

City of Ramsey
Agenda
Regular City Council
Tuesday June 12, 2012
7:00 pm
Council Chambers, 7550 Sunwood Drive NW

- 1. Call to Order**
- 2. Presentation**
 1. Presentation of the 2011 Audit Report by the City's Audit Firm of Malloy, Montague, Karnowski, Radosevich & Company
- 3. Citizen Input**
- 4. Consent Agenda**
 1. Receive Cash & Investments for Period Ending April 30, 2012
 2. Receive 2011 Comprehensive Annual Financial Report (CAFR)
 3. Receive April 2012 Financial Reports - General Fund and Enterprise Funds
 4. Approve License Applications
 5. Approve the Following Council Meeting Minutes:
 1. City Council Work Session - March 13, 2012
 2. City Council Special - March 20, 2012
 3. City Council Work Session - March 27, 2012
 4. City Council Work Session - April 10, 2012
 5. City Council Work Session - April 24, 2012
 6. City Council Work Session - May 1, 2012
 7. City Council Work Session - May 8, 2012
 6. Approve Off-Sale Intoxicating Liquor, Off-Sale 3.2% Liquor, On-Sale Intoxicating Liquor, Beer, Sunday Sales, and Optional 2:00 a.m. Closing
 7. Approve the Policy for the Disposition of Surplus City Owned Land
 8. Adopt Resolution #12-06-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of May 17, 2012 through June 6, 2012
 9. Adopt the City Land Acquisition Policy
 10. Adopt Resolution #12-06-XXX to Accept the Resignation of the Deputy City Clerk
 11. Adopt Resolution #12-06-XXX to Accept the Resignation of the City Engineer
 12. Adopt Resolution #12-06-XXX Authorizing a Fixed Rate Membership in the 4M Fund

13. Report from Public Works meeting dated May 15, 2012.
5. **Approve Agenda**
6. **Public Hearing**
 1. Public Hearing to Consider introducing an Ordinance Amending Chapter 8 (Public Improvements and Special Assessments) of the City Charter as recommended by the Charter Commission.
7. **Council Business**
 1. Adopt Ordinance Amending City Code Chapter 10 (Animals) Related to the Keeping of Non-Domestic Animals on Parcels Less Than Three (3) Acres in Size
 2. Adopt Ordinance amending Chapter 54 of the Ramsey City Code, Known as the Traffic and Vehicle Chapter, Article IV, Sections 96-105 - Recreational Vehicles
 3. Consider Stormwater Improvements on 148th Lane NW and 149th Lane NW, and Approve Change Orders for City Improvement Project 12-24
 4. Consider Scope of Services and Authorize the Preparation of Plans and Specifications for Stormwater Improvements at Rum River Hills Golf Course
 5. Approve Irrigation and Topsoil Test Plots for North Commons
 6. Consider a Resolution to Reclassify Two Public Works Employees
 7. Update on Required Land Acquisitions for the Sunwood Drive Realignment Project - **PORTIONS OF THIS DISCUSSION MAY BE CLOSED TO THE PUBLIC**
8. **Mayor/Council/Staff Input**
9. **Adjournment**

Meeting Date: 06/12/2012

Information

Title:

Presentation of the 2011 Audit Report by the City's Audit Firm of Malloy, Montague, Karnowski, Radosevich & Company

Background:

Form Review

Inbox

Kurt Ulrich

Form Started By: Jo Thieling

Reviewed By

Kurt Ulrich

Date

06/07/2012 04:24 PM

Started On: 06/07/2012 08:47 AM

Final Approval Date: 06/07/2012

CC Regular Session

4. 1.

Meeting Date: 06/12/2012

By: Diana Lund, Finance

Information

Title:

Receive Cash & Investments for Period Ending April 30, 2012

Background:

Report on the City's cash and investments for the period ending April 30, 2012. Cash balances graph reflects the changes in cash balances on the city's major funds for the period ending April 30, 2012 in comparison to year ending December 31, 2011. December 2011 numbers reflect final audited numbers.

Council Action:

None required. Informational only.

Attachments

Cash & Investments for Period Ending April 30, 2012

Cash Balances on Major City Funds - April 30, 2012

Form Review

Inbox

Kurt Ulrich

Form Started By: Diana Lund

Reviewed By

Kurt Ulrich

Date

06/07/2012 02:13 PM

Started On: 05/14/2012 07:42 AM

Final Approval Date: 06/07/2012

CITY OF RAMSEY
REPORT OF POOLED CASH FLOWS
Period Ended April, 2012

	April-12 CURRENT MONTH	2012 YEAR-TO-DATE
CASH AND TEMPORARY INVESTMENTS		
BEGINNING BALANCE	\$ 44,635,484.53	\$ 45,949,302.73
CASH INFLOWS:		
Daily Deposit	190,513.58	2,817,259.68
Tax Settlements	-	89,679.38
U/B Receipts	28,763.19	779,288.56
Credit Cards	13,941.23	102,219.54
Interest Earnings [Net of Interest Paid on Investments]	65,889.30	374,827.43
Bond Proceeds	-	-
TOTAL CASH INFLOW	\$ 299,107.30	\$ 4,163,274.59
TOTAL CASH AVAILABLE	\$ 44,934,591.83	\$ 50,112,577.32
CASH OUTFLOWS:		
Prepaid Checks	1,547,659.05	\$ 2,957,775.28
Bills Lists	759,868.43	2,092,625.11
F&C Draws	435,678.63	435,678.63
Pay Estimates	44,990.00	1,241,336.00
Credit Cards	796.93	3,536.00
Payroll - Net	283,489.40	1,192,207.44
Flex Reimbursement	6,219.41	34,587.20
Void Checks/Dormant Checks Paid	(325.00)	(3,725.00)
Debt Service	-	302,173.75
Miscellaneous [Bank Charges; etc.]	139.40	307.33
TOTAL CASH OUTFLOW	\$ 3,078,516.25	\$ 8,256,501.74
POOLED CASH AND TEMPORARY INVESTMENTS		
ENDING BALANCE	\$ 41,856,075.58	\$ 41,856,075.58
MEMO - NET 2012 CASH INFLOW (OUTFLOW)	(2,779,408.95)	(4,093,227.15)
INVESTMENT PORTFOLIO SUMMARY		
BEGINNING BALANCE	\$ 35,777,986.45	\$ 42,230,022.32
Purchases	200,000.00	9,463,000.00
Maturities/Sales	(1,488,029.59)	(17,203,065.46)
ENDING BALANCE	\$ 34,489,956.86	\$ 34,489,956.86

2012 CASH AND INVESTMENT ACTIVITY

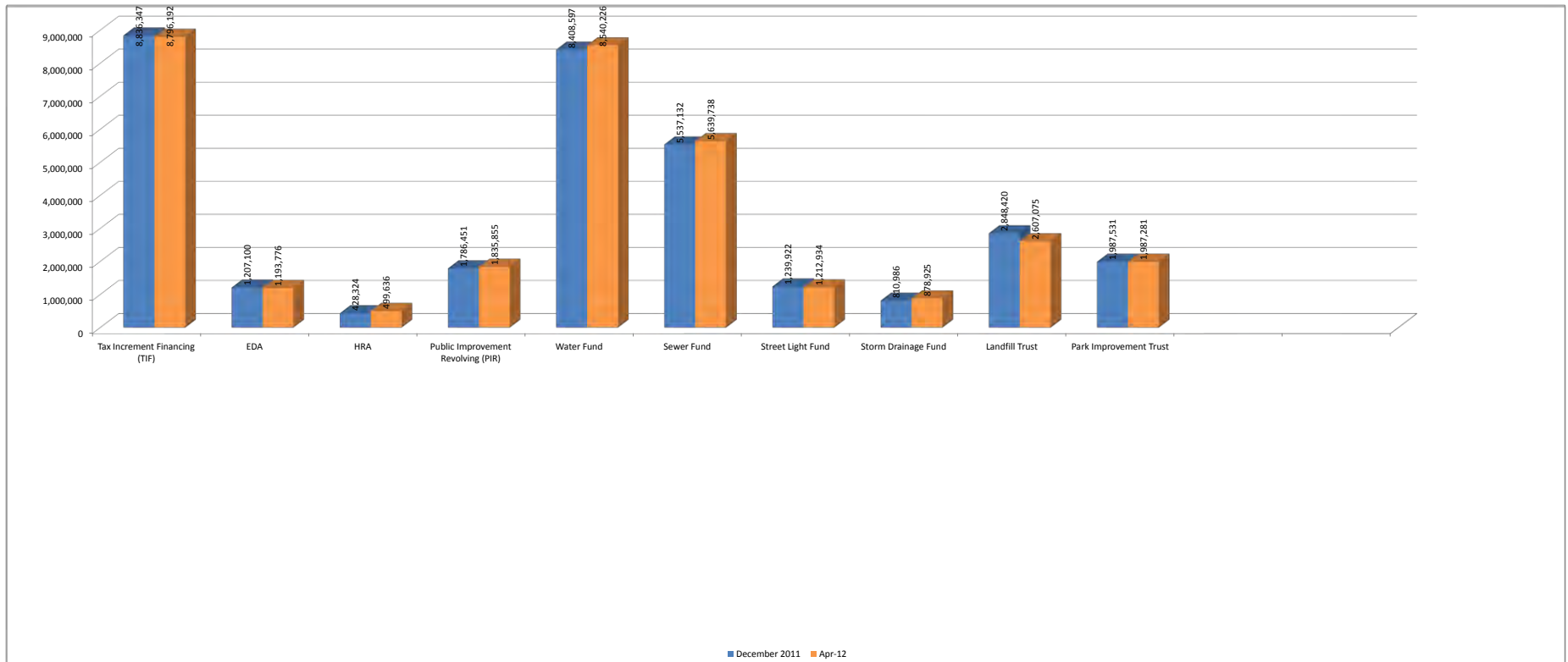
CITY INVEST #	STATED	BROKER	SECURITY DESCRIP	CUSIP	PRIN	PURCH	SOLD/	BV	PAR	YTM
	MAT DATE				BAL 1/1/2012		MATURE 2012	PRIN BAL 12/31/2012		
031029	9/15/2011	Landmark	CD-Landmark	old-12456 new-13672	317,299.14			317,299.14 317,299.14	256,633	1.50%
120320	9/20/2012	4M	Term Series 4M			2,000,000.00	0.00	2,000,000.00	2,000,000	0.13%
110714A	1/12/2012	4M	Term Series 4M		2,000,000.00		2,000,000.00	0.00	2,000,000	0.20%
111220A	3/20/2012	BOW	CD-BOW		1,000,000.00		1,000,000.00	0.00	1,000,000	0.44%
111220B	6/20/2012	BOW	CD-BOW		2,000,000.00		2,000,000.00	0.00	2,000,000	0.72%
111230	3/30/2012	BOW	CD-BOW		2,000,000.00		2,000,000.00	0.00	2,000,000	0.38%
110714C	1/13/2012	BOW	CD-BOW	102992	1,000,000.00		1,000,000.00	0.00	1,000,000	0.26%
110714D	4/13/2012	BOW	CD-BOW	102993	1,000,000.00		1,000,000.00	0.00	1,000,000	0.32%
110727	1/27/2017	BOW	FHLB	313374T34-0V0	500,000.00		500,000.00	0.00	500,000	2.35%
091229D	12/29/2014	BOW	FFCB	31331JAN3	1,000,000.00		0.00	1,000,000.00	1,000,000	2.75%
100305	3/5/2015	BOW	FFCB	31331JFY4	1,000,000.00		1,000,000.00	0.00	1,000,000	2.75%
100723	7/23/2015	BOW	FNMA	3134G1MG6062	1,000,000.00		1,000,000.00	0.00	1,000,000	2.45%
								1,000,000.00		
	FNC		Clearing from 2010							
101227	12/17/2012	ICD SEC	CD-ALLY BANK	02005QBU6	176,000.00		0.00	176,000.00	176,000	0.85%
081028	10/28/2013	ICD SEC	CD-AMERICAN CHA	27006	100,000.00		0.00	100,000.00	100,000	5.50%
110609	6/9/2014	ICD SEC	CD-AMERICAN EXPI	02587DAX6	250,000.00		0.00	250,000.00	250,000	1.25%
100812A	8/13/2012	ICD SEC	CD-AMERICAN PLU	58469	100,000.00		0.00	100,000.00	100,000	1.10%
101229B	6/29/2012	ICD SEC	CD-AURORA BANK	05155TAL2	150,000.00		0.00	150,000.00	150,000	70.00%
110803A	2/4/2013	ICD SEC	CD-BANCO POPULA	05967EGL7	200,000.00		0.00	200,000.00	200,000	0.65%
091217C	6/17/2012	ICD SEC	CD-BANK OF AMERJ	3510	150,000.00		0.00	150,000.00	150,000	2.60%
091203	12/3/2014	ICD SEC	CD-BANK OF AMERJ	3510	100,000.00		0.00	100,000.00	100,000	3.50%
111221B	10/23/2013	ICD SEC	CD-BANK OF CHINA	06425HVS3	100,000.00		0.00	100,000.00	100,000	1.10%
110629B	12/28/2012	ICD SEC	CD-BANK OF CHINA	06425P4P0	146,000.00		0.00	146,000.00	146,000	0.75%
081223	12/23/2013	ICD SEC	CD-BANK OF HOLL	34862	100,000.00		0.00	100,000.00	100,000	4.95%
081009	10/9/2013	ICD SEC	CD-CAPITOL CITY B	33938	100,000.00		0.00	100,000.00	100,000	5.25%
110831	2/28/2014	ICD SEC	CD-CIT BANK	172854AWZ7	200,000.00		0.00	200,000.00	200,000	1.05%
110803C	2/4/2013	ICD SEC	CD-CITIZEN BANK C	57282	100,905.75		0.00	100,905.75	100,000	0.60%
100114	1/17/2012	ICD SEC	CD-COMMERCIAL B	17225	150,000.00		150,000.00	0.00	150,000	1.60%
090220B	2/21/2012	ICD SEC	CD-COMMONWEAL	57201	100,000.00		100,000.00	0.00	100,000	2.80%
110930	9/30/2013	ICD SEC	CD-COMPASS BANK	20449E3C8	100,000.00		0.00	100,000.00	100,000	1.00%
111014	10/21/2013	ICD SEC	CD-COMPASS BANK	20449E3Z7	100,000.00		0.00	100,000.00	100,000	1.05%
110629A	7/1/2013	ICD SEC	CD-DISCOVER BAN	254670G48	248,000.00		0.00	248,000.00	248,000	0.85%
101221	12/23/2013	ICD SEC	CD-DORAL BANK	25811LYN3	249,000.00		0.00	249,000.00	249,000	1.45%
110318	9/18/2012	ICD SEC	CD-FIRST BANK OF	337624U40	100,000.00		0.00	100,000.00	100,000	1.00%
120123	1/23/2015	ICD SEC	CD-STATE BANK OF	33664		100,000.00	0.00	100,000.00	100,000	1.10%
120131	1/30/2015	ICD SEC	CD-SAFRA NATION	786584YA2	150,000.00		0.00	150,000.00	150,000	1.00%
120210	8/12/2013	ICD SEC	CD-BMW BANK	05568PV46	100,000.00		0.00	100,000.00	100,000	0.60%
120208A	5/8/2013	ICD SEC	CD-BEAL BANK USA	07370SK20	249,000.00		0.00	249,000.00	249,000	0.40%
120208B	2/7/2013	ICD SEC	CD-SOVEREIGN BAN	84603MV29	249,000.00		0.00	249,000.00	249,000	0.40%
120307	4/8/2013	ICD SEC	CD-SAFRA NATION	786580A59	100,000.00		0.00	100,000.00	100,000	0.35%
120411	4/10/2013	ICD SEC	CD-BANK OF INDIA	06782D20	100,000.00		0.00	100,000.00	100,000	0.45%
120427	7/29/2013	ICD SEC	CD-PRIVATEBANK &	742676SB4		100,000.00	0.00	100,000.00	100,000	0.35%
091229A	7/2/2012	ICD SEC	CD-FIRST BANK OF	30387	150,000.00		0.00	150,000.00	150,000	2.15%
070129	1/30/2012	ICD SEC	CD-FIRST NATIONAL	9995998P1	100,000.00		100,000.00	0.00	100,000	5.80%
100602B	7/31/2012	ICD SEC	CD-GATEWAY BAN	35160	100,000.00		0.00	100,000.00	100,000	1.60%
110225	2/25/2013	ICD SEC	CD-GE CAPITAL FIN	36160TQJ8	200,000.00		0.00	200,000.00	200,000	0.95%
110513	8/13/2012	ICD SEC	CD-GE MONEY BAN	36159CA63	200,000.00		0.00	200,000.00	200,000	0.65%
11121A	10/23/2013	ICD SEC	CD-GOLDMAN SACF	38143AEB7	100,000.00		0.00	100,000.00	100,000	1.10%
070702	7/2/2012	ICD SEC	CD-JP Morgan Chase (	32633	100,000.00		0.00	100,000.00	100,000	5.45%
071009	10/9/2012	ICD SEC	CD-KEYBANK NATI	21366	100,000.00		0.00	100,000.00	100,000	5.60%
110629C	4/29/2014	ICD SEC	CD-LAKESIDE BANK	51210SG47	249,000.00		0.00	249,000.00	249,000	1.00%
090122A	1/23/2012	ICD SEC	CD-MACON BANK	31445	100,000.00		100,000.00	0.00	100,000	3.05%
110613	12/13/2013	ICD SEC	CD-MEDALLION BA	58403BUW4	100,000.00		0.00	100,000.00	100,000	1.10%
090423	4/23/2012	ICD SEC	CD-NORTHBROOK B	57082	100,000.00		100,000.00	0.00	100,000	2.60%
071030D	10/22/2012	ICD SEC	CD-NOVA SAVINGS	27148	100,000.00		0.00	100,000.00	100,000	5.55%
100202	2/2/2012	ICD SEC	CD-Oriental B&T (EUI	27150	100,000.00		100,000.00	0.00	100,000	1.75%
080103B	1/3/2012	ICD SEC	CD-PNC Bank (NATIC	6557	100,000.00		100,000.00	0.00	100,000	5.05%
090528F	5/29/2012	ICD SEC	CD-R-G PREMIER BA	23018	250,000.00		0.00	250,000.00	250,000	3.00%
071114	11/13/2012	ICD SEC	CD-SAIGON NATION	57974	100,000.00		0.00	100,000.00	100,000	5.35%
110628	6/28/2012	ICD SEC	CD-STATE BANK OF	33664	100,000.00		0.00	100,000.00	100,000	0.85%
110119	1/20/2012	ICD SEC	CD-STATE BANK OF	33664	150,000.00		150,000.00	0.00	150,000	0.85%
070829	8/30/2012	ICD SEC	CD-TEXAS STATE BANK		100,000.00		0.00	100,000.00	100,000	5.90%
100602A	6/4/2012	ICD SEC	CD-UNION NATION	03661	100,000.00		0.00	100,000.00	100,000	1.50%
110921	3/21/2013	ICD SEC	CD-GOLDMAN SACF	381426SW1	100,000.00		0.00	100,000.00	100,000	0.65%
111005	10/5/2026	ICD SEC	CD-WELLS FARGO E	949748K97	150,000.00		0.00	150,000.00	150,000	3.00%

2012 CASH AND INVESTMENT ACTIVITY

CITY INVEST #	STATED MAT DATE	BROKER	SECURITY DESCRIP	CUSIP	PRIN	PURCH 2012	SOLD/	BV	PAR	YTM
					BAL 1/1/2012		MATURE 2012	PRIN BAL 12/31/2012		
081212I	12/12/2013	ICD SEC	CD-WORLDS FOREM	57079	100,000.00		0.00	100,000.00	100,000	4.95%
090331	4/2/2012	ICD SEC	CD-YADKIN VALLE'	19861	100,000.00		100,000.00	0.00	100,000	2.60%
								6,316,905.75		
090102A	1/25/2033	Com Sec	FNR 2003-19 ME	31392JJG5	64,545.19		5,541.65	59,003.54	165,176	3.95%
090102B	12/25/2018	Com Sec	FNR 2003-120 BY	31393UGR8	191,156.41		16,807.21	174,349.20	200,000	3.98%
080128C	1/15/2038	Com Sec	FHR 3404 JC	31397PTH3	385,807.83		50,360.98	335,446.85	1,000,000	4.74%
080128F	7/6/2012	Com Sec	CD-INDEPENDENT E	45383XBJ7'	100,000.00		0.00	100,000.00	100,000	4.10%
								668,799.59		
081119B	2/1/2012	Northland	ELK RIVER SCHOOL	287425-xm-9	95,000.00		95,000.00	0.00	95,000	5.00%
120214A	9/1/2017	Northland	TAOS NEW MEX SD	876014-FV-8		175,000.00	0.00	175,000.00	175,000	4.63%
120214B	2/1/2012	Northland	ST FRANCIS ISD#15	789466-PU-7		250,000.00	0.00	250,000.00	250,000	4.40%
120215A	2/1/2012	Northland	MINNETONKA ISD #	604195-SB-4		50,000.00	0.00	50,000.00	50,000	5.20%
120215B	2/1/2012	Northland	CEDAR RAPIDS IO T.	150528-JU-2		50,000.00	0.00	50,000.00	50,000	4.55%
120215C	2/1/2012	Northland	WESTERN LAKE SUI	958522-WV-4		100,000.00	0.00	100,000.00	100,000	3.15%
120308A	12/1/2020	Northland	SHOREWOOD WIS T.	825230-KU-8		150,000.00	0.00	150,000.00	150,000	2.50%
120308B	12/1/2022	Northland	SHOREWOOD WIS T.	825230-KW-4		150,000.00	0.00	150,000.00	150,000	2.80%
120308C	2/1/2012	Northland	SHOREWOOD WIS T.	825230-LB-9		225,000.00	0.00	225,000.00	225,000	3.25%
120308D	2/1/2012	Northland	SHOREWOOD WIS T.	825230-LC-7		465,000.00	0.00	465,000.00	465,000	3.15%
120321	3/1/2022	Northland	MADISION WI SCHO	558495-KN-6		500,000.00	0.00	500,000.00	500,000	2.50%
081218A	2/1/2012	Northland	CHASKA MN ISD #11	161681-RG-8	250,000.00		250,000.00	0.00	250,000	4.40%
090827	2/1/2012	Northland	MANKATO MN ISD#	563690-MP-8	505,000.00		505,000.00	0.00	505,000	2.20%
090217A	3/1/2012	Northland	STILLWATER ISC #8	860758-PW-5	315,000.00		315,000.00	0.00	315,000	2.65%
080415	4/1/2012	Northland	BURLINGTON AREA	121493-GT-1	115,000.00		115,000.00	0.00	320,000	4.20%
110324	4/1/2012	Northland	NORTHLAND PINES	666509-GU-2	150,000.00		150,000.00	0.00	150,000	5.20%
110816A	10/1/2016	Northland	WESTIN LAKE SUPE	958522-WR-1	350,000.00		0.00	350,000.00	350,000	2.10%
111031	6/1/2018	Northland	RACINE WIS TAXAB	750021-6D-4	755,000.00		0.00	755,000.00	755,000	2.10%
110816B	10/1/2017	Northland	WESTIN LAKE SUPE	958522-WS-9	350,000.00		0.00	350,000.00	350,000	2.50%
110602	7/5/2014	Northland	ELKHART, IN COMM.	287515-SF-8	250,000.00		0.00	250,000.00	250,000	5.75%
110608	2/1/2019	Northland	WADENA MN BAB	930217-JD-7	95,000.00		0.00	95,000.00	95,000	3.94%
110714B	10/1/2016	Northland	WAUWATO WIS REF	943504-R2-8	300,000.00		0.00	300,000.00	300,000	1.90%
110913A	12/1/2015	Northland	APPLETON MN TAX.	03805A-KR-8	140,000.00		0.00	140,000.00	140,000	1.60%
110913B	12/1/2016	Northland	APPLETON MN TAX.	038051-KS-6	110,000.00		0.00	110,000.00	110,000	2.00%
110715	10/22/2014	Northland	CD-BMW BANK	05568P-YZ-4	147,000.00		0.00	147,000.00	147,000	1.55%
101230B	4/1/2012	Northland	LACROSSE CTY WIS	502606-QS-8	100,000.00		100,000.00	0.00	100,000	1.00%
101223	6/1/2012	Northland	LINN CTY IOWA TA.	535783-FY-5	465,000.00		0.00	465,000.00	465,000	0.85%
080707	12/1/2012	Northland	LACROSSE WI TAX	502768-BB-6	135,000.00		0.00	135,000.00	135,000	5.09%
090630A	12/30/2012	Northland	MCGREGOR ISD #00-	580705-GK-1	100,000.00		0.00	100,000.00	100,000	2.75%
040518	2/1/2013	Northland	FNMA 254663	31371K-Z4-6	8,004.83		3,238.02	4,766.81	272,453	3.77%
090212A	2/1/2013	Northland	ROSEMOUNT ISD#15	777594-WW-8	150,000.00		0.00	150,000.00	150,000	3.00%
090820A	2/1/2013	Northland	LACQUI PARLE VAL	505468-AH-1	115,000.00		0.00	115,000.00	115,000	2.65%
090213B	6/1/2013	Northland	ILLINOIS ST PENSIO	452151-LA-9	345,000.00		0.00	345,000.00	345,000	3.85%
080828B	8/28/2013	Northland	CD-NATIONAL REP I	63736Q-JT-9	97,000.00		0.00	97,000.00	97,000	4.95%
090420A	11/1/2013	Northland	BURLINGTON VT PU	122062-MJ-8	125,000.00		0.00	125,000.00	125,000	3.26%
080731	12/15/2013	Northland	BEAVER CITY PA TA	074851-MQ-6	125,000.00		0.00	125,000.00	125,000	5.00%
100106	12/15/2013	Northland	FOX VALLEY PK DIS	351592-GC-8	400,000.00		0.00	400,000.00	400,000	2.06%
080324	12/30/2013	Northland	WHEATON IL PK TA	96257-PB-0	475,000.00		0.00	475,000.00	475,000	4.15%
090630B	12/30/2013	Northland	MCGREGOR ISD #00-	580705-GM-7	100,000.00		0.00	100,000.00	100,000	3.00%
090212B	2/1/2014	Northland	ROSEMOUNT ISD#15	777594-WX-6	300,000.00		0.00	300,000.00	300,000	3.50%
090715A	2/1/2014	Northland	MOWER CTY MIN JA	624662-AH-5	515,000.00		0.00	515,000.00	515,000	3.80%
090820B	2/1/2014	Northland	LACQUI PARLE VAL	505468-AJ-7	120,000.00		0.00	120,000.00	120,000	3.15%
080425A	3/1/2014	Northland	DAUPHIN COUNTY I	238253-RU-4	150,000.00		0.00	150,000.00	150,000	5.00%
090217B	3/1/2014	Northland	STILLWATER ISC #8	860758-PY-1	200,000.00		0.00	200,000.00	200,000	3.40%
100223B	4/1/2014	Northland	WINNEBAGO CITY V	974603-MZ-2	200,000.00		0.00	200,000.00	200,000	2.10%
080507	5/1/2014	Northland	FREEMONT NE ELEC	356730-T7-6	110,000.00		0.00	110,000.00	110,000	4.50%
090528B	5/28/2014	Northland	CD-SUNTRUST BAN	86789V-HM-2	100,000.00		0.00	100,000.00	100,000	4.00%
091103	6/1/2014	Northland	GRIMES IOWA BAB	398526-FV-0	120,000.00		0.00	120,000.00	120,000	3.25%
110201A	6/1/2014	Northland	WINDSOR HTS IOWA	973602-KR-5	255,000.00		0.00	255,000.00	255,000	1.35%
110301B	6/1/2014	Northland	DES MOINES IA COM	250097-YR-7	265,000.00		0.00	265,000.00	265,000	2.00%
110420	10/20/2017	Northland	FMN,A	3136FR-EV-1	250,000.00		250,000.00	0.00	250,000	3.00%
090630C	12/30/2014	Northland	MCGREGOR ISD #00-	580705-GL-9	105,000.00		0.00	105,000.00	105,000	3.40%
090206	2/1/2015	Northland	NORTH ST PAUL MA	6621406D9	355,000.00		0.00	355,000.00	355,000	3.70%
090528A	2/1/2015	Northland	WAYZATA ISD #284	946813-TF-9	500,000.00		0.00	500,000.00	500,000	3.15%
090715B	2/1/2015	Northland	MOWER CTY MIN JA	624662-AJ-1	250,000.00		0.00	250,000.00	250,000	4.10%
090914	2/1/2015	Northland	GRAND RAPIDS MN	386334-2L-9	115,000.00		0.00	115,000.00	115,000	3.70%
100803A	2/1/2015	Northland	NEW PRAGUE BAB	648159-TU-5	60,000.00		0.00	60,000.00	60,000	2.65%
090310	4/1/2015	Northland	WEST ALLIS WIS CO	951172-7R-0	340,000.00		0.00	340,000.00	340,000	3.75%
081106	6/1/2015	Northland	KIRKWOOD COMM (	497595-VC-9	245,000.00		0.00	245,000.00	245,000	5.50%
110301A	6/1/2015	Northland	DES MOINES IA COM	250097-YS-5	100,000.00		0.00	100,000.00	100,000	2.50%
090420B	11/1/2015	Northland	BURLINGTON VT PU	022062-ML-3	200,000.00		0.00	200,000.00	200,000	3.75%
080502	12/15/2015	Northland	ROCKFORD ILL TAX	77316Q-B4-4	205,000.00		0.00	205,000.00	20,500	5.13%
110208	12/30/2015	Northland	MCGREGOR ISD #00-	580705-GN-5	95,000.00		0.00	95,000.00	95,000	2.35%

2012 CASH AND INVESTMENT ACTIVITY

CITY INVEST #	STATED MAT DATE	BROKER	SECURITY DESCRIP	CUSIP	PRIN	PURCH 2012	SOLD/	BV	PAR	YTM
					BAL 1/1/2012		MATURE 2012	PRIN BAL 12/31/2012		
090217C	3/1/2016	Northland	STILLWATER ISC #8	860758-QA-2	245,000.00		0.00	245,000.00	245,000	4.20%
110114B	3/1/2016	Northland	APPLETON WIS SCH	038106-JN-1	100,000.00		0.00	100,000.00	100,000	2.77%
090706	4/1/2016	Northland	SHEBOYGAN WIS SC	821023-GU-6	140,000.00		140,000.00	0.00	140,000	6.25%
100223A	6/1/2016	Northland	DAVENPORT IOWA	238388-FU-1	335,000.00		0.00	335,000.00	335,000	3.25%
110201B	6/1/2016	Northland	WINDSOR HTS IOWA	973602-KT-1	130,000.00		0.00	130,000.00	130,000	2.30%
110114A	2/1/2017	Northland	HOPKINS ISD #270	439881-HB-2	100,000.00		0.00	100,000.00	100,000	2.75%
110203A	4/1/2017	Northland	MEDFORD WIS SCH	58434T-DK-3	40,000.00		0.00	40,000.00	40,000	2.75%
110106	6/1/2017	Northland	CHARLES CTY MD	159807-C3-8	105,000.00		0.00	105,000.00	105,000	4.00%
100803B	2/1/2018	Northland	NEW PRAGUE BAB	648159-TX-9	70,000.00		0.00	70,000.00	70,000	3.75%
090203B	12/15/2018	Northland	FHLMC REMIC	31397B-MQ-1	62,430.47		16,507.49	45,922.98	339,828	5.75%
100803C	2/1/2019	Northland	NEW PRAGUE BAB	648159-TY-7	45,000.00		0.00	45,000.00	45,000	4.00%
110310	2/1/2020	Northland	BROOKLYN CENTEF	113853-KG-9	285,000.00		0.00	285,000.00	285,000	4.65%
100803D	2/1/2020	Northland	NEW PRAGUE BAB	648159-TZ-4	70,000.00		0.00	70,000.00	70,000	4.25%
								<u>13,724,689.79</u>		
061017	1/23/2012	VILLAGE	CD-VILLAGE BANK	41117	523,905.46		523,905.46	0.00	500,000	4.70%
091002	6/2/2011	VILLAGE	CD-VILLAGE BANK	41031	1,036,968.35		0.00	1,036,968.35	1,000,000	
070322	1/30/2012	VILLAGE	CD-VILLAGE BANK	41123	558,356.00		558,356.00	0.00	500,000	4.70%
								<u>1,036,968.35</u>		
971212	11/28/2002	UBS	CD-FNB KEYSTONE,	320950AJ7R	96,000.00		0.00	96,000.00	96,000	6.25%
120228	2/28/2017	UBS	FHLMC	313463NA4		400,000.00	0.00	400,000.00	400,000	1.31%
120229	2/28/2024	UBS	FHLB	313378YK4		1,000,000.00	0.00	1,000,000.00	1,000,000	2.04%
120322	3/22/2027	UBS	FNMA	3136FT6A2		500,000.00	0.00	500,000.00	500,000	1.49%
120323	2/16/2024	UBS	FHLB	313376XL4		500,000.00	0.00	500,000.00	500,000	1.13%
120328	3/28/2022	UBS	FHLMC	313378HH7		500,000.00	0.00	500,000.00	500,000	2.13%
120329A	3/29/2027	UBS	FHLB	3136FT7E3		800,000.00	0.00	800,000.00	800,000	2.25%
120329B	3/29/2027	UBS	FNMA	313378YK4		500,000.00	0.00	500,000.00	500,000	2.00%
101228	4/1/2013	UBS	MPLS SCHOOL DIST	603792PR7	615,000.00		0.00	615,000.00	615,000	1.70%
100824	6/1/2015	UBS	ILLINOIS STATE TA	452151LC5	500,000.00		0.00	500,000.00	500,000	3.82%
030430B	4/25/2018	UBS	FNR 2003-41-JH	31393BD36C	25,851.96		5,167.91	20,684.05	300,000.00	5.00%
030630A	6/15/2018	UBS	FHR 2628 AB	31393VMQ1C	47,358.32		6,966.15	40,392.17	200,000	3.12%
030730A	8/25/2018	UBS	FNR 2003-74-KN	31393EAL3C	90,507.28		13,732.40	76,774.88	300,000.00	3.59%
030930A	9/15/2018	UBS	FHR 2677 KH	31394JTP2	83,349.26		13,366.22	69,983.04	300,000	4.50%
031030A	11/25/2018	UBS	FNR 2003-113KA	31393T2P0	37,872.06		12,082.60	25,789.46	197,000	4.50%
040430	4/25/2019	UBS	FNR-2004-31-DA	31393YAJ4	91,104.15		15,658.79	75,445.36	500,000	4.50%
040730A	7/15/2019	UBS	FHR 2822 DB	31395C3S8	152,174.51		25,848.22	126,326.29	500,000	5.00%
040730B	7/15/2019	UBS	FHR 2822 DQ	31395C3U3	65,233.20		19,720.60	45,512.60	500,000	5.00%
040830	8/25/2019	UBS	FNR 2004 68 BT	31394AYU4	35,572.77		13,799.34	21,773.43	500,000	
970625	2/25/2021	UBS	FNMA FNR-1991-7 H	31358FZW2	3,000.00		0.00	3,000.00	129,000	7.84%
000417	8/15/2021	UBS	FHLMC REMIC 181e	312904AU9C	2,886.47		211.01	2,675.46	88,604	7.00%
970917B	9/15/2021	UBS	FHLMC REMIC SERII	312904GT6C	5,014.03		622.49	4,391.54	255,714	7.21%
001127	7/25/2022	UBS	FNR G92-35	31358PHV2C	3,205.62		166.99	3,038.63	75,215	7.49%
0210004	8/25/2022	UBS	FNR 1992-125L	31358PS40C	3,577.17		218.14	3,359.03	52,000	7.00%
110803B	9/28/2020	UBS	FNMA	3136FPLW5	712,000.00		0.00	712,000.00	712,000	1.00%
110916	9/16/2026	UBS	FHLB	31337FGD3	1,000,000.00		0.00	1,000,000.00	1,000,000	1.00%
110810	8/10/2026	UBS	FNMA	3136FRF24	1,000,000.00		1,000,000.00	0.00	1,000,000	1.00%
110719	7/19/2016	UBS	FNMA	3136FRYJ6	750,000.00		0.00	750,000.00	750,000	1.25%
110311	7/1/2012	UBS	ILLINOIS BAB	452152GL9	1,000,000.00		0.00	1,000,000.00	1,000,000	3.08%
110329	3/29/2021	UBS	FNMA	3136FRDU4	500,000.00		500,000.00	0.00	500,000	1.39%
020826	10/15/2022	UBS	FHR 1391D	312912LUO	2,978.74		164.05	2,814.69	59,990	6.00%
010328	6/25/2023	UBS	FHG14A	312916PD5R	10,000.00		0.00	10,000.00	152,000	6.00%
020816B	1/25/2024	UBS	FNR G94-2D	31359GR40	957.35		254.07	703.28	46,221	6.45%
								<u>9,405,663.91</u>		
Money Mkt							0.00	0.00		
TOTAL INVESTMENTS					42,230,022.32	9,463,000.00	17,222,695.79	34,470,326.53		
Unamortized Premiums					1,125,462.94	91,876.75		1,217,339.69		
Unamortized Discounts					(2,844,409.22)	-7,896.10		-2,852,305.32		
Village Bank Checking					1,788,349.25	22,400,667.87	22,821,589.46	1,367,427.66		
Money Market Accounts					3,649,877.44	9,003,409.58	5,000,000.00	7,653,287.02		
Net Cash and Investments					45,949,302.73	40,951,058.10	45,044,285.25	41,856,075.58		



CC Regular Session

4. 2.

Meeting Date: 06/12/2012

By: Diana Lund, Finance

Information

Title:

Receive 2011 Comprehensive Annual Financial Report (CAFR)

Background:

The City of Ramsey is required to undergo a certified audit, on an annual basis, of all of its funds and account groups. This audit is performed by the firm of Malloy, Montague, Karnowski, Radosevich & Co., P.A.

Incorporated into the 2011 CAFR are changes that are consistent with recommendations from the review for the "Certificate of Achievement for Excellence in Financial Reporting: (Certificate) and any reporting changes required by the Governmental Accounting Standards Board (GASB). It is our belief that this report is again a good candidate for receipt of the Certificate and it is intended that we submit the 2011 CAFR for review and consideration.

The auditors, as a part of their work, prepare a Management Report for the City. This report contains information on the following:

1. Audit Summary
2. Funding Cities in Minnesota
3. Governmental Fund Overview
4. Financial Trends and Conditions of Your City
5. Accounting and Auditing Updates

A presentation from a representative with the firm of Malloy, Montague, Karnowski, Radosevich will review the auditors opinions contained in the CAFR, the Management Report and answer any questions that Council may have. This presentation will be scheduled prior to the regular City Council meeting to account for time and a brief summary will be presented at the beginning the regular City Council meeting.

Notification:

The 2011 CAFR and Management Report were included as attachments with the worksession case entitled "2011 CAFR and Management Report".

Funding Source:

The 2011 annual audit was included in the 2012 General Fund Finance budget.

Council Action:

Motion to recommend Council accept the 2011 Comprehensive Annual Financial Report and authorize the Finance Director to submit to the State of Minnesota Department of Revenue and other agencies, as required, and to submit the report with application for the Certificate to the Government Finance Officers Association of the United States and Canada.

Form Review

Inbox

Reviewed By

Date

Kurt Ulrich

Kurt Ulrich

06/07/2012 02:49 PM

Form Started By: Diana Lund

Started On: 06/05/2012 03:49 PM

Final Approval Date: 06/07/2012

CC Regular Session

4.3.

Meeting Date: 06/12/2012

By: Diana Lund, Finance

Information

Title:

Receive April 2012 Financial Reports - General Fund and Enterprise Funds

Background:

Brief summary of actual revenues and expenditures to date in comparison to adopted budget for the funds of: General, Water, Sewer, Street Lighting, Recycling and Storm Drainage.

Council Action:

No Action Required. Informational only.

Attachments

April 2012 General Fund Financial Report - Budget to Actual

April 2012 Enterprise Funds Financial Reports - Budget to Actual

Form Review

Inbox

Kurt Ulrich

Reviewed By

Kurt Ulrich

Date

06/07/2012 03:02 PM

Form Started By: Diana Lund

Started On: 06/06/2012 07:46 AM

Final Approval Date: 06/07/2012

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD April 30, 2012

GENERAL FUND EXPENDITURES - BY DEPARTMENT -		
Dept	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Admin	1,423,788.00	437,870.83
Com Dev	528,852.00	159,761.85
Contingency	230,648.00	
Council	133,951.00	45,001.35
Finance	454,044.00	115,282.23
Fire	872,656.00	186,351.85
Legal	121,000.00	32,176.53
Police	3,121,261.00	871,979.67
Public Works	2,761,875.00	665,692.69
Grand Total	9,648,075.00	2,514,117.00

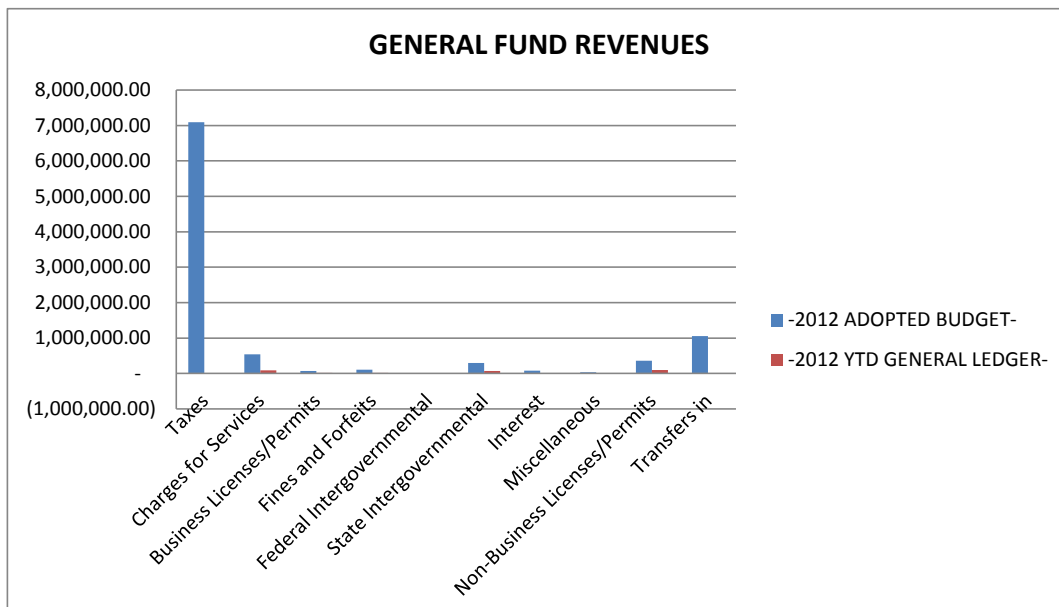
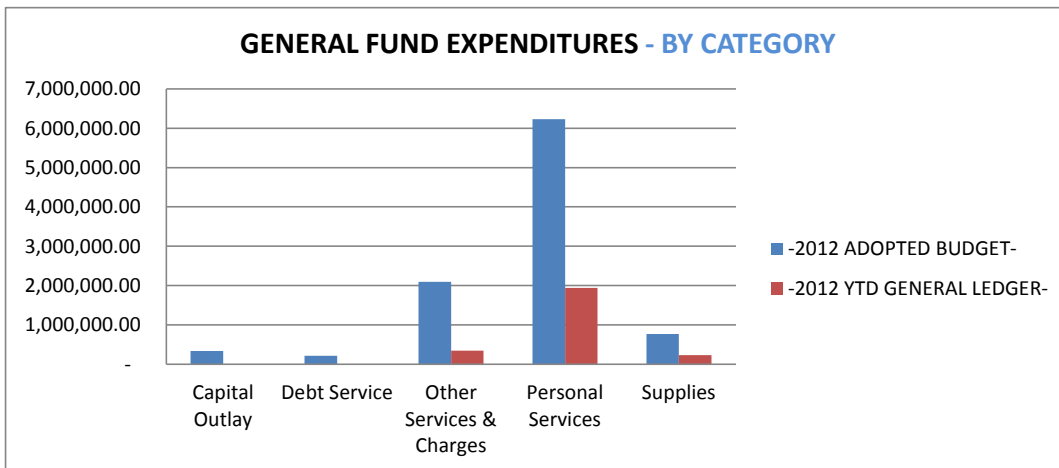
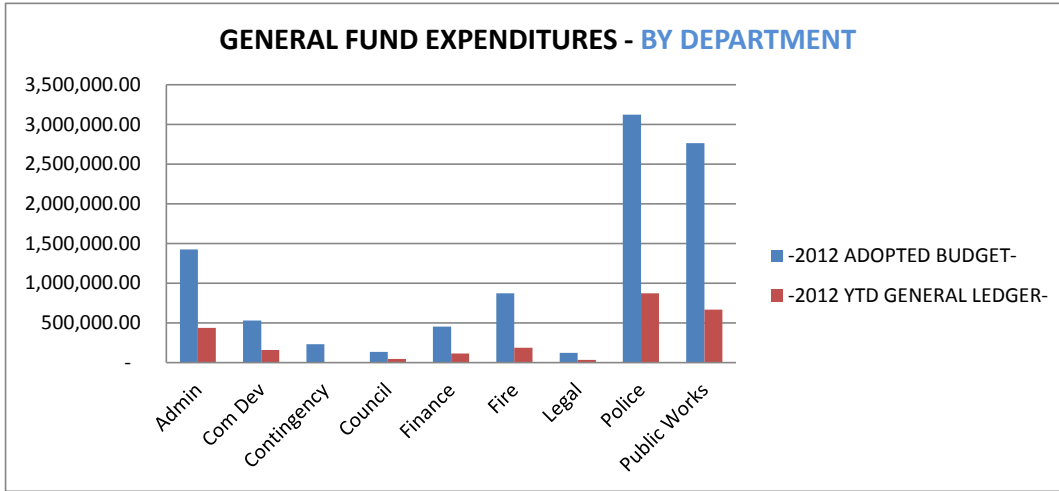
GENERAL FUND EXPENDITURES - BY CATEGORY -		
Category	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Capital Outlay	340,252.00	-
Debt Service	213,113.00	
Other Services & Charges	2,094,613.00	344,575.22
Personal Services	6,232,372.00	1,939,589.06
Supplies	767,725.00	229,952.72
Grand Total	9,648,075.00	2,514,117.00

GENERAL FUND REVENUES - BY CATEGORY -		
Category	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Taxes	7,090,150.00	-
Charges for Services	543,189.00	87,373.03
Business Licenses/Permits	72,020.00	16,054.52
Fines and Forfeits	108,000.00	19,557.29
Federal Intergovernmental	7,000.00	(7,485.38)
State Intergovernmental	298,300.00	69,856.67
Interest	80,000.00	-
Miscellaneous	30,500.00	5,893.78
Non-Business Licenses/Permits	364,300.00	99,305.20
Transfers in	1,054,616.00	-
Grand Total	9,648,075.00	290,555.11

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD April 30, 2012



**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: April 30, 2012

REVENUES		
BUSINESS UNIT	9601	WATER UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
4140 CREDIT CARD PROCESSING FEES		1,099.05
4273 OTHER STATE GRANTS & AIDS	-	7,074.00
4609 OTHER MISCELLANEOUS REVENUES	61,853.00	2,570.05
4651 WATER REVENUE	-	175.09
4652 WATER SALES - RESIDENTIAL	963,401.00	145,895.65
4653 WATER SALES-COMMERCIAL	676,702.00	83,639.98
4654 WATER PENALTIES	32,802.00	5,102.68
4655 WATER METER INSTALLATION	5,000.00	1,820.00
4656 WATER METERS	14,000.00	3,575.00
4657 CONNECTION/RECONNECTION FEES	4,000.00	1,000.00
4701 INTEREST ON INVESTMENTS	150,000.00	-
4506 PREPAID INTEREST	-	763.18
Grand Total	1,907,758.00	252,714.68

EXPENSES		
BUSINESS UNIT	9601	WATER UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
6102 F.T. REGULAR-WAGES & SALARIES	204,354.00	46,477.28
6103 FULL TIME-REGULAR-OVERTIME	12,500.00	4,568.61
6105 TEMPORARY-WAGES & SALARIES	17,000.00	1,056.00
6121 PERA CONTRIBUTIONS	15,722.00	4,194.21
6122 FICA/MEDICARE CONTRIBUTIONS	17,890.00	4,443.08
6131 GROUP INSURANCE	20,470.00	6,782.80
6133 WORKERS COMP INSURANCE PREMIUM	7,436.00	
6208 MISCELLANEOUS OFFICE SUPPLIES	1,000.00	
6223 GASOLINE	5,000.00	1,250.96
6225 DIESEL FUEL	7,000.00	1,446.30
6227 LUBRICANTS & ADDITIVES		-
6229 SHOP MATERIALS	750.00	47.27
6231 UNIFORMS & TURN-OUT GEAR	1,500.00	339.95
6249 MISCELLANEOUS OPERATING SUPPLY	13,000.00	4,603.96
6257 OTHER VEHICLE PARTS	2,500.00	583.44
6273 UTILITY SYSTEM MAINT SUPPLIES	70,000.00	13,527.62
6281 SMALL TOOLS & MINOR EQUIPMENT	110,000.00	
6292 WATER METERS FOR RESALE	20,000.00	
6315 MISCELLANEOUS PROFESSIONAL SER	54,330.00	4,752.32
6322 POSTAGE	2,000.00	157.44
6323 CELLULAR PHONES	2,400.00	564.42
6335 TRAINING	1,600.00	907.72
6352 GENERAL NOTICE & PUBLIC INFOR	600.00	
6361 GENERAL LIABILITY/PROPERTY INS	24,000.00	
6371 ELECTRIC UTILITIES	125,000.00	12,007.10
6372 WATER/IRRIGATION	800.00	
6373 GAS	4,000.00	698.33
6374 REFUSE/RECYCLING	600.00	106.33
6381 BUILDING & STRUCTURE REPAIR	500.00	
6439 OTHER MISCELLANEOUS	17,000.00	10,231.00
6451 MEMBERSHIP DUES	800.00	1,089.50
6489 OTHER CONTRACTED SERVICES	27,000.00	5,256.38
6722 DEPRECIATION	623,308.00	
6820 OPERATING TRANSFERS TO OTHER F	34,000.00	
Grand Total	1,444,060.00	125,092.02

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: April 30, 2012

REVENUES			
BUSINESS UNIT	9602	SEWER UTILITY	
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER	
4140 CREDIT CARD PROCESSING FEES			1,017.49
4356 SEWER AVAILABILITY CHARGE-ADM	1,000.00		236.50
4609 OTHER MISCELLANEOUS REVENUES	18,546.00		-
4661 RESIDENTIAL-SEWER CHARGES	925,057.00		235,847.40
4662 COMMERCIAL-SEWER CHARGES	311,381.00		77,607.78
4663 SEWER PENALTIES	24,729.00		7,223.19
4701 INTEREST ON INVESTMENTS	50,000.00		-
Grand Total	1,330,713.00		321,932.36

EXPENSES			
BUSINESS UNIT	9602	SEWER UTILITY	
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER	
6102 F.T. REGULAR-WAGES & SALARIES	100,563.00		6,197.75
6103 FULL TIME-REGULAR-OVERTIME	2,000.00		156.95
6105 TEMPORARY-WAGES & SALARIES	5,500.00		-
6121 PERA CONTRIBUTIONS	7,436.00		496.70
6122 FICA/MEDICARE CONTRIBUTIONS	8,267.00		504.37
6133 WORKERS COMP INSURANCE PREMIUM	2,798.00		
6225 DIESEL FUEL	2,500.00		
6249 MISCELLANEOUS OPERATING SUPPLY	10,000.00		276.87
6275 OTHER EQUIPMENT PARTS	2,500.00		
6315 MISCELLANEOUS PROFESSIONAL SER	16,000.00		16,985.00
6335 TRAINING	1,500.00		646.00
6361 GENERAL LIABILITY/PROPERTY INS	9,500.00		
6371 ELECTRIC UTILITIES	9,500.00		1,803.16
6373 GAS	2,400.00		491.21
6374 REFUSE/RECYCLING	500.00		106.33
6377 SEWER SERVICE CHARGE	541,073.00		225,447.10
6489 OTHER CONTRACTED SERVICES	22,000.00		6,487.01
6722 DEPRECIATION	497,434.00		
6820 OPERATING TRANSFERS TO OTHER F	28,000.00		
Grand Total	1,269,471.00		259,598.45

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: April 30, 2012

REVENUES		
BUSINESS UNIT	9603	STREET LIGHT UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
4140 CREDIT CARD PROCESSING FEES		156.36
4681 CHARGES FOR STREET LIGHTS	168,312.00	32,212.56
4682 ST LIGHT O/M CHARGE	360.00	-
4683 STREET LIGHTING PENALTIES	3,366.00	1,191.91
4701 INTEREST ON INVESTMENTS	20,000.00	-
4684 PRIORITY STREET LIGHT	-	11,130.15
Grand Total	192,038.00	44,690.98

EXPENSES		
BUSINESS UNIT	9603	STREET LIGHT UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
6271 SIGN REPAIR MATERIALS	-	
6371 ELECTRIC UTILITIES	122,000.00	30,675.91
6489 OTHER CONTRACTED SERVICES	12,328.00	2,666.97
6722 DEPRECIATION	33,997.00	
6820 OPERATING TRANSFERS TO OTHER F	14,000.00	
Grand Total	182,325.00	33,342.88

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: April 30, 2012

REVENUES		
BUSINESS UNIT	9604	RECYCLING UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
4140 CREDIT CARD PROCESSING FEES		187.44
4287 OTHER LOCAL GOVERNMENT GRANTS	50,165.00	-
4609 OTHER MISCELLANEOUS REVENUES	-	573.65
4671 RECYCLING CHARGES	280,000.00	71,944.69
4672 RECYCLING PENALTIES	5,600.00	1,783.53
4701 INTEREST ON INVESTMENTS	200.00	-
Grand Total	335,965.00	74,489.31

EXPENSES		
BUSINESS UNIT	9604	RECYCLING UTILITY
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER
6102 F.T. REGULAR-WAGES & SALARIES	5,143.00	1,565.76
6103 FULL TIME-REGULAR-OVERTIME		-
6121 PERA CONTRIBUTIONS	373.00	120.06
6122 FICA/MEDICARE CONTRIBUTIONS	393.00	99.80
6133 WORKERS COMP INSURANCE PREMIUM	87.00	
6249 MISCELLANEOUS OPERATING SUPPLY	7,000.00	1,309.80
6322 POSTAGE	250.00	42.55
6489 OTHER CONTRACTED SERVICES	299,000.00	96,471.01
6820 OPERATING TRANSFERS TO OTHER F	8,500.00	
Grand Total	320,746.00	99,608.98

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: April 30, 2012

REVENUES			
BUSINESS UNIT	9605	STORM WATER UTILITY	
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER	
4140 CREDIT CARD PROCESSING FEES			245.63
4693 STORM WATER-RESIDENTIAL	305,790.00		78,068.04
4694 STORM WATER-COMMERCIAL	302,629.00		80,286.40
4695 STORM WATER-PENALTIES	12,168.00		3,378.77
4701 INTEREST ON INVESTMENTS	5,000.00		-
Grand Total	625,587.00		161,978.84

EXPENSES			
BUSINESS UNIT	9605	STORM WATER UTILITY	
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER	
6102 F.T. REGULAR-WAGES & SALARIES	86,991.00		6,896.08
6105 TEMPORARY-WAGES & SALARIES			-
6121 PERA CONTRIBUTIONS	6,307.00		499.98
6122 FICA/MEDICARE CONTRIBUTIONS	6,655.00		513.54
6133 WORKERS COMP INSURANCE PREMIUM	2,894.00		
6223 GASOLINE	2,000.00		1,104.18
6225 DIESEL FUEL	300.00		528.24
6229 SHOP MATERIALS			32.70
6249 MISCELLANEOUS OPERATING SUPPLY	13,000.00		1,127.92
6257 OTHER VEHICLE PARTS	7,000.00		1,327.89
6315 MISCELLANEOUS PROFESSIONAL SER	42,000.00		12,700.00
6361 GENERAL LIABILITY/PROPERTY INS	5,000.00		
6371 ELECTRIC UTILITIES	2,420.00		549.66
6373 GAS	2,500.00		491.21
6374 REFUSE/RECYCLING	500.00		106.30
6375 SEWER			9,311.38
6451 MEMBERSHIP DUES	39,162.00		39,162.00
6489 OTHER CONTRACTED SERVICES	15,200.00		2,666.97
6722 DEPRECIATION	235,517.00		
6820 OPERATING TRANSFERS TO OTHER F	23,000.00		
Grand Total	490,446.00		77,018.05

CC Regular Session

4. 4.

Meeting Date: 06/12/2012

By: Jo Thieling, Administrative Services

Information

Title:

Approve License Applications

Background:

Attached is a list of licenses for approval.

Council Action:

Motion to approve license applications.

Attachments

License Requests

Form Review

Inbox

Kurt Ulrich

Form Started By: Jo Thieling

Reviewed By

Kurt Ulrich

Date

06/07/2012 02:46 PM

Started On: 06/05/2012 01:25 PM

Final Approval Date: 06/07/2012

License Applications
06/12/12

*Special Events	Cynthia Warneke for event at Central Park	7925 – 161 st Avenue NW	Ramsey, MN 55303
**Special Events	CenturyLink	6651 – 141 st Avenue NW – Flr 2	Ramsey, MN 55303
***Special Events	Minnesota State Society DAR	27157 Dakota Avenue	Elko, MN 55020
****Peddler	Christopher Bearup	16315 – 230 th Avenue NW	Elk River, MN 55330

*Ms. Warneke, resident of Ramsey has requested the use of Central Park for a casual picnic for approximately 200 people – half of which will be young children - to celebrate a destination wedding that took place earlier in the year. The guests consist mostly of family members. Picnic food and canned beer will be served contingent upon Council approving the request for alcohol in the parks. Ms. Warneke has submitted the proper paperwork and the \$50 application fee as well as the paperwork for park and facility usage. There will be no music and Ms. Warneke has stated the schedule of the event as follows: noon to 2:00 p.m. – set up; 2:00 – 8:00 – reception; 8:00 – 10:00 p.m. – clean up. Parking is on-site.

**CenturyLink has applied for a Special Events Permit to host picnics in Sunfish Lake Park, Central Park, and River’s Bend Park and- to invite the neighbors for food and to view company products and services. The time scheduled is 4:00 p.m. to 8:00 p.m. The \$50 application fee was paid.

***Lori Clausen has properly applied for a Special Events Permit to conduct a 5K Run/Walk fundraiser to benefit the Fisher House. The event will take place on Saturday, June 16 from 7:00 a.m. to 11:00 a.m. – beginning and ending in the VA Clinic parking lot. Ms. Clausen has met with and received instruction from the Police Department.

****Mr. Bearup has applied for a Peddler permit to go door to door selling educational books to families as a summer internship program for college. Mr. Bearup has paid the \$135 fee and has passed his background check.

CC Regular Session

4. 5.

Meeting Date: 06/12/2012

By: Jo Thieling, Administrative Services

Information

Title:

Approve the Following Council Meeting Minutes:

1. City Council Work Session - March 13, 2012
2. City Council Special - March 20, 2012
3. City Council Work Session - March 27, 2012
4. City Council Work Session - April 10, 2012
5. City Council Work Session - April 24, 2012
6. City Council Work Session - May 1, 2012
7. City Council Work Session - May 8, 2012

Background:

Meeting minutes are attached for Council approval.

Council Action:

Motion to approve the following Council Meeting Minutes:

1. City Council Work Session - March 13, 2012
2. City Council Special - March 20, 2012
3. City Council Work Session - March 27, 2012
4. City Council Work Session - April 10, 2012
5. City Council Work Session - April 24, 2012
6. City Council Work Session - May 1, 2012
7. City Council Work Session - May 8, 2012

Attachments

[031312CCWS Minutes](#)

[032012CCSp Minutes](#)

[032712CCWS Minutes](#)

[041012CCWS Minutes](#)

[042412CCWS Minutes](#)

[050112CCWS Minutes](#)

[050812CCWS Minutes](#)

Form Review

Inbox

Kurt Ulrich

Reviewed By

Jo Thieling

Date

06/06/2012 09:30 AM

Form Started By: Jo Thieling

Started On: 06/06/2012 09:16 AM

Final Approval Date: 06/06/2012

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, March 13, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Public Works Director Brian Olson
City Engineer Tim Himmer
Fire Chief Dean Kapler
Economic Development/Marketing Manager Aaron Backman
Senior Planner Tim Gladhill
Planning Intern Patrick Brama

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 6:04 p.m.

2. TOPICS FOR DISCUSSION

2.01: Review Development Proposal from Podawiltz Development for Town Center Gardens 3rd Addition

Senior Planner Gladhill reviewed the staff report and requested feedback on submittal of Livable Communities Act (LCA) application, establishment of TIF District, providing TIF assistance, donation of the HRA parcel, waiving application and license fees, and whether to fast track the approval process.

Mike Podawiltz, Podawiltz Development Corporation, introduced himself and provided a PowerPoint presentation on the proposed workforce townhomes that can be funded through Section 42 of the IRS Tax Code to provide investors to participate. People would be eligible if paying over 30% of their income for housing (rent, utilities, and taxes). This allows communities, with the help of Minnesota Housing Finance, to provide this type of housing, employees for entry level jobs, and create more disposable income by reducing rents. Mr. Podawiltz stated they have found a parcel, identified by the bank, that includes excess right-of-way owned by the City. On this bank-owned property, along Bunker Lake Boulevard, they

would like to develop 50 units of workforce housing. He displayed and described the project site plan, landscaping plan, and indicated the project would extend 147th Avenue to Center Drive to provide another means of egress. This road extension is estimated at \$175,000 at today's market. Mr. Podawiltz stated they propose three-story, two- and three-bedroom units, with drive under double garages, and double balconies. The project will include an on-site community building, on site maintenance, and be constructed of maintenance-free exterior and premium interior products which will minimize maintenance and enhance the tenant's living environment. Mr. Podawiltz presented green building criteria incorporated into this project and displayed pictures of a similar development constructed in 2010 in Buffalo, Minnesota. He then presented statistics related to the need for affordable workforce housing as estimated by the Metropolitan Council, noting 669 such units are needed in Ramsey. Additional information included a comparative tenant profile, rent/income requirements, and target tenant incomes. This project would include 22 two-bedroom and 28 three-bedroom units. Mr. Podawiltz requested the Council consider a resolution supporting the application, zoning and land use modifications required to facilitate the development, and a local contribution with a present value of approximately \$215,000, which can include regulatory cost avoidance and/or regulatory cost reductions, and the vacation of 147th Circle NW. With the donation of the right-of-way vacation, the City's contribution can be reduced to \$197,000. Mr. Podawiltz explained they want to be sure the community is receptive to workforce housing and if the community provides a contribution, more selection points are awarded in this competitive review process. The City's contribution and giving of right-of-way would create two selection points. He explained that last year, eight projects were funded and asked if the Council would be receptive to this level of donation and if it felt there was a need for workforce housing in Ramsey.

Councilmember Elvig asked what they are bringing for equity.

Mr. Podawiltz stated they would buy the 6.5 acres from the bank at \$725,000 or \$119,000/acre and construct the project. He presented the proposed local contribution spread sheet and indicated the new transit facility is worth three selection points and the City's aggregate need is worth 5 selection points so this project would rank highly; however, without a local contribution granting is unlikely. Mr. Podawiltz explained that ultimately the combination of local contribution, tax credits, and mortgage go to lower rents for 50 workforce households for a 30-year period, at a minimum. Benefits to the City include creation of real estate taxes, extension of 147th Lane NW to Center Street, opening an abutting property for development, and donation of the right-of-way facilitates resulting from the expansion of 147th Lane NW.

Mayor Ramsey asked about the impact to future grant opportunities for other projects.

Senior Planner Gladhill explained that in terms of the LCA program, the City had submitted three Transit Oriented Development (TOD) grants, so any affordable units added would bolster that argument and future grant applications.

Councilmember Strommen thanked Mr. Podawiltz for the presentation, spread sheet, and explanation. She noted the proposal is for the City to provide two parcels of land, reducing the requested local contribution, and asked how that would be funded. Councilmember Strommen stated this could be a good project but she has questions from a higher level policy perspective.

She noted the Council has decided to provide funding assistance to apartments within The COR and wanted to put workforce housing on the table. From a policy level, she noted the Council is reacting to what is coming before it but has not yet identified the elements in which it wants to invest its limited resources. She preferred to set goals and priorities in advance so when a project is presented, it can be examined by those goals and priorities. Councilmember Strommen noted this may be a good solution for a piece of land that is difficult to develop but she still has some questions.

Councilmember McGlone explained that when the Council had voted on buying the Town Center/COR land, it had discussions and agreed one of the things it wanted to do was bring The COR back to the downtown and react to the market in the outer areas. He felt the Council did address that issue and he found this project fits those criteria. Councilmember McGlone noted this is private property, not City property.

Senior Planner Gladhill explained that TIF was part of the original request but staff knew it had to look at other ideas, such as land contribution. That has been staff's focus, land contributions and then to identify the gap and go after grant opportunities if the timing works.

Councilmember Backous agreed this is a difficult site to develop and the City had previously tried to come up with other solutions. He felt the proposal for workforce housing is a great solution. Councilmember Backous stated he does not usually support subsidizing projects to attract development but thinks this is reasonable contribution to give up right-of-way and cul-de-sac. He asked about the formula for park dedication and park and trail fees.

Senior Planner Gladhill advised those fees have already been paid and the zoning needed is a replat to this style of housing with a slight reduction in units over the previously approved project. He indicated staff could research whether there could be credits owed to the original developer and bank.

Councilmember Backous stated he does not normally favor giving credits for park and trail but noted there is a nice park proposed at the east end of the project. He supported the project.

Councilmember Elvig stated he likes the project concept, noting the City subsidized two County ACCAP projects of about \$300,000.

City Administrator Ulrich indicated those projects used the County HRA portion of the levy.

Councilmember Elvig stated he preferred that private individuals meet that market instead of public entities but believed there was a need for workforce housing and affordability. He stated he supported providing that opportunity to Ramsey's residents. Councilmember Elvig stated he is concerned about the actual platting and architectural dynamics to avoid appearance of row houses. He preferred undulation to the buildings to create setback from one building to another through open areas, landscaping, and color.

Mr. Podawiltz stated they can add articulation on the back half of the circle and on the front half of the circle they would look to the Planning Commission and Council to help because there is a

large berm and limited side yard. He explained they propose 3-story units to reduce the footprint and would stand ready to add articulation to the extent they can do so within the confines of the City's ordinance.

Councilmember Elvig asked about the building materials.

Mr. Podawiltz stated they would have maintenance-free exteriors and energy efficient products to reduce costs and maintenance. Exterior building materials include metal, steel, stucco board, and balconies on both sides to bring light into the living space.

Councilmember Elvig asked about financing and indicated the City typically sees a performa when asked for a subsidy. He stated he prefers the project involve an institution for overall tax credits instead of selling it off to an individual and he likes it being paid down below market rate to create affordability.

Mr. Podawiltz explained the investors would be limited partners so all they could lose is the investment they put in. He indicated Podawiltz Development Corporation would operate the project, maintain it, and keep it fully occupied. Mr. Podawiltz described their tenant selection criteria. He noted that without the City's \$215,000 contribution, rents would be above the 30% and 50% threshold so one of the things the City's contribution does is write down rents and shows Minnesota Housing Finance that the City is interested in the project. The City's contribution also allows Minnesota Housing Finance to use money for other projects.

Mayor Ramsey asked if there are any objections to this project.

Councilmember Tossey stated he has no problem with vacating the street or HRA parcel but did not support an HRA contribution.

Councilmember Wise asked about SAC and WAC fees.

Senior Planner Gladhill stated staff could look at other permit fee waivers but the City has not considered waiving SAC and WAC fees in the past.

Councilmember Wise agreed this is a troubled site and he thinks this proposal is a good fit. He asked if this project would cause any possible impediment to surrounding developments.

Senior Planner Gladhill stated staff does not think it would impede another project but will study that issue during site plan review.

Mr. Podawiltz advised that this property and the site across the street are under the same ownership and that person thinks it is a benefit, not a deficit.

Councilmember McGlone stated the proposed workforce housing project will finish a road at its expense, which benefits everything over there. He noted the project includes substantial park structure on private property and raised the option of the developer contributing that park structure to the City's North Commons Park to create better utilization.

Mayor Ramsey asked staff to research that option and questioned the original intent of the right-of-way.

Senior Planner Gladhill stated the intent was for a public roadway to service the development as originally platted.

Councilmember Elvig asked if there are drainage issues in that location.

Public Works Director Olson advised there are no drainage issues because it has not been used but there are utilities in the road and if the City wants to extend to Town Center Drive, that is not a problem.

Councilmember Strommen asked that staff assure the criteria considered for contribution is recorded in the Council's discussion, such as completion of the road and creation of affordable housing. She noted it would be helpful to have such criteria identified ahead of time so developers know the Council's criteria for consideration of a contribution.

The consensus of the Council was to support the concept of workforce housing and move forward with the next step.

2.02: Review Fire Station 2 Project

City Administrator Ulrich noted several months ago, staff had presented fire station designs and direction was given by the Council to make modifications to bring the project back into line with the budget and Council's expectations. It was noted the project will involve vacating the site and selling the former municipal center site, potentially as a data center, a highest and best value, which could net over \$2 million.

Butch Schwartzman, BKV Group, described the first fire station and Council determination that the second station had more area than needed and should be more of a satellite station. The plans have now been scaled back from 12,000 sq. ft to 9,000 sq. ft. Mr. Schwartzman explained their involvement to test the two-acre site to determine if it can properly work for a fully-functional fire station with turning movements and site clearances. He indicated that answer is "yes" and explained the updated plan utilizes the same footprint but reduces areas for fitness and other spaces that could be added in the future, if desired. Mr. Schwartzman displayed the conceptual floor plan to serve the existing fire apparatus, noting it is very efficient providing gear storage to support fire fighters, training, and break spaces. He then displayed cost estimates reflecting construction of \$1,960,000 and soft costs of \$450,000 with a total project of \$2,410,000. He explained that as the project is refined, the estimated costs could come down. Mr. Schwartzman presented the time frame required to construct such a facility with a 3-4 month design process, 6 week bidding/award period; 8-10 month construction period; equaling just over one year.

Councilmember McGlone indicated the Councilmembers did not have opportunity to review the reduced building plans prior to the Work Session. He raised the issue that meeting rooms are still included and asked if the building needed to be constructed of brick or block.

Councilmember Elvig asked staff to provide funding options.

Councilmember Tossey asked staff to also provide a list of what is mandated to include.

The consensus of the Council was to schedule discussion of the Fire Station 2 project for a future work session after the Council has had time to review the revised plans and information requested of staff. At that point, the Council will decide whether Mr. Schwartzman should attend the meeting.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR, COUNCIL AND STAFF INPUT

None

5. ADJOURNMENT

Motion by Councilmember Backous, seconded by Councilmember McGlone, to adjourn the Work Session meeting.

The Work Session meeting of the City Council was adjourned at 7:00 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth, *TimeSaver Off Site Secretarial, Inc.*

**CITY COUNCIL SPECIAL MEETING
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a Special City Council Meeting on Tuesday, March 20, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Members Absent: None

Also Present: City Administrator Kurtis Ulrich
Deputy City Administrator Heidi A. Nelson
City Engineer Tim Himmer
Public Works Director Brian Olson
Parks Supervisor Mark Riverblood
City Attorney William Goodrich
Development Manager Lazan

1. CALL TO ORDER

Mayor Ramsey called the Special City Council Meeting to order at 6:08 p.m.

2. CITIZEN INPUT

None.

3. APPROVE AGENDA

Motion by Councilmember Wise, seconded by Councilmember Backous, to approve the agenda as presented.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Wise, Backous, Elvig, McGlone, Strommen, and Tossey. Voting No: None.

4. COUNCIL BUSINESS

4.01: Consider the Introduction of an Ordinance Authorizing the City of Ramsey to Convey its interest in a Property (Lot 3, Block 1, CORE ONE)

Deputy Administrator Nelson reviewed the staff report and indicated if the ordinance is introduced tonight, it would be considered for adoption at the March 27, 2012, Council meeting.

City Attorney Goodrich advised The COR plat will be platted shortly but since it is not yet recorded, the legal descriptions are "Ramsey Town Center." Once platted, it will all become Lot 3, Block 1, The COR.

Motion by Mayor Ramsey seconded by Councilmember Wise, to introduce Ordinance Authorizing the City of Ramsey to convey its interest in Lot 3, Block 1, CORE ONE, Anoka County, Minnesota to the Housing and Redevelopment Authority in and for the City of Ramsey, Minnesota.

Further Discussion: The Council discussed the notice provided for this special meeting. City Attorney Goodrich read the relevant Charter section, noting it states the Mayor or three members can call a meeting to order upon 24-hours notice. With regard to ownership, Deputy Administrator Nelson explained a quitclaim deed had been approved in May 2011 and the Council will gift the land to the HRA prior to it going to Flaherty and Collins. Councilmember Tossey cited State Statutes requiring 72-hours posted notice, regardless of Charter requirements. City Attorney Goodrich explained the City's Charter takes precedence over State Statute and Mr. Bray had scheduled this meeting based on the Mayor's call with 24-hours notice. He advised that no one has filed for special notice of meetings with the City. The Council discussed requirements for meeting notice and timing between ordinance introduction and adoption.

The consensus of the City Council was to direct staff to post three days' notice for a Special City Council Meeting to assure the process is open and transparent.

3. ADJOURNMENT

Due to the concern raised relating to the sufficiency of meeting notice, Mayor Ramsey adjourned the Special City Council Meeting at 6:26 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth, *TimeSaver Off Site Secretarial, Inc.*

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, March 27, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Deputy City Administrator Heidi A. Nelson
Public Works Director Brian Olson
City Engineer Tim Himmer
City Clerk Jo Ann Thieling
Senior Planner Tim Gladhill
Planning Intern Patrick Brama
City Attorney William Goodrich
Development Manager Darren Lazan

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 5:46 p.m.

2. TOPICS FOR DISCUSSION

2.01: Discuss Updates to the City's Comprehensive Sanitary and Water Plans

City Engineer Himmer reviewed the staff report and requested Council feedback on sewer system upgrades, whether to extend a forcemain sanitary sewer extension into the Northeast Sewer Area and include the associated costs within the 2030 MUSA study period; whether to create a residential equivalency factor for determining connection charges on high density residential developments; and, whether to maintain a minimum fund balance in an amount equal to the average annual expenses over the study period.

Brad Dewolf, Bolton & Menk, provided a presentation addressing projected population, projected sanitary sewer flows, six projects contained in the sanitary sewer Capital Improvement Plan, funding of sanitary sewer utility expenses, sanitary sewer utility revenue, and connection charges. Mr. Dewolf then presented alternate funding scenarios: Alternative 1a for a gravity

option extension to the northeast area; Alternative 2a for a lift station and forcemain extension to the northeast area; Alternative 1b for a gravity sewer extension to the northeast area with 0.7 ERU for high-density residential connection; and, Alternative 2b for a lift station and forcemain extension to the northeast area and 0.7 ERU for high-density residential connection.

Public Works Director Olson stated staff recommends Alternative 2b, lift station and allowing a high-density option. He explained that lift stations have higher operational expense and potential for backing up or power failures but the City has procedures in place to assure sewage does not back up. It is expected a lift station would last for 10-15 years until the northeast area is developed. Public Works Director Olson indicated staff thinks Alternative 2b is a reasonable solution to all goals mentioned by the Council and noted that a gravity system is more sustainable, keeps costs down, provides service, and grows as the user base grows.

Councilmember Wise asked about the expense for lift maintenance and if enough funding has been built in for maintenance of the lift stations.

Public Works Director Olson estimated \$60-\$70/month for electricity plus manpower needs for maintenance (cleaning once/3 months and yearly inspection). He noted the City can fund a lot of monthly charges to care for equipment with \$1.3 million.

