project that the application receive conceptual approval at the following dates:

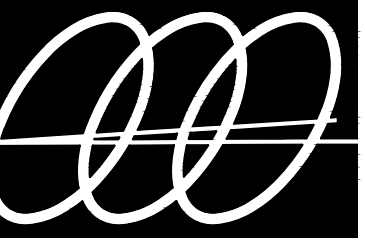
- Technical Review Committee                      July 21, 2016
- Planning and Zoning Board                      August 9, 2016
- City Commission final approval              August 15, 2016

Should you have any questions regarding the submittal, please feel free to contact me at 561-575-9557 or [troy@2gho.com](mailto:troy@2gho.com)

Sincerely,  
Gentile Glas Holloway O'Mahoney & Associates, Inc.

  
M. Troy Holloway, PLA, ASLA, LEED®AP, BD&C  
Partner

cc: Edward Sasso  
Cynthia Angelos, Esq.



**Gentile Glas  
Holloway  
O'Mahoney  
& Associates, Inc.**  
Landscape Architects  
Planners and  
Environmental Consultants

1907 Commerce Lane  
Suite 101  
Jupiter, Florida 33458  
561-575-9557  
561-575-5260 FAX  
www.landscape-architects.com

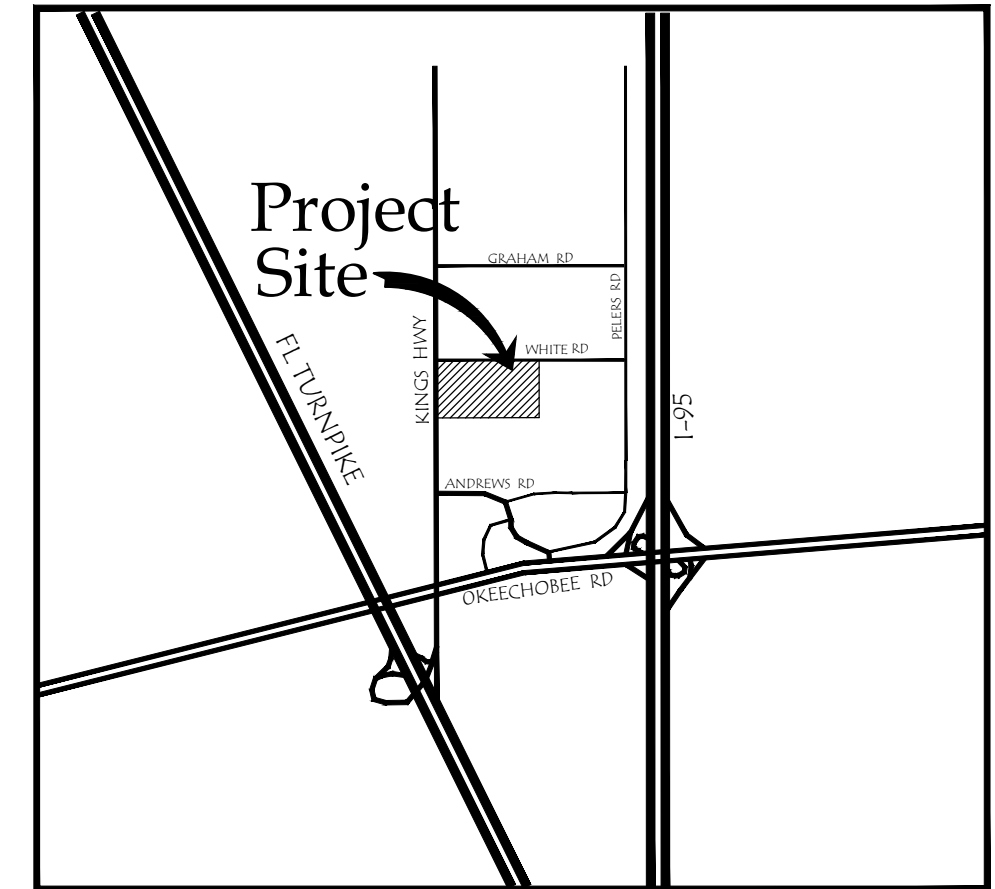
**King's Highway  
Commerce Park**  
City of Ft. Pierce, Florida

