



THE SUNRISE CITY

FORT PIERCE
PLANNING DEPARTMENT
Florida

TO: Nick C. Mimms, P.E., ICMA-CM, City Manager

FROM: Jennifer Hofmeister-Drew, AICP, LCAM, Planning Director

RE: **King's Landing - Application for a Major Amendment to the Planned Development (PD) and Site Plan (Development and Design Review) 21-0700006**

MEETING DATE: January 18, 2022

STAFF REPORT

Property Owner City of Fort Pierce Redevelopment Agency
100 North US Highway 1
Fort Pierce, FL 34950

Representative: Brad Currie, Vice President
Engineering Design & Construction, Inc.
10250 SW Village Parkway, Suite 201
Port St. Lucie FL 34987

Requested Action: Approval of an Amended and Restated ORDINANCE NO. 20-012 to replace the previously approved Conceptual Master Plan with a Site Plan that involves a Major Amendment to the Planned Development (PD) and Site Plan (Development and Design Review) Approval for the King's Landing Mixed Use Development Project

Site Location: 322 North 2nd Street

Parcel IDs: 2410-503-0043-000-2 2410-503-0019-010- 8
2410-503-0012- 010-9 2410-503-0019-000-5
2410-503-0041- 020-4 2410-503-0020-000-5
2410-503-0013- 000-3 2410- 503-0036-010-3
2410-503-0030-000-8 2410-503-0034-000-6

Utilities: Fort Pierce Utilities Authority (FPUA)

Existing Use: Vacant

Parcel Size: 7.824 +/- Acres

Current Future Land Use: Central Business District (CBD)

Current Zoning Planned Development (PD) with an underlying zoning of Central Commercial (C-4)

Surrounding Future Land Use (FLU) and Zoning

	North	East	South	West
FLU:	Residential High (RH) and I-1	CBD	CBD	I and General Commercial (GC)
Zoning:	Edgartown Settlement (ES)	Planned Unit Redevelopment (PUR) and C-4	PUR and C-4	General Commercial (C-3), I-1, and C-4

Staff Analysis:

Background

On November 14, 2018, the City of Fort Pierce and the Fort Pierce Redevelopment Agency issued a Request for Qualifications (RFQ No. 2019-003) to interested and qualified developers to submit a Statement of Qualifications for the redevelopment of several parcels generally known as the former H.D. King Plant site located in the heart of the City’s downtown. Home to the Fort Pierce Utilities Authority’s



RFP Proposed Rendering

electrical facility since circa 1911, the plant was demolished in 2008 and since that time has undergone years of extensive soil remediation and is prepared for redevelopment.

After evaluating the responses submitted in the competitive procurement process in response to RFQ No. 2019-003, the Fort Pierce Redevelopment Agency and Audubon Development, Inc., (Audubon) a Florida corporation, entered into a development agreement on December 2, 2019. At that time, the proposed development plan was conceptual in nature but included a hotel, condominiums, restaurants, retail, various residential unit types for ownership and rental occupancy. In accordance with the development agreement, the City was responsible for processing the approval of amendments to the Future Land Use Map from Industrial (I) to Central Business District (CBD) and a Zoning Atlas amendment from Light Industrial (I-1) to Planned Development (PD) with an underlying zoning of Central Business Zone (C-4).

Additionally, the development order called for the City to process the abandonment of a portion of the road right-of-way of Avenue B and to rectify any other inconsistencies that would affect title, all of which has been completed. In return, Audubon was required to apply and obtain approval from the City of Fort Pierce for a site plan, which is the subject of this application.

Upon approval of a site plan, the development agreement stipulates that the City will then convey the subject property to Audubon to develop in accordance with the terms and conditions set forth in the development agreement and the approved site plan. Please note, a pending amended and restated development agreement is scheduled for City Commission approval at the January 18, 2022, City Commission meeting to adjust the timelines in the agreement following the COVID state of emergency.

Request

Pursuant to Code Sections 125-157, 125-201, 125-212, and 125-313.; 125-314, the applicant is seeking approval for a major amendment to the PD and Site Plan (Development and Design Review) to construct a mixed-use development project on 7.824 acres generally located at 322 North 2nd Street.

Downtown Business and Entertainment District Overlay

The majority of the subject property is located within the Downtown Business and Entertainment District Overlay (downtown overlay) that delineates the downtown and defines desirable characteristics of this area that is distinct from the rest of the City.

The purpose of this overlay is to:

- Recognize the unique urban character of the downtown;
- Recognize the unique orientation to the waterfront; and
- Recognize its historic urban design typified by its pedestrian-oriented design and its human-scale development to further the downtown's desirability as a place to work, play and live through encouraging a broad variety of urban-scale uses.

The area that is not currently within the downtown overlay is the portion proposed for eight (8) townhouses south of AE Backus Avenue. Planning staff is currently working to expand the Central Business District boundaries with subsequent changes to the boundaries of the downtown overlay to correspond with the boundaries of Central Business District that will incorporate the entirety of the subject development.

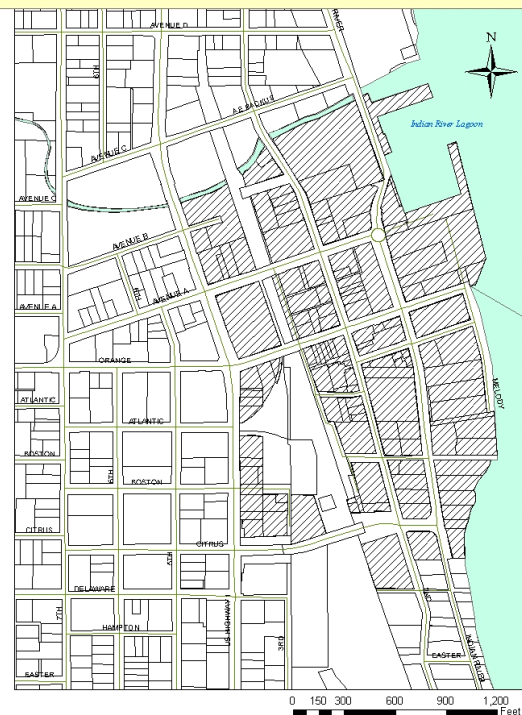
Pursuant to Code Section Sec. 125-157(2)3.(i) and (ii), all parcels within the district are exempt from off-street parking and landscaping requirements.

Amendment to the adopted PD

The purpose of the PD District is to provide a process for the evaluation of individually planned developments which may not otherwise be permitted in the zoning districts. The PD District is to be a voluntary process commenced by an applicant for such zoning designation. The standards and procedures of this district are intended to promote flexibility of design and permit planned diversification and integration of uses and structures, while at the same time granting the city commission the absolute authority to establish such limitations and regulations as it deems necessary to protect the public health, safety and general welfare. The PD district is designed to allow an applicant to submit a proposal for consideration, for any use or any mixture of uses.

Changes to approved PD site plans are either major modifications or minor modifications. A major modification requires the approval of the city commission, while a minor modification may be done administratively by the planning director. Generally, additions, deletions, changes in the use, density, sequence of development or other specifications of an approved PD plan are to be viewed as a major modification. The subject petition is deemed as a major amendment as the master plan for the development that was adopted as an exhibit to the adopting Ordinance No. 20-012, was conceptual in nature and proposed a phasing plan. The subject site plan will replace the conceptual master plan and is proposing a single phase of development.

Downtown Business and Entertainment District
FIGURE 1-2



Site Plan (Development and Design Review) Criteria

All development requiring site plan approval are subject to the City's design review process. In addition, the City's Planning Board functions as the design review board being responsible for such design review, which is completed as part of the development review process. Design review related to any changes to historic structures, or any new construction on an historic site or in an historic district shall also be reviewed by the Historic Preservation Board in accordance with Chapter 111.

The quality and compatibility of any proposed development in the City is of public concern for all building and site improvements. The City's design review guidelines suggest approaches to design situations that will contribute to the existing and developing character of the City. The intent of the City's design review process is not to stifle innovative architecture but rather to ensure respect for and reduce incompatible and adverse impacts on the visual experience throughout the City. The City Code seeks to accomplish appropriate design through respectful interpretation of vernacular building typologies and styles in proposed development.

The City Code encourages proposed designs to take cues from traditional styles characteristic of the City's architecture, including but not limited to wood and masonry vernacular buildings characterized by a blend of Cracker, Classical Revival, Victorian, Mediterranean, including Mission and Spanish Eclectic, Streamlined Art Deco, Mid-Century Modern, Colonial, Prairie and Craftsman styles. Interpretation of this vernacular palette, influenced by the tropical climate and regional architectural patterns, is strongly encouraged. The guidelines serve to encourage quality site and architectural design and construction compatible with the scale and character of the City's mix of existing buildings and land uses.

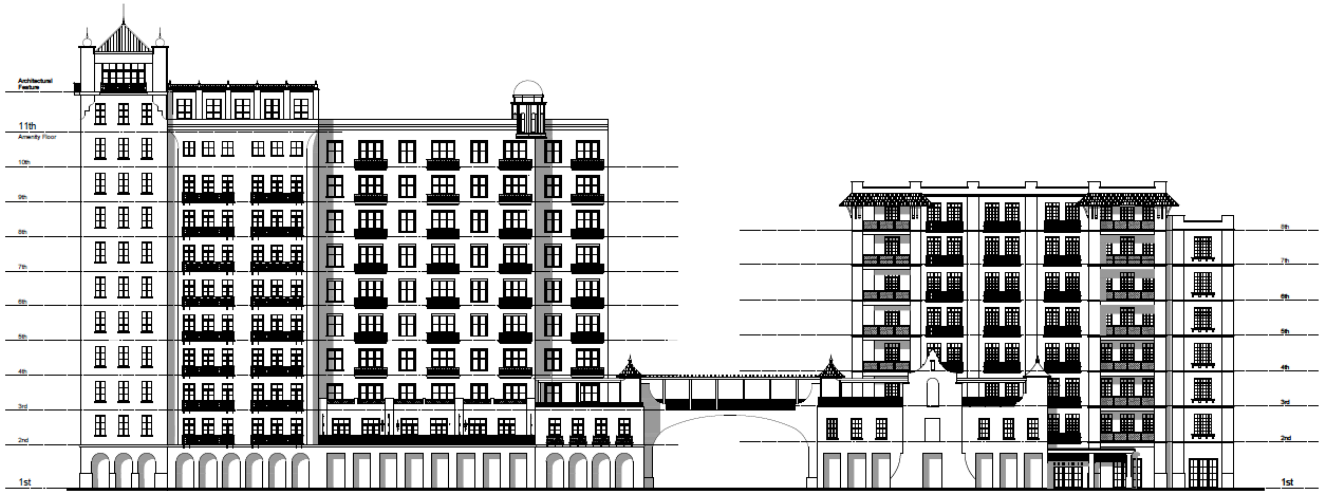
The following elevations shown of the subsequent pages demonstrate the architectural style of the proposed King's Landing mixed use development. Although the design is described as contemporary, featured elements include historical elements of the Mediterranean Revival, Mission, Bungalow, and Masonry Vernacular styles.



NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION

The proposed site plan for King's Landing is an urban mixed-use development comprised of the following uses and associated density and intensity:

USE	DENSITY/INTENSITY
Eight (8) Story Hotel	140 Rooms
Retail	17,290 square feet +/-
Restaurants	35,085 square feet +/-
Residential	114 Units (106 Condos and 8 Townhomes)

The proposed site plan is an exemplary illustration of an urban infill mixed-use development. The mix of uses are internally complementary and also offer a desirable blend with the surrounding community. The site plan was laid out with varying roof heights with the taller, more intense buildings placed internal to the site and gradually reducing in height along to North 2nd Street and AE Backus Avenue.



The proposed structures have been labeled as follows:

- Building A - Three (3)-story restaurant, retail and residential building (50 feet in height)
- Building B - Two (2)-story restaurant building (40 feet in height)
- Building C - Eleven (10 + roof top -story) restaurant, retail and residential building (121 feet in height)
- Building D - Eight (8)-story restaurant, retail and residential building (107 feet in height)
- Building E - Five (8)-story 140 room hotel (107 feet in height)
- Building F - One (1) story restaurant and retail building
- Building G - Eight (8) story retail and residential building (107 feet in height)
- Eight (8) 2 (2) story two (2) unit townhouses (40 feet in height)
- Proposed three (3) story parking structure - 170 spaces

The main entrance into King's Landing will be a two-way access from Indian River Drive. Additional entree is proposed from both Avenue B and North 2nd Street. Entrance into the parking structure is proposed from the north via the main access drive and from the south from Avenue B. The development is providing a total of 299 parking spaces, 129 as surface parking with the remaining 170 spaces proposed within the parking structure.

The entire development will be interconnected with paver brick sidewalks. The pedestrian ambiance is accentuated with ample outdoor seating areas dispersed throughout the King's Landing development. Centerpiece to the project is Moore's Creek and the design takes into consideration the significance of this natural feature by incorporating pedestrian scale amenities such as wide sidewalks along the water's edge, a kayak/canoe launch, and benches. The applicant is also proposing in areas where in-ground landscaping is limited, the use of potted plants and other elements that contribute to the hardscape design of the development.

The plan is also designed so that the area closest to the Edgartown Settlement (ES) Historic District will comprise the eight (8) proposed townhouse units. This will allow a complementary transition into the ES District, comprised of small scale, compatible residential and commercial uses.

Traffic Analysis

With regard to the traffic impacts, an analysis was conducted by O'Rourke Engineering & Planning, who defined the study area roadways in terms of the existing lane geometrics and traffic volumes. The study area was reviewed to determine the existing number and type of lanes, and the traffic control along the roadway. Traffic volumes were obtained from the St. Lucie County Transportation Planning Organization (TPO) and the Florida Department of Transportation (FDOT). The TPO 2019 Level of Service Report was used to establish capacity.

To estimate future traffic generated by the development, the ITE 10th Edition *Trip Generation* rates for the following classifications: multifamily housing; shopping center; hotel; and high-turnover (sit-down) restaurants; were used to estimate the trips generated by the proposed development. It was determined that the project will generate 4,519 net new daily trips, consisting of 306 net new AM peak hour trips with 180 trips entering the project and 126 trips exiting the project. Additionally, the project will generate 329 net new PM peak hour trips, with 193 trips entering the project and 136 trips exiting in the site in the PM peak hour.

The project driveways as well as the intersections of AE Backus Avenue and 2nd Street and Indian River Drive and Avenue B were analyzed for the AM and PM peak hours. The results indicated that AE Backus Avenue and 2nd Street will operate at a LOS A in the AM and PM peak hours. Indian River Drive and Avenue B will operate at a LOS B in the AM and PM peak hours with the addition of project traffic at project build out. The findings of the traffic analysis concluded that with the net new peak hour trips, all links and intersections will operate at acceptable levels of service.

Conclusion

The subject development is perfectly positioned at a prominent downtown location, overlooking the Indian River waterfront and city marina. The architectural style is contemporary but features design elements that blend with the existing surroundings and urban marina setting and scenic beauty. The scale of development provides a transition from the neighboring Edgartown Settlement District with the proposed townhouse villas on the north bank of Moore's Creek and the buildings that front North 2nd Street to the larger mixed use structures center to the development. The proposed development maintains a human scale and pedestrian orientation through dedicated walkways, shaded arcades, and expansive storefronts. The extensive landscaping provides an inviting and relaxing atmosphere. Additionally, it is envisioned that the King's Landing development will also compliment the uses planned for Fisherman's Wharf. It is likely that King's Landing will serve as the catalyst for future redevelopment and economic prosperity for the downtown and the City as a whole.

Technical Review Committee (TRC)

All affected city and county departments have reviewed the proposed major amendment to the approved PD Development and Site Plan (Development and Design) Review applications and all comments have been addressed.

Staff Recommendation on the Planned Development Major Amendment Application

The requested applications meet the criteria specified in Code Sections 125-157, 125-201, 125-212, 125-313 and 125-314 of the City Code, and the Comprehensive Plan, and does not adversely affect the public health, safety, convenience, and general welfare. Staff recommends that the Planning Board recommend **Approval** of the subject application for a major amendment to the Planned Development.

Planning Board Recommendation

At the special meeting of October 25, 2021, the Planning Board recommended approval (6-1) on the Major Amendment to the Planned Development and Site Plan.

Historic Preservation Board Action

On May 24, 2021, the Historic Preservation Board (HPB) approved the Certificate of Appropriateness (COA) 21-19 (4-0, 2 abstained) for King's Landing Mixed Use Development.

On November 15, 2021, the HPB reviewed for the second time the COA 21-19R following the Planning Board meeting as the height and massing of the project changed subsequent to the HPB original meeting. A Motion to Approve failed and a Motion to deny was not made.

On December 14, 2021, the third review of the COA 21-19R by the HPB was required as no Motion was made by the Board. Discussion of the changes to the height and massing in addition to the number of hotel floors was discussed. It was affirmed that the hotel will consist of 8 floors rather than the previously stated five (5) floors, with the height remaining the same at 107 feet. The Board approved the COA 21-19R (3-2, 2 abstained).

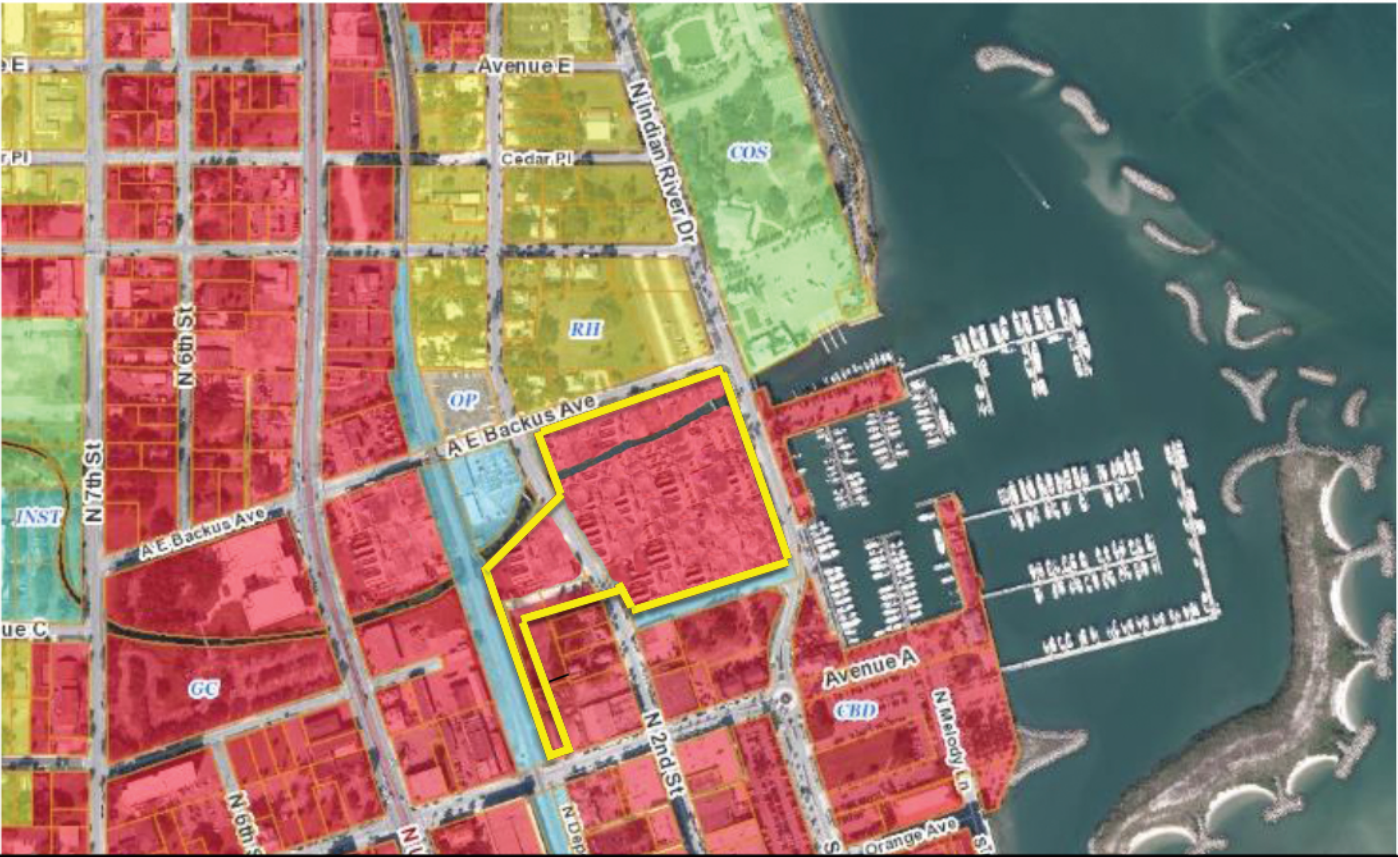
Location Map



KING'S LANDING
322 North 2nd Street



Future Land Use Map



KING'S LANDING
322 North 2nd Street



Zoning Map



KING'S LANDING
322 North 2nd Street



August 27, 2021

Shyanne Harnage
City of Fort Pierce City Manager's Office
100 North US Highway 1
Fort Pierce, FL 34950

**Re: Fort Pierce Redevelopment Agency
Kings Landing Development Review**

Dear Ms. Harnage:

On behalf of our client, please find the attached response to comments received on May 19, 2021 for a project known as Kings Landing. Each comment is identified below followed by a response in ***bold italics***.

