

PRELIMINARY ENGINEERING PLANS AND SPECIFICATIONS

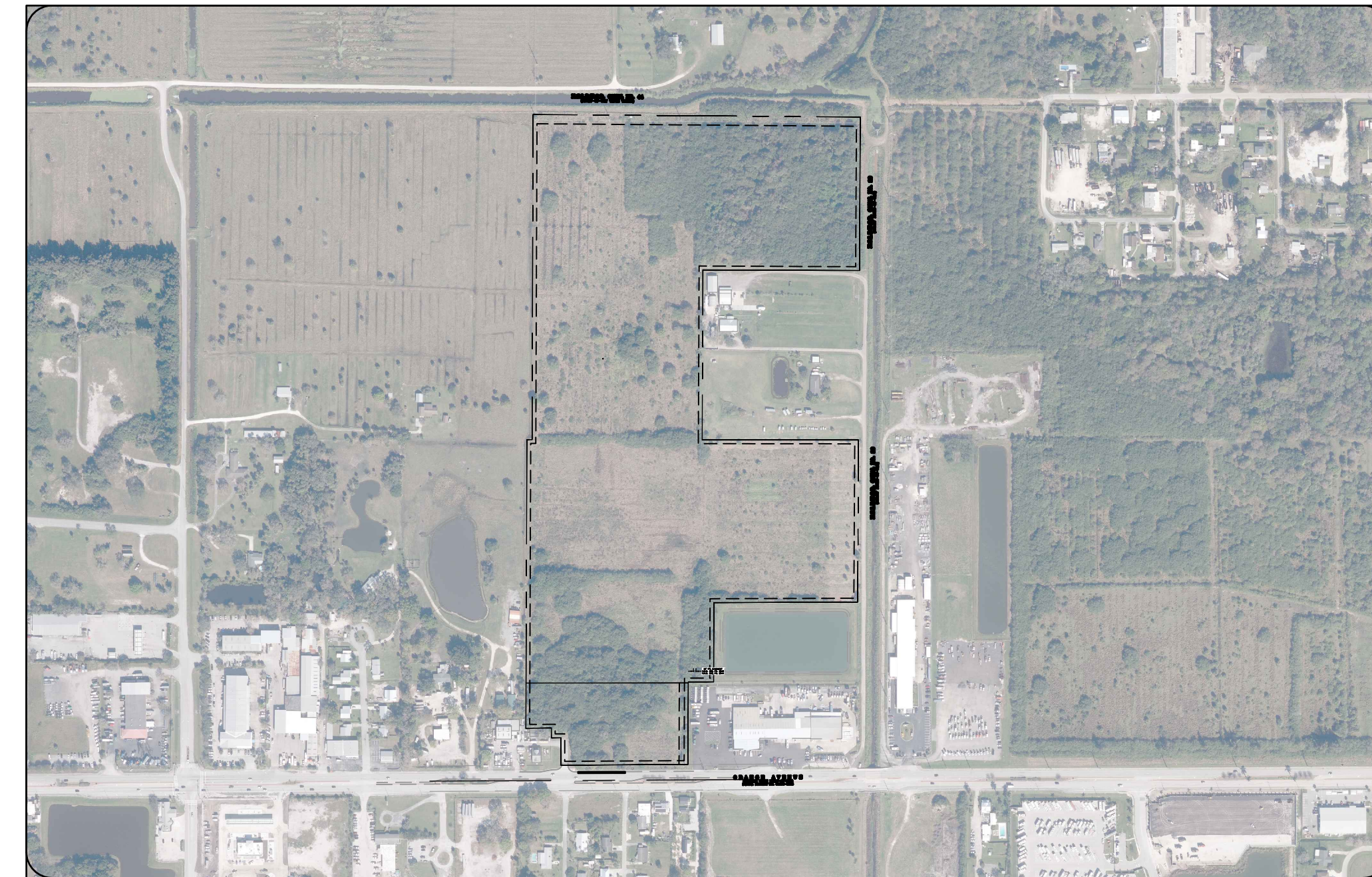
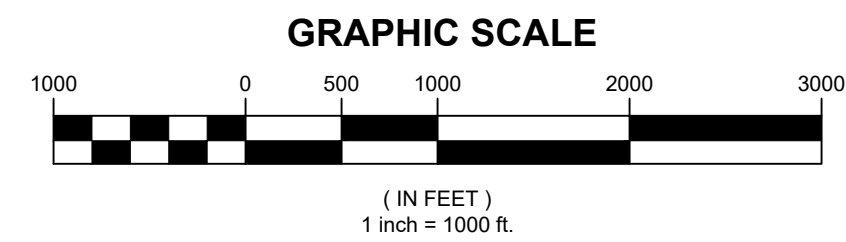
WEST ORANGE PD

SECTION 07, TOWNSHIP 35S, RANGE 40E

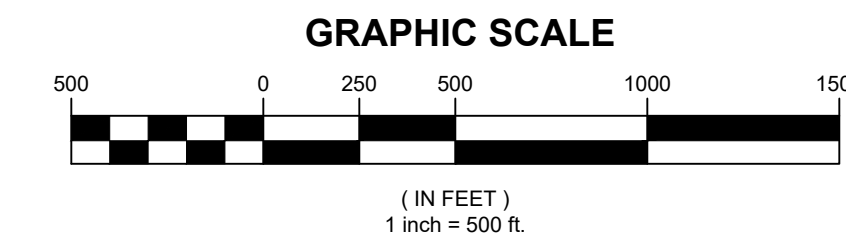
CITY OF FORT PIERCE, FLORIDA



LOCATION MAP



PROJECT AREA



LEGAL DESCRIPTION:

PARCEL 1:
THE WEST ONE-HALF (1/2) OF THE NORTHEAST ONE-QUARTER (1/4) OF THE NORTHWEST ONE-QUARTER (1/4) OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, LESS RIGHT-OF-WAY FOR CANALS, AS RECORDED IN THE PUBLIC RECORDS IN AND FOR ST. LUCIE COUNTY, FLORIDA.

PARCEL 2:
THE EAST 50 FEET OF THE FOLLOWING DESCRIBED PROPERTY: THE WEST 1/2 OF THE SOUTHEAST 1/4 OF THE NORTHWEST 1/4 OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, LESS AND EXCEPT RIGHTS OF WAY FOR PUBLIC ROADS, LYING AND BEING IN ST. LUCIE COUNTY, FLORIDA.

PARCEL 3:
THE NORTH 15 ACRES OF THE FOLLOWING DESCRIBED PROPERTY, THE EAST 1/2 OF THE SOUTHEAST 1/4 OF NORTHWEST 1/4 IN SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, LESS AND EXCEPT RIGHTS OF WAY FOR CANALS, LYING AND BEING IN ST. LUCIE COUNTY, FLORIDA, AND FURTHER LESS AND EXCEPTING THEREFROM THE FOLLOWING DESCRIBED PARCEL AS SET FORTH IN WARRANTY DEED RECORDED IN OFFICIAL RECORDS BOOK 1433, PAGE 2915:
A PORTION OF THE NORTHWEST 1/4 OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, ST. LUCIE COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:
COMMENCE AT A FOUND RAILROAD SPIKE MARKING THE SOUTHWEST CORNER OF SAID SECTION 7; THENCE NORTH 00 DEG 07' 33" EAST ALONG THE WEST LINE OF SAID SECTION 7, A DISTANCE OF 806.208 METERS (2,645.03 FEET) TO A POINT ON A FOUND BOAT SPIKE MARKING THE WEST 1/4 CORNER OF SAID SECTION 7, SAID POINT BEING ON THE BASELINE OF SURVEY FOR STATE ROAD 68 (ORANGE AVENUE) AS SHOWN ON THE FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY MAP FOR SECTION NO. 94070-2505; THENCE NORTH 89 DEG 42' 09" EAST ALONG SAID BASELINE OF SURVEY AND THE SOUTH LINE OF THE NORTHWEST 1/4 OF SAID SECTION 7, A DISTANCE OF 624.027 METERS (2,047.33 FEET); THENCE NORTH 00 DEG 17' 51" WEST ALONG A LINE AT A RIGHT ANGLE TO THE LAST DESCRIBED COURSE, A DISTANCE OF 116.668 METERS (382.77 FEET) TO THE POINT OF BEGINNING; THENCE NORTH 89 DEG 47' 56" EAST, A DISTANCE OF 171.190 METERS (561.65 FEET); THENCE NORTH 00 DEG 18' 45" EAST, A DISTANCE OF 94.635 METERS (310.48 FEET); THENCE SOUTH 89 DEG 47' 56" WEST, A DISTANCE OF 171.309 METERS (562.04 FEET); THENCE SOUTH 00 DEG 14' 26" WEST, A DISTANCE OF 94.634 METERS (310.48 FEET) TO THE POINT OF BEGINNING.
FURTHER LESS AND EXCEPTING THE FOLLOWING DESCRIBED PARCEL AS SET FORTH IN WARRANTY DEED RECORDED IN OFFICIAL RECORDS BOOK 1430, PAGE 1914:
A PARCEL OF LAND LYING IN THE NORTHWEST QUARTER OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, ST. LUCIE COUNTY, FLORIDA, AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:
THE NORTH 15 ACRES OF THE EAST HALF OF THE SOUTHEAST QUARTER OF THE NORTHWEST QUARTER IN SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, EXCLUSIVE OF RIGHTS-OF-WAY FOR CANALS. LYING SOUTHERLY OF THE FOLLOWING DESCRIBED LINE:
COMMENCE AT A FOUND RAILROAD SPIKE MARKING THE SOUTHWEST CORNER OF SAID SECTION 7; THENCE NORTH 00 DEG 07' 33" EAST ALONG THE WEST LINE OF SAID SECTION 7, A DISTANCE OF 2,645.03 FEET TO A POINT ON A FOUND RAILROAD SPIKE MARKING THE WEST QUARTER CORNER OF SAID SECTION 7, SAID POINT BEING ON THE BASE LINE OF SURVEY FOR STATE ROAD 68 (ORANGE AVENUE) AS SHOWN ON A FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP FOR SECTION NO. 94070-2505; THENCE NORTH 89 DEG 42' 09" EAST ALONG SAID BASE LINE OF SURVEY AND THE SOUTH LINE OF THE NORTHWEST QUARTER OF SAID SECTION 7, A DISTANCE OF 2,047.33 FEET; THENCE NORTH 00 DEG 17' 51" WEST ALONG A LINE AT RIGHT ANGLES TO THE LAST DESCRIBED COURSE, A DISTANCE OF 382.77 FEET TO THE POINT OF BEGINNING.
THENCE NORTH 89 DEG 47' 56" EAST, A DISTANCE OF 561.65 FEET TO THE EASTERLY BOUNDARY OF THE EAST HALF OF THE SOUTHEAST QUARTER OF THE NORTHWEST QUARTER, SAID LINE ALSO TO BE EXTENDED TO THE WESTERLY BOUNDARY OF THE EAST HALF OF THE SOUTHEAST QUARTER OF THE NORTHWEST QUARTER.

PARCEL 4:
THE NE 1/4 OF THE NE 1/4 OF THE NW 1/4 OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, ST. LUCIE COUNTY, FLORIDA, EXCEPTING ALL RIGHTS OF WAY FOR PUBLIC ROADS AND DRAINAGE CANALS AND DITCHES.

PARCEL 5:
THE WEST 1/2 OF THE SOUTHEAST 1/4 OF THE NORTHWEST 1/4 AND THE EAST 20' OF THE SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST.
LESS AND EXCEPTING THE EAST 50' THEREOF AND LESS RIGHT-OF-WAY FOR PUBLIC ROADS.
FURTHER EXCEPTING THEREFROM THE FOLLOWING DESCRIBED PARCEL OF LAND: BEGIN 20' WEST OF THE SOUTHWEST CORNER OF THE SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SAID SECTION 7 AND RUN THENCE NORTH 40' TO POINT OF BEGINNING; THENCE CONTINUE NORTH PARALLEL WITH THE EAST LINE OF THE SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SAID SECTION 7, FOR A DISTANCE OF 165' TO A POINT; THENCE TURN AND RUN EAST PARALLEL WITH THE SOUTH LINE OF THE NORTHWEST 1/4 OF SAID SECTION 7, FOR A DISTANCE OF 100' TO A POINT; THENCE TURN AND RUN SOUTH 165' TO A POINT 100' EAST OF POINT OF BEGINNING; THENCE TURN AND RUN WEST 100' TO POINT OF BEGINNING.
LESS D.O.T. EASEMENT AS RECORDED IN OFFICIAL RECORDS BOOK 1433, PAGE 2925, OFFICIAL RECORDS OF ST. LUCIE COUNTY, FLORIDA.
LESS THE WEST 1/2 OF THE CROSS EASEMENT AS RECORDED IN OFFICIAL RECORDS BOOK 1938, PAGE 328 MORE PARTICULARLY DESCRIBED AS FOLLOWS:
COMMENCE 20 FEET WEST OF THE SOUTHWEST CORNER OF THE SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SECTION 7, TOWNSHIP 35 SOUTH, RANGE 40 EAST, THENCE N00°10'09"E FOR 40.00 FEET TO THE NORTH RIGHT OF WAY LINE OF ORANGE AVENUE (STATE ROAD NO. 68); THENCE N89°42'09"E ALONG SAID RIGHT OF WAY LINE OF ORANGE AVENUE FOR 54.99 FEET; THENCE N79°48'50"E FOR 45.07 FEET; THENCE N89°42'09"E FOR 0.67 FEET TO THE POINT OF BEGINNING; THENCE N00°10'09"E FOR 119.20 FEET; THENCE N89°42'09"E FOR 74.89 FEET; THENCE S00°10'09"W FOR 119.20 FEET TO SAID NORTH RIGHT OF WAY LINE OF ORANGE AVENUE; THENCE S89°10'09"W FOR 74.89 FEET TO THE POINT OF BEGINNING.
CONTAINING ##### ACRES, MORE OR LESS.

PROJECT INFORMATION

HW PROJECT NO: 25-144

PARCEL ID#: 2407-231-0000-000-5, 2407-241-0001-000-3
2407-212-0001-000-3, 2407-211-0001-000-0

ZONING INFO: PD

OWNERSHIP: ORANGE AVE DEVELOPMENT II, LLC
28178 HAYES ROAD
ROSEVILLE, MI 48066-2346

PROJECT DATUM AND COORDINATE SYSTEM:
HORIZONTAL: FLORIDA STATE PLANE NAD83 (FL83-EF)
VERTICAL: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVDB88)


GOVERNING STANDARDS & SPECIFICATIONS:
FLORIDA DEPARTMENT OF TRANSPORTATION, FY2024-25 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AT THE FOLLOWING WEBSITE:
<https://www.fdot.gov/programmanagement/Implemented/SpecBooks>
(EFFECTIVE: JULY 2024-2025)

LAND DEVELOPMENT TO CONFORM TO THE CURRENT PORT SAINT LUCIE ENGINEERING DEPARTMENT STANDARDS.