Designed:            MTH  
Drawn:            MTH  
Approved:            GGG MTH/EMO  
Date:            7/1/16  
Job no.             
Revisions:             
            
            
            
Cad no.             
Seal

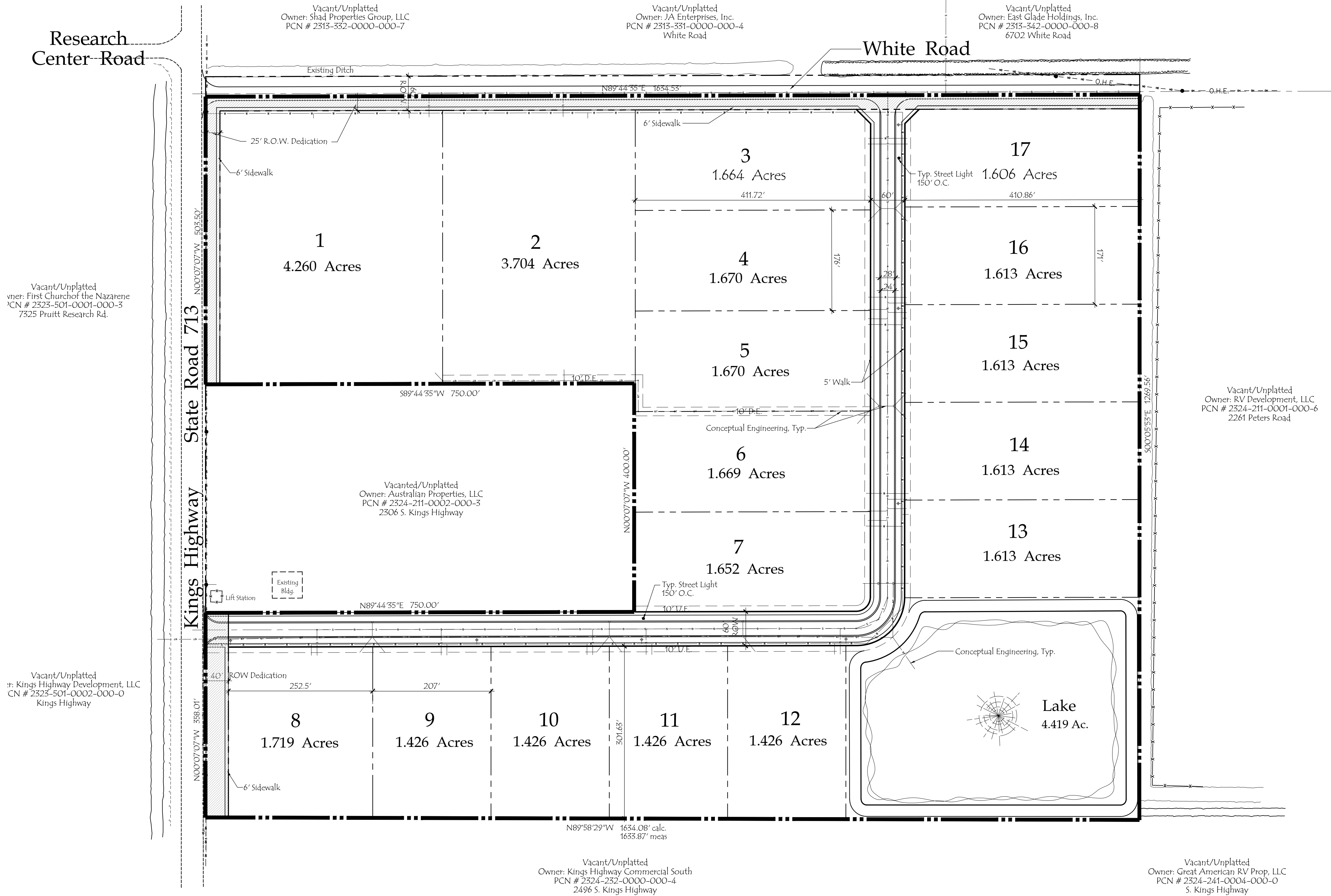
LC 000177  
Sheet Title:  
**Conceptual  
Development  
Plan**  
Scale: 1" = 100'  
Sheet No.