Mayor Ramsey noted there would be capital costs to do the project in addition to maintenance costs and asked whether a comparison was done for maintaining the lift station over 20 years to determine if it is cost effective.

Public Works Director Olson stated it will not cost \$1.3 million to power and maintain a lift station over 20 years, estimating it would cost \$150,000 over 20 years.

Councilmember Elvig asked if new construction should fund infrastructure or existing residents pay for it plus user fees down the road.

Mayor Ramsey noted if the overall cost to develop a home is lower on the front end, they would realize a savings, maybe a bigger house, and build tax base.

Councilmember Wise pointed out the basis of the study is a growth rate of 260 units/year and indicated it appears to be an aggressive growth rate.

Public Works Director Olson stated that is correct, noting the Council had debated that issue for two hours at a previous meeting.

Mayor Ryan indicated that number is amortized over a number of years.

Robert Schunicht, Landform, explained that if the growth rates slow, then expenditures also slow. Also, it is recommended to have a fund balance equal to expenditures for a year to take up fluctuation in growth rates. Mr. Schunicht advised this is conservative/typical approach to this type of system.

Councilmember McGlone asked what criteria was used to determine the nine “development hotspots” identified on the map.

City Engineer Himmer indicated the nine hotspots are consistent with the City’s Comprehensive Plan and identified locations in the Highway 10 and 47 corridors, The COR, new areas within the MUSA, and the northeast area even though not currently within the MUSA.

Councilmember McGlone stated people in his ward do not want to see a lot of development and would probably not support a sewer extension.

City Engineer Himmer explained residents would not pay into the plan if not connected into the system.

Councilmember Elvig noted construction of apartments will add to capacity so he is comfortable with a growth rate of 260 units/year if blended over the years.

Mayor Ramsey noted the Council’s goal was lower charges for big projects.

Public Works Director Olson stated staff currently looks at waiting until the SAC determination has been made because it will indicate how much demand is created by the development. Staff is recommending Alternative 2b so people know when “coming in the door” the SAC charge for high versus low density. He noted the City has struggled with extending sewer into this area because of the infrastructure cost to construct the system and this alternative accomplishes the Council’s goals to keep development costs at a minimum.

City Engineer Himmer indicated the notion is getting fees down for larger, high-density subdivisions, noting the use of 0.7 ERU, means those fees will be less. He explained the 0.7 ERU formula came from Metropolitan Council data and Ramsey’s demographics from 2006 to 2010.

Public Works Director Olson asked if there is consensus to move forward with this philosophy.

Councilmember Elvig asked about residents who want to use their own private water wells.

Public Works Director Olson noted when a connection is made to sanitary sewer, connection is to also be made to the water service. He indicated the City has always encouraged use of private wells for lawn sprinkling noting the water rate structure penalizes for higher consumption.

Mr. Schunicht explained why it is typical for cities to maintain a fund balance of one year’s operating costs.

Mayor Ramsey stated his concern that this may result in creating a regional issue and need to build a regional water treatment plant.

Councilmember Elvig suggested a parallel study be conducted so the difference between a regional and a City-wide system can be compared.

Mr. Dewolf provided a presentation on water service, noting projected population, options for additional wells, or to consider groundwater wells, and groundwater treatment.

Mayor Ramsey asked if regional interconnections have been considered for emergency situations.

Public Works Director Olson reviewed utility service capabilities of area municipalities and advised that a regional treatment facility would involve a significant capital investment by Ramsey, possibly millions higher than addressing the City's own population. In addition, there is a need to prove benefit to downstream users.

Mayor Ramsey asked staff to research action previously taken by a County committee dealing with resolution of the regional issue.

Public Works Director Olson stated he presented at that regional committee and its goal at the time was to come up with interregional or intercity connections. However, it fell short of its mark for garnering Governor support and funding allocation for those improvement projects.

Councilmember Strommen reported that DNR Commissioner Landwehr was open to meeting with Ramsey and holding that discussion if the City felt the need to elevate the issue on the DNR "radar."

The consensus of the City Council was reached to support staff's assumptions and recommendation to plan for a forcemain sanitary sewer extension into the Northeast Sewer Area, including associated costs within the 2030 MUSA study period; create a residential equivalency factor for determining connection charges on high density residential developments (0.7 ERU); and to maintain a minimum fund balance in an amount equal to the average annual expenses over the study period.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR / COUNCIL / STAFF INPUT

None.

5. ADJOURNMENT

Motion by Councilmember Wise, seconded by Councilmember Backous, to adjourn the Work Session meeting.

The Work Session of the City Council was adjourned at 6:32 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, April 10, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig (arrived at 5:24 p.m.)
Councilmember Colin McGlone
Councilmember Sarah Strommen (arrived at 5:56 p.m.)
Councilmember Jason Tossey
Councilmember Jeffrey Wise (arrived at 5:16 p.m.)

Also Present: City Administrator Kurtis Ulrich
Deputy City Administrator Heidi A. Nelson
Public Works Director Brian Olson
Fire Chief Dean Kapler
Police Chief James Way
Finance Officer Diana Lund
Parks Supervisor Mark Riverblood
Human Resources Representative Lasher
Senior Planner Tim Gladhill
Planning Intern Patrick Brama
City Attorney William Goodrich
Development Manager Darren Lazan

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 5:05 p.m.

2. TOPICS FOR DISCUSSION

2.01: Continued Discussion of 2013 Budget

Finance Officer Lund reviewed the staff report and presented the proposal of Councilmember Elvig that the total levy not exceed \$7,150,000 with road reconstruction funded by implementing a \$7/month/franchise fee, generating approximately \$1.5 million. She indicated the net affect on the City's tax rate would be about 38.70% and create a positive cash flow of \$144,988. In addition, \$275,000 related to TIF 4 could be attributed to road reconstruction, resulting in \$1,750,000 for road reconstruction if including a franchise fee.

City Administrator Ulrich noted staff had previously estimated the need for \$3 million per year for full reconstruction if there are no assessments so this proposal is still short in terms of a full reconstruction effort.

Mayor Ramsey asked about the monthly franchise fee.

Finance Officer Lund clarified it is actually \$7 per month per utility, for a total of \$14/month.

City Administrator Ulrich stated this concept puts into place the proposed staff reductions, lowers the levy by taking off \$1.5 million, and then replaces it with the franchise fee implementation.

Mayor Ramsey noted it seems that adding the franchise fee on top will result in a tax capacity rate of 52%.

City Administrator Ulrich stated the publicized tax capacity rate would be 38.7%.

Councilmember Backous stated he thinks the franchise fee is a tax when it comes down to it, but it shows the public there are two pieces, the road reconstruction budget and the levy. He noted the taxpayer would be paying the same dollar amount but the franchise fee brings in more payers. The idea was to present it properly and he supported the franchise fee.

Mayor Ramsey stated it has been indicated that the advantage of the franchise fee is to bring in more entities but that portion only equals about \$40,000, which he considers a minute amount. Mayor Ramsey stated he does not support “pulling wool over resident’s eyes,” thinks it gives the wrong perception, and supports putting all road costs in the general fund budget.

Councilmember Wise arrived at 5:16 p.m.

Mayor Ramsey noted this proposal puts \$1.5 million into the general fund budget with a 52% tax capacity rate, which is 10% more than the current rate so it does not accomplish what needs to be accomplished.

Councilmember Tossey stated he knows the goal is to rid the City of assessments but if the City can pay down assessments and avoid a franchise fee with savings in the budget, it may lower the assessment amount to where residents would reconsider counter petitioning a project.

Mayor Ramsey doubted that would occur. He felt the odds were slim that a project will get done if assessed at 100% because it will be counter petitioned. In addition, the City is getting push back on reconstruction projects that are 50% assessed. Mayor Ramsey felt roads are a core function as well as police and fire and, unfortunately, the City has neglected that responsibility for years. He did not support adding another 30% tax on residents and suggested the Council reassess core functions and adjust accordingly. Mayor Ramsey stated he would not vote to increase taxes or institute a franchise fee. Rather, he supported reassessing services provided to residents, even if that results in cutting back the core services of police and fire.

Councilmember Backous stated he has nothing against the franchise fee concept but would not support \$14/month. He indicated it was his impression that the intention was \$7/month and asked if a lower franchise fee could be considered.

Mayor Ramsey stated he would not support a franchise fee but was not opposed to having a portion of the road project assessed, especially road reconstruction.

Public Works Director Olson stated that has been the challenge with this issue, some feel assessments should be phased out and others support assessments. He noted there are benefits to a franchise fee beyond collecting from non-taxable because it also creates equity between residential units. However, an inequity is with residents not connected to natural gas since they would pay only one-half the franchise fee. Ultimately, it is the City Council's decision in how to come up with \$3 million and whether with or without assessments. Public Works Director Olson stated the franchise fee was presented at \$14/month and noted it could be phased out over five years with a 25% assessment for reconstruction.

Councilmember Backous asked if reconstruction projects can be scaled back and/or put off another year or two. Public Works Director Olson stated he understands it is a difficult situation and decision to make, and projects could be scaled back but the Council still needs to solve the issue of additional funding sources. He pointed out that a 40-year design life capitalizes on the Anoka sand plain and maintenance of the roadway system in sealcoats and overlays will extend the pavement life. Public Works Olson noted staff had presented a conservative approach and spread cost over a 40-year time frame. However, 50% of the City's roads were built between 1975 and 1985 so delaying projects further will result in the reconstruction effort falling behind.

Councilmember Elvig arrived at 5:24 p.m.

Mayor Ramsey stated he would support a 40% tax capacity rate but when you add in the franchise fee, it is back up to about 52%.

Councilmember Elvig noted the first goal was to pull down taxes to get a leaner environment by cutting back costs. Staff's recommended cuts reduce the budget by \$1.4 million and the franchise fee would offset that reduction by raising \$1.5 million, resulting in a zero net increase to taxes. Councilmember Elvig noted the franchise fee would allow the creation of a \$1.5 million dedicated road fund supported by all utility users including non-profits.

Finance Officer Lund corrected that adding the franchise fee would increase the tax capacity to 44%, not 52%.

Mayor Ramsey stated he wants the tax capacity to be at 40%, noting the tax capacity was 44% last year and 39% the previous year.

Councilmember Backous stated if the franchise fee is cut to \$7/month, it brings the net tax capacity to 41%. He indicated he would support a franchise fee under \$10/month.

Finance Officer Lund advised that a \$7/month franchise fee would generate \$825,000.

Councilmember Tossey noted the City cannot bond for a project unless it is 20% assessed. He expressed concern that funding road projects only from franchise fees would limit the City because it would not have the ability to bond should there be a critical road project.

Councilmember McGlone stated what the counter petition process costs the City.

Public Works Director Olson estimated \$150,000 for Andrie Street.

Councilmember McGlone noted residents that are selling their house would not want their property assessed.

Mayor Ramsey advised that staff has made some progress on the counter petition process.

City Administrator Ulrich reported staff had conversation with the Charter Commission and Commissioners indicated they were open to setting a higher percentage requirement, from 35% to 50% or greater, for residents to be able to overturn a project.

Councilmember Backous stated the assessment for Andrie Street was to be \$1,500.

Public Works Director Olson explained that Andrie Street was originally proposed as an overlay but members of the association that came forward said they wanted a reconstruct. Since it is an MSA road, it had to be built larger so that was done and an overlay assessment considered. The City went forward with project design with anticipation that assessment rates would be about \$1,800 to \$2,000 with the rest funded with MSA. He estimated that assessment may have equaled 20% of the total project cost.

Councilmember McGlone noted the residents would not support paying it.

Public Works Director Olson agreed the vast majority of residents were opposed at that time, not because of the assessment, but because of the sidewalk.

Councilmember Wise stated he believes strongly in the franchise fee because of the equity it brings; a person living in an apartment generates the same wear and tear on the roads as someone on a 20-acre parcel. He stated he understands the suggestion to reduce the franchise fee but noted it would be a “band aid” and not fix the problem of funding road projects. Councilmember Wise stated Councilmember Tossey makes a good point about not being able to bond for a project, which may involve a considerable project to overcome.

Public Works Director Olson noted that in January the Council had discussed a 50% assessment and scaled it back to 25% with a franchise fee. He indicated if there are no assessments or franchise fees, internal borrowing or a reserve would need to be built so the City can afford larger projects.

Mayor Ramsey stated the City had not put away funds for streets for years and he would not support asking for a “raise” from residents to meet that need.

Councilmember Wise stated the Council needs to do something and while it might not fix everything, the City needs to start by dedicating some level of funding. He agreed the solution may not be perfect and some problems may be found in the future, but the City needs to start something.

Councilmember Backous agreed that a reduced franchise fee may not take care of the entire problem, but it would at least be a start. He noted that residents will pay for the roads one way or another and the Council is only talking about the mechanism to take the money from the residents. Councilmember Backous felt the assessment process was an inefficient and costly system but a franchise fee would start accumulating funds. He agreed that bonding is an important point that the Council needs to keep in mind and may be a reason to consider a compromise of franchise fee and some low percentage of assessment that allows the option to bond, if needed. In addition, a lower assessment may lessen the likelihood of a counter petition.

Councilmember Tossey stated he cannot support a franchise fee if in addition to assessments. He would rather keep the assessment low and raise additional funds through taxes as opposed to hiding it as a franchise fee. Councilmember Tossey noted residents will be told they are paying a franchise fee to fund roads and then later will ask why they are also being assessed to fund a road project. He supported keeping the assessment process in case the City needs to bond for a critical road project.

Councilmember Backous asked whether the City had that “phantom critical project” in its road system.

Public Works Director Olson stated staff could provide a map identifying the age of roads and dates for reconstruction. He stated he had mentioned earlier that half of the roads were built within a ten-year timeframe so there will come a point when the City will need a glut of funding.

Councilmember Backous stated he thinks the City has a “broken system” of assessing and counter petitioning that is too costly and wastes the City’s money. He noted staff has researched road conditions, knows when the roads were built, and the amount of funding needed per year to tackle that problem. In the past, the City has borrowed from TIF districts and used internal loan funds so he was not afraid there may be such a “phantom critical project” in the future because mechanisms are in place and can be used, if needed. Councilmember Backous stated his support to come up with a funding mechanism and then going with it.

Councilmember Elvig stated he agrees with the comments of Councilmembers Backous and Tossey and is not excited about a mix of franchise fee and assessments. He offered a compromise of a \$20/month franchise fee for five years to address the need of a start up fund, and then reduce the franchise fee over the years.

Finance Officer Lund explained that most TIF Districts are coming off line in the next few years. She advised when TIF Districts will be decertified and noted the project has to be within the District to use those funds.

Councilmember Elvig noted part of the consideration is to reduce government, the City needs to raise funds and create a pool of money for roads, and it will come from the taxpayers. He stated residents can be shown the City is reducing taxes that will be offset when adding a franchise fee to fund roads, but he believed a later assessment would send the wrong message. Councilmember Elvig asked if there is another mechanism available if the City cannot bond for roads.

Public Works Director Olson stated if the Council decides it wants to eventually get rid of assessments, then staff will tailor the message to residents who ask why they have to pay for the franchise fee in addition to a lower assessment by explaining the City is no longer assessing for sealcoating and overlays and will eventually do away with assessments all together. He indicated staff will follow the Council's policy and correctly portray it to residents.

Councilmember Elvig expressed concern that property owners will counter petition road projects until the funding is figured out.

Public Works Director Olson stated that would not be a problem because the City is already behind in maintenance activities, which will take several years to complete.

Councilmember Tossey stated after further thought he supports no franchise fee because taxpayers want their City to hire police to keep them safe, fire protection for their property, and roads to get those services to their property. He thought a franchise fee would create another "finger" to take taxes and in a representative form of government, residents need to know what they are voting for.

Mayor Ramsey asked why police and fire are more important than roads and suggested adjusting levels of funding for each piece because, in his mind, all are equally important. He indicated that if it comes down to it, he supports telling the City Administrator to present levels of funding for police and fire so the Council can find money to fund the roads.

City Administrator Ulrich advised the general model, if taking from the general fund, is with an assessment of 25% to 40% with the City paying the balance of the project cost.

Councilmember Backous noted the Council is talking about, cutting the budget in one area and taxing through a franchise fee to take care of the roads. He stated he supports a franchise fee that brings in all the road users, noting it may only raise an additional \$40,000 with these numbers but it will grow in the future. Councilmember Backous stated he does not understand the debate because all funds come from resident's pockets.

Mayor Ramsey stated he still believes the levy is above where it should be.

Councilmember Strommen arrived at 5:56 p.m.

Councilmember Wise stated he also likes the equity that franchise fees bring and does not support a policy that will "handcuff" a future Council. He stated he has no problem with moving forward, to Councilmember McGlone's point, and tell residents this problem has been here for a

long time and needs to be addressed through a road policy. Councilmember Wise stated it is his goal to have a road policy in place by year end. He supported reducing the general levy by the same amount as taxing from another method, such as a franchise fee.

Councilmember Elvig noted the Council has already made dramatic budget cuts. He stated the Council previously decided to tack on a storm water fee when it realized the problem with poor drainage and that fund has built so those drainage problems can be addressed. The same thing occurred with priority lighting and he believed funding roads was no different. Councilmember Elvig stated it would be prudent to lower the general budget but that cannot be done in addition to rebuilding 40 years of roads and end up at a net deduct. Councilmember Elvig stated when property values go up, then maybe the City can absorb more internally but the road funding problem has to be solved today.

Councilmember McGlone asked what the tax capacity rate would need to be to solve the \$3 million road funding problem in 2013.

Finance Officer Lund stated it would go to a 53% tax capacity.

Councilmember McGlone stated the City cannot address all of the roads today and he hates the idea of assessments because of the problems it causes. He agreed with Councilmember Tossey that residents expect a police officer and fire fighter but the City does not have franchise fees for those services. Councilmember McGlone stated the Council needs to decide and come to a consensus on what level the City can fund for road projects. He agreed the problem is so big that some level of assessments may need to be considered.

Mayor Ramsey stated his plan was 30% for roads, 40% for the tax capacity rate, and 50% for staffing. He stated it seemed plausible to solve some of the issues. Mayor Ramsey stated if the Council ever gets to the point of talking about raising taxes, it better include all funds including spending the Land Trust Fund long before considering raising taxes.

City Administrator Ulrich stated one of the strategies could be to solve \$1.5 million of funding or target money for 2013. He believed there was still a need to have an assessment process as cumbersome as it is. City Administrator Ulrich suggested a property owner should pay a portion of a project that benefits its property and the rest would be paid by the City as a community benefit. If the Council supports a 40% tax capacity bench mark, then services need to be cut to get there. City Administrator Ulrich stated the Council may need to look at additional general fund reductions to stay at a 40% tax capacity rate, noting police and fire budgets have had cuts but not to the extent of other operations.

Mayor Ramsey stated he would like to see more information on the 30/40/50 formula.

Councilmember Strommen stated she liked the plan presented by Public Works Director Olson and City Engineer Himmer for a franchise fee combined with assessment for reconstruction because funding cannot be solved without some level of assessment. She also supported getting assessments out of sealcoat and overlay projects. Councilmember Strommen noted residents said assessments are not an expense people can plan for but if paying a little each month

(franchise fee), residents do not feel as much “pain.” She stated she has talked with a few people about these ideas and found it was fairly well received. Councilmember Strommen noted the City did a formal study and franchise fees were well received because people can plan for it, a small amount each month. She noted that reconstruction projects don’t come along very often but residents can’t plan for them. In addition, franchise fees would start to bring in and build funds today and can be reconsidered in the future if the Council desires. She also felt staff’s plan presented a balanced approach. In terms of cuts to the general fund budget, Councilmember Strommen stated she needed to understand the context and how the service level may change, noting residents want to know the level of service they are getting for what they pay.

Mayor Ramsey stated he would also like to see that information. He agreed the Council studied assessments but he felt the question was not asked correctly.

Councilmember Backous asked on average, what is the assessment.

Public Works Olson answered that it varies dramatically on each project and the highest was Garnet Street with \$22,000 principal, \$27,000 with interest added, and a ten-year payoff. He explained that a residential urban lot with less frontage would equal \$9-10,000 if 100% assessed.

Councilmember Backous noted that would equal about \$75/month for ten years. He asked if residents would rather pay \$75/month for ten years or \$14/month forever, thinking residents would probably support the assessment.

Councilmember Tossey noted the Council is talking about an assessment plus a franchise fee, and he does not support using both.

Councilmember McGlone asked about raising taxes on all \$14/month instead of considering a \$14/month franchise fee.

Finance Officer Lund stated it would be about a 46% tax capacity.

Councilmember McGlone noted property taxes are deductible but franchise fees are not.

Mayor Ramsey asked staff to address the 30/40/50 plan he had proposed because as more development happens, more funds are collected and the road funding also increases.

Finance Officer Lund asked if the percentages include debt service.

Mayor Ramsey stated he was addressing the amount of money the City spends every year and he does not want taxes to go up.

Finance Officer Lund asked if the percentages are calculated on the total budget or the total levy, noting the budget will be \$10 million and the levy has debt service added to it. In addition, staffing is only a general fund number so there are many variables.

Mayor Ramsey supported 30% for roads because it is 30% of the City's job to care for the roads and the other 70% responsibility is for police, fire, and the rest.

Councilmember McGlone stated the Council previously decided to base trunk numbers on a growth of 260 units per year. He stated while that is not reasonable today, it is probably not unreasonable for the future. Councilmember McGlone asked what would be the City's increased tax base if using a similar formula; how much will the City's taxes grow. He noted staff's estimates had not addressed projected growth, which may show more funds will be raised so the assessments can be dropped.

Public Works Director Olson cautioned that growth results in the need for more infrastructure and funding to pay for those improvements when due.

Councilmember Tossey added that along with infrastructure growth comes inflation and the City's biggest possible tax base is TIF from The COR, which cannot be used unless the City decertifies. He believed the City needed to stop spending TIF funds it does not have.

Mayor Ramsey indicated there may be future projects not included within a TIF District.

Councilmember Strommen noted the City has gone through periods of significant growth and if that was the solution to road funding, it would have been solved. She indicated this is why she is concerned about not having an economic development staff person in place, to grow and create a robust tax base since it will not happen in a competitive economy without dedicated staff working on it.

Mayor Ramsey agreed the City had experienced growth but believed the funds were spent instead of being put away for road projects.

Councilmember Elvig stated this and the last Council did not have the foresight because TIF dollars were spent elsewhere. He stated he can get on board with the 30/40/50 formula, having a sliding scale, and a policy of establishing spending ratios; however, the Council needs to address how to raise money. He noted the Mayor does not support raising it through taxes so he would ask how the budget and expenses can be cut and money still put aside for on-going maintenance with reconstructs in three to four years.

Mayor Ramsey indicated there may be unrestricted funds that can be reallocated.

Councilmember McGlone stated he would not consider in any way the idea of basing all on projected growth because that is what happened in the past with constructing and paying for the municipal center. He just wants a basis to start a discussion.

City Administrator Ulrich indicated staff will calculate the 30/40/50 formula, additional reductions to get to \$1.5 million, determine the point of additional growth that would drive down the tax base, and land proceeds that will be gained from The COR. This information will be presented to the Council for additional discussion.

2.03: Consider the Sale of Peltzer Park (Lot 1, Block 5, Peltzer Addition)

Parks Supervisor Riverblood reviewed the staff report and presented a draft policy for the disposition of surplus City-owned lands. He estimated there were 20 distinct groups of people using Peltzer Park, an average of three persons in each group, equating to about 20 hours of playground use, at a minimum, in one March weekend.

Mayor Ramsey stated he favors selling underutilized properties but it is now clear that Peltzer Park is being used so he supports keeping and maintaining the property.

Councilmember Strommen concurred, noting the maintenance of Peltzer Park is not that much, and park use will probably increase during warmer summer months.

Councilmember McGlone suggested consideration whether to sell should not have labeled only “utilization” but, rather, what the City can sell. He stated he is not considering the number of people using the property. Councilmember McGlone noted this is one of 36 parcels with potential for sale and while \$1,000 in maintenance may not be a lot, it may not factor staff traveling to and from the park for the maintenance. He stated the idea was to take money from selling properties and put it into the park fund to pay for the system and create a better value to residents for what they pay for parks in general.

Mayor Ramsey stated his preference to start with considering properties that are not utilized prior to considering parcels that are being utilized.

Councilmember Backous indicated that tough decisions will need to be made to weed out the quantity of parks but he believed there were easier properties the Council can consider than Peltzer Park.

Councilmember Wise stated if this decision is delayed, he wants to make sure it remains on the list of potential properties for sale so it is not forgotten.

Parks Supervisor Riverblood presented pictures taken in Peltzer Park at ten-minute increments.

Councilmember Elvig stated he is concerned with the policy because selling parkland that was dedicated by someone for that purpose should not be taken lightly and requires foresight. He stated he is struggling with this decision, understands the budget consideration, but felt it may be almost impossible to buy park land in the future so he wants to be cautious and assure a good thought process.

Councilmember McGlone asked how large is Peltzer Park.

Planning Intern Brama estimated close to 10 acres.

Parks Supervisor Riverblood added it is primarily low land.

Councilmember McGlone noted this open space will remain, the City has a park that was never dedicated, and that was the criteria used to determine where to start. He stated Peltzer Park was considered to be “low hanging fruit” in what the City can sell in park inventory and that is how Peltzer Park rose to be considered first. Councilmember McGlone stated the play equipment is scheduled to be relocated to a nearby park, not taken out of the area. He believed it was never going to get easier down the road to make a decision to sell park land.

Mayor Ramsey stated that is the case but the Council had the understanding that Peltzer Park was under utilized; however, it appears now that is not the case. He believed the City had other parcels that were not being utilized and supported considering them before considering parcels that impact residents.

Councilmember McGlone pointed out this park equipment was to be moved to another location where it will be utilized even greater.

Councilmember Strommen stated Councilmember McGlone is right but there is a difference in what the City “can” sell and what it “should” sell. She supported discussion on a policy that is clear and defines what is “underutilized” to provide guidance when making a decision on what to sell. She stated the Council needs to be clear on the direction. In terms of moving the play structure, Councilmember Strommen stated she had reviewed comments from residents who live near the new park and liked it because their children would not have to cross the street. She asked how the Council makes the decision to move a play structure and create that same issue (children crossing a street) in this neighborhood. Councilmember Strommen commented on the value for children to get to and from a park, have that level of independence, and be safe in playing. She noted people look for that in a community so she thinks this is not a big investment to keep/maintain Peltzer Park and noted it can be evaluated again later.

Mayor Ramsey asked if any Councilmembers are adamant in selling Peltzer Park.

Councilmember Tossey stated Park Supervisor Riverblood had first thought Peltzer Park was an under utilized park and asked if he still holds that opinion. He stated he does not want to pit two neighborhoods against each other for the play equipment.

Parks Supervisor Riverblood stated staff knew this would be a difficult decision, as has been proven, and it was hard to see this number of children using Peltzer Park and say it is not utilized or is under utilized. He noted that when compared to Central or Elmcrest Parks, all are underutilized but to this neighborhood it appears to be a good utilization given the low maintenance costs and he would recommend not taking action and looking at in the future.

Planning Intern Brama stated there are an additional two parcels not currently being marketed and the remainder of the parcels were dedicated.

Mayor Ramsey supported looking at the two remaining parcels that have not been dedicated.

Planning Intern Brama asked if the Council would like to adopt a policy prior to making these decisions.

Mayor Ramsey stated if no one is using the property and there is no play structure, he thinks sale can be considered prior to policy adoption.

Councilmember Elvig indicated it would be beneficial to bring those properties forward so they can be debated, which may drive the policy.

Councilmember Tossey stated he often hears people change their mind if residents are in attendance, and at the next meeting North Commons residents may be in attendance. However, he has met with them and does not think North Commons' residents would support taking a park away from another neighborhood.

Councilmember McGlone stated he wants to do the right thing for all of Ramsey's residents. He stated his frustration that the Council debates issues and then backpedals when a resident walks in the door. Councilmember McGlone stated \$1,100 per year to mow the park is something to consider and supported streamlining the City to make it more efficient by selling unutilized properties.

The majority consensus of the Council was to not sell Peltzer Park at this time and continue annual maintenance expenditures for the park, approximately \$1,000/year.

2.02: Policy for the Disposition of Surplus City Owned Land

Planning Intern Brama reviewed the staff report, presented the draft policy, and suggested adding criteria staff used to identify surplus properties.

Mayor Ramsey asked who makes the decision relating to future City functions.

Planning Intern Brama explained that would be addressed in the criteria to determine what is needed. He stated when this process first began, a meeting was held with department heads to review the Comprehensive Plan and addressed each parcel to make that determination.

Mayor Ramsey stated that will be difficult to qualify, such as whether the parcel has to be identified in the Comprehensive Plan for future use.

Councilmember Elvig noted the Council has not considered criteria to determine how the City should acquire property, noting if the Council cannot find determination for acquisition, it would then not be needed. He suggested the language also include acquisition findings of facts.

Planning Intern Brama stated staff will bring forward that language so the Council can consider two side-by-side policies, one for acquisition and one for disposition. He explained that when a property is identified, staff would prepare a list of criteria including findings of fact for Council consideration prior to making that decision. In addition, there would be different criteria for properties that were dedicated.

The consensus of the Council was to direct staff to present additional information, as discussed, for consideration at a future Work Session.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR / COUNCIL / STAFF INPUT

None.

5. ADJOURNMENT

Motion by Councilmember Wise, seconded by Mayor Ramsey, to adjourn the Work Session meeting.

The Work Session of the City Council was adjourned at 6:56 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, April 24, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous (arrived at 5:37 p.m.)
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Public Works Director Brian Olson
City Engineer Tim Himmer
Fire Chief Dean Kapler
Police Chief James Way
City Clerk Jo Ann Thieling
Human Resources Representative Colleen Lasher
Senior Planner Tim Gladhill
Planning Intern Patrick Brama
Development Manager Darren Lazan
Robert Schunicht, Landform
Brad Dewolf, Bolton & Menk,

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 5:30 p.m.

2. TOPICS FOR DISCUSSION

2.01: Discuss Request for Special Events Permit

City Administrator Ulrich reviewed the staff report and presented the request of SPM Properties, The Penalty Box Bar & Grill, 14077 St. Francis Boulevard NW, for a special events permit to host a tent party on June 22-23, 2012 including a live band both nights from 8 p.m. to midnight.

Police Chief Way stated his comments were included in the case report.

Councilmember Wise stated he received several telephone calls after the last event with concerns about trash and traffic. He supported relocating the tent.

Shawn McManus, The Penalty Box, applicant, stated he does not object to moving the tent towards the front of the building but not behind the building.

Councilmember Wise asked about past due utility bills.

Mr. McManus stated that is the responsibility of the landlord.

Councilmember Tossey asked if it is common to require an upfront escrow for Police and clean up costs.

City Administrator Ulrich answered it is common as well as requiring a deposit for services provided.

City Clerk Thieling explained that due to past events, there is some experience so they know the number of officers needed and amount of escrow to cover those expenses.

Councilmember Tossey asked how many people will be attending.

Mr. McManus estimated several thousand.

Councilmember McGlone reviewed issues with past events on this property and stated he found the number of complaints to be low when compared to the number of attendees and number of residents. He indicated he has attended past events and agrees with the applicant that a location behind the building would restrict view of the event but placing the tent up to the building, would alleviate a lot of the concerns. Councilmember McGlone noted the applicant occupies the end of the building and the nearest tenant is a dentist that is not open during this time. He supported creating flow of traffic around the edge of the property.

Councilmember Backous arrived at 5:37 p.m.

Councilmember Elvig noted the City had addressed the issues raised during the first event so the second event was better organized and controlled. He asked about the timing to close the event.

City Clerk Thieling advised the request is to allow the band to play until midnight, which can be considered by the Council.

Councilmember Elvig noted the Council has granted a similar request until midnight.

Fire Chief Kapler reported a letter was received on March 29, 2012, indicating the monitoring contract for the alarm system is past due so monitoring service will not be provided after April 30, 2012. Should that occur, this business could no longer remain open after April 30, 2012.

Mr. McManus assured the Council he will take care of that payment.

Mayor Ramsey stated he prefers the tent in back but understands the concern with view of the event and would support the event request as long as the tent is removed by the following Monday morning.

Mr. McManus assured the Council the tent will be removed by Monday morning.

City Administrator Ulrich asked if there was Council consensus to allow the tent in the front and to extend hours of operation until midnight.

Councilmember McGlone used a site map to identify a tent location in the front of the building, which would allow the front drive aisle to remain open.

Mr. McManus indicated the tent is 60' x 100' so the side (end) parking lot is not large enough. He stated he would not object to moving the tent toward the front of the building; however, it would then be located in front of Subway. He stated they have picked up all the trash, addressed any complaints that came up, and this event brings a lot of people to Ramsey.

Mayor Ramsey stated he does not support locating a tent in front of other tax paying businesses, including Subway.

Mr. McManus stated Subway has not objected and they offer Subway to place signs on their tent. He suggested Subway and other businesses benefit from this event.

City Administrator Ulrich noted the staff recommendation is to require the applicant to obtain letters from tenants to assure they are aware of the event and do not object.

Councilmember Backous stated if tenants have not complained in the past they probably don't object.

Police Chief Way advised that tenants have complained about past events. He explained the City does not want traffic between the tent and curb, thus the recommendation to move the tent towards the building. Last year, barricades were supposed to be installed to increase public safety; however, the barricades were not installed.

Councilmember McGlone stated if the tent is on an angle with the building, Subway will still be visible. He noted if the pizza business wants to have a pizza tent, they can make the request. He stated he understands the recommendation to collect fees in advance because this is a tenant (not the building owner) so costs cannot be put on the property tax bill. In addition, there has been a past issue collecting fees from a different tent party applicant.

Mayor Ramsey encouraged Councilmembers to talk with the other tenants to assure their support.

Councilmember Strommen supported requiring the applicant to obtain letters from tenants of this building, as a courtesy.

Councilmember McGlone stated this business has the right to have a tent party if conditions are met.

Mayor Ramsey stated he would prefer the tent be located at the end of the building, because he thinks it will fit, and will make his decision when it is before the Council.

Councilmember Tossey stated support to get permission letters only from tenants that would be impacted by the tent location.

Mayor Ramsey stated he did not support requiring tenant letters and intended to talk with them personally.

City Administrator Ulrich recommended a letter from the building owner be required.

Police Chief Way recommended requiring the applicant to have an employee cover the parking lot, as required during past events.

The consensus of the Council was to approve staff's recommendation to allow the tent to be erected in front of the building and require the applicant to provide disclosure notices of the event, signed by the building owner indicating they are in favor of this event taking place, prior to granting a permit. This item will be considered on the May 8, 2012, consent agenda.

2.02: Discuss Comprehensive Utility Plan Updates

City Engineer Himmer reviewed the staff report.

Councilmember Elvig stated he thought the City was at the DNR capacity for wells.

City Engineer Himmer explained there have been mixed messages from the DNR so there is currently nothing definitive whether the City can or cannot get additional wells.

Councilmember Strommen reported she is working to schedule a meeting with the DNR Commissioner to address that issue.

Brad Dewolf, Bolton & Menk, presented the Comprehensive Water Study Utility Fund Analysis.

Mayor Ramsey noted the previously estimated \$20 million water treatment has now become a \$32 million water treatment plant.

Public Works Director Olson advised that time and inflation have impacted the price.

Mayor Ramsey asked how much of the \$8 million in the bank was generated from fees that were supposed to go to the surface water treatment plant.

City Engineer Himmer stated the water treatment plan was supposed to be on line five years ago so those dollars remained in the account and delaying construction generated additional fees and rates to create a larger fund balance.

Mayor Ramsey asked if current residents are being penalized.

Public Works Director Olson explained the City has been collecting fees from users and new people coming into the system because the Comprehensive Plan indicated there will be some form of treatment. Lately, there has not been a lot of new development so the majority of the fund balance was created by existing users, resulting in an expectation that better water quality should be provided. He noted the creation of this fund has put the City in a good position going forward.

City Administrator Ulrich asked what portion of the \$8 million is for the water treatment plant and what portion is for capital improvement costs and repairs.

Public Works Director Olson anticipated a bond of about \$20 million so just over \$1 million a year was collected for the water treatment plant from existing users.

Mayor Ramsey stated support that something be done to improve water quality and asked how water quality can be portrayed as a regional problem when there is excess revenue in the City's account. Mayor Ramsey also asked if water rates can be lowered as additional fees are collected.

City Engineer Himmer stated this analysis factored in 2% and 2.5% rate increases but growth may exceed or fall behind so rates should be analyzed on an annual basis going forward.

Councilmember Elvig commented that in the past, a lot of residents were fighting growth so the City tried to make growth pay for itself. He cautioned there is a need for balance and dropping rates too far may stimulate the idea to encourage people to come to Ramsey because it is cheaper to build here.

Mayor Ramsey stated he believed the reason people came to Ramsey was for cheap land, not cheap utilities.

Councilmember McGlone stated he has an issue with how much of this cost is a City resident issue as opposed to a developer or current user issue. He noted Ramsey is a developing community but residents who do not want water will push back. He asked how much of the cost should be borne by all residents, developers, or new residents. He also asked how it is progressing to make this a regional problem.

Public Works Director Olson advised the City cannot charge residents a utility fee unless they gain benefit. If the resident has a private well, it would be impossible to prove benefit. As it relates to the regional discussion, staff felt it was important, after analysis, to not fully fund with user rates and fees so the City can show a deficit and unfunded amount when it goes before the Legislature to request capital to address this regional issue.

City Engineer Himmer summarized the sanitary sewer rates and fees and asked the Council if it was comfortable with the assumptions.

Councilmember Tossey asked if the maintenance costs for wells was part of this consideration.

City Engineer Himmer answered in the affirmative, noting maintenance and operation of existing equipment is also included.

Mayor Ramsey asked if there is benefit to pulling the wells and using water from the Mississippi River.

Public Works Director Olson stated the idea was to use the wells as the backup and during spring time when river effluent is high.

Robert Schunicht, Landform, advised the best municipal water supplies have a variety of sources (aquifers, wells, surface) so he would encourage the City to keep both options in case something happens to one source, such as bad surface water conditions during spring time.

Mr. Dewolf completed his presentation by summarizing sanitary sewer user rates.

Councilmember Elvig raised the option of creating a fund that can be drawn upon, if approved by the Council, when a resident has an emergency, an entire neighborhood is getting dry wells, or septic systems are starting to fail and they cannot afford to rebuild a private system.

Councilmember Wise expressed concern with creating a government program to “help.”

Councilmember McGlone stated he likes the current system, which has been exercised for residents that need help, and the costs are put on their taxes.

City Engineer Himmer advised there was a recent resident inquiry for help with their septic system. He agreed that reducing the connection fee may swing residents to connect instead of installing a private system, if it is available in their area.

Councilmember Elvig stated he was considering how to encourage residents to connect to the system.

Councilmember McGlone offered the option of a “hookup amnesty day” during which there would be a reduced fee.

City Engineer Himmer asked if the assumptions used to develop these rates were supported by the Council.

Councilmember Wise asked why Andover or Anoka were not used in the rate comparisons.

Mr. Dewolf answered they had used cities experiencing similar growth to Ramsey.

Councilmember McGlone asked about the annual growth assumptions used.

City Engineer Himmer indicated the assumptions used growth of 260 units per year and 2.6 persons per household.

Councilmember McGlone noted that during years there was no growth the City was collecting fees and asked if the Council is comfortable with the assumptions based on growth.

City Engineer Himmer explained it is for 20 years and the assumptions in the overall Comprehensive Plan were higher. The assumptions used were consistent with the Utility Plan and the Council can adjust up or down. He reviewed the reductions to connection fees of about \$1,300/unit in total.

The consensus of the Council was to approve assumptions used by staff to formulate the comprehensive water system CIP budget and associated rate and fee structures to fund the proposed improvements. The Council directed staff to draft the final comprehensive utility plan reports and schedule for adoption at a regular City Council meeting.

2.03: Expanded Recreational Vehicle Use

Police Chief Way reviewed the staff report and asked if the Council still supported the definition of The COR where ATVs and golf carts would be restricted.

Councilmember Wise stated he thinks it will be all or nothing.

Mayor Ramsey stated he would like to approach this differently, make ATV/golf cart use as liberal as possible, and then hold a work session to debate whether to add restrictions.

Councilmember McGlone stated residents in Northfork were thrilled with the idea of taking some other form of transportation to this place.

Mayor Ramsey stated he knows the consensus by some on the Council the last time this was discussed.

Councilmember Tossey stated he does not think anyone will use these things so he does not have a problem with nonrestricted use.

Councilmember Strommen stated she is not in favor of opening it and asked how much staff and Council time will be spent on this issue, noting there are other issues of importance and she has not heard an outcry that there is a need for this. In addition, staff has been cut so she is concerned about an extended debate on restrictions that should or should not be added.

Councilmember McGlone stated some people in Northfork like this idea so he would support an ordinance without restrictions and noted the Council can change the ordinance if something does not work.

Councilmember Elvig noted there is a big difference between an ATV and a golf cart, which is often electric and makes little or no noise. He acknowledged that other cities have allowed golf carts to come into a core area and it seems effective but he does not envision four-wheelers coming to The COR in the same way since an ATV has soft tires, not made for road use, and are louder. He stated he could consider allowing ATVs in the northern area, but not The COR, and noted that snowmobiles are not allowed all over Ramsey for a reason. Councilmember Elvig stated ATVs can also damage grass and are a faster, stronger, and louder machine than a golf cart.

Mayor Ramsey suggested that maybe the City should then outlaw Harley Davidsons because they are larger, louder, and some do not have signals.

Councilmember Backous indicated he does not object to ATVs being used in the country but his neighborhood has more narrow streets and he would be concerned with the safety of residents and children since four-wheelers are harder to see. He stated he agrees with Councilmember Strommen that many people sit in the “parking lot” of Highways 47 and 10 so maybe the Council should put its energy into those issues instead of wasting time talking about something that is not a problem. Councilmember Backous stated if this issue involves a long debate, he would just as soon scrap it from the beginning.

Mayor Ramsey stated then the City should outlaw bicycles because they can also be dangerous. He stated this consideration does not have to be a long and drawn out process, it can be done quickly, and addressed again next summer if changes are needed.

Councilmember Tossey stated he has not heard an outcry for this either but would favor unrestricted use because if there is a violation, the ATV will get tagged, just like a moving vehicle.

Councilmember Wise agreed with Councilmember Tossey to open it up and stated he also thinks not many will use ATVs. However, he would support revisiting the issue should any problems arise.

Councilmember Elvig stated he is certain there will need to be regulations, rules, or permits and asked how much time staff has already been spent.

Police Chief Way stated he has invested a few hours.

Councilmember Elvig asked how many staff resources will be used to put together this program, draft rules and regulations, and monitor it. He noted the Council wants to cut staff hours and he wants to protect staff resources.

Councilmember Tossey stated the Legislature has already addressed traffic regulations that dictate what motorists can and cannot do.

Mayor Ramsey stated four Councilmembers (a majority) have indicated support to move forward.

City Administrator Ulrich stated staff will draft an ordinance with regulations on hours of operation, in its simplest form.

Councilmember McGlone stated he does not think everyone will buy a four wheeler but people who already own them may like the idea of having more freedom. He supported trying it and if there are problems, he will then vote against it.

Councilmember Strommen stated she did not believe anyone moved into her or Councilmember Backous' neighborhood expecting to drive an ATV down the street. She felt there would be serious safety concerns allowing ATV use in her neighborhood.

Mayor Ramsey stated if the City restricts ATVs from driving on streets at night, it should also restrict Harley Davidsons without a muffler, which are louder.

City Administrator Ulrich noted the City's ordinances and State Statutes restrict noise, but they are difficult to enforce. He indicated if the City sees issues, problems, or complaints, this item will be back before the Council.

The majority consensus of the Council was to direct staff to draft a separate ordinance for ATVs/golf carts for Council consideration at a future Work Session.

2.04: Consider Customer Request Management (CRM) Software Purchase

City Administrator Ulrich indicated staff has identified several vendors of CRM software and described how this software would work to track customer requests across departments.

Administrative Intern Brama summarized the staff report and recommendation to purchase GovQA at a cost of \$9,000 the first year and \$6,000 for subsequent years.

Councilmember Elvig stated his support of staff's recommendation, indicating it is a great idea, seems to have vertical integration/blueprint of City staff, tracks clients/requests, and does so in a proactive manner.

Mayor Ramsey stated staff will have to determine how to handle inappropriate questions.

Administrative Intern Brama indicated staff will develop a policy to address those situations.

The consensus of the Council was to support the purchase of GovQA and direct staff to schedule consideration of adoption at a regular City Council meeting.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR / COUNCIL / STAFF INPUT

None

5. ADJOURNMENT

Mayor Ramsey declared the Work Session of the City Council adjourned at 6:57 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, May 1, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous (arrived at 8:36 p.m.)
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey (arrived at 6:06 p.m.)

Members Absent: Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Public Works Director Brian Olson
City Engineer Tim Himmer
Finance Officer Diana Lund
Fire Chief Dean Kapler
Police Chief James Way
Human Resources Representative Colleen Lasher
Senior Planner Tim Gladhill
Planning Intern Patrick Brama
Development Manager Darren Lazan

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 6:05 p.m.

2. TOPICS FOR DISCUSSION

2.01: Consider Requests from Solomon Group Regarding Sunwood Realignment Project

Development Manager Lazan reviewed the staff report.

Councilmember Tossey arrived at 6:06 p.m.

Tony Gleekel, attorney for Solomon Group, stated The COR is an exciting project and Solomon Group feels it is part of this project even though it signed in 2005 for the Ramsey Town Center. He explained they had inherited the Coborn's lease, a great anchor, and leased up the center successfully though there had been some challenges due to the amount of property and unforeseen circumstances with the Ramsey Town Center. Mr. Gleekel indicated that no one

could have controlled what happened in October 2008 but they had borne it fairly well. He noted that with the realignment of Sunwood Drive, they will have less access, and their access and sight lines will be adversely affected. In addition, the property to the north would be a significant threat to Coborn's if it was a 20,000 sq. ft. grocery store and direct competitor. Mr. Gleekel stated Solomon Group would like to propose that the City consider a restrictive covenant to allow Solomon Group to maintain the viability of its center while allowing a transportation change and rezoning. He explained they want to maximize their viability and success while allowing neighboring properties to develop and came up with this concept (restrictive covenant) prior to the property going into private hands. The Coborn lease will be coming up for an option/renewal and it will cause a major problem if Solomon Group loses its major tenant because it also attracts minor tenants. Mr. Gleekel stated Solomon Group understands the City's concerns about limiting the use but would suggest considering a Trader Joe's or Target P-Fresh type of concept but restrict a Cub Foods or WalMart grocery because either would be a deal killer for Solomon Group and Coborn's. He asked the Council to consider that Solomon Group has weathered the Ramsey Town Center and economics, and advocated for members of their LLC and tenants. Mr. Gleekel stated if the Council was inclined to consider a restrictive covenant, it could be negotiated and the "length and breadth" discussed.

Councilmember McGlone asked why this is not being considered during the HRA meeting.

Development Manager Lazan explained staff had moved it to the Council's agenda to assure timely consideration.

City Administrator Ulrich suggested the Council consider the request and make recommendation to the HRA.

Mayor Ramsey stated the City and Solomon Group are in this together and need to look at history, which has value. He noted the City had "stepped up to the plate" and helped with Acapulco because it was of mutual benefit to both parties, which shows the City is interested in the whole picture to include Solomon Group. Mayor Ramsey suggested that will have to continue for both parties to be successful. He stated he understands the concern of Solomon Group but does not think a grocery restriction is needed because it would not be in the best interest of the City to put in another grocery store. Mayor Ramsey stated it would be foolish to allow a Cub Foods or WalMart with full grocery in The COR and common sense dictates it would never happen.

Mr. Gleekel acknowledged the City has been supportive of the Solomon Group to tenant their space, as witnessed by Acapulco. He noted the Comprehensive Plan allows for retail and the market will dictate but if the zoning allows for a WalMart without legislative action, the City may not be able to stop it from coming in.

Mayor Ramsey noted the HRA owns the land so it would not sell the land for that type of application. He also noted that so far, the HRA has not been willing to consider sale of land without a development agreement.

Development Manager Lazan clarified that the HRA is precluded from selling land for speculative purposes. The plans would be viewed and discussed in the public arena and opportunity provided to comment and express concern as sales come forward. Development Manager Lazan stated the HRA has an obligation to see the plans and precisely what will be constructed.

Mr. Gleekel asked if this is the case even if there is no public financing.

Development Manager Lazan answered in the affirmative.

Councilmember McGlone stated the public are the stakeholders of this property but the City cannot control what happens on the other side of Armstrong Boulevard so, theoretically, a grocery store could be closer to Coborn's than The COR property could provide. He stated he was hesitant to consider a grocery restriction, noting the City has already demonstrated its intent in the past to be good neighbors and gone to extraordinary measures to help Solomon's development.

Mr. Gleekel stated it sounds like there is some protection but the secondary sale may be a concern, even if there are City requirements. He stated that the exclusivity has a lot of connotation and Solomon Group understands that asking for this from the City who has supported it takes a lot of "guts," but Solomon Group wants to say it is here for the "long haul." Solomon Group wants its center to remain economically viable.

Councilmember Tossey questioned who is being asked to hold the restriction.

Mr. Gleekel stated if approved, the concept is that the restriction would be a covenant on the land. He noted the tenant does not have a fee ownership but the restriction could be drafted to provide protection to Coborn's so when the time comes, they will renew.

Councilmember Elvig commented on the history of this property and noted that Coborn's has become a destination and is doing well. With regard to the realignment, he stated he does not see that sight lines are being varied and believed the overpass will be a plus for Coborn's. He noted that having a restrictive covenant in The COR means the use will go across from Armstrong Boulevard or Highway 10. He believed the City had done an outstanding job of "playing partner" and while there is an adjustment, he does not think the "sky is falling on it."

Councilmember Strommen stated she agrees with Councilmember McGlone that the HRA only controls a small part and it does not make sense to restrict one small piece because it does not solve the problem Solomon Group is expressing as a concern. She stated she understands the concern about visibility but there are other Coborn's locations with lower visibility and access that are still successful.

Councilmember McGlone commented on the downside of restrictive covenants that can result.

Mayor Ramsey stated he does not fault Solomon Group for asking, but it is the consensus of the Council to not support a restrictive covenant. He stated the City will not consider something that is a direct competitor of Coborn's.

Mr. Gleekel stated they wanted to ask and if a restrictive covenant is drafted right, and not too broad, there is not a bad connotation. With respect to access, his firm has been in business for 50 years and has done a lot of condemnation work and empirical evidence proves that whether you can or cannot view a retail business, the business does not want a change. This request was not to imply the City has not been decent to Solomon Group but Solomon Group does not want change and wants to maintain the status quo as much as possible. He agreed that people who are going to a grocery store know how to get there because it is a destination. Mr. Gleekel stated they will watch what happens, be honest with the HRA to protect their interests, and believe that consistency is important for credibility.

Mayor Ramsey stated the City will be careful with sight lines when negotiating deals.

Development Manager Lazan stated it had been discussed earlier that the HRA is used to hearing about restrictive covenants on deals but it likes to layer those on deals as they happen, that is usually how restrictive covenants evolve.

Councilmember Elvig stated the City appreciates Solomon Group stepping in to pick up the property, noting it is a sophisticated group that drew other tenants. He stated the HRA is interested in growing that area, has already invested millions of dollars, and remains committed to Solomon Group and Coborn's.

Mr. Gleekel stated they are private investors and understand the City is bound to consider the taxpayers and constituents in making its decision.

The consensus of the Council was to not consider a restrictive land covenant.

2.02: Continued Discussion of 2013 Budget

City Administrator Ulrich reviewed previous Council discussion and staff testimony that \$3 million per year is needed to fully fund a road improvement project. Staff looked at the Mayor's proposal and noted the current components are road funding at 16.34%, tax rate at 44.149%, and staffing at 54.84%. He presented staff's recommendation for road reconstruction funding with a five-year phase out of assessments and matching increase of revenues to offset declining assessments. City Administrator Ulrich explained that a 2% growth in the current tax base is needed to accomplish that revenue. It is roughly \$34 million in additional growth, which is equivalent to 120 housing units or several large projects outside the TIF District, which may be achievable four years from now. He then presented a ten-year phase out of assessments, requiring \$17 million in additional growth, which is equivalent to 60 housing units. With this plan, the City would have to assess at 50% for the next three years and risk residents counter petitioning, or delay those projects to 2015. City Administrator Ulrich requested Council direction on the tax rate and amount of staff reductions.

Mayor Ramsey noted this is helpful information and he would be curious about keeping assessments at a 25% level instead of going to zero assessments. He stated he has never been an advocate of completely cutting assessments because he thinks it is unrealistic unless the City raises taxes.

Councilmember Strommen stated she is uncomfortable with pushing off road funding and hoping growth comes because if it does not occur, there would be no revenue for roads. She noted the Council has been debating budget cuts for a long time and if all it is going to do is stick with assessments and hope growth solves it, the Council is back to where it started on this issue. Councilmember Strommen stated she is uncomfortable with that as a solution and thinks it is the Council's job to find a way to fund road improvements. She indicated she is not opposed to a franchise fee with offset from the levy to even it out.

Mayor Ramsey stated he is happy with this route, versus raising taxes now, and the premise to support growth of the City and a larger tax base. If growth does not come, then a future Council will have to raise taxes or make budget cuts. Mayor Ramsey noted things have changed because the Council has put \$1.5 million into roads, which hasn't been done before.

Councilmember Strommen asked what the proposed level of staff cuts mean to services to residents since staffing is already lean. She noted the Council has not yet had that conversation or where cuts will be made.

Mayor Ramsey stated he believed there had to be cuts in both areas, staffing levels and services to residents because the City needs to reconstruct its roads. He agreed the Council needed to look at impact to service levels by reducing staff.

Councilmember Strommen stated the Council has not discussed that but instead is operating under the assumption there is money from staff cuts. She stated she wants the Council to review the list of staff cuts and hear from staff about what it means to level of service.

Councilmember McGlone stated he has learned a lot about trying to put together road projects from being on the Public Works Committee for last 3.5 years. He stated he was not aware of this mammoth problem until after he was elected and asked why road reconstruction funding cannot be taken from the general tax levy since it is a core function of government, the same as police and fire services. He stated maybe it is okay to have an open-ended issue for the future Council to determine the final piece of the "puzzle." Councilmember McGlone stated he would advocate for "taking the bull by the horns" and funding roads from the tax rolls. He stated he understands it would be "painful" to do it all today but he would like to see a graph to get it done someday. Then, a future Council can tax for the balance or tax and institute a franchise fee. Councilmember McGlone stated he wants to do something constructive and responsible to get the City further down the road to solving this issue.

Mayor Ramsey noted with the 30/40/50 formula, if the tax levy goes down then roads do not get done but the formula provides balance and does not get out of whack with too many staff and not fixing roads.

Councilmember Strommen asked if the staffing component at 54.85% reflects current staffing or incorporates cuts.

Finance Officer Lund answered it incorporates staff cuts as detailed in staff's report, not replacing the Deputy City Administrator or Director of Public Works positions, and adds a Management Analyst.

Councilmember Strommen stated she does not know what the cuts mean in cutting services and thinks it is wrong to not fill the economic development position. Other than the \$1.5 million, she stated she does not know what has changed since the Council's conversation started because it still includes assessments and a future Council finding the final solution.

City Administrator Ulrich explained a 25% assessment level has some merit because it protects the City's ability to bond for the project (which requires 20% assessments). It would provide that option and give individual property owners some "skin in the game" for the direct benefit of an improved roadway. He reviewed the scenario for a five-year phase out of assessment, noting \$349,000 would equate to a \$3/month franchise fee. In addition, there would be land proceeds and future growth could bring in additional revenue. He noted if the Council is willing to look at a franchise fee, it could be phased in over time.

Councilmember McGlone noted Statute requires the City to prove that a road improvement results in raising the property value by the amount assessed, which he did not think was the case, and is the problem with the assessment option. He pointed out there would be no need to bond or assess if road projects were part of the tax levy.

Public Works Director Olson pointed out if the City did 3.5 miles of roads per year it would take 6-7 years to get one year of construction done. However, if bonding, the City could get more than 3.5 miles of roads done in a year.

Councilmember McGlone noted there would be need for more bonding in the beginning.

Public Works Director Olson clarified if the desire is to do only 3.5 miles per year, it could be accomplished but it would spread demand out further so roads would be 45-50 years before reconstructed.

Councilmember Strommen noted that could increase project costs.

Councilmember Tossey stated he never supported a franchise fee but has always been okay with assessments; however, he would like to see it lowered from 50% so projects are not counter petitioned. He supported striving for 25% assessments so the City retains the option to bond, if needed in the future. Councilmember Tossey found roads to be a primary obligation of government, the same as fire and police protection, and preferred to see all City obligations done through the levy, not a franchise fee.

Councilmember Elvig pointed out the City has put \$500,000 to \$600,000 into roads each year so the discussion is to add \$900,000 by cutting “skin” from staff in the amount of \$500,000, resulting in a difference of only \$400,000.

Mayor Ramsey stated the record shows the Council has not been taking money from the general fund but from TIF and MSA.

Councilmember Elvig stated only \$275,000 was taken from TIF.

Mayor Ramsey stated he will not support raising taxes but will support a graduated system so five years from now the Council can say it did everything possible with the information it had at the time. He indicated he can support something like this without needing hundreds of thousands of dollars of future development to take care of it.

Councilmember Elvig noted residents on Andrie Street were offered a 20% assessment, or maybe less than that, and it was counter petitioned. He doubted road projects would get done with assessments as long as the counter petition was in place. Councilmember Elvig stated he supports a franchise fee as part of the funding package and does not support staff cuts to the point where a future Council finds serious deficiencies and need to build back staff levels. He stated he partially agrees with Councilmember Strommen that the Council cannot rely only on future growth and while the economy will come around eventually, he supports “biting part of the bullet” now. He pointed out that a future Council will make changes to this plan but he supported getting road funding off the ground now and a franchise fee would create an automatic built-in pool of funds. Currently the City is borrowing from one fund to pay another fund and that can be done again or the City could bond to create a revenue stream. Councilmember Elvig stated he thinks several options need to be components in the solution.

Councilmember Tossey clarified that the issue with Andrie Street was more with sidewalks than with the road project.

Councilmember McGlone agreed that is correct but the road project started as an overlay and became something greater, resulting in a negative with concrete curb and gutter, striping, inconvenience with the project, in addition to not wanting sidewalks. He stated he looked at the 2006 Springsted study on staffing levels, as suggested by a past Councilmember, and found the study mentioned hiring a Fire Marshal, Deputy City Administrator, and other staffing, which was done. He suggested everyone in the City’s staffing organization was on an “upward wave” that is now a downturn. Councilmember McGlone stated he supported the Council’s decision to subcontract building permits to assure that department is never again “in the red.” He stated there is no easy answer and whatever the Council comes up with, he can guarantee not all will agree but he is willing to support getting something started.

Councilmember Strommen stated there is a problem with “fixing as we go,” noting in the past few months there have been discussions about the Sunwood Drive realignment and Armstrong Boulevard funding scenarios but at the end, there is a funding gap and the Council does not know where the needs will be. In addition, in order to get growth, the City needs to build its tax base but it currently has no economic development staff and the tax base will not grow without staff

to make it happen. Councilmember Strommen noted the Public Works Director works tirelessly to find grant funding for road projects but that position is proposed to not be filled. She asked why it is alright to think that a future Council can raise taxes but it is not alright for this Council to raise taxes if it is pledging growth will occur.

Councilmember Tossey stated there may be a philosophical difference but he does not believe the City needs an economic staff person because the private market will come to Ramsey because of availability and the market will drive it, not having a staff person in that position. He stated he does not support creating a revenue stream through franchise fees because once it starts, it will not go away. Councilmember Tossey stated it should be handled through representative government that responded to the citizens; if citizens want roads improved or more police/fire fighters, government should respond at that time, not dedicate funding that will not go away even after roads are done.

Councilmember McGlone noted Councilmember Strommen had addressed pledging growth in the future for road funding but Bolton & Menk projected a growth rate of 260 units/year and the Council is going on the best information it has at this time. He believed the Council can count on some growth since commercial business will come because of good roads, interchanges, and tax base faster than anything one staffer will do.

City Administrator Ulrich asked if he should present staff impacts tonight or at a future Work Session.

Councilmember Tossey stated he would like that information presented tonight so the Council can discuss service impacts. He noted staff has been attending Work Sessions and also needs that information.

Mayor Ramsey stated he relies on the City Administrator to recommend staffing cuts and asked if the level of service will be maintained with the recommended cuts.

City Administrator Ulrich stated there are selected impacts, as previously presented, so it is not the same level of service but it may be an acceptable level of service.

Mayor Ramsey stated the Council has reduced staffing for three years in a row and other than a few people mentioning the boulevard/parks are not mowed as often, he has not heard any complaints about the level of staffing or service. He stated that is an achievement and credit to staff because they have picked up the work.

City Administrator Ulrich presented information contained in the February 14, 2012, meeting packet relating to budget reductions and list of impacts for each reduction.

Councilmember Strommen noted there would be a reduction in the Public Works intern position in addition to the Public Works Director position not being filled and asked about impact to services.

Public Works Director Olson stated he is unable to answer the question about reduction in the level of service or who will take on the burden of the work he has undertaken over the course of the last 12 years.

City Administrator Ulrich stated the budget as presented in the last scenario would use more contracted services and leave a base of staffing, which could include an intern. That proposal has not been reviewed by the Personnel Committee. He explained the Public Works Director position can be filled, replaced with a consultant, partly covered by an engineering intern, or not replaced and the work responsibilities divided among staff. He supported a solution that kept the money in the budget at a full funding level to allow for either scenario because those duties need to be covered in some form or another, especially if road projects are increased.

Councilmember Elvig noted cuts in engineer are a “double whammy” because outsourcing engineering also outsources the revenue stream and budget projections include some revenue from projects.

Finance Officer Lund stated the amount is \$236,000 and she had factored in a \$100,000 decrease.

Public Works Director Olson explained that over the course of the last three years, the Engineering Department has provided more revenue than expenditures.

Councilmember Elvig asked what is the impact to Engineering staff if the Council adds \$3 million of road projects.

Public Works Director Olson expressed concern with staff’s ability to handle that level of road projects if his position is not filled. He explained how project costs and contingencies are calculated, noting some consulting engineers charge a 24% overhead and staff generally charges 18-21% but it can be as low as 15%.

Councilmember Elvig agreed there is an anomaly with engineering and if talking about raising funds, the City can either keep it in house and parlay those funds, or pay it out of house. He stated he wants to make sure the City maximizes its net revenue capture.

Mayor Ramsey stated staff can figure that out but he wants to protect against saying the City needs a level of staff to maximize money coming in for six months but then carrying those staff costs the other six months.

Councilmember Elvig noted that is why staff uses interns and consultants for peaks.

Councilmember McGlone asked why government does not just charge what it costs.

Public Works Director Olson stated from an engineering’s perspective, the beauty of staffing for baseline improvements is the flexibility to stop plan preparation and do plan review if growth starts. In addition, it provides more consistency in how residents are treated. He noted that last year, staff was so busy that it could not respond to resident’s concerns as fast as it wanted to.

Councilmember McGlone noted the Building Department had more expenses than revenues, that went on for years and no one did anything about it. He questioned how much of that goes on in each department the City has. Councilmember McGlone stated he gets compliments on how people love what the Council has done with the Building Department. Those changes were difficult to make but it was the right decision for the public. He stated he wants what is right for the public in all departments, noting government is not in business to turn a profit and needs to be as lean as it can be.

City Administrator Ulrich continued review of recommended reductions and resulting impact to level of service.

Councilmember Strommen referenced the Park budget reduction for trail overlays and asked about increased liability should there be an injury as a result of the deteriorated trail conditions/sink hole, such as exists on the trail along the north side of Sunfish Lake. She noted the staff report indicates the con of this budget cut is: "trails could become hazardous."

City Administrator Ulrich explained the City has a \$10,000 insurance deductible, that is the City's initial liability, and staff has determined the recommended budget cut would not be negligent related to trail maintenance.

Public Works Director Olson stated it was not too long ago the City put no money into trail overlays, and the budget would not be completely cut but reduced by \$15,000 for one year and then decreased by \$5,000 in subsequent years. He stated the worse spots will be fixed this year and then suspended for one year.

Councilmember Elvig stated this is an overall policy item. He noted there was a definite desire by people in the community to not have growth but they wanted things like nice restaurant so that is the base value of The COR. Council policy was developed that if growth comes, it will pay for itself and be of benefit to current residents. Cost structures and fees were established with that policy in mind. Councilmember Elvig noted if the Council is now saying it wants to entice development and lower fees, that is a different conversation and policy shift. He stated one of the Council's main goals was to add more trails and become multi-modal so he is concerned that cutting maintenance will result in trails that are in the same situation as roads.

Mayor Ramsey supported the Council looking at all options and areas that can be cut. He noted that Fire Chief Kapler's name has come up to fill the Public Works Director position as he had previously served in that capacity. He asked Fire Chief Kapler how he would have enough time to dedicate to that position and what would be left undone.

Fire Chief Kapler noted when times are tough you have to make some sacrifices so you take a life that is busy and make it busier. He stated there are more positives/similarities between the Police and Fire Departments than differences and staff will be talking more about shared services.

Mayor Ramsey agreed there are similarities.

Fire Chief Kapler explained that without the Fire Marshal position, the prevention side, licensing, and sirens will not happen and part of a stable community is assuring prevention issues are adequately addressed.

Mayor Ramsey stated he does not disagree with moving the Fire Secretary to Police because efficiencies can be gained. In addition, a Police Secretary was lost so maybe there could be some shared duties.

Councilmember Tossey advised that option was brought up with the Personnel Committee and the decision about filling that position was delayed because the Committee is thinking about putting Fire with Police and eliminating the Fire Secretary. Or, whether to keep that position and then not fill the transcriptionist position.

City Administrator Ulrich stated that is what the internal posting policy accomplishes, filling vacancies.

Councilmember McGlone noted the Police Department Secretary had certain clearances and training, so the Fire Secretary may not have those qualifications.

Fire Chief Way stated that is correct and described the benefit of having two part-time positions with flexibility for extra hours or covering duties should a staff person take some time off.

Councilmember Strommen asked about training time, time loss, or downsides.

Fire Chief Kapler explained they are looking at merging, a two-discipline approach, and it will depend on skill set. He acknowledged there will be a learning curve unless the position is filled internally since there is specialized software training and the position is responsible for generating Fire Department payroll information and tracking attendance requirements.

The Council agreed with the suggestion of Mayor Ramsey to recess this discussion to accommodate those in attendance for the Fire Study and HRA items. Discussion of the 2013 budget resumed after Item 2.03.

Mayor Ramsey recessed the meeting at 7:57 p.m. The meeting was reconvened at 8:03 p.m.

2.03: Discussion on Shared Fire Services

Fire Chief Kapler reviewed the staff report and asked the Council if it had any questions to bring back to the Study Group. He indicated the Study Group would like to know by June 1st whether or not the Council wants to pursue shared services.

Councilmember Strommen stated the decision is whether to continue to explore shared fire services and issues that need to be resolved. She explained the Study Group decided to ask each community whether it was committed to move forward with exploration. Councilmember Strommen stated she had enjoyed being part of this collaborative group, which did exceptional job of standing back and thinking “outside the box” in how to provide a core service in a

different way and being honest about what the issues may be in an objective way. She believed it merits additional exploration.

Councilmember Tossey agreed that continued exploration is a good idea. He stated he thinks there would be cost savings over time and the rewards would outweigh risks in the near future; however, he was concerned should cities like Oak Grove, St. Francis, or East Bethel start to grow and want independence from a joint fire service.

Fire Chief Kapler stated the Study Group talked about that scenario and the goal will be that this is not a one-time opportunity, the “door will be open,” so there needs to be some methodology/process to get in. The Study Group agreed that should someone decide to leave, it would be prudent to have that process already determined and made “painful” so it will persuade people to get over differences and seek a solution.

Councilmember Strommen stated she can see advantages to look at a shared fire service now before those communities are built out and create their own identity. She felt this opportunity may be now more so than down the road. Councilmember Strommen commented on the importance of finding efficiencies to provide services and while she felt shared fire services holds some promise, there are definitely serious issues to address.

Councilmember McGlone stated the object is to raise questions to bring back to the Study Group. He felt this would be like creating a whole new government, maybe an uncontrollable entity, noting the Council currently has direct control over the City’s fire fighters. He asked which entity would be responsible for liability should one fire fighter injure another fire fighter. He also asked who would be responsible for human resource issues and if Ramsey will be required to add personnel. Councilmember McGlone noted the ISO rating is a “hot topic” with fire fighters but home insurance is cheap so he found that issue to be negligible. He also felt Ramsey could realize savings on equipment, such as breathing apparatus, on its own and without creation of a Fire District.

Fire Chief Kapler stated the Fire Department already realizes savings with purchasing consortiums.

Councilmember McGlone stated he is concerned with creating a “new monster government” and assuming liability.

Councilmember Elvig supported continued discussion because he felt shared services is where Ramsey will go at some point. He stated his concern relates to areas of overlap and potential for Ramsey being required to respond to more multi-alarm fires due to the overlap areas. Councilmember Elvig stated he would not agree with pooling Ramsey Fire Department’s hard assets or cash. He indicated the agreement will need to address how a city can leave after-the-fact but he would not want to put the city into a “tail spin” if the joint fire services was not working for them.

Fire Chief Kapler stated there are not a lot of examples of dissolved Fire Districts so he thinks it can work if put together correctly.

City Administrator Ulrich stated he has been involved with two Fire Districts, the Anoka/Champlin Fire District and the Mounds View/Spring Lake Park/Blaine Fire District, noting both have worked very well and been successful. He noted the Council had raised good questions that will be brought back to the Study Group and staff would prepare a resolution for the Council to consider with a recommendation to move forward for additional study.

Councilmember Tossey noted the West Metro Fire District had problems with elected officials. He asked the Study Group to address areas of overlap and how fire rigs would be dispatched.

Fire Chief Kapler explained they will use a box alarm to assure a controlled response.

Mayor Ramsey stated he used to be skeptical about creating a Fire District but would now be happy with a Fire District and Joint Powers Agreements; however, is concerned with how it is structured, staffing, and process to allow a community to join after-the-fact or leave in the future.

The consensus of the Council was to favor continued discussions to move towards a shared fire service.

Mayor Ramsey recessed the meeting at 8:33 p.m. to conduct the Housing and Redevelopment Authority meeting.

Councilmember Randy Backous arrived at 8:36 p.m.

The meeting was reconvened at 9:36 p.m.

2.02: Discussion of 2013 Budget (continued)

Councilmember Strommen asked the Council to consider tabling further budget discussion to a meeting dedicated to this discussion. She noted the late hour and apologized to staff who had been waiting for this discussion to occur.

City Administrator Ulrich continued presentation of the impacts relating to staffing reductions by cutting the Crime Prevention Officer and Community Service Officer.

Councilmember Strommen asked if restoring the Crime Prevention Officer position would allow the City to continue Night to Unite and the Safety Camp, or if those duties could be shifted to another officer.

Police Chief Way explained the Safety Camp is easier to accommodate since it is coordination for a two-day program but the Night to Unit had 50 parties so it requires more staff resources.

Councilmember Backous asked about using volunteers for those activities.

Police Chief Way stated that is a good idea and could work.

Councilmember Tossey stated Night to Unite is an important event and he would volunteer to help.

Police Chief Way stated there are Captains for each Night to Unite party who could be brought in for orientation.

City Administrator Ulrich explained that paraprofessionals were brought on in the first place to off load duties from higher paid professionals.

Councilmember McGlone stated areas have to be cut back, but maybe this is beyond the level of service we need to cut right now.

City Administrator Ulrich noted Night to Unite and the Safety Camp were not core functions but events that serve the public. He reviewed impacts resulting in reducing the Accounting Clerk to three-fourths time and eliminating positions for Marketing Manager, and IT Intern. It was noted the Council had previously directed to not cut the full-time Receptionist. City Administrator Ulrich explained how EDA volunteers and consultants could be utilized to address economic development in addition to some of his time. He noted the Deputy City Administrator and Public Works Director positions are open and not planned to be filled.

Mayor Ramsey suggested the Council further review staff's recommendation and be prepared to address staff cuts at a future meeting.

Councilmember Strommen pointed out there is an accumulative impact, loss of redundancy, loss of backup, and reduced ability to respond if something unanticipated occurs such as a major equipment failure or storm. She felt the City had to be prepared to respond to those situations and indicated she would like discussion on this accumulative effect and how the City can respond to events not anticipated.

City Administrator Ulrich noted there are some things, such as making grant applications, that are difficult to measure. He stated this item would be scheduled for an upcoming Work Session during which the Council can address the appropriate level of staffing and impacts resulting from reductions.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR / COUNCIL / STAFF INPUT

None

5. ADJOURNMENT

Motion by Councilmember McGlone, seconded by Councilmember Backous, to adjourn the Work Session meeting.

The Work Session of the City Council was adjourned at 9:50 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, May 8, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Public Works Director Brian Olson
City Engineer Tim Himmer
Fire Chief Dean Kapler
Environmental Coordinator Chris Anderson
Senior Planner Tim Gladhill
Administrative Intern Patrick Brama
Development Manager Darren Lazan

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 6:03 p.m.

2. TOPICS FOR DISCUSSION

2.01: Residential Curbside Recycling Contract

Environmental Coordinator Anderson reviewed the staff report.

Mike Berkopec, Ace Solid Waste Manager, offered to be a resource, noting they have been in business in Ramsey for a long time and have a lot of experience with different communities. He stated a curbside recycling contract can work in a number of ways and thinks there is a slight preference to let the hauling company do it. He assured the Council that Ace Solid Waste wants it to work and is the dominant hauler in Ramsey.

Councilmember McGlone noted the Council discussed this matter about a year and a half ago and he had advocated for an open hauling system but the Council decided not to do that. He advocated again for the open hauler system because when the cities were mandated to recycle, someone had to get it started so haulers came to the City and asked for help to get it off the ground. Councilmember McGlone noted the City receives SCORE funds to pay for part of a

staff position and asked if those SCORE funds would be received even with an open haul system.

Environmental Coordinator Anderson answered in the affirmative, noting SCORE funds are intended to reimburse the City's cost for the recycling program.

Councilmember McGlone stated his support for an open system, which would result in no loss in revenue and allow Environmental Coordinator Anderson to migrate to community development activities. He indicated the Council will consider a recommendation to recycle oil at its regular meeting tonight. Councilmember McGlone stated other communities offer a drop-off site where their residents can drop off things like oil and metal, and maybe Ace Solid Waste would offer help in that endeavor. He noted the City may be looking at a site for an oil tank and he would support adding several dumpsters.

Mr. Berkopec stated Andover and Coon Rapids are open market and if Ramsey is serious about that option, he would point to the City of Big Lake as an example to consider.

Councilmember Backous stated he supports free market and competition but in this case there are many reasons why contracted/single source makes sense, as detailed in the case report. He stated he usually also advocates for an RFP process to assure the City is taking a fresh look at it but believed that in this situation it is not practical so he would support staff's recommendation in this case.

Councilmember Strommen stated she would favor staying with a contracted mode due to concern with traffic volumes; an open system would require more staff resources to work with multiple entities; and staff information shows this is a good way to maximize what is being recycled. She stated support for continuing with Ace Solid Waste because they have done a good job and been a good partner.

Councilmember McGlone noted Mr. Berkopec has stated his preference is towards the open market, the business community/hauler is leaning toward an open market if it had a choice. In addition, a drop center was mentioned, which could boost tonnage greater than keeping the same hauler. Councilmember McGlone indicated during the Expo many people asked where to take things they have laying around their property.

Councilmember Strommen noted an open system would require staff to shuffle more papers from more entities yet the City would be paid the same amount from the SCORE grant.

Environmental Coordinator Anderson stated households are charged \$2.95/month for recycling and it is paid on a quarterly basis.

Mr. Berkopec indicated Ace Solid Waste has not increased prices since 2007 yet deals with fuel price increases and changes in commodity prices.