UTILITY CONSTRUCTION TO CONFORM TO THE CURRENT EDITION OF THE CONSTRUCTION STANDARDS AND DETAILS (EFFECTIVE: 2019 WITH 2024 AMENDMENT)

INDEX OF DRAWINGS

C-101	EXISTING CONDITIONS, DEMO, & STORMWATER POLLUTION PREVENTION PLAN
C-102	STORMWATER POLLUTION PREVENTION PLAN DETAILS
C-103, C104	PRELIMINARY PAVING, GRADING & DRAINAGE PLAN
C-105, C106	PRELIMINARY UTILITY PLAN
C-501	PAVING, GRADING & DRAINAGE DETAILS



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PROJECT: **WEST ORANGE PD**
CITY OF FORT PIERCE, FLORIDA

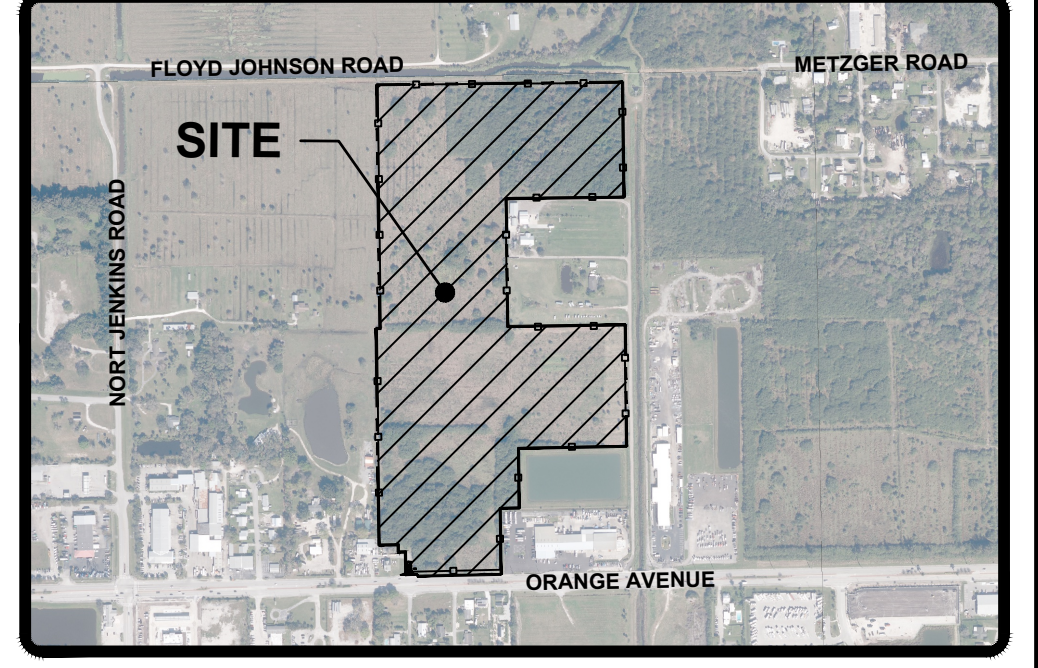
DATE: **October 6, 2025**

J.R. HARRISON, P.E. (DATE) #62270 10250 SW VILLAGE PARKWAY - SUITE 201 PORT SAINT LUCIE, FL 34987 772-462-2455	PROJECT NO: 25-144
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FILE LOCATION: Z:\EDEC\2025\25-144 - LANDMARK BLDG - WEST ORANGE TOWNSHIP\ENGINEERING\AUTOCAD\DWG\25-144 PRELIM.DWG, 10/25/2025 10:24:10 AM

CYPRESS KNEE, LLC
5500 ORANGE AVE
ZONING: CG
PARCEL ID: 2407-231-0001-000-2

CYPRESS KNEE, LLC
5500 ORANGE AVE
ZONING: PD
PARCEL ID: 2407-221-0001-000-1



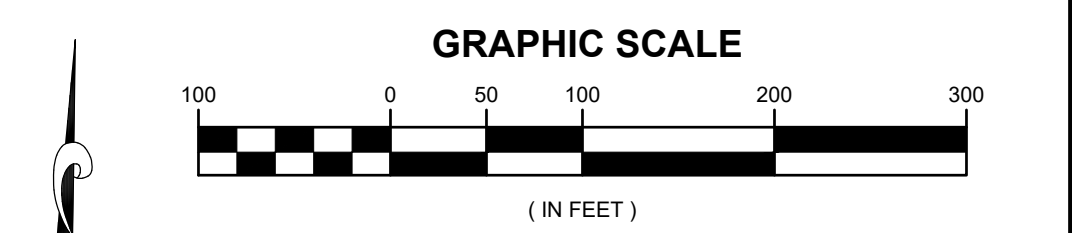
VICINITY MAP
SCALE: 1:1,000

- DEMUCKING:**
1. THE SUBJECT AREA PLUS AN AREA EXTENDING 5 FEET OUTSIDE PROPOSED CONSTRUCTION PERIMETERS SHOULD BE EXCAVATED AS NEEDED TO REMOVE ANY ENCOUNTERED DEEPER DEPOSITS OF ORGANIC SOIL. CARE MUST BE EXERCISED TO AVOID MIXING THE ORGANIC SOILS WITH THE UNDERLYING, SUITABLE GRANULAR MATERIALS. THE ORGANICS ARE TO BE DISPOSED OF AWAY FROM THE BUILDING AREA AS DIRECTED BY THE OWNER.
 2. IN GENERAL IT IS DESIRED TO COMPLETE THE EXCAVATION AND BACKFILLING OPERATIONS IN AS SHORT A TIME AS POSSIBLE TO AVOID OPEN EXCAVATIONS. CONSIDERATION SHOULD BE GIVEN TO PROCEEDING WITH THE DEMUCKING/BACKFILLING OPERATION IN STAGES ACROSS THE SITE SO THAT NO DEEP EXCAVATIONS REMAIN OPEN OVERNIGHT. SUCH GRADUAL APPROACH MAY ALSO REDUCE DEWATERING AND SLOPE BRACING NEEDS.
 3. IF THE EXCAVATION REACHES AREAS NEAR THE PROPERTY LINES, THE SLOPE OF THE EXCAVATION MAY HAVE TO BE PROTECTED WITH A BRACING SYSTEM. THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE INTEGRITY OF ADJACENT PROPERTY, AND FOR IMPLEMENTING ALL NECESSARY SAFETY MEASURES.
 4. ONCE THE BOTTOM OF THE EXCAVATION IS INSPECTED AND APPROVED, BACKFILL CAN BE PLACED TO A HEIGHT OF 2 FEET ABOVE THE WATER TABLE. IF DEWATERING MEASURES APPEAR TO BE NECESSARY ON THIS SITE TO FACILITATE THE REMOVAL OF ANY ORGANIC SOILS, ALLOW THE INSPECTION OF ANY DEMUCKED AREAS, AND ALLOW THE COMPACTION OF THE BACKFILL. THE CONTRACTOR SHOULD CONSIDER DEMUCKING AND AT LEAST PARTIALLY BACKFILLING SMALL SEGMENTS OF THE SITE AT A TIME TO REDUCE DEWATERING NEEDS AND DISCHARGE FLOWS.

- DENSIFICATION:**
1. GRANULAR BACKFILL SHOULD BE PLACED IN THE DRY AND COMPACTED WITH A VIBRATORY ROLLER. ANY SOFT, YIELDING SOILS DETECTED DURING THIS OPERATION SHOULD BE EXCAVATED AND REPLACED WITH CLEAN, COMPACTED BACKFILL THAT CONFORMS WITH THE RECOMMENDATIONS BELOW. SUFFICIENT PASSES SHOULD BE MADE DURING THE COMPACTION OPERATIONS TO PRODUCE DRY DENSITIES NOT LESS THAN 98 PERCENT OF THE MODIFIED PROCTOR (ASTM D1557) MAXIMUM DRY DENSITY OF THE COMPACTED MATERIAL TO DEPTHS OF 1 FOOT BELOW THE COMPACTED SURFACE. IN ANY CASE, THE AREA SHOULD RECEIVE NOT LESS THAN 10 OVERLAPPING PASSES, HALF OF THEM IN EACH OF TWO PERPENDICULAR DIRECTIONS, IF POSSIBLE. THE INITIAL PASSES MAY HAVE TO BE GIVEN WITHOUT VIBRATIONS, AND THEN WITH THE LOWEST POSSIBLE VIBRATION SETTING SO AS TO ALLOW THE TRAFFIC OF THE VIBRATING ROLLER. AN ADDITIONAL SIX INCH LIFT MAY BE ADDED TO THE INITIAL LIFT IF THE TRAFFIC OF THE ROLLER IS MADE DIFFICULT BY HIGH PORE PRESSURE CONDITIONS.
 2. WE RECOMMEND THAT THE SITE PREPARATION CONTRACTOR CLOSELY MONITOR THE VIBRATIONS PRODUCED DURING THE VIBRATORY ROLLER COMPACTION OPERATIONS SO THAT THEY DO NOT ADVERSELY AFFECT ANY NEARBY STRUCTURES.
 3. AFTER THE EXCAVATION AREA HAS BEEN PROOFROLLED AND TESTED TO VERIFY THAT THE DESIRED DRY DENSITY HAS BEEN OBTAINED, THE AREA MAY BE FILLED TO THE DESIRED GRADES. ALL FILL MATERIAL SHOULD CONFORM TO THE RECOMMENDATIONS BELOW. IT SHOULD BE PLACED IN UNIFORM LAYERS NOT EXCEEDING 12 INCHES IN LOOSE THICKNESS. EACH LAYER SHOULD BE COMPACTED TO A DRY DENSITY NOT LESS THAN 98 PERCENT OF ITS MODIFIED PROCTOR (ASTM D1557) MAXIMUM VALUE.

- CONSTRUCTION NOTES:**
1. ALL CONSTRUCTION SHALL COMPLY WITH THE CITY OF FORT PIERCE CODE OF ORDINANCES SECTIONS 17 & 22.
 2. TRANSFORMER AND UTILITY CONDUIT PLACEMENT & ELEVATION TO BE COORDINATED WITH FPIUA AND OWNER.
 3. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND INVERTS OF ALL UTILITIES. NOTIFY E.O.R. OF ANY DISCREPANCIES.
 4. ALL PIPE ELEVATIONS SHALL BE FIELD VERIFIED BEFORE COMMENCEMENT.
 5. ALL PROPOSED AND EXISTING UTILITIES SHALL MAINTAIN MIN. COVER REQUIREMENTS OF FDEP.
 6. SOO ALL DISTURBED AREAS.
 7. THE PROPERTY OWNER, CONTRACTOR, AND AUTHORIZED REPRESENTATIVES SHALL PROVIDE PICKUP, REMOVAL, AND DISPOSAL OF LITTER WITHIN THE PROJECT LIMITS AND SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE AREA FROM THE EDGE OF PAVEMENT TO THE PROPERTY LINE WITHIN THE CITY'S RIGHT-OF-WAY.

PLAN REFERENCE:
INFORMATION BASED ON ELEVATIONS THAT ARE SHOWN RELATIVE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.88) AND ARE GIVEN IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED.
*GENERAL ACCEPTED CONVERSION: NAVD + 1.475 = NGVD



REV.	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				
NOT FOR CONSTRUCTION				

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PROJECT: WEST ORANGE PD
CITY OF FORT PIERCE, FLORIDA

STORMWATER POLLUTION PREVENTION PLAN

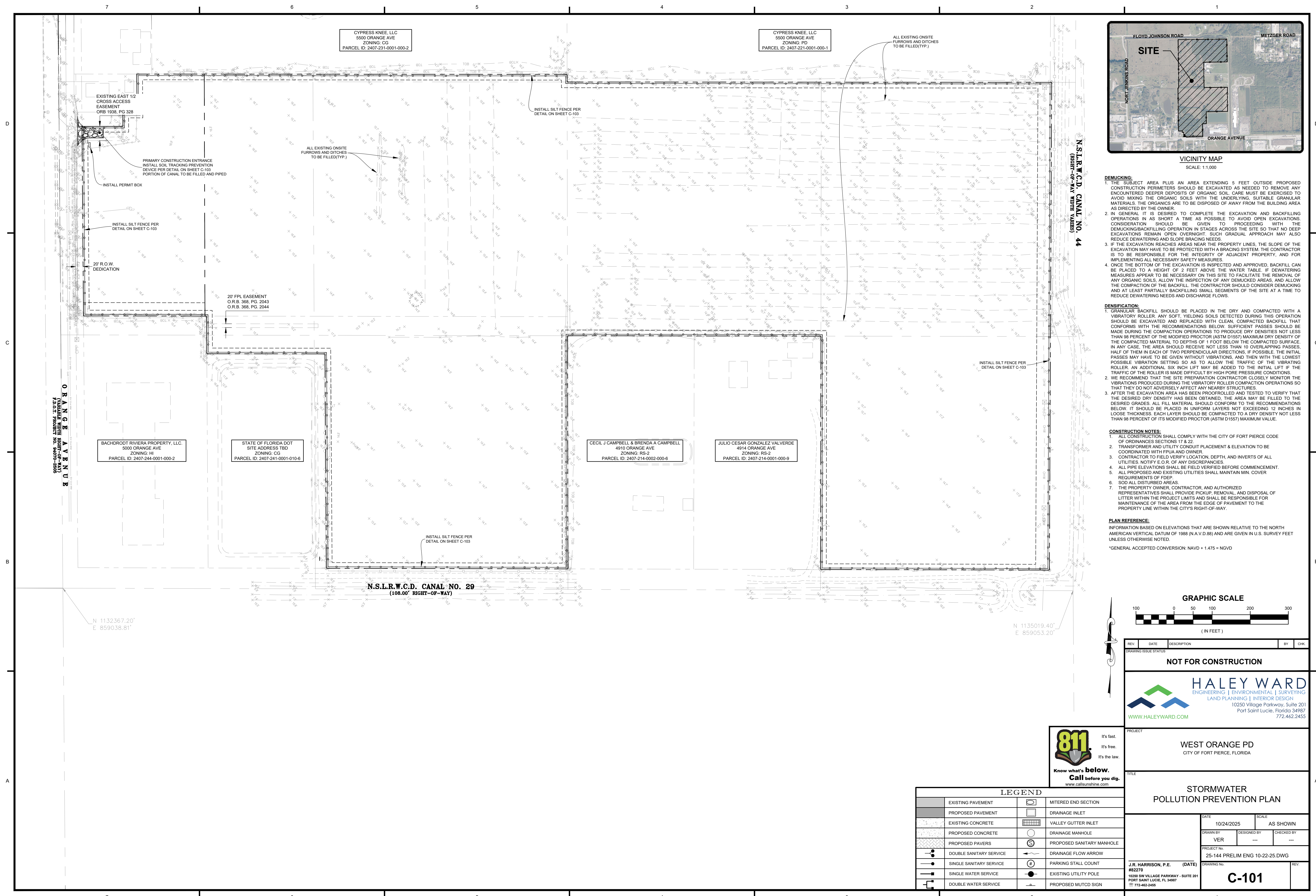
DATE	SCALE
10/24/2025	AS SHOWN
DRAWN BY: VER	DESIGNED BY: ---
CHECKED BY: ---	
PROJECT No.: 25-144 PRELIM ENG 10-22-25.DWG	

J.R. HARRISON, P.E. (DATE) #62270
10250 SW VILLAGE PARKWAY - SUITE 201
PORT SAINT LUCIE, FL 34987
772-462-2455

C-101

LEGEND

EXISTING PAVEMENT	MITERED END SECTION
PROPOSED PAVEMENT	DRAINAGE INLET
EXISTING CONCRETE	VALLEY GUTTER INLET
PROPOSED CONCRETE	DRAINAGE MANHOLE
PROPOSED PAVERS	PROPOSED SANITARY MANHOLE
DOUBLE SANITARY SERVICE	DRAINAGE FLOW ARROW
SINGLE SANITARY SERVICE	PARKING STALL COUNT
SINGLE WATER SERVICE	EXISTING UTILITY POLE
DOUBLE WATER SERVICE	PROPOSED MUTCD SIGN



FILE LOCATION: Z:\EPC\2025\144 - LANDMARK BLDG - WEST ORANGE TOWNHOMEENGINEERING\AUTOCAD\DWG\25-144 PRELIM ENG 10-22-25.DWG, 2025.10.24, 10:48 AM

Section 1	Project Name and location information:	PRIVILEGE DEVELOPMENT TOWNHOUSE RESIDENTIAL DADE ROAD & SUNRISE BLVD
Section 2	Describe the nature of the construction activity:	Buildings w/ Parking, Drainage Infrastructure, and Water & Sewer Infrastructure
Section 3	Describe the intended sequence of major soil disturbing activities:	0-2 days, site prep and stabilized construction entrance; 3-6 days, install perimeter sediment and erosion controls; 7-60 days, install stormwater retention basin; 7-10 days, clearing/grubbing over all areas; 11-90 days, site grading; 90-150 days, install storm sewer and utilities; 150-180 days, stabilize site
Section 4	Total area of the site:	56,757 AC. - R.O.W. DEDICATION = 56,409 AC
Section 5	Total area of the site to be disturbed:	56,409 acres
Section 6	Existing data describing the soil or quality of any stormwater discharge from the site:	
Section 7	Estimate the drainage area size for each discharge point:	56,409 acres
Section 8	Latitude and longitude of each discharge point and identify the receiving water or MS4 for each discharge point:	N27°27'01.34", W80°22'27.90"

Section 9 Give a detailed description of all controls, Best Management Practices (BMPs) and measures that will be implemented at the construction site for each activity identified in the intended sequence of major soil disturbing activities section. Provide time frames in which the controls will be implemented. NOTE: All controls shall be consistent with performance standards for erosion and sediment control and stormwater treatment set forth in s. 62-40.432, F.A.C., the applicable Stormwater or Environmental Resource Permitting requirements of the Department or a Water Management District, and the guidelines contained in the State of Florida Erosion and Sediment Control Designer and Reviewer Manual, FDOT, FDEP, and any subsequent amendments.