Planning

1. Parking

- Angled parking along Indian River Drive is not supported. The subject angled parking requires the vehicle to back into a travel lane, which is not allowed per City Code.

RESPONSE: We are to the understanding that the City is meeting with the County to discuss the angled parking in an effort to include it as shown.

- There appears to be an abundance of parking as both surface and structured parking is proposed. The surface parking provided should provide landscaped tree islands every 10 spaces.

RESPONSE: Please see revised site plan which includes a landscape island every 10 spaces.

- At the southeast corner of Moore's Creek and North 2nd Street, consider on-street parking and landscaped medians to provide an appealing urban entrance for this downtown project.

RESPONSE: This area is needed for surface area parking for the hotel. Sufficient landscaping is proposed to buffer the parking and provide an entrance to the downtown.

- Please provide a parking calculation that is the professionally accepted standard for mixed use with passerby to better determine what the minimum and maximum of parking is appropriate for the development.

RESPONSE: The project is located in the Downtown District. There are no parking requirements established for this area.

2. Relationship to natural features of the area.

- The development should not only take advantage, but fully develop around Moore's Creek. The Creek should be the centerpiece of the development. For that, please consider raising the vehicular bridge on Indian River Drive to allow small vessels into the project area.

RESPONSE: *The proposed development includes a kayak and canoe launch. This amenity along with interpretative signage assists in creating Moore's Creek as the centerpiece of the development.*

- The southeast corner of Moore's Creek and North 2nd Street design should consider incorporating activities related to the Creek's edge. Moore's Creek is a public amenity as well as one that can be enjoyed by the hotel guests. Amenities to be considered include a kayak launch, benches, observation areas, interesting lighting, Florida-friendly landscaping, trash receptacles, etc.

RESPONSE: *Amenities such as benches, kayak and canoe launch are proposed as part of this development at Moore's Creek.*

- Consider interpretative signage and a map that shows the length of the greenway along Moore's Creek into the Historic Lincoln Park neighborhood.

RESPONSE: *Applicant agrees and details will be provided at the time of building permit submittal.*

3. Relationship to natural features of the area.

- Please consider entrances into the site from Indian River Drive, Avenue B and 2nd street.

RESPONSE: *The site plan shows entrances to the development from Indian River Drive, Avenue B and 2nd Street.*

- Consider brick paver crosswalks at the interior Avenue B intersections to provide a pedestrian friendly environment and connect with the surrounding downtown area.

RESPONSE: *Concrete brick pavers crosswalks have been incorporated in the attached revise site plan.*

- Include softscape components to balance the limited areas to landscape.

RESPONSE: *Potted plants have been added to the landscape plans as placeholders at this time as requested. They are located adjacent to the buildings along the roadways where no landscaping is provided with in ground planters. The Client intends to provide potted plants and other elements throughout the project, but would like the opportunity to select those items during the hardscape design phase to allow for harmonious selection of the elements being provided.*

- This development should be a continuation of the downtown area. Connectivity, walkability, and access should be important elements.

RESPONSE: *The attached site plan shows vehicular and pedestrian connectivity to all roadways.*

- Consider widening the perimeter sidewalks to allow for more comfortable two-way pedestrian traffic.

RESPONSE: *The proposed site plan is designed to have 10' sidewalks for most of the*

exterior pedestrian connectivity and where possible.

- Consider having a variety of architectural styles that preserve or enhance the character of the area and reflect a sense of place.

RESPONSE: The elevations have previously been approved by the City Council and as part of the Certificate of Appropriateness Application and we feel it does preserve or enhance the character of the area and reflect a sense of place.

- Additional height, beyond the existing building heights in downtown, is supported as long as the architecture/design will provide comfortable pedestrian urban environments and comfortable transition to the Edgartown historic district.

RESPONSE: Acknowledged.

- The development should maintain and reinforce the human scale and pedestrian orientation by continuing the pattern by which buildings and other site elements relate to the street.

RESPONSE: Acknowledged.

4. Entrance and public features.

- At the northeast corner of North 2nd Street and Avenue B consider camouflaging the Utility Vault Box and configure a brick paver sidewalk from North 2nd Street to Indian River Drive on the north side of Avenue B.

RESPONSE: The utility vault box will be camouflaged and pavers are called out on the north side of Avenue B on the attached site plan.

- Consider more spaces for public gathering and outside entertainment in various areas of the development.

RESPONSE: The current design does not allow for additional space for public gathering. Like many other downtown areas, there is an opportunity to close down the streets for special events. The sidewalks propose exceed the minimum requirements to increase the feel of a more friendly gathering place.

- Consider interesting grand entrances into the site such as a fountain feature with tables and chairs, potted plants / tree wells, signage, etc. This design would help create a public space for gathering or meeting before heading into the development itself.

RESPONSE: Concrete pavers have been added at the entrances and pedestrian connections between the buildings. Building design will include porticos as part of the retail building to assist in creating the feel of a gathering or meeting place before entering into the development itself.

Need for additional information.

- What is the maximum height (show on site plan)?

RESPONSE: Please see revised site plan. The maximum height is depicted on the plans.

- What are the setbacks (show on site plan)?

RESPONSE: The setbacks are now depicted on the site plan.

- Will this development be phased? If so, provide a phasing plan showing what will occur in each phase.

RESPONSE: Vertical construction would be phased per the attached plan but the site work will be constructed as a single phase.

- Provide a colored site plan (aerial overview) that also includes the landscaping.

RESPONSE: This will be provided prior to scheduling of this project for City Council.

- The Traffic Study shall be approved and satisfied by St. Lucie County and their reviewer before the item is forwarded to the Planning Board for review.

RESPONSE: Acknowledged.

Engineering

1. Remove all proposed angled parking along Indian River Drive and Avenue B in accordance with the City of Fort Pierce Code of Ordinances prohibiting the creation of parking spaces that permit vehicles to back into a public roadway.

RESPONSE: We are to the understanding that the City of Fort Pierce is meeting with St. Lucie County to discuss the angled parking in an effort to include it as shown.

2. All proposed driveways shall be comprised of concrete or concrete brick pavers in accordance with the City of Fort Pierce Code of Ordinances Section 32-9.

RESPONSE: Concrete brick pavers have been incorporated into all driveway entrances on the attached site plan.

3. Provide parking lot dimensions for all parking spaces and access aisles. How many overall parking spaces will be provided for the site, including spaces in the garage? Also, how many handicapped spaces are proposed?

RESPONSE: Please see revised site plan which includes the proposed number of parking spaces including handicap along with details for each type.

4. A conceptual drainage plan was not included in the submittal packet, please forward a copy of this plan for review.

RESPONSE: A conceptual drainage plan is included with this response.

5. Avenue B is shown as having a one-way roadway width of 12' at the west end, please revise this width to reflect a 14' minimum one-way width.

RESPONSE: Please see revised site plan. This change is reflected.

6. What is the driveway spacing between the driveways along AE Backus Avenue? Driveway construction for a new platted development shall meet the requirements specified in the City of Fort Pierce Code of Ordinances Section 125-316; such as 50' driveway spacing, setbacks of 25' from side of property line and 75' setback for corner lots.

RESPONSE: The driveway spacing proposed meets the minimum requirements of code.

7. Provide all lot information for the lots fronting AE Backus Avenue. Will this land be

platted into individual lots? If so, how will the on-site stormwater treatment be handled? What are the proposed building setbacks from the east and south property lines? If platting is not proposed, then a Unity of Title will need to be filed combining the two separate parcels.

RESPONSE: The property is going to be platted. The stormwater plan will be developed at the time of site development review.

8. There are three (3) driveways that access AE Backus that will only have an on-site depth of 8'. This is not sufficient depth to provide vehicle parking without encroaching into the right-of-way, please provide a minimum depth of 20' for vehicle parking.

RESPONSE: The site plan has been revised to reflect a 20' setback.

9. Provide curbing within the parking areas as required per Section 123-37(11)(d).

RESPONSE: Please see revised site plan which addresses this comment.

10. Being as multiple improvements are proposed across two parcels of land, a Unity of Title will be needed to be filed which ties Parcel No. 2410-503-0041-020-4 and Parcel No. 2410-503-0043-000-2 together as one parcel of land.

RESPONSE: The entire project is actually comprised of 10 tax parcels. Some of the parcels are owned by the City of Fort Pierce and some are owned by the Fort Pierce Redevelopment Agency (FPRA). The Development Agreement between Audubon Development, the City of Fort Pierce and the FPRA requires that at Closing, the City will convey it's parcels to the FPRA and then the FPRA will convey all of the property to Audubon. However, if the City desires to convey all of the property to FPRA now, and for FPRA to sign and record a Unity of Title unifying all of the parcels (or maybe just the adjacent parcels), we would have no objection.

11. Revise the landscape plan to indicate all required landscaping situated within the limits of the applicant's lease parcel. If additional landscaping is desired to be installed in the limits of public road rights-of-way, a landscape maintenance agreement with the City of Fort Pierce will be needed.

RESPONSE: The Client would like to provide landscaping within the City's right of way. Once an agreed upon landscape plan is approved by the City, the process will be started for the maintenance agreement.

12. The landscape plan shall depict the clear vision areas at all intersections and driveways, refer to the City of Fort Pierce Code of Ordinances Section 125-308 for requirements.