Councilmember Tossey stated if it is not opened there should at least be an RFP to allow the market to speak.

Councilmember Wise noted Councilmember Backous had not advocated for an RFP in this situation.

Councilmember Backous confirmed that is his position in this situation because it would take too many resources, noting the current system is working.

Councilmember McGlone pointed out if recycling is opened to the market, the recycling fee would come off the resident's bill.

Mr. Berkopec explained that will depend on the fee charged by the hauler. In their case, they would add a fee to the garbage bill and the resident would then pay that one fee. Mr. Berkopec stated for Ace Solid Waste customers, the fee would remain the same.

Mayor Ramsey noted residents may realize a savings with open recycling if they find a hauler that charges less for recycling.

Councilmember Backous pointed out it would also result in more trucks on the City's roads and increase maintenance costs.

Mayor Ramsey stated he sees problems with both but would support opening competition.

Environmental Coordinator Anderson stated while Anoka County has never actually pursued this, by signing of the recycling agreement, whether by contract or open hauling, the County can pull SCORE funding if the City does not achieve its recycling goal.

Councilmember Tossey asked if the recycling goal is based on tonnage information submitted by Ace Solid Waste.

Environmental Coordinator Anderson explained the Anoka County Board of Commissioners sets the recycling tonnage goal based on population, generally 175 pounds per person. The City also offers recycling events and that tonnage is included in the City's total. He indicated the City had been close to not meeting the recycling goal set by the County prior to going to single source but from 2006 to this year the City has hit the tonnage goal each year.

Councilmember Tossey stated he would principally lean towards open market but meeting the recycling tonnage is a concern.

Mayor Ramsey noted the City did not have a recycling program at that time but it now has such a program and residents are participating.

Councilmember Elvig stated when the City got involved with Ace Solid Waste, he liked having its large wheeled container with a cover. He commented on its ease of use compared to open bins and indicated he recycles more because of it. Councilmember Elvig asked whether the City should look at providing additional containers to encourage recycling. He stated if this service is

put out for RFP, he would not support a vendor that does not provide wheeled containers with lids.

Councilmember McGlone agreed trash lids are needed so it is not blowing around. With regard to the recycling goal, he noted Anoka County sets that tonnage and if it was not met by the City, he doubted the County would keep the City's \$50,100 of SCORE funds.

Environmental Coordinator Anderson advised there are Anoka County communities that do not meet its goal and the County has not withheld those funds but, theoretically, it could do so.

Mayor Ramsey noted if Ace Solid Waste is not interested in signing a recycling contract, then the City would have to go out for RFPs.

Mr. Berkopec clarified that Ace Solid Waste wants to continue to do the work but has a slight preference for an open system. He noted Ace Solid Waste is in a unique situation because "we live here," it needs to do what everyone wants, and has done so since before 2000.

Councilmember Wise asked if they would do it for the same money if the City extended the contract.

Mr. Berkopec stated that is likely and he would be willing to discuss that with staff.

Councilmember Wise stated Ace Solid Waste charges a reasonable fee for quality service and while there are good things for an open market, others need to be regulated. He felt there were some upsides to keeping the same system if at a fair rate.

Councilmember Backous agreed and stated he preferred to address "things that are broken," noting this is not one of them.

Councilmember Strommen asked if this consideration can be placed on the Council agenda, if there is no rate change, rather than being reconsidered at a Work Session.

Councilmember Tossey agreed that the recycling containers provided by Ace Solid Waste do make it easier for residents to recycle, resulting in the City reaching its tonnage goal. He stated concern that a different vendor may not provide that type of container.

Councilmember McGlone again stated his rationale for supporting an open market system.

The majority consensus of the Council was to direct staff to negotiate with Ace Solid Waste, the current service provider, to provide recycling service and place it on the Council agenda.

2.02: Update on Comprehensive Plan Amendment Discussions

Senior Planner Gladhill reviewed the staff report.

The consensus of the Council was to direct staff to withdraw the current Comprehensive Plan Amendment and prepare a revised amendment after re-evaluating forecasts and potential outcomes of those forecasts.

2.03: Discuss New QCTV Program Format – Ramsey, Your Council, Your Community

City Administrator Ulrich reviewed the staff report.

Councilmember Tossey stated he supports this new format.

City Administrator Ulrich stated it would be broadcast every two weeks and staff will draft talking points from existing updates, estimating it will take an hour to an hour and one-half to prepare that information.

Mayor Ramsey asked if it should be a monthly broadcast to save staff time.

City Administrator Ulrich advised that QCTV recommended bi-monthly broadcasts to keep programming current. He suggested filming from The COR room, which has a variety of backgrounds and is quieter.

Mayor Ramsey stated he supports the new format and would like to use different shot locations or perhaps incorporate clips from Council meetings to make it more entertaining.

Councilmember Elvig agreed the new format solves several problems but he would like to see what QCTV plans to incorporate. He asked if every two weeks creates redundancy or if there is enough new information every two weeks to keep it interesting.

City Administrator Ulrich stated this is a busy time of year so there are many items but in the fall of the year it slows down. He noted they could film flushing hydrants with an explanation of that program or other operational items that would also be educational.

Councilmember Strommen agreed it would be good to include some of those services and events like the Farmer's Market.

Mayor Ramsey noted people who watch this programming may also watch Council meetings so this new format would include additional information.

Councilmember McGlone commented on Quad Cities Municipal Report, the informative cable program hosted by Laura Evans, noting this will be a similar format that people will watch because it contains more news and is informative.

Mayor Ramsey suggested Councilmembers help to guide the process by offering ideas on topics/events.

Councilmember Backous agreed that the same people may be watching the Council meetings so he supports the new formatting since it is not redundant. He noted there may be someone who is good at reporting, such as Laura Evans from Quad, who could do some of the programs.

Mayor Ramsey suggested contacting another Councilmember if you are not available when scheduled.

Planning Intern Brama reported the QCTV meeting scheduled for tonight will need to be rescheduled for next Tuesday.

Councilmember Tossey stated he had previously indicated he would not be available next Tuesday or on July 3, 2012.

The consensus of the Council was to support the “Ramsey, Your Council, Your Community” programming format.

3. FUTURE TOPICS FOR DISCUSSION

Noted.

4. MAYOR / COUNCIL / STAFF INPUT

None

5. ADJOURNMENT

Motion by Councilmember Backous, seconded by Councilmember Wise, to adjourn the Work Session meeting.

The Work Session of the City Council was adjourned at 6:49 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth, *TimeSaver Off Site Secretarial, Inc.*

Meeting Date: 06/12/2012

Submitted For: Jo Thieling

By: Jo Thieling, Administrative Services

Information

Title:

Approve Off-Sale Intoxicating Liquor, Off-Sale 3.2% Liquor, On-Sale Intoxicating Liquor, Beer, Sunday Sales, and Optional 2:00 a.m. Closing

Background:

Applications for renewal of the following liquor licenses have been applied for:

OFF SALE INTOXICATING

- Rum River Wine & Spirits Inc. d/b/a Rum River Wine & Spirits
- J.N.T. Enterprises Inc. d/b/a M & L Country Liquors
- Coborn’s Inc. d/b/a Coborn’s Liquor
- Wiser Choice Liquor, Inc. d/b/a Wiser Choice Liquor

ON SALE & SUNDAY INTOXICATING AND 3.2 AND/OR BEER & OPTIONAL 2:00 A.M.

- The Links at Northfork G.C. LLC d/b/a The Links At Northfork*
- Fairways on the Rum Inc. d/b/a McDuff’s Eatery & Pub
- Templer Group Inc. d/b/a JR’s Outpost
- M&M Super Bowl, Inc. d/b/a Super Bowl
- Wells Catering Inc. d/b/a Wells Catering
- SPM Properties, Inc. d/b/a Penalty Box Bar & Grill*
- Acapulco of Ramsey, Inc. d/b/a Acapulco Mexican Restaurant
- Whiskey Jacks of Ramsey LLC d/b/a Whiskey Jack's*

3.2% OFF SALE

- Rademacher Companies Inc. d/b/a Bill’s Superette #8
- Northern Tier Retail LLC d/b/a SuperAmerica #4508
- Yamoutpour, Frank d/b/a Sunfish Express
- Coborn’s Inc. d/b/a Coborn’s Superstore

*All the paperwork was not received by the time of this writing. Since these are renewals are not new license apps, staff is asking Council to approve these licenses contingent upon proper paperwork being received.

Council Action:

Motion to approve Off-Sale Intoxicating Liquor License renewals for Rum River Wine & Spirits, M & L Country Liquors, Wiser Choice Liquors, and Coborn’s Liquor for the 2012 – 2013 license year.

- and -

Motion to approve On-Sale Intoxicating and 3.2 and Sunday, and Beer Liquor License renewals for The Links At Northfork, McDuff’s Eatery & Pub, JR’s Outpost, Super Bowl, Wells Catering, Penalty Box, Acapulco, and Whiskey Jack's for the 2012 – 2013 license year. Optional 2:00 a.m. licenses have been requested by JR’s, Penalty Box and Whiskey Jack's.

- and

Motion to approve 3.2% Off Sale License for Bill’s Superette, SuperAmerica, Sunfish Express and Coborn’s

Superstore for the 2012 – 2013 license year.

Form Review

Form Started By: Jo Thieling

Started On: 06/05/2012

Final Approval Date: 06/08/2011

CC Regular Session

4. 7.

Meeting Date: 06/12/2012

Submitted For: Patrick Brama

By: Patrick Brama, Administrative Services

Information

Title:

Approve the Policy for the Disposition of Surplus City Owned Land

Background:

At the March 27th City Council meeting, Staff presented a process for the sale of City-owned lands. With discussion, the City Council decided to postpone the case and bring it back to a work session for further discussion. Specifically, the City Council was interested in developing a policy for the disposition of surplus City-owned lands. The proposed policy was discussed in two separate City Council worksessions and the Council suggested a number of revisions.

The purpose of this case is to adopt the proposed policy for the disposition of surplus City-owned lands.

Observations:

Attached to this case is the proposed Policy for the disposition of Surplus City Owned Lands. The policy includes a number of items:

- Purpose
- Objective
- Policy Background
- Reasons the City Council would consider the sale of City-owned lands
- Decision Criteria (to be used as a findings of fact)
- Proposed Process

Recommendation:

Adopt the proposed Policy for the Disposition of Surplus City Owned Land.

Funding Source:

Developing the proposed policy is covered under the normal job duties of administration staff.

Council Action:

Adopt the proposed Policy for the Disposition of Surplus City Owned Land.

Attachments

POLICY FOR THE DISPOSITION OF SURPLUS CITY OWNED LAND

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
06/07/2012 03:07 PM
Started On: 06/06/2012

Form Started By: Patrick Brama

Final Approval Date: 06/07/2012

POLICY FOR THE DISPOSITION OF SURPLUS CITY OWNED LAND

City of Ramsey, Minnesota

SECTION I: PURPOSE

- To maximize public benefit and efficient management of City-owned land by considering disposition of lands that are underutilized and/or unneeded for City functions.
- To establish disposition decision criteria and a disposition process.

SECTION II: POLICY STATEMENT

It is the policy of the City of Ramsey to periodically review and inventory City-owned land to identify parcels that are underutilized and/or unneeded for current or anticipated City functions. It also is the policy of the City of Ramsey to consider disposition of lands identified through this inventory according to established criteria and procedure.

SECTION III: PUBLIC BENEFIT

The disposition of underutilized City-owned properties has a number of benefits to the community:

- (1) Reduced City property maintenance costs
- (2) Reduced City liability
- (3) Increased base of taxable parcels
- (4) Reduction of surplus parcels unneeded by the City
- (5) Reduced City resources needed for management of City owned parcels
- (6) Reallocation of parcels to fit in line with long term City needs and goals

SECTION IV: DETERMINATION OF INVENTORY

City Council determines sites potentially available for disposition by means of a City owned land inventory developed by City staff. Upon direction from the City Council, staff shall develop an inventory of City owned land and determine an inventory of properties available for disposition by using the criteria outlined below. If a potential site does not meet the criteria, it shall be deemed potentially available for disposition.

The City shall consider the disposition of real estate if the subject property *does not* meet one of the following potential uses.

POTENTIAL USE CRITERIA

- (1) Economic development purposes; including removal of blighted properties and enhancing the City's tax base
- (2) Public works and maintenance facilities, staging and storage areas
- (3) Public safety facilities, staging and storage areas
- (4) Administrative service facilities
- (5) Public Right of Way; including roads, railroads and airports

- (6) Drainage, wetland and utility easements
- (7) Enhancement of housing stock; including redevelopment purposes
- (8) Public parks, trails and open space

Consideration for the disposition of real estate by the City shall be given based on the *compliance and consistency* with the criteria outlined below.

COMPLIANCE CRITERIA

- (9) City Comprehensive plan
- (10) City Comprehensive water and sewer plans
- (11) City Master parks and trails plan
- (12) City Transportation plan
- (13) Relevant City boards and commissions review
- (14) City Ordinances, Sec. 117-614. - Subdivision Design Standards

POLICY STATEMENT CRITERIA

- (15) The demand for public parks is shifting away from small neighborhood pocket parks to larger regional community parks. The cost to maintain a large number of small neighborhood pocket parks can be greater than the cost to maintain a small number of regional community parks. As such, the City will consider the consolidation of underutilized pocket parks in favor of larger regional parks.
- (16) The City shall not acquire real estate if there is no foreseeable current or future need for the property.

SECTION V: DISPOSITION DECISION CRITERIA

After a property is deemed unneeded and/or underutilized by the City, as outlined in section IV, consideration for disposition will be given by the City Council based on the decision criteria outlined below. This information will be presented to the Council in a findings-of-fact document.

- (1) Public input and feedback regarding a potential disposition
- (2) Maintenance costs
- (3) Title restrictions
- (4) Ability to develop
- (5) Stormwater and drainage limitations
- (6) Proposed future zoning
- (7) Utilities availability
- (8) Long term City needs and plans
- (9) Assessed value
- (10) Method to market and sell property
- (11) Cost to market and sell property

For properties that are legally dedicated, the City will consider the criteria below in addition to the previously stated list:

- (12) Legal costs, quiet title action
- (13) Legal costs, removal of legal dedication from title

The disposition decision criteria listed above, 1-13, will be reported in a findings-of-fact document for each property before it is brought forward to Council for consideration.

SECTION VI: PROCESS

As the City investigates the potential disposition of City owned lands, the process below shall be followed.

STEP 1

NOTIFY PUBLIC (A):

Notify affected property owners of a potential disposition; and, invite interested property owners to attend the next Mayor's Town Hall Meeting.

The purpose of this meeting is to gather, and document, public input on a potential disposition.

Mail notification will be given to property owners within 350' of the subject property 14 days before said meeting.

FINDINGS OF FACT (B):

Staff will create a draft findings-of-fact document to be brought forward to the Council in a worksession meeting. The findings-of-fact document will contain the decision criteria outlined in Section II of this policy.

At the meeting, Council will review the findings-of-fact document and consider moving forward to step two of this process.

Mail notification will be given to property owners within 350' of the subject property 14 days before said meeting.

STEP 2:

ORDINANCE:

An ordinance shall be passed to declare the intentions of the council. The City should not move forward with listing or rezoning properties until an ordinance is passed.

Adopting an ordinance allows the public another opportunity to voice their opinion, for the council to discuss their viewpoints and fulfills the legal process of selling City-owned land.

Mail notification will be given to property owners within 350' of the subject property 14 days before said meetings.

STEP 3:

ZONING AMENDMENT (A):

Third, properties shall be rezoned to the appropriate classification.

Rezoning a property before it is listed makes the property marketable. If properties are not rezoned, it may make potential buyers apprehensive. Furthermore, a rezoned property allows potential property owners to better evaluate the potential use of a property. This process is done by Ordinance and requires a Public Hearing.

For information on rezoning: Chapter 117, Section 49.

COMPREHENSIVE PLAN (B):

If the proposed zoning amendment also changes the land use designation on the Future Land Use Map, a Comprehensive Plan Amendment shall be required. This process requires approval by the Metropolitan Council, often times done administratively. This process is done by Resolution and requires a Public Hearing.

STEP 4:

MARKET:

Marketing properties could be done by means of a realtor, auction or for-sale-by-owner.

The City Council will make this decision on a case by case basis, based on the criteria outlined in the findings-of-fact document.

STEP 5:

PURCHASE AGREEMENT:

City staff and a potential buyer will negotiate a purchase agreement. After an agreement is negotiated, a draft will be brought back to the City Council for discussion and approval.

For properties identified as dedicated or restricted will begin this process at step one, as outlined above. Then, under direction of the City Attorney, follow the process outlined below:

- A. Contact previous property owner(s) to if they would be willing to give their property to the City, with free and clear title
- B. Take property title to court and attempt to remove restrictions
- C. Return property with free and clear title to original owner; or retain City ownership
- D. In the event the City retains ownership, the process would continue at step two outlined above. In the event the property is returned to the original owner, the disposition process would end.

CC Regular Session

4. 8.

Meeting Date: 06/12/2012

By: Jackie Lipski, Finance

Information

Title:

Adopt Resolution #12-06-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of May 17, 2012 through June 6, 2012

Funding Source:

n/a

Council Action:

Motion to Adopt Resolution #12-06-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of May 17, 2012 through June 6, 2012.

Attachments

Bills List 6/12/2012

Resolution 6/12/2012

Form Review

Inbox	Reviewed By	Date
Diana Lund	Diana Lund	06/06/2012 03:20 PM
Kurt Ulrich	Kurt Ulrich	06/07/2012 03:42 PM
Form Started By: Jackie Lipski		Started On: 06/06/2012 02:39 PM
	Final Approval Date: 06/07/2012	

RAMSEY CITY COUNCIL MEETING
6/12/2012
BILLS LIST

DISBURSEMENTS TO BE APPROVED THIS MEETING:

DISBURSEMENT TYPE:	<u>SUBMITTED FOR APPROVAL</u>
Purchase Journal:	
Prepays 5/17/12-6/6/12	149,957.00
Accounts Payable 5/17/12-6/6/12	404,857.17
Payroll 5/24/12	123,343.52
Debt Service	89,406.25

TOTAL SUBMITTED FOR APPROVAL THIS MEETING

\$ 767,563.94

<u>DISBURSEMENTS PREVIOUSLY APPROVED AND PAID:</u>	<u>APPROVED PREV. MTG</u>	<u>2012 Y.T.D.</u>
NET PAYROLL TOTAL	\$ 163,449.54	\$ 1,073,096.41
- CORRECTION TO PAYROLL		
PREPAIDS	161,845.35	2,135,635.08
- PREPAID ADJUSTMENTS		
WIRE TRANSFERS FOR DEBT SERVICE		446,013.75
- CORRECTION TO D.S.		
ACCOUNTS PAYABLE INVOICING - PREVIOUS MEETING:		
- BILLS LIST SUBMITTED	191,679.17	2,313,817.76
ADD (DELETE) BILLS LIST SUBMITTED		
PAY ESTIMATE(S)		1,196,346.00
- CHECKS VOIDED	0.00	0.00

TOTAL CASH DISBURSEMENTS PREVIOUSLY APPROVED

\$ 516,974.06

\$ 7,164,909.00

CITY OF RAMSEY

Council Check Register

5/17/2012 - 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
88423	5/17/2012		110734 CITY OF RAMSEY						
		142.74	CASH PYMT 721662		62730	051712	9601.4651		WATER REVENUE
		142.74							
88424	5/17/2012		100413 RANDALL AND GOODRICH, P L .C						
		275.00	APRIL 2012 CIVIL BILLING		62727	05162012	0295.6304		LEGAL FEES
		1,210.35	APRIL 2012 CIVIL BILLING		62727	05162012	9496.6304		LEGAL FEES
		4,161.75	APRIL 2012 CIVIL BILLING		62727	05162012	0161.6304		LEGAL FEES
		2,925.00	MAY 2012 PROSECUTION BILLING		62728	051512	0161.6304		LEGAL FEES
		8,572.10							
88425	5/22/2012		100297 CENTERPOINT ENERGY						
		91.56	7550 SUNWOOD DR		62731	6702493-5 APR 12	0194.6373		GAS
		91.56							
88426	5/22/2012		106583 DELTA DENTAL PLAN OF MINNESOTA						
		2,152.00	JUNE 2012 DENTAL		62732	4857603	9101.2170		DENTAL/DISABILITY/LIFE
		2,152.00							
88427	5/22/2012		113118 FABYANSKE,WESTRA,HART AND THOM						
		4,950.00	HRA- CONVENIENCE SITE		62733	110161	0295.6315		MISCELLANEOUS PROFESSIO
		4,950.00							
88428	5/22/2012		100291 MET COUNCIL SAC						
		70.95-	APRIL 2012 SAC CHARGES		62735	052112	9602.4356		SEWER AVAILABILITY CHARGI
		7,095.00	APRIL 2012 SAC CHARGES		62735	052112	9602.2083		SAC CHARGES
		7,024.05							
88429	5/22/2012		110469 MN STATE PATROL						
		16.00	2012- 8 INSPECTION STICKERS		62734	051512	0311.6249		MISCELLANEOUS OPERATING
		16.00							
88430	5/22/2012		100678 PETTY CASH						
		3.00	PARKING		62736	052212	0455.6489		OTHER CONTRACTED SERVIC
		5.33	SWITCH FOR PW		62736	052212	0194.6259		BUILDING MAINT/REPAIR SUPI
		6.00	PD PARKING		62736	052212	0211.6331		TRAVEL & LODGING
		9.62	TRAINING		62736	052212	0211.6207		TRAINING SUPPLIES
		11.12	SPRING SHOOT EXPENSES		62736	052212	0211.6207		TRAINING SUPPLIES
		15.17	PD KITCHEN SUPPLIES		62736	052212	0211.6208		MISCELLANEOUS OFFICE SUF
		16.90	PD SUPPLIES		62736	052212	0211.6207		TRAINING SUPPLIES
		67.14							
88431	5/22/2012		106915 SHANK, KENNETH L						
		250.00	SAFETY CAMP-2012 PRESENTER		62737	051712	0237.6249		MISCELLANEOUS OPERATING
		250.00							
88514	5/24/2012		100325 MN BOARD OF AELSLAGID						
		264.00	RENEW- T. HIMMER & L. LINTON		62835	052312	0301.6451		MEMBERSHIP DUES
		264.00							
88516	5/24/2012		107484 MN DEPARTMENT OF HEALTH						
		150.00	WATERMAIN-SUNWOOD REALIGNMENT		62834	052312	9496.6315		MISCELLANEOUS PROFESSIO
		150.00							
88516	5/24/2012		100602 MN POLLUTION CONTROL AGENCY						
		310.00	SUNWOOD DR- SANITARY SEWER		62836	052312	9496.6315	00201220	MISCELLANEOUS PROFESSIO

CITY OF RAMSEY

Council Check Register

5/17/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
		310.00							
88517	5/24/2012		113122 SCHILLER, BILL						
		55.00	DAMAGE/KEY REFUND 30949		62837	052412	9804.1160		KEY & DAMAGE DEPOSIT
		100.00	DAMAGE/KEY REFUND 30949		62837	052412	9804.1160		KEY & DAMAGE DEPOSIT
		155.00							
88518	5/24/2012		100510 VERIZON WIRELESS						
		26.02	APR/MAY 2012 BILLING		62838	2741241621	0301.6323		CELLULAR PHONES
		26.02	APR/MAY 2012 BILLING		62838	2741241621	0111.6249		MISCELLANEOUS OPERATING
		52.04	APR/MAY 2012 BILLING		62838	2741241621	0130.6323		CELLULAR PHONES
		104.08							
88519	5/24/2012		107962 GENESIS EMPLOYEE BENEFITS						
		4,259.90			62822	05231214131212	9101.2176		LIFE/HEALTH-EMPLOYEE
		4,259.90							
88520	5/24/2012		100257 LAW ENFORCEMENT LABOR SRV INC						
		420.00			62577	0508121324572	9101.2177		UNION DUES
		420.00			62826	0523121413122	9101.2177		UNION DUES
		840.00							
88521	5/24/2012		100298 MN AFSCME COUNCIL 5						
		467.91			62578	0508121324573	9101.2177		UNION DUES
		467.89			62827	0523121413123	9101.2177		UNION DUES
		935.80							
88522	5/29/2012		100297 CENTERPOINT ENERGY						
		39.51	14515 E TOWN CNTR APRIL 2012		62839	8782239-1APRIL 12	9601.6373		GAS
		39.51							
88523	5/29/2012		110734 CITY OF RAMSEY						
		19.77	#695579683		62841	052912	9601.4651		WATER REVENUE
		20.00	#655842131		62841	052912	9601.4651		WATER REVENUE
		31.64	#709445780		62841	052912	9601.4651		WATER REVENUE
		71.41							
88525	5/30/2012		112529 TPC TWIN CITIES						
		53.00	GOVT MTG (5)		62842	053012	0111.6335		TRAINING
		53.00	GOVT MTG (5)		62842	053012	0130.6335		TRAINING
		106.00							
88526	5/31/2012		111364 ASSURANT EMPLOYEE BENEFITS						
		1,159.34	JUNE 2012 BILLING		62878	5413144 JUNE 2012	9101.2170		DENTAL/DISABILITY/LIFE
		1,159.34							
88527	5/31/2012		107523 CREATURE ENCOUNTERS						
		200.00	SAFETY CAMP PRESENTOR		62879	053112	0237.6249		MISCELLANEOUS OPERATING
		200.00							
88528	5/31/2012		111818 DEANO'S COLLISION SPECIALISTS						
		582.47	REPAIR 318		62880	33002	9702.6315	00000318	MISCELLANEOUS PROFESSIO
		15,838.20	ACCIDENT 318		62881	33002A	9702.6315	00000318	MISCELLANEOUS PROFESSIO
		16,420.67							

5/17/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
88529	5/31/2012		100681 DWYER, TIMOTHY						
		307.25	SCHOOL REIMBURSEMENTS		62882	053012	0211.6331		TRAVEL & LODGING
		307.25							
88530	5/31/2012		101103 NEXTEL COMMUNICATIONS						
		54.99	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0191.6323		CELLULAR PHONES
		87.36	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0194.6323		CELLULAR PHONES
		100.08	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0111.6249		MISCELLANEOUS OPERATING
		105.22	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0130.6323		CELLULAR PHONES
		196.83	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	9601.6323		CELLULAR PHONES
		203.84	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0311.6323		CELLULAR PHONES
		209.04	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0452.6323		CELLULAR PHONES
		261.80	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0220.6323		CELLULAR PHONES
		269.56	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0301.6323		CELLULAR PHONES
		693.47	APRIL 15-MAY 14-2012 BILLING		62883	570683319-126	0211.6323		CELLULAR PHONES
		2,182.19							
88531	5/31/2012		100413 RANDALL AND GOODRICH, P L .C						
		3,117.60	MAY 2012 PROSECUTION BILLING		62884	053012	0161.6304		LEGAL FEES
		3,117.60							
88532	5/31/2012		109415 SOLBREKK						
		1,603.13	USER/SNAPSHOT/EMAIL		62885	90773	0192.6405		OFFICE & DATA PROCESSING
		113.60	USER/SNAPSHOT/EMAIL		62886	90772	0192.6405		OFFICE & DATA PROCESSING
		1,603.13	EMAIL/RETRIEVAL		62887	90972	0192.6405		OFFICE & DATA PROCESSING
		56.67	EMAIL/RETRIEVAL		62888	90973	0192.6405		OFFICE & DATA PROCESSING
		1,885.13	SCANNER		62889	91189	0192.6281		SMALL TOOLS & MINOR EQUIP
		5,261.66							
88533	6/5/2012		106945 AMERICAN EXPRESS						
		110.00	MEMBER RENEWAL		62921	051612	0280.6241		COMMUNITY POLICING SUPPL
		110.00							
88534	6/5/2012		100297 CENTERPOINT ENERGY						
		45.87	GAS - APRIL 2012		62920	80000014064-2	9601.6373		GAS
		45.87	GAS - APRIL 2012		62920	80000014064-2	9602.6373		GAS
		45.87	GAS - APRIL 2012		62920	80000014064-2	9605.6373		GAS
		83.55	GAS - APRIL 2012		62920	80000014064-2	0452.6373		GAS
		137.62	GAS - APRIL 2012		62920	80000014064-2	0311.6373		GAS
		432.98	GAS - APRIL 2012		62920	80000014064-2	0220.6373		GAS
		791.76							
88535	6/5/2012		100116 CONNEXUS ENERGY						
		764.27	16300 ST FRANCIS BLVD-IRRIG		62922	444931-267449MA Y12	0452.6371		ELECTRIC UTILITIES
		764.27							
88536	6/5/2012		110725 HEDBERG, JUDY						
		55.00	REFUND KEY/DAM DEP RC 30951		62927	060412	9804.1160		KEY & DAMAGE DEPOSIT
		55.00							
88537	6/5/2012		112601 LEGACY CHRISTIAN ACADEMY						
		300.00	SPONSOR-SCRAMBLE FOR STUDENTS		62924	052512A	0295.6246		MARKETING & PROMOTIONS
		300.00							
88538	6/5/2012		100268 LRRWMO CITY OF ANOKA						
		1,875.00	COR WETLAND SEQ. APPLICATION		62929	060512	9605.6315		MISCELLANEOUS PROFESSIO

5/17/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
		1,875.00							
88539	6/5/2012		107244 NORTHLAND TRUST SERVICES INC						
		75,406.25	BONDS 2005B		62925	032112	9340.6611		BOND INTEREST
		14,000.00	BONDS 2007B		62926	032112A	9330.6611		BOND INTEREST
		<u>89,406.25</u>							
88540	6/5/2012		100678 PETTY CASH						
		4.79	CITY COUNCIL DESSERT		62928	060412	0111.6249		MISCELLANEOUS OPERATING
		6.00	PARKING REIMBURSEMENT		62928	060412	0211.6331		TRAVEL & LODGING
		6.66	MILEAGE FOR TRAINING		62928	060412	0211.6334		MILEAGE REIMBURSEMENT
		6.66	MILEAGE FOR TRAINING		62928	060412	0211.6334		MILEAGE REIMBURSEMENT
		8.84	MEAL AT TRAINING		62928	060412	0211.6331		TRAVEL & LODGING
		19.63	REIMBURSEMENT FOR MEALS		62928	060412	0211.6331		TRAVEL & LODGING
		<u>52.58</u>							
88541	6/5/2012		110306 HARTFORD LIFE INSURANCE COMPAN						
		1,284.23	RENEWAL-FF ACCIDENT POLICY		62923	VP 710153	0220.6104		PART TIME-WAGES & SALARIE
		<u>1,284.23</u>							
90212105	5/24/2012		100398 PUBLIC EMPLOYEES RETIREMENT AS						
		50.00			62739	0522121159002	9101.2174		PERA-EMPLOYEE
		50.00			62740	0522121159003	9101.2183		PERA-EMPLOYER
		13,389.99			62830	0523121413126	9101.2174		PERA-EMPLOYEE
		17,853.45			62831	0523121413127	9101.2183		PERA-EMPLOYER
		<u>31,343.44</u>							
94120163	5/24/2012		100301 MN CHILD SUPPORT PAYMENT CNTR						
		339.66			62738	0522121159001	9101.2185		GARNISHMENTS/SUPPORT
		864.42			62828	0523121413124	9101.2185		GARNISHMENTS/SUPPORT
		126.95			62829	0523121413125	9101.2185		GARNISHMENTS/SUPPORT
		<u>1,331.03</u>							
95381670	5/24/2012		107784 VILLAGE BANK						
		277.49			62742	0522121159005	9101.2173		FICA & MEDICARE-EMPLOYEE
		375.73			62743	0522121159006	9101.2182		FICA & MEDICARE-EMPLOYER
		6,861.57			62820	05231214131210	9101.2173		FICA & MEDICARE-EMPLOYEE
		8,975.56			62821	05231214131211	9101.2182		FICA & MEDICARE-EMPLOYER
		18,660.85			62833	0523121413129	9101.2171		FEDERAL WITHHOLDING
		<u>35,151.20</u>							
95655936	5/24/2012		100601 MN DEPT OF REV WH						
		4.90			62741	0522121159004	9101.2172		STATE WITHHOLDING
		7,780.84			62832	0523121413128	9101.2172		STATE WITHHOLDING
		<u>7,785.74</u>							
95850741	5/24/2012		110306 HARTFORD LIFE INSURANCE COMPAN						
		503.73			62574	05081213245712	9101.2176		LIFE/HEALTH-EMPLOYEE
		505.62			62823	05231214131213	9101.2176		LIFE/HEALTH-EMPLOYEE
		45.80			62823	05231214131213	9101.2176		LIFE/HEALTH-EMPLOYEE
		<u>1,055.15</u>							
99042712	5/17/2012		100219 HOME DEPOT COMMERCIAL ACCT PRO						
		29.71	MAR/APRIL BILLING		62726	042712	0194.6249		MISCELLANEOUS OPERATING
		121.93	MAR/APRIL BILLING		62726	042712	0312.6267		OTHER STREET MAINTENANC
		274.15	MAR/APRIL BILLING		62726	042712	0311.6267		OTHER STREET MAINTENANC
		285.90	MAR/APRIL BILLING		62726	042712	0452.6269		LANDSCAPE MATERIALS

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
		711.69							
99051812	5/18/2012		100629 MN DEPT OF REV SALES TX						
		.73	APR12 SALES/USE/TRANS TAX		62729	APR12	9101.4609		OTHER MISCELLANEOUS REV
						SALES/USE/TRANS TAX			
		4.26	APR12 SALES/USE/TRANS TAX		62729	APR12	9101.4308		SALES OF MAPS & PUBLICATI
						SALES/USE/TRANS TAX			
		6.77	APR12 SALES/USE/TRANS TAX		62729	APR12	9101.4305		RENTAL FEES
						SALES/USE/TRANS TAX			
		58.77	APR12 SALES/USE/TRANS TAX		62729	APR12	9601.2085		ANOKA COUNTY TRANSIT TAX
						SALES/USE/TRANS TAX			
		135.40	APR12 SALES/USE/TRANS TAX-METE		62729	APR12	9601.2085		ANOKA COUNTY TRANSIT TAX
						SALES/USE/TRANS TAX			
		401.80	APR12 SALES/USE/TRANS TAX		62729	APR12	9101.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			
		1,981.27	APR12 SALES/USE/TRANS TAX-UB		62729	APR12	9601.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			
		<u>2,589.00</u>							
99052412	5/24/2012		100223 ICMA RETIREMENT TRUST 457						
		2,444.18			62819	0523121413121	9101.2175		DEFERRED COMPENSATION
		<u>2,444.18</u>							
99052451	5/24/2012		111465 STATE STREET BANK						
		2,779.00			62824	05231214131214	9101.2175		DEFERRED COMPENSATION
		<u>2,779.00</u>							
99052454	5/24/2012		111465 STATE STREET BANK						
		383.73			62825	05231214131215	9101.2176		LIFE/HEALTH-EMPLOYEE
		<u>383.73</u>							
		<u>239,363.25</u>	Grand Total						

Payment Instrument Totals

Check Total	153,789.09
Transfer Total	85,574.16
Total Payments	239,363.25

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Amount	
ATLANTA GA 30353-5208					45.00	
Payment Amount					45.00	
113134	AECOM TECHNOLOGY CORPORATION	REFUND EROSION-7205 HIWAY 10	PV	62890 001 09804	5/31/2012 113084	1,500.00
AECOM TECHNOLOGY CORPORATION 4840 COX ROAD GLEN ALLEN MN 23060-6292					Summary Total	1,500.00
Payment Amount					1,500.00	
113127	AISTROP, TRAVIS	MAILBOX DAMAGE	PV	62844 001 09101	5/29/2012 052912	75.00
TRAVIS AISTROP 7646 158TH AVENUE NW RAMSEY MN 55303					Summary Total	75.00
Payment Amount					75.00	
105902	ALEXANDRIA TECHNICAL COLLEGE	RIFLE INSTRUCT.CLAS S-HEMMERICH	PV	62845 001 09101	5/18/2012 0067738	250.00
ALEXANDRIA TECHNICAL COLLEGE 1601 JEFFERSON STREET					Summary Total	250.00
ALEXANDRIA MN 56308-3799					PV 62891 001 09101 5/22/2012 00067786	525.00
Summary Total					525.00	
Payment Amount					775.00	

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Payment Instrument Check Payment
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Payee	Stub	Document	Due	Invoice	Payment				
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
	1318 MCKAY DRIVE NE SUITE 300 HAM LAKE MN 55304								350.00
Summary Total									350.00
Payment Amount									350.00
100035	ANOKA COUNTY CENTRAL COMMUNICATION S	WIRELESS INTERNET APRIL 2012	PV	62744	001	09101	5/10/2012	2012-247	364.53
	ANOKA COUNTY CENTRAL COMMUNICATION S	WIRELESS INTERNET APRIL 2012	PV	62744	002	09101	5/10/2012	2012-247	175.14
Summary Total									539.67
Payment Amount									539.67
100052	ANOKA POLICE DEPARTMENT	ANIMAL CONTAINMENT APRIL 2012	PV	62932	001	09101	5/22/2012	052212	530.00
Summary Total									530.00
Payment Amount									530.00
100063	ASPEN MILLS	SHIRT- K. SKALSKY	PV	62745	001	09101	5/10/2012	121127	43.95
Summary Total									43.95
	ASPEN MILLS 8201 C	HEMMERICH-	PV	62846	001	09101	5/16/2012	121353	17.63

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
	CENTRAL AVE NE SPRING LAKE PARK MN 55432	SILENT KEY							
								Summary Total	17.63
								Payment Amount	61.58
100070 B AND B	HOFFMAN SOD FARMS INC	ROLLS OF SOD/STAPLES	PV	62847	001	09101	5/22/2012	41581	104.52
	B AND B HOFFMAN SOD FARMS INC 22390 GRANT STREET NW ELK RIVER MN 55330							Summary Total	104.52
								Payment Amount	104.52
101084 BATTERIES	PLUS STORE	12 V LEAD BATTERIES	PV	62746	001	09101	5/10/2012	033-838859	110.72
	BATTERIES PLUS STORE 18180 ZANE STREET NW ELK RIVER MN 55330							Summary Total	110.72
								Payment Amount	110.72
112314 BCA CJTE		CLASS- WAY-DWYER-KAT ERS	PV	62848	001	09101	5/9/2012	722	525.00
	BCA CJTE 1430 MARYLAND AVENUE E ST PAUL MN 55106							Summary Total	525.00
								Payment Amount	525.00

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment			
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount		
108635	BEBERGS LANDSCAPE SUPPLY	FOAM GUN AND CLEANER	PV	62747	001	09101	4/25/2012	107913	109.54		
	BEBERGS LANDSCAPE SUPPLY								Summary Total	109.54	
	13535 GROUSE ST NW										
	ANDOVER MN 55304									Payment Amount	109.54
112640	BIO LAWN	EMERALD POND PARK	PV	62748	001	09101	5/7/2012	47188	545.06		
	BIO LAWN								Summary Total	545.06	
	P O BOX 889	FIRE STATION 1	PV	62749	001	09101	5/8/2012	47191	430.71		
	HUDSON WI 54016								Summary Total	430.71	
		RIVERDALE PARK	PV	62750	001	09101	5/8/2012	47186	398.64		
									Summary Total	398.64	
		ALPINE PARK	PV	62751	001	09101	5/8/2012	47190	1,100.81		
									Summary Total	1,100.81	
		SOLSTICE PARK	PV	62752	001	09101	5/8/2012	47189	237.26		
									Summary Total	237.26	
		CENTRAL PARK	PV	62753	001	09101	5/8/2012	47192	2,888.51		
									Summary Total	2,888.51	
		ELMCREST PARK	PV	62754	001	09101	5/9/2012	47187	1,495.18		
									Summary Total	1,495.18	
									Payment Amount	7,096.17	

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Payment Instrument Check Payment
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Number	Payee Name / Mailing Address	Stub Message	Document Ty	Document Number	Document Itm	Document Co	Due Date	Invoice Number	Payment Amount
108513	BLAINE LOCK AND SAFE, INC. 12052 CENTRAL AVE NE BLAINE MN 55434	.6 KEYS- CONSESSION	PV	62755	001	09101	5/18/2012	09589	41.68
								Summary Total	41.68
								Payment Amount	41.68
100647	BOLTON AND MENK, INC 1960 PREMIER DRIVE MANKATO MN 56001-5900	RAM/COMP WATER STUDY	PV	62756	001	09601	4/23/2012	0146914	2,600.00
								Summary Total	2,600.00
								Payment Amount	2,600.00
113133	CALIBER HOMES INC 20123 NOWTHEN BLVD NW ELK RIVER MN 55330	REFUND 7712 147TH LANE NW	PV	62898	001	09252	5/31/2012	053112	7,000.00
								Summary Total	7,000.00
								Payment Amount	7,000.00
104474	CARQUEST AUTO PARTS P O BOX	AIR FILTERS BRAKES/ROTORS	PV	62757	001	09101	5/17/2012	6975-130010	6.95
								Summary Total	6.95
			PV	62758	001	09101	5/17/2012	6975-130012	138.78

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
503589		/ OIL FILTER 643							
	ST LOUIS MO 63150-3589	BRAKES/ROTORS / OIL FILTER 643	PV	62758	002	09101	5/17/2012	6975-130012	14.68
				Summary Total					153.46
		FUSE	PV	62759	001	09101	5/18/2012	6975-130065	2.98
				Summary Total					2.98
		LOCK EASE	PV	62760	001	09101	5/18/2012	6975-130078	2.94
				Summary Total					2.94
		MISC PARTS	PV	62761	001	09101	5/11/2012	6975-129786	33.55
		MISC PARTS	PV	62761	002	09101	5/11/2012	6975-129786	10.13
		MISC PARTS	PV	62761	003	09101	5/11/2012	6975-129786	15.26
				Summary Total					58.94
		STANDARD CAPSULES	PV	62762	001	09101	5/9/2012	6975-129681	38.09
				Summary Total					38.09
		FINANCE CHARGE	PV	62763	001	09101	4/27/2012	042712	2.13
				Summary Total					2.13
		AIR FILTER FOR 353	PV	62899	001	09101	5/24/2012	6975-130297	10.20
				Summary Total					10.20
		MISC SUPPLIES	PV	62900	001	09101	5/23/2012	6975-130271	54.82
				Summary Total					54.82
		MINI LAMP	PV	62901	001	09101	5/23/2012	6975-130244	12.61
				Summary Total					12.61
		CAR WASH	PV	62933	001	09101	5/17/2012	6975-130011	12.27

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment
Number	Name / Mailing Address	Ty Number Itm Co	Date	Number	Amount
SOLUTIONS					
Summary Total					12.27
Payment Amount					355.39
111496	CENTRAL IRRIGATION SUPPLY INC 8 WILLIAMS STREET ELMSFORD NY 10523	MISC SUPPLIES	PV 62764 001 09101 5/18/2012	6023364-00	1,297.67
Summary Total					1,297.67
Payment Amount					1,297.67
106670	CENTRAL POWER DISTRIBUTORS INC 3801 THURSTON AVENUE ANOKA MN 55303	MISC PARTS	PV 62849 001 09101 5/21/2012	970752	84.06
Summary Total					84.06
Payment Amount					45.05
100111	COMMERCIAL ASPHALT COMPANY P O BOX 1480 MAPLE GROVE MN 55311-6480	ASPHALT	PV 62902 001 09101 5/23/2012	974122	45.05
Summary Total					45.05
Payment Amount					129.11
100111	COMMERCIAL ASPHALT COMPANY P O BOX 1480 MAPLE GROVE MN 55311-6480	ASPHALT	PV 62765 001 09101 5/15/2012	051512	589.11
Summary Total					589.11

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Payment Instrument Check Payment
Pay Through Date 12/31/2012.

Payee	Stub	Document	Due	Invoice	Payment				
Number	Name / Mailing Address	Message	Ty	Number	Item	Co	Date	Number	Amount
Payment Amount									589.11
100116	CONNEXUS ENERGY	MAY 2012-STREET LIGHTS	PV	62934	001	09603	5/21/2012	759126-303101 MAY12	10,017.14
Summary Total									10,017.14
Payment Amount									10,017.14
100118	CONSTRUCTION BULLETIN	AD STORM SEWER IMPROV 2012	PV	62850	001	09605	5/17/2012	68290	143.50
Summary Total									143.50
Payment Amount									143.50
100122	COOP'S LOCKSMITH SERVICES	2 KEYS	PV	62766	001	09101	5/17/2012	38797	4.29
Summary Total									4.29
Payment Amount									4.29
100125	COUNTRYSIDE PRINTING INC	BUS CARD- P. BRAMA	PV	62851	001	09101	5/18/2012	31044	40.61

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
	BLAINE MN 55449-4430								540.00
				Payment Amount					540.00
100158	ECM PUBLISHERS INC	LEGAL- CAND. FILINGS	PV	62771	001	09101	5/11/2012	01798221	51.25
	ECM PUBLISHERS INC			Summary Total					51.25
	4095 COON RAPIDS BLVD	LEGAL-WHISKEY JACK ON SALE	PV	62772	001	09101	5/11/2012	01798223	61.50
	COON RAPIDS MN 55433			Summary Total					61.50
		BID STORM SEWER PROJECT 2012	PV	62805	001	09605	5/11/2012	01798222	246.00
				Summary Total					246.00
		HEAR:AMEND CHAPTER 8	PV	62852	001	09101	5/18/2012	01798491	97.38
				Summary Total					97.38
		HEAR-FREE. CHRISTIAN CENTER	PV	62935	001	09804	5/25/2012	01798817	71.75
				Summary Total					71.75
				Payment Amount					527.88
106624	EHLERS AND ASSOCIATES, INC	RE : COR APARTMENTS	PV	62773	001	09214	5/10/2012	344591	585.00
	EHLERS & ASSOCIATES, INC			Summary Total					585.00
	3060 CENTRE POINTE DRIVE								

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee Number	Payee Name / Mailing Address	Stub Message	Document Ty	Document Number	Document Itm	Document Co	Due Date	Invoice Number	Payment Amount
112157	FANFARE PROMOTIONS INC	SHIRTS FOR ICSC EVENT	PV	62774	001	09295	5/14/2012	00028275	460.69
	FANFARE PROMOTIONS INC							Summary Total	460.69
	11311 DAYTON RIVER ROAD DAYTON MN 55327-7500	PENS FOR ICSC	PV	62854	001	09295	5/14/2012	00028280	534.02
								Summary Total	534.02
								Payment Amount	994.71
107099	FASTENAL	MISC PARTS	PV	62775	001	09101	5/15/2012	MNTC886612	1.34
	FASTENAL COMPANY							Summary Total	1.34
	P O BOX 978 WINONA MN 55987	MISC PARTS	PV	62855	001	09601	5/11/2012	MNTC886393	352.99
								Summary Total	352.99
		MISC PARTS	PV	62906	001	09101	5/23/2012	MNTC886960	18.39
								Summary Total	18.39
								Payment Amount	372.72
110760	FIRE SAFETY USA, INC	BLADES	PV	62776	001	09101	5/18/2012	52813	240.00
	FIRE SAFETY USA, INC							Summary Total	240.00
	3253 19TH STREET NW ROCHESTER MN 55901	POWER CORD	PV	62907	001	09101	5/23/2012	52938	48.00
								Summary Total	48.00
								Payment Amount	288.00

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Number	Payee Name / Mailing Address	Stub Message	Document Ty	Document Number	Document Itm	Document Co	Due Date	Invoice Number	Payment Amount	D
111709	FIRST TRANSIT INC	APRIL 2012 RAMSEY STAR EXPRESS	PV	62856	001	09803	5/2/2012	558090412R	52,992.74	
									52,992.74	
									55,504.73	
									55,504.73	
									108,497.47	
									64.13	
									64.13	
									64.13	
									74.00	
									10.00	
									124.45	
									124.46	
									332.91	
									111.77	
									111.77	

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Payee	Stub	Document	Due	Invoice	Payment
Number	Name / Mailing Address	Message	Ty Number Itm Co Date	Number	Amount
	UNIFORMS	PV 62908 001 09101	5/23/2012	1006750339	74.00
	UNIFORMS	PV 62908 002 09101	5/23/2012	1006750339	10.00
	UNIFORMS	PV 62908 003 09101	5/23/2012	1006750339	63.75
	UNIFORMS	PV 62908 004 09101	5/23/2012	1006750339	63.76
		Summary Total			211.51
	MATS	PV 62909 001 09101	5/23/2012	1006750342	92.34
		Summary Total			92.34
		Payment Amount			748.53
100650 GRAINGER	BROOM HANDLE	PV 62780 001 09101	5/9/2012	9823341780	12.87
		Summary Total			12.87
GRAINGER INC DEPT. 806511127	MISC PARTS FOR RAMP	PV 62781 001 09240	5/14/2012	9827383846	47.99
		Summary Total			47.99
PALATINE IL 60038-0001		Payment Amount			60.86
110568 GREEN VALLEY GREENHOUSE	MISC FLOWERS CH	PV 62911 001 09101	5/21/2012	185035	155.23
		Summary Total			155.23
GREEN VALLEY GREENHOUSE 6530 GREEN VALLEY ROAD RAMSEY MN 55303		Payment Amount			155.23
109262 GREENBERG IMPLEMENT INC	2 TRIMMERS	PV 62782 001 09101	5/14/2012	ES09075	653.99
		Summary Total			653.99
GREENBERG IMPLEMENT INC 19745 NOWTHEN	HYDRO OIL	PV 62858 001 09101	5/21/2012	PS76525	42.74

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Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment
Number Name / Mailing Address	Message	Ty Number Itm Co	Date	Number	Amount
ANOKA MN 55303					36.26
				Payment Amount	36.26
100266 LOGIS	APRIL 2012 BILLING	PV 62938 001 09101	5/22/2012	35179	384.00
LOGIS 5750 DULUTH STREET GOLDEN VALLEY MN 55422-4036				Summary Total	384.00
				Payment Amount	384.00
112512 MADDEN,GALANT ER,HANSEN LLP	SERVICES THROUGH 4-30-12	PV 62784 001 09101	5/1/2012	050112	750.30
MADDEN,GALANTER,HANSEN LLP 505 NORTH HIGHWAY 169 SUITE 295 PLYMOUTH MN 55441-6444				Summary Total	750.30
				Payment Amount	750.30
109380 MCGLONE, COLIN	ICSC REIMBURSEMENT S	PV 62913 001 09295	5/25/2012	052512	15.13
COLIN MCGLONE 15890 SAINT ANDREWS LANE NW RAMSEY MN 55303				Summary Total	15.13
				Payment Amount	15.13
100283 MENARDS COON	RETURN-	PD 62785 001 09101	6/12/2012	30231	53.43-

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Payee Number Name / Mailing Address	Stub Message	Document Ty Number Itm Co	Due Date	Invoice Number	Payment Amount
FRIDLEY MN 55432					69.47
				Summary Total	69.47
				Payment Amount	126.31
100289 METRO SALES INC METRO SALES INC 1620 EAST 78TH STEET MINNEAPOLIS MN 55423	COPIER 1ST QUARTER COPIER 1ST QUARTER	PV 62788 001 09101 PV 62788 002 09101	5/8/2012 5/8/2012	458184 458184	738.77 220.67
				Summary Total	959.44
				Payment Amount	959.44
100293 MIDC ENTERPRISES MIDC ENTERPRISES 1450 WEST COUNTY ROAD C ROSEVILLE MN 55113	SOIL MOISTURE SENSOR	PV 62865 001 09101	5/17/2012	10029476	973.23
				Summary Total	973.23
				Payment Amount	973.23
106555 MINNEAPOLIS, CITY OF MINNEAPOLIS FINANCE DEPT P O BOX 77038 MINNEAPOLIS MN 55480-7738	APRIL TRANSACTIONS	PV 62866 001 09101	5/9/2012	400413002945	137.40
				Summary Total	137.40
				Payment Amount	137.40
100303 MINNESOTA CONWAY INC	WET CHEMICAL- K	PV 62789 001 09101	5/16/2012	39762	180.62

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Number	Payee Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
	MINNESOTA CONWAY 575 MINNEHAHA AVE WEST ST PAUL MN 55103-1573								Summary Total 180.62
									Payment Amount 180.62
100326	MN CHIEFS OF POLICE ASSN MN CHIEFS OF POLICE ASSN 1951 WOODLANE DRIVE WOODBURY MN 55125	PERMITS TO ACQUIRE	PV	62940	001	09101	5/22/2012	3393	115.43
									Summary Total 115.43
									Payment Amount 115.43
100602	MN POLLUTION CONTROL AGENCY MINNESOTA POLLUTION CONTROL AGENCY 520 LAFAYETTE ROAD NORTH ST PAUL MN 55155-4194	APRIL 2012 BILLING	PV	62941	001	09230	5/1/2012	7700004675	62.50
									Summary Total 62.50
									Payment Amount 62.50
100341	MTI DISTRIBUTING INC MTI DISTRIBUTING INC SDS 12-1900 P O BOX 86 MINNEAPOLIS MN 55486-1900	PULLEY	PV	62790	001	09101	5/14/2012	842546.00	68.84
									Summary Total 68.84
									Payment Amount 68.84

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Number	Payee Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
100345	NAPA AUTO PARTS ELK RIVER NAPA AUTO PARTS ELK RIVER 17137 YALE STREET NW P O BOX 1041 ELK RIVER MN 55330	OIL FILTER 643	PV	62791	001	09101	5/14/2012	622235	10.93
								Summary Total	10.93
								Payment Amount	10.93
100363	NORTHERN SANITARY SUPPLY CO NORTHERN SANITARY SUPPLY CO 341 COON RAPIDS BLVD MINNEAPOLIS MN 55433	MISC SUPPLIES	PV	62792	001	09101	5/16/2012	155095	152.40
								Summary Total	152.40
								Payment Amount	152.40
103461	NORTHERN TECHNOLOGIES INC NORTHERN TECHNOLOGIES INC 6588 141ST AVENUE N.W. RAMSEY MN 55303	RE:NORTH COMMONS-CMT NORTH COMMONS-GEO	PV	62942	001	09468	5/8/2012	7171	522.00
								Summary Total	522.00
			PV	62943	001	09805	5/8/2012	7172	1,515.00
								Summary Total	1,515.00
								Payment Amount	2,037.00

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
100368	OFFICE DEPOT	OFFICE SUPPLIES	PV	62867	001	09101	4/30/2012	607083795001	87.20
	OFFICE DEPOT			Summary Total					87.20
	P O BOX 70049	OFFICE SUPPLIES	PV	62868	001	09101	4/14/2012	605034360001	14.94
	LOS ANGELES CA 90074-0049			Summary Total					14.94
				Payment Amount					102.14
112759	ON SITE SANITATION INC	DRAW PARK	PV	62794	001	09101	4/27/2012	A-453359	38.17
	ON SITE SANITATION INC			Summary Total					38.17
	95 WOODLYNN AVE	TITTERUD PARK	PV	62795	001	09101	4/27/2012	A-452721	30.46
	ST PAUL MN 55117			Summary Total					30.46
		RIVERBEND PARK	PV	62796	001	09101	4/27/2012	A-452720	30.46
				Summary Total					30.46
		RIVERDALE PARK	PV	62797	001	09101	4/27/2012	A-452719	30.46
				Summary Total					30.46
		FOX PARK	PV	62798	001	09101	4/27/2012	A-452718	30.46
				Summary Total					30.46
		FLINTWOOD TERRACE	PV	62799	001	09101	4/27/2012	A-452717	30.46
				Summary Total					30.46
		ALPINE PARK	PV	62800	001	09101	4/27/2012	A-452715	60.92

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Number	Payee Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
Summary Total									60.92
	ELMCREST PARK		PV	62801	001	09101	4/27/2012	A-452716	121.84
Summary Total									121.84
Payment Amount									373.23
110480	OPUS 21 MANAGEMENT SOLUTIONS	APRIL 2012 BILLING	PV	62869	001	09601	5/14/2012	120416	3,001.32
	OPUS 21 MANAGEMENT SOLUTIONS	APRIL 2012 BILLING	PV	62869	002	09601	5/14/2012	120416	2,401.06
	680 COMMERCE DRIVE SUITE 160	APRIL 2012 BILLING	PV	62869	003	09601	5/14/2012	120416	2,401.06
	WOODBURY MN 55125	APRIL 2012 BILLING	PV	62869	004	09601	5/14/2012	120416	1,800.79
		APRIL 2012 BILLING	PV	62869	005	09601	5/14/2012	120416	2,401.06
Summary Total									12,005.29
Payment Amount									12,005.29
111935	PPL INDUSTRIES	RECYCLE MATTRESSES	PV	62793	001	09604	5/5/2012	0000512	150.00
	PPL INDUSTRIES		Summary Total						150.00
	1179 15TH AVENUE SE MINNEAPOLIS MN 55414		Payment Amount						150.00

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document				Due	Invoice	Payment
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
112920	PRESCRIPTION LANDSCAPE	MOWING 5/9/12	PV	62870	001	09101	5/17/2012	178978	440.00
	PRESCRIPTION LANDSCAPE								Summary Total 440.00
	481 FRONT AVENUE	MOWING 5/2/2012	PV	62871	001	09101	5/10/2012	178530	440.00
	ST PAUL MN 55117								Summary Total 440.00
									Payment Amount 880.00
100403	QUICKSILVER EXPRESS COURIER	RE: F & C CLOSING PAPERS	PV	62872	001	09214	5/31/2012	6455364	31.51
	QUICKSILVER EXPRESS COURIER								Summary Total 31.51
	PO BOX 64417								
	ST PAUL MN 55164-0417								Payment Amount 31.51
100411	RAMSEY ORGANIC INC	BLACK DIRT	PV	62802	001	09101	4/30/2012	4650	181.69
	RAMSEY ORGANIC								Summary Total 181.69
	8702 - 181ST AVENUE NW	BLACK DIRT	PV	62803	001	09101	4/9/2012	4648	54.51
	RAMSEY MN 55303								Summary Total 54.51
		BLACK DIRT	PV	62804	001	09101	5/9/2012	4653	109.01
									Summary Total 109.01
									Payment Amount 345.21
112474	RAMSEY RETAIL	BAL DUE-	PV	62806	001	09290	5/17/2012	0517.12	350.00

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

..... Payee	Stub	.. Document	Due	Invoice	Payment
Number Name / Mailing Address	Message	Ty Number Itm Co	Date	Number	Amount
RENTAL INC	BUSINESS EXPO				
RAMSEY RETAIL RENTAL INC 7533 SUNWOOD DRIVE SUITE 220 RAMSEY MN 55303					Summary Total 350.00
					Payment Amount 350.00
111366 RAMSEY, ROBERT	DATA PLAN REIMBURSEMENT	PV 62873 001 09101	5/29/2012	052912	25.00
ROBERT RAMSEY 9495 164TH LANE NW RAMSEY MN 55303					Summary Total 25.00
					Payment Amount 25.00
100421 REGAL AWARDS AND TROPHIES	2 FIRE PLAQUES	PV 62807 001 09101	5/18/2012	120163	96.19
REGAL AWARDS AND TROPHIES 530 WEST MAIN STREET ANOKA MN 55303					Summary Total 96.19
					Payment Amount 96.19
100921 RETROFIT RECYCLING INC	RECYCLE 2012 SPRING EVENT	PV 62808 001 09604	5/9/2012	0055840	146.80
RETROFIT RECYCLING INC 3855 HWY 14 WEST OWATONNA MN 55060					Summary Total 146.80
					Payment Amount 146.80

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment
Number	Name / Mailing Address	Ty Number Itm Co	Date	Number	Amount
112475 S AND T	OFFICE	PV 62914 001 09101	5/1/2012	01PE0836	120.51
OFFICE	SUPPLIES				
PRODUCTS INC					
S AND T	OFFICE	PV 62914 002 09101	5/1/2012	01PE0836	66.97
OFFICE	SUPPLIES				
PRODUCTS INC					
1000 KRISTEN	OFFICE	PV 62914 003 09101	5/1/2012	01PE0836	59.20
COURT	SUPPLIES				
ST PAUL MN 55110					
		Summary Total			246.68
		Payment Amount			246.68
106398 S W WOLD	EROSION	PV 62813 001 09804	5/22/2012	113052	1,500.00
CONSTRUCTION	REFUND 7574				
INC	163RD AVE				
S W WOLD CONSTRUCTION INC					
9457 HIGHWAY 10 NW SUITE 200					
RAMSEY MN 55303					
		Summary Total			1,500.00
		Payment Amount			1,500.00
101126 SAUTER AND	RE:	PV 62944 001 09468	5/15/2012	3806	40,482.00
SONS INC	PARK/HOUSE				
	LOTS				
SAUTER AND	RE:	PV 62944 002 09468	5/15/2012	3806	18,200.00
SONS INC	PARK/HOUSE				
	LOTS				
6651 141ST AVE NW					
RAMSEY MN 55303					
		Summary Total			58,682.00
		Payment Amount			58,682.00

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee Number	Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
100435	SCHARBER AND SONS INC	MISC PARTS	PV	62809	001	09101	5/14/2012	011098375	250.27
	SCHARBER AND SONS INC							Summary Total	250.27
	13725 MAIN STREET	MISC PARTS FOR 635	PV	62915	001	09101	5/21/2012	011099047	121.40
	ROGERS MN 55374							Summary Total	121.40
								Payment Amount	371.67
100613	SHAW, JOANN	TRAINING-MILE AGE	PV	62810	001	09101	5/21/2012	052112	23.87
	JOANN SHAW							Summary Total	23.87
	8120 154TH AVENUE NW. RAMSEY MN 55303							Payment Amount	23.87
100920	SHERWIN WILLIAMS	SHERSTRIPE ATH WHITE	PV	62811	001	09101	5/11/2012	2577-8	33.92
	SHERWIN WILLIAMS							Summary Total	33.92
	3564 MAIN STREET NW MINNEAPOLIS MN 55448-1002							Payment Amount	33.92
112996	STANTEC CONSULTING SERVICES INC	SERVICES THROUGH 5-4-2012	PV	62812	001	09101	5/9/2012	587170	117.42
	STANTEC CONSULTING SERVICES INC							Summary Total	117.42

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee Number	Name / Mailing Address	Stub Message	Document Ty	Number Number	Item Itm	Co	Due Date	Invoice Number	Payment Amount
Payment Amount									136.08
111742	TWIN CITY WATER CLINIC INC	WATER TESTS APRIL 2012	PV	62916	001	09601	5/1/2012	2248	150.00
Summary Total									150.00
Payment Amount									150.00
109912	ULRICH, KURTIS G.	REIMBURSE FOR ICSC CONF	PV	62876	001	09295	5/29/2012	052912	60.28
Summary Total									60.28
Payment Amount									60.28
100497	UNIFORMS UNLIMITED	BADGE SHIELD- B. BLUML	PV	62814	001	09101	5/8/2012	120984	267.19
Summary Total									267.19
935 NORTH DALE STREET ST. PAUL MN 55103	BADGE-SHIELD		PV	62877	001	09101	5/8/2012	120986	133.59
Summary Total									133.59
	MISC SUPPLIES		PV	62917	001	09101	5/30/2012	123396	108.73
Summary Total									108.73
Payment Amount									509.51

R04570

CITY OF RAMSEY
Create Payment Control Groups

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Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	ltn	Co	Date	Number	Amount
100508	VANCE BROTHERS INC	TACK OIL	PV	62815	001	09101	5/14/2012	22129	147.17
	VANCE BROTHERS INC P O BOX 877366 KANSAS CITY MO 64187-7366								Summary Total 147.17
									Payment Amount 147.17
110473	VERNON COMPANY	ENVIRO EXPO BOTTLES	PV	62816	001	09101	4/29/2012	1799370	244.75
	VERNON COMPANY DEPT C								Summary Total 244.75
	ONE PROMOTION PLACE P O BOX 600 NEWTON IA 50208-2065	ENVIRO EXPO BEND A BOTTLE	PV	62817	001	09101	5/14/2012	1801610	140.82
									Summary Total 140.82
									Payment Amount 385.57
112721	W W GOETSCH ASSOCIATES INC	PARTS FOR LIFT STATION 3	PV	62918	001	09602	5/17/2012	88676	1,761.30
	W W GOETSCH ASSOCIATES INC 5250 WEST 74TH STREET MINNEAPOLIS MN 55439-2226								Summary Total 1,761.30
									Payment Amount 1,761.30
107877	WILSON DEVELOPMENT SERVICES	RE; RELOCATIONS-W ISER/DIV PAV	PV	62947	001	09496	5/14/2012	22761	543.12

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment
Number	Name / Mailing Address	Message	Ty	Number	Amount
	WILSON DEVELOPMENT SERVICES			Summary Total	543.12
	510 N CHESTNUT STREET SUITE 200				
	CHASKA MN 55318				
				Payment Amount	543.12
100539	WRIGHT TIRE SERVICE INC	TRAILER TIRE	PV	62919 001 09101 5/23/2012 078799	89.57
	WRIGHT TIRE SERVICE INC			Summary Total	89.57
	710 WEST MAIN STREET				
	ANOKA MN 55303				
				Payment Amount	89.57
112515	WSB AND ASSOCIATES INC	SUNWOOD/ARMST RONG BLVD	PV	62818 001 09496 5/3/2012 7-2012	45,131.00
	WSB AND ASSOCIATES INC			Summary Total	45,131.00
	701 XENIA AVENUE SOUTH SUITE 300				
	MINNEAPOLIS MN 55416				
				Payment Amount	45,131.00
				Total Amount to be Processed	404,857.17
				Total Number of Payments to be Processed	96


NORTHLAND TRUST SERVICES, INC.
45 South 7th Street, Ste 2000
Minneapolis, Mn 55402

Phone: 612-851-4931
Fax: 612-851-4933

03/21/12

RAMS05B

CITY OF RAMSEY
ATTN: FINANCE OFFICER
7550 SUNWOOD DRIVE NW
RAMSEY MN 55303-5137

107244
9340,664
75,406.25


Re:
CITY OF RAMSEY, MINNESOTA
\$4,335,000 GENERAL OBLIGATION
IMPROVEMENT BONDS, SERIES 2005B

Debt Service			
Principal paydown due	06/15/12	\$	0.00
Interest due	06/15/12	\$	75,406.25

Paying Agent Annual Fee 0.00

=====

Amount Due by Check 6-8-12 \$ 75,406.25

*This earlier date allows for Bank check clearing of 3-5 business days prior to payment date to ensure good funds. Make Checks payable to NORTHLAND TRUST SERVICES, INC.

MN School District wires due 3 days prior to payment date to comply with State Credit Enhancement requirements.

All other Wires due 1-2 days prior to payment date:
Wells Fargo Bank, San Francisco, CA.
ABA#121-000-248
Northland Trust Services, #143-6412710
Ref: Issuer Name

If wiring, please advise by Fax, phone or email:
rstephney@northlandtrust.com or 612-851-4931
vvevea@northlandtrust.com or 612-851-5985
lgiampaolo@northlandtrust.com or 612-851-4932

Thank You for your business!

NORTHLAND TRUST SERVICES, INC.
45 South 7th Street, Ste 2000
Minneapolis, Mn 55402

Phone: 612-851-4931
Fax: 612-851-4933

03/21/12

RAMS07B

CITY OF RAMSEY
ATTN: FINANCE OFFICER
7550 SUNWOOD DRIVE NW
RAMSEY MN 55303-5137

107244
9330.660
14,000
[Signature]

Re:
CITY OF RAMSEY, MINNESOTA
\$945,000 GENERAL OBLIGATION TAX
INCREMENT REFUNDING BONDS, SERIES 2007B

Debt Service

Principal paydown due 06/15/12	\$	0.00
Interest due 06/15/12	\$	14,000.00

Paying Agent Annual Fee 0.00

=====

Amount Due by Check 6-8-12 \$ 14,000.00

*This earlier date allows for Bank check clearing of 3-5 business days prior to payment date to ensure good funds. Make Checks payable to NORTHLAND TRUST SERVICES, INC.

MN School District wires due 3 days prior to payment date to comply with State Credit Enhancement requirements.

All other Wires due 1-2 days prior to payment date:
Wells Fargo Bank, San Francisco, CA.
ABA#121-000-248
Northland Trust Services, #143-6412710
Ref: Issuer Name

If wiring, please advise by Fax, phone or email:
rstephney@northlandtrust.com or 612-851-4931
vvevea@northlandtrust.com or 612-851-5985
lgiampaolo@northlandtrust.com or 612-851-4932

Thank You for your business!

Councilmember _____ introduced the following resolution and moved for its adoption:

RESOLUTION #12-06-XXX

RESOLUTION APPROVING CASH DISBURSEMENTS MADE AND AUTHORIZING PAYMENT OF ACCOUNTS PAYABLE INVOICING RECEIVED DURING THE PERIOD OF MAY 17, 2012 THROUGH JUNE 6, 2012.

WHEREAS, the City of Ramsey Finance Department has made cash disbursements and received accounts payable invoicing during the period of May 17, 2012, through June 6, 2012, in the amount of \$767,563.94; and

WHEREAS, the City Council of the City of Ramsey is required to authorize payment for all disbursement transactions.

NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:

- 1) That the Ramsey City Council hereby approves the cash disbursements made and authorizes payment of the accounts payable invoices as detailed in the attached Bills List for the period May 17, 2012, through June 6, 2012, in the amount of \$767,563.94.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember _____, and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 12th day of June 2012.

Mayor

ATTEST:

City Clerk

Meeting Date: 06/12/2012

Submitted For: Patrick Brama

By: Patrick Brama, Administrative Services

Information

Title:

Adopt the City Land Acquisition Policy

Background:

At the April 10th City Council worksession, staff presented a proposed Policy for the Sale of City Owned Land. In discussing the proposed Policy for the Disposition of City Owned Land, the City Council suggested that staff begin developing a second policy--a City Land Acquisition Policy. The proposed policy was discussed in two separate City Council worksessions and the Council suggested a number of revisions.

The purpose of this case is to adopt the attached proposed City Land Acquisition Policy.

Observations:

The purpose of the attached City Land Acquisition Policy is to establish a set of guidelines and criteria that the City shall consider when reviewing the potential acquisition of real estate.

The attached policy includes the following sections:

- purpose
- objective
- public benefit
- decision criteria

Recommendation:

Adopt the proposed City Land Acquisition Policy.

Funding Source:

Developing the proposed policy is covered under the normal job duties of administration staff.

Council Action:

Adopt the proposed City Land Acquisition Policy.

Attachments

POLICY FOR THE ACQUISITION OF CITY OWNED LAND

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
06/07/2012 03:09 PM
Started On: 06/06/2012

Form Started By: Patrick Brama

Final Approval Date: 06/07/2012

POLICY FOR THE ACQUISITION OF CITY OWNED LAND

City of Ramsey, Minnesota

SECTION I: PURPOSE

To establish a set of guidelines and criteria the City shall review when considering the acquisition of real estate. This policy is meant to act as a formal guide for the City and shall not be considered a legal requirement.

SECTION II: POLICY STATEMENT

The objective of this policy is to limit the City from acquiring real estate that is unneeded for either current or foreseeable future demands.

SECTION III: PUBLIC BENEFIT

There are a number of benefits to the public in limiting the City from acquiring unneeded real estate.

- (1) Reduced future City property maintenance costs
- (2) Reduced future City liability
- (3) Preservation of tax base
- (4) Reduced future City resources needed for management of City owned parcels

SECTION III: DECISION CRITERIA

The following three sub sections, including potential use, compliance and policy statements, outline the criteria that shall be considered before the City acquires real estate. Each criterion shall be considered both for current and future demand.

The City shall consider the acquisition of real estate if the subject property meets one of the following potential uses.

POTENTIAL USE CRITERIA

- (1) Economic development purposes; including removal of blighted properties and enhancing the City's tax base
- (2) Public works and maintenance facilities, staging and storage areas
- (3) Public safety facilities, staging and storage areas
- (4) Administrative service facilities
- (5) Public Right of Way; including roads, railroads and airports
- (6) Drainage, wetland and utility easements
- (7) Enhancement of housing stock; including redevelopment purposes
- (8) Public parks, trails and open space

- (9) Dedication of land through the subdivision process; including unrestricted land to be used in lieu of a cash contribution

Consideration for the acquisition of real estate by the City shall be given based on the compliance and consistency with the criteria outlined below.

COMPLIANCE CRITERIA

- (10) City Comprehensive plan
- (11) City Comprehensive water and sewer plans
- (12) City Master parks and trails plan
- (13) City Transportation plan
- (14) Relevant City boards and commissions review
- (15) City Ordinances, Sec. 117-614. - Subdivision Design Standards

The City shall consider the acquisition of City owned land based on the policy statements outlined below

POLICY STATEMENT CRITERIA

- (16) The demand for public parks is shifting away from small neighborhood pocket parks to larger regional community parks. The cost to maintain a large number of small neighborhood pocket parks can be greater than the cost to maintain a small number of regional community parks. As such, the City will consider the consolidation of underutilized pocket parks in favor of larger regional parks.
- (17) The City shall not acquire real estate if there is no foreseeable current or future need for the property.
- (18) Regarding the dedication of parks and open spaces, unless it is desired to acquire new parkland, the City shall favor the dedication of cash over the dedication of real estate. If real estate is dedicated the City shall favor an unrestricted title over restricted title.

CC Regular Session

4. 10.

Meeting Date: 06/12/2012

By: Colleen Lasher, Administrative Services

Information

Title:

Adopt Resolution #12-06-XXX to Accept the Resignation of the Deputy City Clerk

Background:

Ms. Amy Dietl submitted a letter of resignation dated May 15, 2012 with her last day of employment being June 1, 2012.

Staff is not requesting to replace the Deputy City Clerk position and has identified alternative means to address the work of Ms. Dietl. The work distribution plan will be reviewed as a separate case.

Ms. Dietl's letter of resignation is attached.

Funding Source:

No funding is required.

Council Action:

Motion to approve resolution # 12-06-XXX to accept the Deputy City Clerk's resignation.

Attachments

Letter of Resignation
Resolution

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

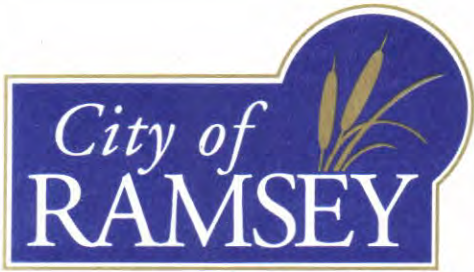
Date

06/07/2012 02:16 PM

Form Started By: Colleen Lasher

Started On: 06/04/2012 08:56 AM

Final Approval Date: 06/07/2012



7550 Sunwood Drive NW • Ramsey, Minnesota 55303
City Hall: 763-427-1410 • Fax: 763-427-5543
www.cityoframsey.com

May 15, 2012

Kurt Ulrich
City of Ramsey
7550 Sunwood Drive NW
Ramsey, MN 55303

Dear Kurt,

I am writing to inform you that I will be resigning my position as Deputy City Clerk effective June 1, 2012.

I have accepted a position with the City of Arden Hills. Although my decision was not an easy one to make, it is one that will allow me to further my career and meets my goal of becoming a City Clerk.

I am very grateful for the opportunity to have worked for the City of Ramsey. In the course of the ten years I have been here, I have learned a lot and value the relationships I have made. I wish the City of Ramsey continued success.

Sincerely,

A handwritten signature in blue ink that reads "Amy Dietl".

Amy Dietl
Deputy City Clerk

Councilmember introduced the following resolution and moved for its adoption:

RESOLUTION #

**RESOLUTION TO ACCEPT THE
RESIGNATION OF THE DEPUTY CITY CLERK**

WHEREAS, Ms. Amy Dietl, the Deputy City Clerk, submitted a letter of resignation on May 15, 2012; and

WHEREAS, staff recommends accepting Ms. Dietl's resignation; and

WHEREAS, staff has identified an alternative means to address the Deputy City Clerk's work and is not requesting to replace the position.

**NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE
CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:**

Accept Ms. Amy Dietl's resignation as the Deputy City Clerk.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember , and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 12th day of June 2012.

CC Regular Session

4. 11.

Meeting Date: 06/12/2012

By: Colleen Lasher, Administrative Services

Information

Title:

Adopt Resolution #12-06-XXX to Accept the Resignation of the City Engineer

Background:

Mr. Tim Himmer submitted a letter of resignation dated June 5, 2012 stating that his last day of employment will be June 29, 2012.