* Prior to clearing, a silt fence (trenched 6 inches deep and backfilled on the uphill side), shall be installed around the perimeter of the site. 2 rock entrance roads (that are 60ft long with a 6 inch depth of FDOT #1 stone and lined with filter fabric) shall be constructed to minimize the effects of truck traffic and sedimentation tracking both on and off of the site. There will be only one construction entrance at this site.

* After the initial site grading work, all proposed inlet(s) / outfall, once installed, shall be protected from erosion and sediment runoff by the use of filter fabric and properly installed inlet filters. Disturbed portions of the site where construction activities have permanently ceased shall be stabilized with sod or other permanent stabilization methods (if other methods are used, this SWPPP will be modified) no later than 7 days after the last construction activity. Seeding shall be the same as in temporary seeding.

* All installation shall be commenced as depicted on the attached site map and installation "typicals" sheet.

Section 10 Describe all temporary and permanent stabilization practices. Stabilization practices include temporary seeding, mulching, permanent seeding, geotextiles, sod stabilization, vegetative buffer strips, protection of trees, vegetative preservation, etc.

* Temporary seeding shall be rye grass or other appropriate ground covers depending upon season of installation that is applied at manufacturer's recommendations to any disturbed areas that are inactive more than 7 days.

* Sod shall be used to stabilize the sides of the detention basins.

* Filter fabric shall be placed under the rock entrance / exit.

* Vegetative buffers shall be left undisturbed along the boundaries of the property, with the exception of exotic vegetation removal.

Section 11 Describe all structural controls to be implemented to divert stormwater flow from exposed soils and structural practices to store flows, retain sediment on-site or in any other way limit stormwater runoff. These controls include silt fences, earth dikes, diversions, swales, sediment traps, check dams, subsurface drains, pipe slope drains, level spreaders, storm drain inlet protection, rock outlet protection, reinforced soil retaining systems, gabions, coagulating agents and temporary or permanent sediment basins.

* A silt fence (with the same installation as indicated under the Best Management Practices heading) shall be placed around the entire perimeter of each construction phase.

* A wet detention basin shall be constructed which may be used as a temporary sediment basin (prior to being connected to a discharge structure) if needed. Care shall be taken to assure the removal of accumulated fine sediments and that the excessive compaction of soil by construction machinery is avoided.

* Inlets / Outfalls shall be protected with filter fabric and properly installed inlet filters (with the same installation as indicated under the Best Management Practices heading).

Section 12 Describe all sediment basins to be implemented for areas that will disturb 10 or more acres at one time. The sediment basins (or an equivalent alternative) should be able to provide 3,600 cubic feet of storage for each acre drained. Temporary sediment basins (or an equivalent alternative) are recommended for drainage areas under 10 acres.

No temporary sedimentation basins are proposed. The detention basins (prior to being connected to a discharge structure) may be used as a temporary sediment basin if needed.

Section 13 Describe all permanent stormwater management controls such as, but not limited to, detention or retention systems or vegetated swales that will be installed during the construction process.

* A stormwater detention basin shall be constructed per ERP permit and all disturbed areas shall be grassed.

DESCRIPTION	FOUND (YES/NO)	AGENCY CONTACT INFORMATION	MANAGEMENT PLAN (YES/NO)	RELOCATION PLAN (YES/NO)
WETLANDS	YES	---	---	---
RARE HABITAT	NO	---	---	---
THREATENED SPECIES	NO	---	---	---
ENDANGERED SPECIES	NO	---	---	---
SPECIES OF SPECIAL CONCERN	NO	---	---	---
INVASIVE/EXOTIC VEGETATION	YES	---	---	---

REFER TO ENVIRONMENTAL ASSESSMENT COMPLETED BY EDC (A DIVISION OF HALEY WARD, INC) ON 02-14-25

Section 14	Waste disposal, this may include construction debris, chemicals, litter, and sanitary wastes:	All construction materials and debris will be placed in a dumpster and hauled off site to a landfill or other proper disposal site. No materials will be buried on site.
Section 15	Offsite vehicle tracking from construction entrances/exits:	Off site vehicle tracking of sediments and dust generation will be minimized via a rock construction entrance, street sweeping and the use of water to keep dust down.
Section 16	The proper application rates of all fertilizers, herbicides and pesticides used at the construction site:	Florida-friendly fertilizers and pesticides will be used at a minimum and in accordance with the manufacturer's suggested application rates.
Section 17	The storage, application, generation and migration of all toxic substances:	All paints and other chemicals will be stored in a locked covered shed.
Section 18	Other:	Port-o-lets will be placed away from storm sewer systems, storm inlet(s), surface waters and wetlands. No vehicle maintenance shall be conducted on-site. A washdown area shall be designated at all times and will not be located in any area that will allow for the discharge of polluted runoff.

Section 19 Provide a detailed description of the maintenance plan for all structural and non-structural controls to assure that they remain in good and effective operating condition.

Contractor shall provide routine maintenance of permanent and temporary sediment and erosion control features in accordance with the technical specifications or as follows, whichever is more stringent:

- Silt fence shall be inspected at least weekly. Any required repairs shall be made immediately. Sediment deposits shall be removed when they reach approximately one-half the height of the barrier.
- Maintenance shall be performed on the rock entrance when any void spaces are full of sediment.
- Inlet(s)/outfalls shall be inspected immediately after each rain event and any required repairs to the filter inlets, silt fence, or filter fabric shall be performed immediately.
- Bare areas of the site that were previously seeded shall be reseeded per manufactures' instructions.
- Mulch and sod that has been washed out shall be replaced immediately.
- Maintain all other areas of the site with proper controls as necessary.

Section 20 Inspections: Describe the inspection and inspection documentation procedures, as required by the FDEP NPDES Generic Permit for Stormwater Discharge from Large and Small Construction Activities.

Qualified personnel will inspect all points of discharges, all disturbed areas of construction that have not been stabilized, constructed areas and locations where vehicles enter and exit the site, and all BMPs at least once every 7 calendar days and within 24 hours of the end of a rainfall event that is 0.5 inches or greater. Where sites have been finally stabilized, said inspections shall be conducted at least once every month until the Notice of Termination is filed.