**C-1**  
15-0207

Location Map

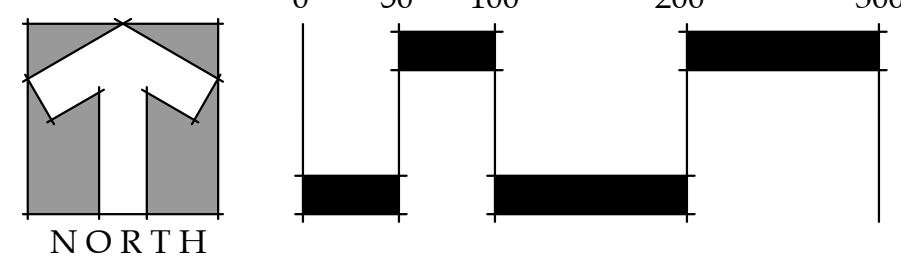


SECTION 24 TOWNSHIP 35S RANGE 39E



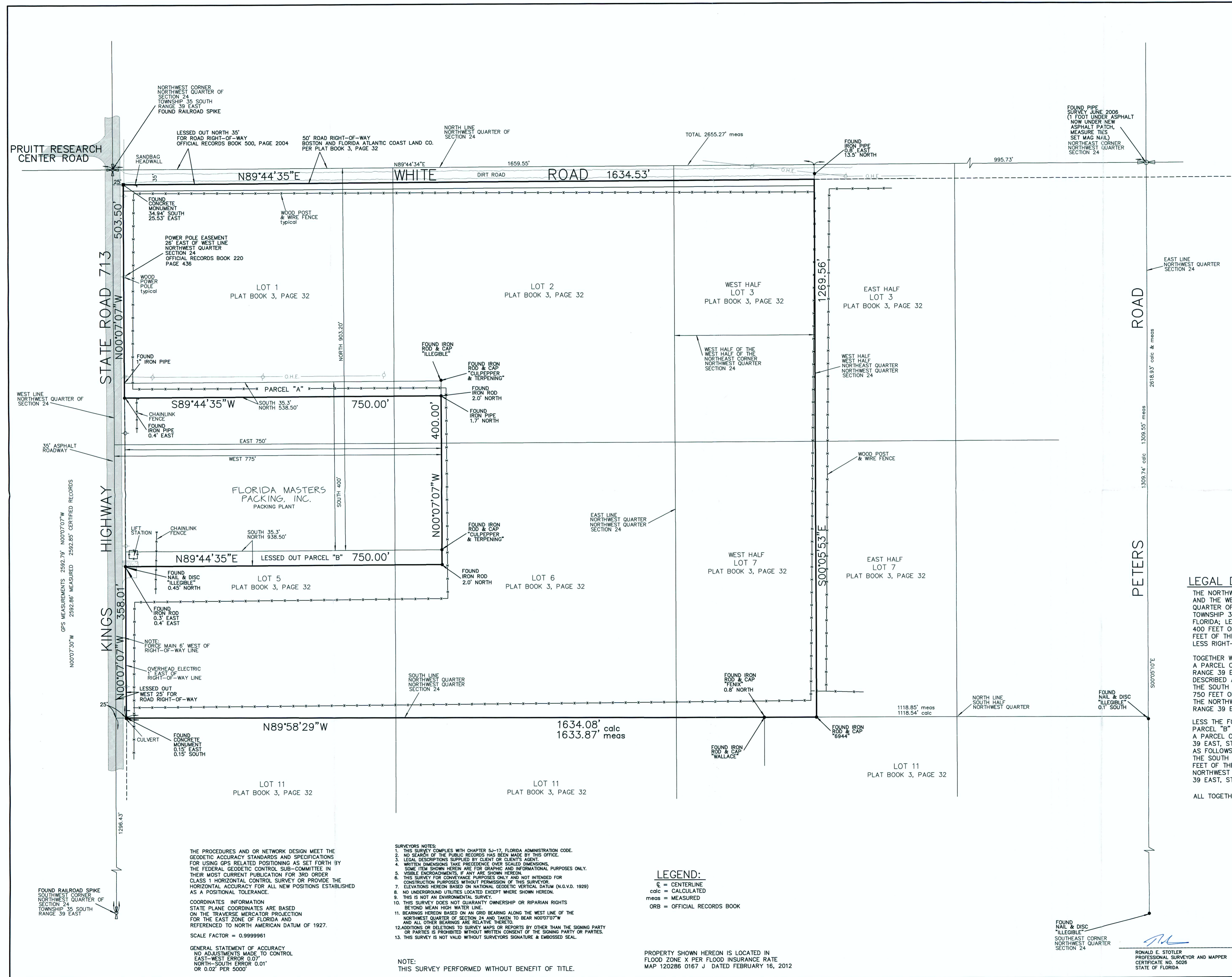
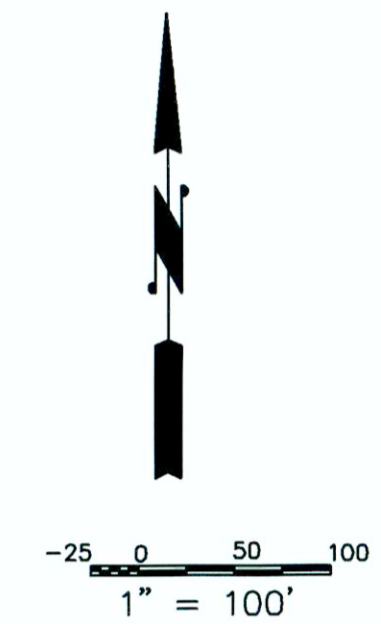
Site Data