RESPONSE: The 20' clear vision areas have been added to the landscape plan per the City's Code listed above. Any plantings that didn't meet those requirements were relocated outside of those triangles.

13. Relocate the landscaping from the property on the south side of Avenue B as this land is under private ownership.

RESPONSE: The landscaping was removed from this area as requested.

14. Indian River Drive is a St. Lucie County roadway; the applicant shall contact St. Lucie County directly for possible R/W donation and required improvements with the right-

of-way.

RESPONSE: Comments have been received from St. Lucie County and responses are included as part of this response.

15. The submitted site plan indicates that the proposed zoning for the development is PUD, will a Rezoning application request be submitted as the current zoning is identified as C4?

RESPONSE: The rezone to C4 has been approved by the City of Fort Pierce.

16. The proposed pedestrian walkway south previously abandoned Avenue B right-of-way is shown to decrease in width from 10' to 6'. The applicant shall revise the plan to reflect the continuation of the 10' wide walkway south to the noted POB of Parcel 5.

RESPONSE: The attached site plan maximizes the sidewalk width where possible.

Police

1. Please review the photometric plan calculations to ensure the average illumination for the parking lot area meets or exceeds the requirement of two (2) foot-candles set forth by city code.

(Lighting) Off-street parking shall be provided with lighting as follows: For commercial, industrial and governmental uses, a minimum average of two footcandles.

Currently, the calculation summaries indicate an average of 0.94 and 1.43 foot-candles for two of the parking lot areas listed ("off street parking / site & parking).

RESPONSE: Proposed site lighting will comply with the City of Fort Pierce Land Development Code.

FPUA

1. FPUA W/WW Engineering: Water and wastewater are available to serve the proposed development. Please submit three sets of utility construction drawings to FPUA's W/WW Engineering Department for formal review and approval. It is recommended to Contact Lugey Dawson @ (772) 466-1600 ext. 3428 to setup a pre-design meeting to discuss the required utility improvements surrounding the site.

RESPONSE: Acknowledged.

2. FPUA Electric & Gas Engineering: Approved. Below are comments in reference to FPUA electric and gas service availability and requirements: Electric service is available to the site from Avenue B. Please provide electric load information for the new service and AutoCAD drawing. Customer will be responsible for all transformer pads, conduits and secondary conductors. For more information and project coordination, please contact Sal Scimeca.

Sal Scimeca

Engineering Technician II

Electric and Gas Engineering

1701 S. 37th Street, Fort Pierce, FL 34947

sscimeca@fpu.com

Office: (772)466-1600 Ext. 6957

RESPONSE: Acknowledged.

3. Gas service is available to the site from Avenue B. Please provide a copy of the gas riser diagram with the anticipated load (if owner is interested in natural gas service). For more information and incentive available, please contact Ana Johnson.

RESPONSE: Acknowledged.

St. Lucie County Public Works

1. The traffic report will be reviewed by a third party through the County.

RESPONSE: Acknowledged.

2. The landscape along Indian River Drive shall not impair line of site.

RESPONSE: The 20' clear vision areas have been added to the landscape plan per the City of Fort Pierce's Code of Ordinances Section 125-308. Any plantings that didn't meet those requirements were relocated outside of those triangles.

3. A Site Development Permit will be required from the County prior to implementation of site activities. The SD Permit will provide for right-of-way improvements.

RESPONSE: Acknowledged.

4. Written response to Pre-application review has not been received.

RESPONSE: Please find the attached response to comments to address Public Works pre-application comments.

5. For discussion regarding these comments, please contact me at 772-462-1491, haysd@stlucieco.org or Grant Chambers at 772-462-2741, chambersg@stlucieco.org.

RESPONSE: Acknowledged.

St. Lucie County TPO

1. Historical Growth Rate: The calculations were not provided in the Traffic Analysis for the development to justify the use of a historical growth rate of 1.88 percent. In the absence of the calculations, a historical growth rate of 2.5 percent should be utilized which is the actual annual growth rate in St. Lucie County since 1995.

RESPONSE: The attached calculations were inadvertently left out of the report. The data summarized in this Table was included in Appendix B. The average annual growth rate may be 2.5% for the County overall, but that rate is not consistent throughout the County. In the western part of St. Lucie when considering the pace of development, the effective resultant growth rate is much higher so one would expect it to be lower elsewhere.

We do not recommend changing the growth rate. However, reviewing the link tables, the link that had a volume the closest to its capacity was Orange Avenue from 7th Street to US 1. During the AM peak hour the westbound value was 485 (existing $427 * 1.088^4 + 25$) versus a capacity of 540. If we applied the 2.5%/year versus the 1.88% per year to the background volume, the result would be a total traffic value of 496 (existing $427 * 1.025^4 + 25$). Therefore, a change of the growth rate would not significantly alter the

results. Similarly, the intersections and driveway all operated at LOS A or B. Therefore, if the growth rate were increase the results would not change significantly.

2. Shared-Use Path and Historic Downtown Fort Pierce Retrofit SUN Trail Connectivity: The development appears to include wide sidewalks along Moore's Creek and the streets surrounding the development. However, a shared-use path of at least 12 feet in width should be provided instead of the sidewalks along AE Backus Avenue, North 2nd Street, and Moore's Creek. The path should be identified on the development plans to connect with the Historic Downtown Fort Pierce Retrofit segment of the Florida Shared-Use Nonmotorized (SUN) Trail/East Coast Greenway depicted in the attached map that is being designed and constructed through the entirety of St. Lucie County. The provision of this path and connectivity will serve as an additional attracter to the development.

RESPONSE: There are sidewalks and loggias proposed within the development that allow for pedestrian connectivity.

St. Lucie County Fire

1. Please submit a completed application for Development/Site Plan Review (St. Lucie County Fire District Development & Site Plan Review Application). This form is available on-line at <https://www.slcfcd.com>

RESPONSE: This will be submitted to SLCFD under separate cover.

2. Fire District review fees are due at the time of submittal. An abbreviated fee schedule is included on the application form.

RESPONSE: This will be submitted to SLCFD under separate cover.

3. Please send the Fire District electronic plans for the site and buildings.

RESPONSE: This will be submitted to SLCFD under separate cover.

4. A separate review and permit is required for Underground Fire Mains connected to standpipes or sprinkler systems.

RESPONSE: Acknowledged.

5. Fire hydrants (shall be) are provided for buildings other than detached one-and-two-family dwellings IAW both of the following 1) The maximum distance to a fire hydrant from the closest point in the building shall not exceed 400 feet. 2) The maximum distance between fire hydrants shall not exceed 500 feet. NFPA 1:18.5.3. Please provide fire flow calculations for hydrants.

RESPONSE: Acknowledged.

6. An approved water supply capable of supplying the required fire flow for fire protection (shall be) is identified to all premises upon which facilities, buildings, or portions of buildings which are to be constructed or moved into the jurisdiction. The approved water supply shall be in accordance with NFPA 1:18.4. See "Needed NFPA Fire Flow Calculator Spreadsheet".

RESPONSE: Acknowledged.

7. Per the St. Lucie County Fire District Fire Prevention Code Resolution 647-17. At Least 13 feet 6 inches nominal vertical clearance shall be provided and maintained over the full width of all means of access. Including, but not limited to trees, canopies, etc.

RESPONSE: Acknowledged.

8. Fire department connections shall be located on the street side of buildings and shall be located and arranged so that hose lines can be readily attached to the inlets without interference from any nearby objects, including buildings, fences, landscaping, or other fire department connections. The locations of connections shall be based upon the access requirements of the fire department.

RESPONSE: Acknowledged.

9. The distance allowed between the fire department connection and a fire hydrant shall be no more than one hundred fifty (150) feet as a vehicle travel.

RESPONSE: Acknowledged.

10. The Fire District reserves the right for future comments at the site plan & building construction phase.

RESPONSE: Acknowledged.

11. FD access roads (shall be) are provided such that any portion of an unsprinklered facility or exterior wall is located not more than 150' from FD access roads as measured by an approved route around the exterior of the building or facility. (450' for NFPA 13, 13D, 12R sprinklered protected buildings).

RESPONSE: Acknowledged.

12. Per NFPA 1114 Chapter 9, Section 1.3. Prior to the final occupancy of any building, the permitted water supply for fire protection, including fire hydrants and fire suppression systems, shall be installed, tested, and acceptable to the AHJ (SLCFD).

RESPONSE: Acknowledged.

13. Be advised: Dimensions of largest vehicle are as follows: 38 tons or 77k lbs., 47.5 ft. total length, 21.5 ft. wheel base, 10.5 ft. total width, 41.5 degree turning radius.

RESPONSE: Acknowledged.

14. Site Plans shall include all necessary fire department access roadways and fire lanes as determined by the Fire Marshal. At least 13 feet 6 inches nominal vertical clearance shall be provided and maintained over the full width of all means of access. Including, but not limited to trees, canopies, etc., as referred to in Exhibit "A," attached hereto.

Minimum roadway pavement width (two-way traffic) shall be twenty (20) feet.
Minimum roadway pavement width (one-way traffic) shall be twelve (12) feet.

RESPONSE: Acknowledged.

15. Two-Way Radio Enhancement Systems/BDAS shall be installed, inspected and operationally tested in accordance with the manufacturer's published requirements, by the local fire department, and comply with the most current edition of the Florida

Fire Prevention Code and its incorporated standards and codes. Pre-surveys of radio signal strength shall be submitted to the Fire Marshal in the form of heat signature mapping or a certification document of radio signal strength provided by a licensed engineer.

RESPONSE: Acknowledged.

16. Minimum Size of Water Mains

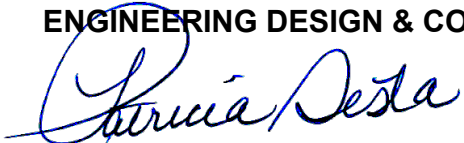
- a. The minimum size of water mains for supplying water for firefighting purposes shall be six (6) inches.
- b. The minimum size of water mains supplying hydrants on a dead-end main shall be eight (8) inches.
- c. The maximum number of hydrants located on a dead-end main shall be one (1).
- d. Grid or Tee systems shall be supplied by a minimum of an eight (8) inch looped main. Exceptions may be granted based on the capacity of the water distribution system but in no case shall the main size be less than six (6) inches.
- e. The minimum size water main(s) shall be determined by the needed fire flow as established by the Fire Marshal and based on the current Insurance Service Office (ISO) requirements.