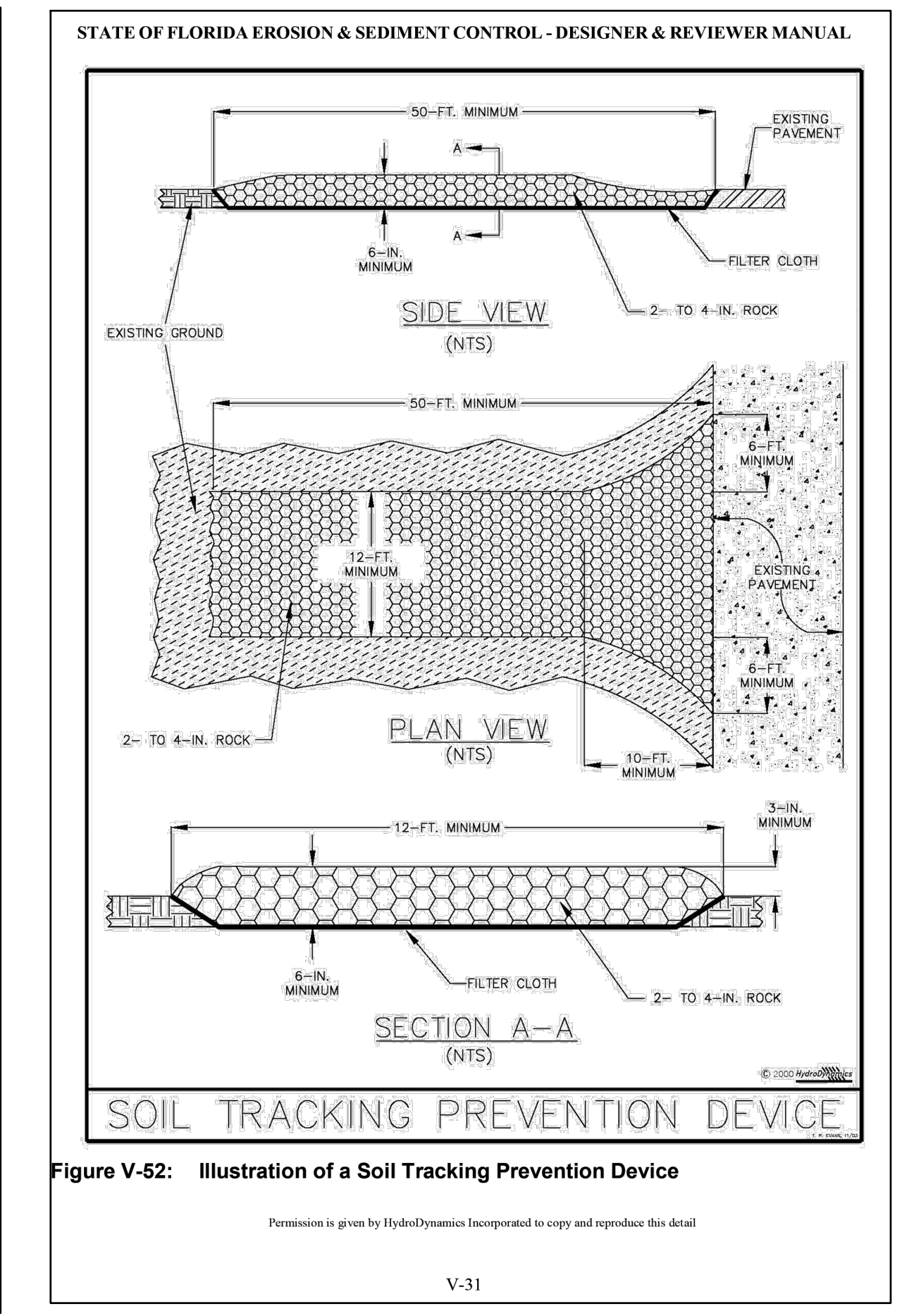
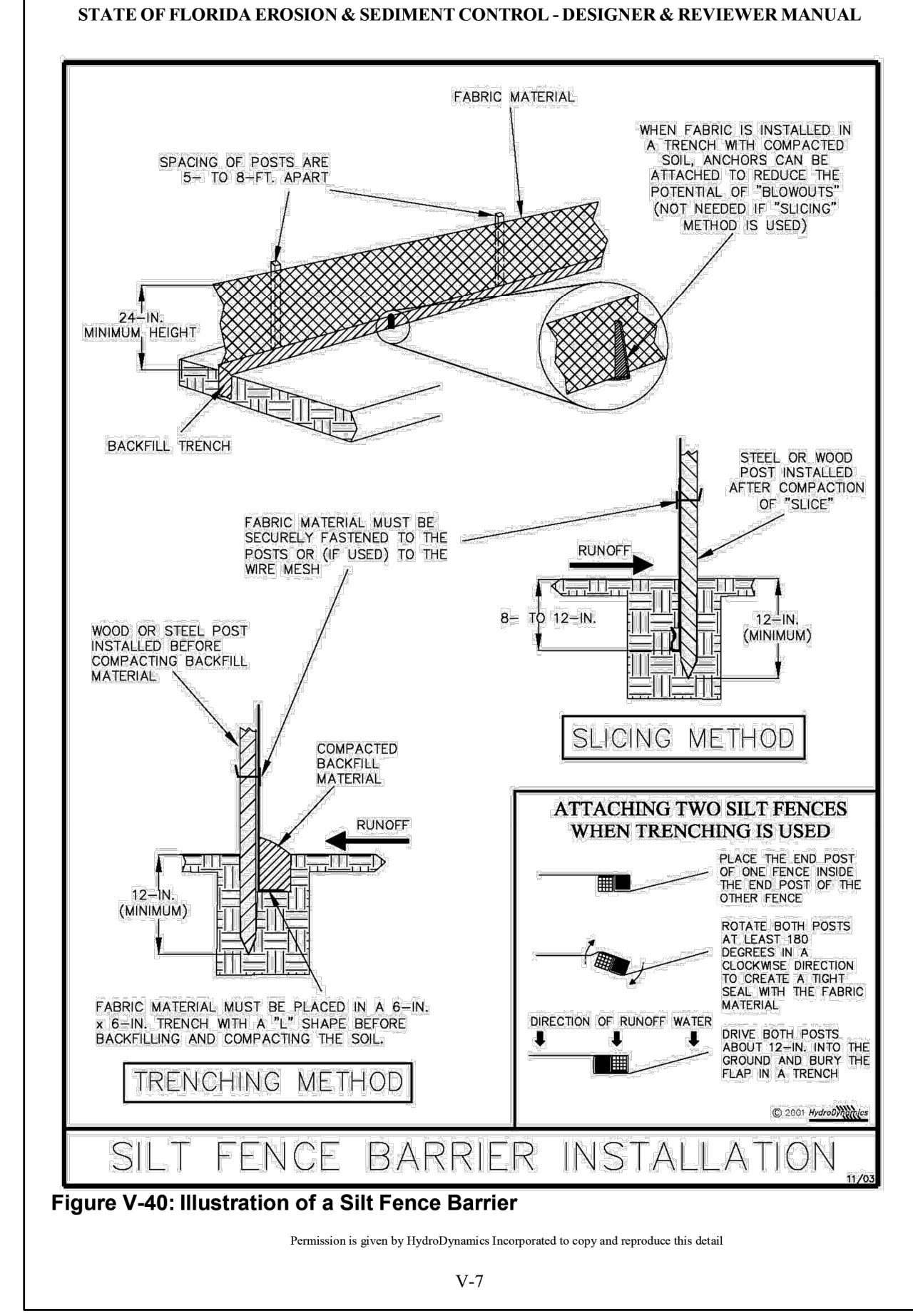
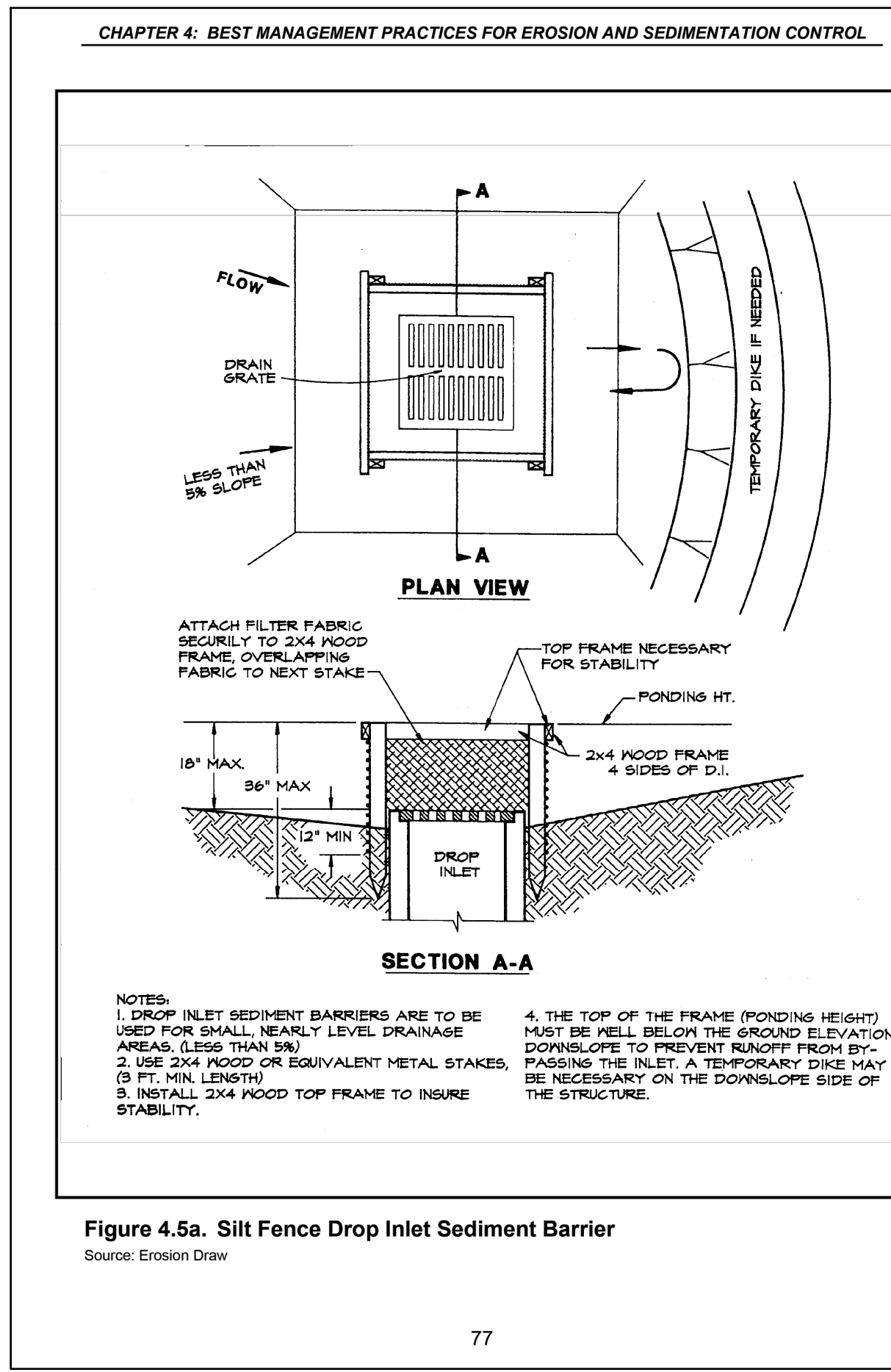
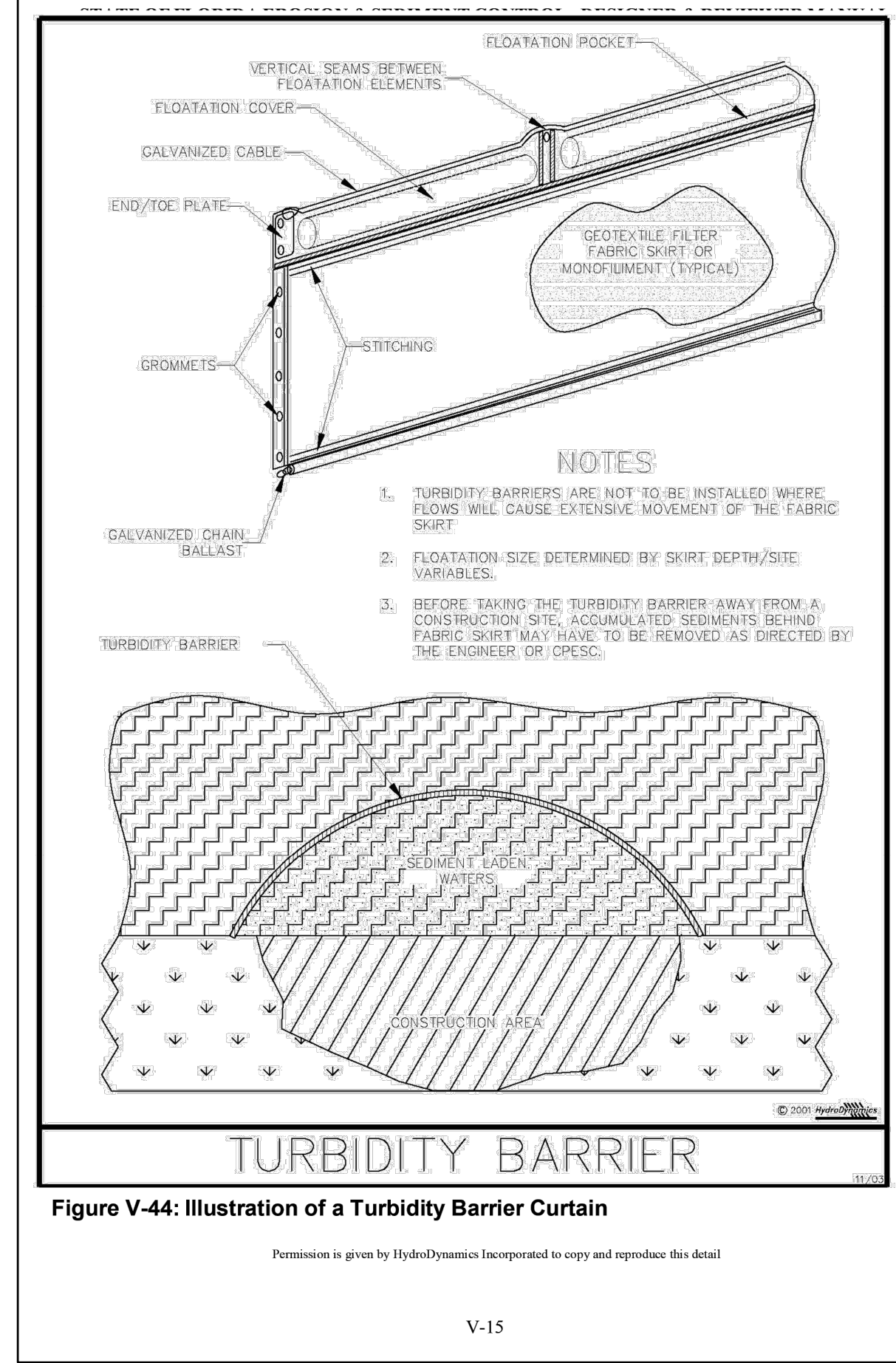
Section 21 Identify and describe all sources of non-stormwater discharges as allowed by the FDEP NPDES Generic Permit for Stormwater Discharge from Large and Small Construction Activities.

It is expected that the following non-stormwater discharges may occur from the site during construction period: wash water (where no spills or leaks of toxic or hazardous materials have occurred), and uncontaminated groundwater (from dewatering excavation). Dewatering shall be done in accordance with the requirements of the SFWMD Water Use Permit. If said discharges do occur, they will be directed to the temporary sediment basin prior to discharge. Turbid water from the stormwater pond shall not be pumped directly into the receiving waters. Any pumped water from the stormwater pond shall be treated so as to not allow a discharge of polluted stormwater. Treatment can include silt fences, settling ponds, the proper use of flocculating agents, or other appropriate means.

Section 22 All contractor(s) and subcontractor(s) identified in the SWPPP must sign the following certification:


"I certify under penalty of law that I understand, and shall comply with, the terms and conditions of the State of Florida Generic Permit for Stormwater Discharge from Large and Small Construction Activities and this Stormwater Pollution Prevention Plan prepared thereunder."

Name	Title	Company Name, Address and Phone Number	Date



EXCAVATION WARNING SIGN DETAIL

N.T.S.

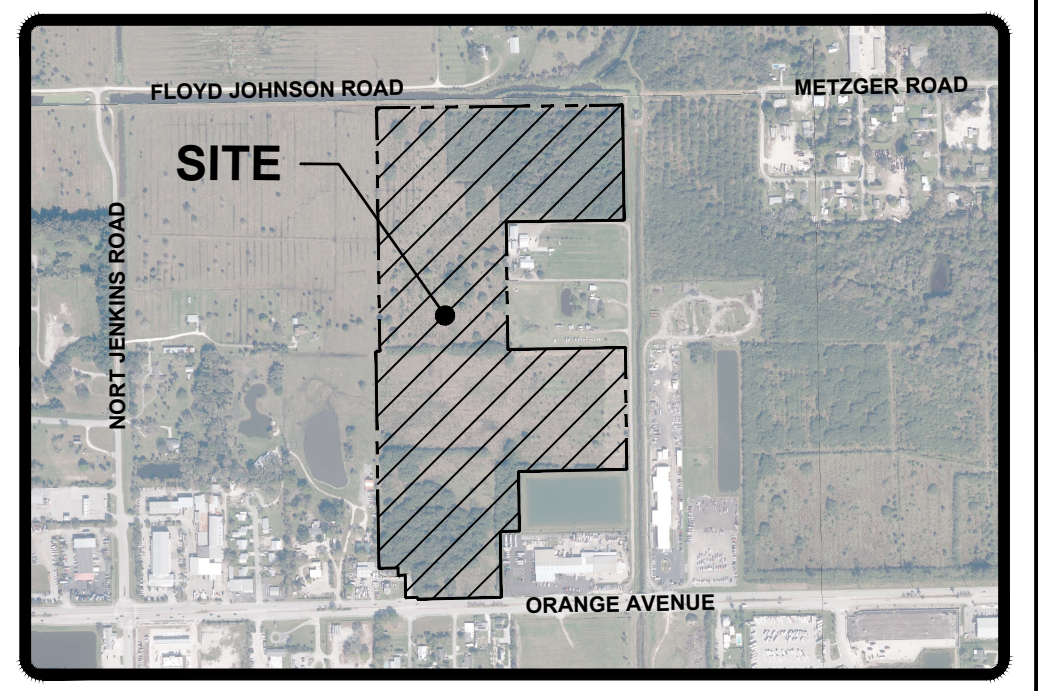
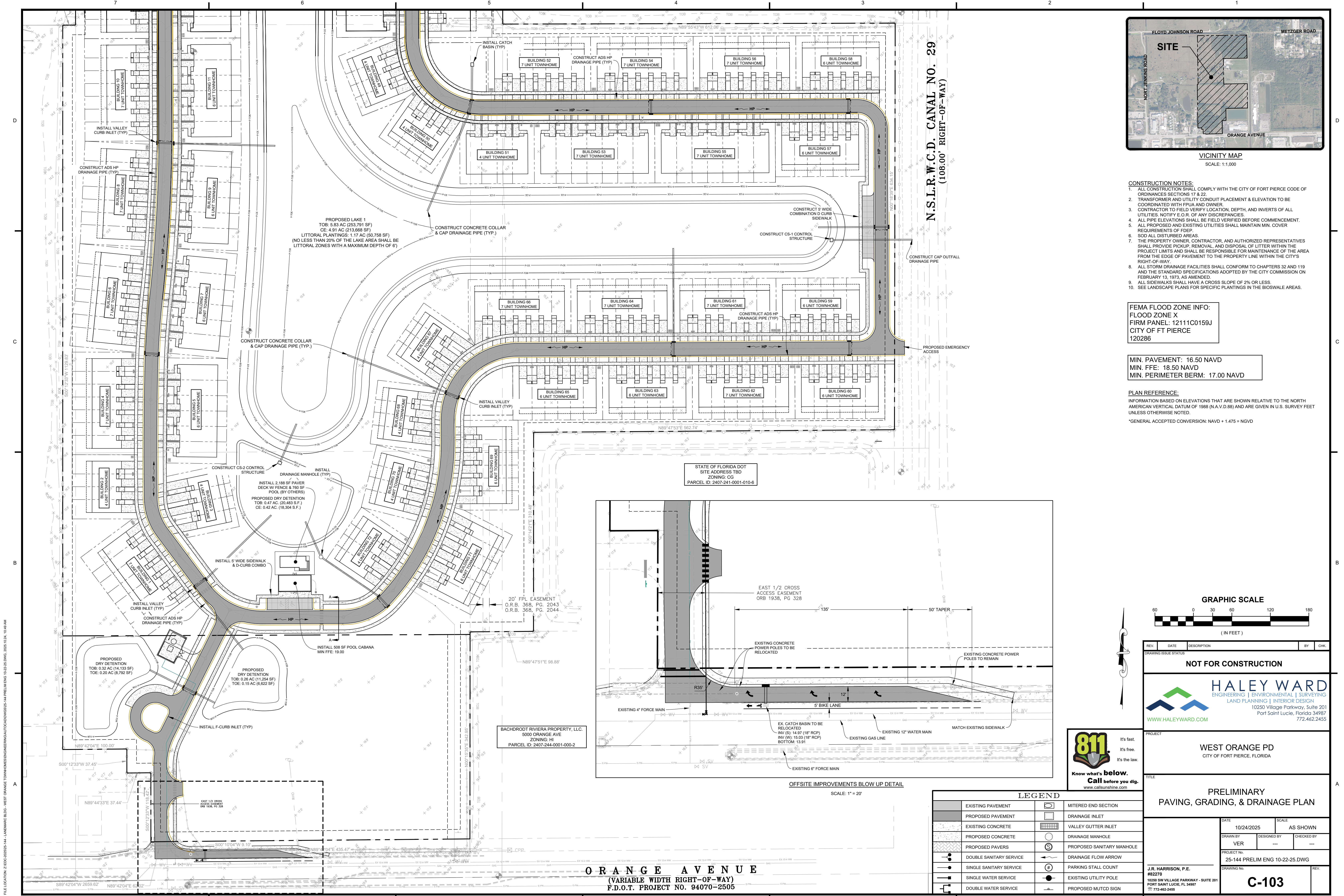
REV	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				
NOT FOR CONSTRUCTION				
 HALEY WARD ENGINEERING ENVIRONMENTAL SURVEYING LAND PLANNING INTERIOR DESIGN 10250 Village Parkway, Suite 201 Port Saint Lucie, Florida 34987 772.462.2455 WWW.HALEYWARD.COM				
PROJECT				
WEST ORANGE PD CITY OF FORT PIERCE, FLORIDA				
TITLE				
STORMWATER POLLUTION PREVENTION PLAN DETAILS				
DATE		SCALE		
10/24/2025		AS SHOWN		
DRAWN BY	DESIGNED BY	CHECKED BY		
VER	---	---		
PROJECT No.				
25-144 PRELIM ENG 10-22-25.DWG				
DRAWING No.				
C-102				
J.R. HARRISON, P.E. #62270 10260 SW VILLAGE PARKWAY - SUITE 201 PORT SAINT LUCIE, FL 34987 772-462-2455				