Existing Zoning	C-3; General Commercial
Existing Land Use	Commercial
Total Site Area	40.59 Acres; 1,768,273.1 s.f.
Site Area after dedications	39.06 Ac.; 1,701,272.0 s.f.
Maximum Bldg. Lot Coverage	60% of the lot area
Maximum Bldg. Height	65'
Lake Area	4.42 Acres; 11.33% (after dedications)





LOCATION MAP NOT TO SCALE



**LEGAL DESCRIPTION** ORB 2382, PAGE 1143

THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER AND THE WEST HALF OF THE WEST HALF OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA; LESS AND EXCEPTING THERE FROM THE SOUTH 400 FEET OF THE NORTH 903.20 FEET OF THE EAST 750 FEET OF THE WEST 775 FEET OF SAID SECTION 24; LESS RIGHT-OF-WAY FOR KINGS HIGHWAY AND WHITE ROAD.

TOGETHER WITH THE FOLLOWING DESCRIBED PARCEL "A" A PARCEL OF LAND LYING IN SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA; MORE PARTICULARLY DESCRIBED AS FOLLOWS:

THE SOUTH 35.30 FEET OF THE NORTH 538.50 FEET OF THE EAST 750 FEET OF THE WEST 775 FEET OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA.

LESS THE FOLLOWING DESCRIBED PARCEL "B" A PARCEL OF LAND LYING IN SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA; MORE PARTICULARLY DESCRIBED AS FOLLOWS:

THE SOUTH 35.30 FEET OF THE NORTH 938.50 FEET OF THE EAST 750 FEET OF THE WEST 775 FEET OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 35 SOUTH, RANGE 39 EAST, ST. LUCIE COUNTY, FLORIDA.

ALL TOGETHER CONTAINING 40.594 ACRES MORE OR LESS.

THE PROCEDURES AND OR NETWORK DESIGN MEET THE GEODETIC ACCURACY STANDARDS AND SPECIFICATIONS FOR USING GPS RELATED POSITIONING AS SET FORTH BY THE FEDERAL GEODETIC CONTROL SUB-COMMITTEE IN THEIR MOST CURRENT PUBLICATION FOR 3RD ORDER CLASS 1 HORIZONTAL CONTROL SURVEY OR PROVIDE THE HORIZONTAL ACCURACY FOR ALL NEW POSITIONS ESTABLISHED AS A POSITIONAL TOLERANCE.

COORDINATES INFORMATION STATE PLANE COORDINATES ARE BASED ON THE TRAVERSE MERCATOR PROJECTION FOR THE EAST ZONE OF FLORIDA AND REFERENCED TO NORTH AMERICAN DATUM OF 1927.