Per the City Council's authorization at the June 5, 2012 special city council meeting, staff will begin a recruitment to replace the City Engineer position immediately.

Mr. Himmer's letter of resignation is attached.

Recommendation:

Accept the City Engineer's resignation.

Funding Source:

No funding is required.

Council Action:

Motion to approve resolution # 12-06-XXX to accept the City Engineer's resignation.

Attachments

Letter of Resignation
Resolution

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
06/07/2012 03:09 PM
Started On: 06/06/2012 11:45 AM

Form Started By: Colleen Lasher

Final Approval Date: 06/07/2012

June 5, 2012

Kurt Ulrich, City Administrator
City of Ramsey
7550 Sunwood Drive NW
Ramsey, MN 55303

Dear Mr. Ulrich:

Per our conversation last week, I am writing to formally notify you that I am resigning from my position as City Engineer with the City of Ramsey. As agreed my last day of employment will be June 29, 2012.

This was not an easy decision to make, on my part. The past 5 years have been both challenging and rewarding. I'm honored to have worked alongside a very talented and dedicated staff, and will leave the City of Ramsey proud of the accomplishments we have completed together as a team. My experience at the City has taught me a lot and prepared me well for the next step in my career development; I am extremely thankful for the opportunities provided to me.

I wish you and the City of Ramsey all the best, and will assist in any way possible during this transition. I leave with a heavy heart and will miss many good friends and colleagues.

Sincerely,

A handwritten signature in black ink, appearing to read 'Tim Himmer', with a long horizontal flourish extending to the right.

Tim Himmer

cc: Coleen Lasher

Councilmember introduced the following resolution and moved for its adoption:

RESOLUTION #

**RESOLUTION TO ACCEPT THE
RESIGNATION OF THE CITY ENGINEER**

WHEREAS, Mr. Tim Himmer, the City Engineer, submitted a letter of resignation on June 5, 2012; and

WHEREAS, staff recommends accepting Mr. Himmer's resignation; and

WHEREAS, per City Council direction on June 5, 2012, staff will begin a recruitment for a new City Engineer immediately.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:

Accept Mr. Tim Himmer's resignation as the City Engineer.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember , and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 12th day of June 2012.

CC Regular Session

4. 12.

Meeting Date: 06/12/2012

By: Diana Lund, Finance

Information

Title:

Adopt Resolution #12-06-XXX Authorizing a Fixed Rate Membership in the 4M Fund

Background:

The city has been a member of the 4M fund since the 1990's in its money market account. The 4M fund is currently holding the bond funds related to The Residence at the COR apartment project. Approximately \$386,000 of the bond proceeds are attributed to capitalized interest which need to be retained until December 2014.

The city would like to take advantage of a higher rate of interest for this time period, which is offered through the 4M fund's fixed rate program. To become a member of this program, the city just needs to adopt a resolution authorizing this membership.

Council Action:

Motion to adopt Resolution Authorizing Membership the the 4M Fund's Fixed Rate Program.

Attachments

Membership in 4M's Fixed Rate Fund

Form Review

Inbox	Reviewed By	Date
Kurt Ulrich	Kurt Ulrich	06/07/2012 04:25 PM
Form Started By: Diana Lund		Started On: 06/07/2012 09:04 AM
		Final Approval Date: 06/07/2012

Resolution Authorizing or Reauthorizing Membership in the 4M Fund

WHEREAS, Minnesota Statutes (the Joint Powers Act) provides that governmental units may jointly exercise any power common to the contracting parties; and

WHEREAS, the Minnesota Municipal Money Market Fund (the 4M Fund) was formed in 1987, pursuant to the Joint Powers Act and in accordance with Minnesota Investment Statutes, by the adoption of a joint powers agreement in the form of a Declaration of Trust; and

WHEREAS, the Declaration of Trust, which has been presented to this Council, authorizes municipalities of the State of Minnesota to become Participants of the Fund and make use from time to time including the 4M Liquid Asset Fund, the 4M Plus Fund, the Fixed Rate Program, and other Fund services offered by the Fund; and

WHEREAS, this Council deems it to be in the best interest for the municipality to make use of, from time to time, the approved services provided by the 4M Fund's service providers including the Investment Advisor and Administrator, RBC Global Asset Management, the Sub Administrator, PMA Financial Network, Inc. or the Fixed Rate Investment Program Administrators, PMA Financial Network, Inc. and PMA Securities, Inc., and the Custodian, U.S. Bancorp N.A., ("Service Providers") and/or their successors.

WHEREAS, this Council deems it advisable for this municipality to enter into the Declaration of Trust and become a Participant of the Fund for the purpose of joint investment with other municipalities so as to enhance the investment earnings accruing to each; now, therefore BE IT RESOLVED AS FOLLOWS:

Section 1. This municipality shall become (or renew its membership as) a Participant of the Fund and adopt and enter into the Declaration of Trust, a copy of which shall be filed in the minutes of this meeting. The appropriate officials are hereby authorized to execute those documents necessary to effectuate entry into the Declaration of Trust and the participation of all Fund programs.

Section 2. This municipality is authorized to invest monies from time to time and to withdraw such monies from time to time in accordance with the provisions of the Declaration of Trust. The following officers of the municipality or their successors are designated as "Authorized Officials" with authority to effectuate investments and withdrawals in accordance with the Declaration of Trust:

DIANA LUND/FINANCE DIRECTOR

Print Name/Title Signature

Print Name/Title Signature

Print Name/Title Signature

(Additional names may be added on a separate list. The treasurer shall advise the Fund of any changes in Authorized Officials in accordance with Fund procedures.)

Section 3. The Trustees of the Fund are designated as having official custody of those monies invested in accordance with the Declaration of Trust.

Section 4. That the municipality may open depository accounts, enter into wire transfer agreements, safekeeping agreements, third party surety agreements securing deposits, collateral agreements, letters of credit, lockbox agreements, or other applicable or related documents with institutions participating in Fund programs including U.S. Bancorp N.A. or its successor, or programs of PMA Financial Network, Inc. or PMA Securities, Inc. for the purpose of transaction clearing and safekeeping, or the purchase of certificates of deposit ("CDs") and that these institutions shall be deemed eligible depositories for the municipality. PMA Financial Network, Inc. and PMA Securities, Inc. and their successors are authorized to act on behalf of this municipality as its agent with respect to such accounts and agreements. Monies of this entity may be deposited in such depositories, from time to time in the discretion of the Authorized Officials, pursuant to the Fund's Programs available through its Services Providers.

It is hereby certified that the Council of THE CITY OF RAMSEY adopted this Resolution at a duly convened meeting of the Council held on the ____ day of _____, 201_, and that such Resolution is in full force and effect on this date, and that such Resolution has not been modified, amended, or rescinded since its adoption.

Secretary to the Council

Dated: _____

Meeting Date: 06/12/2012

Submitted For: MaryJo Warner

By: MaryJo Warner, Engineering/Public Works

Information

Title:

Report from Public Works meeting dated May 15, 2012.

Background:

The Public Works Committee held its regular meeting on May 15, 2012 and discussed the following seven cases:

Case #5.1: Consider Stormwater Improvements at 149th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns

This is being presented as a separate case at tonight's meeting.

Case #5.2: Consider Stormwater Improvements at Rum River Hills Golf Course - A Continuation of Discussion Related to 2011 Flooding Concern

This is being presented as a separate case at tonight's meeting.

Case #5.3: Consider Stormwater Improvements at 148th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns

This is being presented as a separate case at tonight's meeting.

Case #5.4 Consider Installation of Stop Signs and an In-Street Pedestrian Crossing Sign Near 150th Lane NW and Ute Street

City Engineer Tim Himmer reviewed in the staff report that on April 3, 2012 staff received a petition signed by twelve property owners in the neighborhood near the intersection of 150th Lane NW and Ute Street NW. They are requesting the installation of stop signs at this intersection, consideration for the installation of an in street pedestrian crossing at the location of the existing City trail crossing on 150th Lane, and reduction of the speed limit on 150th Lane to 15mph.

Motion to recommend to the City Council to install pedestrian signage on either side of the existing trail crossing along 150th Lane to push them out far enough in advance to caution of pedestrian crossing ahead, and review the sight lines in the area at the approaches.

It was further discussed that the property owners could eliminate the concern associated with sight lines by voluntarily removing the existing trees within their properties that are causing the major concern.

Case #5.5: Discuss Trail Easement at 15620 Krypton Street NW

City Engineer Tim Himmer reviewed in the staff report that the prospective buyers of the property at 15620 Krypton Street are concerned about having a trail through the center of the backyard, and are requesting that the City relocate the trail into the easement along the common lot line along the east side of the property.

Staff reviewed the property files and discovered that this trail was intended for a dual purpose; a maintenance access to the City's sanitary sewer system, and a pedestrian trail connection.

City Engineer Himmer summarized three separate options, as summarized in a letter written by the prospective property buyer: purchase of the pedestrian trail easement at a reduced price now, do nothing and deal with it at a later date, or the City could come through and acquire the trail easement through eminent domain.

Motion to recommend to the City Council, that upon closing on the property, the City enter into an agreement with new homeowners whereby they would dedicate a 7.5' wide pedestrian trail easement along their easterly lot line in exchange for the City removing the existing bituminous trail that falls outside of this easement area and reestablishing the turf.

Case #5.6: Winter Maintenance Summary

Street Supervisor Riemer reviewed in the staff report a comparison between the last two seasons. The Committee wanted to discuss why the last snow event in 2012 took two days to clean up.

Staff explained one of the issues was periodically the tandems would get thrown across the road and it was characterized as wet concrete. There was also discussion of the equipment breakdown of a broken serpentine belt and a hydraulic hose rupture on separate trucks.

Street Supervisor Riemer gave a breakdown of the calls for the year as:

6-mailbox damage, 8-sod damage, 61 misc. calls.

No action taken, for discussion purposes.

Case #5.7: The Use of AVL System in Public Works

Public Works Street Supervisor Riemer explained the Automatic Vehicle Locator system and how it would be a benefit to the City and its residents. He reviewed the fleet management package that could come with it. The abbreviated costs are upfront costs for hardware and operator training for 15 snow removal vehicles at about \$9112.00. The recurring monthly costs for the system is \$259.60/month or \$3,115.00/yr.

Motion to recommend to City Council to direct staff to continue to gather information and pricing for consideration in the 2013 budget.

Recommendation:

Staff recommends ratifying the recommendation of the Public Works Committee.

Council Action:

Motion to ratify the recommendation of the Public Works Committee.

Attachments

Agenda 05.15.12

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Tim Himmer	06/05/2012 12:53 PM
Kurt Ulrich	Kurt Ulrich	06/07/2012 02:29 PM

Form Started By: MaryJo Warner Started On: 06/04/2012 01:44 PM

Final Approval Date: 06/07/2012

City of Ramsey
Agenda
Public Works Committee
Tuesday May 15, 2012
6:00 pm or immediately following Special City Council meeting
Lake Itasca Room, 7550 Sunwood Drive NW

- 1. Call to Order**
- 2. Citizen Input**
- 3. Approve Agenda**
- 4. Approve Minutes**
 1. Approve Public Works Committee meeting minutes dated March 20, 2012.
- 5. Committee Business**
 1. Consider Stormwater Improvements at 149th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns
 2. Consider Stormwater Improvements at Rum River Hills Golf Course - A Continuation of Discussion Related to 2011 Flooding Concerns
 3. Consider Stormwater Improvements at 148 th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns
 4. Consider the Installation of Stop Signs and an In-Street Pedestrian Crossing Sign Near 150th Lane NW and Ute Street NW
 5. Discuss Trail Easement at 15620 Krypton Street NW
 6. Winter Maintenance Summary
 7. The Use of AVL Systems in Public Works
- 6. Committee/Staff Input**
- 7. Adjournment**

Public Works Committee

4. 1.

Meeting Date: 05/15/2012

Submitted For: MaryJo Warner

By: MaryJo Warner, Engineering/Public Works

Title:

Approve Public Works Committee meeting minutes dated March 20, 2012.

Background:

The Public Works Committee held its regular meeting on March 20, 2012. Please note there was no meeting held in April, therefore there are no minutes for approval for that month.

Notification:

Observations:

Funding Source:

n/a

Staff Recommendation:

Committee Action:

Motion to approve Public Works Committee meeting minutes date March 20, 2012.

Attachments

Minutes 03.20.12

Form Review

Inbox	Reviewed By	Date
Brian Olson	Brian Olson	05/08/2012 09:59 AM
Kurt Ulrich	Kurt Ulrich	05/10/2012 01:58 PM
Form Started By: MaryJo Warner		Started On: 05/08/2012 09:34 AM
Final Approval Date: 05/10/2012		

**PUBLIC WORKS COMMITTEE
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Public Works Committee conducted a regular meeting on Tuesday, March 20, 2012 at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Chairperson Randy Backous
 Councilmember David Elvig
 Councilmember Colin McGlone

Also Present: City Administrator Kurtis G. Ulrich
 Deputy City Administrator Heidi Nelson
 Public Works Director Brian Olson
 City Engineer Tim Himmer
 Street Supervisor Grant Reimer
 Park Supervisor Mark Riverblood
 Civil Engineer Leonard Linton
 City Attorney William Goodrich
 Mayor Bob Ramsey
 Councilmember Sarah Strommen
 Councilmember Jason Tossey
 Councilmember Jeff Wise
 Development Manager Darren Lazan

CALL TO ORDER

Chairperson Backous called the regular meeting of the Public Works Committee to order at 5:00 p.m.

CITIZEN INPUT

There was none.

APPROVE AGENDA

Motion by Councilmember McGlone, seconded by Councilmember Elvig, to approve the agenda, as presented.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers McGlone and Elvig.
Voting No: None.

APPROVE MINUTES

Motion by Councilmember McGlone, seconded by Councilmember Elvig, to approve the minutes from the February 21, 2012, Public Works Meeting.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers McGlone and Elvig. Voting No: None.

COMMITTEE BUSINESS

Case #1: Consider Storm Sewer Improvements on Sodium Street – A Continuation of Discussions Related to 2011 Flooding Concerns

City Engineer Himmer reviewed the staff report and described the proposed project to address drainage on the 16756 Sodium Street property.

Joel Kamrowski, 16756 Sodium Street, asked about the easement to put in a culvert to direct water to the pond and what will be done to prevent water from backing up farther into his yard.

City Engineer Himmer advised the City, along with the Lower Rum River Water Management Organization, will undertake a wetland mitigation project across the street that would remove the high ground to create wetlands, which will provide some relief. Also, the City cleaned the west side ditch so the water can drain to the County ditch on the south side of the park. City Engineer Himmer indicated he can't say Mr. Kamrowski's property will never experience high water but the downstream has been addressed and the wetland mitigation project will also help.

Mr. Kamrowski stated he installed rain tile and still his back yard is flooded this spring with no snow.

City Engineer Himmer explained that easements were not taken with older plats and if the City has to purchase those types of easements, it would be a large expenditure and require more discussion. He noted that until there is regional ponding, it is difficult to fix these situations but if the pond fills up there is relief downstream.

Mr. Kamrowski stated his foundation is cracked and the pad in front of his garage has settled and cracked.

City Engineer Himmer explained it may be the low area has ponded and is backing up or it may be water flowing to the pipe.

Mr. Kamrowski stated it appears most is coming from the north and working towards the south. He thought that once water is in the swamp, it stays there.

City Engineer Himmer indicated the proposed project will get water from the south property line and provide positive drainage.

Chairperson Backous asked if staff has addressed the easement issue with Mr. Kamrowski.

City Engineer Himmer stated staff has not yet talked with the property owner about the easement because staff was not sure this project would be supported by the Public Works Committee. He asked if this is a reasonable project, noting the project cost will be covered by the Storm Water Utility but does not include payment for an easement. He asked Mr. Kamrowski if he would be willing to give a 10-foot easement so the City can do the project, if approved by the City Council on April 10, 2012.

Mr. Kamrowski stated it sounds like a good start.

City Engineer Himmer stated the project proposal is included in the staff report

Councilmember Elvig asked if creating drainage will also create a wicking effect of surface water.

City Engineer Himmer stated the project does not include grading up to the house and will create positive drainage. However, if there is a dip in the elevation of the Kamrowski yard, they may have to fill it in.

Councilmember Elvig asked about drain tile.

City Engineer Himmer explained there would not be a hard pipe, just a culvert.

Ms. Kamrowski, 16756 Sodium Street, stated they already have water issues on the back side and she believes the pond overflows into the back yard, which has already wrecked their garage.

City Engineer Himmer stated he had asked if the water gets to the pond and backs up, or the water flowing to the pond is causing the problem. He clarified this project will not grade the Kamrowski property but will bring the water to the low side and the Kamrowskis could do some grading or filling upstream to force the water.

Motion by Councilmember Elvig, seconded by Councilmember McGlone, to recommend that the City Council include the work at 16756 Sodium Street (Installing a culvert under Sodium Street, regrading the west ditch of Sodium Street, constructing a berm to keep the water in the ditch, and replacing the existing driveway culvert. The ditch would be graded to drain to the south and a culvert would be installed near the south property line to drain the water to the west.) in the proposed 2012 Sealcoating Program.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers Elvig, and McGlone.
Voting No: None.

Case #2: Consider Storm Sewer Improvements on 163rd Lane – A Continuation of Discussions Related to 2011 Flooding Concerns

City Engineer Himmer reviewed the staff report and described the proposed project to address drainage on 163rd Lane. He indicated the City has no easement for the outlet pipe for the property of impact and the estimated project cost is \$80,000. City Engineer Himmer requested

input, noting only \$75,000 had been budgeted. If supported, the project may be instituted in the fall and staff would contact the property owner to explain this project and address the crushed culvert.

Mike Dealing, 6310 163rd Avenue, stated he has a drainage easement and when the water goes over, it goes into his drain field. The water is not yet up to the house, but very close. Mr. Dealing stated the easement is not a problem and asked if the water will be moved via gravity or pump.

City Engineer Himmer stated it will be moved via gravity. The properties behind Mr. Dealing's property have dedicated easements along the common lot lines, platted easements. He explained staff looked at the distance between the ponding easement and existing easement and the project would directional bore to lessen impact to the trees.

Mr. Dealing asked if the project would start from the back.

City Engineer Himmer answered in the affirmative and stated the project would then go due south.

Mr. Dealing explained that in the spring the ground is frozen so the pond doesn't work. He asked if the pipe will be below the frost line.

Public Works Director Olson explained the problem with freezing is experienced at the end portions at the entry/exit, so it still has to thaw. However, if there was an issue creating an emergency situation, or jeopardizing the septic system, equipment can be brought in to thaw the pipe.

City Engineer Himmer explained the project would not keep the pond drained; there will still be water and infiltration but not to exceed the easement.

Mr. Dealing indicated when the pond was installed 20-25 years ago, they did a good job because in two to three days water seeps into the ground during the summer. He stated staff's proposed project is a good plan.

Chairperson Backous asked how many homes are impacted.

Civil Engineer Linton noted that on the north side of the road, it is the same elevation so water goes through the culvert.

Mr. Dealing stated when the culvert is fixed, it will probably flood toward his house and basement.

Councilmember Elvig asked staff to assure the project timing so fixing the culvert does not flood Mr. Dealing's property.

Public Works Director Olson noted the budget is \$75,000 yet the balance in the Storm Drainage Utility is in the hundreds of thousands so exceeding by \$10-\$15,000 may not be a concern. He clarified the staff recommendation is to delay until the Council looks at the 2015 CIP but would recommend moving forward and creating a plan set to re-identify cost estimates.

Tom Heifort, 6311 163rd Lane NW, stated he met with staff to determine how the system is to work; however, it is not working now so his yard is taking a lot of water before it gets to Mr. Dealing's property. He indicated that last summer, he had to cut grass in water and hopes the project is done right so it fixes all the problems.

Chairperson Backous agreed it has to be done right so water doesn't impact a different property.

City Engineer Himmer noted staff's report identified six different locations and asked how the projects should be addressed, as they come forward or to select two projects.

Councilmember McGlone stated he knows of three projects, those who show up.

Chairperson Backous stated the Public Works Committee counts on staff to prioritize the projects based on the factors involved.

City Engineer Himmer stated there are two more projects on 148th and 149th and the final two projects are the Obermaiers and the golf course, which can wait since the short-term issues are solved. He explained the two projects being discussed tonight were prioritized because both involve road projects. City Engineer Himmer stated the two projects approved last year will be packaged with the two projects discussed tonight, if approved.

Public Works Director Olson explained the intent is to handle the projects under one contract to capitalize on economies of scale.

Motion by Councilmember Elvig, seconded by Councilmember McGlone, to recommend that the City Council add this project to the list to be evaluated when the Capital Improvement Program is updated.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers Elvig, and McGlone. Voting No: None.

Case #3: Consider Revisions to the City's Minnesota State Aid (MSA) Street System

City Engineer Himmer reviewed the staff report and recommendation to adjust and reconfigure some Minnesota State Aid (MSA) street designations to better align with stated transportation goals and planned development activities.

Public Works Director Olson indicated the new MSA standards will be based on traffic points, not road segments.

City Engineer Himmer stated MSA will be upgrading its software and consider basing needs solely on traffic volumes so non-existing segments would only get minimal dollars. There will still be a requirement to connect a State Aid road with a State Aid road.

Councilmember Elvig asked how the system of “ghosting” State Aid roads is changing.

City Engineer Himmer explained the City cannot designate as State Aid, build the road, and then remove the designation because the City is collecting needs for 20 years. If the City removes the designation, it has to repay the State Aid.

Councilmember Elvig asked how that changes “ghosting” streets.

City Engineer Himmer answered it does not change the system the City has now, except it won't get as much funding for ghost streets. He stated he looked at the traffic counts and considered which streets may not be constructed and could be removed from the MSA designation. Staff would recommend keeping open the local flyover for Highway 10, potential River crossing, and also factored in the realignment of Sunwood Drive.

Councilmember McGlone asked how the change in the system will affect the City moving forward or affect people who own property on a State Aid road and want it improved.

Public Works Director Olson explained it will not change anything with existing State Aid streets and as the City's roadway system continues to grow, the City can designate 20% of its existing mileage to State Aid. He noted the recommended changes are minor and are roads that will not impact residential properties.

City Engineer Himmer stated the City ultimately decides how to spend State Aid dollars and if they approve the revision, and the City will still receive about \$1 million.

Motion by Chairperson Backous, seconded by Councilmember McGlone, to recommend that the City Council approve the staff requested revision to the City's MSA street system.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers McGlone and Elvig. Voting No: None.

Case #4: Consider Project Scope for Phase 2 of the Alpine Drive Overlay Project

City Engineer Himmer reviewed the staff report and requested input on the trail and alternate treatments. He then described each of the three trail segments, existing conditions, potential impacts, and estimated costs, noting there are also grant opportunities.

Park Supervisor Riverblood indicated moving ahead with Segment 1 would meet the Council's strategic goals and inform staff where to focus the City's resources because there are millions of dollars of trail needs. He believed Riverdale Drive made sense because it would be consistent with street reconstruction and the Mississippi River Trail.

Councilmember McGlone asked if staff is suggesting to hold off on the project.

Park Supervisor Riverblood suggested the Public Works Committee consider not moving forward with Segments 2 and 3 and consider Segment 1, noting it provides community connectivity along Alpine Drive up to the Sweet Bay boardwalk and trail. Park Supervisor Riverblood advised staff has submitted two grant applications for larger projects and once priorities are known, staff can break out funding priority.

Councilmember Elvig asked about submitting additional trail grants.

Park Supervisor Riverblood stated there is a small concern staff may be successful in two grants and the City will need funds for the match. He noted, however, if the agency is awarding \$9 million in projects, one community would typically not be awarded two grants.

Councilmember Elvig stated the Met Council is pushing trails.

Public Works Director Olson indicated the TED program looks geographically at where they put money and take pride in assuring it is geographically spread round the State.

Chairperson Backous asked if \$33,000 from the MSA Fund would be used for the trail.

City Engineer Himmer answered in the affirmative.

Chairperson Backous stated his position that trails need to wait if the City does not have the money. He noted the City has been waiting to build at Elmcrest for many years and the City needs to balance out the projects.

Park Supervisor Riverblood stated staff has not approached it recently but could also ask Anoka County, as part of the prioritization process, if they want to be co-applicant next year for a DNR trail grant for reasons of public safety.

City Engineer Himmer explained that once constructed, staff will apply on the City's behalf for reimbursement from the Metropolitan Council.

Councilmember Elvig stated support to proceed with Segment 1.

City Engineer Himmer stated funding would come from State Aid and this segment can be added to the plans and bid as an alternate. The City can then decide once the bids are received. He noted that Segments 2 and 3 involve the need for delineation and acquisition.

Councilmember McGlone stated funding is always a factor but an equal factor is working to take advantage of economies of scale. He believed there was a definite potential for savings with Segment 1 and doing it now would inconvenience the public only once. Councilmember McGlone stated he would argue for trails where the City is already doing a roadway project. He supported Segment 1 and asked if there were any other trail segments to consider, noting the City will be paving Sunfish Lake Boulevard.

Chairperson Backous agreed unless it involved right-of-way acquisition.

Councilmember Elvig noted the problem with Segment 2 is the need for acquisitions.

City Engineer Himmer stated the road is not platted or dedicated, it is prescriptive rights.

Councilmember Elvig asked if this could also be part of a bid alternate.

City Engineer Himmer stated it could but requires delineations, watershed permitting, banking, and design. Segment 1 doesn't need that extra work.

Motion by Councilmember Elvig, seconded by Councilmember McGlone, to recommend that the City Council eliminate the alternative road base technologies on this project but, rather, include that investigation and analysis with the future Riverdale Drive/MRT project, and to include Segment 1 as an add alternate within the scope of the 2012 Alpine Drive overlay.

Motion carried. Voting Yes: Chairperson Backous, Councilmembers Elvig, and McGlone. Voting No: None.

COMMITTEE INPUT

City Engineer Himmer stated the Bunker Lake Boulevard project has started and Anoka County will install signs for closures. Staff is working to assure it is properly signed and enforceable.

Public Works Director Olson indicated staff is evaluating the Sunwood Drive realignment project and working with the Police and Fire Departments to decide whether it should be constructed under traffic or closed during construction.

City Engineer Himmer explained if closed, three to four weeks could be removed from the schedule and there would be a savings of close to \$100,000 in material since bypass lanes would not be needed, liability reduction, traffic staging, and mobilization, equaling up to \$150,000 in savings. City Engineer Himmer advised Anoka County said they will entertain this option if there is no objection by the City relating to public safety and the business community. Staff recommends closing since it involves a limited closure, there are not many businesses that would be impacted, and a detour could be used to access those properties. If approved, staff will draft a letter to Anoka County and the business community, then develop a project under closure.

ADJOURNMENT

Motion by Chairperson Backous, seconded by Councilmember McGlone, to adjourn the Public Works meeting.

Motion carried.

The regular meeting of the Public Works Committee adjourned at 6:03 p.m.

Respectfully submitted,

Brian Olson
Public Works Director

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

Public Works Committee

5. 1.

Meeting Date: 05/15/2012

Submitted For: Len Linton

By: Len Linton, Engineering/Public Works

Title:

Consider Stormwater Improvements at 149th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns

Background:

Last summer the City experienced several significant rainfall events that lead to many localized flooding concerns, and resident complaints. The large volumes of precipitation that occurred over a short period of time appears to have elevated the groundwater within areas of the City, and prohibited the generous rate of infiltration that typically takes place in the Anoka sandplain. Throughout the summer and fall staff worked hard at registering and responding to the calls, and evaluating the situations on an individual basis to determine whether quick fixes could be implemented (culvert obstructions, re-ditching, etc.) to alleviate the immediate concerns.

This item was discussed at the Public Works Committee on August 15, 2011, and at that time staff summarized the areas of concern that were being investigated based upon citizen complaints received. The attached summary was presented at the meeting; which outlines the concern identified, actions steps to evaluate the concern, additional investigations that would be necessary to fully understand the situation, and recommendations for specific projects that could be implemented rather quickly and inexpensively. At that time we classified the issues into 3 categories:

1. Those that required no further action. They were evaluated and corrected, or did not need correcting because the water was fully contained within a dedicated drainage & utility easement (functioning as designed).
2. Those that required additional investigation and evaluation before deciding on a long term solution, and implementing corrective actions.
3. Those that had an identified recommendation for immediate action.

At that meeting the Committee briefly discussed the areas of concern, and directed staff to prepare plans and specifications for the items identified in category 3. Once this direction was ratified by the City Council on September 13, 2011, and the plans completed, it was too late in the season to secure bids and complete the improvements in 2011. It was then decided to delay construction until 2012, where some of the improvements could be completed by inclusion in a larger stormwater improvement plan set. Staff has finalized plans for these items, incorporated additional items that were approved at the Public Works Committee meeting in March, and now has these items out for bid. Depending on direction received tonight, for potential improvements to alleviate some of the outstanding concerns, additional plans will need to be prepared.

Notification:

The homeowner referenced on the petition as the contact person for this item has been notified of the meeting and has received a copy of the agenda.

Observations:

Staff prepared a brief RFP in the fall of 2011, to investigate the areas of concern, and distributed it to members of the City's consultant pool in the fall of 2011. The goal of this project was to have an independent third party evaluate the areas, provide options for corrective actions, and associated estimates to implement the work. Hakanson Anderson was awarded the project to undertake these investigations, and they have since completed their analysis and compiled a report that references potential solutions for each area identified. Attached to this case is the final report.

This item was introduced in general terms at the February Public Works Committee meeting, with the direction being that staff would come back before the committee over the next several months to discuss the merits of each potential improvement in more detail. This case will focus on section two of the report - 149th Lane.

This area is west of TH 47 and east of 149th Lane. The area in question is adjacent to the large DNR wetland which abuts TH 47. The petition was initiated by the resident at Lot 3, Block 1, Ramsey Meadows 4th Addition (5410 149th Lane NW). The homeowner contacted the City about water standing in the backyard. Review of the grading plan indicates this area was part of the wetland mitigation plan prepared for the plat and is controlled by the Wetland Conservation Act rules. There is a draintile that connects the backyard to the DNR wetland. Stormwater from the wetland flowed into the back yard which is lower than the wetland. A drainage and utility easement is recorded over the low area of the property. The DNR lists the normal water level of the wetland as 860.0. There is a retaining wall with steps in the backyard, with an elevation at the lowest step at approximately 860.1. The backyard slopes down from the steps so there will be water present when the wetland is at normal elevation.

There is a pipe under TH47, which is the outlet for the wetland, and there is an outlet structure on the pipe that controls the water elevation. The lowest point on the structure was approximately 2 1/2 feet above the normal water elevation at the time the elevated water was occurring last summer. The structure was quickly modified to lower the outflow elevation in an attempt to alleviate the high water concern. The overflow is now 0.8 feet above the normal water elevation, but the structure can still be modified slightly to lower the invert to the approximate normal water level of the wetland (860).

Funding Source:

The funding source for this improvement would be the Stormwater Utility Fund. It is estimated that this structure modification would cost between \$2,000 - \$5,000, but staff would like to meet with local contractors to discuss all options prior to making a final determination.

Staff Recommendation:

Staff recommends modifying the outlet structure to lower the wetland outfall elevation, placement of a flared end section, and removal of vegetation around the outlet pipe.

Committee Action:

Motion to recommend to the City Council that staff seek quotes to modify the outlet structure to lower the outlet elevation of the wetland.

Attachments

Project Location Map

Ramsey Meadows 2nd Additon Plat

Resident Petition

Petition Map

Section 2 of Hakanson Anderson Report

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Len Linton	05/10/2012 03:01 PM
Len Linton (Originator)	Len Linton	05/10/2012 04:08 PM
Tim Himmer	Len Linton	05/11/2012 08:35 AM
Len Linton (Originator)	Len Linton	05/11/2012 09:51 AM
Tim Himmer	Len Linton	05/11/2012 11:03 AM
Len Linton (Originator)	Len Linton	05/11/2012 11:08 AM
Tim Himmer	Tim Himmer	05/11/2012 11:28 AM
Brian Olson	MaryJo Warner	05/11/2012 11:29 AM
Kurt Ulrich	Kurt Ulrich	05/11/2012 11:55 AM

Form Started By: Len Linton

Started On: 05/09/2012 02:46 PM

Final Approval Date: 05/11/2012

Feb 13, 2012 - 2:51pm
K:\ecad_surr\land Desktop 2008\RA432.dwg\RA432-EXHIBITS.dwg

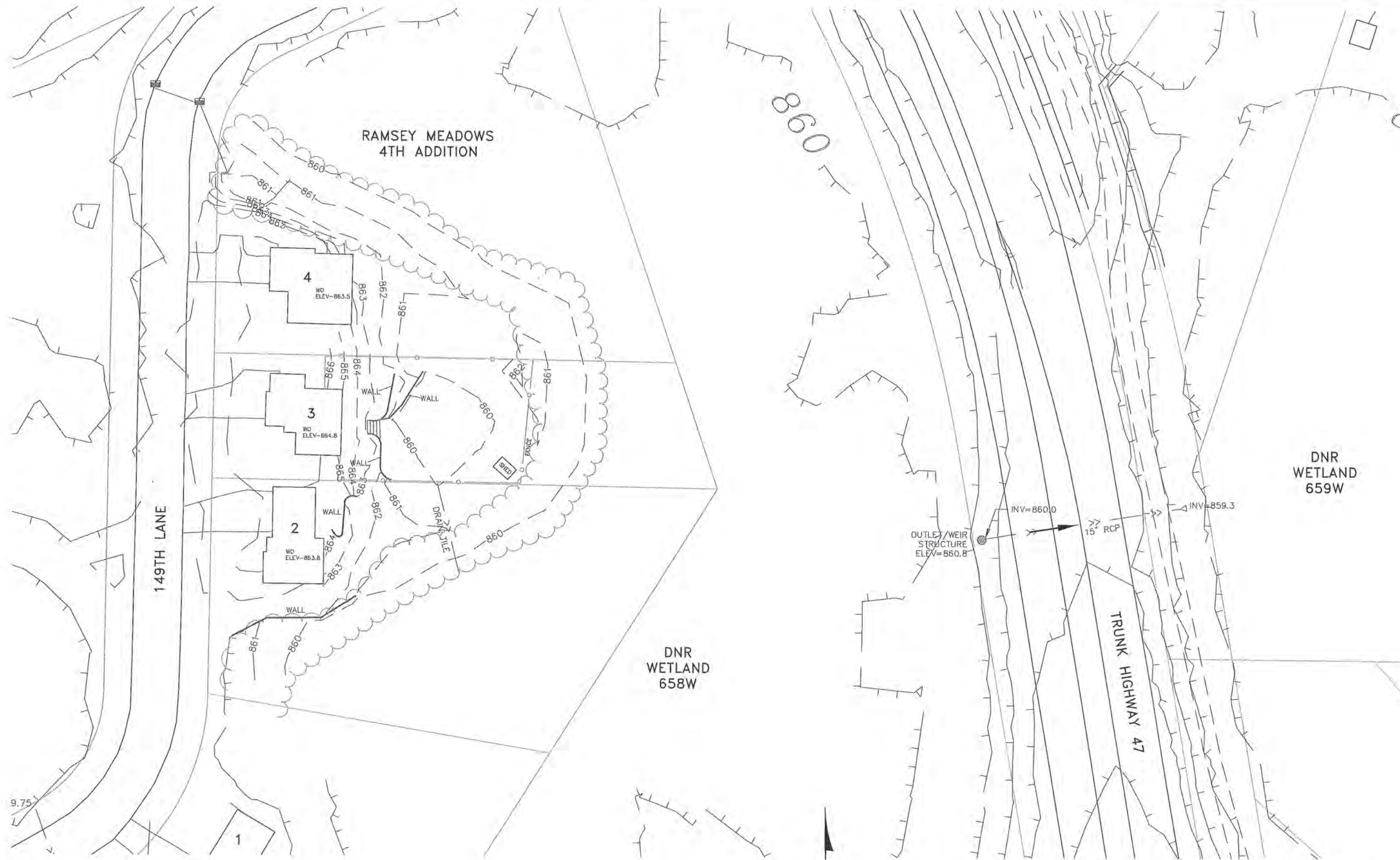
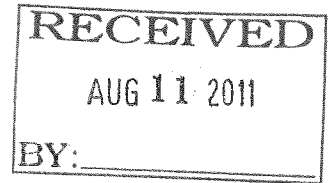


EXHIBIT 5
149TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

11 pages
total

Petition



Target: Review drainage problems in the city of Ramsey to cope with the volume of water entering our wetland and to lower the culvert that drains to Rum River.

Background:

We have lived in our homes for over seven years and some residents many more. We have never seen water even close to this amount in our yards. We have little children that this could cause injury or a health threat to. Many areas are filled over their capacity and we feel this is dangerous for our children to play near or around.

Petition:

Many of the houses that are backed up to the wetlands off of 149th lane NW ; Sunwood drive; 148th lane; Fluorine street; Germanium and Erkium are in the process or have lost their backwards due to the overflow of these wetlands . There is as much as 2 ½ feet of standing water and large portions of are yards, gardens, and sheds under water. The flooding started in May, before the recent rainfalls, and has continued to rise.

We as residents of the neighborhood believe that due to the new construction of houses and roads in the neighborhood this has changed the natural drainage process that was originally planned for this area and needs to be addressed by the city of Ramsey. The wetland we are told is still another foot or so away from reaching the overflow that eventually drains to the Rum River.

We are concerned now for the health and the safety of our children (newborns, special needs children), ourselves, and our pets due to the bacteria produced from the standing water along with the high concentration of mosquitoes that spread disease and we are obviously concerned of the flooding of our homes. The trees that are the noise barrier on highway 47 are starting to die. The market value of our homes has dropped considerably over the last few years and the standing stagnant water in our yards will definitely make it decrease even more. We need this problem to be addressed A.S.A.P.

Contact Person

First Name: Jessica P Rocky

Last Name: Belmonte

Email Address: j.belmonte@hotmail.com

Street Address: 5410 149th lane NW

City: Ramsey State: MN Zip Code: 55303

Signature: Jessica Belmonte Date: 8.5.11

First Name: RANDALL Crumpler

Last Name: Crumpler

Email Address: randallcrumpler@hotmail.com

Street Address: 14780 Fickion St NW

City: Ramsey State: MN Zip Code: 55303

Signature: [Signature] Date: 8-6-11

1/

First Name: Doug Donald Turbett
Last Name: Jaguer
Email Address: DJRollin33@yahoo
Street Address: 5440 149th Ln NW
City: Ramsey State: MN Zip Code: 55303
Signature: [Signature]

First Name: Patrick
Last Name: Daly
Email Address: PJ Daly 7@yahoo.com
Street Address: 5450 149 LN NW
City: Ramsey State: MN Zip Code: 55303
Signature: [Signature]

First Name: LEO
Last Name: MAJENSKI
Email Address: leo.majenksi@Q.com
Street Address: 5480 149TH LANE NW
City: RAMSEY State: MN Zip Code: 55303
Signature: [Signature]

2

First Name: TEJ Sherry Klaus

Last Name: CRUTE

Email Address: TS CRUTE@MSN.COM

Street Address: 14780 FLUORINE ST. N.W.

City: RAMSEY State: MN. Zip Code: 55303

Signature: Tej Crute

First Name: KENNETH E SMITH

Last Name: _____

Email Address: KOSMITH@Q.COM

Street Address: 14771 FLUORINE ST NW

City: RAMSEY State: MN Zip Code: 55303

Signature: 

First Name: DANA Julie Gardapee

Last Name: SPAH

Email Address: _____

Street Address: 14801 FLUORINE ST. NW

City: RAMSEY State: MN Zip Code: 55303

Signature: Dana Spah


First Name: Kami
Last Name: Schauer
Email Address: KSchauer@live.com
Street Address: 5428 149th Ln NW
City: Ramsey State: Mn Zip Code: 55303
Signature: Kami Schauer

same address


First Name: KRIS FITZGERALD
Last Name: C/O GRACELYN & ESTELLE FITZGERALD
Email Address: _____
Street Address: 5420 149th Ln NW
City: RAMSEY State: MN Zip Code: 55303
Signature: [Signature]

First Name: Charles
Last Name: Monette
Email Address: cmonette2004@msn.com
Street Address: 5423 149th Ln. N.W.
City: Ramsey State: Mn Zip Code: 55303
Signature: [Signature]

AAT Investments

First Name: John Gibson & Tepi Smith
Last Name: Gibson / Smith Phone 612.437.0705
Email Address: jgbsn@nym.hush.com fax
Street Address: 5400 149 Lane NW 1715-421-0044
City: Ramsey State: MN Zip Code: 55303
Signature:  POA Date 8/11

First Name: Garett & Dee Hanson
Last Name: Hanson
Email Address: dah75@netzero.net
Street Address: 14790 Fluorine St NW
City: Ramsey State: MN Zip Code: 55303
Signature: Dee Hanson / ~~Garett~~ Hanson

First Name: Youssif Elmagri
Last Name: Elmagri
Email Address: youssifelmagri@yahoo.com
Street Address: 5430 149th lane NW
City: Ramsey State: MN Zip Code: 55303
Signature: 

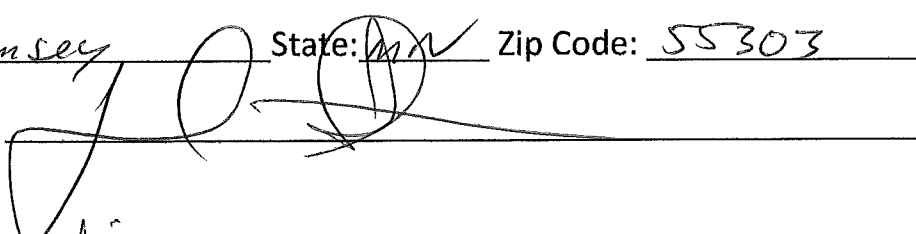
First Name: Joe Different Watershed

Last Name: Schaer

Email Address: joetanaers@hotmail.com

Street Address: 5429 145th Ave NW

City: Ramsey State: MN Zip Code: 55303

Signature: 

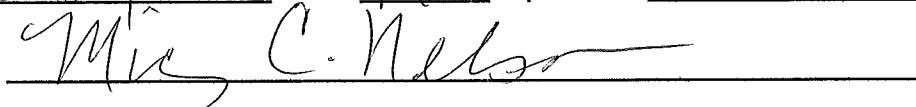
First Name: Mickey

Last Name: Nelson

Email Address: ~~mick~~ murranelsons@a.com

Street Address: 5470 149th Lane N.W.

City: Ramsey State: Mn. Zip Code: 55303

Signature: 

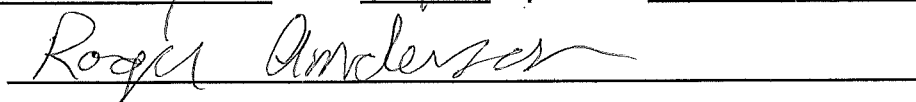
First Name: Roger

Last Name: Anderson

Email Address: RWANDERSON36@comcast.net

Street Address: 5490 149th LN. N.W.

City: Ramsey State: MN Zip Code: 55303

Signature: 

First Name: Somchith

Last Name: Chitmany

Email Address: _____

Street Address: 5460 149 LN MN

City: Ramsey State: MN Zip Code: 55308

Signature: Somchith Chitmany

✓ In to Mast
look

First Name: Pam

Last Name: Varner

Email Address: pjvpamela10@gmail

Street Address: 14931 Germanium St NW

City: Ramsey State: mn Zip Code: 55303

Signature: Pam Vo

First Name: RICK L. BERGELLEN

Last Name: _____

Email Address: TRANSAM ONE THREE A.O.L.COM

Street Address: 14851 GERMANIUM ST.

City: RAMSEY State: MN Zip Code: 55303-5113

Signature: Rick L. Bergellen

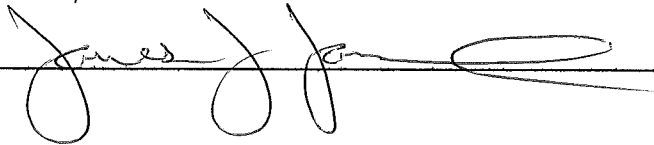
First Name: JAMES JAM

Last Name: JAMINSKI

Email Address: JJAMINSKI@COMCAST.NET

Street Address: 14827 GERMANIUM ST NW

City: RAMSEY State: MN Zip Code: 55303

Signature: 

First Name: Chad

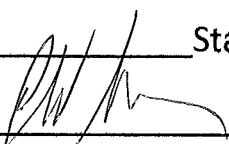
*Anna Hejchar
Chad Selger
O.K.*

Last Name: Selger

Email Address: Cselger@winroc.com

Street Address: 14801 Germanium St

City: Ramsey State: MN Zip Code: 55303

Signature: 


First Name: Jerry

Last Name: Smith

Email Address: _____

Street Address: 14790 Erbium St. NW

City: Ramsey State: MN Zip Code: 55303

Signature: 

First Name: TRISH David & Tricia
Last Name: BATES Osborn
Email Address: 14711 ERKIN ST NW
Street Address: NANTRISH@HOTMAIL.COM
City: Ramsey State: MN Zip Code: 55303
Signature: Trish Bates

First Name: Kathy Strobush
Last Name: Strobush
Email Address: DICKSTROBUSH@COMCAST.NET
Street Address: 5409 145th Ln NW
City: Ramsey State: MN Zip Code: 55303
Signature: Kathy Strobush

First Name: Ryan & Katre Lang
Last Name: _____
Email Address: Rlang1018@gmail.com
Street Address: 5115 149th Lane NW
City: Ramsey State: MN Zip Code: 55303
Signature: Ryan & Katre Lang AAT Investments

First Name: Tracey

Last Name: Dowd

Email Address: dowdt@comcast.net

Street Address: 5500 149th Lane NW

City: Ramsey State: MN Zip Code: 55303

Signature: Tracey Dowd

First Name: Cheri + Mike

Last Name: Johnson

Email Address: cheriandmike@usfamily.net

Street Address: 14791 Fluorine St NW

City: Ramsey State: MN Zip Code: 55303

Signature: Cheri + John

First Name: Chris + Alexandra

Last Name: Bertz

Bertolas

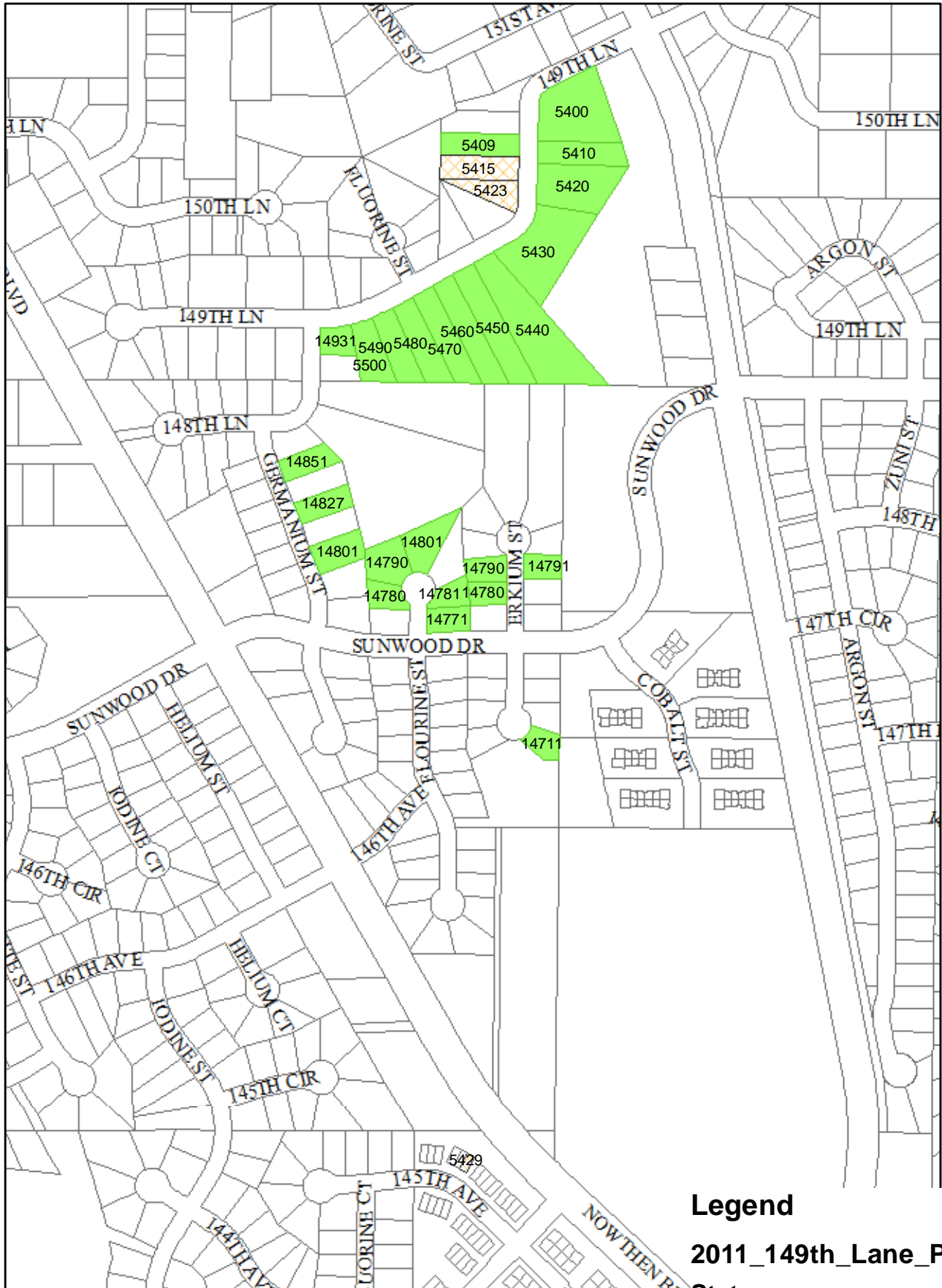
Email Address: bertz.chris@gmail.com

Street Address: 14781 Fluorine St NW

City: Ramsey State: MN Zip Code: 55303

Signature: Chris Bertz

2011 149th Lane Petitioning Properties



Legend

2011_149th_Lane_Petition
Status

-  Occupant
-  Owner

Section 2
149th Lane

149th Lane

Description

As shown on Exhibit 5, a low area exists on Lot 3, Block 1 of Ramsey Meadows 4th Addition. There is a drain tile that drains the low area to DNR Wetland 658W. During large storm events, DNR Wetland 658W backs up through the drain tile and into the low area. DNR Wetland 658W drains east under Trunk Highway 47 (TH 47) through a small weir structure and 15" pipe to DNR Wetland 659W. The weir structure is at an elevation of 860.9 and drains to the 15" pipe at an elevation of 860.0.

The current outlet elevation is approximately two feet above the bottom of Wetland 658W. The existing 100-year high water level (HWL) for DNR Wetland 658W is 862.1 and for DNR Wetland 659W is 860.4. As shown on Exhibit 6, there is an existing drainage and utility easement over a majority of Block 1 of Ramsey Meadows 4th Addition including the low area in question.

Alternatives

The following alternatives address the water elevation in DNR Wetland 658W.

Alternative 1

Alternative 1 will include removing the existing weir structure and leaving only the 15" culvert as the outlet from DNR Wetland 658W. By removing the weir structure, the wetland will begin to discharge at an elevation of 860.0 as opposed to 860.8.

Removing the weir structure, resulting in a lower normal water level in the wetland, will result in a 100-year HWL for DNR Wetland 658W of 862.0. The 100-year HWL for DNR Wetland 659W did not change. The estimated cost to remove the weir structure is \$1,265. Table 8 includes the individual costs for this alternative.

It is our understanding that any work proposed below an elevation of 860 will need the approval of the Minnesota Department of Natural Resources (DNR). This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. It appears the weir structure is part of the TH 47 storm sewer system. Removing the weir structure may require Mn/DOT's approval.

Alternative 2

Alternative 2 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at the same elevation as the existing outlet, 860.0. It was assumed the additional pipe would have to be jacked under TH 47.

Three different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + 15"	861.8	861.2	\$15,290
Existing + 18"	861.7	861.3	\$18,755
Existing + 24"	861.6	861.4	\$29,040

Tables 9 through 11 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. Removing the weir structure and constructing a culvert under TH 47 will require Mn/DOT's approval.

Alternative 3

Alternative 3 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at an elevation of 859.0, one foot lower than the existing outlet. It was assumed the additional pipe would have to be jacked under TH 47.

Two different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + lower 18"	861.6	861.3	\$18,755
Existing + lower 24"	861.6	861.5	\$29,040

Tables 12 and 13 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative has work proposed below an elevation of 860 and will need the approval of the DNR. Removing the weir structure and constructing a culvert under Trunk Highway 47 will require Mn/DOT's approval.

Alternative 4

Alternative 4 will include filling the low area on Lot 3, Block 1 of Ramsey Meadows 4th Addition. Filling the low area will reduce the frequency of the backyard flooding. During large storm events, the area may be inundated by water, but will likely be less frequently and for a shorter duration than under existing conditions.

The estimated cost to fill the low area is \$4,681. Table 14 includes the individual costs for this alternative.

As shown on Exhibits 7 and 8, this low area appears to have been designed as wetland mitigation area. Further research may be necessary to determine if this area is protected by the Wetland Conservation Act (WCA). If the area is protected by the WCA, filling this low area will not be a viable alternative.

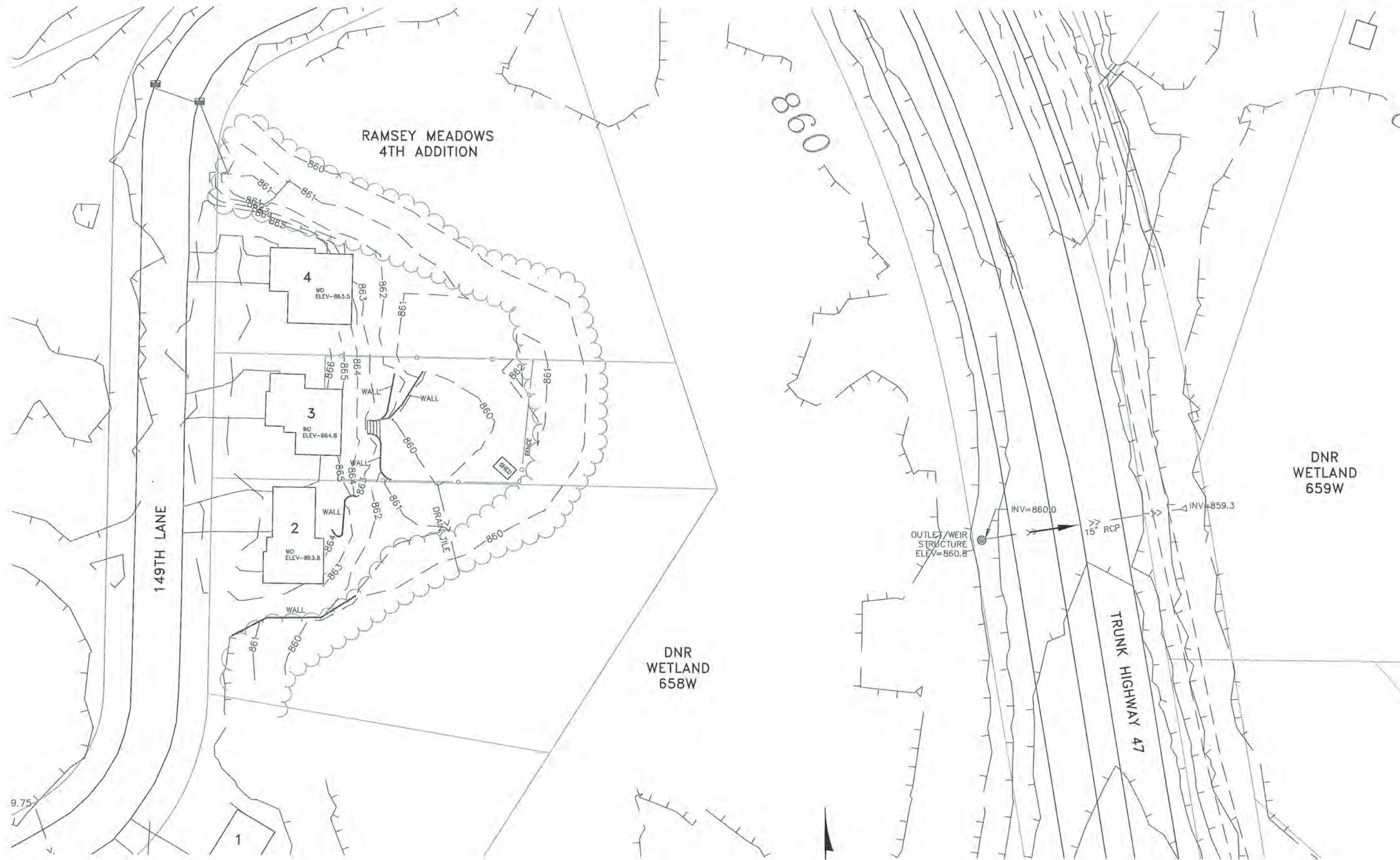


EXHIBIT 5
149TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY MEADOWS 4TH ADDITION

CITY OF RAMSEY COUNTY OF ANOKA

pg 25

KNOW ALL PERSONS BY THESE PRESENTS: That J. A. Menkveld & Associates, Inc., a Minnesota corporation, owner and proprietor and Builders Mortgage Corporation, a Minnesota corporation, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

That part of the Southwest Quarter of Section 24, Township 32, Range 25, Anoka County, Minnesota, described as follows:

Beginning at the northeast corner of Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota; thence South 89 degrees 07 minutes 51 seconds East, assumed bearing, parallel with the south line of said Southwest Quarter, a distance of 208.50 feet to the center line of State Trunk Highway No. 47, per the plat of AMBER RIDGE, according to the recorded plat thereof, Anoka County, Minnesota; thence northerly along said center line and along the center line of said State Trunk Highway No. 47, per the plat of WILLOW RIDGE, according to the recorded plat thereof, Anoka County, Minnesota, a distance of 789.96 feet to the intersection with the northeasterly extension of the following described line:

Beginning at a point on the center line of State Trunk Highway No. 47, per the plat of GORHAM'S ADDITION, according to the recorded plat thereof, Anoka County, Minnesota, said point being distant 93.00 feet southeasterly of the northeasterly extension of the southeasterly line of Block 3, said GORHAM'S ADDITION, as measured along said center line; thence South 64 degrees 18 minutes West, parallel with the southeasterly line of Block 3, said GORHAM'S ADDITION, a distance of 376.78 feet, and said line there terminating;

thence South 64 degrees 18 minutes 00 seconds West, along said last described line, a distance of 376.83 feet to the point of termination of said line; thence southwesterly along a tangential curve concave to the southeast, having a radius of 103.25 feet and a central angle of 63 degrees 14 minutes 20 seconds, a distance of 113.96 feet; thence South 1 degree 03 minutes 40 seconds West, tangent to said curve, a distance of 345.42 feet; thence southwesterly along a tangential curve concave to northwest, having a radius of 115.75 feet and a central angle of 60 degrees 08 minutes 15 seconds, a distance of 121.49 feet; thence South 61 degrees 11 minutes 55 seconds West, tangent to said curve, a distance of 53.09 feet to the intersection with the westerly extension of the north line of said Outlot A, RAMSEY MEADOWS 3RD ADDITION; thence South 87 degrees 06 minutes 09 seconds East, along said north line and its westerly extension, a distance of 517.62 feet to the point of beginning.

AND

Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota.

Have caused the same to be surveyed and platted as RAMSEY MEADOWS 4TH ADDITION and do hereby dedicate to the public for public use forever the boulevard, lane and drainage and utility easements as shown on the plat. In witness whereof said J. A. Menkveld & Associates, Inc., a Minnesota corporation, has caused these presents to be signed by its proper officer this 7th day of OCT, 1996. Also in witness whereof said Builders Mortgage Corporation has caused these presents to be signed by its proper officer this 7th day of OCT, 1996.

SIGNED:

J. A. MENKVELD & ASSOCIATES, INC.:

J. A. Menkveld
J. A. Menkveld, President

1275655
OFFICE OF COUNTY RECORDS
STATE OF MINNESOTA, COUNTY OF ANOKA
I hereby certify that the within instrument was filed in this office for record on the 7th day of MAY, 1997, in Book 54, page 25.
Edward M. Jruka
County Recorder



CAINE & ASSOCIATES
LAND SURVEYORS, INC.

BUILDERS MORTGAGE CORPORATION:

Ronald Stratton
Ronald Stratton, as President

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by J. A. Menkveld, President of J. A. Menkveld & Associates, Inc., a Minnesota Corporation, on behalf of the Corporation.



Teresa Vinje
Notary Public, Anoka County, Minnesota
My Commission expires 1-31-00

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by Ronald Stratton, President of Builders Mortgage Corporation, a Minnesota corporation, on behalf of the corporation.

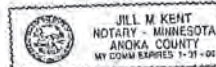


Kathleen R. Caven
Notary Public, Ramsey County, Minnesota
My Commission expires 1-31-2004

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY MEADOWS 4TH ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wetlands or public highways to be designated on said plat other than as shown thereon.

Jeffrey N. Caine
Jeffrey N. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyor's certificate was acknowledged before me a Notary COUNTY OF ANOKA) Public, this 12th day of October, 1996, by Jeffrey N. Caine, Land Surveyor.



Jill M. Kent
Notary Public, Anoka County, Minnesota
My Commission expires 01-31-00

CITY OF RAMSEY

We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY MEADOWS 4TH ADDITION at a regular meeting held this 24th day of September, 1996. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 505.03, Subd. 2.

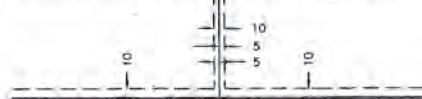
By *Ryan R. Schroeder* Mayor By *Ryan R. Schroeder* Clerk

Checked and approved this 7th day of MAY, 1997

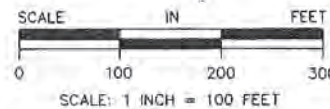
By *Merlyn D. Anderson*
Anoka County Surveyor
by *Larry S. Ham* deputy

I HEREBY CERTIFY THAT THE MONUMENTS AND DELINEATED TIES ON THE LINES DESCRIBED WITHIN ARE PAID AND THE SURVEY IS DATED May 7, 1997
J. Culver
DEPUTY PROSPECTOR & REGISTRAR

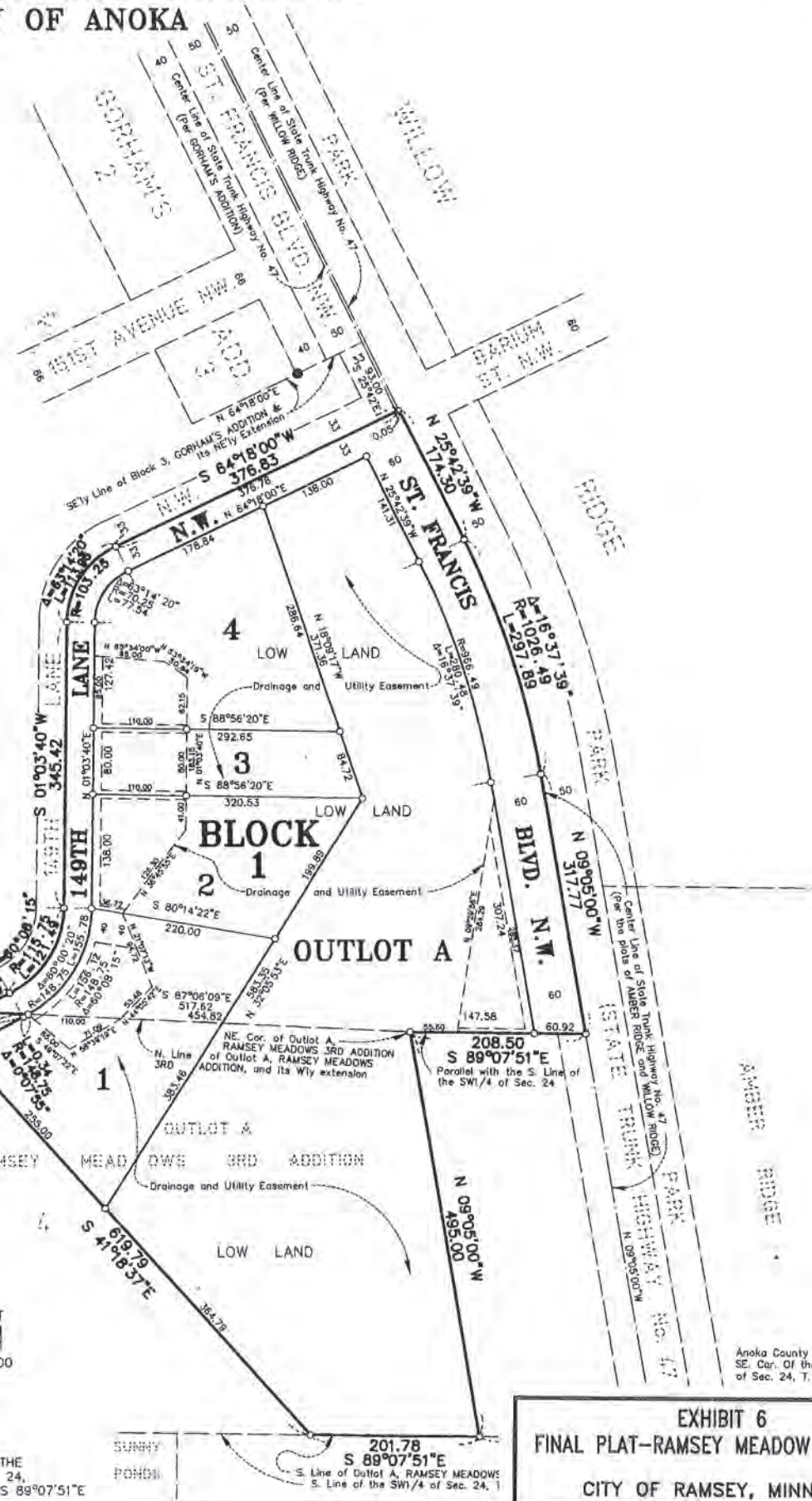
DRAINAGE AND UTILITY EASEMENTS SHOWN THUS:



BEING 10 FEET WIDE AND ADJOINING ALL STREET RIGHT-OF-WAY LINES AND REAR LOT LINES, AND 5 FEET WIDE AND ADJOINING ALL SIDE LOT LINES, UNLESS OTHERWISE SHOWN ON THE PLAT.



• DENOTES IRON MONUMENT FOUND.
○ DENOTES 1/2 INCH IRON PIPE SET.
◎ DENOTES ANOKA COUNTY MONUMENT.
NOTE: FOR THE PURPOSES OF THIS PLAT, THE SOUTH LINE OF THE SW1/4 OF SEC. 24, T. 32, R. 25 IS ASSUMED TO BEAR S 89°07'51"E



Anoka County Monument at SE. Cor. Of the SW1/4 of Sec. 24, T. 32, R. 25

EXHIBIT 6
FINAL PLAT-RAMSEY MEADOW 4TH ADD.
CITY OF RAMSEY, MINNESOTA

30397/824500

RAMSEY MEADOWS 2ND ADDITION

CITY OF RAMSEY COUNTY OF ANOKA

PRELIMINARY PLAT AND WETLAND MITIGATION PLAN

OWNER AND SURVIVORS: Gilbert A. Heavels
257 E. Main St.
Box 247
Anoka, MN 55303

LAND SURVEYOR AND FINDER: Caine and Associates Land Surveyors, Inc.
17720 Hwy 65 N.W.
Map Lake, MN 55304
612-794-6666

TOTAL AREA: 36.5 Acres, more or less
PROPOSED USE: R-1 Single Family Urban Residential, 1/2 acre lots and
1/4 acre lots
PROPOSED DENSITY: 1.5 lots per acre

PROPOSED WATER SOURCE AND SEWER DISPOSAL: City water and sewer

EXISTING ZONING: R-1 Residential

NORTHERN BUILDING SET BACKS: Front = 30
Side House = 10
Side Garage = 5
Rear = 30 or line up with existing houses

DRAINAGE AND UTILITY EASEMENTS: 10 feet wide adjoining all street lines and rear lot lines, and 5 feet wide adjoining all side lot lines, unless otherwise indicated on the plat.

DATE OF CORPORATION: 1952

Proposed Legal Description:

That part of the SW 1/4 of Section 24, Township 33, Range 25, in Anoka County, Minnesota, described as follows: Beginning at a point on the South line of said Block 3, a distance of 275 feet, as measured along said South line, Westward of the West right of way line of State Trunk Highway No. 47 as it is now laid out and traveled; thence North 89 degrees 41 minutes 41 seconds West on a line parallel to said West right of way line a distance of 295 feet; thence North 89 degrees 41 minutes 41 seconds West a distance of 516.99 feet; thence South 89 degrees 41 minutes 41 seconds East a distance of 516.99 feet; thence South 89 degrees 41 minutes 41 seconds East a distance of 516.99 feet; thence South 89 degrees 41 minutes 41 seconds East a distance of 516.99 feet to a point on the South line of said Block 3, a distance of 275 feet East of the point of beginning; thence North 89 degrees 41 minutes 41 seconds West on a line parallel to the South line of said Block 3 a distance of 275 feet to the point of beginning.

And that part of the Southwest Quarter of Section 24, Township 33, Range 25, in Anoka County, Minnesota, described as follows: Beginning at the intersection of the South line of said Southwest Quarter and the center line of State Trunk Highway No. 47 as it is now laid out and traveled; thence North 7 degrees 58 minutes 10 seconds West on said center line a distance of 282.57 feet; thence North 7 degrees 47 minutes West on said center line a distance of 109.24 feet to a point which is 93 feet Southwardly as measured along said center line; thence North 7 degrees 47 minutes West on a curve to the left having a radius of 1030.40 feet, a distance of 370.44 feet; thence North 23 degrees 47 minutes West on said center line a distance of 109.24 feet to the point which is 93 feet Southwardly as measured along said center line; thence North 7 degrees 47 minutes West on the Southwesterly line of Block 3 in Gorham's Addition; thence South 74 degrees 18 minutes West on a line parallel to the Southwesterly line of said Block 3 a distance of 376.78 feet; thence Southwesterly on a curve to the left having a radius of 103.25 feet, a distance of 113.55 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 348.42 feet; thence Southwesterly on a curve to the right having a radius of 113.75 feet, a distance of 121.85 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 113.09 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 208.40 feet to the point of beginning.

And that part of the Southwest Quarter of Section 24, Township 33, Range 25, in Anoka County, Minnesota, described as follows: Beginning at a point on the center line of State Trunk Highway Number 47 as it is now laid out and traveled, said point being 93 feet Southwardly as measured along said center line from the Northwesterly extension of the Southwesterly line of Block 3 in Gorham's Addition; thence South 64 degrees 11 minutes West on a line parallel to the Southwesterly line of said Block 3 a distance of 376.78 feet; thence Southwesterly on a curve to the left having a radius of 103.25 feet, a distance of 113.55 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 348.42 feet; thence Southwesterly on a curve to the right having a radius of 113.75 feet, a distance of 121.85 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 113.09 feet to a point, hereinafter known as Point A; thence North 1 degree 03 minutes 40 seconds East a distance of 326.42 feet to a point which is 290 feet Southwardly of, as measured at right angles to, the Northwesterly extension of the Northwesterly line of said Block 3, said point being the point of beginning of road to be described; thence Northwesterly on a curve to the right having a radius of 113.75 feet, a distance of 121.85 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 113.09 feet to a point, hereinafter known as Point B; thence North 1 degree 03 minutes 40 seconds East a distance of 326.42 feet to a point which is 290 feet Southwardly of, as measured at right angles to, the Northwesterly extension of the Northwesterly line of said Block 3, said point being the point of beginning of road to be described; thence Northwesterly on a curve to the right having a radius of 113.75 feet, a distance of 121.85 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 113.09 feet to a point, hereinafter known as Point C; thence North 1 degree 03 minutes 40 seconds East a distance of 326.42 feet to a point which is 290 feet Southwardly of, as measured at right angles to, the Northwesterly extension of the Northwesterly line of said Block 3, said point being the point of beginning of road to be described; thence Northwesterly on a curve to the right having a radius of 113.75 feet, a distance of 121.85 feet; thence South 74 degrees 18 minutes 40 seconds West a distance of 113.09 feet to a point, hereinafter known as Point D; thence North 1 degree 03 minutes 40 seconds East a distance of 326.42 feet to a point which is 290 feet Southwardly of, as measured at right angles to, the Northwesterly extension of the Northwesterly line of said Block 3, said line being described as follows:

Beginning at the Northwest corner of Lot 3, Block 3, GORHAM'S ADDITION; thence South 65 degrees 16 minutes West on the Southwesterly line of 151st Avenue N.W. in said GORHAM'S ADDITION extended Southwesterly for a distance of 210 feet; thence Southwesterly on a curve to the right having a radius of 101 feet, a distance of 149.02 feet; thence North 25 degrees 42 minutes West for a distance of 21.98 feet to the actual point of beginning of the line to be described; thence South 45 degrees 18 minutes West for a distance of 281.80 feet, more or less, to the aforementioned point of intersection and there terminating; thence South 35 degrees 31 minutes 39 seconds East to an intersection with a line from through the point of beginning parallel with the Southwesterly extension of the Northwesterly line of said Block 3 in GORHAM'S ADDITION; thence South 18 degrees 52 minutes 56 seconds East on a line parallel with said line to the point of beginning.

Excepting therefrom all that part of the above described tract which lies Southwardly of a line drawn parallel with and 23 feet Northwardly of the following described line:

Beginning at the aforementioned point 'A'; thence South 61 degrees 11 minutes 55 seconds West for a distance of 233.44 feet; thence Northwesterly on a curve to the right having a radius of 439.27 feet for a distance of 97.72 feet and there terminating; said parallel line being lengthened or shortened as necessary to intersect the boundaries of the above described tract.

And also except:

Lots 1 through 5, Block 1, RAMSEY MEADOWS, according to the recorded plat thereof, Anoka County, Minnesota;

And also except:

That part of Gorham Street N.W. and 149th Lane N.W. as indicated in said plat of RAMSEY MEADOWS.

--- DNR --- Denotes edge of D.N.R. Wetland Number 728 or 658

--- W --- Denotes edge of wetland as located by John C. Anderson of Wetlands Data and surveyed by Caine and Associates Land Surveyors, Inc.