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It's free.
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Call before you dig.
www.callbeforeyoudig.com

FILE LOCATION: Z:\EDC\2025\25-144 LANDMARK BLDG - WEST ORANGE TOWNHOMESENGINEERING\TOD\DWG\25-144 PRELIM ENG 10-22-25.DWG, 2025.10.24, 10:48 AM

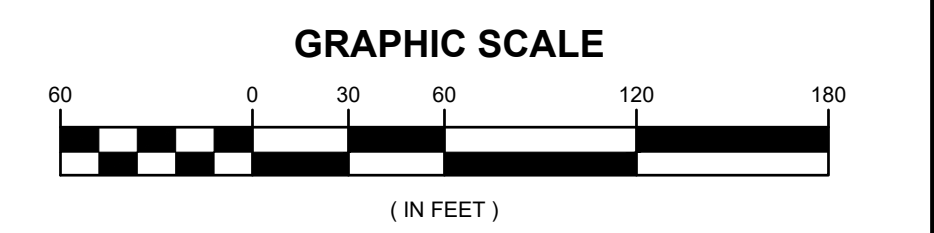
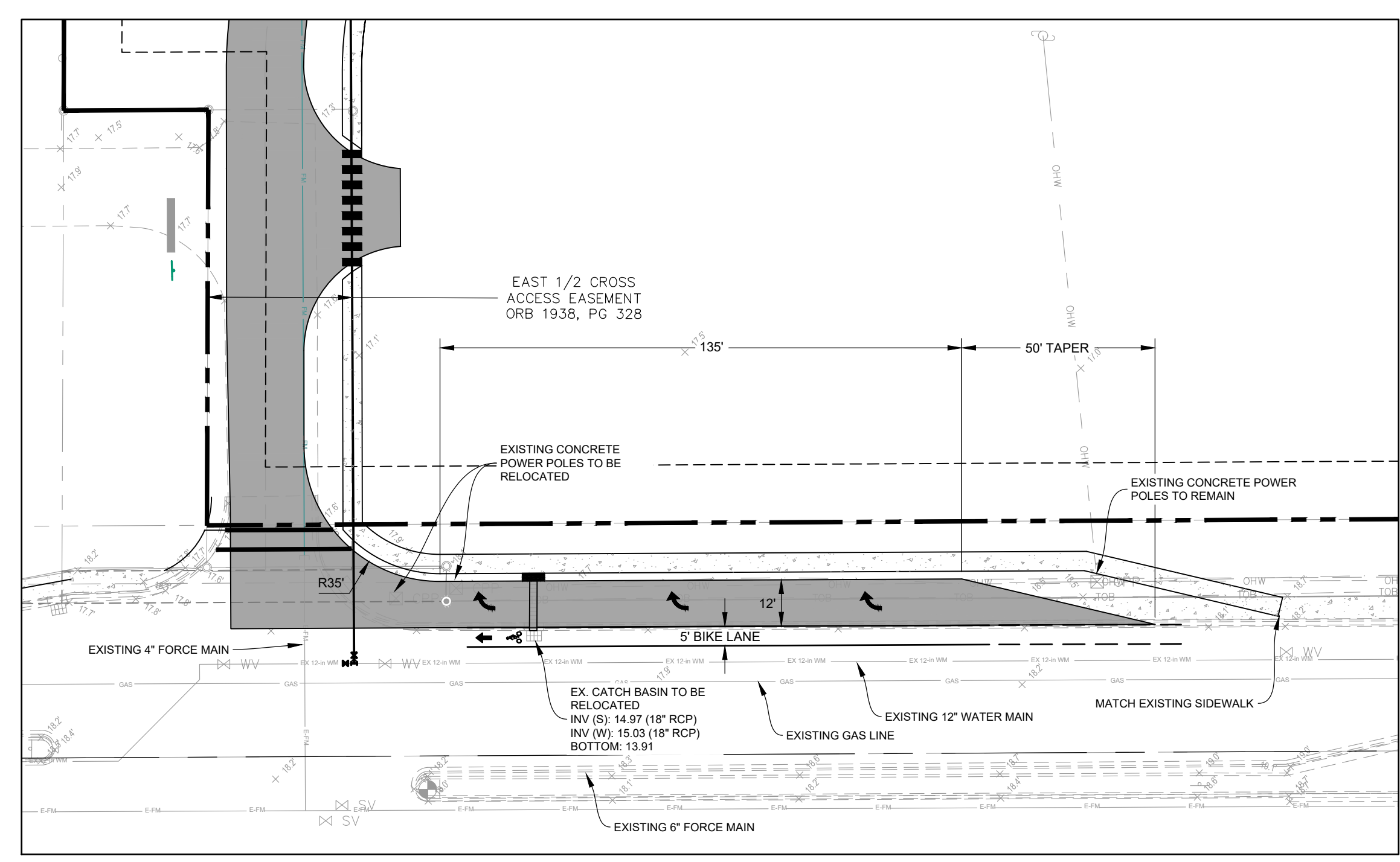


- CONSTRUCTION NOTES:**
- ALL CONSTRUCTION SHALL COMPLY WITH THE CITY OF FORT PIERCE CODE OF ORDINANCES SECTIONS 17 & 22.
 - TRANSFORMER AND UTILITY CONDUIT PLACEMENT & ELEVATION TO BE COORDINATED WITH FPLUA AND OWNER.
 - CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND INVERTS OF ALL UTILITIES. NOTIFY E.O.R. OF ANY DISCREPANCIES.
 - ALL PIPE ELEVATIONS SHALL BE FIELD VERIFIED BEFORE COMMENCEMENT.
 - ALL PROPOSED AND EXISTING UTILITIES SHALL MAINTAIN MIN. COVER REQUIREMENTS OF FDEP.
 - SOD ALL DISTURBED AREAS.
 - THE PROPERTY OWNER, CONTRACTOR, AND AUTHORIZED REPRESENTATIVES SHALL PROVIDE PICKUP, REMOVAL, AND DISPOSAL OF LITTER WITHIN THE PROJECT LIMITS AND SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE AREA FROM THE EDGE OF PAVEMENT TO THE PROPERTY LINE WITHIN THE CITY'S RIGHT-OF-WAY.
 - ALL STORM DRAINAGE FACILITIES SHALL CONFORM TO CHAPTERS 32 AND 119 AND THE STANDARD SPECIFICATIONS ADOPTED BY THE CITY COMMISSION ON FEBRUARY 13, 1973, AS AMENDED.
 - ALL SIDEWALKS SHALL HAVE A CROSS SLOPE OF 2% OR LESS.
 - SEE LANDSCAPE PLANS FOR SPECIFIC PLANTINGS IN THE BIOSWALE AREAS.

FEMA FLOOD ZONE INFO:
FLOOD ZONE X
FIRM PANEL: 12111C0159J
CITY OF FT PIERCE
120286

MIN. PAVEMENT: 16.50 NAVD
MIN. FFE: 18.50 NAVD
MIN. PERIMETER BERM: 17.00 NAVD

PLAN REFERENCE:
INFORMATION BASED ON ELEVATIONS THAT ARE SHOWN RELATIVE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.88) AND ARE GIVEN IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED.
*GENERAL ACCEPTED CONVERSION: NAVD + 1.475 = NGVD



REV	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				
NOT FOR CONSTRUCTION				

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
LAND PLANNING | INTERIOR DESIGN
10250 Village Parkway, Suite 201
Port Saint Lucie, Florida 34987
772.462.2455
WWW.HALEYWARD.COM

PROJECT:
WEST ORANGE PD
CITY OF FORT PIERCE, FLORIDA

TITLE:
PRELIMINARY
PAVING, GRADING, & DRAINAGE PLAN

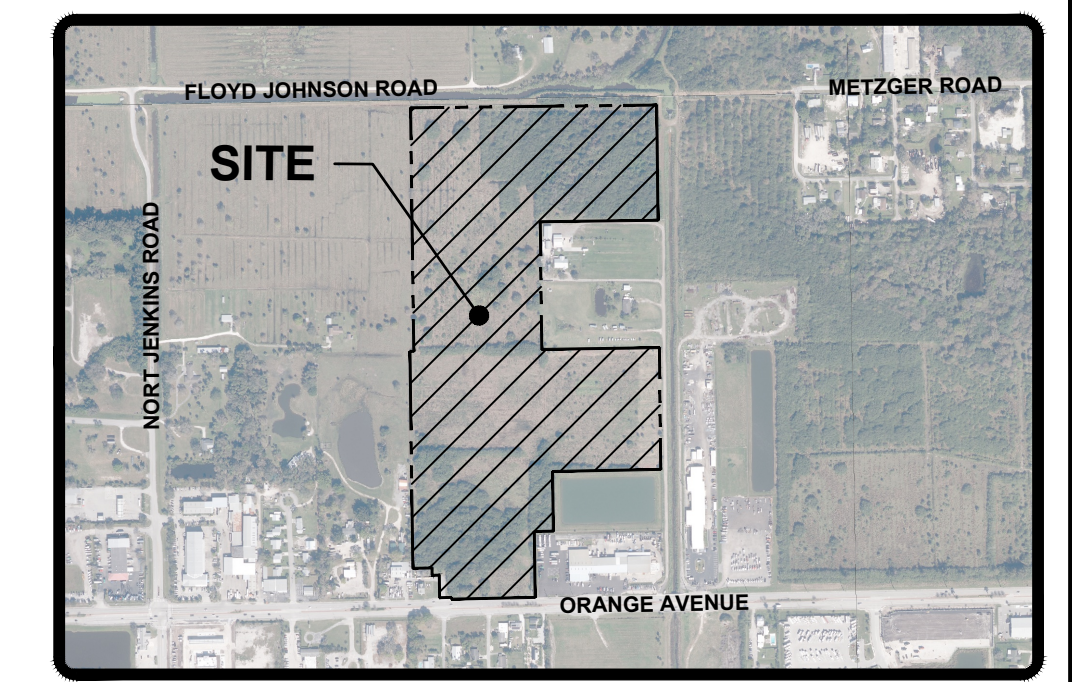
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10/24/2025		
DRAWN BY: VER	DESIGNED BY: ---	CHECKED BY: ---
PROJECT NO.: 25-144 PRELIM ENG 10-22-25.DWG		
DRAWING NO.: C-103		

N.S.L.R.W.C.D. CANAL NO. 29
(108.00' RIGHT-OF-WAY)

ORANGE AVENUE
(VARIABLE WIDTH RIGHT-OF-WAY)
F.D.O.T. PROJECT NO. 94070-2505

FILE LOCATION: Z:\EPC\2025\14 - LANDMARK BLDG - WEST ORANGE TOWNHOMEENGINEERING\A\04\DWG\25-144 PRELIM ENG 10-22-25.DWG, 2025.10.24, 10:48 AM

N.S.L.R.W.C.D. CANAL NO. 44
(RIGHT-OF-WAY WIDTH VARIES)



VICINITY MAP
SCALE: 1:1,000

CONSTRUCTION NOTES:

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- ALL SIDEWALKS SHALL HAVE A CROSS SLOPE OF 2% OR LESS.
- SEE LANDSCAPE PLANS FOR SPECIFIC PLANTINGS IN THE BIOSWALE AREAS.

FEMA FLOOD ZONE INFO:
FLOOD ZONE X
FIRM PANEL: 12111C0159J
CITY OF FT PIERCE
120286

MIN. PAVEMENT: 16.50 NAVD
MIN. FFE: 18.50 NAVD
MIN. PERIMETER BERM: 17.00 NAVD

PLAN REFERENCE:

INFORMATION BASED ON ELEVATIONS THAT ARE SHOWN RELATIVE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.88) AND ARE GIVEN IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED.

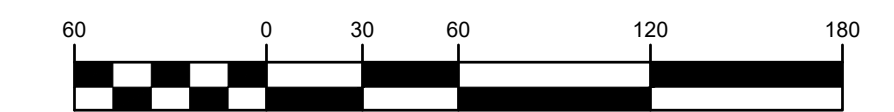
*GENERAL ACCEPTED CONVERSION: NAVD + 1.475 = NGVD

N.S.L.R.W.C.D. CANAL NO. 29
(108.00' RIGHT-OF-WAY)

JULIO CESAR GONZALEZ VALVERDE
4914 ORANGE AVE
ZONING: RS-2
PARCEL ID: 2407-214-0001-000-9

CECIL J CAMPBELL & BRENDA A CAMPBELL
4910 ORANGE AVE
ZONING: RS-2
PARCEL ID: 2407-214-0002-000-6

GRAPHIC SCALE



(IN FEET)

REV	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				

NOT FOR CONSTRUCTION



WEST ORANGE PD
CITY OF FORT PIERCE, FLORIDA

PRELIMINARY
PAVING, GRADING, & DRAINAGE PLAN

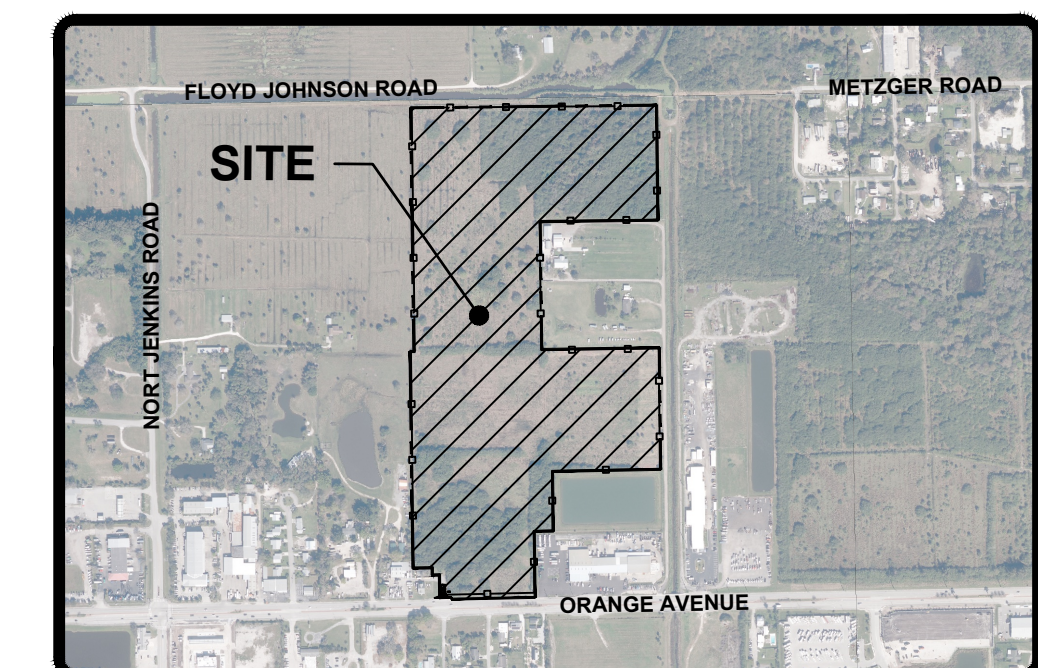


LEGEND	
EXISTING PAVEMENT	MITERED END SECTION
PROPOSED PAVEMENT	DRAINAGE INLET
EXISTING CONCRETE	VALLEY GUTTER INLET
PROPOSED CONCRETE	DRAINAGE MANHOLE
PROPOSED PAVERS	PROPOSED SANITARY MANHOLE
DOUBLE SANITARY SERVICE	DRAINAGE FLOW ARROW
SINGLE SANITARY SERVICE	PARKING STALL COUNT
SINGLE WATER SERVICE	EXISTING UTILITY POLE
DOUBLE WATER SERVICE	PROPOSED MUTCD SIGN