SCALE FACTOR = 0.99999961

GENERAL STATEMENT OF ACCURACY NO ADJUSTMENTS MADE TO CONTROL EAST-WEST ERROR 0.07' NORTH-SOUTH ERROR 0.01' OR 0.02' PER 5000'

- SURVEYORS NOTES:**
1. THIS SURVEY COMPLIES WITH CHAPTER 53-17, FLORIDA ADMINISTRATION CODE.
  2. NO SEARCH OF THE PUBLIC RECORDS HAS BEEN MADE BY THIS OFFICE.
  3. LEGAL DESCRIPTIONS SUPPLIED BY CLIENT OR CLIENTS AGENT.
  4. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.
  5. SOME ITEMS SHOWN HEREIN ARE FOR GRAPHIC AND INFORMATIONAL PURPOSES ONLY.
  6. VISIBLE ENCROACHMENTS, IF ANY ARE SHOWN HEREON.
  7. THIS SURVEY FOR CONVEYANCE PURPOSES ONLY AND NOT INTENDED FOR CONSTRUCTION PURPOSES WITHOUT PERMISSION OF THIS SURVEYOR.
  8. ELEVATIONS HEREON BASED ON NATIONAL GEODETIC VERTICAL DATUM (N.G.V.D. 1929)
  9. THIS IS NOT AN ENVIRONMENTAL SURVEY.
  10. THIS SURVEY DOES NOT GUARANTEE OWNERSHIP OR RIPARIAN RIGHTS BEYOND MEAN HIGH WATER LINE.
  11. BEARINGS HEREON BASED ON AN GRID BEARING ALONG THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 24 AND TAKEN TO BEAR N00°07'07"W AND ALL OTHER BEARINGS ARE RELATIVE THERETO.
  12. ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.
  13. THIS SURVEY IS NOT VALID WITHOUT SURVEYORS SIGNATURE & EMBOSSED SEAL.

**LEGEND:**

⊙ = CENTERLINE  
 calc = CALCULATED  
 meas = MEASURED  
 ORB = OFFICIAL RECORDS BOOK

PROPERTY SHOWN HEREON IS LOCATED IN FLOOD ZONE X PER FLOOD INSURANCE RATE MAP 120286 0167 J DATED FEBRUARY 16, 2012

NOTE: THIS SURVEY PERFORMED WITHOUT BENEFIT OF TITLE.

**MAGELLAN SURVEYING & MAPPING, INC.**  
 PROFESSIONAL LAND SURVEYORS  
 LICENSED BUSINESS - 7577  
 450 S. OLD DIXIE HIGHWAY, SUITE 10  
 JUPITER, FLORIDA 33458  
 561-746-8745 FAX 561-746-9632 E-mail info@magellansurveying.com

**Boundary Survey**

Prepared For: **KINGS HIGHWAY 40 ACRES**

Drawn by RES Date of Survey **MARCH 23, 2016** Scale **1" = 100'**

Field: B1-TFF.B.109Pg39 Certificate No. 5028 State of Florida

16-087



# CITY OF FORT PIERCE TECHNICAL REVIEW COMMITTEE

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## A. **Conceptual Site Plan** – Kings Hwy Business Park– 7001 White Road

Mr. Benton: Staff may recall a previous proposal for a commercial park subdivision across this property connecting White Road to Kings Highway with 10 -12 lots on site. The conceptual development plan was selected to generate some feedback from the City of Ft. Pierce in consideration of moving forward with this development and investment in this infrastructure.

### **FP Planning:**

The C-3 district is intended to provide for a broad variety of business activities including shoppers' goods stores, convenience goods and service establishments, offices and tourist/entertainment facilities. Many public and semi-public uses are also appropriate. Compared to the C-4 zone, this district is more suitable for uses requiring a high degree of accessibility to vehicular traffic, low intensity uses on large tracts of land, most repair services and small warehousing and wholesaling operations. Although this zone should be located along or near arterial or collector streets, it is not the intent of this district to encourage the extension of strip commercial areas. Instead it should promote concentrations of commercial activities. Eligible permitted and conditional uses are presented in the City's Use Table located within Section 22-22, with further consideration of consistency with the City's Comprehensive Plan/ GC- General Commercial Land Use designation.