RESPONSE: Acknowledged.

We feel the attached adequately addresses staff comments and respectfully request approval of the submitted applications. If you have any questions regarding this application, the attached documents, or the project, please contact our office.

Respectfully,

ENGINEERING DESIGN & CONSTRUCTION, INC.



Patricia Sesta
Planner

cc: Dale Matteson – Audubon Development

Z:\EDC-2019\19-361 - Kings Landing - HD King\ENGINEERING\Documents\Submittal Documents\Comment Response Letter\2021-08-27_8_Creagan_Kings_Landing_Rsp2Cmts_City_Ltr_19-361.docx



May 19, 2021

Brad Currie
10250 SW Village Parkway, Suite 201
Port St. Lucie, FL 34987

**Subject: Development Review – Kings Highway Commercial Park
Technical Review Project # 21-0700006**

Fort Pierce Planning:

1. Parking

- o Angled parking along Indian River Drive is not supported. The subject angled parking requires the vehicle to back into a travel lane, which is not allowed per City Code.
- o There appears to be an abundance of parking as both surface and structured parking is proposed. The surface parking provided should provide landscaped tree islands every 10 spaces.
- o At the southeast corner of Moore's Creek and North 2nd Street, consider on-street parking and landscaped medians to provide an appealing urban entrance for this downtown project.
- o Please provide a parking calculation that is the professionally accepted standard for mixed use with passerby to better determine what the minimum and maximum of parking is appropriate for the development.

2. Relationship to natural features of the area

- o The development should not only take advantage, but fully develop around Moore's Creek. The Creek should be the centerpiece of the development. For that, please consider raising the vehicular bridge on Indian River Drive to allow small vessels into the project area.
- o The southeast corner of Moore's Creek and North 2nd Street design should consider incorporating activities related to the Creek's edge. Moore's Creek is a public amenity as well as one that can be enjoyed by the hotel guests. Amenities to be considered include a kayak launch, benches, observation areas, interesting lighting, Florida-friendly landscaping, trash receptacles, etc.
- o Consider interpretative signage and a map that shows the length of the greenway along Moore's Creek into the Historic Lincoln Park neighborhood.

3. Relationship to the surrounding built environment

- o Please consider entrances into the site from Indian River Drive, Avenue B and 2nd street.
- o Consider brick paver crosswalks at the interior Avenue B intersections to provide a pedestrian friendly environment and connect with the surrounding downtown area.
- o Include softscape components to balance the limited areas to landscape.
- o This development should be a continuation of the downtown area. Connectivity, walkability, and access should be important elements.
- o Consider widening the perimeter sidewalks to allow for more comfortable two-way pedestrian traffic.
- o Consider having a variety of architectural styles that preserve or enhance the character of the area and reflect a sense of place.
- o Additional height, beyond the existing building heights in downtown, is supported as long as the architecture/design will provide comfortable pedestrian urban environments and comfortable transition to the Edgartown historic district.
- o The development should maintain and reinforce the human scale and pedestrian orientation by continuing the pattern by which buildings and other site elements relate to the street.

4. Entrance and public features

- o At the northeast corner of North 2nd Street and Avenue B consider camouflaging the Utility Vault Box and configure a brick paver sidewalk from North 2nd Street to Indian River Drive on the north side of Avenue B.
- o Consider more spaces for public gathering and outside entertainment in various areas of the development.
- o Consider interesting grand entrances into the site such as a fountain feature with tables and chairs, potted plants / tree wells, signage, etc. This design would help create a public space for gathering or meeting before heading into the development itself.

• Need for additional information

- o What is the maximum height (show on site plan)
- o What are the setbacks (show on site plan)
- o Will this development be phased? If so, provide a phasing plan showing what will occur in each phase.
- o Provide a colored site plan (aerial overview) that also includes the landscaping.

- The Traffic Study shall be approved and satisfied by St. Lucie County and their reviewer before the item is forwarded to the Planning Board for review.

Please provide a written response to all TRC comments and provide an electronic submittal of materials (complete submittal packet). All comments must be satisfied and addressed to move onto the Planning Board review stage. The presented review is specific to the proposed facility. Please contact me should you have any questions regarding the project at (772) 467-3742 or by e-mail: bcreagan@cityoffortpierce.com.

Sincerely,

A handwritten signature in blue ink, appearing to read "Brandon Creagan".

Brandon Creagan, MCRP, LEED Green Associate
Planner



THE SUNRISE CITY

FORT PIERCE

ENGINEERING
DEPARTMENT

Florida

TO : Brandon Creagan, Planner

FROM : John R. Andrews, P.E., City Engineer

JRA

**RE : Kings Landing Development Review
TRC No. 21-0700006**

DATE : May 18, 2021

This is to advise you that we have completed the review of the following documents as received by this office on May 12, 2021:

- | | |
|---|--|
| <input checked="" type="checkbox"/> Development Application | <input type="checkbox"/> Construction Drawings |
| <input type="checkbox"/> Test Reports & Related Documents | <input type="checkbox"/> Executed Construction Contract |
| <input type="checkbox"/> Record Drawings | <input type="checkbox"/> Permits from applicable Local, State & Federal Agencies |
| <input type="checkbox"/> Clearances from all applicable Local, State and Federal Agencies | |

Based on our reviews and appropriate site final inspection, we

- Recommend Do Not Recommend

- Development Approval Building Permit C/O

Developer, Owner, Engineer, Contractor and other members of the Development Team must be aware, the above recommendation is based only on the construction requirements of the engineering plans and other engineering documentation approved by this department. The Development Team shall be responsible for the compliance with other City department requirements and all approved documents, as well as Local, State and Federal regulations. The development requirements for this project may necessitate additional construction requirements that are not subject to this department's review for approval.

- See attached for comments

ENGINEERING COMMENTS:

1. Remove all proposed angled parking along Indian River Drive and Avenue B in accordance with the City of Fort Pierce Code of Ordinances prohibiting the creation of parking spaces that permit vehicles to back into a public roadway.
2. All proposed driveways shall be comprised of concrete or concrete brick pavers in accordance with the City of Fort Pierce Code of Ordinances Section 32-9.
3. Provide parking lot dimensions for all parking spaces and access aisles. How many overall parking spaces will be provided for the site, including spaces in the garage? Also, how many handicapped spaces are proposed?
4. A conceptual drainage plan was not included in the submittal packet, please forward a copy of this plan for review.
5. Avenue B is shown as having a one-way roadway width of 12' at the west end, please revise this width to reflect a 14' minimum one-way width.
6. What is the driveway spacing between the driveways along AE Backus Avenue? Driveway construction for a new platted development shall meet the requirements specified in the City of Fort Pierce Code of Ordinances Section 125-316; such as 50' driveway spacing, setback of 25' from side property lines and 75' setback for corner lots.
7. Provide all lot information for the lots fronting AE Backus Avenue. Will this land be platted into individual lots? If so, how will the on-site stormwater treatment be handled? What are the proposed building setbacks from the east and south property lines? If platting is not proposed, then a Unity of Title will need to be filed combining the two separate parcels.
8. There are three (3) driveways that access AE Backus that will only have an on-site depth of 8'. This is not sufficient depth to provide vehicle parking without encroaching into the right-of-way, please provide a minimum depth of 20' for vehicle parking.
9. Provide curbing within the parking areas as required per Section 123-37(11)(d).
10. Being as multiple improvements are proposed across two parcels of land, a Unity of Title will need to be filed which ties Parcel No. 2410-503-0041-020-4 and Parcel No. 2410-503-0043-000-2 together as one parcel of land.
11. Revise the landscape plan to indicate all required landscaping situated within the limits of the applicant's lease parcel. If additional landscaping is desired to be installed in the limits of public road rights-of-way, a landscape maintenance agreement with the City of Fort Pierce will need to be completed.
12. The landscape plan shall depict the clear vision areas at all intersection and driveways, refer to the City of Fort Pierce Code of Ordinances Section 125-308 for requirements.
13. Relocate the landscaping from the property on the south side of Avenue B as this land is under private ownership.
14. Indian River Drive is a St. Lucie County roadway; the applicant shall contact St. Lucie County directly for possible R/W donation and required improvements with the right-of-way.
15. The submitted site plan indicates that the proposed zoning for the development is PUD, will a Rezoning application request be submitted as the current zoning is identified as C4?
16. The proposed pedestrian walkway south previously abandoned Avenue B right-of-way is shown to decrease in width from 10' to 6'. The applicant shall revise the plan to reflect the continuation of the 10' wide walkway south to the noted POB of Parcel 5.

JRA/TST/tst 

Q:\ENGINEERING\Site Development Projects\Kings Landing\Site Plan\Submittal No. 1 - 050721\Development Comments - 051721.docx



BUILDING DEPARTMENT
TECHNICAL REVIEW COMMITTEE (TRC)
COMMENT FORM

Meeting Date: 5.20.21
Property Address: Site Plan - Kings Landing - 322 N. 2nd Street

Please be advised that the project may trigger the requirements indicated below:

- 1. Building Official or his representative has no comment at the time of this meeting, but reserves submission of comments upon completion of the official plan review.
2. Pre-construction meeting with the City's Building Department is requested.
3. Any construction will need to meet the requirements of the Florida Building Code 7th Edition.
4. Means of egress is required.
5. Means of ingress is required.
6. Must meet the following Accessibility requirements:
- Accessible route
- Handicapped parking spaces
- Means of egress
7. Change of Use required
- to include a signed and sealed Life Safety Plan
- to include a signed and sealed detailed comprehensive building plan.
8. Property exists in Special Flood Hazard Area. All Federal and State requirements shall be addressed.
9. Flood Development Permit required.
10. Building Permit required.
11. Signed and sealed construction drawings required.
12. Will need to meet the Fire Code.
13. Sprinkler system is required.
14. Smoke alarm system is required.
15. Other

Additional Comments/Requirements:

Empty rectangular box for additional comments or requirements.

Building Official's or Representative's Signature [Signature] Date: 5/14/21



Technical Review Committee meeting

May 20, 2021

Case # 21-07000006

Planner: Brandon Creagan

Development Review / Design Review.

King's Landing, 322 North 2nd Street, Ft. Pierce.