DATE	SCALE
10/24/2025	AS SHOWN
DRAWN BY VER	DESIGNED BY ---
PROJECT NO. 25-144 PRELIM ENG 10-22-25.DWG	
DRAWING NO. C-104	

J.R. HARRISON, P.E.
#62276
10260 SW VILLAGE PARKWAY - SUITE 201
PORT SAINT LUCIE, FL 34987
772-462-2455

FILE LOCATION: Z:\EPC\2025\25-144-LANDMARK BLDG - WEST ORANGE TOWNHOMEENGINEERING\TOWNHOME\DWG\25-144 PRELIM ENG 10-22-25.DWG, 2025.10.24, 10:48 AM



VICINITY MAP
SCALE: 1:1,000

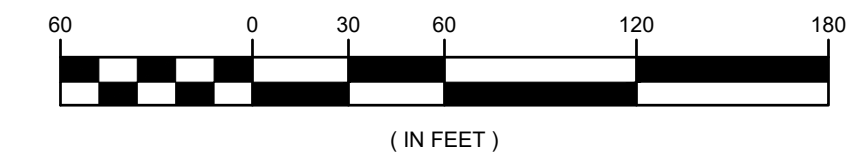
- CONSTRUCTION NOTES:**
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 9. ALL SIDEWALKS SHALL HAVE A CROSS SLOPE OF 2% OR LESS.
 10. SEE LANDSCAPE PLANS FOR SPECIFIC PLANTINGS IN THE BIOSWALE AREAS.

FEMA FLOOD ZONE INFO:
FLOOD ZONE X
FIRM PANEL: 12111C0159J
CITY OF FT PIERCE
120286

MIN. PAVEMENT: 16.50 NAVD
MIN. FFE: 18.50 NAVD
MIN. PERIMETER BERM: 17.00 NAVD

PLAN REFERENCE:
INFORMATION BASED ON ELEVATIONS THAT ARE SHOWN RELATIVE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.88) AND ARE GIVEN IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED.
*GENERAL ACCEPTED CONVERSION: NAVD + 1.475 = NGVD

GRAPHIC SCALE



REV	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				

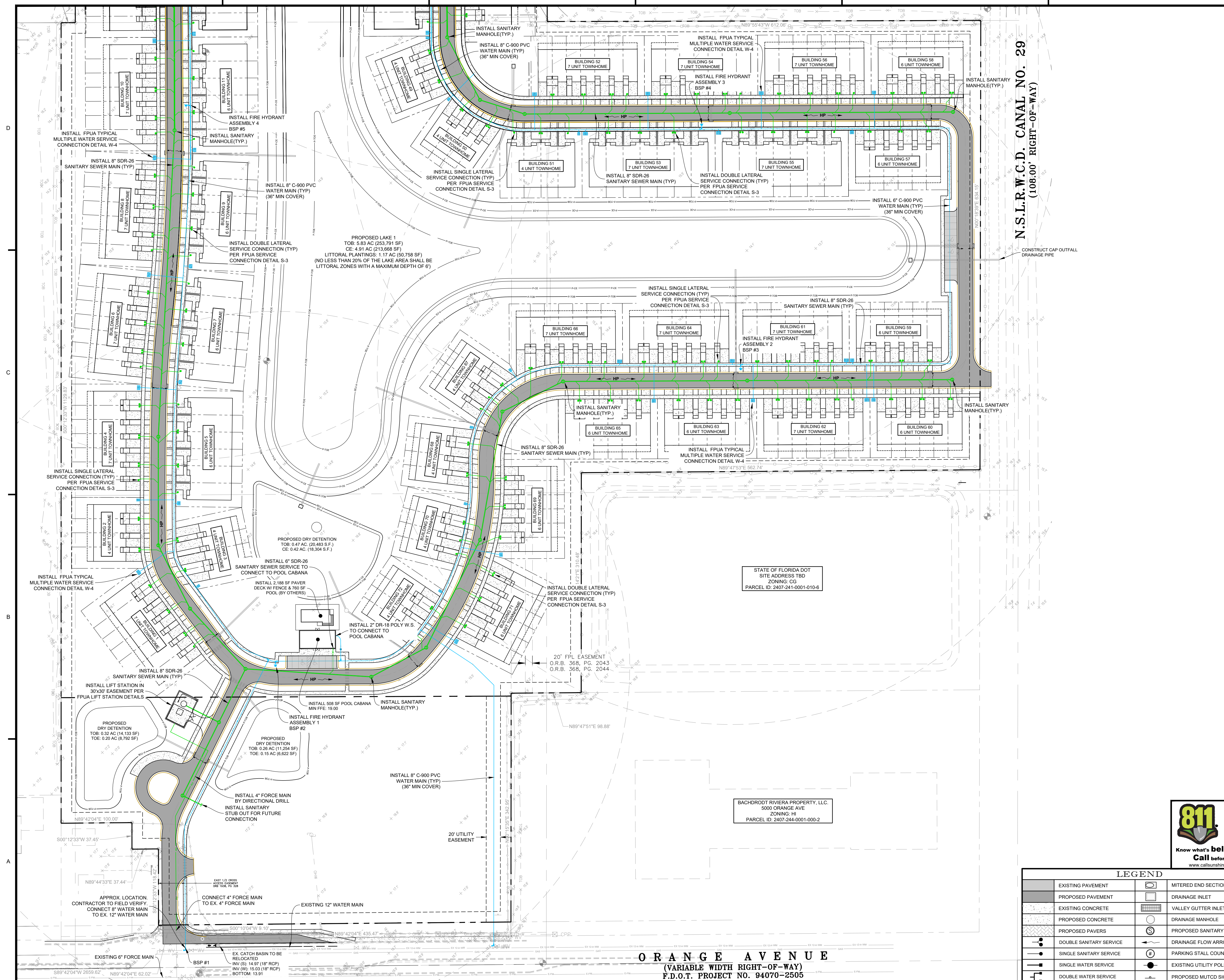
NOT FOR CONSTRUCTION



WEST ORANGE PD
CITY OF FORT PIERCE, FLORIDA

OVERALL UTILITY PLAN

N.S.L.R.W.C.D. CANAL NO. 29
(108.00' RIGHT-OF-WAY)



ORANGE AVENUE
(VARIABLE WIDTH RIGHT-OF-WAY)
F.D.O.T. PROJECT NO. 94070-2505

LEGEND

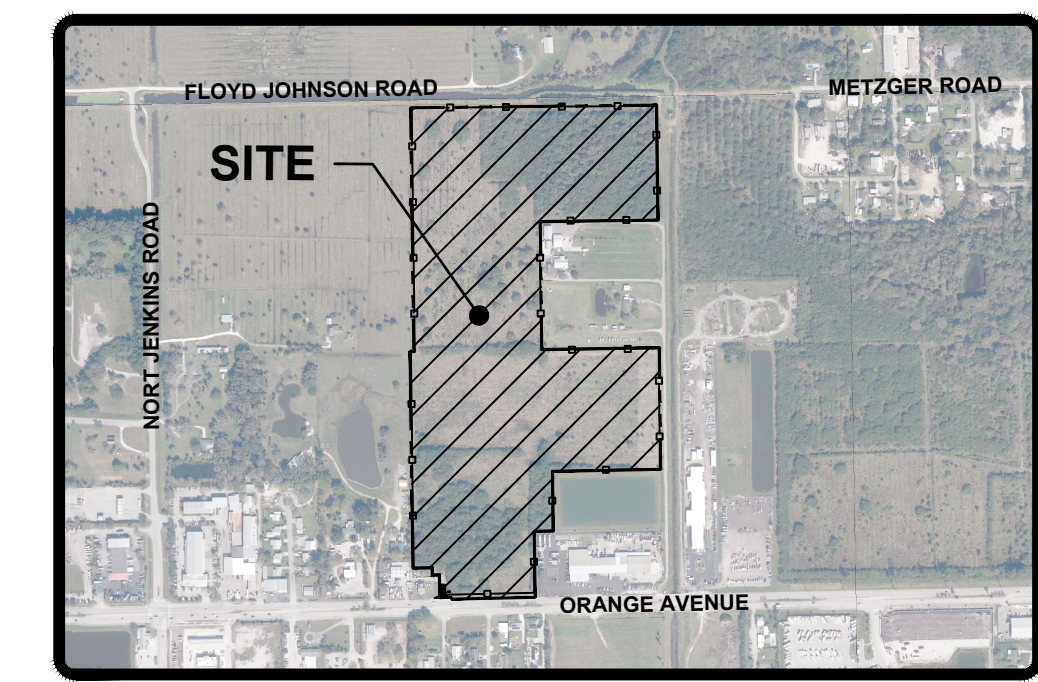
EXISTING PAVEMENT	MITERED END SECTION
PROPOSED PAVEMENT	DRAINAGE INLET
EXISTING CONCRETE	VALLEY GUTTER INLET
PROPOSED CONCRETE	DRAINAGE MANHOLE
PROPOSED PAVERS	PROPOSED SANITARY MANHOLE
DOUBLE SANITARY SERVICE	DRAINAGE FLOW ARROW
SINGLE SANITARY SERVICE	PARKING STALL COUNT
SINGLE WATER SERVICE	EXISTING UTILITY POLE
DOUBLE WATER SERVICE	PROPOSED MUTCD SIGN

DATE	SCALE	
10/24/2025	AS SHOWN	
DRAWN BY	DESIGNED BY	CHECKED BY
PROJECT NO.	25-144 PRELIM ENG 10-22-25.DWG	
DRAWING NO.	C-105	

J.R. HARRISON, P.E.
#22276
10250 SW VILLAGE PARKWAY - SUITE 201
PORT SAINT LUCIE, FL 34987
772-462-2455

FILE LOCATION: Z:\EPC\2025\25-144 LANDMARK BLDG. WEST ORANGE TOWNHOMEENGINEERING\10-22-25.DWG, 2025.10.24. 10:48 AM

N.S.L.R.W.C.D. CANAL NO. 44
(RIGHT-OF-WAY WIDTH VARIES)



VICINITY MAP
SCALE: 1:1,000

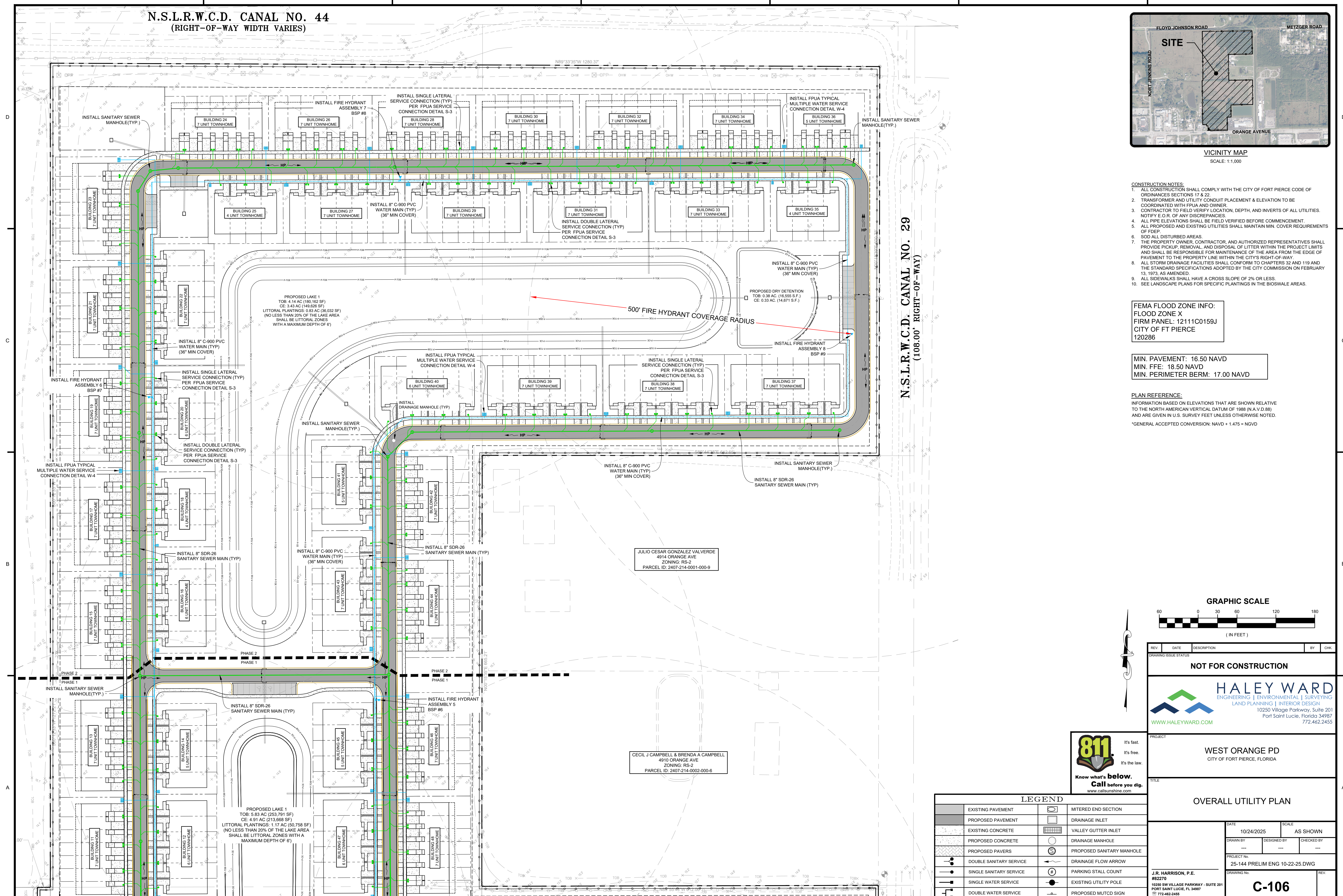
- CONSTRUCTION NOTES:
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FEMA FLOOD ZONE INFO:
FLOOD ZONE X
FIRM PANEL: 12111C0159J
CITY OF FT PIERCE
120286

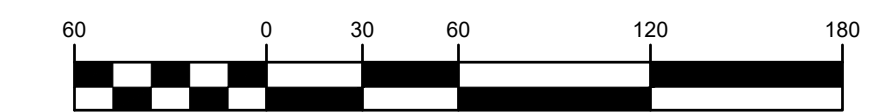
MIN. PAVEMENT: 16.50 NAVD
MIN. FFE: 18.50 NAVD
MIN. PERIMETER BERM: 17.00 NAVD

PLAN REFERENCE:
INFORMATION BASED ON ELEVATIONS THAT ARE SHOWN RELATIVE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.88) AND ARE GIVEN IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED.
*GENERAL ACCEPTED CONVERSION: NAVD + 1.475 = NGVD