- Pursuant to City Code Section 22-31 (c)(1) & 22-58, Site plan review shall be required for uses which have buildings with more than four thousand (4,000) square feet of floor area. Furthermore any development upon lots abutting Kings Highway or above 4,000 sq. ft. will be subject to the city's design review guidelines. The existing design review guidelines would guide relocation of building #1 & #6 towards Kings Highway for enhancement of streetscape and building design along Kings Highway.
- The presented parking "detail" exhibits present spaces compliant with City Code, however the site plan presents a collection of face-to-face parking spaces, which only feature an 18 ft. depth, whereas 19ft. depth is required.
- Per City Code Section 22-60(e) the Number of off-street loading spaces required will be guided based upon specific uses contemplated for inclusion within the development. The plan appears to feature adequate areas designated for such spaces.
- Height, size, and quantity of landscaping elements and corresponding irrigation systems will be considered during site plan review.
- The applicant may elect to present a comprehensive site plan for the complete development, with the potential for inclusion of conditional use for some variable uses or consider a PD,

Planned Development with a specific set of uses for the site, and variable phasing/building site options.

- Open Space requirements will be reviewed for compliance with the Comprehensive Plan at the time of formal development application.
- The C-3 zoning district requires interior vehicular use area landscape at a ratio of 1 SF per 15 SF of vehicular use area. Complete tabulation and review will be considered with subsequent development application.
- Pursuant to City Code Section 22-179. - Permit applications, an application for tree removal and land clearing shall be filed concurrent with any site development applications. The property owner is encouraged to preserve native trees where possible.
- Further review of traffic generation for the development, based upon intended/projected uses will be reviewed for consideration of traffic impacts to Kings Highway and Peters Road as well as parking levels.
- It's noted that Peters Road has abutting sections that are substandard at the current time.
- Additional landscaping requirements (22-187 (10) are provided for commercially zoned developments, which abut residentially zoned property. Appropriate separation and buffering strategies from adjacent residential properties will be required for formal development review.

#### **FP Building:**

1. They are going to put the road in and then develop the lots and sell the lots off?
2. The lake would be constructed during construction?
3. Utilities would be coming off from Kings Highway?
4. White road is an unimproved road?

Mr. Benton: The conceptual plan is considering a potential subdivision of general commercial properties, creating a base infrastructure, roadway connectivity and access. They would have the ability to build to suit, complete the structure and sell and or lease to a prospective business or sell the lot for individual development.

#### **FPUA Electric:**

Electric is available for the east half of this site. The west half is served by FPL. Please see attached map. Please contact Jason Mittler (772) 466-1600 x 6306 for details. Gas is available please contact Ana Johnson (772) 466-1600 x 4705 for details.

#### **FPUA Water/Wastewater:**

Approved as noted City approval does **not** constitute final acceptance by Fort Pierce Utilities Authority. Completed utility drawings need to be submitted for final approved by Fort Pierce Utilities Authority. Please contact James Carnes of Fort Pierce Utilities Authority Water/Wastewater

Engineering Department at (772) 466-1600 x 3472 to discuss detailed utility requirements, fees and other administrative items.

**FP Engineering:**

The survey and conceptual plan were submitted on 11" x 17" paper and were not to scale. At time of Site Plan submittal, the applicant shall submit full sized, scalable plans for review.

**SLC Engineering:**

1. Please provide a sealed copy of the survey. It has no validity without being sealed. I reserve my comments until receipt of sealed survey.
  2. The applicant is advised that a Road Improvement Agreement (RAI) together with the appropriate surety will be required for the construction of all improvements within County right of way. Please contact Rod Reed, County Surveyor at 462-1721 for additional information. Please be advised that a right of way permit will not be issued until the RIA and surety have been submitted.
  3. Please be advised that a right of way permit shall be required for the construction activities within the County road right of way. Please contact Selena Griffett, P.E. at 462-2097 for additional information.
- 
1. Advisory Comment: Advisory Comment: Please contact FDOT as major roadway and drainage improvements are proposed along King's highway and White road adjacent to your site.
  2. Advisory Comment: The site is located inside the Urban Service Boundary and additional requirements may need to be met.

**SLC Fire District:** Show water/fire lines plus show all hydrants within 1000' of property.

The following departments had no comments at the meeting: FP Code and SLC TPO

**Troy Holloway from Gentile Glas Holloway O'Mahoney & Associates:** We have reviewed and acknowledged all the comments and we will re-submit the full size sealed plans as we move forward.