Comments:

- Please review the photometric plan calculations to ensure the average illumination for the parking lot area meets or exceeds the requirement of two (2) foot-candles set forth by city code.

(Lighting) Off-street parking shall be proved with lighting as follows: For commercial, industrial and governmental uses, a minimum average of two footcandles.

Currently, the calculation summaries indicate an average of 0.94 and 1.43 foot-candles for two of the parking lot areas listed ("off street parking / site & parking).

Officer Damian Spotts

Crime Prevention Practitioner

Fort Pierce Police Department.



Fort Pierce Utilities Authority
Water/Wastewater Engineering
1701 South 37th Street
Fort Pierce, FL 34947
772.466.1600 x3473

Technical Review Committee Meeting

May 20, 2021

TECHNICAL REVIEW PROJECT # 21-07000006

Kings Landing-322 N 2nd St

Comments

FPUA W/WW Engineering: Water and wastewater are available to serve the proposed development. Please submit three sets of utility construction drawings to FPUA's W/WW Engineering Department for formal review and approval. It is recommended to Contact Lugey Dawson @ (772) 466-1600 ext. 3428 to setup a pre-design meeting to discuss the required utility improvements surrounding the site.

FPUA Electric & Gas Engineering: Approved. Below are comments in reference to FPUA electric and gas service availability and requirements: Electric service is available to the site from Avenue B. Please provide electric load information for the new service and AutoCAD drawing. [Customer will be responsible for all transformer pads, conduits and secondary conductors.](#) For more information and project coordination, please contact Sal Scimeca.

Sal Scimeca
Engineering Technician II
Electric and Gas Engineering
1701 S. 37th Street, Fort Pierce, FL 34947
sscimeca@fpu.com
Office: (772)466-1600 Ext. 6957

Gas service is available to the site from Avenue B. Please provide a copy of the gas riser diagram with the anticipated load (if owner is interested in natural gas service). For more information and incentive available, please contact Ana Johnson.

Ana María Johnson

Business Development Officer
Fort Pierce Utilities Authority
206 S. 6th Street
Fort Pierce, FL 34950
772.466.1600 Ext. 3012
772.467.3115 Fax
anjohnson@fpu.com



Our mission is to provide our customers with economical, reliable, and friendly service in a continuous effort to enhance the quality of life in our community.

www.fpu.com





FPUA will require a utility easement for all propose FPUA facilities. [Please find attached a preliminary plan markup, showing the propose electric and Gas utility route.](#)

If the owner/representatives should have any questions, please have them contact me or the individuals listed above.

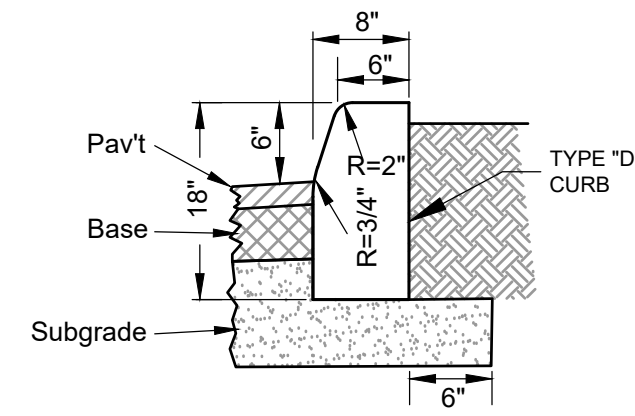
Thank you,
Paul Laguerre
Utility Designer
Fort Pierce Utilities Authority
1701 S. 37th Street
Fort Pierce, Florida 34947
Phone (772)466-1600 Ext. 6757
Fax (772)461-1938
Plaguerre@fpu.com



Our mission is to provide our customers with economical, reliable, and friendly service in a continuous effort to enhance the quality of life in our community.



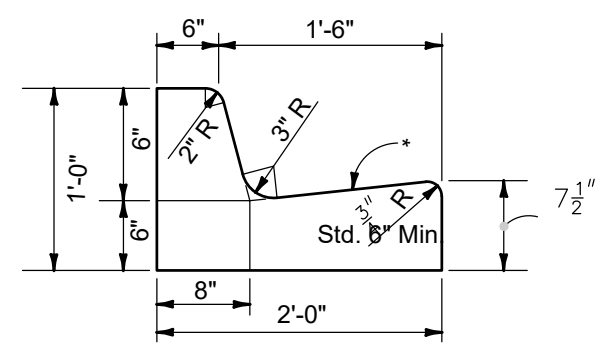
THIS DOCUMENT, TOGETHER WITH THE CONCEPTS AND DESIGNS PRESENTED HEREIN, IS INTENDED ONLY FOR THE SPECIFIC PURPOSE AND CLIENT FOR WHICH IT WAS PREPARED. REUSE OF AND IMPROPER RELIANCE ON THIS DOCUMENT WITHOUT WRITTEN AUTHORIZATION AND ADOPTION BY EDC, INC. SHALL BE WITHOUT LIABILITY TO EDC, INC.



NOTES:

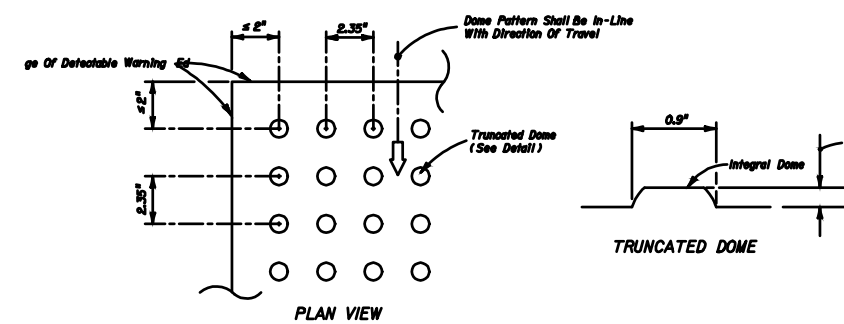
1. Use 3,000 P.S.I. concrete at 28 days for construction.
2. Subgrade to extend an additional 6" beyond Type "D" Curb.
3. Type "D" Curb to be constructed in accordance with FDOT Index #522-001.

TYPE "D" CURB



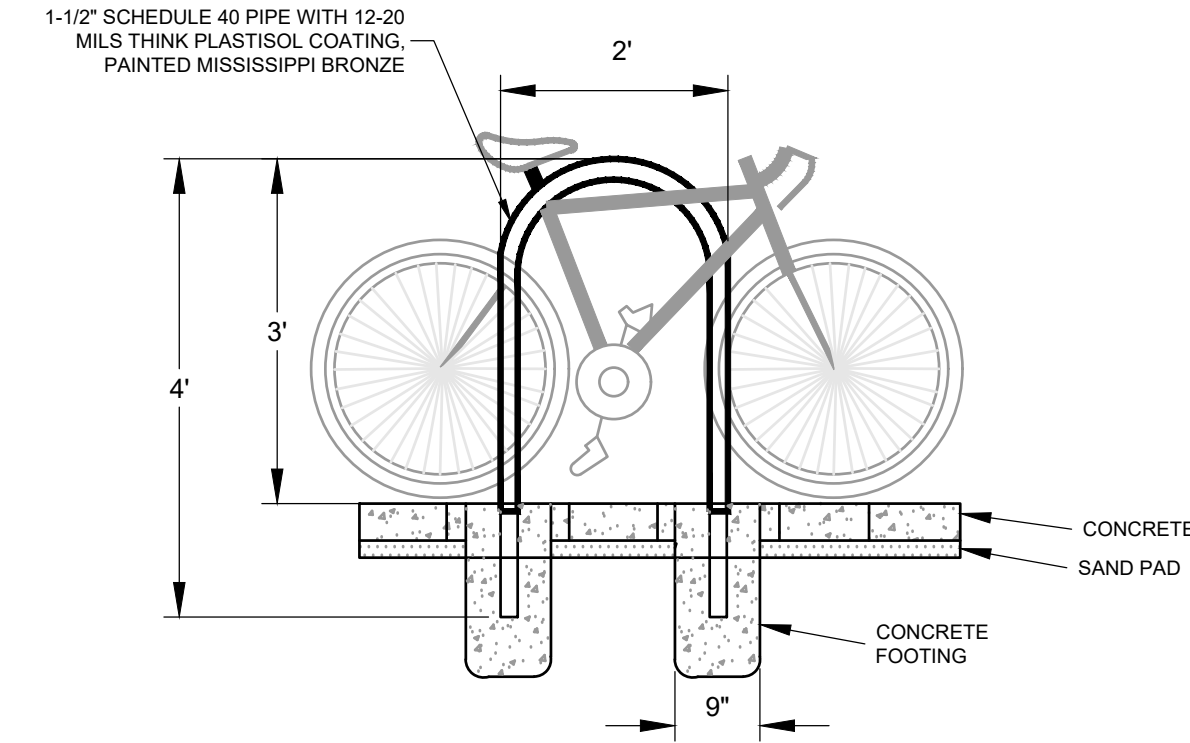
*NOTE: WHEN USED ON HIGH SIDE OF ROADWAYS, THE CROSS SLOPE OF THE GUTTER SHALL MATCH THE CROSS SLOPE OF THE ADJACENT PAVEMENT THE THICKNESS OF THE LIP SHALL BE 6", UNLESS OTHERWISE SHOWN ON PLANS.