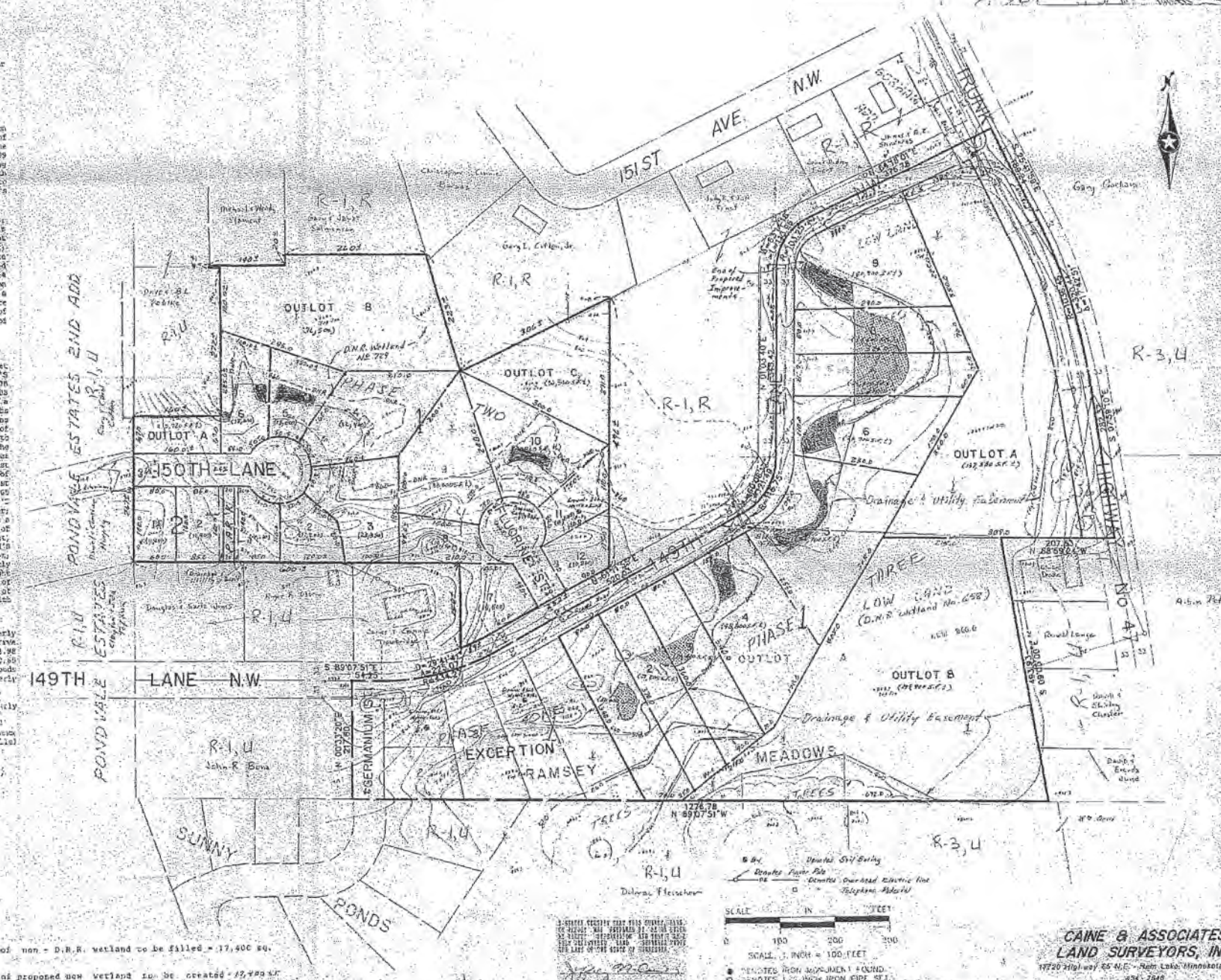
--- F --- Denotes non-D.N.R. wetland proposed to be filled.

--- N --- Denotes proposed new wetland to be created.

Note: Area of non-D.N.R. wetland to be filled = 17,900 sq. ft.
Area of proposed new wetland to be created = 15,990 sq. ft.



VICINITY MAP



SCALE: 1" = 100 FEET
SCALE: 1" = 200 FEET

--- S --- Denotes 1/2" iron pipe set.
--- B --- Denotes 1/2" iron pipe set.
--- T --- Denotes 1/2" iron pipe set.

**CAINE & ASSOCIATES
LAND SURVEYORS, INC.**
17720 Hwy 65 N.W. • Map Lake, Minnesota 55304
612-794-6666

**EXHIBIT 8
RAMSEY MEADOWS WETLAND MITIGATION PLAN
CITY OF RAMSEY, MINNESOTA**

**TABLE 8
149TH LANE
ALTERNATIVE 1**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
5	TURF ESTABLISHMENT	LUMP SUM	\$50.00	1	\$50

Estimated Construction Cost	\$1,150
Contingency (10%)	\$115
Total Estimated Construction Cost	<u>\$1,265</u>

TABLE 9
149TH LANE
ALTERNATIVE 2A - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	3	\$900
4	15" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$100.00	118	\$11,800
5	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
6	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$13,900
Contingency (10%)	\$1,390
Total Estimated Construction Cost	<u>\$15,290</u>

TABLE 10
149TH LANE
ALTERNATIVE 2B - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	\$1,705
Total Estimated Construction Cost	<u>\$18,755</u>

**TABLE 11
149TH LANE
ALTERNATIVE 2C - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

TABLE 12
149TH LANE
ALTERNATIVE 3A - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	\$1,705
Total Estimated Construction Cost	<u>\$18,755</u>

**TABLE 13
149TH LANE
ALTERNATIVE 3B - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

**TABLE 14
149TH LANE
ALTERNATIVE 4**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	COMMON EXCAVATION	CU YD	\$5.00	133	\$665
3	GRANULAR BORROW	CU YD	\$8.00	380	\$3,040
4	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.3	\$450

Estimated Construction Cost	\$4,255
Contingency (10%)	\$426
Total Estimated Construction Cost	<u>\$4,681</u>

Public Works Committee

5. 2.

Meeting Date: 05/15/2012

Submitted For: Len Linton

By: Len Linton, Engineering/Public Works

Title:

Consider Stormwater Improvements at Rum River Hills Golf Course - A Continuation of Discussion Related to 2011 Flooding Concerns

Background:

Last summer the City experienced several significant rainfall events that lead to many localized flooding concerns, and resident complaints. The large volumes of precipitation that occurred over a short period of time appears to have elevated the groundwater within areas of the City, and prohibited the generous rate of infiltration that typically takes place in the Anoka sandplain. Throughout the summer and fall staff worked hard at registering and responding to the calls, and evaluating the situations on an individual basis to determine whether quick fixes could be implemented (culvert obstructions, re-ditching, etc.) to alleviate the immediate concerns.

This item was discussed at the Public Works Committee on August 15, 2011, and at that time staff summarized the areas of concern that were being investigated based upon citizen complaints received. The attached summary was presented at the meeting; which outlines the concern identified, actions steps to evaluate the concern, additional investigations that would be necessary to fully understand the situation, and recommendations for specific projects that could be implemented rather quickly and inexpensively. At that time we classified the issues into 3 categories:

1. Those that required no further action. They were evaluated and corrected, or did not need correcting because the water was fully contained within a dedicated drainage & utility easement (functioning as designed).
2. Those that required additional investigation and evaluation before deciding on a long term solution, and implementing corrective actions.
3. Those that had an identified recommendation for immediate action.

At that meeting the Committee briefly discussed the areas of concern, and directed staff to prepare plans and specifications for the items identified in category 3. Once this direction was ratified by the City Council on September 13, 2011, and the plans completed, it was too late in the season to secure bids and complete the improvements in 2011. It was then decided to delay construction until 2012, where some of the improvements could be completed by inclusion in the street maintenance program. Staff is in the process of finalizing plans for these items and intends to solicit quotes for the improvements this spring/summer. Depending on the direction staff receives for potential improvements to alleviate some of the outstanding concerns identified over the next couple of months, additional plans can be incorporated into one plan set for bidding.

Notification:

The Golf Course representatives have been notified of the meeting and have recieved a copy of the agenda.

Observations:

Staff prepared a brief RFP in the fall of 2011, to investigate the areas of concern, and distributed it to members of the City's consultant pool in the fall of 2011. The goal of this project was to have an independent third party evaluate the areas, provide options for corrective actions, and associated estimates to implement the work. Hakanson Anderson was awarded the project to undertake these investigations, and they have since completed their analysis and compiled a report that references potential solutions for each area identified. Attached to this case is an excerpt of the final report. This item was introduced in general terms at the February Public Works Committee meeting, with the direction being that staff would come back before the committee over the next several months to discuss the merits of each potential improvement in more detail. This case will focus on section three of the report - Rum River Hills Golf Club.

Rum River Hills Golf Club is located east of Trunk Highway 47 at 167th Avenue NW. Several areas were evaluated and summarized in the report. The golf course was platted in the early 1980's, and at that time linear drainage and utility easements were secured on the site to encumber areas where off-site stormwater was routed through the site. Many areas of the site experienced flooding concerns last summer, and they are summarized as follows:

Hole #1 (Area 1)

There is a large pond located adjacent to Hole #1, which has a 12" plastic pipe that serves as the outlet. This outlet pipe was televised after the flooding was brought to the attention of the City last year, and the video revealed that there were several sags in the line, and many joints have been compromised with root intrusion. This pond was evaluated with the City storm water model, and it was determined that the 12" pipe was the limiting factor and slightly undersized to accommodate the flows. Up-sizing this line to an 18" pipe, and installing a manhole at the outfall would improve the efficiency of the stormwater routing. It should be noted that the City has always worked with the golf course on the outlet to this pond and previously installed a removable weir so they could control the flow leaving the pond with a removable weir. We are proposing to maintain this pond elevation adjustment for the golf course by installing a new manhole structure with a similar feature, as they currently utilize this pond as a course amenity and irrigation supply. The golf course likes to retain as much water as possible in the pond, installing the removable weir will provide the flexibility to retain small storm events and store the water while allowing larger events to pass through the system quickly.

Cart path area near the club house (Area 2A, B)

There is a pond north of the club house which is connected to the pond on hole #1. There are several cart paths around this smaller pond which experience localized flooding on occasion. By up sizing the existing undersized culvert connections we can allow more water to pass through the system quicker. The elevations on certain areas of the paths are below the maximum elevation of the pond. The paths would need to be raised if the intent is to not have water on the paths during the larger storm events, but doing so would have ramifications on other areas of the site upstream.

Saturated soil along Hole #15 (Area 3)

Hole #15 is along the Rum River, and there is a rock dam across the main drainage way from the golf course down to the river. This dam creates a ponding area adjacent to the fairway, and the water elevation of this pool is very close to the fairway grades, resulting in saturated soils in the area. The golf course excavated an area adjacent to the fairway last fall in an effort to determine the source of the water and dry the area up. Several draitile lines were uncovered by the excavation and they appeared to be plugged and collapsed in several locations.

Staff visited the site May 10th and observed the conditions in this area. The golf course has excavated a low area and swale on the edge of the 15th fairway. Golf course representatives said the low area had open water in it that flowed through the swale all winter, but the ponding area behind the dam froze over. This excavated low area has standing water that flows through the swale to the river. The water level behind the dam is approximately 1 foot below the adjacent fairway grades.

The recommendations for this area are two-fold; the golf course will undertake work that is outside of the drainage and utility easement (regrading and/or draitile installation), and the City will provide improvements within the drainage and utility easement. The golf course will monitor the low area this summer and are thinking about improving it into another water feature on the course.

They will also contact a draitile company to evaluate the installation of a new system along the fairway. They are also interested in removing sediment from behind the dam so that they can re-install a floating fountain. Staff will assist by researching state requirements for disposal of pond dredge material.

The City will evaluate options for changing the control structure on the rock dam. Currently there is an open top metal channel approximately 2 feet wide, which directs water over the top of the dam. Rocks are placed against the edges of the channel and as the depth increases water flows over the rocks. There is a metal dam approximately 70 yards upstream which has a flow width of 4 feet. The control structure on the dam should be widened to match

the upstream structure, and the elevation of the top may be lowered.

Hole #3 and #17 (Area 4 A, B, C)

The fairways of Hole #3 and #17 also experienced flooding last summer. These areas receive stormwater runoff from an area north of the golf course. The water passes through several ponds connected by culverts, which form a water hazard on the course. The pipes have shifted over the years, and there are several locations where the upstream invert is lower than the downstream invert, thereby decreasing the capacity of the pipe and holding back flow. The golf course is also interested in dredging a pond in this location. The idea in this location is to replace the pipes with slightly larger pipes to increase flow, and re-establish the proper invert elevations. This work will require coordination with the golf course so that alternate routes around the work areas can be set up before construction begins.

Funding Source:

The funding source for these improvements is the Storm Water Utility Fund. The opinion of probable construction costs, as prepared by Hakanson Anderson, are estimated at \$47,000, with a breakdown as follows:

Area 1 \$ 30,000
Area 2 \$ 7,000
Area 3 \$ 5,000
Area 4 \$ 5,000
Total \$ 47,000

Staff Recommendation:

Staff recommends authorizing preparation of plans and specifications for these various improvements, and requests authority to advertise for bids. Depending on current staff workload, this project may have to be consulted out to our engineering pool for completion this year to avoid future flooding concerns on the golf course.

Committee Action:

Motion to recommend to the City Council that staff be authorized to prepare plans and specifications for these various improvements, and authorization to advertise for bids.

Attachments

Golf Course Drainage Map

Section 3 of Hakanson Anderson Report

Existing Dedicated Easements on Site

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Len Linton	05/11/2012 08:35 AM
Len Linton (Originator)	Len Linton	05/11/2012 09:51 AM
Tim Himmer	Tim Himmer	05/11/2012 10:57 AM
Brian Olson	Len Linton	05/11/2012 11:03 AM
Len Linton (Originator)	Len Linton	05/11/2012 11:08 AM
Tim Himmer	Tim Himmer	05/11/2012 11:11 AM
Brian Olson	MaryJo Warner	05/11/2012 11:29 AM
Kurt Ulrich	Kurt Ulrich	05/11/2012 11:53 AM
Form Started By: Len Linton		Started On: 05/09/2012 10:36 AM
Final Approval Date: 05/11/2012		



EXHIBIT 9
RUM RIVER HILLS GOLF CLUB
EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

Section 3
Rum River Hills Golf Club

Rum River Hills Golf Club

Summary

Rum River Hills Golf Club has recently experienced flooding on some fairways and cart paths. Exhibit 9 shows the discharge points and problem areas throughout the golf course. The following will address the problem areas throughout the golf course.

Flooding along Hole #1

The first area of concern includes flooding of the large pond along Hole #1. This pond discharges through an existing concrete outlet structure and a 12" plastic pipe to the east. The outlet is labeled as Area 1 on Exhibit 9. It is our understanding that after large storm events, the pond remains elevated for long periods of time. The 100-year high water level (HWL) is 870.3.

After reviewing video recordings of the outlet pipe, it appears that the pipe has several sags in it and several joints have been compromised. The outlet pipe is relatively shallow and may have been affected by frost heave. This outlet pipe is likely causing the pond to operate inefficiently.

We believe that the best alternative for this outlet is to remove the existing outlet structure and the 12" outlet pipe and replace them with a new 4-foot diameter concrete outlet structure and 18" high density polyethylene pipe outlet in the same location as the existing pipe. Exhibit 10 shows the proposed outlet structure. The new pipe would be installed at a lower elevation than it exists now, reducing the impact of frost heave on the pipe. The polyethylene pipe is also more rigid and is solid (no air voids) and therefore more resistant to frost heave or buoyancy when soils are saturated. A removable, weir wall would be installed in the new outlet structure. We would propose to leave the weir height at the same elevation as existing. By installing the weir wall, it will allow for greater flexibility in controlling the water elevations of the pond. Installing this outlet structure would result in a 100-year HWL of 870.0.

The estimated cost to construct the new outlet pipe and structure is \$29,853. Table 15 includes the individual costs for this alternative.

Cart path flooding near the clubhouse

Based on the HWL of the pond along Hole #1, it appears that there could be an issue with the cart paths flooding near the clubhouse, Areas 2A and 2B on Exhibit 9. The existing culverts under the cart paths are a 15" diameter and 12" diameter. If flooding the cart paths is an issue, the best alternative would be to install larger diameter culverts under the cart paths.

Installing 24" diameter culverts will reduce the flooding during small storm events. However, since the HWL elevation for the area is controlled by the downstream outlet structure discussed above, the paths will still flood during large storm events. To reduce the flooding during the large storm events, the cart paths would have to be raised

approximately 1.5 feet. This would, however, increase the HWL in the pond near the clubhouse, which would appear to adversely affect the fairway for Hole #18.

The estimated cost to replace the two culverts under the carts paths is \$6,642. Table 16 includes the individual costs this alternative.

Saturated soil along Hole #15

Another issue is occurring along Hole #15 near the Rum River. The soil in the area is saturated. There is an existing rock dam, Area 3 on Exhibit 19, southeast of the most saturated area of the fairway. Part of the fairway was excavated to help to determine the problem and drain the area.

Based on our review, it appears that the water being contained by the rock dam may be infiltrating into the soil and causing the saturation. There is a dropped of approximately seven feet from the rock dam to the bottom of the downstream channel. We also noticed that the existing drain tile that was excavated along Hole #15 was plugged with roots and soil.

We recommend two alternatives to address the soil saturation along Hole #15. First, the drain tile should be replaced to improve the drainage in the area. Second, the area of the creek that is being contained by the rock dam should be lined with an impermeable material to eliminate the water infiltrating through the soil. Lining the creek will eliminate the infiltration into the soil and the new drain tile will help to keep the existing soil dry.

The estimated cost to replace the drain tile and line the creek bed is \$13,530. Table 17 includes individual costs for this alternative.

Flooding of Hole #3 and #17

Flooding of Hole #3 and #17 is also an issue at the golf course. The stormwater drains south through three 15” culverts, labeled Area 4A, Area 4B and Area 4C on Exhibit 9, and then to the Rum River.

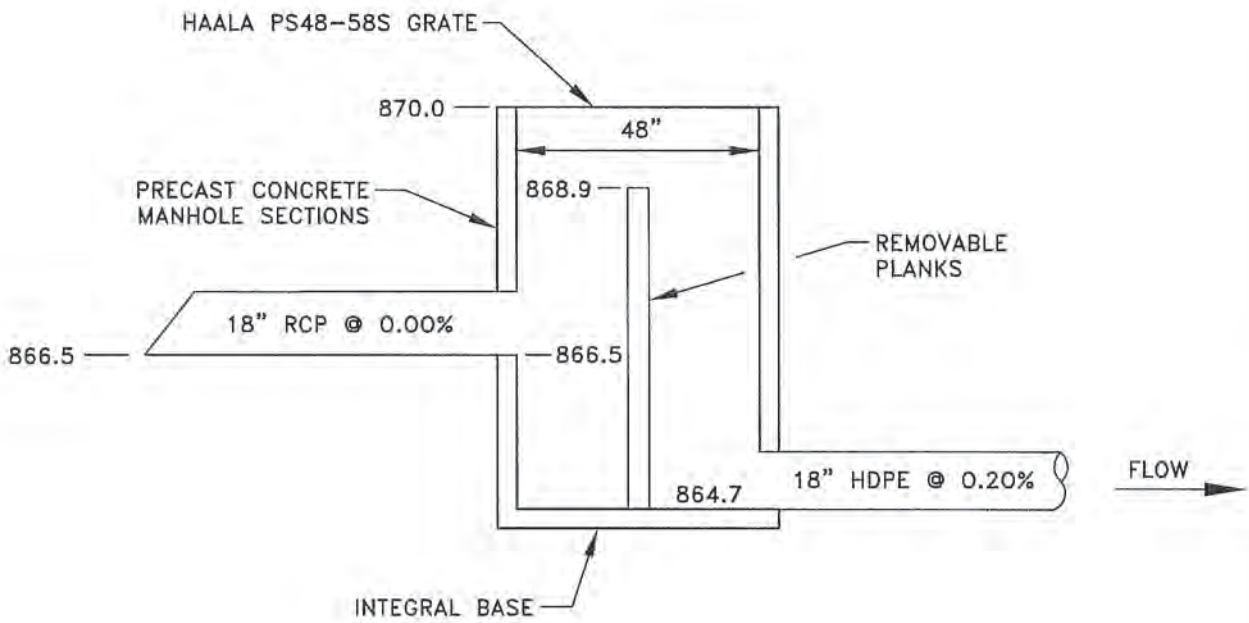
One alternative to lower the HWL’s in the area would be to install bigger culverts. A 21” culvert was modeled to replace the culverts at Area 4A and 4B and a 24” culvert was modeled to replace the culvert at 4C. The following table summarizes the 100-year HWL’s for the three areas:

Existing 100-Year HWL			Proposed 100-Year HWL		
Area 4A	Area 4B	Area 4C	Area 4A	Area 4B	Area 4C
859.3	859.3	858.5	859.1	858.7	858.3

The estimated cost to replace the three culverts is \$4,990. Table 18 includes individual costs for this alternative.



EXHIBIT 9
RUM RIVER HILLS GOLF CLUB
EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA



Feb 14, 2012 - 11:42am
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EXHIBIT 10
 RUM RIVER HILLS GOLF CLUB
 PROPOSED OUTLET STRUCTURE
 CITY OF RAMSEY, MINNESOTA

TABLE 15
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #1 - NEW OUTLET STRUCTURE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	ACRE	\$1,500.00	0.2	\$300
3	GRUBBING	ACRE	\$1,500.00	0.2	\$300
4	REMOVE STORM SEWER	LIN FT	\$1.50	856	\$1,284
5	18" METAL APRON	EACH	\$275.00	1	\$275
6	18" RC PIPE APRON	EACH	\$350.00	1	\$350
7	18" HDPE PIPE SEWER	LIN FT	\$22.00	850	\$18,700
8	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	10	\$230
9	OUTLET CONTROL STRUCTURE	EACH	\$4,000.00	1	\$4,000
10	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.6	\$900

Estimated Construction Cost	\$27,139
Contingency (10%)	\$2,714
Total Estimated Construction Cost	<u>\$29,853</u>

TABLE 16
RUM RIVER HILLS GOLF CLUB
CART PATH FLOODING NEAR CLUBHOUSE - NEW CULVERTS

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	96	\$384
3	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	30	\$150
4	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	48	\$144
5	4" AGGREGATE BASE CLASS 5	SQ YD	\$8.00	30	\$240
6	4" BITUMINOUS PAVEMENT	SQ YD	\$34.00	30	\$1,020
7	24" METAL APRON	EACH	\$325.00	4	\$1,300
8	24" CP PIPE CULVERT	LIN FT	\$25.00	96	\$2,400
9	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$6,038
Contingency (10%)	\$604
Total Estimated Construction Cost	<u>\$6,642</u>

TABLE 17
RUM RIVER HILLS GOLF CLUB
HOLE #15 SATURATION - CREEK LINING AND DRAIN TILE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$350.00	1	\$350
2	4" PERF PIPE DRAIN	LIN FT	\$15.00	450	\$6,750
3	CREEK LINING	LUMP SUM	\$5,000.00	1	\$5,000
4	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$12,300
Contingency (10%)	\$1,230
Total Estimated Construction Cost	<u>\$13,530</u>

**TABLE 18
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #3 AND #17 - NEW CULVERTS**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	72	\$288
3	21" METAL APRON	EACH	\$300.00	4	\$1,200
4	24" METAL APRON	EACH	\$325.00	2	\$650
5	24" CP PIPE CULVERT	LIN FT	\$26.00	46	\$1,196
6	24" CP PIPE CULVERT	LIN FT	\$27.00	26	\$702
7	TURF ESTABLISHMENT	LUMP SUM	\$300.00	1	\$300

Estimated Construction Cost	\$4,536
Contingency (10%)	\$454
Total Estimated Construction Cost	<u>\$4,990</u>



EXISTING EASEMENTS

PERPETUAL PONDING EASEMENT 11/9/82

RUM RIVER HILLS GOLF CLUB & DRIVING RANGE

OUTLOT G DRAINAGE EASEMENT 11/9/82

EASEMENT 2

EASEMENT 2

EASEMENT 3

EASEMENT 1

EASEMENT 3

1 BARTHEL'S

14 13

165TH LA NW

ESTATES

165TH

RIVER



Public Works Committee

5. 3.

Meeting Date: 05/15/2012

By: Len Linton, Engineering/Public Works

Title:

Consider Stormwater Improvements at 148 th Lane - A Continuation of Discussion Related to 2011 Flooding Concerns

Background:

Last summer the City experienced several significant rainfall events that lead to many localized flooding concerns, and resident complaints. The large volumes of precipitation that occurred over a short period of time appears to have elevated the groundwater within areas of the City, and prohibited the generous rate of infiltration that typically takes place in the Anoka sandplain. Throughout the summer and fall staff worked hard at registering and responding to the calls, and evaluating the situations on an individual basis to determine whether quick fixes could be implemented (culvert obstructions, re-ditching, etc.) to alleviate the immediate concerns.

This item was discussed at the Public Works Committee on August 15, 2011, and at that time staff summarized the areas of concern that were being investigated based upon citizen complaints received. The attached summary was presented at the meeting; which outlines the concern identified, actions steps to evaluate the concern, additional investigations that would be necessary to fully understand the situation, and recommendations for specific projects that could be implemented rather quickly and inexpensively. At that time we classified the issues into 3 categories:

1. Those that required no further action. They were evaluated and corrected, or did not need correcting because the water was fully contained within a dedicated drainage & utility easement (functioning as designed).
2. Those that required additional investigation and evaluation before deciding on a long term solution, and implementing corrective actions.
3. Those that had an identified recommendation for immediate action.

At that meeting the Committee briefly discussed the areas of concern, and directed staff to prepare plans and specifications for the items identified in category 3. Once this direction was ratified by the City Council on September 13, 2011, and the plans completed, it was too late in the season to secure bids and complete the improvements in 2011. It was then decided to delay construction until 2012, where some of the improvements could be completed by inclusion in a larger stormwater improvement plan set. Staff has finalized plans for these items, incorporated additional items that were approved at the Public Works Committee meeting in March, and now has these items out for bid. Depending on direction received tonight, for potential improvements to alleviate some of the outstanding concerns, additional plans will need to be prepared.

Notification:

The homeowners adjacent to his ponding area have been notified of the meeting, and have received a copy of the agenda.

Observations:

Staff prepared a brief RFP in the fall of 2011, to investigate the areas of concern, and distributed it to members of the City's consultant pool in the fall of 2011. The goal of this project was to have an independent third party evaluate the areas, provide options for corrective actions, and associated estimates to implement the work. Hakanson Anderson was awarded the project to undertake these investigations, and they have since completed their analysis and compiled a report that references potential solutions for each area identified. Attached to this case is the final report.

This item was introduced in general terms at the February Public Works Committee meeting, with the direction being that staff would come back before the committee over the next several months to discuss the merits of each

potential improvement in more detail. This case will focus on section one of the report - 148th Lane

The site is east of Nowthen Boulevard (CSAH 5), southeast of Ramsey Elementary School. There is a low area across three (3) lots south of 148th Lane, that was created as a stormwater pond for the area and wetland mitigation for the development. There is a storm sewer inlet to this area along the west edge, which drains the subdivision to the north. There is also an outlet from this area on the east end that connects to a pipe that runs easterly and parallel to 148th Lane. The invert of this outlet pipe is approximately 2.5 feet above the low point in the basin. The outlet pipe also connects to a structure in the boulevard of 148th Lane that takes storm water runoff from the roadway and directs it easterly as described above. The inverts on both these pipes are at approximately the same elevation, and therefore water may be entering this basin that is not intended to flow there from the stormwater system. It was also observed that the basin has several undulations in the ground surface that do not allow the water to evenly spread out across the entire drainage and utility easement; basically there appears to be three separate, somewhat isolated basins.

Hakanson Anderson explored several options for installing another outlet and determined that each option would not significantly affect the high water elevation in the basin, and would justify the costs associated with performing them. Additional easements would need to be acquired, and disruption of neighboring properties would occur. Staff is currently researching check valves that could be placed in the outlet pipe at the manhole to block drainage from the street and only allow water to exit the basin through this structure, thereby eliminating additional flow to the area.

Funding Source:

The funding source for these improvements is the storm water utility fund. It is estimated that regrading the basin and installing a backflow valve on the outlet pipe would cost approximately \$7,500 to \$12,500.

Staff Recommendation:

Staff recommends regrading the backyards of the three properties to better distribute water in the basin, and re-establish the original pond contours. It is further recommended to install a backflow valve on the outlet pipe to direct storm water runoff from 148th Lane to the intended downstream receiving body.

Committee Action:

Motion to recommend to the City Council that staff seek quotes to regrade the backyards of the three properties to better distribute water in the basin and re-establish the original pond contours, and to install a backflow valve on the outlet pipe to direct storm water runoff from 148th Lane to the intended downstream receiving body.

Attachments

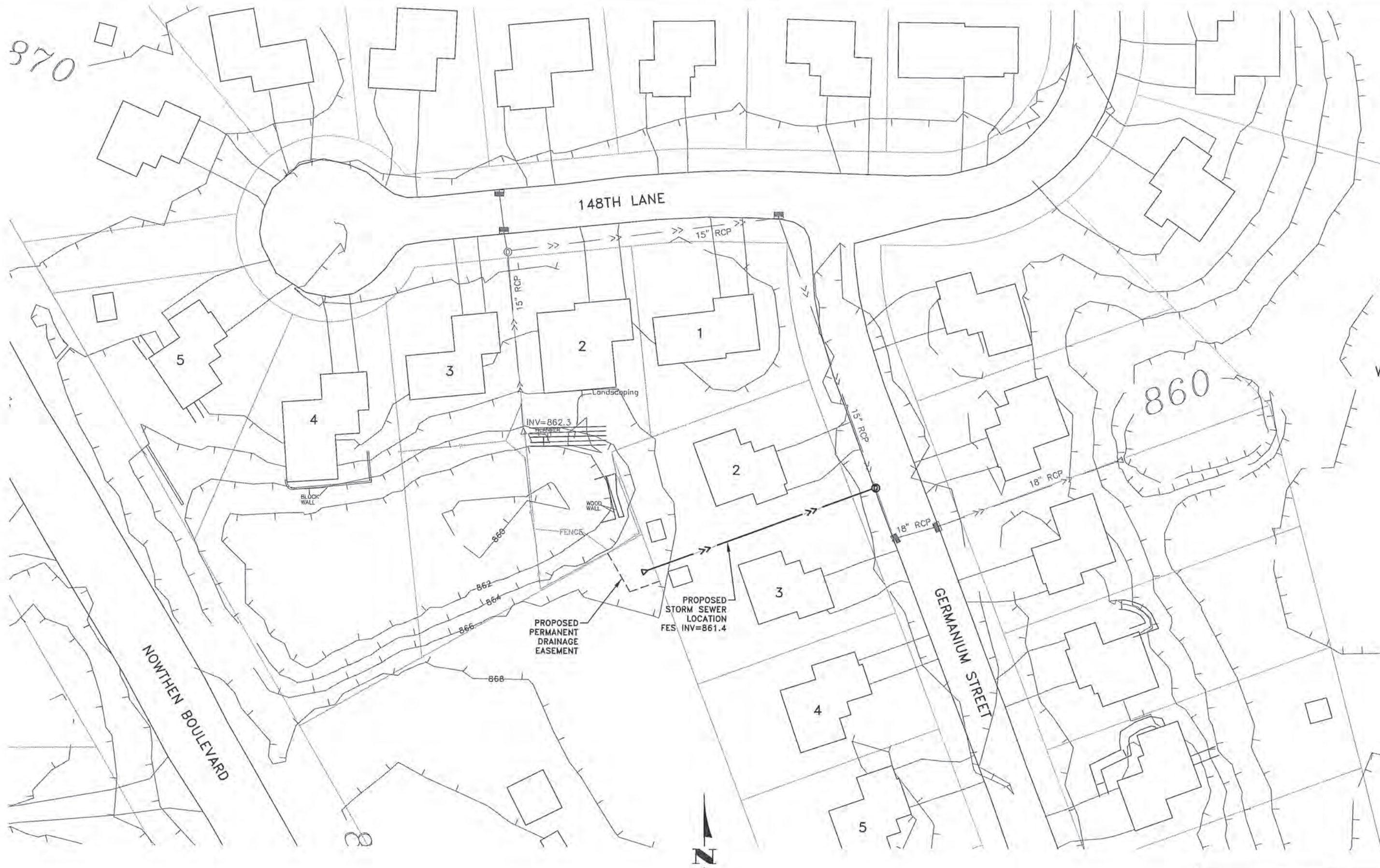
Project Location Map

Section 1 of Hakanson Anderson Report

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Len Linton	05/11/2012 08:35 AM
Len Linton (Originator)	Len Linton	05/11/2012 09:51 AM
Tim Himmer	Len Linton	05/11/2012 11:03 AM
Len Linton (Originator)	Len Linton	05/11/2012 11:08 AM
Tim Himmer	Tim Himmer	05/11/2012 11:25 AM
Brian Olson	MaryJo Warner	05/11/2012 11:29 AM
Kurt Ulrich	Kurt Ulrich	05/11/2012 11:54 AM
Form Started By: Len Linton		Started On: 05/09/2012 02:41 PM

Final Approval Date: 05/11/2012



DNR
WETLAND
658W

NORTHEN BOULEVARD

GERMANIUM STREET

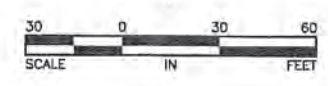


EXHIBIT 4
148TH LANE ALTERNATIVE 2 STORM SEWER
 CITY OF RAMSEY, MINNESOTA

Feb 13, 2012 - 2:41pm
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Section 1
148th Lane

148th Lane

Description

As shown on Exhibit 1, a low area exists south of 148th Lane. A 15" outlet pipe drains the low area north and then east to DNR Wetland 658W. The invert of the outlet pipe is at elevation 862.3 and the bottom of the low area is at elevation 859.8. Water in the low area has to rise 2.5 feet prior to discharging. The water below the outlet infiltrates into the soil, which may take days depending on the condition of the soil.

The existing 100-year high water level (HWL) for the low area is 865.5. As shown on Exhibit 2, a drainage easement exists in the rear of Lots 2 through 5, Block 2 of Ramsey Commons 2nd Addition.

Alternatives

The following alternatives address the water elevation in the existing low area.

Alternative 1

In this alternative, a new outlet pipe would be installed between Lot 1 of Ramsey Commons 2nd Addition and Lot 2 of Sunny Ponds, as shown on Exhibit 3. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.6. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.4	\$16,264
18"	865.1	\$31,566 *
24"	864.3	\$45,381 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

Tables 1 through 3 include the individual costs for this alternative.

Alternative 2

In this alternative, a new outlet pipe would be installed between Lots 2 and 3 of Sunny Ponds, as shown on Exhibit 4. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.4. This alternative would require additional grading and the acquisition of a permanent easement on the property south of Ramsey Commons 2nd Addition and west of Sunny Ponds. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.3	\$13,098
18"	865.0	\$18,846 *
24"	864.3	\$32,543 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

Tables 4 through 6 include the individual costs for this alternative.

Alternative 3

In this alternative, the bottom of the low area would be filled to the same elevation as the outlet pipe invert. The pond would then drain dry and would not be sitting with water until it infiltrated. By filling the bottom of the low area, the resultant 100-year HWL will be 866.2, 0.7 feet higher than the existing HWL. This higher HWL would end up outside the existing drainage and utility easement, creating the need for additional drainage easement.

The estimated cost to fill the low area is \$15,321. Table 7 includes the individual costs for this alternative.

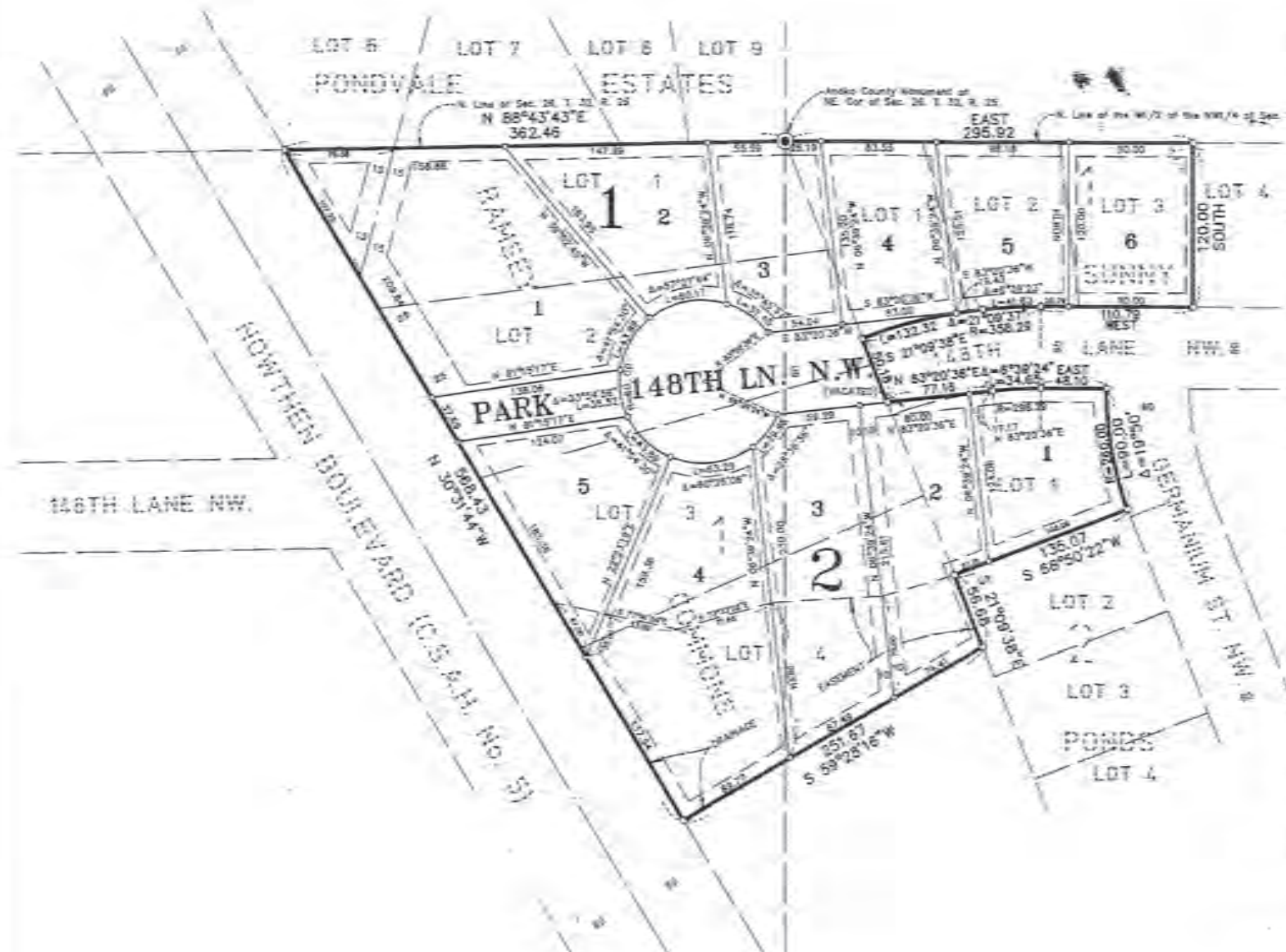
Further research is required to determine if this low area was designed to treat a water quality volume. The volume required would dictate if Alternatives 1 and 2 are viable options. Alternative 3 would not be viable, because the water quality volume is being eliminated in this alternative.



EXHIBIT 1
148TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY COMMONS 2ND ADDITION

CITY OF RAMSEY
COUNTY OF ANOKA



KNOW ALL PERSONS BY THESE PRESENTS: That North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Delores S. Fleischer, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2 and 3, Block 1, and Lot 1, Block 2, all in SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota.

And that part of vacated 148th Lane N.W., as dedicated in the plat of SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota, lying north of the north line of Lot 1, Block 2, in said SUNNY PONDS and lying south of the following described line:

Commencing at the northeast corner of said Lot 1, Block 2, thence on an assumed bearing of West, along the north line of said Lot 1, Block 2, a distance of 48.10 feet, thence westerly continuing along said north line and along a tangential curve, concave to the south having a radius of 290.29 feet and a central angle of 5 degrees 39 minutes 24 seconds, a distance of 34.55 feet to the point of beginning of the line to be described; thence South 81 degrees 20 minutes 08 seconds West, a distance of 77.38 feet to the intersection with the northerly extension of the west line of said Lot 1, Block 2, and said line thence terminating.

AND that North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Delores Steis, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2, 3 and 4, Block 1, RAMSEY COMMONS, according to the recorded plat thereof, Anoka County, Minnesota.

Have caused the same to be surveyed, and platted as RAMSEY COMMONS 2ND ADDITION and do hereby donate and dedicate to the public for public use forever the lands shown on the plat. Also dedicating the drainage and/or utility easements as shown on the plat. Also dedicating to the County of Anoka the right of access into County State Aid Highway No. 5 from Lot 1, Block 1 and from Lots 1 and 2, Block 2. In witness whereof said North Suburban Development, Inc. has caused these presents to be signed by its proper officer this 20th day of MAY, 1993. Also in witness whereof said Delores S. Fleischer has hereunto set her hand this 20th day of MAY, 1993. Also in witness whereof said Delores Steis has hereunto set her hand this 21st day of MAY, 1993.

NORTH SUBURBAN DEVELOPMENT, INC.

[Signature]
A. Harkfeld, as President

DELORES S. FLEISCHER
Delores S. Fleischer

DELORES STEIS
Delores Steis

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 20th day of MAY, 1993, by J. A. COUNTY OF ANOKA) Harkfeld, President of North Suburban Development, Inc., a Minnesota corporation, on behalf of the corporation.

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 20th day of MAY, 1993, by Delores COUNTY OF ANOKA) S. Fleischer, single.

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 21st day of MAY, 1993, by Delores COUNTY OF ANOKA) Steis, single.

SHARLENE D. CHENOWETH
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

SHARLENE D. CHENOWETH
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

MARGARET G. MCGURRY
Notary Public, ANOKA County, Minnesota
My Commission expires 2-21-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 2-21-96

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY COMMONS 2ND ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wet lands or public highways to be designated on said plat other than as shown thereon.

JEFFREY W. CAINE
Jeffrey W. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyors certificate was acknowledged before me as a Notary Public, this 17th day of May, 1993, COUNTY OF ANOKA) by Jeffrey W. Caine, Land Surveyor.

MOLLY W. CAINE
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY COMMONS 2ND ADDITION at a regular meeting held this 11th day of May, 1993. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 469.02, Subd. 2.

JAMES B. BLOOM
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

MOLLY W. CAINE
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

Checked and approved this 20th day of June, 1993.

M. J. D. A. D.
Anoka County Surveyor

[Signature]
Anoka County Surveyor

EXHIBIT 2. FINAL PLAT-RAMSEY COMMONS 2ND ADD. CITY OF RAMSEY, MINNESOTA

DRAINAGE AND UTILITY EASEMENTS SHOWN THUS:



BEING 10 FEET WIDE AND ADJOINING ALL STREET RIGHT-OF-WAY LINES AND REAR LOT LINES AND 5 FEET WIDE AND ADJOINING ALL SIDE LOT LINES, UNLESS OTHERWISE SHOWN ON THE PLAT.

SCALE 1 INCH = 60 FEET



SCALE: 1 INCH = 60 FEET

○ DENOTES 1/2 INCH IRON PIPE SEWER
● DENOTES ANOKA COUNTY MONUMENT

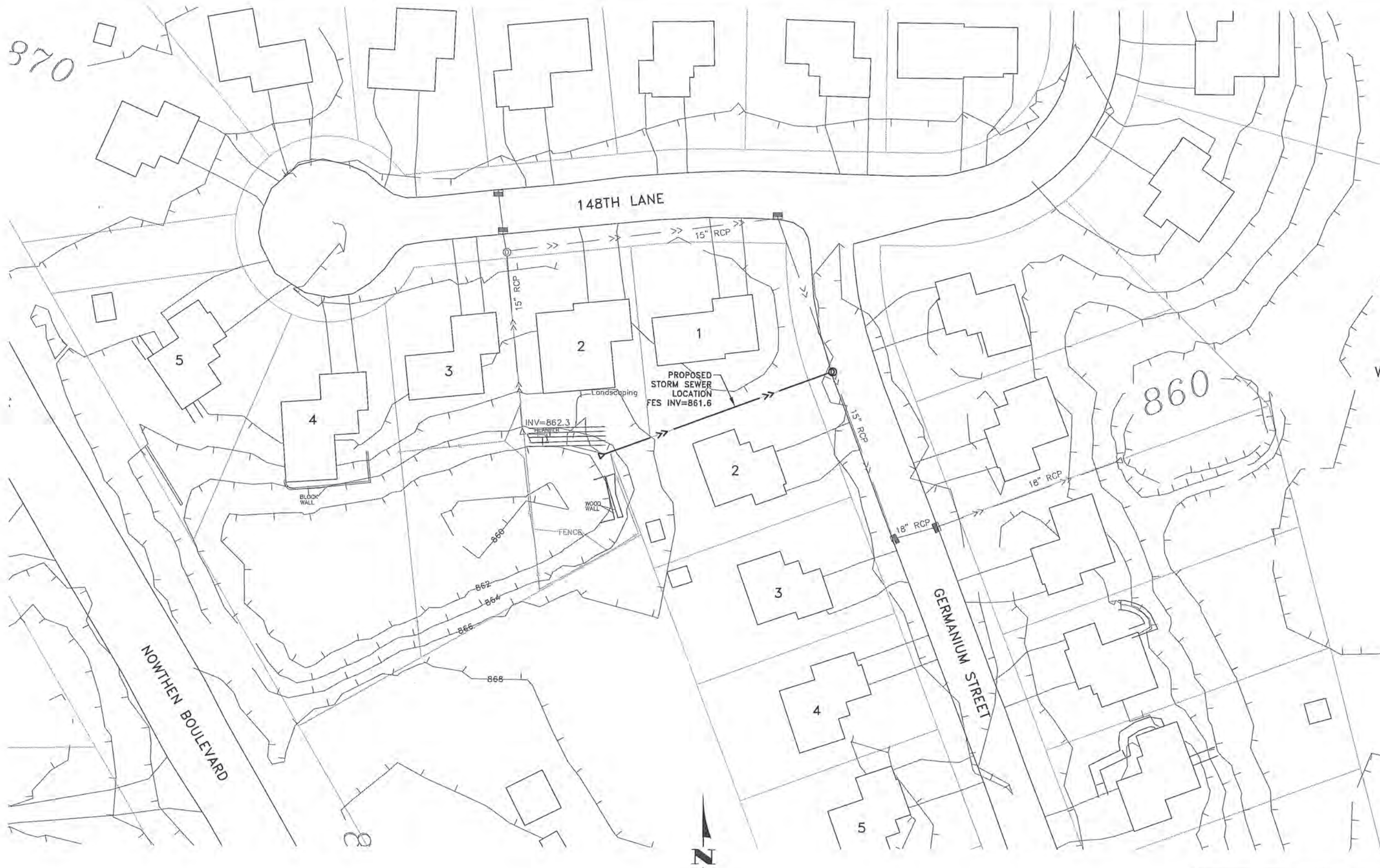
NOTE: FOR THE PURPOSES OF THIS PLAT, THE NORTH LINE OF THE NW/4 OF THE NW/4 OF SEC. 25, T. 32, R. 25 IS ASSUMED TO HAVE A BEARING OF EAST.

1049281

OFFICE OF COUNTY RECORDER
STATE OF MINNESOTA, COUNTY OF ANOKA
I hereby certify that the within instrument was filed in this office for record on the JUNE 29, A.D. 1993 at 4:15 o'clock P.M. and was duly recorded in book 614 page 41

Edward M. Traska
Notary Public, ANOKA County, Minnesota
My Commission expires 6-17-96

CAINE & ASSOCIATES
LAND SURVEYORS, INC.



870

860

DNR
WETLAND
658W

148TH LANE

GERMANIUM STREET

NOWTHEN BOULEVARD

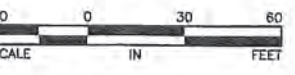


EXHIBIT 3
148TH LANE ALTERNATIVE 1 STORM SEWER
CITY OF RAMSEY, MINNESOTA



DNR
WETLAND
658W

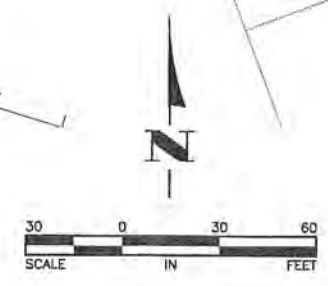


EXHIBIT 4
148TH LANE ALTERNATIVE 2 STORM SEWER
 CITY OF RAMSEY, MINNESOTA

Feb 13, 2012 - 2:41pm
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**TABLE 1
148TH LANE
ALTERNATIVE 1A - 15" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	15" RC PIPE APRON	EACH	\$300.00	1	\$300
12	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
17	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$14,786
Contingency (10%)	\$1,479
Total Estimated Construction Cost	<u>\$16,264</u>

**TABLE 2
148TH LANE
ALTERNATIVE 1B - 18" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	233	\$1,165
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	135	\$1,350
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	150	\$750
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	155	\$465
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	150	\$1,125
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	150	\$4,200
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	18" RC PIPE APRON	EACH	\$350.00	1	\$350
12	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
14	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	267	\$6,141
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	135	\$2,700
17	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$28,696
Contingency (10%)	\$2,870
Total Estimated Construction Cost	<u>\$31,566</u>

**TABLE 3
148TH LANE
ALTERNATIVE 1C - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,300.00	1	\$1,300
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$4.00	388	\$1,552
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	155	\$775
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	195	\$975
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	175	\$525
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	195	\$1,463
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	195	\$5,460
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	24" RC PIPE APRON	EACH	\$450.00	2	\$900
12	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	414	\$12,006
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	155	\$3,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$41,256
Contingency (10%)	\$4,126
Total Estimated Construction Cost	<u>\$45,381</u>

TABLE 4
148TH LANE
ALTERNATIVE 2A - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	15" RC PIPE APRON	EACH	\$300.00	1	\$300
13	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
18	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$11,186
Contingency (10%)	\$1,119
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	\$13,098

TABLE 5
148TH LANE
ALTERNATIVE 2B - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	153	\$765
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	55	\$550
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	61	\$305
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	75	\$225
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	61	\$458
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	61	\$1,708
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	18" RC PIPE APRON	EACH	\$350.00	1	\$350
13	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
15	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	187	\$4,301
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	55	\$1,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$16,412
Contingency (10%)	\$1,641
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$18,846</u>

TABLE 6
148TH LANE
ALTERNATIVE 2C - 24" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$900.00	1	\$900
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$4.00	306	\$1,224
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	75	\$375
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	106	\$530
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	95	\$285
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	106	\$795
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	106	\$2,968
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	24" RC PIPE APRON	EACH	\$450.00	2	\$900
13	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	334	\$9,686
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
18	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	75	\$1,500
19	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
20	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
21	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$28,863
Contingency (10%)	\$2,886
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$32,543</u>

**TABLE 7
148TH LANE
ALTERNATIVE 3**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	10	\$1,000
3	GRUBBING	TREE	\$100.00	10	\$1,000
4	COMMON EXCAVATION	CU YD	\$5.00	270	\$1,350
5	GRANULAR BORROW	CU YD	\$8.00	1166	\$9,328
6	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$13,928
Contingency (10%)	\$1,393
Total Estimated Construction Cost	<u>\$15,321</u>

Public Works Committee

5. 4.

Meeting Date: 05/15/2012

By: Tim Himmer, Engineering/Public Works

Title:

Consider the Installation of Stop Signs and an In-Street Pedestrian Crossing Sign Near 150th Lane NW and Ute Street NW

Background:

On April 3, 2012 staff received a petition signed by twelve property owners in the neighborhood near the intersection of 150th Lane NW and Ute Street NW, including three of the four properties that make up the corner lots. They are requesting the installation of stop signs at this intersection, consideration for the installation of an in-street pedestrian crossing at the location of the existing City trail crossing on 150th Lane, and reduction of the speed limit on 150th Lane to 15 MPH (see attached maps). Staff has reviewed the signatures and compared them to Anoka County property records, and there appears to be one signature that does not match the property owner as identified in the Anoka County system. There is often a lag in updating the County system once a property transfers, so this may be the case for this particular property, but at this time staff is considering the petition complete with eleven confirmed property owner signatures.

City Charter requires the Council to receive all petitions submitted at the next available meeting, so this item was placed on the April 10th regular City Council agenda as a consent case and accepted. Traffic related requests such as this are typically forwarded to the Public Works Committee prior to Council action, therefore the item is before you tonight for consideration.

The organizer of the petition states that vehicles travel through the area at a high rate of speed, and use these roadways and a way to avoid the traffic signal on Alpine Drive and TH 47. It should be noted that Waco Street and 149th Avenue are Municipal State Aid (MSA) streets in the City's transportation system. Staff has performed a cursory review of the intersection in anticipation of this case and offers the following information.

- **Accident history** - Staff has requested an accident history report for this intersection, along with 150th Lane and Waco Street. The information was not available at the time of case preparation, but should be available for the meeting.
- **Traffic** - twenty-four hour traffic counts were taken on all four legs of this intersection by public works crews on Tuesday May 8th. The police department also placed the speed trailer along 150th Lane for a twelve hour period on April 17th. The peak hour count represents an approximate average value of 10% of the total daily traffic volume, and was found to occurred on 150th Lane between approximately 3:30 pm and 4:30 pm. The results of this traffic volume study are as follows:

STREET	LEG	DAILY TRAFFIC	PEAK HOUR TRAFFIC
Ute St	West	184	
Tonto St	North	170	
151st Ave	East	142	
150th Ln	South	417	51
INTERSECTION TOTAL		913	

- **Sight Distance** - The sight line for all approaches to this intersection are good and unobstructed.

Notification:

The organizer of the petition has been notified that this item is being placed on the Public Works Committee agenda.

Observations:

The Minnesota Manual on Uniform Traffic Control Devices (MNMUTCD) provides guidance on the placement of stop signs. The manual states that in many low traffic volume situations with no unusual history of intersection crashes, no control is an effective strategy because each driver is required to assume responsibility for proceeding safely through the intersection. This uncontrolled intersection can be considered a low volume intersection as is evidenced by the daily traffic counts shown above, therefore the installation of stop signs does not appear to be a good solution for this intersection.

The south leg of the intersection (150th Lane) is by far the busiest as it functions as a collector route off our MSA system to both Alpine Drive and Th 47. While speed was not a concern on this roadway; average speed of 20 MPH, maximum speed of 31 MPH, and 85th percentile speed of 24 MPH, the roadway curvature and mid-block City trail crossing do provide some special circumstances that may warrant additional investigation. A speed study could be requested from the state Commissioner of Transportation, in an effort to determine whether this section of street could receive a lower posted speed limit. An in-street pedestrian crossing may also further accentuate the striped trail crossing and serve to slow traffic through the area. Street chokers could also be consider for the roadway section at the location of the trail crossing, and could be incorporated into the design when this area is due for road maintenance activities, which is currently due for a sealcoat application. This type of improvement would be additional work beyond the scope of a typical sealcoat. Future road maintenance and reconstruction throughout the City are currently being discussed by the Council, and the timing for this street will be budget dependent.

Funding Source:

The investigation and analysis of this petition request has been and will continue to be accomplished utilizing staff time.

Staff Recommendation:

Staff recommends the following:

- A stop sign not be posted on this intersection.
- Staff should investigate the opportunity to include enhanced signage and chokers on 150th Lane when future maintenance activities are completed in the area. Since this is an improvement that is more than what is typically done, and is at the request of the residents in the area, additional assessments may be required.

Additionally, if the Council and residents should desire, a request could be forwarded to the state Commissioner of Transportation to request a speed study be conducted on this segment of 150th Lane.

Committee Action:

Based upon discussion.

Attachments

[Location Map](#)

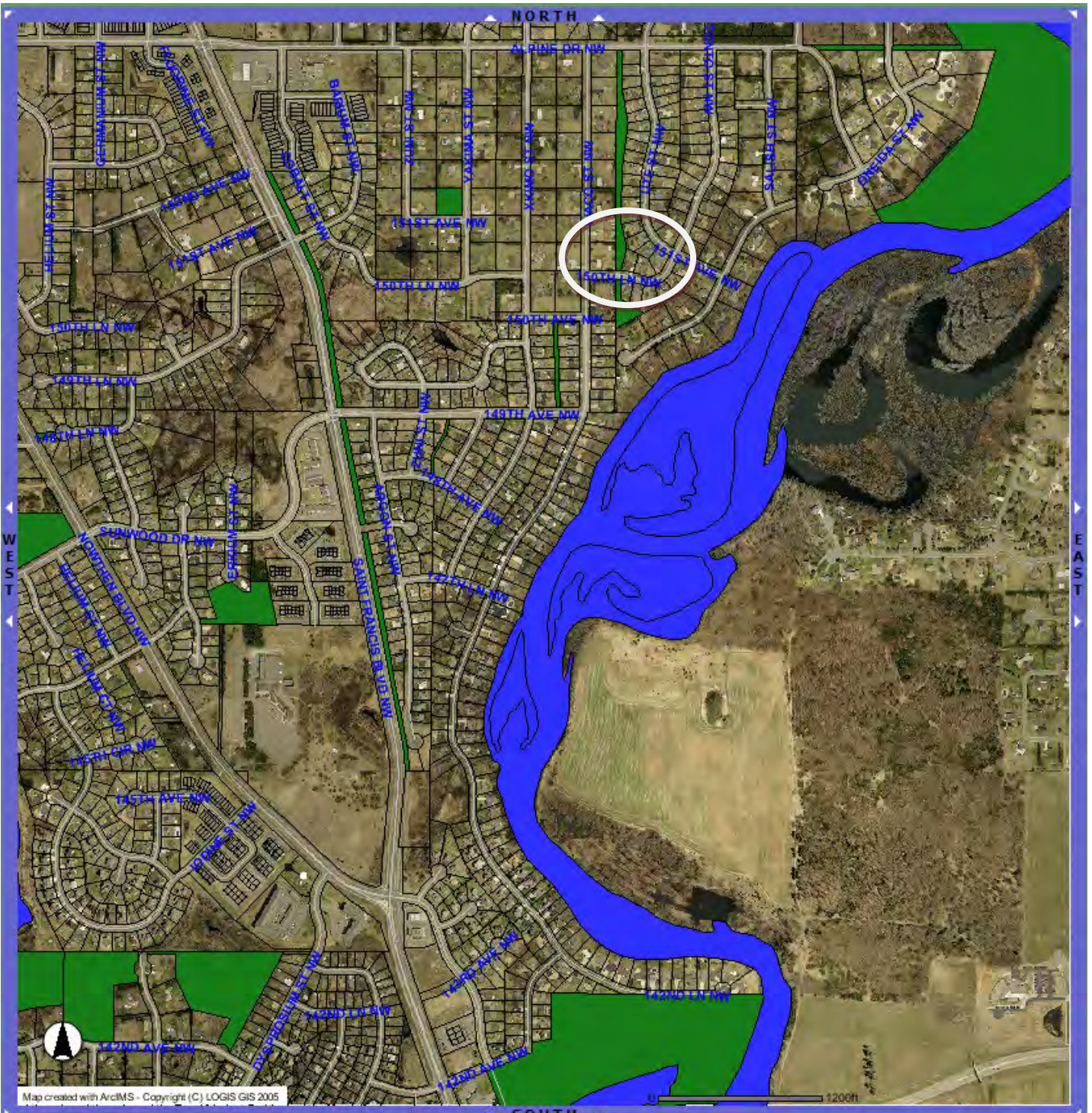
[Petition Map](#)

[Petition](#)




Form Review

Inbox	Reviewed By	Date
Brian Olson	Kathy Schmitz	05/11/2012 10:48 AM
Kurt Ulrich	Kurt Ulrich	05/11/2012 11:50 AM
Form Started By: Tim Himmer		Started On: 05/08/2012 05:09 PM

Final Approval Date: 05/11/2012





-  Confirmed signature (11)
-  Unconfirmed signature (1)
-  Existing City trail crossing

Petition for Traffic and Speed study as well as a Informal Crosswalk for:

150th lane Ramsey Mn,55303

PRINT NAME

ADDRESS

SIGNATURE

✓ 1. BEN BERGAN / 15108 UTE ST / Ben Bergan

✓ 2. Deborah Meyer / 4970 150LN / Deborah Meyer

✓ 3. Jesse Anderson / 4980 150LN / Jesse Anderson

④ Edward Otterness / 4990 150th Ln / Ed Otterness

✓ 5. Kelly Provance / 4981 150th LN / Kelly Provance

✓ 6. April Miller Larson / 4960 150th Ln NW / April Miller Larson

✓ 7. Kris Urman / 4940 150th Ln NW / Kris Urman

GOAL:

-
- **Stop sign on intersection of 150th lane & Ute st**
- **Informal crosswalk on 150th lane**
- **Reduction of speed to 15 MPH on 150th lane**

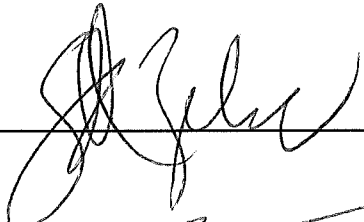
Petition for Traffic and Speed study as well as a Informal Crosswalk for:


150th lane Ramsey Mn,55303

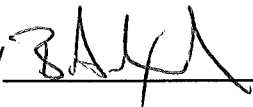
PRINT NAME

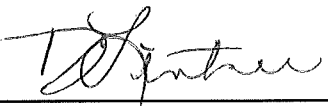
ADDRESS

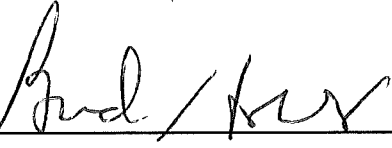
SIGNATURE

✓ 1. Scott Zehner / 15110 JRESTMW / 

✓ 2. Baha Faran / 15110 Tonto St / 

✓ 3. Bill Alexandre / 15111 Tonto / 

✓ 4. Diane Lentz / 15119 Tonto / 

✓ 5. BRAD HOLMBO / 15110 Tonto St / 

6. _____ / _____ / _____

7. _____ / _____ / _____

8. _____ / _____ / _____

9. _____ / _____ / _____

10. _____ / _____ / _____

Public Works Committee

5. 5.

Meeting Date: 05/15/2012

By: Tim Himmer, Engineering/Public Works

Title:

Discuss Trail Easement at 15620 Krypton Street NW

Background:

In January of 2012 staff was contacted by an individual that was considering purchasing the property at 15620 Krypton Street NW. The main topic of conversation was related to the existing bituminous trail that extends down the east property line and into the backyard. He was inquiring whether there was a purpose for this trail, and when it might be utilized in the future. Reviewing property files it was discovered that this trail was intended for a dual purpose; a maintenance access to the City's sanitary sewer system, and a pedestrian connection.

When WILDLIFE SANCTUARY 3RD was subdivided in 2003 the trail construction was an obligation of the development agreement (see attached development agreement excerpt), and was to be located along the common lot line between lots 5 and 6, Block 3. Extension of a gravity sanitary sewer was also a requirement of the project, such that an existing lift station could be eliminated when a future trunk extension was complete through a neighboring development to the north and west. During construction the sanitary sewer had to be realigned slightly due to environmental constraints, and additional sanitary sewer easements were granted for this new alignment at that time.

It appears the trail was constructed over the top of the sanitary sewer trunk line that will be used in the future, and not along the entire extents of the common lot line, as referenced. The separate recordable trail easement referenced in the development agreement does not appear to exist, therefore we currently only have rights to access the easement area for drainage and utility purposes.

This item was discussed by the Public Works Committee on February 21, 2012, with the direction being to leave things as is until such time that a pedestrian trail easement could be secured. There was also discussion that since this individual was not the property owner there was no ability to negotiate acquisition of such an easement. Staff made contact with the property owner to the east of this lot to determine whether that they would be open to the discussion of a pedestrian trail easement along the common lot line, as was originally proposed within the dedicated drainage and utility easement. The property owner expressed the desire to vacate a portion of the drainage and utility easement along the rear lot line, potentially in exchange for a pedestrian trail dedication along the west lot line. After reviewing the existing sanitary sewer in the rear yard it was determined that the City would not be able to accommodate this request, which was relayed to the resident in early April, and discussed terminated at that point.

City staff relayed this information back to the prospective lot purchaser of 15620 Krypton Street, who then asked that the attached letter be forwarded to the Public Works Committee and Mayor for consideration. It was also discussed that they would soon be closing on the property, which will be occurring on May 25th. They are concerned about having a trail through the center of his backyard forever and are requesting that the City relocate the trail into the easement along the common lot line, as it was originally intended. They understand the dual purpose of the trail and are accepting of its intended use, for utility maintenance and pedestrians, but would like to see it placed on the lot line as intended. The letter outlines a couple of proposed scenarios to resolve this matter within their property and are seeking Committee and Council reaction.

Notification:

Staff has sent notice to both property owners regarding this meeting.

Observations:

From a public works perspective a paved access is preferred for utility maintenance in these situations because we often receive complaints from residents after we access their property with heavy equipment and rut up/damage their lawns and/or irrigation systems. Currently there is no need to access the sanitary sewer in this location, because it will not be utilized until the adjacent area to the north and west develops, or the City decides it wants to construct this gravity system. It will most likely remain in place until such time that development of the adjacent land occurs and the final trunk alignment is reviewed with a land use application. Staff believes it would even be appropriate to require the adjacent future development to be responsible for revisions to the sanitary sewer system that would support their project, and also to construct approximately 100' of trail along the common lot line as a pedestrian connection (if necessary).

A pedestrian trail in this location is currently shown on the City's Master Park and Trail Plan, so this matter will need to be addressed at some point.

Funding Source:

There is no funding request associated with this matter, to date staff time has been used for research and communications.

Staff Recommendation:

Staff would like to know if the Public Works Committee has changed it's stance on resolving this matter in the short term, and whether an agreement can be reached with the property owner on their proposal letter.

Committee Action:

Based upon discussion.

Attachments

[Location Map](#)

[Property Aerial](#)

[Wildlife Sanctuary 3rd Plat](#)

[Development Agreement Excerpt](#)

[Property Owner Letter to the Committee](#)

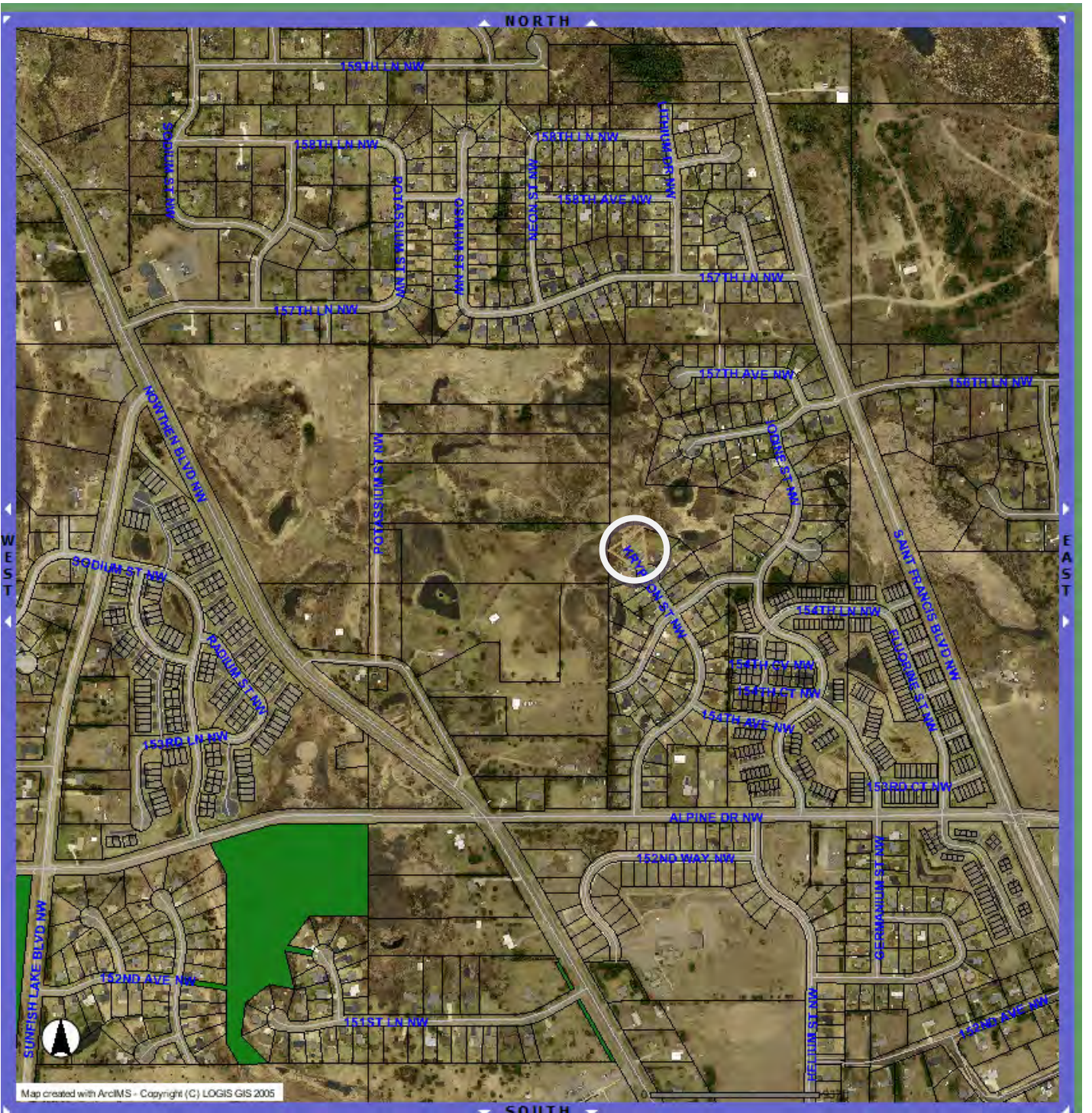
[Sanitary Sewer Alignment](#)

[Master Park and Trail Map](#)

Form Review

Inbox	Reviewed By	Date
Brian Olson	Kathy Schmitz	05/11/2012 10:48 AM
Kurt Ulrich	Kurt Ulrich	05/11/2012 11:52 AM
Form Started By: Tim Himmer		Started On: 05/08/2012

Final Approval Date: 05/11/2012

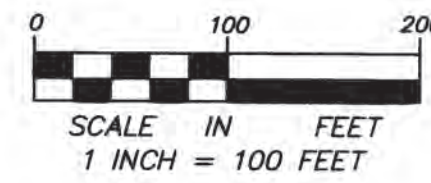


15620 Krypton Street NW Location Map



15620 Krypton Street NW

WILDLIFE SANCTUARY THIRD ADDITION

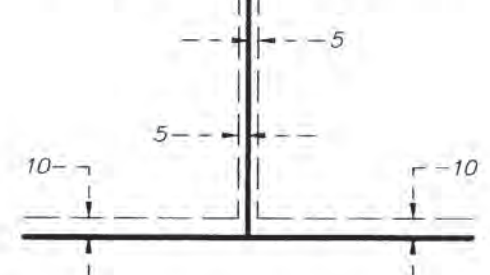


FOR THE PURPOSES OF THIS PLAT, THE SOUTH LINE OF THE SOUTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 23, TOWNSHIP 32, RANGE 25 IS ASSUMED TO HAVE A BEARING OF SOUTH 88 DEGREES 59 MINUTES 07 SECONDS WEST.

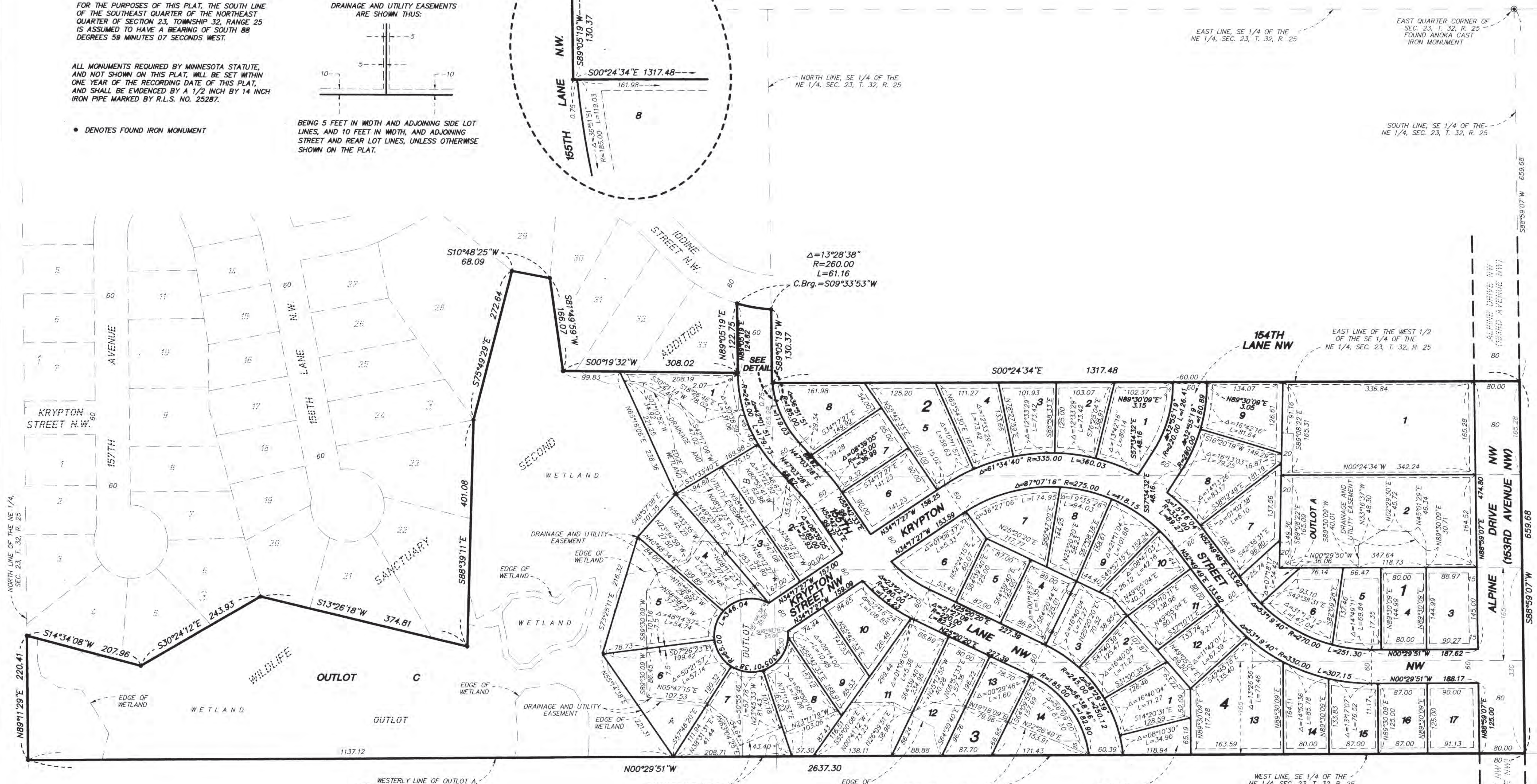
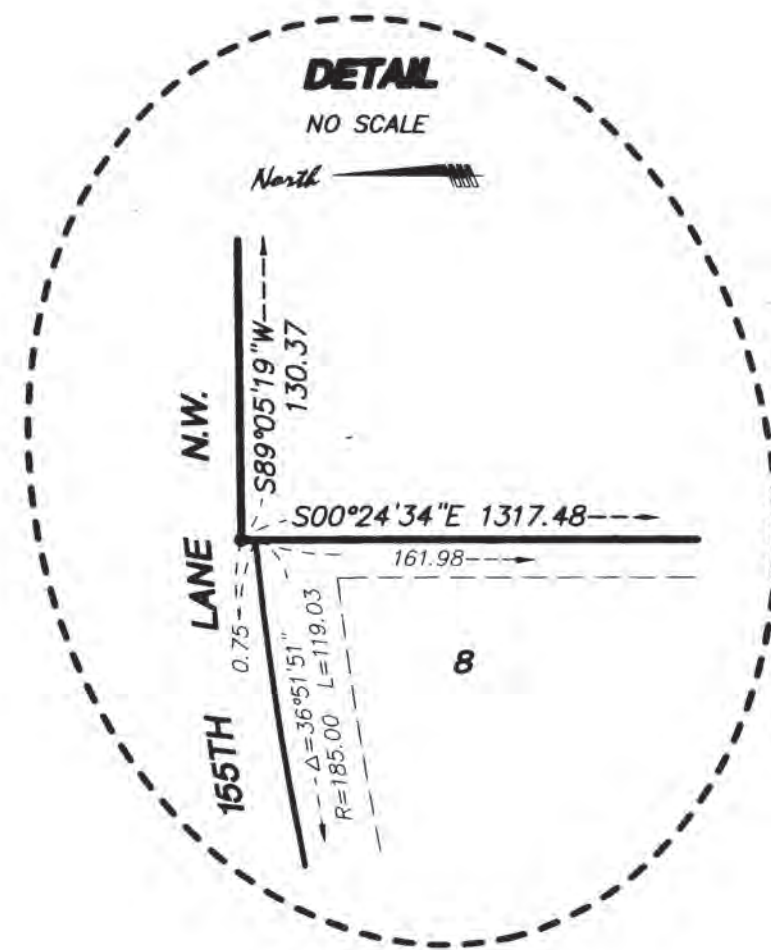
ALL MONUMENTS REQUIRED BY MINNESOTA STATUTE, AND NOT SHOWN ON THIS PLAT, WILL BE SET WITHIN ONE YEAR OF THE RECORDING DATE OF THIS PLAT, AND SHALL BE EVIDENCED BY A 1/2 INCH BY 14 INCH IRON PIPE MARKED BY R.L.S. NO. 25287.