N.S.L.R.W.C.D. CANAL NO. 29
(108.00' RIGHT-OF-WAY)



GRAPHIC SCALE



(IN FEET)

REV	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				

NOT FOR CONSTRUCTION



WEST ORANGE PD
CITY OF FORT PIERCE, FLORIDA

OVERALL UTILITY PLAN



LEGEND	
EXISTING PAVEMENT	MITERED END SECTION
PROPOSED PAVEMENT	DRAINAGE INLET
EXISTING CONCRETE	VALLEY GUTTER INLET
PROPOSED CONCRETE	DRAINAGE MANHOLE
PROPOSED PAVERS	PROPOSED SANITARY MANHOLE
DOUBLE SANITARY SERVICE	DRAINAGE FLOW ARROW
SINGLE SANITARY SERVICE	PARKING STALL COUNT
SINGLE WATER SERVICE	EXISTING UTILITY POLE
DOUBLE WATER SERVICE	PROPOSED MUTCD SIGN

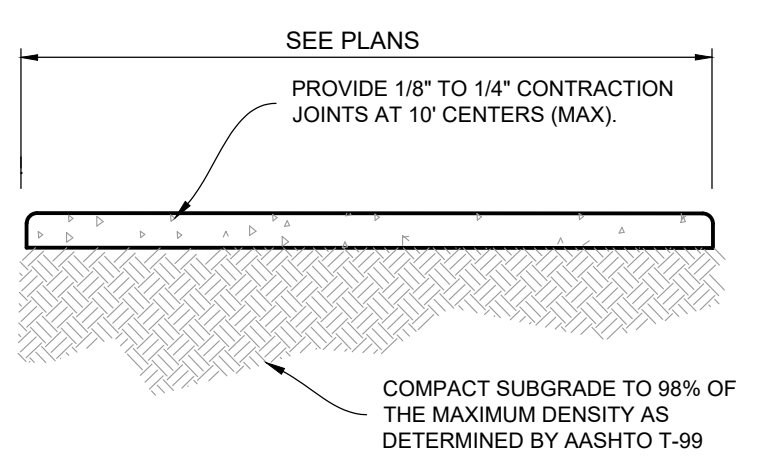
DATE	SCALE
10/24/2025	AS SHOWN
DRAWN BY	DESIGNED BY
PROJECT NO.	CHECKED BY
PROJECT NO. 25-144 PRELIM ENG 10-22-25.DWG	
DRAWING NO. C-106	
J.R. HARRISON, P.E. #2270 10250 SW VILLAGE PARKWAY - SUITE 201 PORT SAINT LUCIE, FL 34987 772-462-2455	

FILE LOCATION: Z:\EPC\2025\25-144 LANDMARK BLDG - WEST ORANGE TOWNHOME\ENGINEERING\AUTOCAD\DWG\25-144 PRELIM ENG 10-22-25.DWG, 2025.10.24, 10:48 AM

SPACING OR REQUIRED ROADWAY / PARKING AREA TESTS

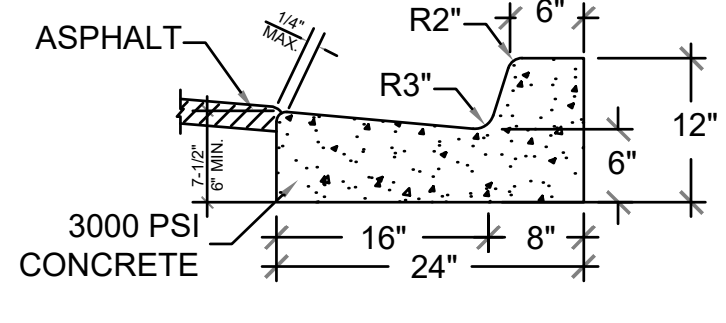
ITEMS TO BE TESTED	DENSITY		L.B.R.		THICKNESS	
	MAX. SPACING LIN. FT.	SQ. FT.	MAX. SPACING LIN. FT.	SQ. FT.	MAX. SPACING LIN. FT.	SQ. FT.
COMPACTED OR STABILIZED SUBGRADE	300	10,000	300	10,000	300	10,000
LIMEROCK BASE	300	10,000	300	10,000	300	10,000
SHELLROCK BASE	300	10,000	300	10,000	300	10,000
ASPHALT					PER NSP.	PER NSP.

NOTE: ALL TESTING SHALL BE TAKEN IN A STAGGERED SAMPLING PATTERN FROM A POINT 12" INSIDE THE LEFT EDGE OF THE ITEM TESTED, TO THE CENTER, TO A POINT 12" INSIDE

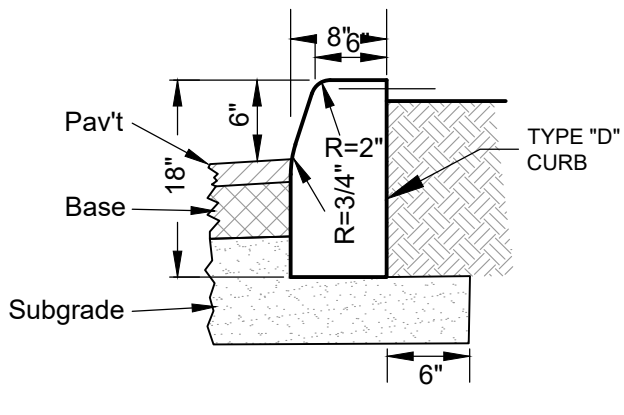


NOTES:
 1. USE 3,000 P.S.I. CONCRETE AT 28 DAYS FOR SIDEWALK CONSTRUCTION.
 2. SIDEWALK THICKNESS TO BE INCREASED TO 6" THICK AT DRIVEWAY LOCATIONS.
 3. ALL REPAIRS REQUIRED DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED 10' MINIMUM TO NEXT FULL JOINT.
 4. ALL SIDEWALK WITHIN FDOT RIGHT-OF-WAY TO BE CONSTRUCTED PER STANDARD PLANS 522-001

TYPICAL SIDEWALK DETAIL
NTS

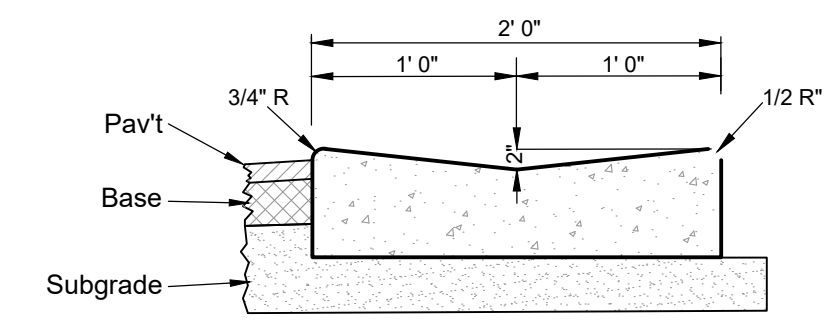


TYPE "F" CURB DETAIL
NTS



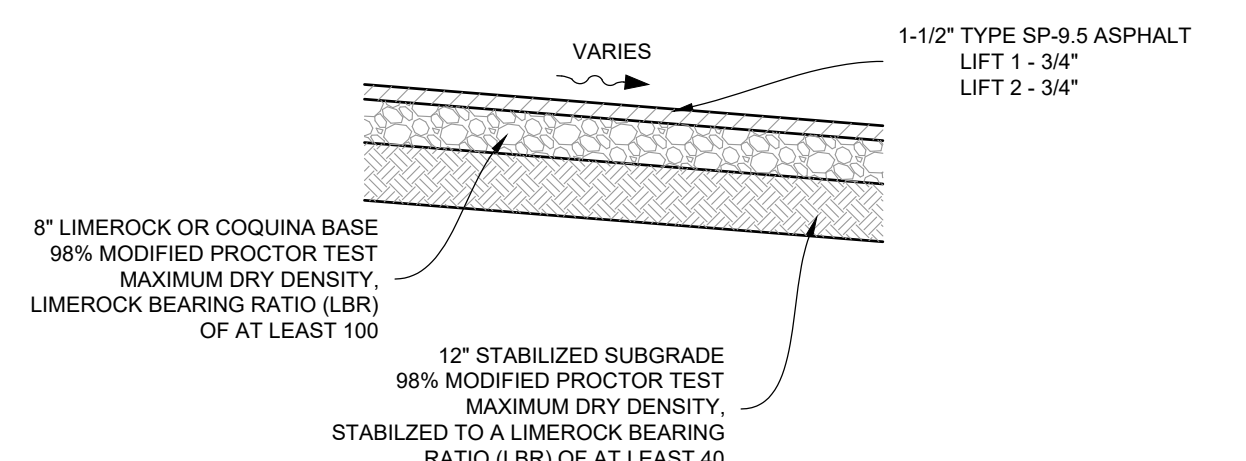
TYPE "D" CURB DETAIL
NTS

1. Use 3,000 P.S.I. concrete at 28 days for construction.
 2. Subgrade to extend an additional 6" beyond Type "D" Curb.
 3. Type "D" Curb to be constructed in accordance with FDOT Index #522-001.

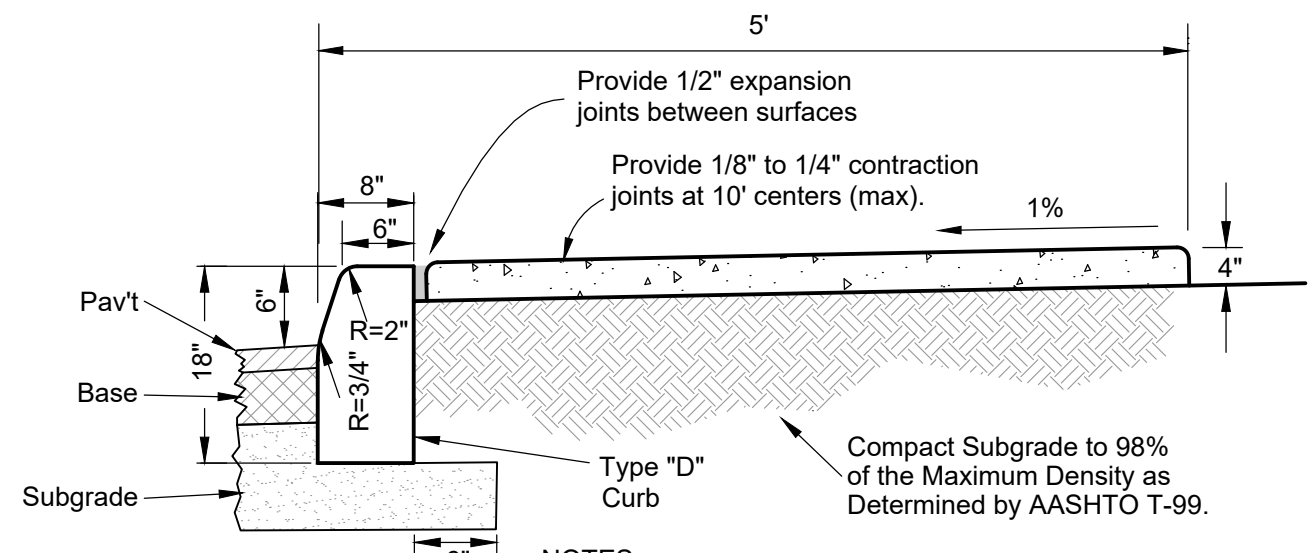


VALLEY GUTTER DETAIL
NTS

NOTES:
 3,000 P.S.I. CONCRETE AT 28 DAYS

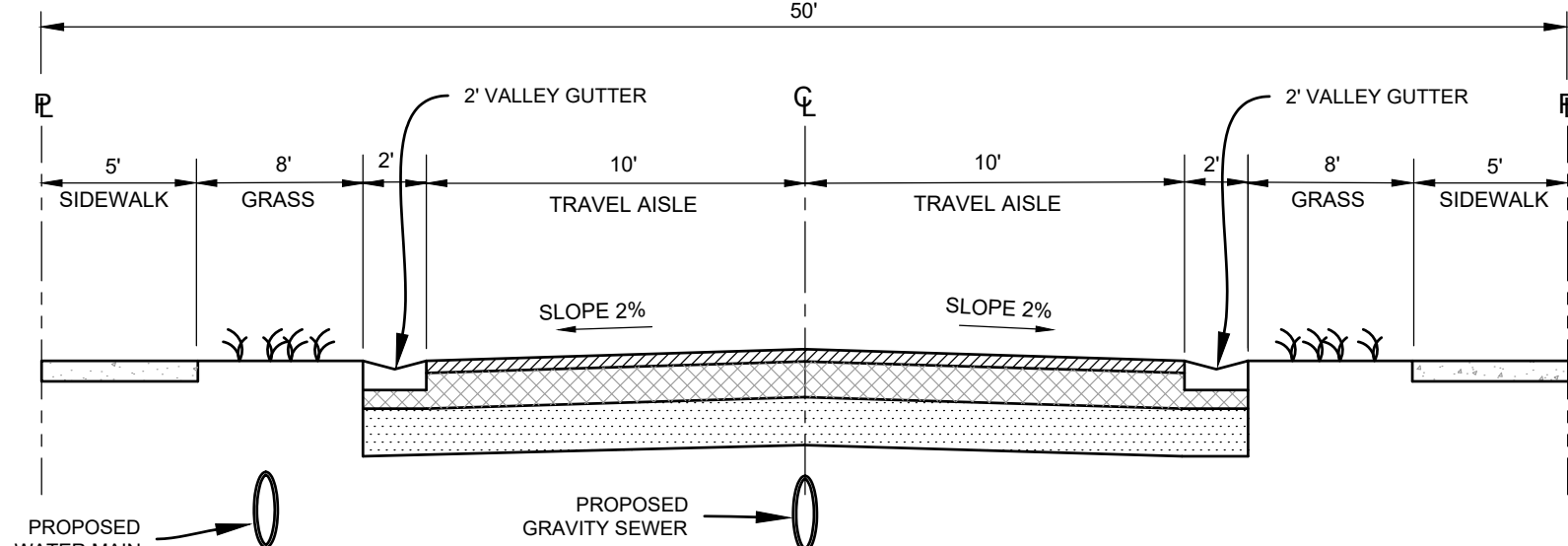


TYPICAL RIGID PAVEMENT SECTION
NTS

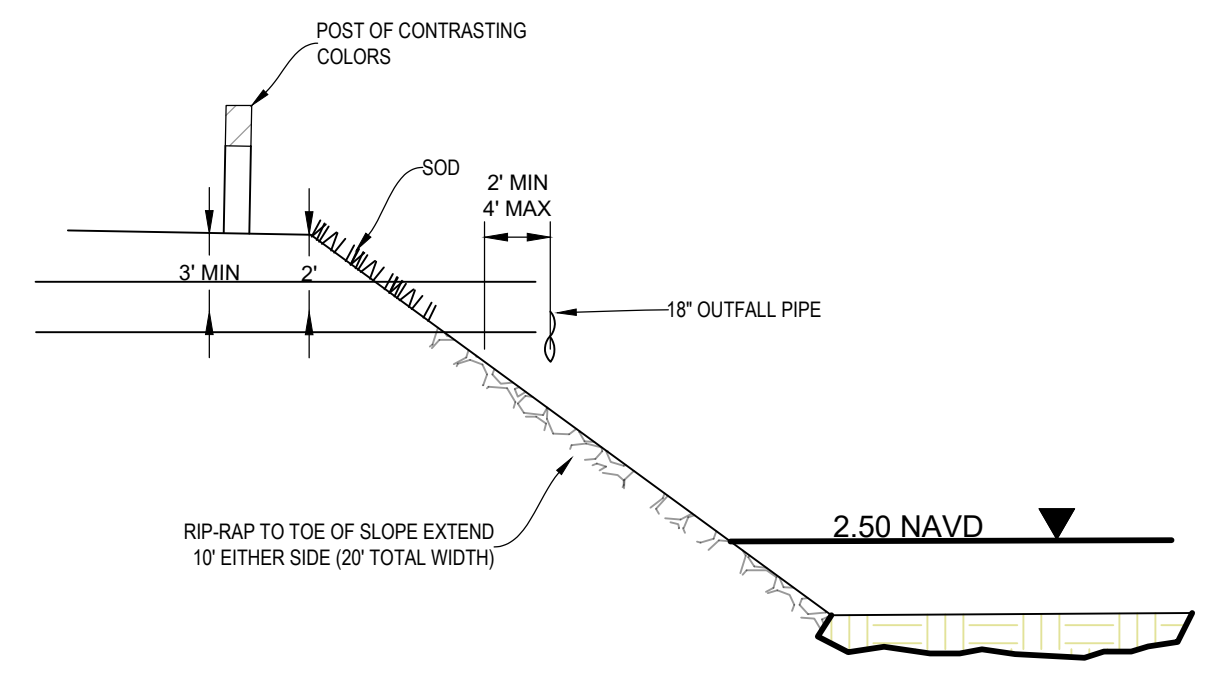


NOTES:
 1. Use 3,000 P.S.I. concrete at 28 days for construction.
 2. Subgrade to extend an additional 6" beyond Type "D" Curb.