TYPE F



NOTES:
REFER TO FDOT INDEX 522-002 FOR FURTHER DETAILS

**CURB RAMP
DETECTABLE WARNING**



BIKE RACK DETAIL

**ENGINEERS & SURVEYORS
ENVIRONMENTAL**

10250 VILLAGE PARKWAY
SUITE 201
PORT ST. LUCIE, FL 34987
772-462-2455
www.edc-inc.com

F.B.P.E. CERTIFICATE OF AUTHORIZATION 8935
L.B. CERTIFICATE OF AUTHORIZATION 8098

DESIGNED BY	JLW
DRAWN BY	JLW
FILE NAME	19-361 Site Plan (12.dwg)
DATE	02/28/21
SCALE	AS SHOWN
DATE	02/28/21

REVISION COMMENTS	DATE

**HD KING
KINGS LANDING
SITE PLAN DETAILS**

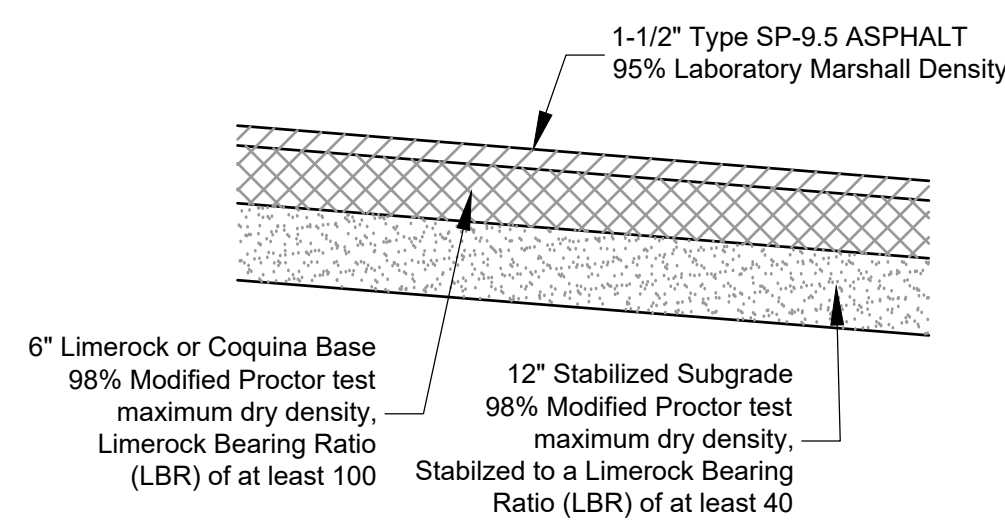
FLORIDA
FORT PIERCE

DAVID C. BAGGETT, P.E.(DATE)
#81375

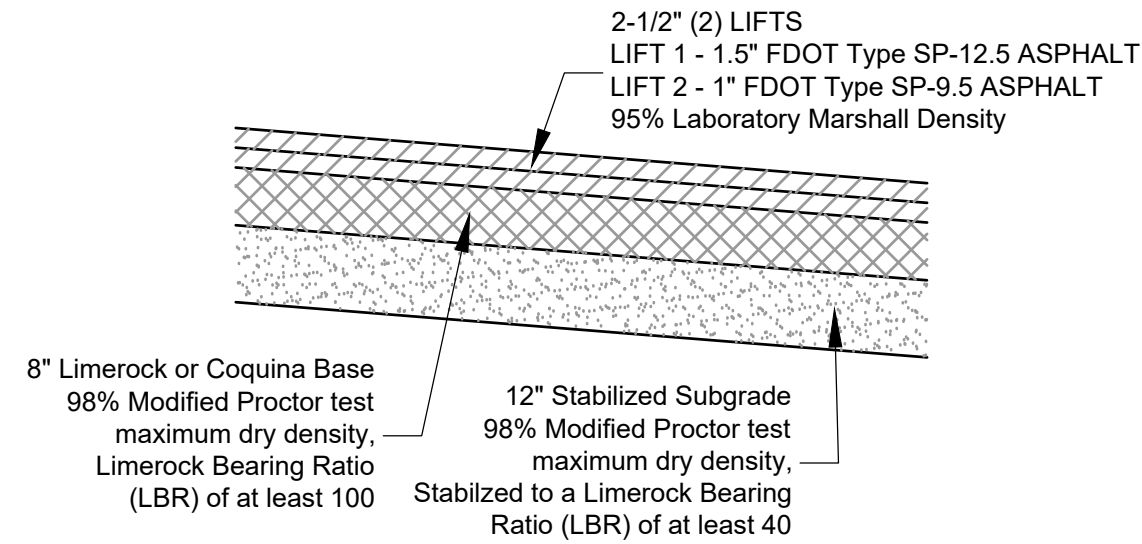
10250 SW VILLAGE PARKWAY - SUITE 201
PORT SAINT LUCIE, FL 34987
772-462-2455

19-361

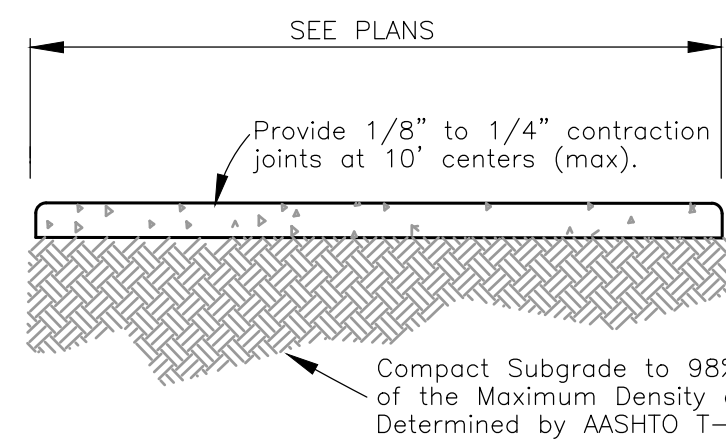
2 OF 2



**TYPICAL LIGHT DUTY FLEXIBLE
PAVEMENT SECTION**

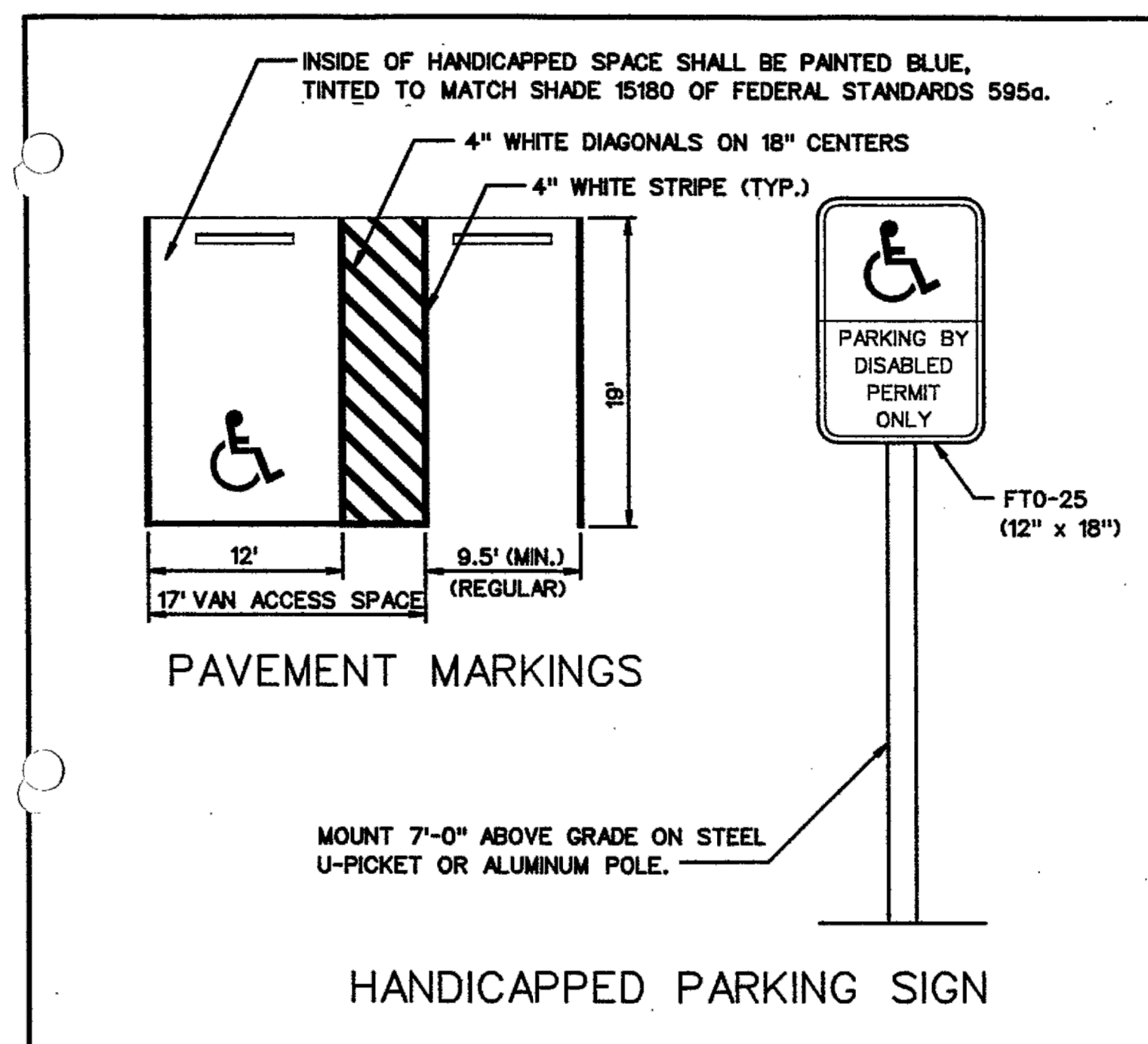


**TYPICAL HEAVY DUTY FLEXIBLE
PAVEMENT SECTION**



NOTES:
1. Use 3,000 P.S.I. Concrete At 28 Days for sidewalk construction.
2. Sidewalk thickness to be increased to 6" thick at driveway locations.
3. All repairs required during construction shall be removed and replaced 10' minimum to next full joint.

4" THICK SIDEWALK DETAIL



NOTE:
FOR PARKING REQUIREMENTS SEE FORT PIERCE CODE.