• DENOTES FOUND IRON MONUMENT

DRAINAGE AND UTILITY EASEMENTS ARE SHOWN THUS:



BEING 5 FEET IN WIDTH AND ADJOINING SIDE LOT LINES, AND 10 FEET IN WIDTH, AND ADJOINING STREET AND REAR LOT LINES, UNLESS OTHERWISE SHOWN ON THE PLAT.



PASSE ENGINEERING, INC.
REGISTERED PROFESSIONAL ENGINEERS
LICENSED LAND SURVEYORS

SECTION V
TRANSPORTATION AND LANDSCAPING

17. **Park Trail Development and Fees.** The City Council has established a trail development fee in the amount of Four Hundred Seventy Five Dollars and no cents (\$475.00) per lot. The three (3) existing homesteads are exempt from this fee. The amount of trail fees due on the Plat is Twenty One Thousand Dollars Three Hundred Seventy Five Dollars and no cents (\$21,375.00) (45 lots x \$475.00 per lot). The **DEVELOPER** herein agrees to construct a sidewalk along one side of the Krypton Street N.W. cul-de-sac to the common property line of Lots 5 and 6 of Block 3. The **DEVELOPER** also agrees to construct an 8 foot wide bituminous trail along the entirety of the common property line between Lots 5 and 6 of Block 3. The **CITY** herein agrees to credit the trail development fees due on the **Plat** in an amount equal to the surface costs associated with the trail and sidewalk segments described in this paragraph. The amount of the credit shall be One Thousand Eight Hundred Sixty Dollars and no cents (\$1,860.00). Therefore, a payment in the amount of Nineteen Thousand Five Hundred Fifteen Dollars and no cents (\$19,515.00) is due to the **CITY** for this **Plat**.

The **DEVELOPER** herein agrees to provide the City with a separate document, in recordable form, granting the City a fifteen foot (15') wide easement for trail and access purposes on the common property line between Lots 5 and 6 of Block 3.

18. **Sidewalk Construction.** The **DEVELOPER** herein agrees to construct and pave, in accordance with **CITY** standards, a five (5) foot wide concrete sidewalk with pedestrian ramps in locations prescribed by the **CITY**, not to exceed 5% in grade, along the east side of Krypton Street N.W., and the south side of 155th Lane N.W. and 154th Lane N.W. The costs associated with sidewalk construction are not eligible as a credit towards the Trail Development Fees and/or Park Dedication requirements due on the **Plat**.

19. **Tree and Sod Planting Plan.** Sodded boulevards, in addition to yard trees in accordance with the Final Tree Plan dated March 27, 2003, revised April 17, 2003, are required for each lot in the **Plat** prior to issuance of a Certificate of Occupancy. The **DEVELOPER** is required to submit a Final Tree Plan for **CITY** approval that identifies existing tree growth within the **Plat** that will be protected during construction, and the location and species of the new plantings. The trees to be protected must be identified on the grading plan, and the plan must require the installation of 'tree save fences' prior to land clearing or grading. In addition, the requirement for this Plan shall be fulfilled by the **DEVELOPER** as follows:

- a) Minimizing the impact of construction on trees in accordance with Minnesota Extension Service publication "Protecting Trees From Construction Damage" (Publication #NR-FO-6135-S).
- b) For trees intended to be planted between the sidewalk and the street, the specific locations must be staked in advance of planting and approved by the **CITY**. In the event the weather is not conducive to tree or planting sod in the boulevard at the time of Certificate of Occupancy, the Building Permit holder shall place a cash deposit in

Josh & Shawna Dvorak
17131 Nixon Drive NW
Elk River, MN 55330
February 26, 2012

City of Ramsey
Public Works and City Council Members
7550 Sunwood Dr NW
Ramsey, MN 55303

Dear Mr. Himmer:

We wanted to thank you for all of the support you have provided in our pursuit to resolve the issue with the path in the backyard of our future home. We, as the potential homeowners of 15620 Krypton Street NW have developed three proposed scenarios for the dual-purpose utility maintenance and pedestrian trail located on this property. We believe it is best to form a partnership with the city and provide a solution that is consistent with the original plan, but will also be a beneficial and cost effective solution for all parties involved; the city, us as the future homeowner, and the taxpayers of the City of Ramsey.

This correspondence outlines the three potential scenarios for this dual-purpose trail though the backyard at 15620 Krypton Street NW. It outlines the benefits to the city and the homeowner, the concessions that would be made by both, the responsibilities for each, and the estimated costs.

OBJECTIVE

We are proposing a partnership with the City of Ramsey to move the utility path currently located across the middle of the backyard to a straight line following the lot line as documented in the original plans in a way that would meet the needs of all parties involved.

SCENARIO ANALYSIS

Scenario one (most optimal for all involved):

To assist the city achieve their goal for the development of a interconnected pedestrian trail system throughout the city, we would agree to sign the agreement allowing this trail to reside on our property. We suggest the sale of the land to the city at 50% of the fair market value ⁽¹⁾ \$1,365.64 (7.5 * 199 * 1.83 *.50). In this scenario the city would obtain full use of 7.5 ft. of land along the eastern lot line. The City would be responsible for the removal and disposal of the current pavement crossing the yard, back to the easement along the property line. The homeowner would be responsible for restoration of the lawn following the removal. To provide access to service the current and future utility line, a manhole could be installed on the paved trail. As discussed with the city engineer and public works committee, this is a reasonable request and would satisfy the needs of the city.

Though this partnership, the benefits to the city would be:

- City would obtain the legal rights for a pedestrian trail in addition to the utility access needed along the property lot line

- City would not be responsible for lawn restoration
- Pavement removed can be recycled for the pavement needs of future paths, reducing the cost for disposal and materials
- City pays only 50% of fair market value for the pedestrian trail access rights

Though this partnership, the benefits to the homeowner would be:

- Path would be straight, preserving integrity of the backyard
- Homeowner would have less security risks with the path directed away from the property and further from the home vs. in the middle of the backyard (i.e: vandalism, theft, break-ins, etc.)
- Homeowner would have path moved to a more acceptable location

Scenario Two:

This scenario would entail the city waiting for the need to arise for the pedestrian trail development, leaving the path as is until an unknown future date. At that time, the city would be required to approach the property owner to make an offer for the purchase of property for this new use. Due to the delay in resolution for the homeowner, it is uncertain if the property would be made available for the pedestrian access. If the homeowner were to agree to selling the access to the city for a pedestrian trail, the homeowner would require the original design specifications, locating the path along the property line as stated in scenario one, and would require the city to be responsible for removing and disposing of the current pavement, the lawn restoration, and the payment for 100% of the lands full market value.

Just some of the downfalls of this scenario are:

- A higher cost to the City and taxpayers (the City would be responsible for the removal and disposal of pavement as described in scenario one in addition to the cost of lawn restoration)
- Additional cost for the purchase of the property (the City would also be required to pay 100% of fair market value vs. 50% as offered in scenario one)
- The path would be a nuisance for homeowner for extended period of time

Scenario Three:

This scenario would entail the city forcefully obtaining land through eminent domain. This law is used for government utilities, roadways, etc. which provide for the greater need of the community. (Not specifically designed for trails) This scenario would be a long, drawn out process as the city would first have to receive judgment proving that easement is in greater interest of the community adding court costs and time. Homeowner would then have the right to contest which would bring suit back to court adding to the overall cost and time. Upon settlement, assuming the city obtains the rights to the easement, the city would then make offer to

the homeowner for the purchase of the easement. Not inclined to work with the city homeowner would decline the offer bringing the matter back into the legal system where through constitutional law, the judge would rule for the city to pay fair market value at that point in time. Currently this would be approximately \$1.83 per square foot to purchase, or approximately 60-70% of purchase price to obtain easement.

The downfalls of this scenario would be as follows, but not limited to:

- City and homeowner would incur added expense
- This could be a long, drawn out process delaying the plans for the city to move forward with future use of the trail
- City would pay more than 50% which is offered in scenario one
- Depending on easement granted, the city could potential be responsible for removal and disposal of pavement in addition to lawn restoration

CLOSING

I feel I should provide some context around why this is so important for us to find a fair and reasonable resolution to the situation. After being raised in Ramsey, where my parents still reside, I feel that this community with the strong family values is where we want to live and raise our family. We having been looking for a home like this for over a year and we were very excited to find this home. The only thing holding us back is the current location of this trail. As our offer has been accepted by the current homeowner and is now in the hands of the bank, we are ready to move forward provided we can come to an agreement with the future of this trail.

We understand your hesitation to work with us since we do not yet own this home and would like to confirm that any of the solutions above are contingent upon our purchase of this home. Also, please know that we would like to have a resolution and agreement made soon so we can abandon our pursuit for the purchase of this home if necessary.

We appreciate the opportunity and time to work alongside the city to obtain the most beneficial option for all parties involved. If you wish to discuss further please don't hesitate to contact us.

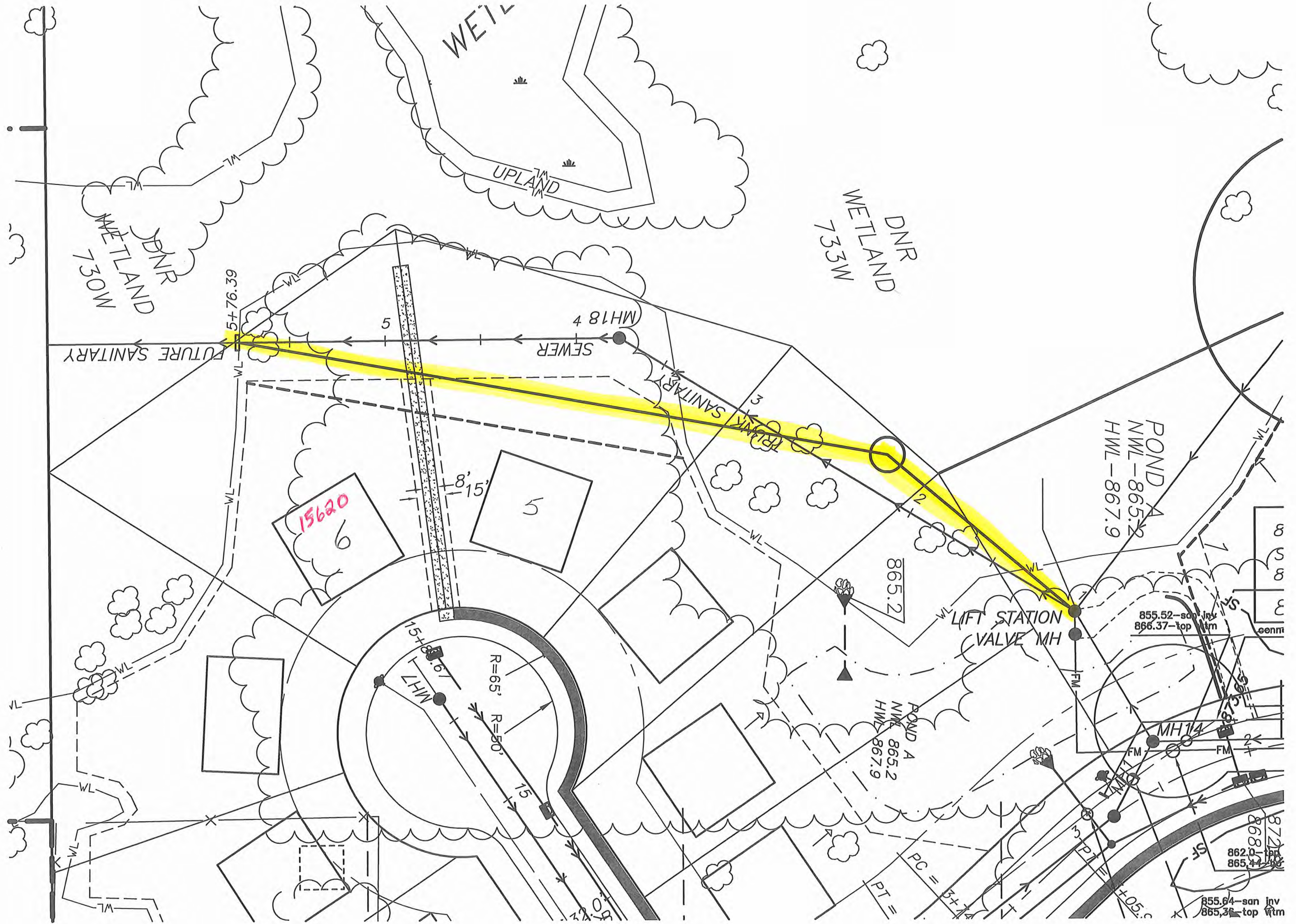
Sincerely,



Josh & Shawna Dvorak

Cc: Mayor Bob Ramsey
Cc: Public Works Committee

(1) Estimate based on- numbers provided of city documentation, could vary slightly



DNR WETLAND
7330M

WETLAND

UPLAND

DNR
WETLAND
733W

FUTURE SANITARY

SEWER

MH18 4

15620
6

5

865.2

LIFT STATION
VALVE MH

POND A
NWL-865.2
HWL-867.9

855.52-san inv
866.37-top atm

POND A
NWL 865.2
HWL 867.9

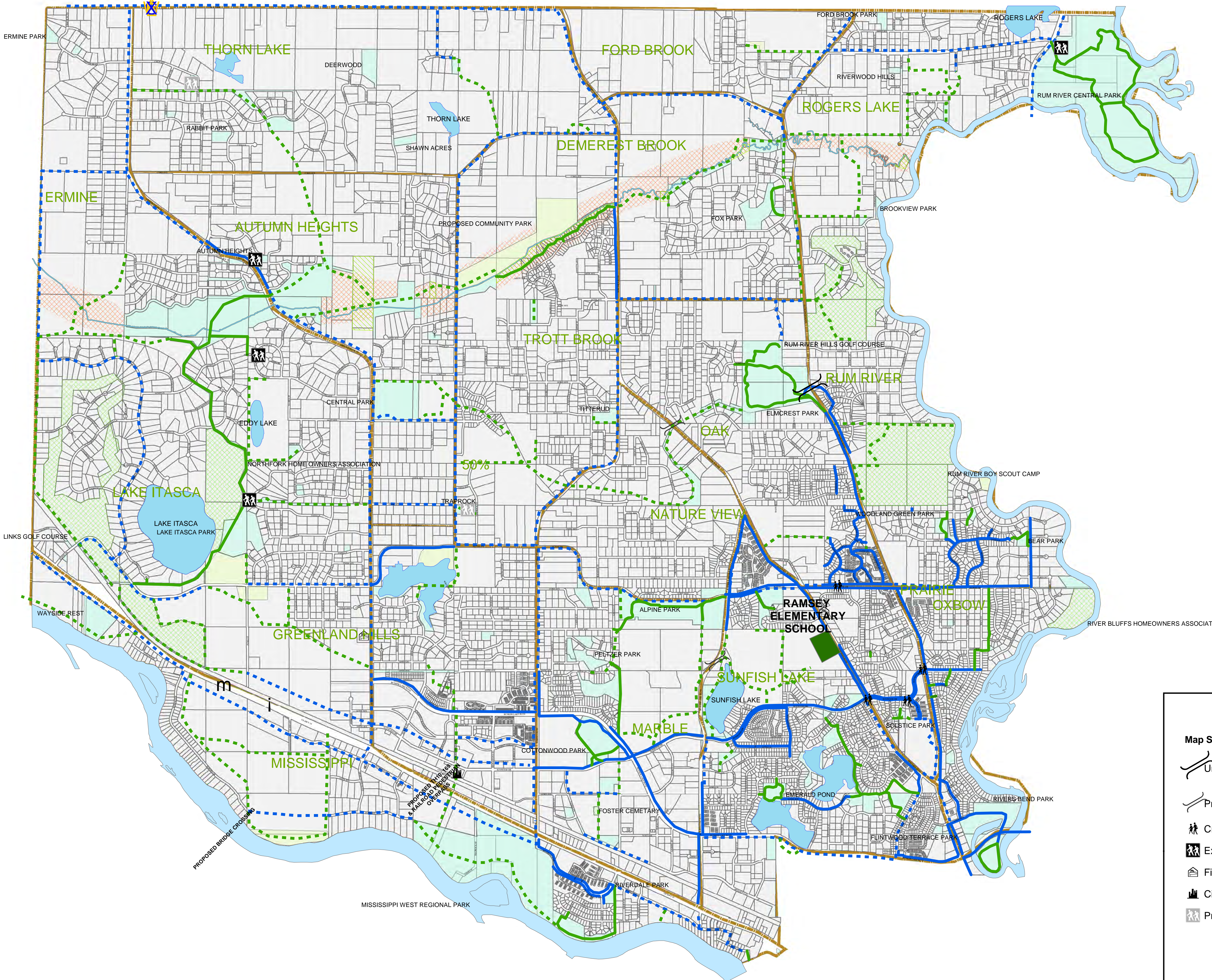
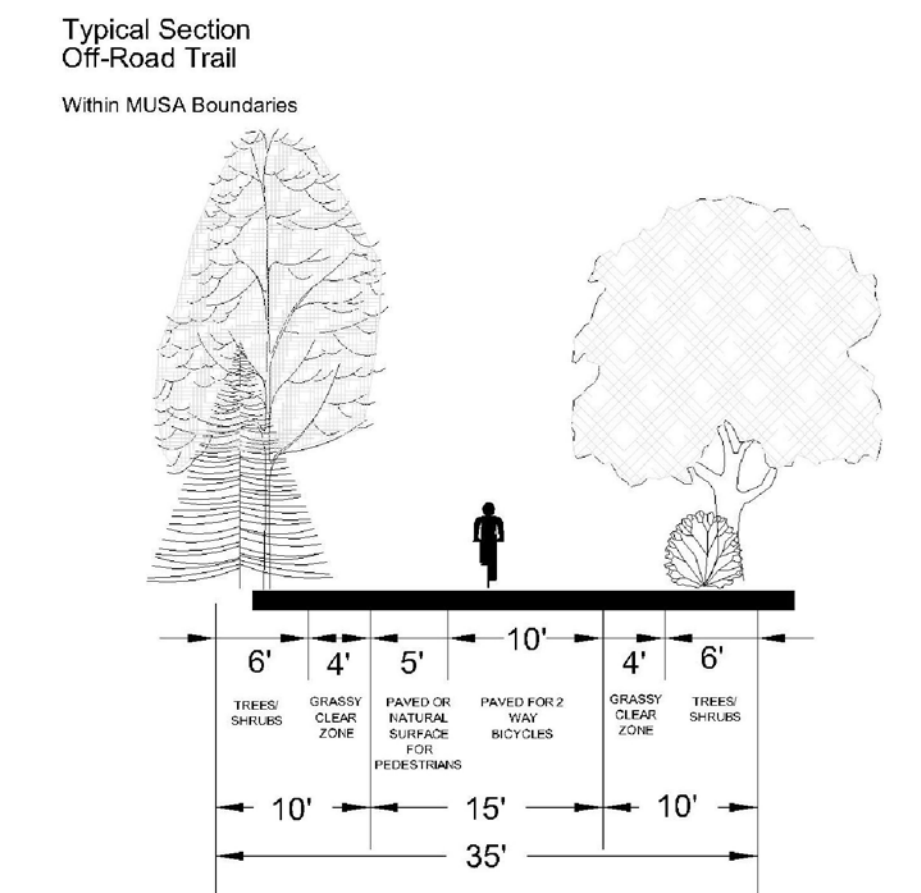
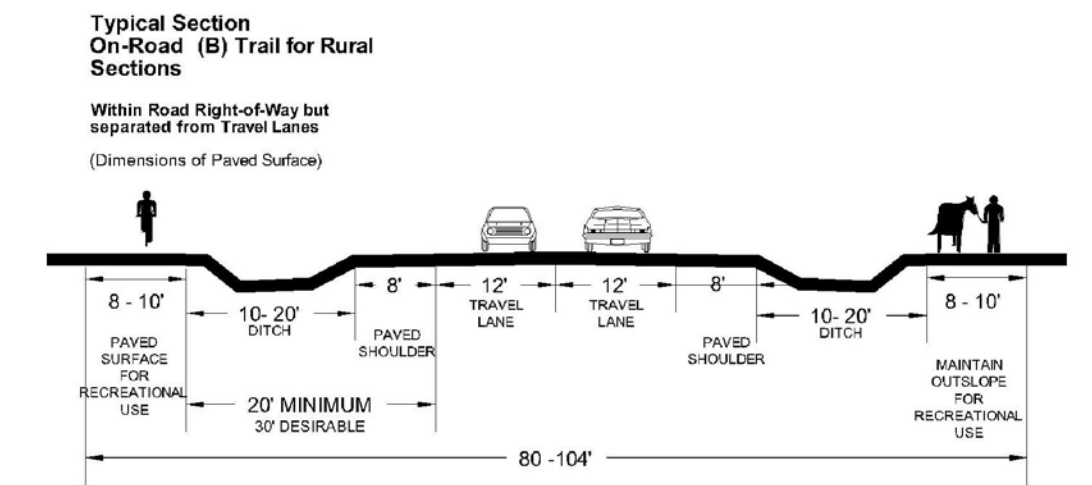
MH14

872.0
868.0
862.0
865.4

855.64-san inv
865.36-top atm

PC =
PT =

City of Ramsey Master Park and Trail Plan



Legend

Map Symbols	Existing Trails
Underpass	Off-Road
Proposed Underpass	On-Road (Separated)
Crosswalk	Proposed Trails
Existing Trailhead	Off-Road
Fire Station	On-Roadway (separated)
City Hall	Conservation Easements
Proposed Trailhead	Parks
	Quasi-Public
	Existing Parks
	Proposed Park
	TROTT BROOK SPECIAL PROTECTION CORRIDOR
	Ramsey Recreation Districts

Public Works Committee

5. 6.

Meeting Date: 05/15/2012

Submitted For: Grant Riemer

By: Grant Riemer, Engineering/Public Works

Title:

Winter Maintenance Summary

Background:

With the winter of 2010-2011 being one of the worst on record, 2011-2012 was without a doubt one of the mildest. Attached to the case is a comparison of the two seasons. In the 2011-2012 season there were four 2” snow events and one 6”-8”event that took two days to clean up. The last storm consisted of several false start times from the weather service, combined with high moisture content to make for a very frustrating time for everybody. Most drivers compared it to plowing concrete. People I spoke with in surrounding communities dealt with the same problems, late start, heavy snow and equipment breakdowns. The two equipment breakdowns that delayed plowing for us were a broken serpentine belt and a hydraulic hose rupture on separate trucks. Not big ticket items, but time consuming. Most of the complaints received during the storm were related to plowing time. With the combination of problems mentioned above, some areas weren’t plowed out till late afternoon. Overall for the season we had approximately 75 calls with probably 60 coming from the last snow fall. The breakdown of calls for the year was:

6-Mailbox damage

8- Sod damage

61-Misc. The majority of these calls came in the last event and were questions about when plowing would be done, why is plowing taking so long, etc...

For the 2012-2013 we will be making some changes to the plowing program. Some route changes will be made to speed up the plowing in the COR area, seasonal help will plow a majority of the trail system and contracted services will be used around the Municipal Center and possibly the fire stations. One other possible change would be the introduction of an Automatic Vehicle Location (AVL) system for the plow fleet. The biggest change though is the salt shed will be full at the beginning of the season, and that is a first for us!

Notification:

Observations:

Funding Source:

N/A

Staff Recommendation:

Informational only

Committee Action:

Based on discussion

Attachments

Budget Comparison

Form Review

Inbox	Reviewed By	Date
Brian Olson	Brian Olson	05/08/2012 09:58 AM
Kurt Ulrich	Kurt Ulrich	05/10/2012 01:58 PM
Form Started By: Grant Riemer		Started On: 04/24/2012 12:43 PM

Final Approval Date: 05/10/2012

Snow Removal Budget Comparison

	Cost center	2010-2011	2011-2012	Diff (\$)
Wages				
Full time regular	6102-0312	\$71,159.15	\$22,049.47	\$49,109.68
Full time OT	6103-0312	\$30,724.77	\$8,341.00	\$22,383.77
On call Temp	6105-0312	\$3,777.00	\$1,617.00	\$2,160.00
Fuel				
Diesel	6225-0312	\$28,849.39	\$14,040.74	\$14,808.65
Gas	6223-0312	\$4,205.58	\$744.00	\$3,461.58
Parts (in house repairs)	6257-0312	\$15,993.70	\$21,041.59	-\$5,047.89
Parts (outsourced repairs)	6388-0312	\$2,308.23	\$3,263.13	-\$954.90
Sand	6261-0312	\$2,000.00	\$1,988.71	\$11.29
Salt	6263-0312	\$76,013.33	\$74,336.63	\$1,676.70
		Diff		
Salt used		1,600.00	400.00	1,200.00
Sand used		400.00	120.00	280.00
Full plow		16.00	6.00	10.00
Minor plow		20.00	6.00	14.00

Diff (%)

222.725%

268.358%

133.581%

105.469%

465.266%

-23.990%

-29.263%

0.568%

2.256%

300.000%

233.333%

166.667%

233.333%

Public Works Committee

5. 7.

Meeting Date: 05/15/2012

Submitted For: Grant Riemer

By: Grant Riemer, Engineering/Public Works

Title:

The Use of AVL Systems in Public Works

Background:

AVL or Automatic Vehicle Locator systems have been around since the late 1990's in public works. Fleet managers wanted a way to track their vehicles in a real time application and to improve service to their residents. With today's technology, prices have come down considerably. Systems can be bare bone and provide just real time location on a map, to just about anything you can imagine. In a public works application some common uses are snow removal, refuse collection, street sweeping, etc.... Sensors can be mounted to let you know if the plow is down, is the sander on, engine diagnostics, what type and how much chemical is being applied and numerous other data streams. An AVL system would also feedback information on where the truck was in its route, direction the truck is traveling and percentage of route completed. Staff could easily spot any roads that could have been missed or give residents an estimate on when they could expect to be plowed out. Information would be monitored by PW staff and could provide information to residents on line. AVL systems also have the capability of being integrated into other software programs that may be already in use. With the use of an on board camera the driver can take a quick snap shot of a maintenance problem, have the GPS mark it, and develop a work order from the information provided.

There are 3 vendors on the CPV (Cooperative Purchase Venture) list for the State of Minnesota that sell AVL systems. Force America, Ameritrak, and Location Technology. Staff contacted Location Technology and arranged a short webinar presentation to view their product line. Location Technology also was given an inventory of our snow removal fleet and has provided a cost estimate for startup and system maintenance. That information attached to the case. The abbreviated version of the costs are as follows:

Upfront costs for hardware and operator training for 15 snow removal vehicles-\$9112.00

Recurring monthly costs for system-\$259.60/month \$3115.00/yr

The base units can also be moved from vehicle to vehicle, so when snow season is over, the AVL equipment could be moved to sweeping and mowing equipment if desired.

Notification:

Observations:

Funding Source:

General Fund Cost Center 6580-0311

Staff Recommendation:

AVL would be a useful tool in our public works operation. The data gathered would help streamline PW operations and provide useful information for staff and residents as well.

Committee Action:

Direct staff to continue to gather information and pricing for consideration in the 2013 budget.

Attachments

AVL Cost Estimate

Form Review

Inbox
Brian Olson
Kurt Ulrich

Reviewed By
Kathy Schmitz
Kurt Ulrich

Date
05/11/2012 10:48 AM
05/11/2012 11:47 AM
Started On: 04/24/2012 12:48 PM

Form Started By: Grant Riemer

Final Approval Date: 05/11/2012



INTELLIGENT MOBILE BUSINESS AND FLEET MANAGEMENT SOLUTIONS

Proposal For

The City of Ramsey, MN Public Works

Navigo GPS/AVL Fleet Management System

Submitted By

Location Technologies, Inc.

4/25/2012

Parties Involved

Proposed

City of Ramsey, MN
14100 Jaspar Street NW
Ramsey, MN 55303

Grant Riemer, Street Supervisor
griemer@ci.ramsey.mn.us
Phone (763) 433-9863

Proposing Company

Location Technologies, Inc. (LTI)
6214 NW Kelly Drive
Kansas City, MO 64152

Eric Cowger, President
ecowger@loctech.com
Phone (816) 741-3169 x110
Fax (816) 741-3405

Proposal Contents

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Warranty, Terms, & Conditions	22

4/25/2012

Grant Riemer, Street Supervisor
City of Ramsey, MN
14100 Jaspar Street NW
Ramsey, MN 55303

Dear Mr. Riemer,

Location Technologies, Inc. (LTI) is pleased to provide the City of Ramsey, MN the attached proposal for a Navigo GPS/AVL Fleet and Operations Monitoring System for snow and ice control management.

LTI has manufactured and installed AVL and Wireless Data Systems around the world for nearly 20 years. We specialize in developing solutions for Public Works, Public Safety, and Transportation. We have 100's of satisfied public sector clients across the country in addition to a number of commercial clients.

We are proposing our Navigo GPS/AVL Fleet Management Solution for this project. The equipment and software proposed herein is fully compliant with the specifications of the project and we accept the terms and conditions given to provide a system.

LTI intends to continue our position as a leader in fleet management through a combination of participation in industry organizations, continued product improvement, and keeping abreast of current trends in all of the various technologies surrounding the industry. For example, the LT6 GPS/AVL Modem is literally the 6th generation of our GPS/AVL Modem devices.

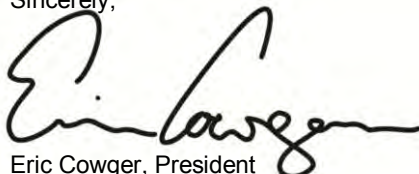
As systems integrators and experienced engineers our approach to all projects is to partner with the customer to provide the best system to meet the particular needs of a customer and not try to 'force fit' a canned solution. From start to finish you will find that we will not only be responsive to your stated requirements but will also be flexible and able to handle changes in scope and requirements.

We sincerely believe that we are offering the overall best proposal for the following reasons:

- Unparalleled experience in the industry.
- Proven, field tested equipment.
- Full support from company management for the successful completion of project.
- A modular equipment approach that offers the greatest flexibility and maintainability.
- A financially strong, stable, and debt-free company with nearly 20 years experience.
- We are a fully certified Sprint Business Partner.

In closing, we would like to thank you again for the opportunity to provide a proposal for this important project and to ask you to please contact us should you require further information.

Sincerely,



Eric Cowger, President
(816) 741-3169 x110
ecowger@loctech.com

Executive Summary of Proposal

For the City of Ramsey, MN (“City”) we are proposing our Navigo GPS/AVL Fleet Management System. The proposed Navigo GPS/AVL Fleet Management System is a solution that provides vehicle performance data, location tracking, operations monitoring, and reporting via a user-friendly web interface. Location Technologies, Inc. is proposing and is prepared to supply all project management, installation, and implementation services necessary to deliver a working solution.

The Proposed Navigo GPS/AVL Fleet Management System will provide the City with an automated real-time GPS/AVL system that is simple to operate and will reliably collect and disseminate data in a useable format based on information provided by on-vehicle sensors, monitors, and other sources (i.e., weather data, operator-generated data). This accumulated operations data can be used to plan maintenance activities and direct the maintenance vehicles to carry out their planned activities. The proposed system will give the City a holistic, department wide view of its operations and allow it to streamline its maintenance and operations functions by using aggregated data to make informed decisions simply and efficiently.

Location Technologies, Inc. takes a unique operations-centric approach to GPS/AVL Fleet Management. We are pushing the boundaries of GPS/AVL fleet management systems past the vehicle and on to the entire operation. Managers of fleet-based operations have been limited by the dot-on-a-map mentality for too long. We are pioneering a next-generation GPS/AVL fleet management approach that envisions the role of our systems as great aggregators of whole operations data, and as such must openly accumulate, interface, and extend all the operations data that is relevant to the tasks being performed. Operations managers need information from numerous sectors of their operations in order to make the correct decisions. These areas include environmental data, traffic data, geographic data, Route data, vehicle data, fleet data, operations data, inventory/asset data, HR/personnel data, fiscal data, and others.

The proposed NAVIGO GPS/AVL Fleet Management System is extremely beneficial in the management and daily operations of a statewide maintenance fleet program by providing:

- Tools to better monitor maintenance systems, including data necessary for managers and operators to make informed and timely operational decisions (e.g., understanding the necessary amount of de-icing chemicals to apply to particular stretches of road);
- Tools to better track the resources (vehicle, personnel, materials, etc) used to maintain specific roadway segments.
- Planning information to other stakeholders such as City and regional transportation agencies or metropolitan planning organizations with adjoining responsibilities;
- Useful traveler information for dissemination to the public (i.e., icy road alerts).

LTI is proposing, at the very high level, the following items:

In-Vehicle Hardware

- LT6 GPS/AVL CDMA Modem
- LT Vehicle Diagnostic Bus Interface Module
- Interface assemblies for Snow/Ice Control

Network Servers

- LT Wireless Data Server Software
- LT Events Engine
- .NET Web Services (3rd Party Interface)

Management Software

- LT Navigo Explorer WebMap Platform
- Public Works Management Module
- Fleet Management Module
- 3rd Party Dispatch & Mgmt SW Interface Module

Field Communications

- CDMA or GPRS Wireless Data Communications

The proposed City GPS/AVL Fleet Management System will give the following functionality:

- Vehicle Tracking
- Field Communications
- In-Vehicle Information Management
- Work Order Management and Dispatch Monitoring
- Maintenance Decision Making
- Inventory and Asset Management
- Data Archiving and Reports

In order to maximize the use of the equipment and the annual investment in communications the proposed Navigo GPS/AVL Fleet Management System will have the ability to move a device from vehicle to vehicle within the City fleet. This feature allows the same device to move from winter operations (plowing, sanding) to summer operations (mowing, weed, spraying and/or paint striping).

The GPS/AVL Fleet Management Systems offered by Location Technologies, Inc. help communities and agencies all across the United States to achieve many of their Environmental Protection Policy goals by minimizing vehicle emissions through reductions in fossil fuel use, reducing vehicle idle times, speeding, unauthorized use, and keeping vehicles in proper operating condition through automated vehicle diagnostic data reporting.

Experience

Dubuque, IA Public Works

John Klostermann
Street/Sewer Maintenance Supervisor
(563) 589-4250
City-wide AVL System including street maintenance and sanitation vehicles

Pierce County Public Works

Bruce Wagner
Road Operations Division Manager
Tacoma, WA
(253) 798-6051
Large Metro-area County (Tacoma, Seattle)
Multiple Vehicle types

Johnson Co KS Public Works

Bob Swanson
Maintenance Supervisor
Olathe, KS
(816) 806-8707
Large Metro Area County (Kansas City)
Multiple Vehicle Types/Operations

Kansas City Board of Public Utilities

Ron Wilson
Kansas City, KS
(913) 573-9459
Large Utility Service includes Water, Gas, Electric

Overland Park, KS Public Works

Mike Ross
Supervisor
Overland Park, KS
(913) 895-6038
Large Metro Area City (Kansas City)
Multiple Vehicle Types/Operations

Washington Department of Transportation

Debbi Achord
Project Manager
(509) 633-0629
AVL System for State DOT with combination Radio/Cellular System, Interface to MDSS
Multiple Vehicle types/Operations

Hydro Pure Technologies

Nelena Nelson
505-334-1765
Aztec, NM
Large Commercial Water Supplier in the 4-corners area of Nevada, New Mexico, Colorado, Arizona

Personnel

The primary staff for this project, and their estimated percentage of time required over the duration of the project are Project Manager: Eric Cowger (15%) and Project Engineer: Glenn Courtney (10%).

Eric Cowger

President

Mr. Cowger has over 20 years of experience in the electronics industry. He has served in a variety of management and lead technical positions at several companies. These include Vice President of Engineering for Information Technologies, Inc., Design Engineering Manager at Thomson-Wilcox (now Thales), and senior engineer at Rockwell Collins Avionics.

Education: BS Electrical Engineering, Kansas State University, 1984
MS Electrical Engineering, University of Iowa, 1986

Glenn Courtney

Principal Engineer

Mr. Courtney has over 25 years of varied experience in the navigation and radio communications industry. He has served on numerous national standards committees and is recognized nationally as an expert in the field of navigation. Mr. Courtney has been involved in the successful design and implementation of several large-scale data communication systems in a variety of industries including Rail, Public Safety, Construction, Avionics, ground-based Nav-Aids, and Fleet Management.

Education: Electrical Engineering, University of Missouri, 1981

Fern Tsukada

Lead Software and Firmware Engineer

Ms Tsukada is our lead Software Engineer. She has broad experience in the development and implementation of large database related applications, data communications applications, and embedded software.

Education: BS Computer Science, Park University, 1995
BS Electrical Engineering, California State University, Long Beach, 2000

All key employees who are associated with this project will have sufficient time and resources available to complete all work and obligations that are specified within the negotiated contract.

Location Technologies, Inc. and its key employees maintain registrations and relationships with numerous professional and industry organizations. These organizations include the American Public Works Association, Canadian Public Works Association, National Emergency Number Association, Association of Public Communications Officials, American Public Transportation Association, American Public Power Association, American Engineering Association, International Wireless Convention, and many more. Location Technologies, Inc. engineering staff and key employees have attained several certifications and credentials through these associations in the course of continuing education and professional development.

Location Technologies, Inc. (LTI) is organized in a traditional functional matrix fashion where individual project and product managers may pull on resources from the functional groups. In the event of the loss of key employee(s) LTI is sufficiently staffed and experienced to continue successful completion of work and obligations as stated in the negotiated contract.

Project Approach

For the City of Ramsey, MN (“City”) we are proposing our Navigo GPS/AVL Fleet Management System. The proposed Navigo GPS/AVL Fleet Management System is a solution that provides vehicle performance data, location tracking, operations monitoring, and reporting via a user-friendly web interface. Location Technologies, Inc. is proposing and is prepared to supply all project management, installation, and implementation services necessary to deliver a working solution.

The Proposed Navigo GPS/AVL Fleet Management System will provide the City with an automated real-time GPS/AVL system that is simple to operate and will reliably collect and disseminate data in a useable format based on information provided by on-vehicle sensors, monitors, and other sources (i.e., weather data, operator-generated data). This accumulated operations data can be used to plan maintenance activities and direct the maintenance vehicles to carry out their planned activities. The proposed system will give the City a holistic, department wide view of its operations and allow it to streamline its maintenance and operations functions by using aggregated data to make informed decisions simply and efficiently.

When in real time vehicle tracking mode the LT6 will send position and monitored input operations data packets over the Internet from the wireless network to our hosted LT Wireless Data Server software operating on a secure, designated AVL Server. Information received will include position, heading, speed and input status if status wires are connected to the modem. These packets are then decoded, time tagged and logged, and made available for manipulation from any internet browser with an HTTP connection. Mobiles can be programmed to transmit on a timed basis, on distance traveled, on event, or on command. The Logger Module (SW) of the LT Wireless Data Server monitors the size of the active log table and may be periodically burned to a more permanent medium such as CD and stored indefinitely for future retrieval and analysis. The active log is always available for immediate replay and analysis. The LT Wireless Data Server will reside outside the City network and remotely Interface with existing City resource management software. Garage supervisors and others can monitor fleet performance and generate stand-alone reports in near real-time via any standard, up-to-date web browser by logging into the Navigo Explorer WebMap. Supervisors can review and edit data and have the ability to transfer information to the Resource Management System and other systems including GIS.

Our Navigo Explorer WebMap Platform standard map views cover all of North America with the ability to import your existing GIS information in its native format. The Navigo Explorer WebMap Platform can import data from the City’s Geospatial Infrastructure. On a periodic basis or on user command the map will refresh the position of the vehicle icons by refreshing its view of the vehicle position table maintained by the LT Wireless Data Server.

Field Communications

Field communications will be cellular using a 3G or better digital network for data transmission to and from the participating vehicles. The proposed field communication solution is satisfactory not only for initial deployment, but also for future expansion in the state. The proposed solution is scalable to the entire City maintenance fleet using commercial-off-the-shelf equipment.

Operations Status and Monitoring

A key feature available for Garage Supervisors is the ability to monitor the condition of the vehicle and material usage in near real-time. In-vehicle systems can provide the status of a number of vehicle and equipment conditions, including but not limited to:

- Status of vehicle equipment
- Vehicle speed
- Engine diagnostics
- Available on-board materials for dispersion

Vehicle routes, whether assigned to one vehicle or a group of vehicles, are able to be played back over a defined period of time for review by system operators and managers. This feature allows for review of

vehicle paths and operations as part of overall maintenance system function review processes. The proposed Navigo GPS/AVL Fleet Management System is able to track any fleet vehicle relative to its expected location at a desired system site at any time, including an estimate of arrival time based on current position and current speed.

Asset, Equipment, and Materials Management

Tracking material quantities allows system operators/managers to determine the appropriate amount of materials already loaded or to be loaded on maintenance vehicles. This system also tracks rate of material usage and amount of coverage possible based on remaining on-board material. The locations where materials are deployed or where activities such as plowing, mowing or road repair/maintenance are performed can be automatically geo-coded as maintenance vehicles respond. This information provides an accurate, historical log of all maintenance functions performed at any point within the network.

Keeping track of equipment necessary for maintenance functions can be done efficiently. The proposed Navigo GPS/AVL system has the capability to identify and document various pieces of maintenance equipment, facilities and roadside features. This system can be used to determine and document the location of existing maintenance equipment, facilities and a variety of roadside features. Maintenance personnel can view equipment inventory locations on-screen by type of equipment and region where equipment is stored. They can also easily identify equipment through the use and storage of maintenance equipment ID information.

Example unique inputs from snow plows:

- Plow position (up/down)
- Wing position (up/down)
- Under body position (up/down)
- Material Application(liquid/granular)
- Air Temperature
- Pavement temperature
- Odometer
- Fuel Mileage
- Total amount of material used
- Wind Direction
- Wind Speed

A number of these inputs can be added as future enhancements to the system; however, the ability to monitor over sixteen unique inputs to the data controller will allow for the expansion of the system as the City evaluates technology enhancements and operational requirements for the GPS/AVL System.

Environmental Conditions

Environmental conditions affect fleet operators and affect system performance. On-board systems that display the condition of the roadway on which the vehicle is traveling, show current weather conditions including wind speed and visibility (which greatly affect rate and flow of material dispersion upon the roadway and/or safety) or provide other real-time information for the operator are extremely valuable resources and available within the proposed Navigo GPS/AVL System.

System Architecture

The proposed NAVIGO GPS/AVL Fleet Management System includes an in vehicle device, connected to a data controller to collect in-vehicle, operational, and GPS data. This in vehicle device will have an interface with on-board diagnostics (OBD) which monitor engine operations through the vehicle bus. The in vehicle device will also connect to a vehicle data controller. This controller will monitor the specialized sensors on each of the state vehicles.

Alert Communications & Messaging

- Vehicle to Vehicle
- Vehicle to Center (Main Operations)
- Vehicle to Other (i.e., nearby devices, roadside DMS)
- Group Text Messages
- Individual Text Messages
- Blanket Alerts (i.e., Amber Alert)

Data Archiving and Reporting

System data can be archived and a number of reports can be customized to provide City management with accurate and up-to-date system performance statistics.

Reports that are standardized, including utilization, historical weather, roadway conditions, mileage, materials coverage, job completion and others, can be quickly accessed through existing report configurations. As necessary, City staff can use the user interface to select specific data and generate ad hoc reports at any time. City staff can interface directly to the database select specific data and generate specialized reports.

Power

Power for the data controller and the in-vehicle device are come from a power terminal. The additional power draw is not expected to be significant, however, separate fuses for installed GPS/AVL equipment is included. All in-vehicle devices being proposed are engineered to perform under extreme weather conditions and under normal rugged driving conditions while in operation. These in-vehicle units provide connectivity to all sensors and any external GPS antennas and/or equipment.

Communication System

The proposed City Navigo GPS/AVL Fleet Management System will communicate over a cellular data network. CDMA is the data service available over the Sprint wireless data service. The coverage area for CDMA covers the entirety of the City's operational footprint. A monthly fee is required for use of this service and is included in this proposal. Depending upon implementation the monthly fee will be contracted through the Sprint Corporation or Location Technologies, Inc. Location Technologies, Inc. is a certified Sprint Business Partner.

All data formats are the same between the different communication schemes. This allows the user to mix and match communication schemes across their fleet as appropriate.

LTI Hosting Services*

Location Technologies, Inc. Hosting Services utilize the Amazon Elastic Compute Cloud (Amazon EC2) web service to provide resizable compute capacity in the cloud. It is designed to make web-scale computing easier for developers. This web service interface allows for scaling, obtaining, and configuring capacity with minimal friction. It provides complete control of computing resources and operates on Amazon's proven computing environment. Amazon EC2 offers a highly reliable environment where replacement instances can be rapidly and predictably commissioned. The service runs within Amazon's proven network infrastructure and datacenters. The Amazon EC2 Service Level Agreement commitment is 99.95% availability for each Amazon EC2 Region.

Security

Amazon EC2 includes web service interfaces to configure firewall settings that control network access to and between groups of instances. When launching Amazon EC2 resources within Amazon Virtual Private Cloud (Amazon VPC), LTI can isolate instances by specifying the IP range, and connect to existing IT infrastructure using industry-standard encrypted IPsec VPN.

*Citation: < <http://aws.amazon.com/ec2/> > (accessed 11/3/ 2010).

LT6 GPS/AVL Modem

The LT6 GPS/AVL Modem is the backbone of the Navigo GPS/AVL Fleet Management System. Each vehicle to be monitored will utilize an LT6 AVL Modem device. GPS antenna options include a combo GPS/Cellular bulkhead mount antenna, mag-mount, and trunklip-mount antennas. The LT6 GPS/AVL Modem is capable of multiple and/or “dual” communication methodologies using either or both Cellular Data Network (GSM/CDMA), Bluetooth, WiFi (80211“x”) technology, or passive ‘log and dump’ protocols to transmit accumulated AVL and operations data.



The LT6 is capable of monitoring and reporting a variety of analog and digital inputs. There are also up to four analog outputs available for remote control. This generic I/O capability is fully software configurable. Examples might include door lock control, ignition circuit defeat, light and emergency switch monitor, temperature, shock, and humidity. Standard interface packages are available for street maintenance vehicles such as plows, spreader, vacuum, and sweeper vehicles. Each AVL device may monitor up to 8 generic digital inputs. The 8 inputs may be used to trigger various events on the map. Digital inputs may be programmed to plot paths, display messages, mark spots on the map, or change the vehicles color. Six inputs may be used to monitor analog levels in the vehicle. An example would be battery voltage. Similar map actions as described for digital inputs are available when a given input drops below or rises above preset levels. Four remote controlled outputs are available to perform functions such as remote vehicle disable, flash lights, honk horn, unlock doors. All LTI GPS Modules use a state of the art SIRFIII high-sensitivity 20 Channel GPS receiver to ensure fast time-to-first-fix times and continuous tracking in the most challenging environments. The LT6 also hosts two RS-232 ports for connection to external peripherals such as laptop computers, text messaging terminals, and simple push button status heads. LTI offers laptop software for mobile data messaging, mobile mapping, remote database operation, LT MDT Mobile Data Terminal, and the LT MDC.

Physical Specifications:

Size: Extruded Aluminum Casing 5.5"x6"x1.5"
 Power Requirements: 200mA at a 9-16VDC input
 GPS Antenna Options: DualBand, trunklip, mag, bulkhead
 Serial Interface: 2 RS-232 Msg. Slotting: Up to 62.5 mSec
 Digital Inputs: 8 Analog Inputs: 6 Digital Outputs: 4

GPS System:

SIRFIII Chipset 20 parallel channels
 RTC crash auto recovery 0.1 sec. reacquisition time
 SBAS (WAAS, EGNOS, MSAS) support.
 NMEA-0183 compliant protocol/custom protocol
 Extremely fast TTFF at low signal levels

Garmin Navigation Device Integration

Several Garmin devices allow us to interface with them through their Fleet Management IO. This allows us to control in-vehicle guidance, multiple stop optimization, driver log-in and statusing, and generic messaging on these functional and economical devices.

LT Vehicle Diagnostic Bus Interface

LTI gives a standard list of possible parameters to monitor from vehicle maintenance bus setups including OBDII or J1708/1939 (heavy duty vehicles). Not all parameters will be available with all vehicle types.

- Throttle Position
- Road Speed
- Power Takeoff Status
- % Accelerator Pedal Position
- Fuel Level
- Average Fuel Rate
- Engine Speed
- Total Idle Hours
- Total Idle Fuel Used
- Total Vehicle Distance
- Total Vehicle Hours
- Total Engine Hours
- Instantaneous Fuel Rate
- Instantaneous Fuel Economy
- Average Fuel Economy
- Total PTO Hours
- Seatbelt Status
- Any Vehicle Maintenance Codes

LT Asset Tracking

Location Technologies, Inc. utilizes an asset tracking device designed to provide the user with a once per day daily locate. The device has been specifically engineered to update itself and then go back to sleep enabling this self contained battery powered device to last a minimum of 1 year without need to replace the battery. This device can be affixed to any asset, designed to provide intelligent monitoring of non-powered mobile and fixed assets. The Asset Tracker is a water resistant device, pursuant to the IP-66 code rating. This rating specifies that the device is protected against dust and high pressure jets of water.

Because it was designed to be installed exposed to the environment the GPS and Cellular antennas are completely integrated.

Spreader Controller Interface

Some Spreader Control Manufacturers require that certain components, modules, or keys be in place for data. For example the Force America Controller requires a key to enable data output. Certain Component Tech models require an additional add-on communication board. Other models may need firmware updates and/or calibration. If required prior to signing a contract we will inspect each of the vehicles to determine exactly what will be required for each of the controller types.

LT Plow Monitor

The LTI Blade Position Sensor is a rugged device for sensing blade position in winter road maintenance equipment. The Sensor has been specifically designed to survive the demanding environment of snow fighting and road surface treatment equipment. The heavy steel case is powder coated and the sensor is protected by a thick walled steel tube filled with potting compound.

Specifications:

Display Screen: 7 inch TFT LCD
Resolution: 800*4800 (1,152,000) pixels
Aspect ratio: 16:9
OS: Windows CE 5.0
CPU: Samsung S3C2410A,266 MHz
Memory: 64MB Flash ROM + 64 MB SDRAM
Data Storage: SD card/USB Flash memory
Expansion Slot: SD card, SD/IO (Up to 4GB)
USB 1.1(USB Host and Device)
Internal Speaker: 1W
Working Temperature: -10 - +65 deg
Storage Temperature: -25- +80 deg

Key features:

7 inch TFT LCD with touchscreen
Windows CE 5.0 Operating System
External A/V Input
Memory with SD Card
2 USB Input
1 RS232 input
Earphone Output
Multi-language OSD

Power: AC/DC 12V Supp/Dir 12V dc
Dimension (mm): 188L*114W*35H

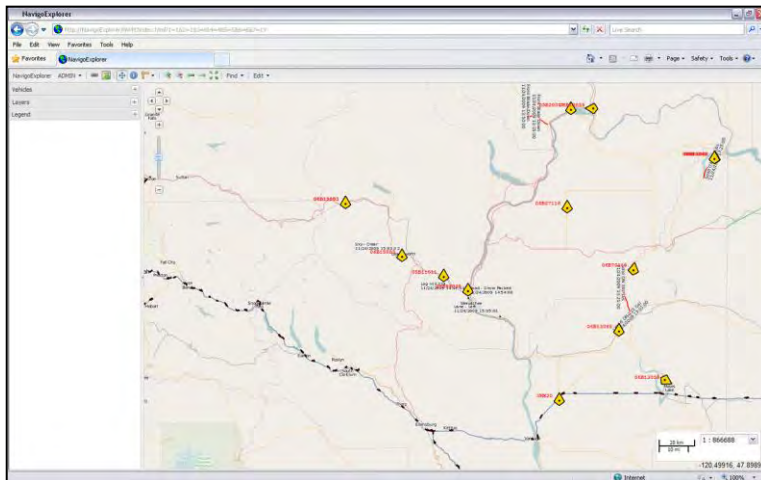


Mounting requires only two holes to be drilled into any mechanical surface that move angularly with the blade or on the blade itself. The sensor will require a minimum of 15° of movement between the up and down position of the blade to reliably indicate position. The adjustment slot provides 35° of adjustment, allowing the sensor to be mounted in a wide variety of locations and orientations.

Navigo Explorer WebMap Platform Software

The Navigo Explorer WebMap Platform includes standard map views that cover all of North America and allows you the ability to import your existing GIS information, often in its native format or using WMS. Like the Desktop Map vehicle position and status are displayed using colored icons with unique labels. On a periodic basis or on user command the map will refresh the position of the vehicle icons by refreshing its view of the vehicle position table maintained by the LT Wireless Data Server.

Our Navigo Explorer WebMap platform is available for user hosting or you may subscribe to our own internally hosted system. If you have data coming from a variety of device types or you have special archiving requirements or third party software interface requirements then we would suggest a user-hosted configuration. All two-way radio based systems must be user hosted.



Additionally, logging, replay, bread crumbing, and reporting options allow users to analyze every aspect of their fleets operations through analysis of the data logs. The ability to track and record status changes, switch closures, and sensor readings makes using the Navigo Explorer WebMap for reporting and analyzing your fleet's activities a very powerful tool. The Navigo Explorer WebMap provides all of the standard map features one would expect from a desktop mapping program including map display control tools (zoom, pan, etc.) along with vehicle display and reporting functionality.

The Navigo Explorer WebMap Platform is user credential controlled and allows for administrator control over user groups for assigning user operation permissions and data views. All reporting and operations management functionality is controlled by individual and user group credentials that are defined by administrative (management) users. The LT Wireless Data Server allows for third party software integration.

Our LT Wireless Data Server may be easily configured for use with third-party software integration and third-party AVL devices. Once the packet format is defined a unique port will be configured to receive and process packets from the third-party device. LTI also produces a wide variety of AVL devices and have solutions for virtually all municipal operations.

Map Features

- Integrates with all GIS including WMS, and SQL Based Systems
- Path Length and Area Measurements
- Point or Interpolation Geocoding
- Path or Road Mile post Geocoding
- Snow and Sweeper Route Matching and Automatic Status Updates
- Public Facing Web Display Options for Street Maintenance During Snow Events
- Completely Browser Based – No software required on the desktop
- Dozens of pre-canned reports
- Ability to Schedule Reports to run automatically
- Fast Short-term History (24 Hour) Vehicle Activity Display
- Integration with NEXRAD Radar Products – Storm Reflectivity and Precipitation Amounts
- Integration with Fleet Management Package including Vehicle Diagnostic Bus, Mileage and fuel use tracking, scheduled and unscheduled maintenance tracking.
- Updated and improved SQL-based Wireless Data Server with new condition and event based engine for automatic monitoring and control of fleet activity.

Broad and Comprehensive Report Manager

Dozens of reports detailing all aspects of the fleets operation are available. Many can be scheduled to be run automatically.

Time and Distance Traveled in Region Report: This report will calculate the total time spent and distance traveled within in the specified region. If for example the user selects State as the region type then the program will compute the time and distance the a vehicle has traveled in each state the vehicle has entered for the specified time period.

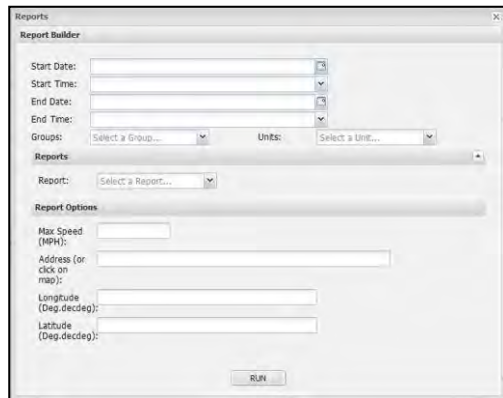
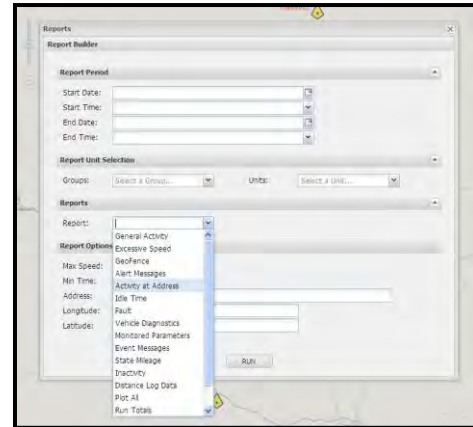
General Activity Report: This option will report all activity for the selected vehicle over the specified time period.

Activity at an Address Report: This report will calculate and display the amount of time a given vehicle spends within a specified radius of the entered address. For example the user could find how long a specific unit spent within 0.25 miles of 100 Main St two days ago.

Time Between Status Changes: This report option will calculate and display the time between successive status changes over the specified time period for fleets.

Error or Fault Messages Report: Reports all fault messages received over the specified time period.

Excessive Speed Report: Reports all occurrences of exceeding the given speed. This is shown for each position update.



Idle Time Report: This report will show all instances when a vehicle remains idle for a specified period of time.

Inactivity Report: This report will show all instances when a vehicle has failed to send a message of any sort over a specified period of time.

Stop and Start Report: This report will show all instances of start and stop messages from a given vehicle or vehicles.

Digital and Analog Status Report: Shows the time, value, and location of all externally monitored analog and digital; inputs to the AVL Modem.

Reports may be viewed in text form or on the Map Display. The user may also opt to have the map reverse geocode the vehicle coordinates giving an address and closest intersection for each report element.

AVL Specific Mapping Operations

GeoFence

A geofence is a geographic area a user may define for reporting purposes. Along with defining the specific map area the user can also specify certain actions the map will take when a vehicle enters or departs from the geofenced area. These include map center and text message. Ten geofence areas may be loaded directly in the AVL Modem. The modem will then automatically send an arrival and departure message when entering or leaving the programmed areas. These are especially useful for fixed locations that your vehicles return to such as parking areas, fueling, dumping, remote staging, or office locations.

Analog/Digital Input Monitoring

Each AVL device may monitor up to eight digital and six analog inputs. These generic inputs may be used to trigger various events on the map. For instance a digital input may be tied to a hydraulic pressure switch that indicates the up or down position of a snow remove blade. When the blade is down the system could be set to plot a blue path behind the vehicle. This would indicate that the path had been plowed. Digital inputs may be programmed to plot paths, display messages, mark spots on the map, or change the vehicles color. Analog inputs may be used to monitor analog levels in the vehicle. An example would be battery voltage. Similar map actions as described for digital inputs are available when a given input drops below or rises above preset levels.

Closest Vehicle Request

A simple point and click closest vehicle function is available that provides an ordered list of all vehicles closest to an indicated point on the map.

Map Follow

Any vehicle maybe selected for Map Follow, which forces the map to redraw on each position update from the specified vehicle. This keeps the vehicle in view at all times without requiring any interaction by the map user.

AVL Modem Commands

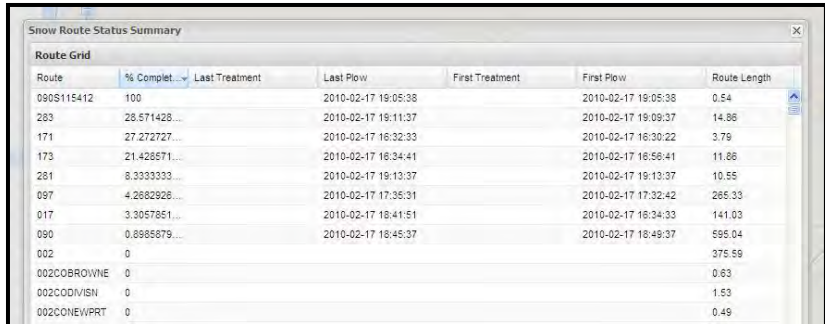
A variety of commands are available to the user to request information or force certain operating conditions on the AVL Modems. These include position polling, log downloads, remote turn off, and stop transmission requests to name a few.

In-Modem Coordinates

Up to 10 coordinates may be programmed into each modem that will cause the modem to automatically generate arrive at and depart from messages for each location. These should be set for fixed locations in your operating area. Examples include landfills, fuel locations, jails, hospitals, garage, parking lot, material storage facilities, etc. All arrival and departure messages are logged to allow you to run 'dwell time' reports at each location (time between arrival and departure).

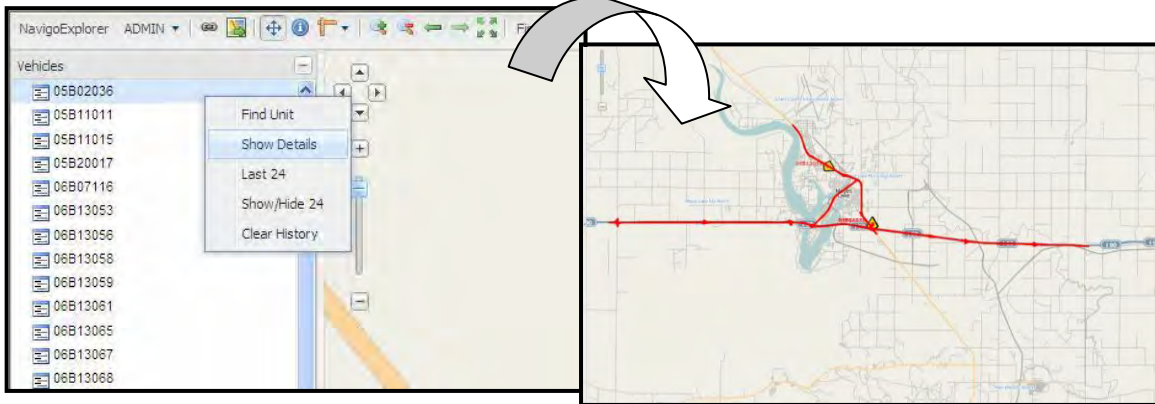
Intuitive Data Display Options

A Grid option is also available to view the percent completion of all routes in your area of responsibility.



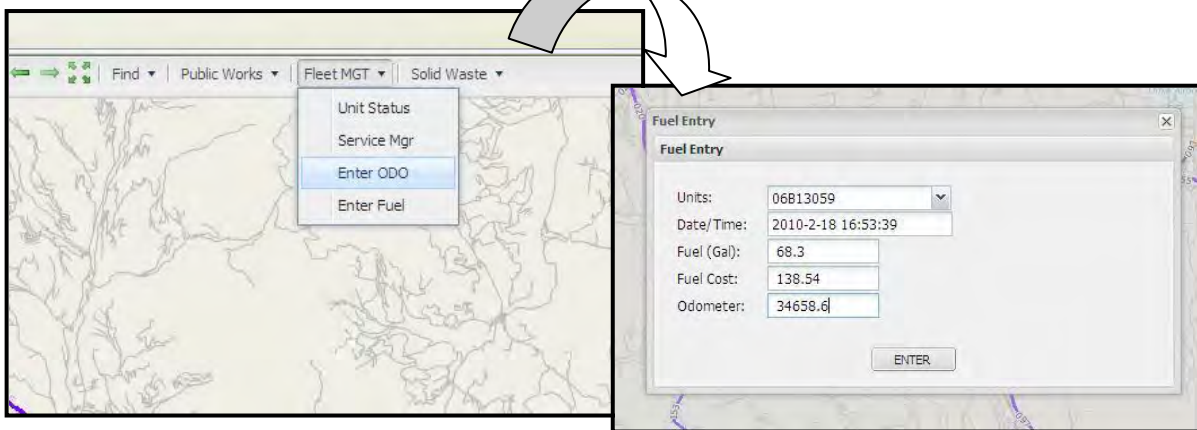
Route	% Complet...	Last Treatment	Last Plow	First Treatment	First Plow	Route Length
090S115412	100		2010-02-17 19:05:38		2010-02-17 19:05:38	0.54
283	28.571428...		2010-02-17 19:11:37		2010-02-17 19:09:37	14.86
171	27.272727...		2010-02-17 16:32:33		2010-02-17 16:30:22	3.79
173	21.428571...		2010-02-17 16:34:41		2010-02-17 16:56:41	11.86
281	8.3333333...		2010-02-17 19:13:37		2010-02-17 19:13:37	10.55
097	4.2682926...		2010-02-17 17:35:31		2010-02-17 17:32:42	265.33
017	3.3057851...		2010-02-17 18:41:51		2010-02-17 16:34:33	141.03
090	0.8988879...		2010-02-17 18:45:37		2010-02-17 18:45:37	595.04
002	0					375.59
002COBROWNE	0					0.63
002CODIVISN	0					1.53
002CONWPRT	0					0.49

Fast 24 Hour Activity Plots



Fleet Management Module Integration

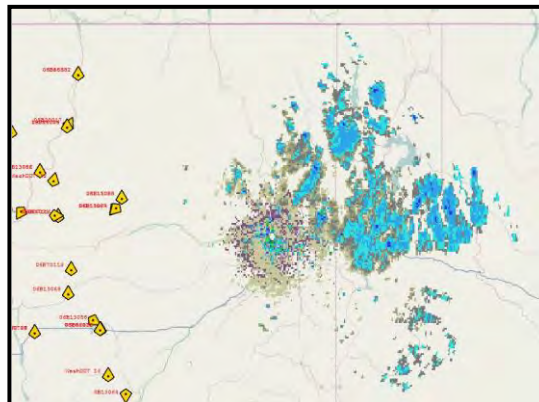
Quickly track fuel use and other Fleet Activity.



Integration with RWIS and Weather Radar products

The Navigo Explorer WebMap platform also allows for the overlay of commercial aerial imagery and real time weather information. You will be able to see your vehicle positions on top of images and street lines. This includes NEXRAD weather RADAR information.

Quickly display Storm intensities and Precipitation totals. Also shows all Weather Service produced Watch and Warning Boxes



Integration with Application Specific Modules

These include Public Works, Solid Waste Management, Transit Systems, Public Safety, Public Utility, and Work Order Management Systems.

Integration includes special handling of vehicle activities unique to the application. For example in the transit package the server automatically tracks the route compliance and ETA's to all scheduled stops for Fixed Route Bus fleets. The Solid Waste Package will track and display route exceptions such as can-not-out or yard waste in can and will assign and manage special pickup activity. Landfill activities are also tracked.

The work order package interfaces to in-vehicle personal navigation devices in the GARMIN line to allow for in-vehicle stop ordering, driver guidance, text messaging through the device, and stop ETA management.

SQL Based Wireless Data Server and Event Engine

The new LTWDS can utilize MS-SQL or the economical open-sourced MySQL.

Event Engine

The Event Engine will monitor all specified vehicle activities and take predefined actions based on the condition of the monitored activity. Conditions are defined for each vehicle and one or several events may be taken based on the condition.

Current Defined Conditions and Monitored Activity

- | | | |
|---|---|------------------------------|
| • Engine On/Off | • Panic Switch | • Vacuum On/Off |
| • Vehicle Start/Stop | • Lights On | • Hopper Dump |
| • Equipment Fault | • Alarm On/Off | • Hose Feedout Length |
| • Digital Input On/Off | • Seatbelt | • Pump On |
| • Analog Input On/Off | • Door Open | • Road or Air Temperature |
| • Status Message Receipt | • Driver Log In/Out | • Wind Speed/Direction |
| • In/Out of Geofenced Area | • Text Message Received | • Compactor On/Off |
| • On/Off Route | • ETA Message Received | • Pickup arm extend |
| • Spreader ON/Off and rate | • Stop Order Message Received | • Siren/Lights On/Off |
| • Blade Up/Down | • Painter On/Off | • Tilt Sensor |
| • Broom On/Off | • Mower On/Off | • In/Out of State Boundaries |
| • PTO On/Off | • Sealer On/Off | • RFID Scans |
| • Vehicle Speed | • Sprayer On/Off | • Passenger In/Out |
| • Impact/Hard Brake | • Position Mark | |
| • Arrive/Depart from one of 10 Home Coordinates | • Engine Fault Message from Engine Diagnostic Bus | |
| • Operation outside of prescribed Operating Times | • Inactivity over a prescribed time period | |

Defined Actions That May Be Taken On Any Of The Above Conditions

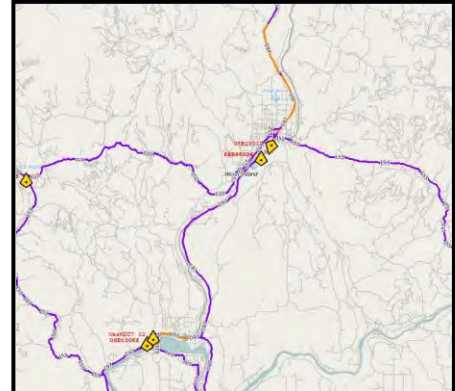
- | | | |
|-----------------------------|------------------------------------|--------------------------------|
| • Log Event | • Write out file | • Clear In-Modem Log |
| • Send an Email | • Start Path | • Poll Unit |
| • Send an SMS | • Run Report and Email | • Hide Unit |
| • Plot Point | • Disable a Vehicle | • Unhide Unit |
| • Change Unit Symbol Color, | • Enable a Vehicle | • Arm Alarm |
| • Clear Output | • RouteUpdate-Solid Spread-Active | • RouteUpdate-Reserved1-Active |
| • Query In-Modem Log | • RouteUpdate-liquid Spread-Active | • RouteUpdate-Reserved2-Active |

- Start In-Modem Log
- Stop In-Modem Log
- Set Vehicle Output On – uses one of the 6 outputs available on each mode
- RouteUpdate-Blade-Active
- RouteUpdate-Sweeper-Active
- RouteUpdate-Reserved3-Active
- RouteUpdate-Reserved4-Active

Navigo Explorer WebMap and Public Works

With the Public Works option the user is able to monitor Route-based operations commonly performed. The Navigo Explorer will require a user supplied GIS layer that describes the operating routes. The Server software will then match Vehicle location to route line segments. The line segment will be updated with the time and activity of the vehicle. A range of colors are then assigned to each line segment to display the relative time since the last activity was performed on a given segment. Four colors are assigned to represent 0 – 6 Hours, 6 – 18 Hours, 18 – 24 Hours, and over 24 Hours. A fifth color is used to display the default or 'untouched' state.

For route based operations the Navigo Explorer will track monitored operating, atmospheric, and vehicle conditions. The server will automatically match the vehicles location to the nearest route segment when any of the monitored conditions listed above are active. The segment will be updated with the date and time and the condition of the monitored input. As vehicle and field events progress and operations are performed the Server will continually update the Route Layer database with current treatment times and amounts. To update the Map View with current Route Status the operator should select the Update Route item from the menu. The route display may be toggled off and on using a checkbox.



Using the Route Summary menu item will open a window showing all Routes. A column showing percent completion indicates the percentage of road segments have been treated for a given route. Segment completion is indicated by a valid Time and Date assigned to a segment. Material usage totals, distances travelled, and fuel usage may be reported using the ADMIN-Reports menu item and running the appropriate report from the Report Builder window.

Sweepers

All of the various brooms on a sweeper may be monitored for On/Off Status. The dumping of the hopper may also be monitored. If a sweeper route map layer is provided the user may use the automatic route matching functionality to track the completion status of each route. Specific Routes travelled and Hopper Dump Locations may be displayed using the Reports Builder.

Paint Trucks

On/Off Status of the Sprayer may be monitored on Paint Trucks. The path and distance travelled while spraying may be displayed using the Report Builder.

Weed Spraying

On/Off Status of the Sprayer may be monitored on Weed Control Trucks. The path and distance travelled while spraying may be displayed using the Report Builder.

Chipper/Sealers, Graders

On/Off Status of the Sealer Equipment and Graders may be monitored on the vehicles. The path and distance travelled while spraying may be displayed using the Report Builder.

Sanitary/Storm Sewer Maintenance

Hose feed out lengths may be monitored on vehicles along with pump status.

Public Safety

In the field of public safety, efficiency and performance translate to the safety and well-being of those you serve. Sheriffs, Chiefs, and Directors responsible for public safety depend on Location Technologies for accurate, detailed information that promotes quick assessment and action. With Intelligent Fleet Management and dispatch solutions from Location Technologies, public safety professionals leverage real-time intelligence on the operational efforts of the organization.

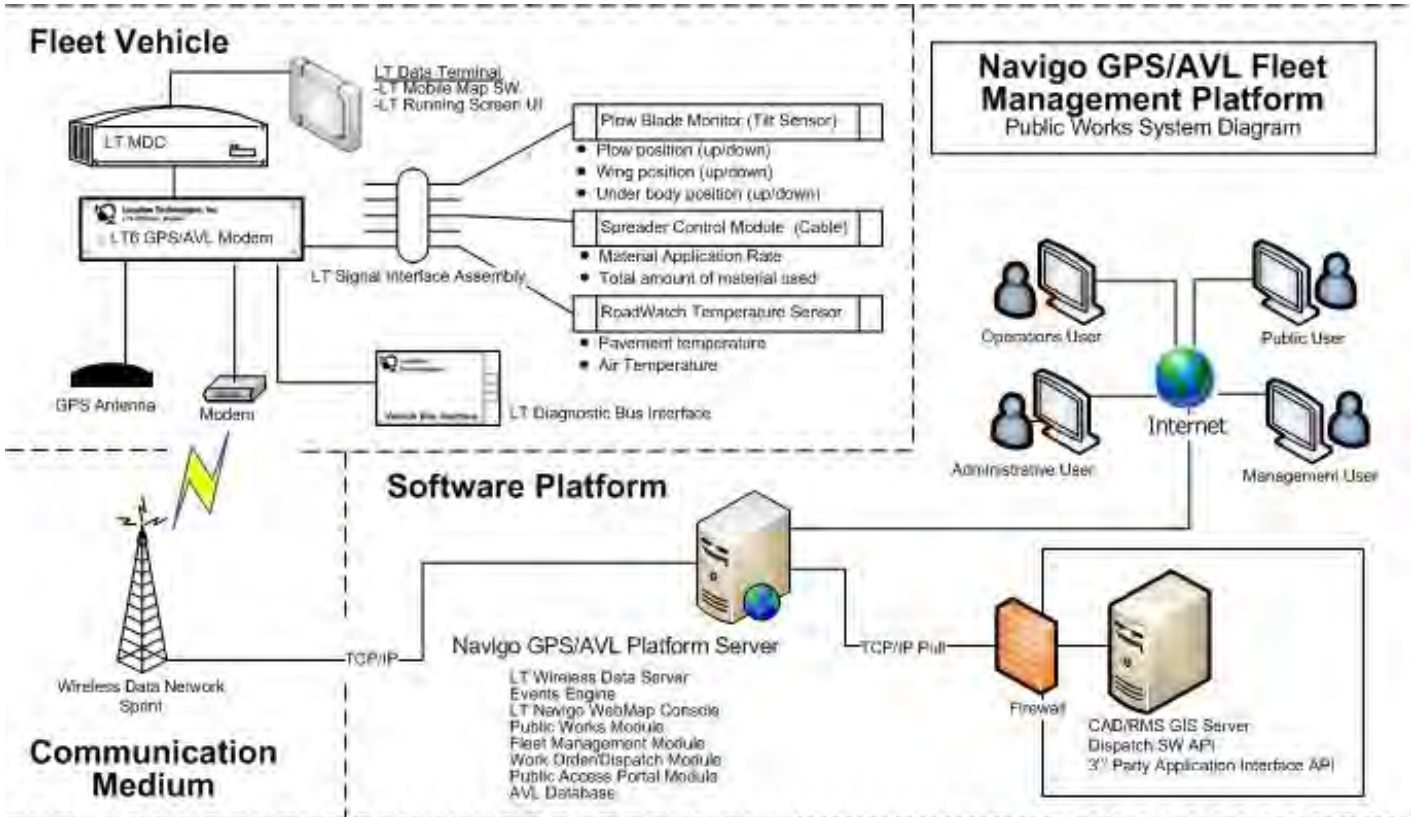
Our unique versatility and experience with operations of all shapes and sizes enables us to customize solutions for your unique needs. With a Location Technologies solution integrated seamlessly into your operation, you gain a level of awareness and flexibility that delivers safer, more efficient performance by empowering you and your public safety

professionals to deliver your very best. E911 support is available on Public Safety GPS/AVL Fleet Management systems implementing the LT 911 Server software. Computer Aided Dispatch (CAD) integration is available for certain 3rd party dispatching systems

Fleet Management

Vehicles equipped with our optional OBD interface can monitor all engine diagnostic information including engine fault codes as they appear. OBD data is polled on command or is automatically sent each time an engine fault occurs. We support the OBDII standard found on all cars and light trucks and the J1708 and J1939 standard used in heavy trucks, buses, and equipment. The Fleet Manager Package allows for the complete management of all scheduled and unscheduled service and repairs, tracks personnel, materials, and suppliers, and is also a browser-based application requiring no software to be loaded on the users workstation.