TYPE "D" CURB & SIDEWALK COMBINATION
NTS



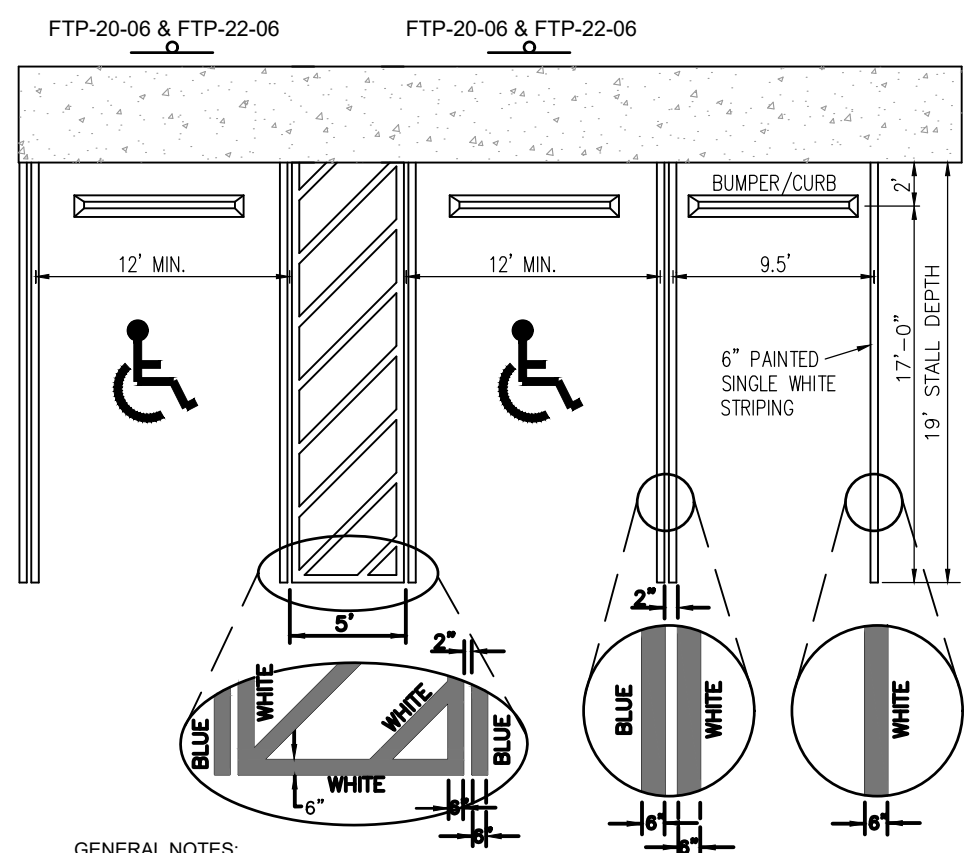
TYPICAL 50' ROADWAY SECTION
NTS



NSLRWCD TYPICAL CONNECTION DETAIL
NTS

NOTE: ALL NSLRWCD CANAL SLOPES SHALL BE SODDED EXCEPT FOR RIP-RAP AS SHOWN ON THIS DETAIL.

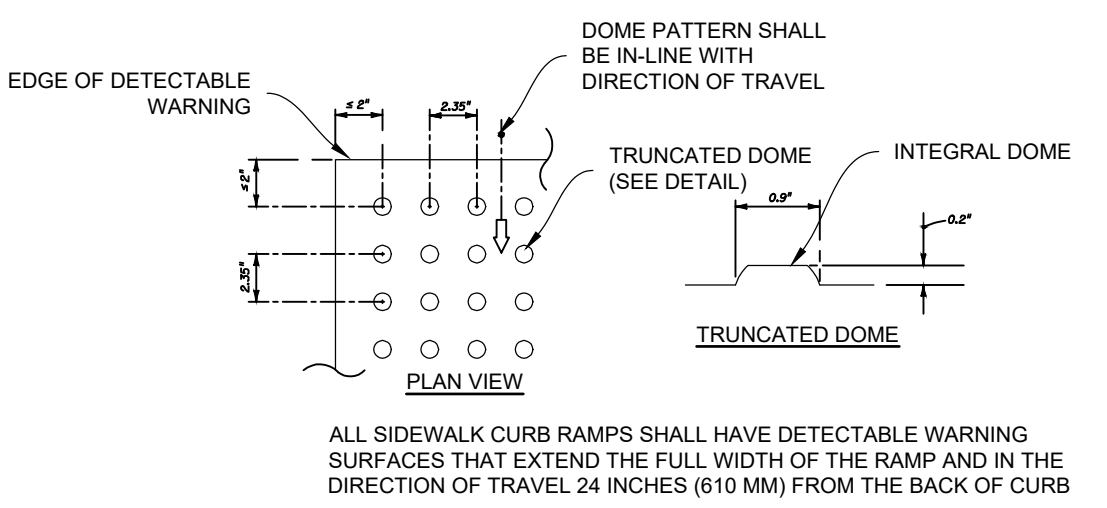
MATERIAL BENCHING DETAIL (TO EXISTING ROADWAY)
NTS



GENERAL NOTES:
 A) ALL PARKING SPACES WITH EXCEPTION OF ADA PARKING SPACES SHALL BE MARKED IN WHITE, RETRO-REFLECTIVE TRAFFIC PAINT AND BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2010 SECTION 710.
 B) ALL ADA PARKING SPACES SHALL BE PROPERLY SIGNED AND STRIPED IN ACCORDANCE WITH THE FDOT STANDARD INDEX 17346, FY 2025-26.
 C) ALL COMPACT SPACES SHALL BE MARKED "COMPACT" ON THE STALL OR TIRE STOP.

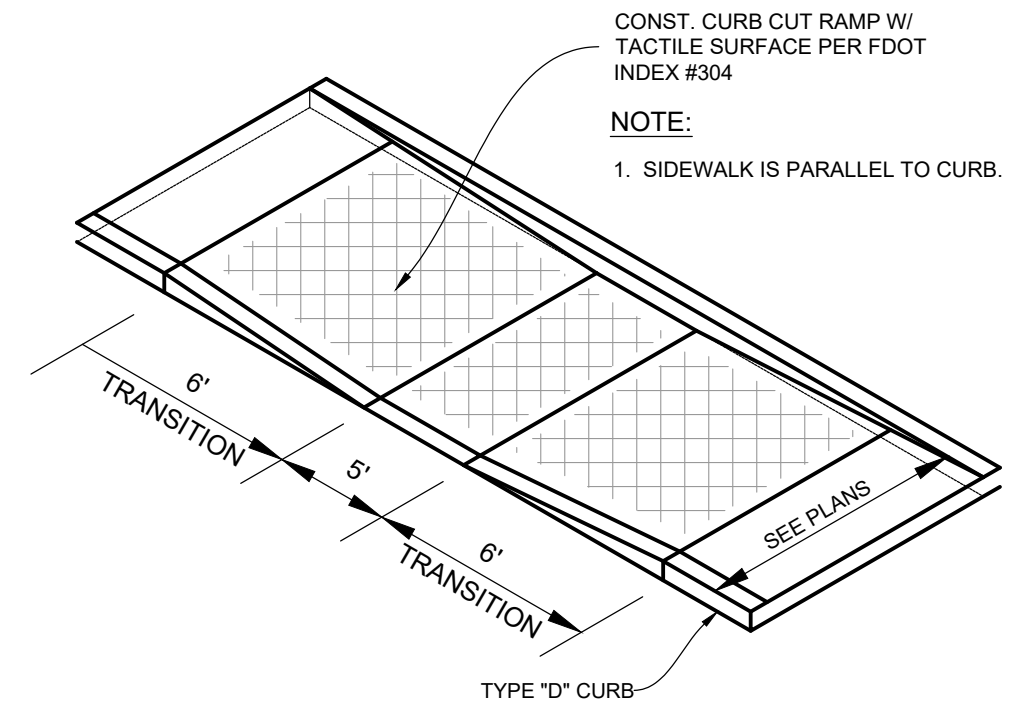
NOTES:
 1. FTP 20-06: TOP PORTION SHALL HAVE REFLECTIVE BLUE BACKGROUND WITH WHITE REFLECTIVE SYMBOL AND BORDER. BOTTOM PORTION SHALL HAVE A REFLECTIVE WHITE BACKGROUND WITH BLACK OPAQUE LEGEND AND BORDER.
 2. FTP 21-08: TOP PORTION SHALL HAVE REFLECTIVE BLUE BACKGROUND WITH WHITE REFLECTIVE SYMBOL AND BORDER. BOTTOM PORTION SHALL HAVE A REFLECTIVE WHITE BACKGROUND WITH BLACK OPAQUE LEGEND AND BORDER.
 3. FTP 20-06 IS FOR USE IN AREAS WHERE SPACE IS LIMITED.
 4. FTP 22-06 IS SUPPLEMENTAL PANEL FOR THE FTP 20-06 & FTP 21-08 SIGNS.
 5. HEIGHT SHALL BE 7 FEET MEASURED FROM THE GROUND OR SIDEWALK TO THE BOTTOM OF "PERMIT ONLY" SIGN OR 6 FEET TO THE BOTTOM OF "2500 FINE" SIGN.
 6. REFER TO FDOT INDEX No. 711-001 FOR PAVEMENT MARKING DETAILS & FDOT INDEX No. 700-102 SIGN DETAILS PER THE LATEST FDOT DESIGN STANDARDS.

HANDICAP AND TYPICAL PARKING DETAIL
NTS

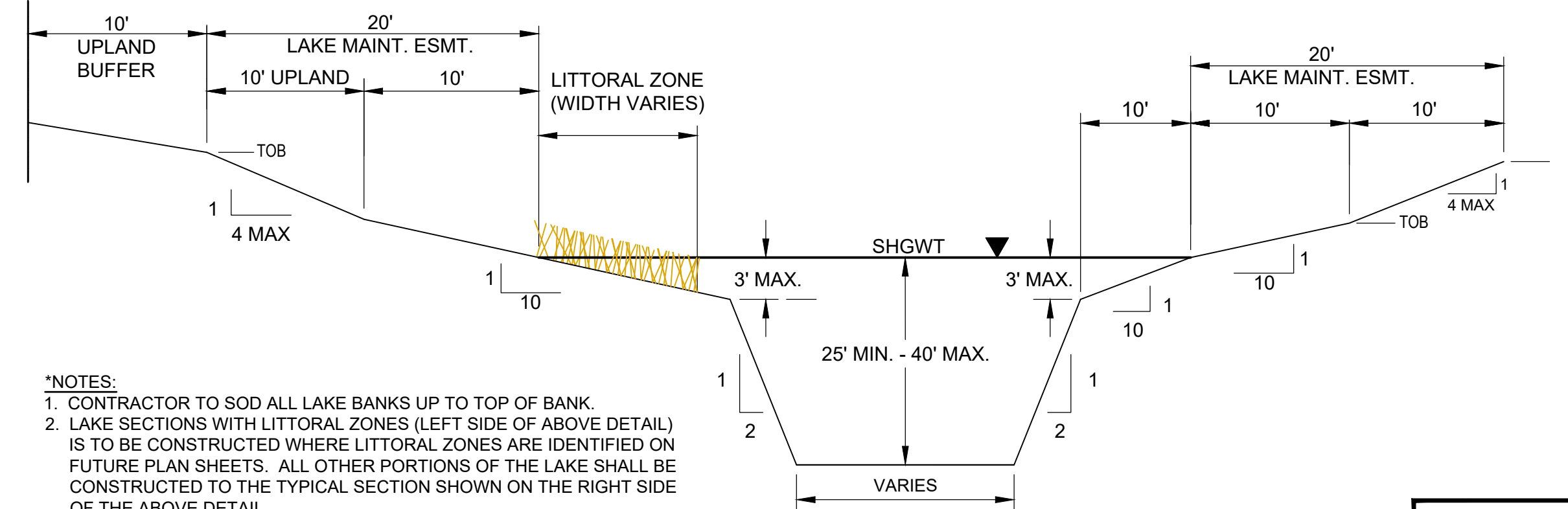


NOTES:
 REFER TO FDOT INDEX 304 FOR FURTHER DETAILS CAST IN PLACE

DETECTABLE WARNING SURFACE DETAIL
NTS



ADA RAMP DETAIL
NTS



TYPICAL LAKE CROSS SECTION
NOT TO SCALE

*NOTES:
 1. CONTRACTOR TO SOD ALL LAKE BANKS UP TO TOP OF BANK.
 2. LAKE SECTIONS WITH LITTORAL ZONES (LEFT SIDE OF ABOVE DETAIL) IS TO BE CONSTRUCTED WHERE LITTORAL ZONES ARE IDENTIFIED ON FUTURE PLAN SHEETS. ALL OTHER PORTIONS OF THE LAKE SHALL BE CONSTRUCTED TO THE TYPICAL SECTION SHOWN ON THE RIGHT SIDE OF THE ABOVE DETAIL.
 3. NO LESS THAN 20% OF THE LAKE WILL BE LITTORAL. PLEASE SEE ASSOCIATED LANDSCAPE PLANS FOR PLANTING DETAILS



REV.	DATE	DESCRIPTION	BY	CHK.
DRAWING ISSUE STATUS				
NOT FOR CONSTRUCTION				
 HALEY WARD ENGINEERING ENVIRONMENTAL SURVEYING LAND PLANNING INTERIOR DESIGN 9820 SW Village Parkway, Suite 200 Port Saint Lucie, Florida 34987 WWW.HALEYWARD.COM 222.850.2859				
PROJECT				
WEST ORANGE PD CITY OF FORT PIERCE, FLORIDA				
TITLE				
PAVING, GRADING & DRAINAGE DETAILS				
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