CITY OF FORT PIERCE • DEPARTMENT OF ENGINEERING
TECHNICAL SPECIFICATIONS AND DETAILS

**HANDICAPPED AND
REGULAR PARKING
DETAIL**

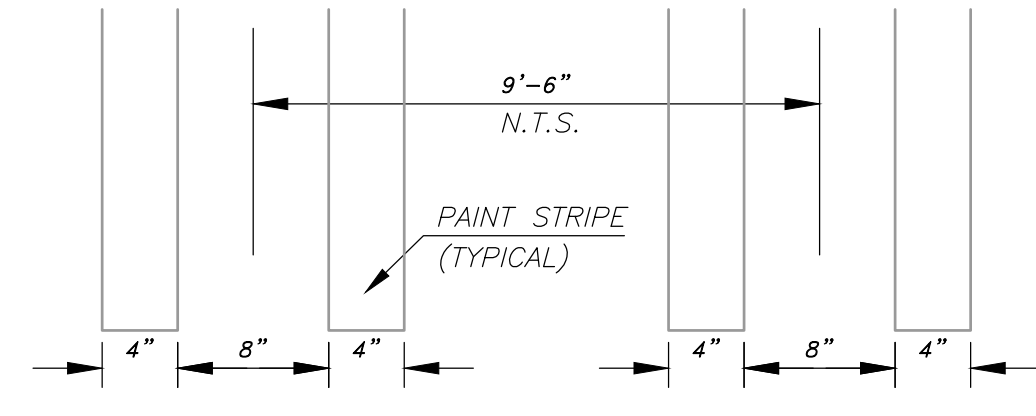
SCALE: N.T.S. | ISSUED AUGUST 1998

REVISIONS

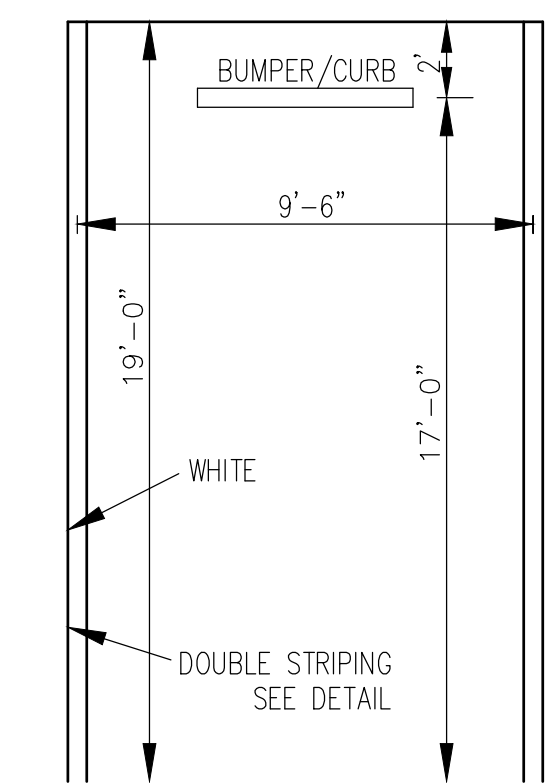
SHEET No. **PL2**

ITEMS TO BE TESTED	DENSITY		L.B.R.		THICKNESS	
	MAX. SPACING LIN. FT.	SQ. FT.	MAX. SPACING LIN. FT.	SQ. FT.	MAX. SPACING LIN. FT.	SQ. FT.
COMPACTED OR STABILIZED SUBGRADE	300	10,000	300	10,000	300	
LIMEROCK BASE	300	10,000	300	10,000	300	10,000
SHELLROCK BASE	300	10,000	300	10,000	300	10,000
ASPHALT					PER INSP.	10,000

NOTE: ALL TESTING SHALL BE TAKEN IN A STAGGERED SAMPLING PATTERN FROM A POINT 42" INSIDE THE LEFT EDGE OF THE ITEM TESTED, TO THE CENTER, TO A POINT 12" INSIDE



DOUBLE STRIPING DETAIL



19' PARKING STALL DETAIL

May 18, 2021

PROJECT: Kings Landing
REF: TRC App. #21-07000006
TO: Brandon Creagan
FROM: David Hays

The following comments are offered by SLC Public Works Department:

1. The traffic report will be reviewed by a third party through the County.
2. The landscape along Indian River Drive shall not impair line of site.
3. A Site Development Permit will be required from the County prior to implementation of site activities. The SD Permit will provide for right-of-way improvements.
4. Written response to Pre-application review has not been received.
5. For discussion regarding these comments, please contact me at 772-462-1491, haysd@stlucieco.org or Grant Chambers at 772-462-2741, chambersg@stlucieco.org.

Cc: Alicia Rosenthal



May 19, 2021

VIA EMAIL

Mr. Brandon Creagan, LEED Green Associate, Planner
City of Fort Pierce
100 North U.S. Highway 1
Fort Pierce, Florida 34950

**RE: Proposed Transportation Impact Review
Kings Landing
Fort Pierce, Florida**

Dear Mr. Creagan:

As the Federal and State-designated agency responsible for transportation planning for the City of Fort Pierce, City of Port St. Lucie, St. Lucie Village, and St. Lucie County, the St. Lucie Transportation Planning Organization (St. Lucie TPO) has completed a transportation impact review with regard to the above-referenced proposed development. The proposed development consists of a mixed use development in Downtown Fort Pierce that will include residences, commercial uses, restaurants, and a hotel. Based on the review, the following comments are provided:

- **Historical Growth Rate:** The calculations were not provided in the Traffic Analysis for the development to justify the use of a historical growth rate of 1.88 percent. In the absence of the calculations, a historical growth rate of 2.5 percent should be utilized which is the actual annual growth rate in St. Lucie County since 1995.
- **Shared-Use Path and Historic Downtown Fort Pierce Retrofit SUN Trail Connectivity:** The development appears to include wide sidewalks along Moore's Creek and the streets surrounding the development. However, a shared-use path of at least 12 feet in width should be provided instead of the sidewalks along AE Backus Avenue, North 2nd Street, and Moore's Creek. The path should be identified on the development plans to connect with the Historic Downtown Fort Pierce Retrofit segment of the Florida Shared-Use Nonmotorized (SUN) Trail/East Coast Greenway depicted in the attached map that is being designed and constructed through the entirety of St. Lucie County. The provision of this path and connectivity will serve as an additional attracter to the development.

Please contact me should any additional information or clarification be required with regard to this review.

Sincerely,

Peter Buchwald
Executive Director

cc: Jack Andrews, P.E., Fort Pierce City Engineer

Attachment

FINAL FEASIBILITY STUDY

**Historic Downtown Fort Pierce Retrofit SUN Trail
St. Lucie County, Florida**

Financial Project ID:

440033-1

Prepared for:



Florida Department of Transportation

District Four

3400 West Commercial Boulevard

Fort Lauderdale, Florida 33309

January 2019

Figure 11: Recommended Alternative





BUREAU OF FIRE PREVENTION

SITE PLAN REVIEW

TO: Site Plan Applicant

SITE PLAN: Kings Landing 322 N. 2nd Street

REVIEW DATE: 5/11/2021

PLANNER: CREAGON

REVIEWED BY: Captain Wayne Boyer

Site Plan Approved: _____

Site Plan Approved with conditions: X

Site Plan Approval withheld pending written acknowledgement of conditions: _____

Site Plan Rejected: _____

The Following Revisions Are Necessary:

- 1. Please submit a completed application for Development/Site Plan Review (St. Lucie County Fire District Development & Site Plan Review Application). This form is available on-line at <https://www.slcfcd.com>**
- 2. Fire District review fees are due at the time of submittal. An abbreviated fee schedule is included on the application form.**
- 3. Please send the Fire District electronic plans for the site and buildings.**
- 4. A separate review and permit is required for Underground Fire Mains connected to standpipes or sprinkler systems.**
- 5. Fire hydrants (shall be) are provided for buildings other than detached one-and-two-family dwellings IAW both of the following 1) The maximum distance to a fire**

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hydrant from the closest point in the building shall not exceed 400 feet. 2) The maximum distance between fire hydrants shall not exceed 500 feet. NFPA 1:18.5.3. Please provide fire flow calculations for hydrants.

6. An approved water supply capable of supplying the required fire flow for fire protection (shall be) is identified to all premises upon which facilities, buildings, or portions of buildings which are to be constructed or moved into the jurisdiction. The approved water supply shall be in accordance with NFPA 1:18.4. See "Needed NFPA Fire Flow Calculator Spreadsheet".
7. Per the St. Lucie County Fire District Fire Prevention Code Resolution 647-17. At Least 13 feet 6 inches nominal vertical clearance shall be provided and maintained over the full width of all means of access. Including, but not limited to trees, canopies, etc.
8. Fire department connections shall be located on the street side of buildings and shall be located and arranged so that hose lines can be readily attached to the inlets without interference from any nearby objects, including buildings, fences, landscaping, or other fire department connections. The locations of connections shall be based upon the access requirements of the fire department.
9. The distance allowed between the fire department connection and a fire hydrant shall be no more than one hundred fifty (150) feet as a vehicle travels.
10. The Fire District reserves the right for future comments at the site plan & building construction phase.
11. FD access roads (shall be) are provided such that any portion of an unsprinklered facility or exterior wall is located not more than 150' from FD access roads as measured by an approved route around the exterior of the building or facility. (450' for NFPA 13, 13D, 12R sprinklered protected buildings).
12. Per NFPA 1114 Chapter 9, Section 1.3. Prior to the final occupancy of any building, the permitted water supply for fire protection, including fire hydrants and fire suppression systems, shall be installed, tested, and acceptable to the AHJ (SLCFD).
13. Be advised: Dimensions of largest vehicle are as follows: 38 tons or 77k lbs, 47.5 ft. total length, 21.5 ft. wheel base, 10.5 ft. total width, 41.5 degree turning radius.

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14. Site Plans shall include all necessary fire department access roadways and fire lanes as determined by the Fire Marshal. At least 13 feet 6 inches nominal vertical clearance shall be provided and maintained over the full width of all means of access. Including, but not limited to trees, canopies, etc., as referred to in Exhibit "A," attached hereto.

**Minimum roadway pavement width (two-way traffic) shall be twenty (20) feet.
Minimum roadway pavement width (one-way traffic) shall be twelve (12) feet.**

15. Two-Way Radio Enhancement Systems/BDAS shall be installed, inspected and operationally tested in accordance with the manufacturer's published requirements, by the local fire department, and comply with the most current edition of the Florida Fire Prevention Code and its incorporated standards and codes. Pre-surveys of radio signal strength shall be submitted to the Fire Marshal in the form of heat signature mapping or a certification document of radio signal strength provided by a licensed engineer.

16. Minimum Size of Water Mains

a. The minimum size of water mains for supplying water for firefighting purposes shall be six (6) inches. b. The minimum size of water mains supplying hydrants on a dead end main shall be eight (8) inches. c. The maximum number of hydrants located on a dead end main shall be one (1). d. Grid or Tee systems shall be supplied by a minimum of an eight (8) inch looped main. Exceptions may be granted based on the capacity of the water distribution system but in no case shall the main size be less than six (6) inches. e. The minimum size water main(s) shall be determined by the needed fire flow as established by the Fire Marshal and based on the current Insurance Service Office (ISO) requirements.

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