System Diagram



Implementation

Project Plan Development

Upon award of contract Location Technologies, Inc. (LTI) Project Managers will work closely with City Management on a jointly developed Project Plan Document. The LTI Project Manager will attend scheduled meetings during normal City business hours, Monday through Friday. LTI Project Managers are also available for online webinar meetings at the discretion of the City.

Software Configuration

Configuration of the Navigo Explorer WebMap Platform for the City GPS/AVL Fleet Management System is comprised of three interconnected tasks. These tasks are described below:

Load City GIS/Map data

LTI Project Managers will engage with the City Management Team and GIS Department to determine what Map Projections and Layers will be made available to the Navigo AVL System.

Load City vehicle and operations data

LTI Project Managers will engage with the City Operations and Fleet Management Team to acquire all necessary vehicle and operations data.

Configure City user/group credentials for Navigo AVL System access/functionality

LTI Project Managers will engage with the City Operations Management Team to determine the appropriate user/group credentials configurations for City staff.

System Report Configuration and Development

LTI Project Managers will engage with the City Operations Management and Fleet Management Team to set-up and configure system reports as identified in the mandatory requirements.

Software and Hardware Testing

LTI Project Managers will demonstrate that the GPS/AVL Fleet Management System successfully meets City business requirements using a City supplied test plan. LTI will remediate any critical or blocking issues before the System goes into full production.

Acceptance Field Testing

City end-users will conduct field testing the production GPS hardware and software solution noting any issues. LTI has read and understands its requirement to remediate any critical or blocking issues before the system is accepted.

Systems Documentation

LTI Project Managers will deliver a complete set of technical and end-user system documentation in electronic format to the City Operations Management and Fleet Management Teams.

Project Team

Location Technologies, Inc. (LTI) is organized in a traditional functional matrix fashion where individual project and product managers may pull on resources from the functional groups. In the event of the loss of key employee(s) LTI is sufficiently staffed and experienced to continue successful completion of work and obligations as stated in the negotiated contract.

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ccowger@loctech.com

Support

Warranty, maintenance, and support is comprehensive, responsive, and benefits the City upon implementation and into the future. The vendor should describe how software maintenance and support is provided. Location Technologies provides technical and information technology support by phone Monday – Saturday 8:00am – 5:00pm CST and 24/7 support by email. Response time and escalation procedures are variable dependent upon support necessities. Software updates and upgrades are released as they become available. Software updates are available for download with an internet connection or can be shipped in CD/DVD-ROM format via US Mail or UPS/FedEx. The LT6 CDMA GPS/AVL Modem has many over-the-air programmable features as well as Ethernet/serial connectivity options for programming with our AVLCon software.

Training

All training sessions will be presented by Location Technologies, Inc. support and education specialists. Initial training programs will include hardware installation, hardware maintenance, software operations and software maintenance. These training sessions include onsite training for managers, dispatchers, operators and all support personnel and encompass all aspects of system use and maintenance.

Vehicle Hardware Maintenance and Installation Training

LTI Project Managers will engage the City Fleet Management Team to determine a feasible schedule and work plan for vehicle hardware installation training. Typical vehicle installations take one to two hours per vehicle depending upon location, vehicle, additional interface installation, and personnel availability.

Installation: LT Mobile Data Terminal GPS/AVL Unit

Location: On-Site

Duration: 60-120 minutes per vehicle*

Navigo User Training

LTI Project Managers will engage the City Operations Management and Fleet Management Teams to determine a feasible schedule for systems training. All training sessions will be presented by Location Technologies, Inc. support and education specialists. Initial training programs will include hardware installation and maintenance, software operations and software maintenance. These training sessions include onsite training for managers, dispatchers, operators and all support personnel and encompass all aspects of system use and maintenance.

Course: Training for Navigo Explorer WebMap Platform Management Users

Learner: Dispatch User; Management User

Location: On-Site

Duration: 2-3 hours*

Course: Training for LTI Navigo Mobile Map and Operations Users

Learner: Fleet Maintenance Personnel; Fleet Operations User

Location: On-Site

Duration: 2-3 hours*

Cost Breakdown

Upfront Hardware Costs				
Public Works Vehicle				
<u>Qty</u>	<u>Item</u>	<u>Description</u>	<u>Unit</u>	<u>Total</u>
15	LT6 GPS/AVL Modem	Cellular Modem, Incl. 1yr Warranty	425.00	6,375.00
15	Cabling/Antennas	Combo GPS/Cellular, Mag-mount	55.00	825.00
28	LT Plow Monitor	HW & Cabling	59.00	1,652.00
4	LT Spreader Interface	HW & Cabling	65.00	260.00
15	LT Diagnostic Bus Interface	HW & Cabling (optional, not incl. in total)	325.00	4,875.00
Public Works Vehicle Hardware Total				\$ 9,112.00

Recurring Monthly Costs				
Monthly Per Vehicle Costs				
<u>Qty</u>	<u>Item</u>	<u>Description</u>	<u>Unit</u>	<u>Total</u>
15	Navigo Unit Subscription	1.65 per vehicle/month	1.65	24.75
15	Wireless Data Plan	10.99 per vehicle/month	10.99	164.85
Monthly Per Vehicle Costs Total				\$ 189.60
Monthly Per Account Costs				
<u>Qty</u>	<u>Item</u>	<u>Description</u>	<u>Unit</u>	<u>Total</u>
1	LT Hosting Service		35.00	35.00
1	Public Works Module		35.00	35.00
1	Fleet Manager Module	(Diagnostic Bus option only, not incl. in total)	35.00	35.00
Monthly Per Account Costs Total				\$ 70.00

Professional Services				
<u>Qty</u>	<u>Item</u>	<u>Description</u>	<u>Unit</u>	<u>Total</u>
1	User Training	On-site, unlimited attendance	1,250.00	1,250.00
1	HW Maint. & Install. Training	On-site, unlimited attendance	1,250.00	1,250.00
15	Ext. LT6 12month Warranty	Annual, per device (After 1 st year)	30.00	450.00

Location Technologies, Inc. 12 Month Limited Warranty and Disclaimer

Limited Warranty

Location Technologies, Inc. (LTI) warrants all hardware products to be free from defective material and workmanship for a period of one (1) year from the original ship date from our factory. LTI agrees to repair or replace, at our sole discretion, a defective device that has been returned to LTI within the warranty period and has a proper RMA.

If service is required under this warranty:

1. Call 816-741-3169 or email: service@loctech.com to receive an RMA number. This number must be prominently displayed on the return box and on any shipping documents.
2. Send the device postage prepaid to the address listed below.

Note: All returned devices must be shipped in a static free container. This may be a static free bag within a regular cardboard box. The warranty may be immediately voided if the device is not protected from static discharge during shipping.

3. Location Technologies, Inc is not responsible for shipping damage.

This warranty does not extend to any LTI products that have been subject to misuse, neglect, accident, incorrect wiring, improper use, violation of instructions or operating parameters, damage caused by connected equipment, nor does the warranty extend to any units that have been altered or repaired by anyone other than LTI.

Limitation of Liability and Disclaimer

In no way shall LTI be liable for any loss, damage, fire, explosion, injury, including any incidental or consequential damages, or death as a result of operating and/or installing any LTI products.

ALL PRODUCTS SUPPLIED BY LOCATION TECHNOLOGIES, INC. ARE "AS IS" AND WITHOUT WARRANTIES EXPRESS OR IMPLIED. LTI DISCLAIMS ALL WARRANTIES INCLUDING THE IMPLIED WARRANTY OF MERCHANTABILITY AND FITNESS FOR A PARTICULAR PURPOSE. LTI DOES NOT WARRANT THAT FUNCTIONS AND FEATURES CONTAINED IN OUR PRODUCTS WILL BE UNINTERRUPTED OR ERROR-FREE, THAT DEFECTS WILL BE CORRECTED. LTI DOES NOT WARRANT OR REPRESENT THE USE OF OUR PRODUCTS IN TERMS OF THEIR CORRECTNESS, ACCURACY, RELIABILITY, OR OTHERWISE.

For further information regarding product warranties and return procedures please contact us at

-

Location Technologies, Inc
Service Dept.
6214 NW Kelly Drive
Parkville, MO 64152
1-816-741-3169
support@loctech.com

Location Technologies, Inc.
Standard Terms and Conditions

Hardware and Network Requirements:

Please note the following minimum PC hardware requirements for our Software Products. LTI products will work over Windows NT or Novell networks running TCP/IP.

PC's must be a Pentium class machine with a minimum of 400 MHz processor speed, 128 Mb (256Meg recommended) RAM, at least one open serial port and a minimum of 1 Gb free space on a fixed disk drive. The operating system should be Windows '9X, Windows 2000 or Windows NT Workstation version 4.0.

Payment Terms:

Software Purchases

Payment terms from LTI are net 30 days from date of shipment subject to credit approval. Purchases made from licensed distributors of LTI's products are subject to the distributors payment terms.

On-site Installation / Engineering / Training

Payment for services must be made net 30 days from the date of service. Expenses, including travel, lodging and meals will be billed.

Past Due Accounts

Past due accounts are subject to a surcharge of 1.5% per month. Past due services invoices will result in technical support services being suspended.

Acceptance of Products:

Opened software cannot be returned. Hardware products may not be returned except for repair. **You must request an RMA from LTI prior to returning any item** for restock or repair. A 15% restocking fee is required and no returns will be accepted after 30 days from date of shipment. Product acceptance is indicated by accepting delivery from your requested shipper unless otherwise provided for in a separate written agreement.

Training:

LTI will provide training as arranged per each individual contract. We follow a "train the trainer" format and require that all personnel receiving training have a familiarity with the Windows operating system.

Licensing:

All software products are licensed for use on a single machine only unless otherwise specified by written agreement.

Our Liability:

Location Technologies, Inc. is only responsible for the repair or replacement of our hardware products under the terms of the attached warranty document. Specifically, LTI is not responsible and shall be held harmless for the consequential damage or injury of any attached or associated equipment, property, or persons which may be associated with the use of our products.

Meeting Date: 06/12/2012

Submitted For: Kurt Ulrich

By: Bill Goodrich, Administrative Services

Information

Title:

Public Hearing to Consider introducing an Ordinance Amending Chapter 8 (Public Improvements and Special Assessments) of the City Charter as recommended by the Charter Commission.

Background:

On April 19, 2012, the Charter Commission recommended amending Chapter 8 of the City Charter (Public Improvements and Special Assessments) relating to the process for petitioning for a Local Improvement.

Currently pursuant to State Statute, if 35% or more of the affected property owners petition for a Local Improvement, the project can be approved by a 3/5ths vote of the City Council. If the percent of affected property owners petitioning is less than 35% or no petition is presented then the Local Improvement can only be ordered by a super majority or a 4/5ths vote of the City Council.

The Charter Commission has recommended that the City Charter be amended to increase the 35% affected property owners petition requirement to 50% of the affected property owners. City staff advised the Charter Commission that the merits to changing to a higher percent is that the larger number of required initial petitioners will reduce the chance of a petition against a project after time and money have been expended for feasibility studies, public hearing etc. all required to move the project forward. This would be more efficient and more cost effective on the front end but still allow the project to be petitioned against as permitted by Section 8.4.3 of the Charter. Said section is titled "Petition against the Local Improvement" and permits a proposed petitioned for Local Improvement to be stopped if a petition of affected property owners OPPOSED to the project is presented by a percent at least equal to those who petitioned FOR the project. Thus if the initial petition in favor of an improvement is 50% of the affected property owners, an opposing petition will also need 50% of the affected property owners to be effective to stop proceedings on the Local Improvement.

The charter amendment process is dictated by state statute. The process before you now is per Minn. Stat. 410.12 subd. 7 titled "Amendment by Ordinance." The Charter Commission may recommend an amendment to the City Council, which may adopt the recommendation in the form of an ordinance. The recommended ordinance cannot be enacted until after the City Council has conducted a public hearing on it and it must be adopted by a unanimous vote of the City Council. The public hearing has been published and is being held this evening. This procedure does not require a voter election, except that the ordinance is not effective for 90 days after its passage in order to allow for a referendum petition by residents which must be presented within 60 days of the ordinance's adoption by the Council. Please note, historically this is the amendment process most often used in the City to amend its charter.

Recommendation:

The Charter Commission and staff recommend introducing the proposed ordinance and subsequently placing it on the June 26, 2012 Agenda for formal adoption.

Funding Source:

N/A

Council Action:

Motion to introduce the Charter Commission recommended Ordinance Amending Chapter 8 (Public Improvement and Special Assessment).

Attachments

Proposed Charter Amendment Ordinance

Form Review

Inbox	Reviewed By	Date
Kurt Ulrich	Kurt Ulrich	06/07/2012 03:04 PM
Form Started By: Bill Goodrich		Started On: 06/06/2012 08:43 AM
	Final Approval Date: 06/07/2012	

ORDINANCE #12-_____

CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA

AMENDMENTS TO CHAPTER 8 OF THE HOME RULE CHARTER OF THE CITY OF RAMSEY. SAID AMENDMENTS RELATE GENERALLY TO THE PETITIONING REQUIREMENT FOR INTITATION OF A LOCAL IMPROVEMENT.

THE CITY OF RAMSEY ORDAINS.

SECTION 1. AUTHORITY:

This Ordinance is enacted pursuant to Minnesota Statutes §410.12, Subd. 7.

SECTION 2. AMENDMENT.

Section 8.4 and Subsections 8.4.1, 8 4.2, 8.4.3, and 8.44 of the Home Rule Charter of the City of Ramsey are hereby amended to provide as written below:

CHAPTER 8
PUBLIC IMPROVEMENTS AND SPECIAL ASSESSMENTS

Section 8.4 Local Improvement Procedure. When the city undertakes any local improvement to which the state local improvement code, Minn. Stat. Chapter 429 applies, it shall comply with the provisions of that law, except as modified below. The council may by ordinance prescribe the procedure to be followed in making any other local improvement and levying assessments therefore.

8.4.1 Sixty Day Waiting Period. A period of sixty (60) days shall elapse after the conclusion of the public hearing required by law to precede the ordering of the construction of a local improvement project (the “Public Hearing”) before the actual ordering of the local improvement by the council and the council entering into a contract for the local improvement construction.

8.4.2 Percentage of Owners Required. When the local improvement has been petitioned for by the owners of not less than 50 percent in frontage of the real property abutting on streets named in the petition as the location of the local improvement the resolution ordering the local improvement by the council may be approved by vote of a majority of all members of the council. When there has been no such petition, the resolution may be adopted only by vote of four-fifths of all members of the council.

8.4.3 Petition Against the Local Improvement. A proposed local improvement may be initiated by petition of the owners of real property abutting on the streets

named in the petition. If, within thirty (30) days of the conclusion of the public hearing, a petition against such local improvement is filed with the city administrator which petition is signed by a percentage of the owners of real property abutting on the streets named in the initiating petition as the location of the improvement which percentage is greater than the percentage of owners of real property who signed the initiating petition, the council shall not make such local improvement at the expense of the abutting property owners. For purposes of the foregoing sentence, "owners of real property" shall not include owners of properties zoned for commercial or industrial uses or owners of properties zoned residential greater than 10 acres in size based on zoning classifications in effect at the date of such petition, or owners of non-homestead real property greater than one acre in size.

8.4.4 Counter Petition in Favor of the Improvement. If within thirty (30) days of the filing with the city administrator of a petition against the local improvement as described in Section 8.4.3 above, there is filed with the city administrator a petition in favor of the local improvement, signed by owners of real property abutting on the streets named in the initiating petition as the location of the improvement, in a percentage greater than the percent of owners of real property who signed the petition against the local improvement, then in such event the council may disregard the petition against the local improvement and proceed with the local improvement.

SECTION 3. EFFECTIVE DATE.

This Ordinance shall become effective 90 days after its passage and publication subject to the provisions of Minnesota Statutes §410.12, Subd. 7.

PASSED by the City Council of the City of Ramsey, Minnesota, the _____ day of June, 2012.

Mayor

ATTEST:

City Administrator

Date recommend by Charter Commission: April 19, 2012

City Council Introduction date: June 12, 2012

Posting dates: _____

Public hearing publication dates: _____

Public hearing date: June 26, 2012

Adoption date: _____

Publication date: _____

Effective date: _____

Meeting Date: 06/12/2012

By: Chris Anderson, Community
Development

Information

Title:

Adopt Ordinance Amending City Code Chapter 10 (Animals) Related to the Keeping of Non-Domestic Animals on Parcels Less Than Three (3) Acres in Size

Background:

Over the past several months, Staff has fielded an increased number of calls regarding the keeping of certain non-domestic animals on parcels less than three (3) acres in size. A majority of the questions fielded are in regard to chickens, with a few also related others such as beekeeping, pot-bellied pigs and alpacas. Along with many other non-domestic animals addressed in City Code, at least three (3) acres are needed to be eligible to maintain chickens. Others, including alpacas and honeybees, are not addressed presently in City Code at all. A case was presented at the May 8, 2012 regular City Council meeting to introduce an ordinance that would permit up to four (4) chickens on parcels between 0.25 acres and three 2.99 acres. However, City Council directed Staff to incorporate more of a sliding scale according to lot size rather than a one size fits all approach and bring the matter back for introduction at a subsequent meeting.

Observations:

As directed at the May 8, 2012 City Council meeting, the ordinance amendment bases the permitted number of chickens on lot size, with an increase of four (4) chickens per quarter acre in size, up to the existing three (3) acre threshold. When the ordinance amendment was again presented to City Council for introduction at the May 22, 2012 meeting, there was discussion about increasing the number of chickens permitted on a parcel up to one quarter acre to six (6). The proposed ordinance amendment has been revised slightly, again based on that discussion by the City Council, to permit six (6) chickens on a parcel up to 0.24 acres in size, eight (8) chickens on parcels between 0.25 acres and 0.49 acres, and then an additional four (4) chickens for each additional quarter acre. It should be noted that this would only be applicable in the R-1 zoning district (single family zoning).

The ordinance amendment specifies that only hens (i.e., no roosters) are permitted on lots less than three (3) acres; once at three (3) or more acres, it reverts to the existing regulations which do not specifically prohibit roosters. On larger acreage, it may be possible that animal owners are more interested in breeding, which requires a rooster. Egg production does not require fertilization by a rooster. No changes are being proposed for parcels three (3) acres in size or greater other than to include alpacas in the table of animals and animal units (similar to a llama).

The ordinance amendment also incorporates provisions for beekeeping within the community, which is something that City Code does not presently address. These standards include requiring a conditional use permit to keep up to six (6) hives on a property, as well as requiring fly-ways, setbacks from property lines and habitable structures, and concerns with possible swarming or aggressiveness.

Chapter 10 of the City Code (Animals) is not found as part of the Zoning or Subdivision Code, and therefore is not required to be reviewed by the Planning Commission. The proposed ordinance was introduced at the May 22, 2012 City Council meeting and is now eligible for adoption.

Recommendation:

Staff recommends that the City Council adopt the ordinance to amend City Code Chapter 10 (Animals) to reduce the minimum lot size threshold to maintain chickens and provide proper standards for allowing for bee keeping in certain circumstances.

Funding Source:

Preparation of the Ordinance is being handled as part of regular Staff duties.

Council Action:

Motion to waive the Charter requirement to read the ordinance aloud -AND- adopt the ordinance to amend Chapter 10 of City Code reducing the minimum lot size threshold to maintain chickens and provide proper standards for allowing bee keeping in certain circumstances..

Roll Call Vote:

Councilmember Elvig
Councilmember McGlone
Councilmember Wise
Councilmember Strommen
Councilmember Backous
Councilmember Tossey
Mayor Ramsey

Attachments

Draft City Council Meeting Minutes Dated May 22, 2012

Proposed Ordinance with Edits

Form Review

Inbox	Reviewed By	Date
Tim Gladhill	Tim Gladhill	06/06/2012 09:38 AM
Kurt Ulrich	Kurt Ulrich	06/07/2012 02:13 PM

Form Started By: Chris Anderson Started On: 06/01/2012 01:44 PM

Final Approval Date: 06/07/2012

upon review and approval of a drainage and erosion control plan and declaring the terms of the permit, based on findings of fact.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Tossey, Backous, Strommen, and Wise. Voting No: None. Absent: Councilmember McGlone.

7.02: Introduce Ordinance to Amend City Code Chapter 10 (Animals) Related to Keeping of Non-Domestic Animals on Parcels Less Than Three (3) Acres in Size

Senior Planner Gladhill reviewed the staff report.

The Council agreed with the suggestion of Mayor Ramsey to increase the number of chickens to six on a parcel up to one-quarter acre in size.

Motion by Mayor Ramsey, seconded by Councilmember Backous, to introduce an ordinance to amend City Code Chapter 10 (Animals) to reduce the minimum lot size threshold for certain non-domestic animals and provide proper standards for allowing for bee keeping in certain circumstances, as amended.

Further discussion: Senior Planner Gladhill indicated the typographical error noted by Councilmember Strommen will be corrected prior to second reading.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Backous, Elvig, Strommen, Tossey, and Wise. Voting No: None. Absent: Councilmember McGlone.

7.03: Consider Award of Contracts for the 2012 Storm Sewer Improvement Projects; City Improvement Project #12-24

City Engineer Himmer reviewed the staff report and presented bids.

Motion by Councilmember Backous, seconded by Mayor Ramsey, to award a contract for Division A of the 2012 storm sewer improvements; City Improvement Project #12-24 to Driden Excavating in the amount of \$157,262 plus overhead costs; and award a contract for Division B of the 2012 storm sewer improvements; City Improvement Project #12-24 to Julian Johnson, in the amount of \$77,383 plus overhead costs.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Backous, Elvig, Strommen, Tossey, and Wise. Voting No: None. Absent: Councilmember McGlone.

7.04: Adopt Resolution #12-05-XXX for Continuing Efforts Towards a Shared Fire Service

Fire Chief Kapler presented the staff report and indicated the proposed resolution had been presented to all five communities involved, adopted by three (Nowthen, Oak Grove, St. Francis), was being considered by Ramsey tonight, and he had not yet heard back from one community (Bethel).

ORDINANCE #12-__

CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA

AN AMENDMENT TO CHAPTER 10 WHICH IS KNOWN AS THE ANIMAL REGULATIONS CHAPTER OF THE CITY CODE OF RAMSEY, MINNESOTA.

AN ORDINANCE AMENDING CHAPTER 10 BY AMENDING CHAPTER 10, ARTICLE I, ENTITLED IN GENERAL.

The City of Ramsey ordains:

SECTION 1. AMENDMENT

City Code Chapter 10, Article I is amended as follows (insertions indicated as underline, deletions indicated as ~~strikethrough~~):

Sec. 10-1. - Keeping of certain animals, livestock and poultry.

(a) *Definitions.* The following words, terms and phrases, when used in this section, shall have the meanings ascribed to them in this subsection, except where the context clearly indicates a different meaning:

Pony means a horse that does not exceed 54 inches or 13.2 hands in height.

(b) ~~*Prohibited animals*~~ *Animals on less than three (3) acres.* None of the following animals shall be maintained on any parcel of land in the city that is not at least 3.0 acres (130,680 square feet) in size: horses, llamas, cows, mules, donkeys, burros, ponies, goats, pigs, sheep, lambs, turkey, geese, ~~chickens,~~ and ducks. EXCEPTION: Chickens (hens only, no roosters) may be maintained on parcels less than three (3) acres. The following chart prescribes the number of chickens that can be maintained on lots of record to which the animal owner has fee title and desires to maintain chickens:

<u>Lot Size</u>	<u>Number of Chickens Permitted (no roosters)</u>
<u>0.00 to 0.24 Acres</u>	<u>46</u>
<u>0.25 to 0.49 Acres</u>	<u>8</u>
<u>0.50 to 0.74 Acres</u>	<u>12</u>
<u>0.75 to 0.99 Acres</u>	<u>16</u>
<u>1.00 to 1.24 Acres</u>	<u>20</u>
<u>1.25 to 1.49 Acres</u>	<u>24</u>
<u>1.50 to 1.74 Acres</u>	<u>28</u>

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1.75 to 1.99 Acres	32
2.00 to 2.24 Acres	36
2.25 to 2.49 Acres	40
2.50 to 2.74 Acres	44
2.75 to 2.99 Acres	48

(c) *Animal unit.* Animal unit is defined as a measure used to compare differences in production of animal wastes. The following chart establishes the number of animal units assigned to certain livestock and poultry:

Animal	Number of Units
Cow	1.5
Horse, mule, llama, alpaca	1.0
Donkey, burro, pony, goat, pig, sheep, lamb	0.5
Turkey, goose	0.1
Chicken, duck	0.04

(d) *Permitted animals.*

(1) The following chart prescribes the number of animal units that can be maintained on lots of record to which the animal owner has fee title and desires to maintain a mix of livestock, equines, poultry, or fowl (refer to section 10-24 for regulations for maintaining horses or equines exclusively):

Parcel Size	Number of Animal Units Allowed
3 acres (130,680 square feet)	2
5 acres (217,800 square feet)	3
10 acres (435,600 square feet)	5
15 acres (653,400 square feet)	7
20 acres+ (871,200 square feet)	No limitation

(2) Maintenance of more animals than the prescribed number in subsection (d)(1) of this section is permitted only with a conditional use permit in accordance with section 117-50

(e) *Non-traditional or exotic animals.*

(1) Non-traditional or exotic animals are those that are not normally considered to be domesticated but are not wild animals as defined in section 10-119. A person may keep a non-traditional or exotic animal as defined herein only upon issuance of a conditional use permit in accordance with section 117-50. In determining whether an animal is considered non-traditional or exotic, it shall be the responsibility of the applicant to supply the city with the necessary data and information to reasonably prove that the animal is not dangerous in captivity. This information shall be part of the public record and shall be discussed as part of the conditional

use permit review.

(2) In reviewing a request to keep non-traditional or exotic animals, the city may impose conditions and restrictions as it finds necessary, including but not limited to the following:

- a. Restrictions on the number and type of animals;
- b. Setbacks greater than those required in section 10-23 or the underlying zoning district for the property in question;
- c. Restrictions on the size, height, and type of enclosures;
- d. Screening or landscaping of the proposed use.

(3) The applicant must also submit a copy of all permits required from any other regulatory agencies.

(f) *Prohibition of nuisance animals.* No animals of any type may be maintained on any parcel within the city, no matter what size the parcel, if the maintenance of said animals creates a nuisance as defined in chapter 30.

(g) *Beekeeping.* Bee hives may be maintained on a parcel with the issuance of a Conditional Use Permit.

(1) Hives may only be located on lots with an existing use.

(2) No more than six (6) hives may be located on a lot.

(3) No hive shall exceed twenty (20) cubic feet in volume.

(4) No hive shall be located closer than three (3) feet from any property line or within any easement, whichever is more restrictive.

(5) No hive shall be located closer than ten (10) feet from a neighboring, inhabited structure.

(6) A constant supply of water shall be supplied or available to all hives.

(7) A flyway barrier at least six (6) feet in height shall shield any part of a property line that is within twenty-five (25) feet of a hive. The flyway barrier shall consist of 100% opaque fencing, dense, year-round (coniferous) vegetation, existing structures, or combination thereof, and shall extend at least twenty-five (25) feet beyond the hive.

(8) Colonies shall be maintained in moveable frame hives with adequate space and management techniques to prevent overcrowding and swarming.

(9) In any instance in which a colony exhibits unusually aggressive characteristics by stinging or attempting to sting without due provocation or exhibits an unusual disposition towards swarming, it shall be the duty of the beekeeper to requeen the colony. Queens shall be selected from stock bred for gentleness and nonswarming characteristics.

(h) *Maintenance Standards.*

(1) Chickens must be contained in an enclosure -and said enclosure- must be designed in accordance with City Code Chapter 117, Article II (Zoning) and MN State Building Code. The enclosure shall be counted as an accessory building when calculating total number of permitted accessory buildings on a property. The enclosure and surrounding grounds shall be maintained in a clean and sanitary condition and in good repair. Flies, rodents

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and noxious odors shall be controlled.

(2) The enclosure shall remain completely enclosed to prevent unwanted intrusion by outside animals and migratory birds.

(3) A confined exercise area must be provided for the animals. The exercise area shall be fenced off to prevent the animals from roaming at large.

(3) Manure shall not be permitted to accumulate on the property. Manure shall be removed at least once (1) per week, but while on the property shall be properly stored.

(e) Setbacks. All enclosures must maintain a minimum setback of ten (10) feet from all property lines and must maintain a minimum setback of thirty (30) feet from all adjacent inhabited structures.

(1) If 100% opaque fencing (privacy fencing) is utilized for the exercise area, said fencing is subject to the standard fence regulations and setbacks within City Code Chapter 117. If alternative fencing is used (for example, chain link fencing), then said fencing shall be no closer than ten (10) feet to a property line.

SECTION 2. PURPOSE

The purpose of this amendment is to reduce the minimum lot size required to maintain chickens, ~~duck, turkey, goose, goat and pot-bellied pigs~~ on a property as well as to include provisions to consider beekeeping through the issuance of a conditional use permit.

SECTION 3. SUMMARY

The following official summary of Ordinance #12-__ has been approved by the City Council of the City of Ramsey as clearing informing the public of the intent and effect of the Ordinance.

It is the intent and effect of Ordinance #12-__ to reduce the minimum lot size required to maintain chickens on a property, ~~duck, turkey, goose, goat and pot-bellied pigs~~ and to allow for ~~the provision of~~ bee-keeping, ~~with certain safety provisions~~ through the issuance of a conditional use permit.

SECTION 4. EFFECTIVE DATE

The effective date of this ordinance is thirty (30) days after its passage and publication, subject to City Charter Section 3.9.

Passed by the Ramsey City Council this the ~~22nd~~-12th day of ~~May~~June, 2012.

Mayor

ATTEST:

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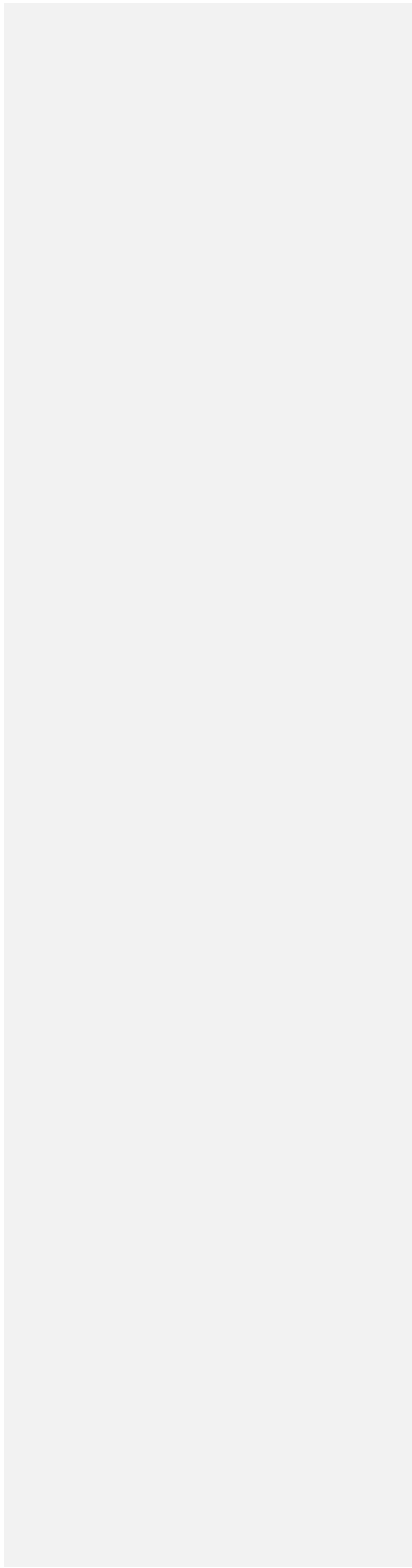
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City Clerk

Introduction Date:
Posting Dates:
Adoption Date:
Publication Date:
Effective Date:



CC Regular Session

7. 2.

Meeting Date: 06/12/2012

By: Jim Way, Police

Information

Title:

Adopt Ordinance amending Chapter 54 of the Ramsey City Code, Known as the Traffic and Vehicle Chapter, Article IV, Sections 96-105 - Recreational Vehicles

Background:

City Council members indicated a desire to expand the use of recreational vehicles in the City of Ramsey. The Ramsey City Council discussed the use of recreational vehicles at two City Council work sessions. Staff received work direction at the last work session to open up the entire City to the use of recreational vehicles. At the April 8, 2012 Council Meeting, a revised recreational vehicle ordinance was introduced. The revised ordinance now includes the use of Golf Carts and All Terrain Vehicles (ATV's) on city roadways. The ordinance that is up for adoption, refers to MN State Statute and complies with the provisions and requirements prescribed by law.

Observations:

Staff would like to offer a training session for citizens to review the new ordinance and answer questions about the use of golf carts and ATV's on the roadway.

Funding Source:

A \$10 per permit fee would be charged to cover the cost of purchasing permits and processing permit applications.

Council Action:

Motion to waive the City Charter requirement to read the ordinance aloud and Adopt Ordinance #12-XXX to Amend Chapter 54 of the Ramsey City Code, which is the Chapter known as the Traffic and Vehicle Chapter of the Ramsey, Minnesota, City Code - an Ordinance Amending Article IV, Sections 96-105 Recreational Vehicles.

Roll Call Vote:

- Councilmember McGlone
- Councilmember Elvig
- Councilmember Backous
- Councilmember Tossey
- Councilmember Wise
- Councilmember Backous
- Mayor Ramsey

Attachments

Rec Vehicles

Form Review

Inbox	Reviewed By	Date
Jo Thieling	Jo Thieling	06/06/2012 04:08 PM
Kurt Ulrich	Kurt Ulrich	06/07/2012 02:42 PM
Form Started By: Jim Way		Started On: 06/05/2012 07:58 AM

Final Approval Date: 06/07/2012

**ORDINANCE #11-16
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

AN AMENDMENT TO CHAPTER 54 OF THE RAMSEY CITY CODE, WHICH CHAPTER IS KNOWN AS THE TRAFFIC AND VEHICLE CHAPTER OF THE RAMSEY, MINNESOTA, CITY CODE.

AN ORDINANCE AMENDING ARTICLE IV, SECTIONS 96-105 RECREATIONAL VEHICLES.

The City of Ramsey ordains:

SECTION 1. AUTHORITY

This ordinance is adopted pursuant to and under the authority of the City Charter of the City of Ramsey.

SECTION 2. REPEAL

Article IV Recreational Vehicles of the City Code is hereby repealed and replaced with the following new Article IV Recreational Vehicles.

SECTION 3. AMENDMENTS

ARTICLE IV. RECREATIONAL VEHICLES

Sec. 54-96. - Purpose.

The purpose of this article is to provide reasonable regulations for the use of recreational vehicles on public and private property in the city. This article is not intended to allow what the Minnesota state statutes expressly prohibit nor to prohibit what the state statutes allow. It is intended to prevent public nuisance.

Sec. 54-97. – Definitions.

The following words, terms and phrases, when used in this article, shall have the meanings ascribed to them in this section, except where the context clearly indicates a different meaning:

Agricultural zone means the areas in the state lying south and west of a line along the following highway rights-of-way: starting at the North Dakota border, the line goes east along State Highway 10 to State Highway 23, then follows Highway 23 east to State Highway 95, which it follows to the Wisconsin border.

All-terrain vehicle (ATV) means a motorized flotation-tired vehicle of not less than three but not more than six low-pressure tires. It includes Class 1 (engine displacement of less than 960 cubic centimeters and total dry weight of less than 1,000 pounds) and

Class 2 (engine displacement of less than 960 cubic centimeters and total dry weight of 1,000 to 1,800 pounds) ATV's as per Minnesota State Statute.

Boulevard means that portion of the street or highway between the roadway and private property.

Designated non-use area means that area south of a straight line running from east to west at the 16700 block, starting at the Rum River on the east and running to the City of Elk River border on the west. Within the non-use area, access to Elmcrest Park and Central Park will be made and restricted to loading and unloading of snowmobiles and ATVs in the parking lots only. A designated route from the parking lot will be clearly posted to allow access to 167th Avenue from Elmcrest Park. A designated route from the parking lot of Central Park will be clearly posted to allow access to Armstrong Blvd. for direct northbound travel to the designated use area.

Designated trail shall mean a clearly marked route for recreational vehicles to follow.

Designated use area means the area north of a straight line running from east to west at the 16700 block, starting at the Rum River on the east and running to the City of Elk River border on the west.

Go-cart means a low, four-wheeled motorized device with an open or closed frame designed and used for off-street operation only.

Golf-cart means a passenger conveyance, electric or gasoline motor powered vehicle, with four low pressure tires and a engine displacement not exceeding 800 cubic centimeters and a total dry weight not exceeding 800 pounds.

Mini-bike means a small, two-wheeled motorized device designed for off-street operation. Mini-bikes may include miniature motorcycles and do not qualify as a moped, motorcycle or an ATV.

Off-highway motorcycle (OHM) means vehicles traveling on two wheels with a seat to be straddled by the operator with handlebars for steering control. OHMs may include some mini-bikes. They are for off-street operation only.

Operate means to ride in or on, and control the operation of a recreational vehicle.

Operator means every person who operates, or is in actual physical control of a recreational vehicle.

Public property means property that may be used by the public, subject to reasonable regulation by a governmental body. Such property includes city parks, city parking lots and public school parking lots and grounds.

Public road right-of-way means the entire right of way of a roadway that is not privately owned, including the traveled portions, banks, ditches, shoulders and medians.

Recreational vehicle means all-terrain vehicles, utility task vehicles, golf carts, snowmobiles, off-highway motorcycles, mini-bikes, go-carts, and other motorized vehicles, that by their use fit this definition.

Roadway means that portion of a street or highway improved, designed or ordinarily used for vehicular travel but not including the boulevard. For the purposes of this article, roadways not dedicated for public use and not maintained by the city are not included within this definition.

Semi-public property means private property generally for use by the public but not owned or maintained by a governmental body. Such property includes without limitation: church property, shopping center property and other property generally used by patrons of a commercial or private business establishment.

Snowmobile means a self-propelled vehicle designed for travel on snow or ice steered by skis or runners.

Street or highway means the entire width between boundary lines of any way or place when any part thereof is open to the use of the public, as a matter of right, for the purpose of vehicular traffic (includes the right-of-way or boulevard). For the purposes of this article, streets or highways not dedicated for public use and not maintained by the city are not included within this definition.

Utility Task Vehicle means a side-by-side, four-wheel drive that has four wheels, is propelled by an internal combustion engine with a piston displacement capacity of 1,200 cubic centimeters or less, and has a total dry weight of 1,800 but less than 2,600 pounds.

Sec. 54-98. - Operation regulations for ATV's and Utility Task Vehicles

- (a) *ATV's and Utility Task Vehicles MAY NOT be operated in the following areas of the city:*
- (1) Any area posted that ATV's or Utility Task Vehicles are not allowed.
 - (2) On or along the Burlington Northern railroad right-of-way.
 - (3) On lots less than two and one-half (2 ½) acres in size unless it is for maintenance or plowing.
 - (4) On city sidewalks, bike or pedestrian trails.
 - (5) On semi-public, public or private property unless permission is posted.
 - (6) Within a city parking ramp.
 - (7) Within a county or state highway right-of-way other than as listed in Specific Limitations defined below.
 - (8) Within the boulevard of a city roadway.
 - (9) Within a county or state highway right-of-way in the Designated Non-Use Area.
 - (10) Within a county or state highway right-of-way from April 1 to August 1 as part of Minnesota's Agricultural Zone.
- (b) *ATVs and Utility Task Vehicles MAY be operated in the following areas of the city:*

- (1) On your own property or the property of another with written permission, only if the property is two and one-half (2 ½) acres or greater in size, subject to the provisions of this article.
 - (2) On privately owned property if the operator has in possession written permission from the property owner; or on privately owned property if the property owner has posted a clearly visible notice indicating "ATV's Allowed" or "Utility Task Vehicles Allowed" or words substantially similar.
 - (3) On all city roadways as allowed by Minnesota State Statute, however, NOT including county or state highways.
 - (4) On public or semi-public property only if posted with a clearly visible notice indicating "ATV's Allowed" or "Utility Task Vehicles Allowed" or words substantially similar.
- (c) *Specific Limitations for operation of ATV's and Utility Task Vehicles.*
- (1) A direct crossing of a street, roadway, county or state highway is permitted if:
 - a. The crossing is made at an angle of approximately 90 degrees to the roadway at a place where no obstruction prevents a quick and safe crossing or blocks the view of oncoming traffic for 300 feet.
 - b. The vehicle is brought to a complete stop before crossing the shoulder or entering upon the traveled portion of the street, road or highway.
 - c. The driver yields the right-of-way to all oncoming traffic that constitutes an immediate hazard.
 - d. In the case of a divided highway, the crossing shall be made only at an intersection of the highway with another street or road.
 - e. If the crossing is made during period of one-half hour after sunset to one-half hour before sunrise or in conditions of reduced visibility, the front and rear lights of the vehicle must be on.
 - (2) An ATV may only be operated with the number of passengers that it was designed to carry.
- (d) Every person operating an ATV or Utility Task Vehicle on the roadway has the rights and duties applicable to the driver of any other vehicle under the provisions of Chapters 169 and 84 of Minnesota Statutes, except those provisions that cannot be reasonably applied.
- (e) Every operator/owner of an ATV or Utility Task Vehicle must provide proof of insurance complying with Minnesota State Statute Section 65B.48, subd 5.
- (f) For an ATV or Utility Task Vehicle to be used off of the owner's property, the owner first must request and obtain a City permit from the police department.
- (g) *Permits*
- (1) A permit application containing the following information must be completed and approved by the city's police department, prior to receiving a permit.
 - a. Registered owner name, address and telephone number.
 - b. Make, model, vehicle identification number and DNR registration number of the ATV or Utility Task Vehicle.
 - c. Proof of insurance for the vehicle.
 - d. Class description (ATV's only).
 - (2) Permits must be affixed to the front center handlebar, lower center windshield area, or other location visible from the front center of the recreational vehicle.

- (3) A granted permit will be valid for a maximum term of three (3) years.
- (4) A permit may be revoked for a violation of any provision of this article, after due investigation by the city's police department and final determination by the police chief. Any revocation may be appealed to the city council by providing a request for appeal in writing within 14 days of the final revocation. The revocation will remain in force until the appeal process has been completed.

Sec. 54-99. – Operation regulations for Golf Carts.

- (a) *Golf Carts MAY NOT be operated in the following areas:*
 - (1) Any area posted that Golf Carts are not allowed.
 - (2) On or long the Burlington Northern railroad right-of-way.
 - (3) On city sidewalks, bike or pedestrian trails.
 - (4) On semi-public, public or private property unless permission is posted.
 - (5) Within a county or state highway right-of-way other than as listed in Specific Limitations defined below.
 - (6) Within the boulevard of a city roadway.
 - (7) Within a county or state highway right-of-way in the Designated Non-Use Area.
 - (8) Within a county or state highway right-of-way from April 1 to August 1 as part of Minnesota's Agricultural Zone.
- (b) *Golf Carts may be operated only as follows:*
 - (1) On your own property or the property of another with written permission, subject to the provisions of this article.
 - (2) On privately owned property if the operator has in possession written permission from the property owner; or, on privately owned property if the property owner has posted a clearly visible notice indicating "Golf Carts Allowed" or words substantially similar.
 - (3) On all city roadways as allowed by Minnesota State Statute, however, NOT including county or state highways.
 - (4) On public or semi-public property only if posted with a clearly visible notice indicating "Golf Carts Allowed" or words substantially similar.
- (c) *Specific limitations for operation of Golf Carts.*
 - (1) A direct crossing of any street, roadway, county or state highway is permitted if:
 - a. The crossing is made at an angle of approximately 90 degrees to the roadway at a place where no obstruction prevents a quick and safe crossing or blocks the view of oncoming traffic for 300 feet.
 - b. The vehicle is brought to a complete stop before crossing the shoulder or entering upon the traveled portion of the street, road or highway.
 - c. The driver yields the right-of-way to all oncoming traffic that constitutes an immediate hazard.
 - d. In the case of a divided highway, the crossing shall be made only at an intersection of the highway with another street or road.
 - e. If the crossing is made during period of one-half hour after sunset to one-half hour before sunrise or in conditions of reduced visibility, the front and rear lights of the vehicle must be on.

- (2) A Golf Cart may only be operated with the number of passengers that it was designed to carry.
- (3) If ridden on a roadway, a valid driver's license is required by the operator.
- (d) *Golf Cart Equipment Requirements.*
 - (1) Slow moving vehicle emblem.
 - (2) Operable horn.
 - (3) Seat belts for driver and passengers.
- (e) *Every person operating a Golf Cart on the roadway has the rights and duties applicable to the driver of any other vehicle* under the provisions of Chapter 169 of the Minnesota State Statutes, except those provisions that cannot be reasonably applied.
- (f) *Every operator/owner of a Golf Cart must be able to provide proof of insurance when required, by an authorized enforcement officer, complying with Minnesota State Statute Section 65B.48, subd 5.*
- (g) *For a Golf Cart to be used off of the owner's property, the owner first must request and obtain a City of Ramsey permit* from the police department.
- (h) *Permits*
 - (1) A permit application containing the following information must be completed and approved by the city's police department, prior to receiving a permit.
 - a. Registered owner's name, address and telephone number.
 - b. Make, model, and vehicle identification number of the Golf Cart.
 - c. Proof of liability insurance for the Golf Cart.
 - (2) Permits must be affixed to the lower center windshield area or other location visible from the front center of the recreational vehicle.
 - (3) A permit will be valid for a maximum term of three (3) years.
 - (4) A permit may be revoked for a violation of any provision of this article, after due investigation by the city's police department and final determination by the police chief. Any revocation may be appealed to the city council by providing a request for appeal in writing within 14 days of the final revocation. The revocation will remain in force until the appeal process has been completed.

Sec 54-100 Operation regulations for Snowmobiles

- (a) *Designated non-use areas.* Snowmobiles may not be operated in the designated non-use areas except in those limited areas where permitted as follows:
 - (1) Within the non-use area, access to Elmcrest Park and Central Park will be made and restricted to loading and unloading of snowmobiles in the parking lots only.
 - (2) A designated route from the parking lot will be clearly posted to allow access to 167th Avenue from Elmcrest Park. A designated route from the parking lot of Central Park will be clearly posted to allow access to Armstrong Boulevard for direct travel to the designated trail.
 - (3) On any clearly marked designated trail.
 - (4) On your own property or the property of another with written permission only if the property is greater than two and one-half (2 ½) acres in size.
- (b) *Designated use areas.* Snowmobiles may be operated only in the areas defined in section 54-97 as the designated use areas and then only as follows:

- (1) On the operator's property subject to the provisions of this article.
 - (2) On privately owned property if the operator has in possession written permission from the property owner; or, on privately owned property if the property owner has posted a clearly visible notice indicating "Snowmobiles Allowed" or words substantially similar.
 - (3) On county or state highways within the designated use area as regulated by state statute.
 - (4) On city streets within the designated use area only as allowed by state statute.
 - (5) On public or semi-public property within the designated use area only if said property is clearly posted with signs designating the area or specific areas as open to recreational vehicles.
- (c) *Agricultural Zone.* No snowmobiles may be operated within the right-of-way, including the ditch of a trunk, county state-aid, or county highway from April 1 to August 1 as part of Minnesota's Agricultural Zone.
 - (d) *Hours of operation.* Snowmobiles may not be operated any place in the city between the hours of 10:00 p.m. and 8:00 a.m. Sunday through Friday, and between the hours of 1:00 a.m. and 8:00 a.m. Saturday and Sunday.
 - (e) *Snow cover.* From November 1 to March 31, Snowmobiles may not be operated off the operator's property or the property of another that the operator has written permission to be on, unless there is a minimum six inches of snow cover on the ground.
 - (f) *Reflective Material.* Snowmobiles must have reflective material of at least 16 square inches on each side forward of the handlebars, so as to reflect lights at a 90-degree angle.

Sec. 54-101 – Operation limitations for mini-bikes and go-carts.

- (a) Mini-bikes greater than 110cc may not be operated within the Designated Non-Use Area, regardless of lot size.
 - (b) Go-carts may not be operated within the Designated Non-Use Area.
 - (c) Mini-bikes and go-carts may not be operated off of their own property without the written permission of the owner of the property being ridden on.
- (Code 1978, § 6.04.04; Ord. No. 88-1, 2-25-1988; Ord. No. 88-19, 12-18-1988; Ord. No. 93-18, 12-13-1993; Ord. No. 09-14, § 1, 9-22-2009)

Sec. 54-102 - Towing restriction.

It is unlawful for any person to operate a recreational vehicle so as to tow any person or thing on a street or highway except through the use of a rigid tow bar, as defined by state statute, attached to the rear of the recreational vehicle.
(Code 1978, § 6.04.05; Ord. No. 88-1, 2-25-1988; Ord. No. 88-19, 12-18-1988; Ord. No. 93-18, 12-13-1993)

Sec. 54-103. - Map.

A map showing the boundaries of the designated use and non-use areas shall be on display at the city hall and the police department for public viewing, during normal business hours.

Sec. 54-104. – Permit Fee.

The permit fee for a Recreational Vehicle required by this section shall be

set by council resolution and listed in the rates and fees ordinance.

Sec. 54-105. - Penalty.

Violation of any provision of this section shall constitute a petty misdemeanor.

SECTION 3. SUMMARY

The following is the official summary of Ordinance XXXX which has been approved by the City Council of the City of Ramsey as clearly informing the public of the intent and effect of the Ordinance.

It is the intent and effect of Ordinance XXXX to amend Ramsey, Minnesota City Code Sections Article IV, Sections 96-102 Recreational Vehicles. Expand the Designated Recreational Use Areas within the City of Ramsey to include a Restricted Use Area of the City.

SECTION 4. EFFECTIVE DATE

The effective date of this Ordinance is thirty (30) days after its passage and publication, subject to City Charter.

Adopted by the Ramsey City Council the XX day of May 2012.

Mayor

ATTEST:

City Clerk

Introduction Date:	May 8, 2012
Posting Dates:	?????????????
Adoption Date:	May 22, 2012
Publication Date:	?????????????
Effective Date:	June 21, 2012

Meeting Date: 06/12/2012

Submitted For: Tim Himmer

By: Len Linton, Engineering/Public Works

Information**Title:**

Consider Stormwater Improvements on 148th Lane NW and 149th Lane NW, and Approve Change Orders for City Improvement Project 12-24

Background:

Last summer the City experienced several significant rainfall events that lead to many localized flooding concerns and resident complaints. The large volumes of precipitation that occurred over a short period of time appears to have elevated the groundwater within areas of the City, and prohibited the generous rate of infiltration that typically takes place in the Anoka sand plain. Throughout the summer and fall staff worked hard at registering and responding to the calls, and evaluating the situations on an individual basis to determine whether quick fixes could be implemented (culvert obstructions, re-ditching, etc.) to alleviate the immediate concerns.

This item was discussed at the Public Works Committee on August 15, 2011, and at that time staff summarized the areas of concern that were being investigated based upon citizen complaints received. A summary was presented that outlined the concern identified, actions steps to evaluate the concern, additional investigations that would be necessary to fully understand the situation, and recommendations for specific projects that could be implemented rather quickly and inexpensively. At that time staff classified the issues into 3 categories:

1. Those that required no further action. They were evaluated and corrected, or did not need correcting because the water was fully contained within dedicated drainage & utility easements (functioning as designed).
2. Those that required additional investigation and evaluation before deciding on a long term solution, and implementing corrective actions.
3. Those that had an identified recommendation for immediate action.

At that meeting the Committee briefly discussed the areas of concern, and directed staff to prepare plans and specifications for the items identified in category 3. Once this direction was ratified by the City Council on September 13, 2011, and the plans completed, it was too late in the season to secure bids and complete the improvements in 2011. It was then decided to delay construction until 2012, where some of the improvements could be completed by inclusion in a larger stormwater improvement plan set. Staff finalized plans for all these items, including incorporation of additional items that were approved at the Public Works Committee meeting in March of 2012, and bid them under two separate contracts, which were awarded by the City Council on May 22, 2012.

The final three areas of flooding concern from 2011 were discussed by the Public Works Committee on May 15, 2012, with the recommendation being to perform corrective actions on all three this year. These items were included in the report from Public Works on the consent agenda earlier this evening, and include the Rum River Hills Gold Course, 148th Lane NW, and 149th Lane NW. City staff has prepared plan sheets for the corrective actions contemplated for both 148th Lane and 149th Lane, and has received quotes from the two storm sewer improvement contractors that are already working in the City. The Rum River Hills Gold Course will be handled as a separate case on this evenings agenda.

Notification:

The property owners that may be affected by these project were notified of the meeting and have received a copy of the agenda.

Observations:

In the fall of 2011, City staff contracted out the investigation of all the significant areas of flooding concern to Hakanson Anderson. The goal was to have an independent third party evaluate the areas, provide options for corrective actions, and associated estimates to implement the work. They completed their analysis and compiled a report that references potential solutions for each area identified (see attached final report). This report was introduced in general terms at the February 2012 Public Works Committee meeting, with the direction being that staff would come back before the committee over the next several months to discuss the merits of each potential improvement in more detail. This case will focus primarily on section one (148th Lane) and section two (149th Lane) of the report.

148TH LANE NW

The site is east of Nowthen Boulevard (CSAH 5), southeast of Ramsey Elementary School. There is a low area across three (3) lots south of 148th Lane, that was created as a stormwater pond for the area and wetland mitigation for the development. There is a storm sewer inlet to this area along the west edge, which drains the subdivision to the north. There is also an outlet from this area on the east end that has an invert elevation approximately 2.5 feet above the low point in the basin. The outlet pipe connects to a manhole in the boulevard south of 148th Lane, which also takes storm water runoff from the roadway; this structure directs both of these flows to the east and into a downstream receiving body in the City's stormwater system. The inverts on both pipes in the manhole are at approximately the same elevation, and therefore water may be entering this basin that is not intended to flow there from the stormwater system. It was also observed that the basin has several undulations in the ground surface that do not allow the water to evenly spread out across the entire drainage and utility easement; basically there appears to be three separate, somewhat isolated basins.

Hakanson Anderson explored several options for installing another outlet and determined that each option would not significantly affect the high water elevation in the basin, nor justify the costs associated with performing them. Additional easements would need to be acquired, and disruption of neighboring properties would occur. For this project staff is proposing to install a check valve in the storm pond outlet to block drainage from the street and only allow water to exit the basin through this structure, thereby eliminating additional flow to the area. It is also planned to regrade the area at the rear of these three properties to restore the contours originally designed for the basin.

149TH LANE NW

This area is west of TH 47 and east of 149th Lane, and the area in question is adjacent to the large DNR wetland that abuts TH 47. The City Council accepted a petition on August 23, 2011 that was initiated by the resident at Lot 3, Block 1, Ramsey Meadows 4th Addition (5410 149th Lane NW). The homeowner contacted the City about water standing in their backyard. Review of the grading plan indicates this area was part of the wetland mitigation plan prepared for the plat and is controlled by Wetland Conservation Act rules. There is a draintile present across the neighboring lot to the south that connects this backyard to the DNR wetland. Stormwater and/or groundwater have flowed into the backyard, which is lower than the wetland in locations, and drainage and utility easements are recorded over this area on the property. The DNR lists the normal water level of the wetland as 860.0, and there is a retaining wall with steps in the backyard with an elevation on the lowest step at approximately 860.1. The backyard slopes down to the east, away from the retaining wall, so there may be water present when the wetland is at normal elevation.

There is an outlet pipe under TH 47, with an outlet structure that controls the water elevation in the wetland. The lowest opening of the outlet structure was approximately 2 1/2 feet above the normal water elevation of the wetland at the time the elevated water concern was reported last summer. This structure was quickly modified to lower the outflow elevation, in an attempt to alleviate the high water concern, and the invert from the wetland is now approximately 10 inches above the normal water elevation. For this project staff is proposing to remove the outlet control structure and 8 feet of pipe. We then propose to remove vegetation around this location, that may be impeding flow, and reinstall another section of pipe to create a flared end section outlet with an invert elevation of approximately 860 (the normal water level of the wetland).

DIVISION A CHANGE ORDER

Additionally, due to the significant rainfall that has occurred throughout the month of May, water levels are elevated in all locations that some improvements are planned for. Under Division A of the Stormwater Improvement Project, a couple of modifications are being proposed. A culvert was initially proposed to be installed under 175th Avenue NW (Old County Road 63) to alleviate flooding concerns on the roadway that occur from time to time; there is currently no culvert in place that allows water to flow south to north. After reviewing the downstream area following the recent extreme rainfalls it was determined that work needs to be performed on the downstream brooks and ditches to adequately allow for this water to eventually reach Trott Brook. Therefore, this portion of the overall project is now proposed to be eliminated (deduct) from Division A, and staff will begin meeting with property owners in the area to review this drainage concern and seek approval to perform a ditch cleaning project. Once the downstream drainage has been resolved then we can revisit this culvert installation in the future. Attached to this case is a location map showing this drainage area.

The elevated water levels in County Ditch 66 are causing the culverts to flow full through the area near Wolfram Street. Division A of the contract contemplated an 18" bypass culvert to be installed while the existing failing 48" culvert is being replaced. Due to the high water levels and extreme flows a larger bypass system is needed to maintain the integrity of the channel and roadway during installation of the new culvert. Therefore a change order (addition) is being requesting for this additional work. Attached to this case is the requested change order for the above described work, which results in a net deduct change order for Division A of the 2012 Storm Sewer Improvement Project (IP #12-24) in the amount of \$119.

Funding Source:

The funding source for these improvements is proposed to be the Stormwater Utility Fund. Staff has received quotes from two existing contractors, so as to include this work as a change order under a contract for City Improvement Project 12-24; 2012 Storm Sewer Improvements. The engineers estimate for the two projects is \$21,000, and the results of the quotes are as follows:

Dryden Excavating (Division A contractor)	\$16,610.00
Julian Johnson (Division B contractor)	\$16,460.00

Staff Recommendation:

Staff recommends approving a change order for City Improvement Project 12-24 - Division B, with Julian M. Johnson Construction in the amount of \$16,460.00

Committee Action:

Motion to approve a change order for City Improvement Project 12-24 - Division B, with Julian M. Johnson Construction in the amount of \$16,460.00

Attachments

Hakanson Anderson Report

148th Project Location Map

Ramsey Commons 2nd Addition Plat

148th Proposed Corrections

149th Project Location Map

Ramsey Meadows 2nd Additon Plat

149th Resident Petition

149th Petition Map

175th Avenue Drainage

Division A Change Order Request

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Tim Himmer	06/06/2012 12:40 PM
Kurt Ulrich	Tim Himmer	06/06/2012 12:43 PM
Len Linton (Originator)	Len Linton	06/06/2012 12:46 PM
Tim Himmer	Tim Himmer	06/06/2012 03:29 PM
Kurt Ulrich	Kurt Ulrich	06/07/2012 02:39 PM

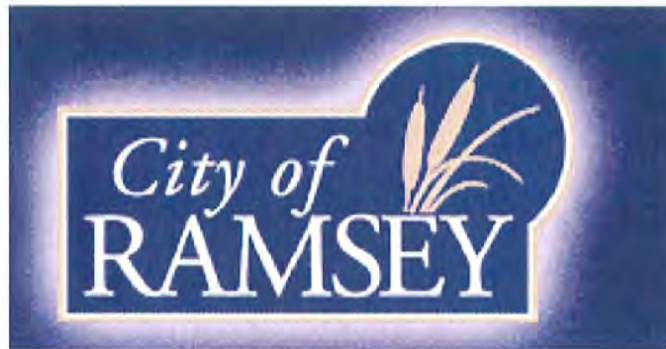
Form Started By: Len Linton

Final Approval Date: 06/07/2012

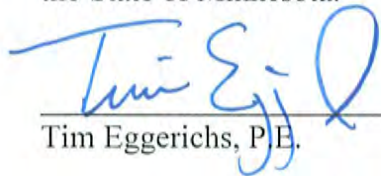
INVESTIGATION OF 2011 FLOODING CONCERNS

FOR THE

City of Ramsey
Anoka County, Minnesota



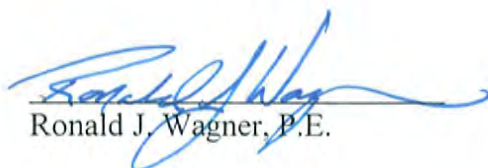
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.



Tim Eggerichs, P.E.

43362
License No.

February 14, 2012
Date



Ronald J. Wagner, P.E.

26052
License No.

February 14, 2012
Date

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SECTION 3	Rum River Hills Golf Club
SECTION 4	163 rd Lane
SECTION 5	156 th Lane
SECTION 6	Sodium Street

SUMMARIES AND RECOMMENDATIONS

148th Lane

There is an existing low area on the south side of 148th Lane. During the wet spring of 2011, stormwater would pond for extended periods in this low area. A 15" outlet exists approximately 2.5 feet above the bottom of the low area that drains the area to the east. The stormwater below the outlet pipe infiltrates into the soil. A drainage easement exists over this low area.

Different sized outlet pipes at different elevations were analyzed. These different sized outlets did not have a significant impact on the high water level in the area. Since the low area is within an existing drainage easement, it is our recommendation that nothing be changed in the area.

149th Lane

There is an existing low area on the east side of Lot 3, Block 1 of Ramsey Meadows 4th Addition (5410 149th Lane NW). This low area is connected to the large DNR Wetland to the east with a drain tile. During the wet spring of 2011, stormwater would back up through this drain tile and flooded the low area. It appears that this low area may have been constructed as part of a wetland mitigation plan and may be controlled by Wetland Conservation Act rules. A drainage easement exists over this low area.

The outlet from the DNR Wetland, which drains east under Trunk Highway 47 (TH 47), was analyzed. The current outlet has a weir structure that is approximately 0.8 feet above the invert of the 15" pipe that crosses TH 47. Additional culverts under TH 47 were analyzed, but the additional outlets did not have a significant impact on the high water levels in the DNR Wetland. Removing the weir structure was also analyzed.

It is our recommendation that the weir structure be removed from the outlet. This will not have a significant impact on the high water level of the wetland; however, the wetland will drain down to an elevation near the elevation of the bottom of the low area at 5410 149th Lane. The estimated cost to remove the weir structure is \$1,265.

Rum River Hills Golf Club

Rum River Hills Golf Club has been experiencing flooding issues throughout the golf course. The large ponds near the clubhouse drain to the east through an existing outlet structure and 12" pipe. This stormwater then drains over an existing steel weir structure and over a rock dam prior to discharging to the Rum River.

Area 1

The ponds near the clubhouse have been flooding and the outlet does not seem to drain the ponds effectively. The existing 12" outlet from the large ponds near the clubhouse has several sags in it and several joints have been compromised.

It is our recommendation to replace this pipe and install a new outlet structure with removable planks that will give the golf course more flexibility in controlling the water

elevation in the ponds. This includes an 18" HDPE outlet and precast concrete outlet structure with a removable grate for access to the planks. It is proposed to leave the overflow at the same elevation as existing, which will have minimal impacts on the elevation of the standing water and the high water levels of the ponds. The estimated cost to install this new outlet is \$29,853.

Area 2

Based on the high water levels in the ponds near the clubhouse, it appears flooding of the cart paths near Hole #1 may be an issue during large storm events. The existing culverts consist of a 12" CMP and 15" HDPE.

To reduce the frequency of the flooding, we recommend installing 24" diameter culverts under the two existing cart paths. The paths would continue to be inundated during storm events greater than 3.5", but would not be inundated during storm events less than 3.5". The estimated cost to replace the culverts is \$6,642. If the golf course does not feel that the cart path flooding is a concern, replacing the culverts is not a necessity.

Area 3

The soil in the fairway of Hole #15 has become saturated. There is an existing rock dam southeast of this fairway and it appears that water being contained by the rock dam may be infiltrating into the soil and saturating the fairway.

We recommend lining the creek upstream of the rock dam with an impermeable liner and replacing and/or installing new drain tile in the fairway of Hole #15. Lining the creek will eliminate the infiltration into the soil and the drain tile will keep the soil from becoming saturated. The estimated cost for these improvements is \$13,530.

Area 4

Holes #3 and #17 have had flooding issues. The ponds and swales near the two holes are drained through three 15" CMP culverts.

Different sized culverts at different elevations were analyzed. These different sized culverts did not have a significant impact on the high water levels in the area. Without lowering the entire swale and creating more storage, it does not appear that replacing the culverts would have a significant impact. It is our recommendation that the three culverts not be replaced.

163rd Lane

There is an existing, landlocked low area south of 163rd Lane and east of Wolfram Street. During a majority of the year, stormwater runoff infiltrates into the soil. However, during early spring when the ground is frozen and during periods of heavy rainfall, water levels have risen to levels that cause flooding of adjacent properties.

Different sized outlet pipes were analyzed to drain the low area. Installing an outlet will have a significant impact on the high water levels in the area and, most importantly, the length of inundation will be greatly reduced. We recommend directionally drilling an

18" HDPE pipe south to County Ditch #3 with an invert elevation of 872.0. Installing the pipe at an elevation of 872.0 will continue to allow 3" storm events to infiltrate into the soil. The estimated cost to install this outlet is \$80,795.

156th Lane

During large storm events, the wetland in Woodland Green Park ponds water in the backyard of 5220 156th Lane. The area where water ponds was platted with a 75-foot drainage and utility easement, however, this easement has been vacated. The wetland discharges east through an existing storm sewer system to a low area and then north through another storm sewer system to the Rum River.

Different sized ponds and outlet configurations were analyzed. One alternative to reduce the high water level of the wetland in Woodland Green Park included constructing a new outlet pipe from the wetland to the low area to the east. This alternative would also require constructing a new outlet at a lower elevation from the low area directly to the Rum River. This would require the approval of the Minnesota Department of Natural Resources and does not appear viable at this time.

We recommend filling the backyard of 5220 156th Lane to an elevation equal to the 100-year high water level. Stormwater would then be contained within Woodland Green Park and would not impact this homeowner. Filling the backyard would require grading in the Woodland Green Park site to create storage to compensate for the lost volume. The estimated cost to complete the grading is \$21,175.

Sodium Street

The house at 16756 Sodium Street has been experiencing water in the basement. One reason for having water in the basement could be caused by the road ditch in front of the house filling and then overflowing toward the house. The water then seeps along the basement wall and eventually into the basement. Another reason for water in the basement could be caused by a high groundwater elevation in the area.

We recommend installing a culvert under Sodium Street, regrading the west ditch of Sodium Street, constructing a berm to keep the water in the ditch, and replacing the existing driveway culvert. The ditch would be graded to drain south and a culvert would be installed near the south property line to drain the water to the west. The estimated cost to complete this project is \$12,225.

Section 1
148th Lane

148th Lane

Description

As shown on Exhibit 1, a low area exists south of 148th Lane. A 15" outlet pipe drains the low area north and then east to DNR Wetland 658W. The invert of the outlet pipe is at elevation 862.3 and the bottom of the low area is at elevation 859.8. Water in the low area has to rise 2.5 feet prior to discharging. The water below the outlet infiltrates into the soil, which may take days depending on the condition of the soil.

The existing 100-year high water level (HWL) for the low area is 865.5. As shown on Exhibit 2, a drainage easement exists in the rear of Lots 2 through 5, Block 2 of Ramsey Commons 2nd Addition.

Alternatives

The following alternatives address the water elevation in the existing low area.

Alternative 1

In this alternative, a new outlet pipe would be installed between Lot 1 of Ramsey Commons 2nd Addition and Lot 2 of Sunny Ponds, as shown on Exhibit 3. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.6. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.4	\$16,264
18"	865.1	\$31,566 *
24"	864.3	\$45,381 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

Tables 1 through 3 include the individual costs for this alternative.

Alternative 2

In this alternative, a new outlet pipe would be installed between Lots 2 and 3 of Sunny Ponds, as shown on Exhibit 4. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.4. This alternative would require additional grading and the acquisition of a permanent easement on the property south of Ramsey Commons 2nd Addition and west of Sunny Ponds. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.3	\$13,098
18"	865.0	\$18,846 *
24"	864.3	\$32,543 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

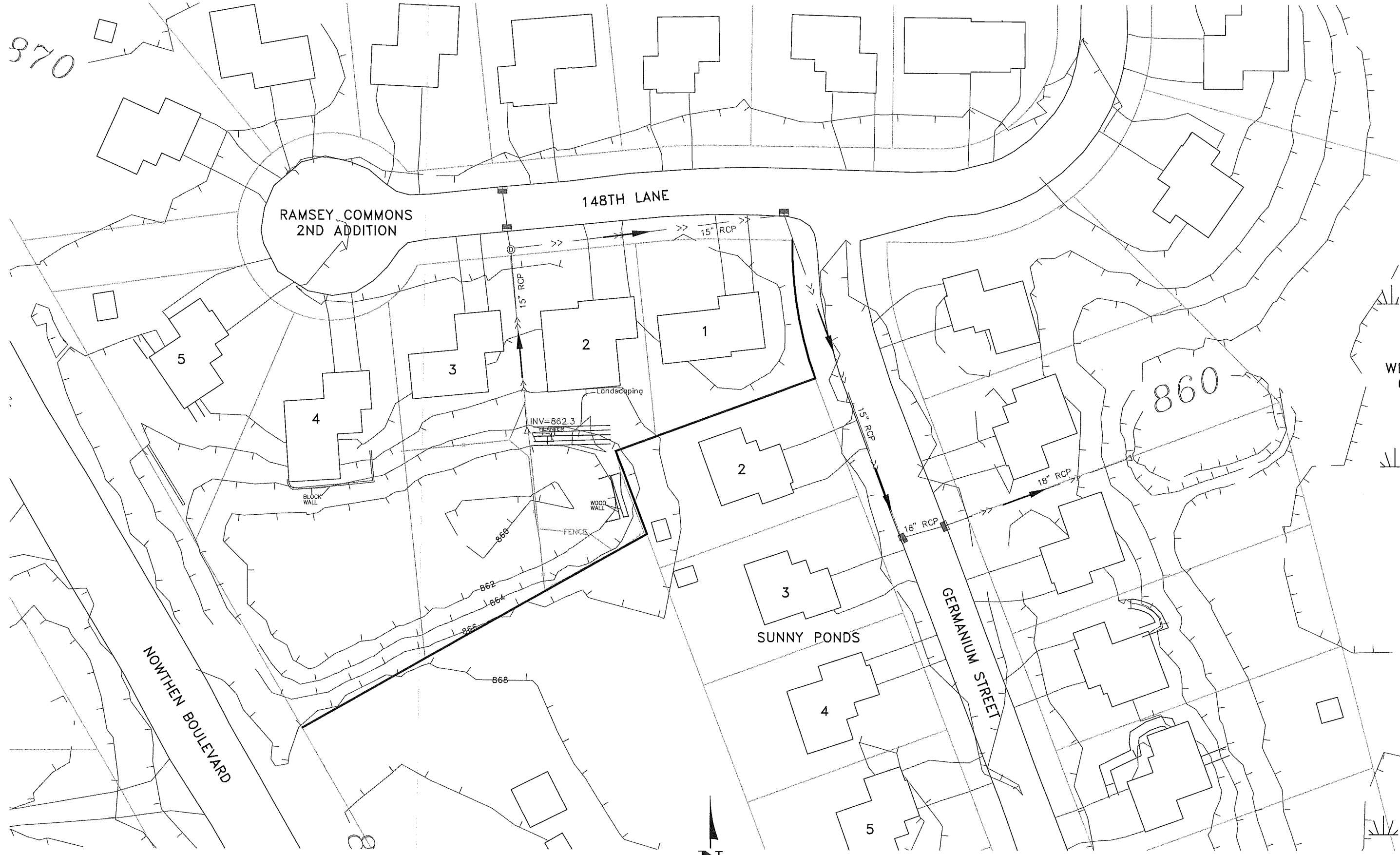
Tables 4 through 6 include the individual costs for this alternative.

Alternative 3

In this alternative, the bottom of the low area would be filled to the same elevation as the outlet pipe invert. The pond would then drain dry and would not be sitting with water until it infiltrated. By filling the bottom of the low area, the resultant 100-year HWL will be 866.2, 0.7 feet higher than the existing HWL. This higher HWL would end up outside the existing drainage and utility easement, creating the need for additional drainage easement.

The estimated cost to fill the low area is \$15,321. Table 7 includes the individual costs for this alternative.

Further research is required to determine if this low area was designed to treat a water quality volume. The volume required would dictate if Alternatives 1 and 2 are viable options. Alternative 3 would not be viable, because the water quality volume is being eliminated in this alternative.



870

RAMSEY COMMONS
2ND ADDITION

148TH LANE

DNR
WETLAND
658W

860

SUNNY PONDS

GERMANIUM STREET

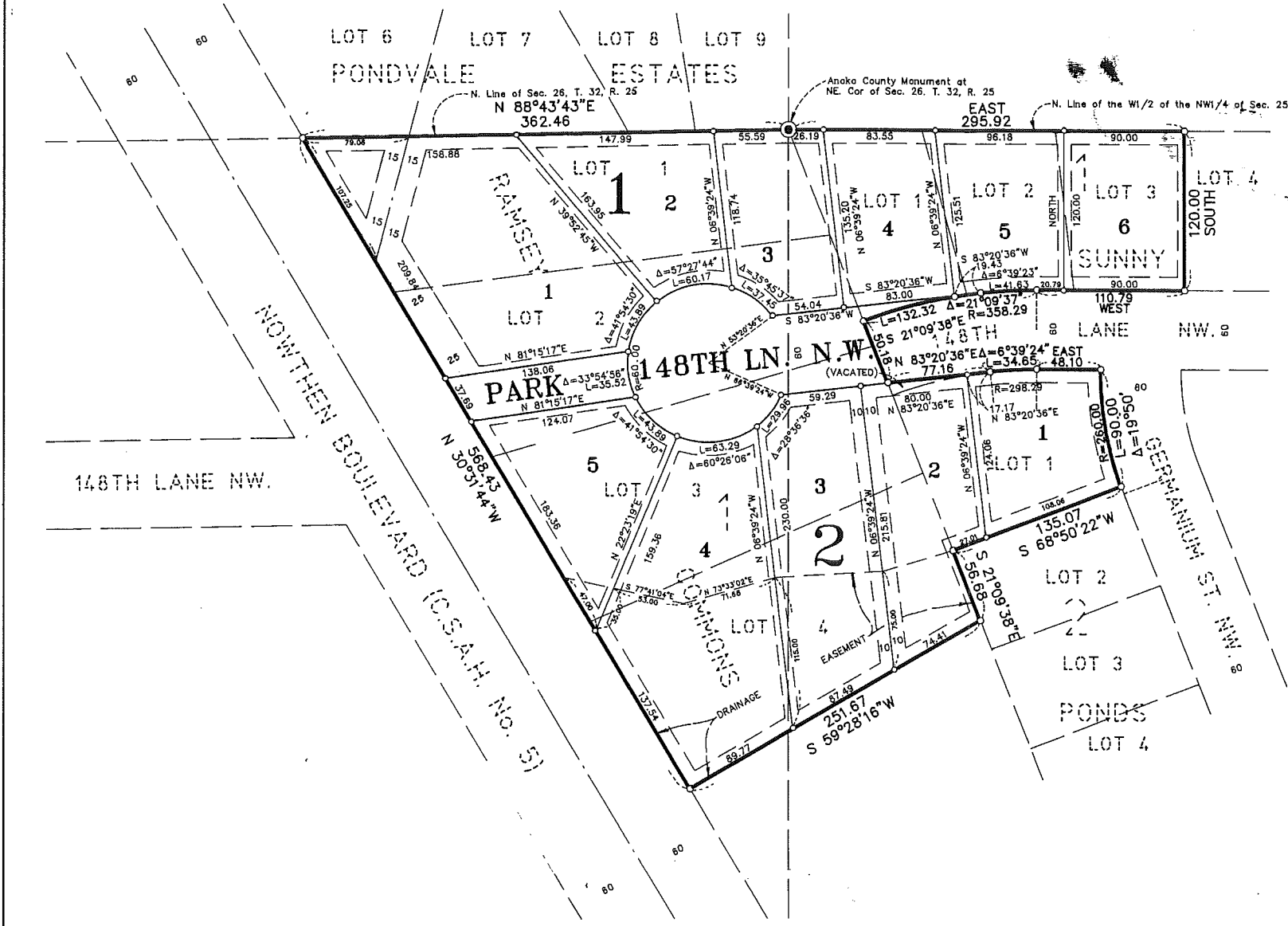
NORTHEN BOULEVARD



EXHIBIT 1
148TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY COMMONS 2ND ADDITION

CITY OF RAMSEY
COUNTY OF ANOKA



KNOW ALL PERSONS BY THESE PRESENTS: That North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Dolores S. Fleischer, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2 and 3, Block 1, and Lot 1, Block 2, all in SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota,
And that part of vacated 148th Lane N.W., as dedicated in the plat of SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota, lying north of the north line of Lot 1, Block 2, in said SUNNY PONDS and lying south of the following described line:

Commencing at the northeast corner of said Lot 1, Block 2; thence on an assumed bearing of West, along the north line of said Lot 1, Block 2, a distance of 48.10 feet; thence westerly continuing along said north line and along a tangential curve, concave to the south having a radius of 298.29 feet and a central angle of 6 degrees 39 minutes 24 seconds, a distance of 34.65 feet to the point of beginning of the line to be described; thence South 83 degrees 20 minutes 36 seconds West, a distance of 77.16 feet to the intersection with the northerly extension of the west line of said Lot 1, Block 2, and said line there terminating.

AND that North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Delano Skeim, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2, 3 and 4, Block 1, RAMSEY COMMONS, according to the recorded plat thereof, Anoka County, Minnesota.
Have caused the same to be surveyed and platted as RAMSEY COMMONS 2ND ADDITION and do hereby donate and dedicate to the public for public use forever the lane/as shown on the plat. Also dedicating the drainage and/or utility easements as shown on the plat. Also dedicating to the County of Anoka the right of access onto County State Aid Highway No. 5 from Lot 1, Block 1 and from Lots 4 and 5, Block 2. In witness whereof said North Suburban Development, Inc. has caused these presents to be signed by its proper officer this 26th day of MAY, 1993. Also in witness whereof said Dolores S. Fleischer has hereunto set her hand this 26th day of MAY, 1993. Also in witness whereof said Delano Skeim has hereunto set his hand this 26th day of MAY, 1993.

NORTH SUBURBAN DEVELOPMENT, INC.
[Signature]
A. Henkveld, as President

SIGNED:
[Signature]
Dolores S. Fleischer

SIGNED:
[Signature]
Delano Skeim

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 26th day of MAY, 1993, by J. A. COUNTY OF ANOKA) Henkveld, President of North Suburban Development, Inc., a Minnesota corporation, on behalf of the corporation.

SHIRLEY D. CHENOWETH
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 26th day of MAY, 1993, by Dolores COUNTY OF ANOKA) S. Fleischer, single.

SHIRLEY D. CHENOWETH
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 21st day of May, 1993, by Delano COUNTY OF ANOKA) Skeim, single.

MARGARET A. McINERNEY
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires FEB. 22, 1998

[Signature]
Notary Public, Anoka County, Minnesota
My Commission expires 2/22/96

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY COMMONS 2ND ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wet lands or public highways to be designated on said plat other than as shown thereon.

[Signature]
Jeffrey N. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyors certificate was acknowledged before me a Notary Public, this 19th day of May, 1993, COUNTY OF ANOKA) by Jeffrey N. Caine, Land Surveyor.

MOLLY W. CAINE
Notary Public-Minnesota
Anoka County
My Commission Expires 6-13-96

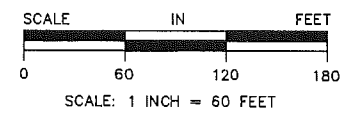
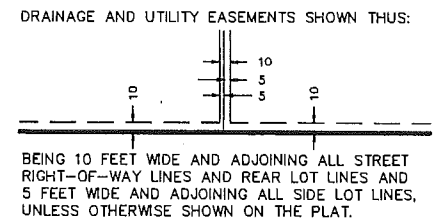
[Signature]
Notary Public, Anoka County, Minnesota
My Commission expires 6-13-96

CITY OF RAMSEY
We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY COMMONS 2ND ADDITION at a regular meeting held this 11th day of May, 1993. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 509.03, Subd. 2.
By *[Signature]* Mayor By *[Signature]* Clerk

Checked and approved this 29th day of June, 1993

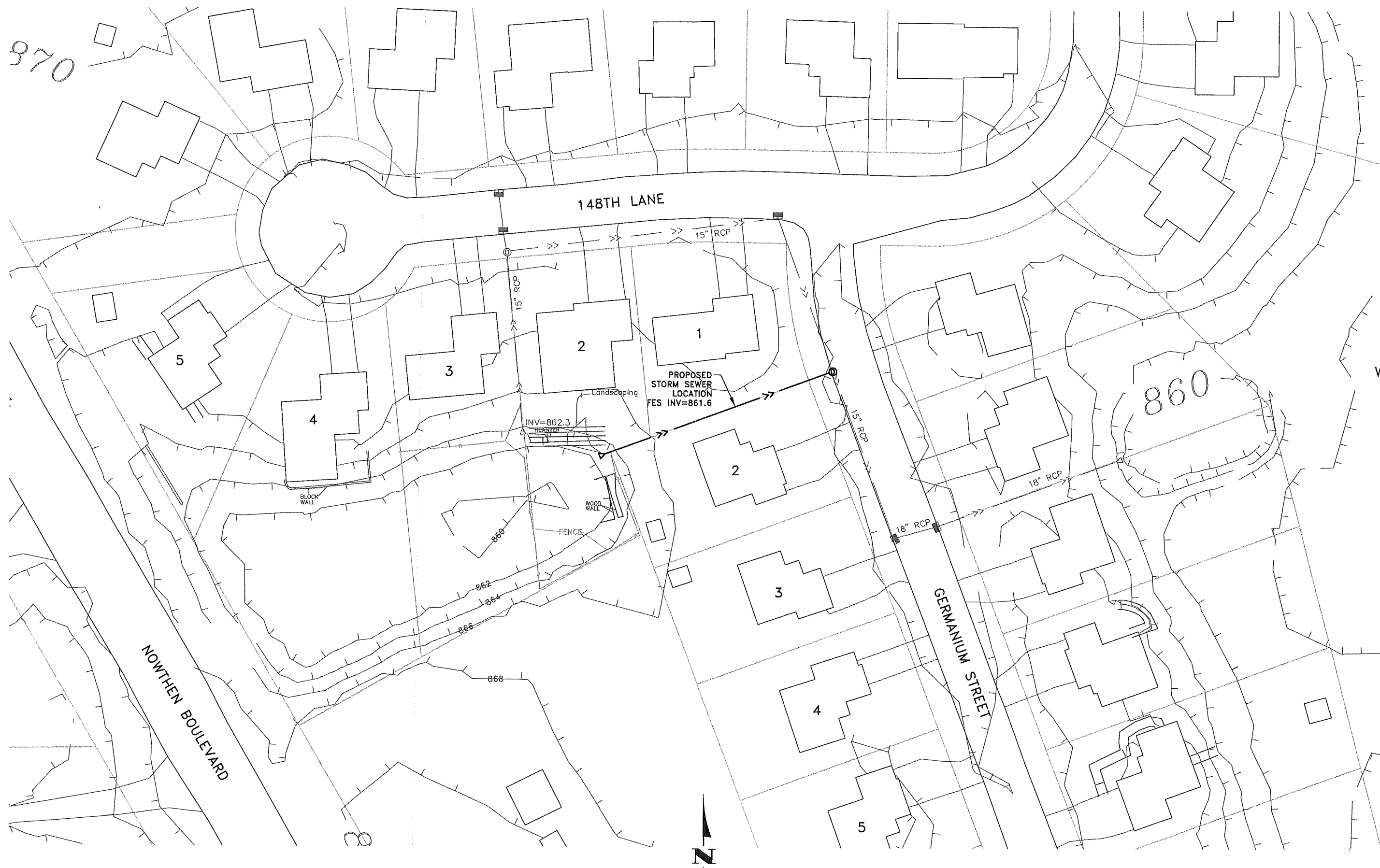
By *[Signature]*
Anoka County Surveyor

EXHIBIT 2.
FINAL PLAT-RAMSEY COMMONS 2ND ADD.
CITY OF RAMSEY, MINNESOTA



NOTE: DENOTES 1/2 INCH IRON PIPE SET.
DENOTES ANOKA COUNTY MONUMENT.
FOR THE PURPOSES OF THIS PLAT, THE NORTH LINE OF THE W1/2 OF THE NW1/4 OF SEC. 25, T. 32, R. 25 IS ASSUMED TO HAVE A BEARING OF EAST.

CAINE & ASSOCIATES
LAND SURVEYORS, INC.



870

860

DNR
WETLAND
658W

NORTHEN BOULEVARD

GERMANIUM STREET

148TH LANE

PROPOSED
STORM SEWER
LOCATION
FES INV=861.6

INV=862.3

Landscaping

FENCES

WOOD WALL

BLOCK WALL



EXHIBIT 3
148TH LANE ALTERNATIVE 1 STORM SEWER
CITY OF RAMSEY, MINNESOTA



DNR
WETLAND
658W

NORTHEN BOULEVARD

GERMANIUM STREET

148TH LANE

860

PROPOSED
PERMANENT
DRAINAGE
EASEMENT

PROPOSED
STORM SEWER
LOCATION
FES INV=861.4

INV=862.3

FENCE

WOOD
WALL

Landscaping

868

866

864

862

860



EXHIBIT 4
148TH LANE ALTERNATIVE 2 STORM SEWER
CITY OF RAMSEY, MINNESOTA

TABLE 1
148TH LANE
ALTERNATIVE 1A - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	15" RC PIPE APRON	EACH	\$300.00	1	\$300
12	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
17	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$14,786
Contingency (10%)	\$1,479
Total Estimated Construction Cost	<u>\$16,264</u>

TABLE 2
148TH LANE
ALTERNATIVE 1B - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	233	\$1,165
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	135	\$1,350
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	150	\$750
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	155	\$465
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	150	\$1,125
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	150	\$4,200
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	18" RC PIPE APRON	EACH	\$350.00	1	\$350
12	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
14	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	267	\$6,141
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	135	\$2,700
17	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$28,696
Contingency (10%)	\$2,870
Total Estimated Construction Cost	<u>\$31,566</u>

TABLE 3
148TH LANE
ALTERNATIVE 1C - 24" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,300.00	1	\$1,300
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$4.00	388	\$1,552
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	155	\$775
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	195	\$975
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	175	\$525
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	195	\$1,463
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	195	\$5,460
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	24" RC PIPE APRON	EACH	\$450.00	2	\$900
12	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	414	\$12,006
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	155	\$3,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$41,256
Contingency (10%)	\$4,126
Total Estimated Construction Cost	<u>\$45,381</u>

**TABLE 4
148TH LANE
ALTERNATIVE 2A - 15" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	15" RC PIPE APRON	EACH	\$300.00	1	\$300
13	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
18	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$11,186
Contingency (10%)	\$1,119
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$13,098</u>

TABLE 5
148TH LANE
ALTERNATIVE 2B - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	153	\$765
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	55	\$550
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	61	\$305
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	75	\$225
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	61	\$458
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	61	\$1,708
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	18" RC PIPE APRON	EACH	\$350.00	1	\$350
13	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
15	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	187	\$4,301
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	55	\$1,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$16,412
Contingency (10%)	\$1,641
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$18,846</u>

**TABLE 6
148TH LANE
ALTERNATIVE 2C - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$900.00	1	\$900
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$4.00	306	\$1,224
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	75	\$375
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	106	\$530
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	95	\$285
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	106	\$795
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	106	\$2,968
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	24" RC PIPE APRON	EACH	\$450.00	2	\$900
13	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	334	\$9,686
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
18	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	75	\$1,500
19	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
20	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
21	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$28,863
Contingency (10%)	\$2,886
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$32,543</u>

**TABLE 7
148TH LANE
ALTERNATIVE 3**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	10	\$1,000
3	GRUBBING	TREE	\$100.00	10	\$1,000
4	COMMON EXCAVATION	CU YD	\$5.00	270	\$1,350
5	GRANULAR BORROW	CU YD	\$8.00	1166	\$9,328
6	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$13,928
Contingency (10%)	\$1,393
Total Estimated Construction Cost	<u>\$15,321</u>

Section 2
149th Lane

149th Lane

Description

As shown on Exhibit 5, a low area exists on Lot 3, Block 1 of Ramsey Meadows 4th Addition. There is a drain tile that drains the low area to DNR Wetland 658W. During large storm events, DNR Wetland 658W backs up through the drain tile and into the low area. DNR Wetland 658W drains east under Trunk Highway 47 (TH 47) through a small weir structure and 15" pipe to DNR Wetland 659W. The weir structure is at an elevation of 860.9 and drains to the 15" pipe at an elevation of 860.0.

The current outlet elevation is approximately two feet above the bottom of Wetland 658W. The existing 100-year high water level (HWL) for DNR Wetland 658W is 862.1 and for DNR Wetland 659W is 860.4. As shown on Exhibit 6, there is an existing drainage and utility easement over a majority of Block 1 of Ramsey Meadows 4th Addition including the low area in question.

Alternatives

The following alternatives address the water elevation in DNR Wetland 658W.

Alternative 1

Alternative 1 will include removing the existing weir structure and leaving only the 15" culvert as the outlet from DNR Wetland 658W. By removing the weir structure, the wetland will begin to discharge at an elevation of 860.0 as opposed to 860.8.

Removing the weir structure, resulting in a lower normal water level in the wetland, will result in a 100-year HWL for DNR Wetland 658W of 862.0. The 100-year HWL for DNR Wetland 659W did not change. The estimated cost to remove the weir structure is \$1,265. Table 8 includes the individual costs for this alternative.

It is our understanding that any work proposed below an elevation of 860 will need the approval of the Minnesota Department of Natural Resources (DNR). This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. It appears the weir structure is part of the TH 47 storm sewer system. Removing the weir structure may require Mn/DOT's approval.

Alternative 2

Alternative 2 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at the same elevation as the existing outlet, 860.0. It was assumed the additional pipe would have to be jacked under TH 47.

Three different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + 15"	861.8	861.2	\$15,290
Existing + 18"	861.7	861.3	\$18,755
Existing + 24"	861.6	861.4	\$29,040

Tables 9 through 11 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. Removing the weir structure and constructing a culvert under TH 47 will require Mn/DOT's approval.

Alternative 3

Alternative 3 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at an elevation of 859.0, one foot lower than the existing outlet. It was assumed the additional pipe would have to be jacked under TH 47.

Two different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + lower 18"	861.6	861.3	\$18,755
Existing + lower 24"	861.6	861.5	\$29,040

Tables 12 and 13 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative has work proposed below an elevation of 860 and will need the approval of the DNR. Removing the weir structure and constructing a culvert under Trunk Highway 47 will require Mn/DOT's approval.

Alternative 4

Alternative 4 will include filling the low area on Lot 3, Block 1 of Ramsey Meadows 4th Addition. Filling the low area will reduce the frequency of the backyard flooding. During large storm events, the area may be inundated by water, but will likely be less frequently and for a shorter duration than under existing conditions.

The estimated cost to fill the low area is \$4,681. Table 14 includes the individual costs for this alternative.

As shown on Exhibits 7 and 8, this low area appears to have been designed as wetland mitigation area. Further research may be necessary to determine if this area is protected by the Wetland Conservation Act (WCA). If the area is protected by the WCA, filling this low area will not be a viable alternative.

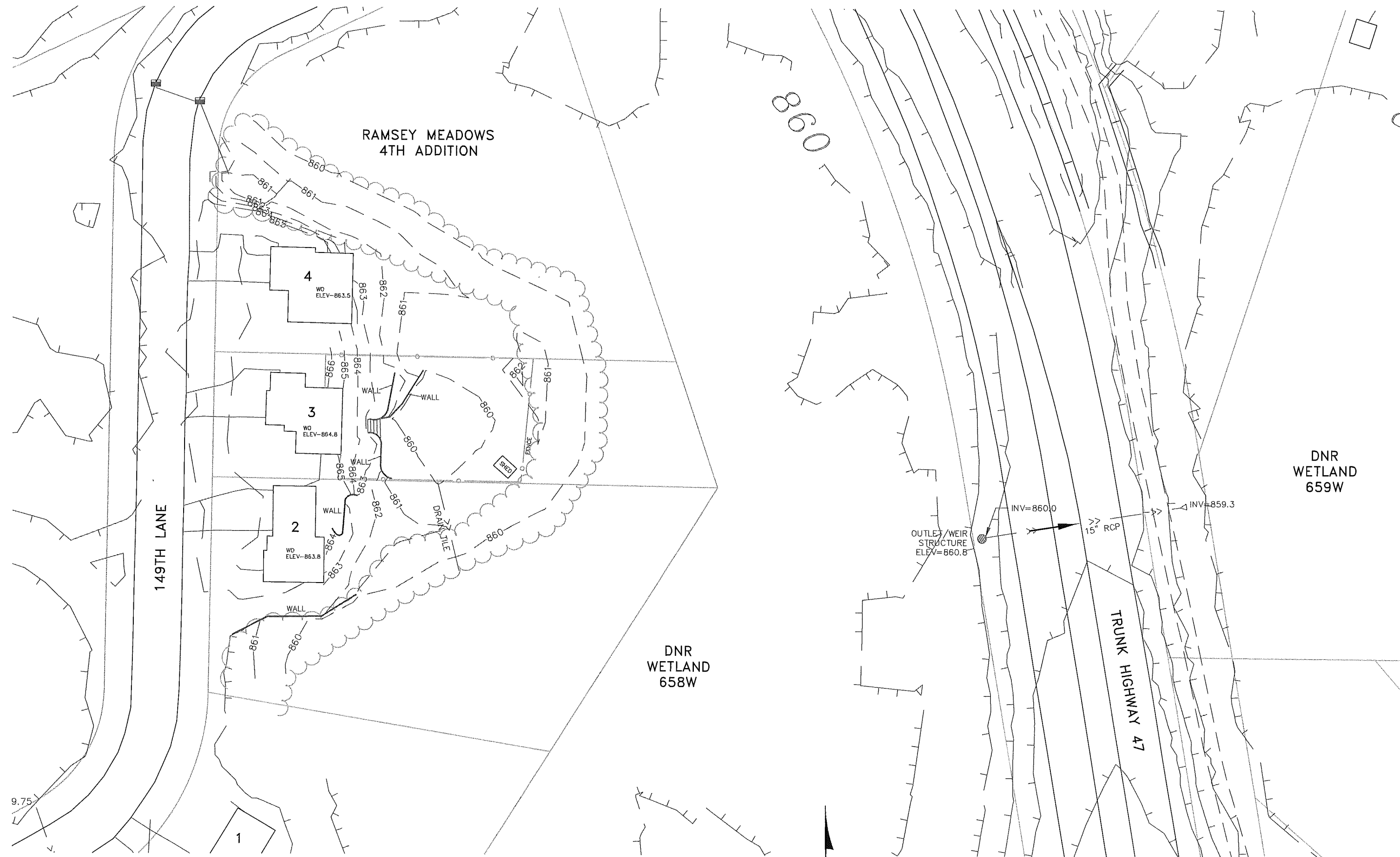


EXHIBIT 5
149TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY MEADOWS 4TH ADDITION

CITY OF RAMSEY COUNTY OF ANOKA

pg 25

KNOW ALL PERSONS BY THESE PRESENTS: That J. A. Menkveld & Associates, Inc., a Minnesota corporation, owner and proprietor and Builders Mortgage Corporation, a Minnesota corporation, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

That part of the Southwest Quarter of Section 24, Township 32, Range 25, Anoka County, Minnesota, described as follows:

Beginning at the northeast corner of Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota; thence South 89 degrees 07 minutes 51 seconds East, assumed bearing, parallel with the south line of said Southwest Quarter, a distance of 208.50 feet to the center line of State Trunk Highway No. 47, per the plat of AMBER RIDGE, according to the recorded plat thereof, Anoka County, Minnesota; thence northerly along said center line and along the center line of said State Trunk Highway No. 47, per the plat of WILLOW RIDGE, according to the recorded plat thereof, Anoka County, Minnesota, a distance of 789.96 feet to the intersection with the northeasterly extension of the following described line:

Beginning at a point on the center line of State Trunk Highway No. 47, per the plat of GORHAM'S ADDITION, according to the recorded plat thereof, Anoka County, Minnesota, said point being distant 93.00 feet southeasterly of the northeasterly extension of the southeasterly line of Block 3, said GORHAM'S ADDITION, as measured along said center line; thence South 64 degrees 18 minutes West, parallel with the southeasterly line of Block 3, said GORHAM'S ADDITION, a distance of 376.78 feet, and said line there terminating;

thence South 64 degrees 18 minutes 00 seconds West, along said last described line, a distance of 376.83 feet to the point of termination of said line; thence southwesterly along a tangential curve concave to the southeast, having a radius of 103.25 feet and a central angle of 63 degrees 14 minutes 20 seconds, a distance of 113.96 feet; thence South 1 degree 03 minutes 40 seconds West, tangent to said curve, a distance of 345.42 feet; thence southwesterly along a tangential curve concave to northwest, having a radius of 115.75 feet and a central angle of 60 degrees 08 minutes 15 seconds, a distance of 121.49 feet; thence South 61 degrees 11 minutes 55 seconds West, tangent to said curve, a distance of 53.09 feet to the intersection with the westerly extension of the north line of said Outlot A, RAMSEY MEADOWS 3RD ADDITION; thence South 87 degrees 06 minutes 09 seconds East, along said north line and its westerly extension, a distance of 517.62 feet to the point of beginning.

AND

Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota.

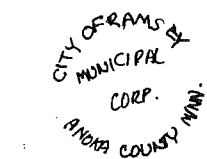
Have caused the same to be surveyed and platted as RAMSEY MEADOWS 4TH ADDITION and do hereby dedicate to the public for public use forever the boulevard, lane and drainage and utility easements as shown on the plat. In witness whereof said J. A. Menkveld & Associates, Inc., a Minnesota corporation, has caused these presents to be signed by its proper officer this 7th day of OCT, 1996. Also in witness whereof said Builders Mortgage Corporation has caused these presents to be signed by its proper officer this 7th day of OCT, 1996.

SIGNED:

J. A. MENKVELD & ASSOCIATES, INC.:

J. A. Menkveld
J. A. Menkveld, President

1275655
OFFICE OF COUNTY RECORDS
STATE OF MINNESOTA, COUNTY OF ANOKA
I hereby certify that the within instrument was filed in this office for record on the 7th day of MAY, 1997.
Book AM, and was duly recorded in book 54, page 65.
Edward M. Truka
County Recorder
By *KHG*
Deputy



CAINE & ASSOCIATES
LAND SURVEYORS, INC.

BUILDERS MORTGAGE CORPORATION:

Ronald Stratton
Ronald Stratton, as President

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by J. A. Menkveld, President of J. A. Menkveld & Associates, Inc., a Minnesota Corporation, on behalf of the Corporation.



Teresa Vinje
Notary Public, Anoka County, Minnesota
My Commission expires 1-31-00

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by Ronald Stratton, President of Builders Mortgage Corporation, a Minnesota corporation, on behalf of the corporation.

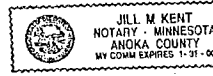


Kathleen R. Caven
Notary Public, Ramsey County, Minnesota
My Commission expires 1-31-2000

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY MEADOWS 4TH ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wetlands or public highways to be designated on said plat other than as shown thereon.

Jeffrey N. Caine
Jeffrey N. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyors certificate was acknowledged before me a Notary COUNTY OF ANOKA) Public, this 1st day of October, 1996, by Jeffrey N. Caine, Land Surveyor.



Jill M. Kent
Notary Public, Anoka County, Minnesota
My Commission expires 01-31-00

CITY OF RAMSEY

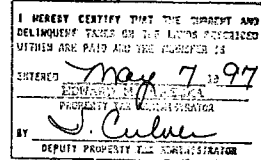
We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY MEADOWS 4TH ADDITION at a regular meeting held this 24th day of September, 1996. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 505.03, Subd. 2.

By *Ryan R. Schneider* Mayor By *Ryan R. Schneider* Clerk

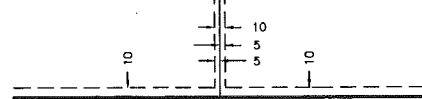
Checked and approved this 7th day of MAY, 1997.

By *Merlyn D. Anderson*
Anoka County Surveyor

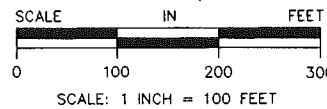
By *Larry S. Ham* deputy



DRAINAGE AND UTILITY EASEMENTS SHOWN THUS:



BEING 10 FEET WIDE AND ADJOINING ALL STREET RIGHT-OF-WAY LINES AND REAR LOT LINES AND 5 FEET WIDE AND ADJOINING ALL SIDE LOT LINES, UNLESS OTHERWISE SHOWN ON THE PLAT.



- DENOTES IRON MONUMENT FOUND.
 - DENOTES 1/2 INCH IRON PIPE SET.
 - ⊙ DENOTES ANOKA COUNTY MONUMENT.
- NOTE: FOR THE PURPOSES OF THIS PLAT, THE SOUTH LINE OF THE SW1/4 OF SEC. 24, T. 32, R. 25 IS ASSUMED TO BEAR S 89°07'51"E

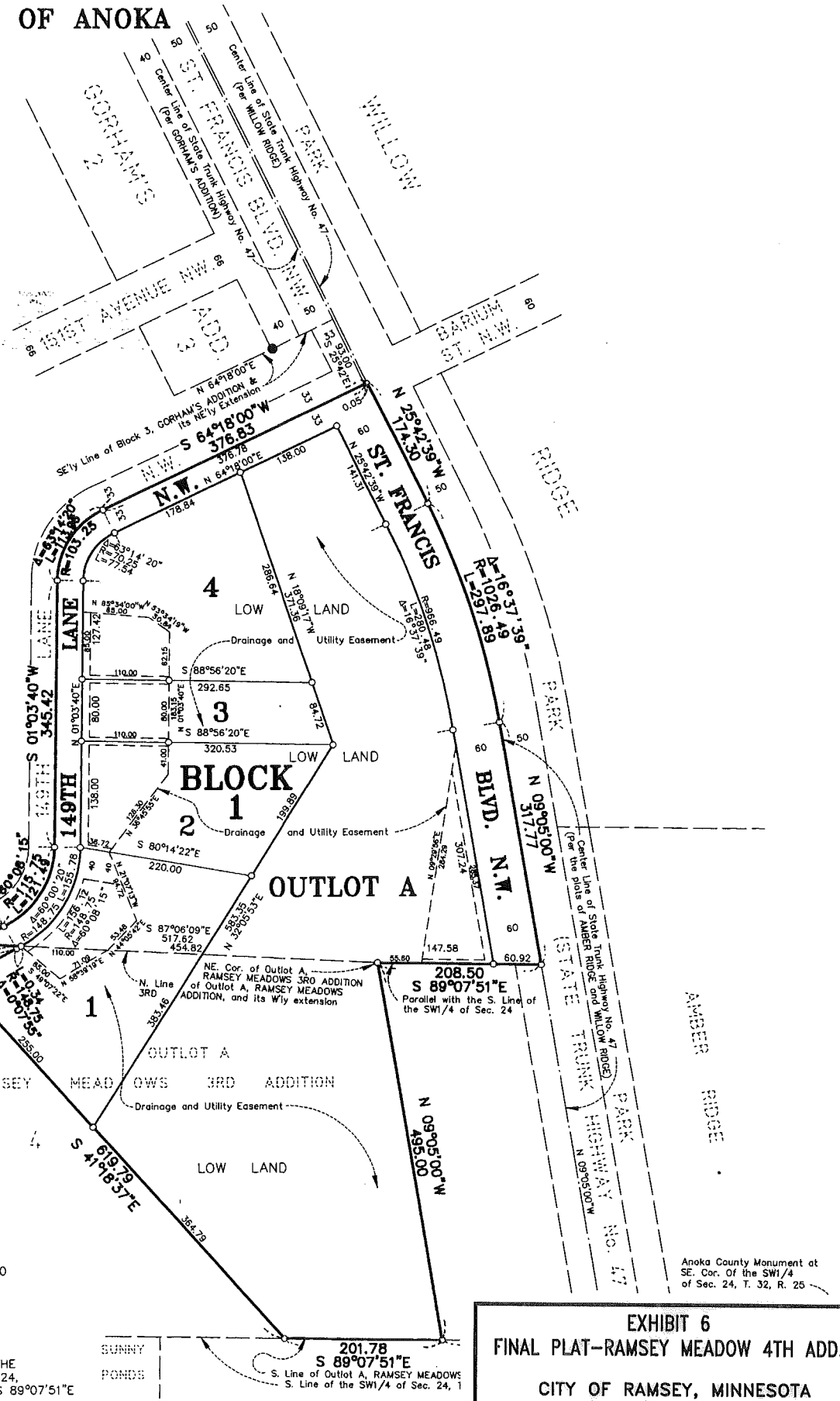
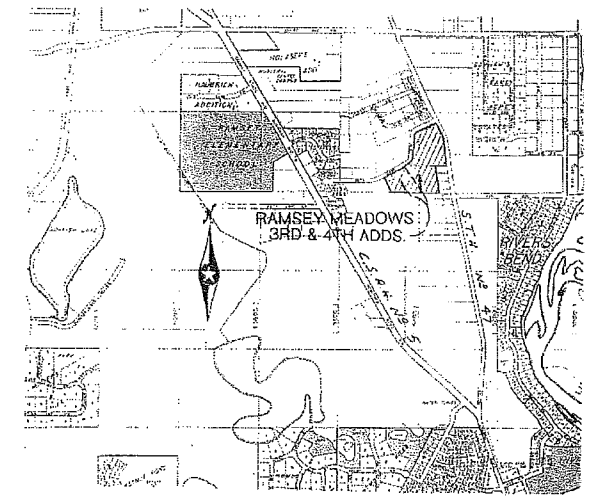
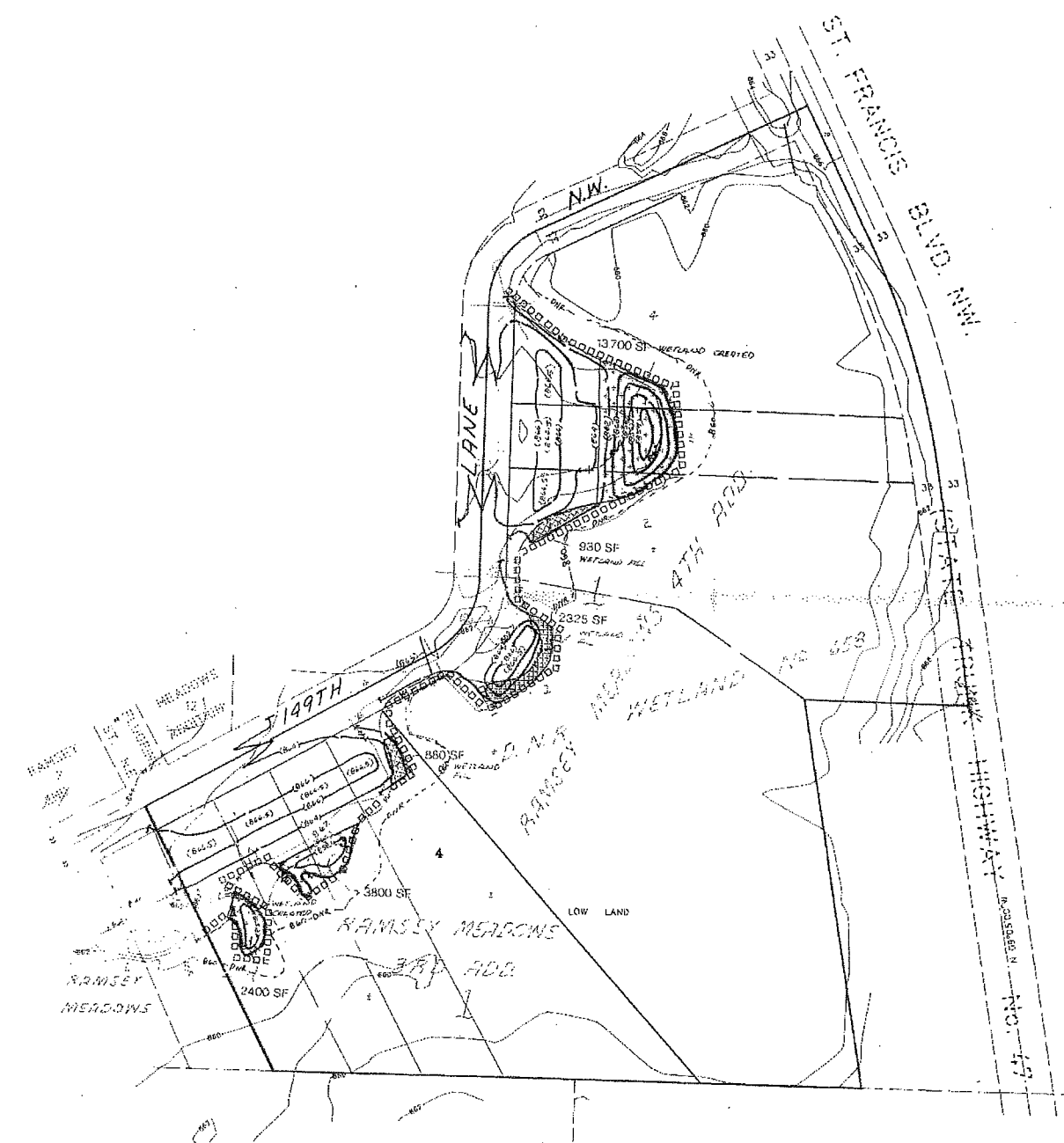


EXHIBIT 6
FINAL PLAT-RAMSEY MEADOW 4TH ADD.
CITY OF RAMSEY, MINNESOTA

30997/824500

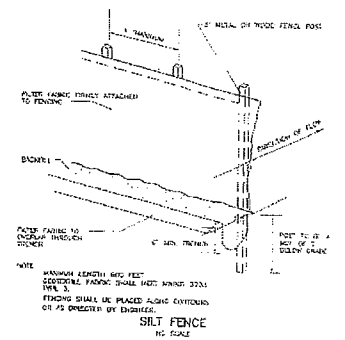
PRELIMINARY GRADING, DRAINAGE & EROSION CONTROL PLAN FOR: **RAMSEY MEADOWS 3RD & 4TH ADDITIONS**



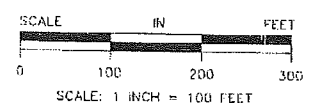
EROSION CONTROL DURING CONSTRUCTION

NOTES:

- All disturbed soils within 100 lineal feet from wetlands shall be covered with four inches (4") of topsoil and seeded with a minimum of seventy-five (75) pounds per acre of MNDOT Specification Section 3876, Mixture No. 12, with fertilizer 12-12-12 mixture applied at 600 pounds per acre with Mulch-Type 1. This work shall be constructed in accordance with MNDOT Specification Section 2575, within the following timeframe:
 Slopes steeper than 3:1 - 7 days
 Slopes 10:1 to 3:1 - 14 days
 Flatter than 10:1 - 21 days
- Excavate ponding areas before upland grading.
- The bottom of all drainage ditches shall be stabilized within 100 feet of all wetlands. Stabilization must be initiated within 24 hours of connection to wetlands and be completed within five days. All pipes connecting to drainage swales must be provided with energy dissipation structures prior to connecting to wetland.
- Sediment control structures must be in place prior to starting of grading and must be maintained until final stabilization has been established.
- Inspect and maintain after rainfall (as required). The inspector is to be
- Vehicle tracking onto unpaved surfaces must be minimized.
- All silt fence shall be removed after the site has undergone final stabilization.



	PROPOSED NON-DNR WETLAND DISTURBED	PROPOSED WETLAND CREATED
Ramsey Meadows 3rd Addition	880 S.F. ±	6,200 S.F. ±
Ramsey Meadows 4th Addition	4,135 S.F. ±	19,900 S.F. ±
TOTALS	5,015 S.F. ±	26,100 S.F. ±



● DENOTES IRON MONUMENT FOUND
 ○ DENOTES 1/2 INCH IRON PIPE SET.
 BEARINGS SHOWN ARE BASED ON ASSUMED DATUM.

I HEREBY CERTIFY THAT THIS SURVEY, PLAN OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

Jeffrey M. Caine
 DATE: Jan. 23, 2015 REG. NO. 12251

--- 000.0 --- DENOTES EXISTING SPOT ELEVATION
 (000.0) " " PROPOSED " "
 --- 000 --- " " EXISTING CONTOUR
 --- (000) --- " " PROPOSED CONTOUR

--- DNR --- DENOTES EDGE OF D.N.R. WETLAND N.D. 658.
 --- W --- W --- DENOTES EDGE OF WETLAND AS LOCATED BY JOHN C. ANDERSON OF WETLANDS DATA, INC. & SURVEYED BY CAINE & ASSOCIATES, INC.
 □□□□□□□□□□ DENOTES PROPOSED SILT FENCE

CAINE & ASSOCIATES
LAND SURVEYORS, INC.
 17720 Highway 85 N.E. - Ham, Ia.
 634-7846

EXHIBIT 7
RAMSEY MEADOWS GRADING PLAN
 CITY OF RAMSEY, MINNESOTA

**TABLE 8
149TH LANE
ALTERNATIVE 1**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
5	TURF ESTABLISHMENT	LUMP SUM	\$50.00	1	\$50

Estimated Construction Cost	\$1,150
Contingency (10%)	\$115
Total Estimated Construction Cost	<u>\$1,265</u>

TABLE 9
149TH LANE
ALTERNATIVE 2A - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	3	\$900
4	15" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$100.00	118	\$11,800
5	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
6	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$13,900
Contingency (10%)	\$1,390
Total Estimated Construction Cost	<u>\$15,290</u>

**TABLE 10
149TH LANE
ALTERNATIVE 2B - 18" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	\$1,705
Total Estimated Construction Cost	<u>\$18,755</u>

**TABLE 11
149TH LANE
ALTERNATIVE 2C - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

TABLE 12
149TH LANE
ALTERNATIVE 3A - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	<u>\$1,705</u>
Total Estimated Construction Cost	<u><u>\$18,755</u></u>

**TABLE 13
149TH LANE
ALTERNATIVE 3B - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

**TABLE 14
149TH LANE
ALTERNATIVE 4**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	COMMON EXCAVATION	CU YD	\$5.00	133	\$665
3	GRANULAR BORROW	CU YD	\$8.00	380	\$3,040
4	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.3	\$450

Estimated Construction Cost	\$4,255
Contingency (10%)	\$426
Total Estimated Construction Cost	<u>\$4,681</u>

Section 3
Rum River Hills Golf Club

Rum River Hills Golf Club

Summary

Rum River Hills Golf Club has recently experienced flooding on some fairways and cart paths. Exhibit 9 shows the discharge points and problem areas throughout the golf course. The following will address the problem areas throughout the golf course.

Flooding along Hole #1

The first area of concern includes flooding of the large pond along Hole #1. This pond discharges through an existing concrete outlet structure and a 12" plastic pipe to the east. The outlet is labeled as Area 1 on Exhibit 9. It is our understanding that after large storm events, the pond remains elevated for long periods of time. The 100-year high water level (HWL) is 870.3.

After reviewing video recordings of the outlet pipe, it appears that the pipe has several sags in it and several joints have been compromised. The outlet pipe is relatively shallow and may have been affected by frost heave. This outlet pipe is likely causing the pond to operate inefficiently.

We believe that the best alternative for this outlet is to remove the existing outlet structure and the 12" outlet pipe and replace them with a new 4-foot diameter concrete outlet structure and 18" high density polyethylene pipe outlet in the same location as the existing pipe. Exhibit 10 shows the proposed outlet structure. The new pipe would be installed at a lower elevation than it exists now, reducing the impact of frost heave on the pipe. The polyethylene pipe is also more rigid and is solid (no air voids) and therefore more resistant to frost heave or buoyancy when soils are saturated. A removable, weir wall would be installed in the new outlet structure. We would propose to leave the weir height at the same elevation as existing. By installing the weir wall, it will allow for greater flexibility in controlling the water elevations of the pond. Installing this outlet structure would result in a 100-year HWL of 870.0.

The estimated cost to construct the new outlet pipe and structure is \$29,853. Table 15 includes the individual costs for this alternative.

Cart path flooding near the clubhouse

Based on the HWL of the pond along Hole #1, it appears that there could be an issue with the cart paths flooding near the clubhouse, Areas 2A and 2B on Exhibit 9. The existing culverts under the cart paths are a 15" diameter and 12" diameter. If flooding the cart paths is an issue, the best alternative would be to install larger diameter culverts under the cart paths.

Installing 24" diameter culverts will reduce the flooding during small storm events. However, since the HWL elevation for the area is controlled by the downstream outlet structure discussed above, the paths will still flood during large storm events. To reduce the flooding during the large storm events, the cart paths would have to be raised

approximately 1.5 feet. This would, however, increase the HWL in the pond near the clubhouse, which would appear to adversely affect the fairway for Hole #18.

The estimated cost to replace the two culverts under the carts paths is \$6,642. Table 16 includes the individual costs this alternative.

Saturated soil along Hole #15

Another issue is occurring along Hole #15 near the Rum River. The soil in the area is saturated. There is an existing rock dam, Area 3 on Exhibit 19, southeast of the most saturated area of the fairway. Part of the fairway was excavated to help to determine the problem and drain the area.

Based on our review, it appears that the water being contained by the rock dam may be infiltrating into the soil and causing the saturation. There is a dropped of approximately seven feet from the rock dam to the bottom of the downstream channel. We also noticed that the existing drain tile that was excavated along Hole #15 was plugged with roots and soil.

We recommend two alternatives to address the soil saturation along Hole #15. First, the drain tile should be replaced to improve the drainage in the area. Second, the area of the creek that is being contained by the rock dam should be lined with an impermeable material to eliminate the water infiltrating through the soil. Lining the creek will eliminate the infiltration into the soil and the new drain tile will help to keep the existing soil dry.

The estimated cost to replace the drain tile and line the creek bed is \$13,530. Table 17 includes individual costs for this alternative.

Flooding of Hole #3 and #17

Flooding of Hole #3 and #17 is also an issue at the golf course. The stormwater drains south through three 15” culverts, labeled Area 4A, Area 4B and Area 4C on Exhibit 9, and then to the Rum River.

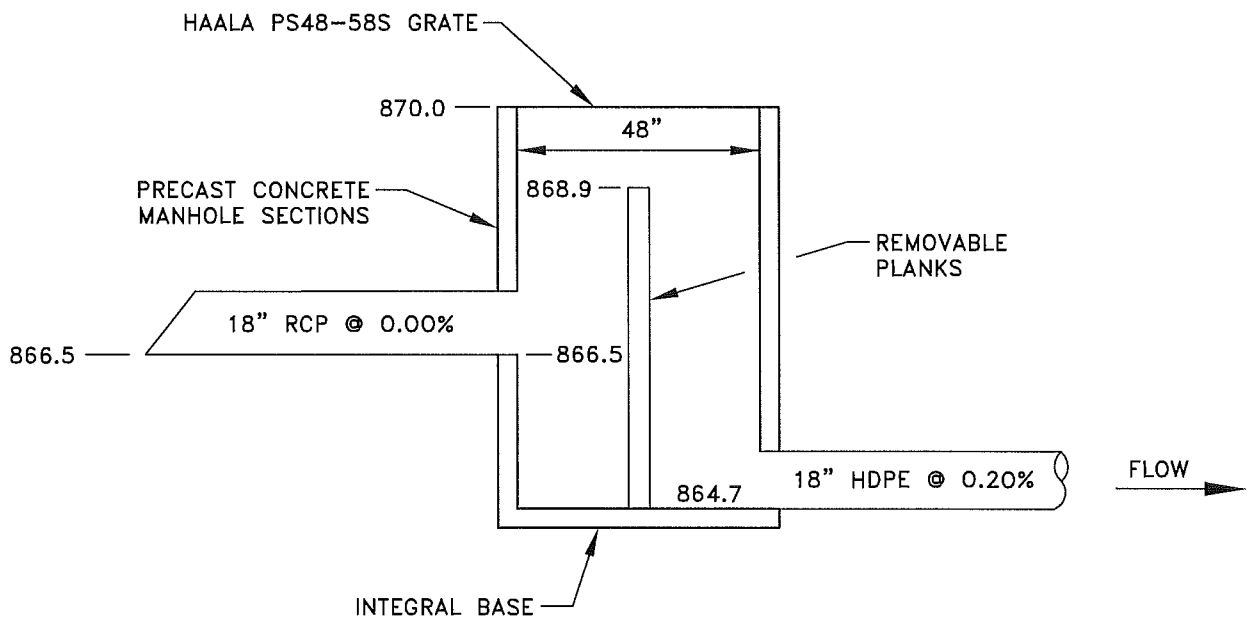
One alternative to lower the HWL’s in the area would be to install bigger culverts. A 21” culvert was modeled to replace the culverts at Area 4A and 4B and a 24” culvert was modeled to replace the culvert at 4C. The following table summarizes the 100-year HWL’s for the three areas:

Existing 100-Year HWL			Proposed 100-Year HWL		
Area 4A	Area 4B	Area 4C	Area 4A	Area 4B	Area 4C
859.3	859.3	858.5	859.1	858.7	858.3

The estimated cost to replace the three culverts is \$4,990. Table 18 includes individual costs for this alternative.



EXHIBIT 9
RUM RIVER HILLS GOLF CLUB
EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA



Feb 14, 2012 - 11:42am
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EXHIBIT 10
RUM RIVER HILLS GOLF CLUB
PROPOSED OUTLET STRUCTURE
CITY OF RAMSEY, MINNESOTA

TABLE 15
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #1 - NEW OUTLET STRUCTURE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	ACRE	\$1,500.00	0.2	\$300
3	GRUBBING	ACRE	\$1,500.00	0.2	\$300
4	REMOVE STORM SEWER	LIN FT	\$1.50	856	\$1,284
5	18" METAL APRON	EACH	\$275.00	1	\$275
6	18" RC PIPE APRON	EACH	\$350.00	1	\$350
7	18" HDPE PIPE SEWER	LIN FT	\$22.00	850	\$18,700
8	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	10	\$230
9	OUTLET CONTROL STRUCTURE	EACH	\$4,000.00	1	\$4,000
10	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.6	\$900

Estimated Construction Cost	\$27,139
Contingency (10%)	\$2,714
Total Estimated Construction Cost	<u>\$29,853</u>

TABLE 16
RUM RIVER HILLS GOLF CLUB
CART PATH FLOODING NEAR CLUBHOUSE - NEW CULVERTS

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	96	\$384
3	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	30	\$150
4	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	48	\$144
5	4" AGGREGATE BASE CLASS 5	SQ YD	\$8.00	30	\$240
6	4" BITUMINOUS PAVEMENT	SQ YD	\$34.00	30	\$1,020
7	24" METAL APRON	EACH	\$325.00	4	\$1,300
8	24" CP PIPE CULVERT	LIN FT	\$25.00	96	\$2,400
9	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$6,038
Contingency (10%)	\$604
Total Estimated Construction Cost	<u>\$6,642</u>

TABLE 17
RUM RIVER HILLS GOLF CLUB
HOLE #15 SATURATION - CREEK LINING AND DRAIN TILE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$350.00	1	\$350
2	4" PERF PIPE DRAIN	LIN FT	\$15.00	450	\$6,750
3	CREEK LINING	LUMP SUM	\$5,000.00	1	\$5,000
4	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$12,300
Contingency (10%)	\$1,230
Total Estimated Construction Cost	<u>\$13,530</u>

TABLE 18
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #3 AND #17 - NEW CULVERTS

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	72	\$288
3	21" METAL APRON	EACH	\$300.00	4	\$1,200
4	24" METAL APRON	EACH	\$325.00	2	\$650
5	24" CP PIPE CULVERT	LIN FT	\$26.00	46	\$1,196
6	24" CP PIPE CULVERT	LIN FT	\$27.00	26	\$702
7	TURF ESTABLISHMENT	LUMP SUM	\$300.00	1	\$300

Estimated Construction Cost	\$4,536
Contingency (10%)	\$454
Total Estimated Construction Cost	<u>\$4,990</u>

Section 4
163rd Lane

163rd Lane

Description

As shown on Exhibit 11, a low area exists south of 163rd Lane and east of Wolfram Street. The low area does not have a piped outlet to County Ditch #3. During a majority of the year, stormwater runoff infiltrates into the soil. However, during early spring when the ground is frozen and during periods of heavy rainfall, water levels have risen to levels that cause flooding of adjacent properties.

Alternatives

The following alternatives address the water elevation in the low area.

Alternative 1

In this alternative, an outlet pipe would be installed from the low area to County Ditch #3, as shown on Exhibit 12. The outlet pipe will not be installed at the bottom of the low area, rather, it will be installed at an elevation that would allow smaller storm events to continue to infiltrate into the soil. The outlet pipe invert is proposed at an elevation of 872.0, which is the approximate elevation of a 3-inch rainfall event. The following table summarizes the 100-year HWL's, the detention time above an elevation of 873.0 and estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL	Detention Time Above Elevation 873 During a 100-Year Storm Event (hours)	Estimated Cost
Existing	874.8	23.4	N/A
12"	874.3	6.2	\$68,640
15"	874.0	2.9	\$74,305
18"	873.8	1.5	\$80,795

As shown, the area may continue to flood during large storm events; however, the duration of flooding will be much shorter.

Tables 19 through 21 include the individual costs for this alternative. It is proposed to directionally drill the pipe as shown on Exhibit 12 as opposed to open cutting a trench. A trench would require excessive cuts in the surrounding area. The outlet pipe will be approximately 700 feet long. Cleaning the pipe will require access on both ends of the pipe.

Alternative 2

Alternative 2 included draining the low area east to a ditch system in Elmcrest Park. This alternative was reviewed and deemed not feasible. There is not enough difference in elevation from the low area to the ditch in Elmcrest Park.

Alternative 3

Alternative 3 included constructing a pipe west and then south along 163rd Lane and Wolfram Street to County Ditch #3. The length of this alternative is approximately 500 feet longer than Alternative 1, therefore decreasing the pipe slope and increasing the costs. The impact of constructing the pipe along the existing streets would also add to the costs of this alternative. For these reasons, this alternative was deemed not feasible.

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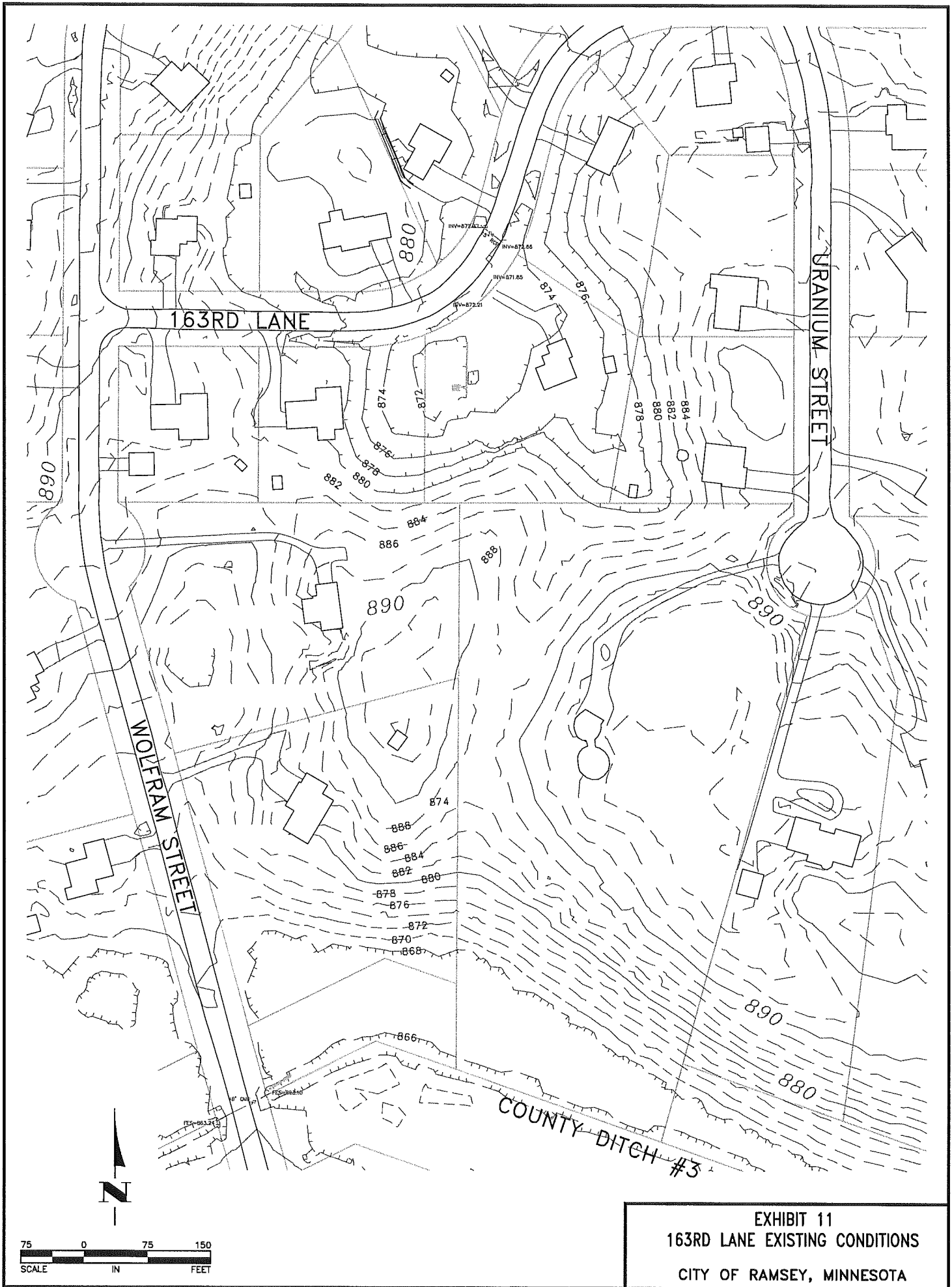


EXHIBIT 11
163RD LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

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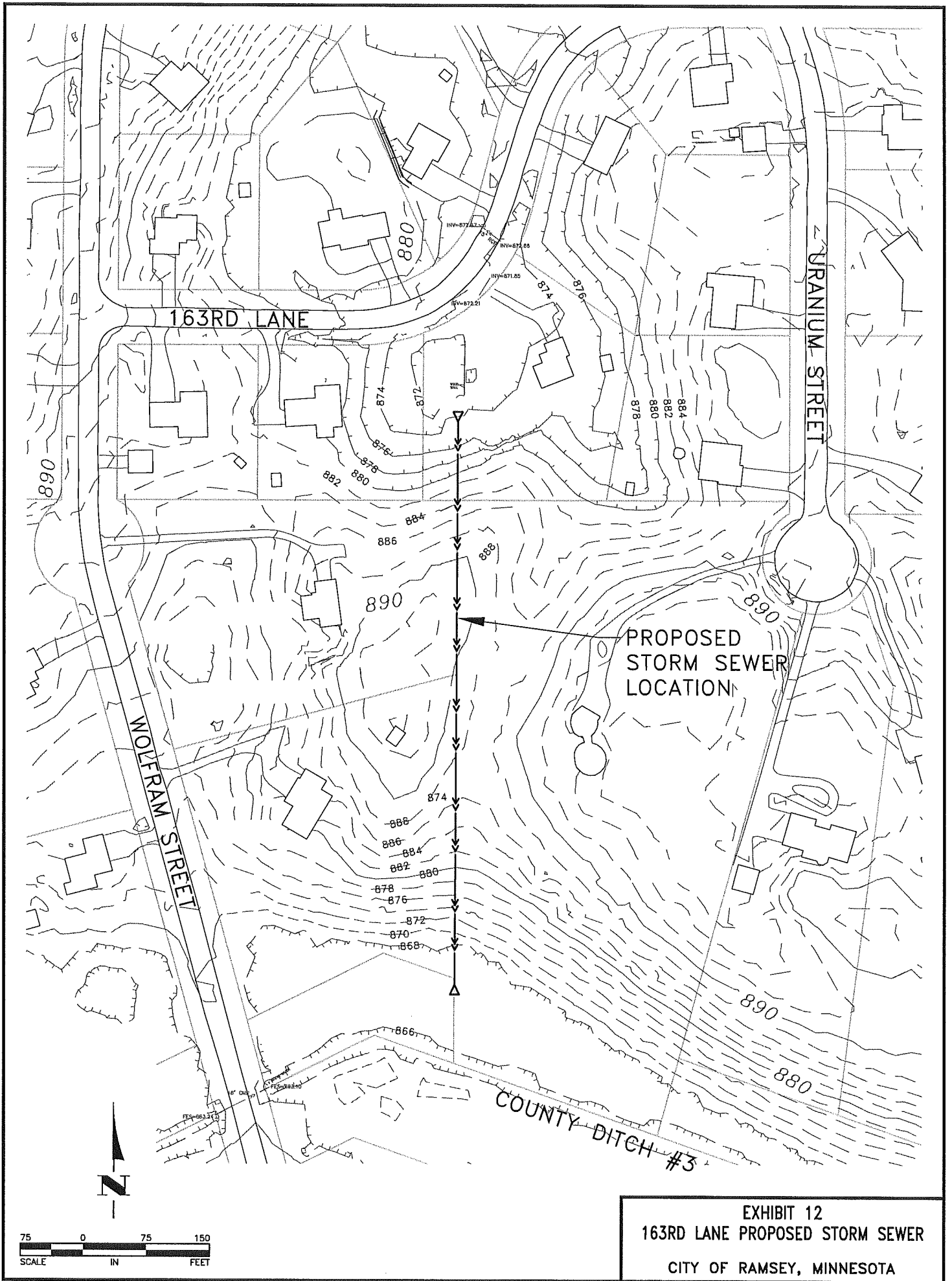


EXHIBIT 12
163RD LANE PROPOSED STORM SEWER
CITY OF RAMSEY, MINNESOTA

TABLE 19
163RD LANE
ALTERNATIVE 1A - 12" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,800.00	1	\$1,800
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	12" METAL APRON	EACH	\$200.00	2	\$400
4	12" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$85.00	700	\$59,500
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$62,400
Contingency (10%)	\$6,240
Total Estimated Construction Cost	<u>\$68,640</u>

TABLE 20
163RD LANE
ALTERNATIVE 1B - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$2,000.00	1	\$2,000
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	15" METAL APRON	EACH	\$225.00	2	\$450
4	15" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$92.00	700	\$64,400
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$67,550
Contingency (10%)	\$6,755
Total Estimated Construction Cost	<u>\$74,305</u>

TABLE 21
163RD LANE
ALTERNATIVE 1C - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$2,200.00	1	\$2,200
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	18" METAL APRON	EACH	\$275.00	2	\$550
4	18" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$100.00	700	\$70,000
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$73,450
Contingency (10%)	\$7,345
Total Estimated Construction Cost	<u>\$80,795</u>

Section 5
156th Lane

156th Lane

Description

As shown on Exhibit 13, a low area exists south of 156th Lane and west of Yakima Street, referred to as Depression 1 on Exhibit 13. Depression 1 drains south and east through an existing storm sewer system to another low area west of Juniper Ridge Drive, referred to as Depression 2 on Exhibit 13. From Depression 2 the stormwater drains north through an existing storm sewer system to the Rum River. The outlet elevations for both Depression 1 and Depression 2 are 859.8.

During storm events, water ponds in the backyard of 5220 156th Lane. The area where water ponds was platted with a 75-foot drainage and utility easement, however, this easement has been vacated. The existing 100-year high water level (HWL) for Depression 1 is 864.7.

Alternatives

The following alternatives address the stormwater in the area.

Alternative 1

Alternative 1 includes filling the backyard of 5220 156th Lane. Excavation would be required in Woodland Green Park to the south to compensate for the storage being lost by filling the backyard. The proposed grading is shown on Exhibit 14. In this alternative the proposed 100-year HWL would remain 864.7, but it would not encroach into the backyard to the extent it does under existing conditions.

The estimated cost for this alternative is \$21,175. Table 22 includes the individual costs for this alternative.

Alternative 2

Alternative 2 also includes filling the backyard of 5220 156th Lane and excavating Woodland Green Park to the south to compensate for the storage being lost by filling the backyard. As opposed to Alternative 1, this alternative proposes to over-excavate the area in Woodland Green Park, which will lower the 100-year HWL by adding storage. The proposed grading is shown on Exhibit 15. In this alternative the proposed 100-year HWL would drop to 864.1.

The estimated cost for this alternative is \$76,274. Table 23 includes the individual costs for this alternative.

Alternative 3

Alternative 3 includes constructing an additional 12" outlet pipe from Depression 2. As shown on Exhibit 16, the new outlet pipe would run east along 156th Lane and then north along Juniper Ridge Drive to an existing catch basin that drains to the Rum River. The

new outlet would be constructed at an elevation of 857.8, two feet lower than the existing outlet.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.6
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.9

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.0
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.8

As shown, adding this additional outlet pipe has very little effect on the high water levels of the two depressions. One reason for this is that the storm sewer system downstream of Depression 2 is at or above capacity and is flowing back into Depression 2 prior to draining downstream.

The estimated cost to construct this additional outlet pipe is \$158,609. Table 24 includes the individual costs for this alternative.

Alternative 4

Alternative 4 includes constructing an additional 18" outlet pipe from Depression 1. As shown on Exhibit 16, the new outlet pipe would run east from Depression 1 to Depression 2. The new outlet would be constructed at an elevation of 858.2; 1.6-feet lower than the existing outlet. This alternative assumes that the new outlet from Depression 2, as discussed in Alternative 3, would also be constructed.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.6
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.5

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	863.8
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.3

As shown, adding this additional outlet pipe has very little effect on the high water levels of the two depressions. After adding the pipe, stormwater actually flows from Depression 2 back to Depression 1 prior to flowing downstream, lowering the HWL in Depression 2.

The estimated cost to construct this additional outlet pipe is \$37,697. Table 25 includes the individual costs for this alternative.

Alternative 5

Alternative 5 includes constructing a 36" outlet pipe from Depression 2. As shown on Exhibit 16, the new outlet pipe would run east from Depression 2 to the Rum River. The new outlet would be constructed at an elevation of 857.8. As part of this alternative, the existing outlet pipe from Depression 2 will be eliminated. This alternative also assumes that the new outlet from Depression 1, as discussed in Alternative 4, would also be constructed.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	863.8
Depression 2 – Existing	866.0
Depression 2 – Proposed	859.9

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	862.7
Depression 2 – Existing	866.0
Depression 2 – Proposed	859.8

As shown, adding this outlet pipe has a significant effect on the high water levels of the two depressions. This alternative would have to be approved by the Minnesota Department of Natural Resources.

The estimated cost to construct this additional outlet pipe is \$142,772. Table 26 includes the individual costs for this alternative.



EXHIBIT 13
156TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

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156TH LANE

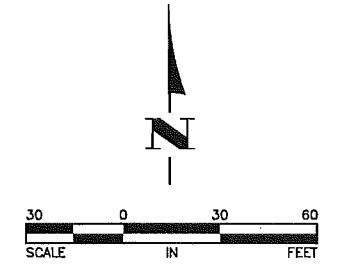
5220

5200

DEPRESSION 1

WOODLAND GREEN PARK

870



STORM MH
RIM=872.02
INV=859.52

SKIMMER
HIGH=863.48
LOW=862.74
BTM=858.89

INV=859.76

INV=859.23

FES=857.47

FES=858.77

FES=859.89

18" PVC

21" RCP

15" RCP

15" RCP

EXHIBIT 14
156TH LANE ALTERNATIVE 1 GRADING PLAN
CITY OF RAMSEY, MINNESOTA

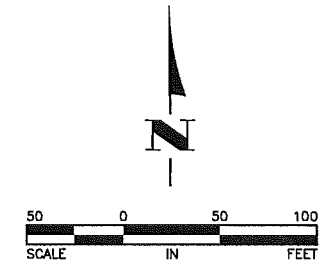
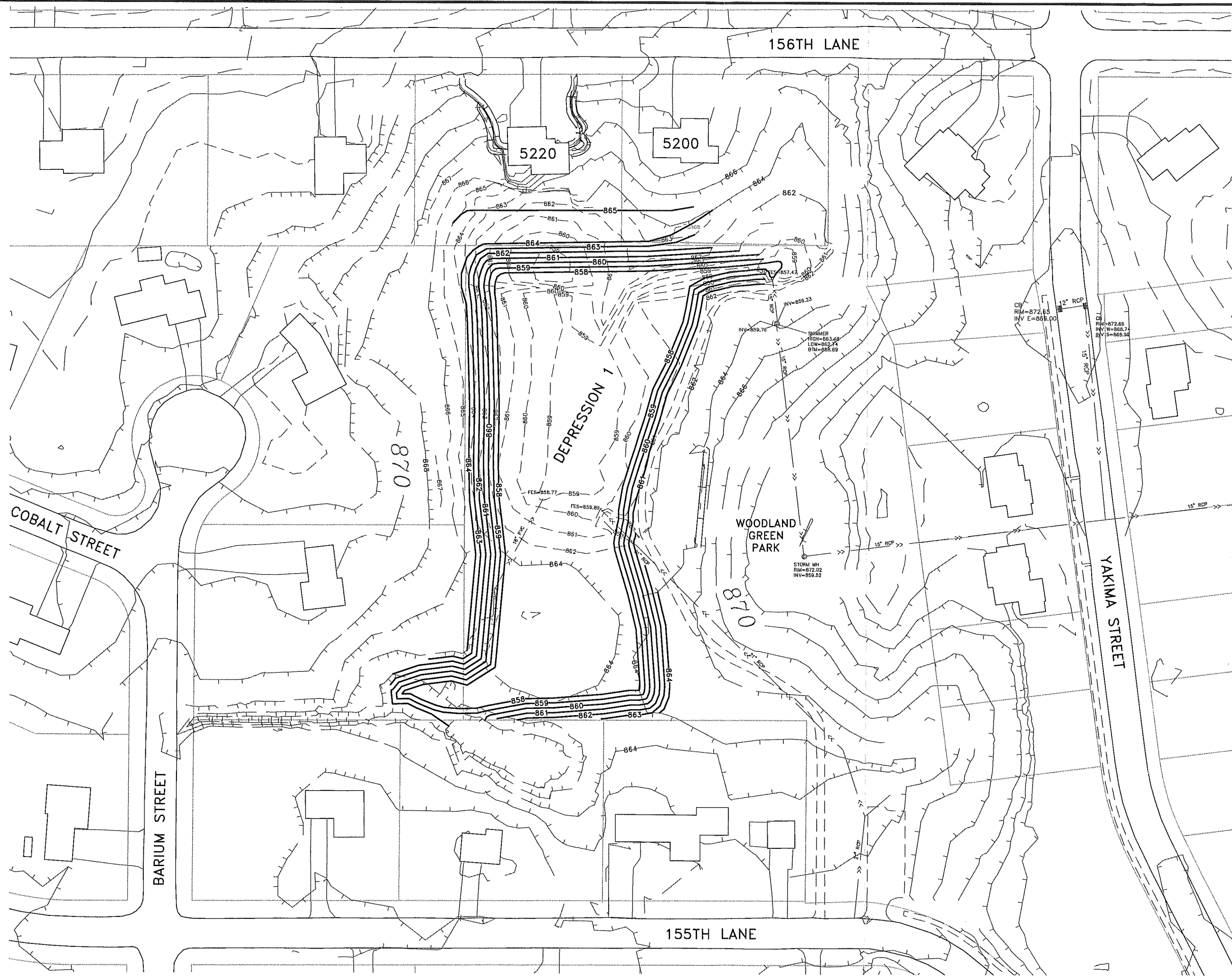


EXHIBIT 15
156TH LANE ALTERNATIVE 2 GRADING PLAN
CITY OF RAMSEY, MINNESOTA

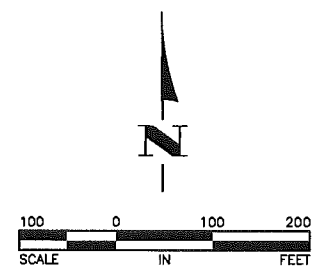
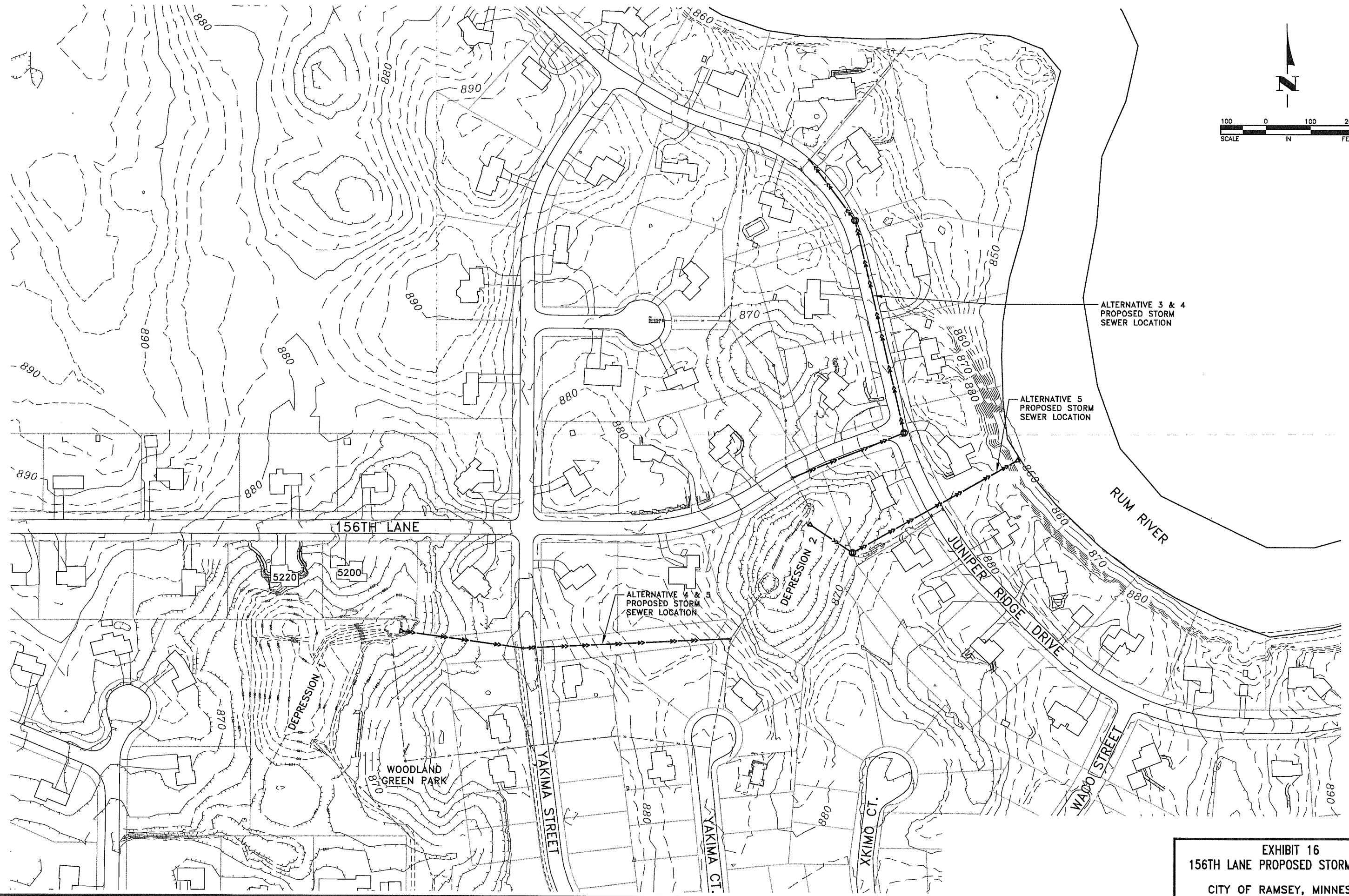


EXHIBIT 16
156TH LANE PROPOSED STORM SEWER
CITY OF RAMSEY, MINNESOTA

TABLE 22
156TH LANE
ALTERNATIVE 1

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$600.00	1	\$600
2	CLEARING	ACRE	\$1,500.00	0.75	\$1,125
3	GRUBBING	ACRE	\$1,500.00	0.75	\$1,125
4	COMMON EXCAVATION	CU YD	\$5.00	1820	\$9,100
5	ADJUST SEWER MANHOLES	LUMP SUM	\$4,000.00	1	\$4,000
6	ADJUST SEWER CLEANOUTS	LUMP SUM	\$500.00	1	\$500
7	REINSTALL SPRINKLER SYSTEM	LUMP SUM	\$1,000.00	1	\$1,000
8	TURF ESTABLISHMENT	ACRE	\$1,500.00	1.2	\$1,800

Estimated Construction Cost	\$19,250
Contingency (10%)	\$1,925
Total Estimated Construction Cost	<u>\$21,175</u>

**TABLE 23
156TH LANE
ALTERNATIVE 2**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,900.00	1	\$1,900
2	CLEARING	ACRE	\$1,500.00	2.35	\$3,525
3	GRUBBING	ACRE	\$1,500.00	2.35	\$3,525
4	COMMON EXCAVATION	CU YD	\$5.00	10123	\$50,615
5	ADJUST SEWER MANHOLES	LUMP SUM	\$4,000.00	1	\$4,000
6	ADJUST SEWER CLEANOUTS	LUMP SUM	\$500.00	1	\$500
7	REINSTALL SPRINKLER SYSTEM	LUMP SUM	\$1,000.00	1	\$1,000
8	TURF ESTABLISHMENT	ACRE	\$1,500.00	2.9	\$4,275

Estimated Construction Cost	\$69,340
Contingency (10%)	\$6,934
Total Estimated Construction Cost	<u>\$76,274</u>

TABLE 24
156TH LANE
ALTERNATIVE 3

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$4,200.00	1	\$4,200
2	SALVAGE STORM SEWER	LIN FT	\$20.00	72	\$1,440
3	REMOVE MANHOLE	EACH	\$500.00	1	\$500
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$2.00	3,290	\$6,580
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	100	\$300
6	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.00	3,290	\$23,030
7	4" BITUMINOUS PAVEMENT	SQ YD	\$21.00	3,290	\$69,090
8	BITUMINOUS CURB	LIN FT	\$3.00	1,850	\$5,550
9	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
10	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	925	\$20,350
11	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$2,000.00	3	\$6,000
12	TRAFFIC CONTROL	LUMP SUM	\$5,000.00	1	\$5,000
13	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$144,190
Contingency (10%)	\$14,419
Total Estimated Construction Cost	<u>\$158,609</u>

TABLE 25
156TH LANE
ALTERNATIVE 4

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,000.00	1	\$1,000
2	SALVAGE STORM SEWER	LIN FT	\$20.00	120	\$2,400
3	REMOVE CONCRETE CURB	LIN FT	\$10.00	60	\$600
4	REMOVE MANHOLE	EACH	\$500.00	1	\$500
5	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	110	\$550
6	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	70	\$210
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	110	\$825
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	110	\$3,080
9	18" RC PIPE APRON	EACH	\$350.00	1	\$350
10	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
11	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
12	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	735	\$16,905
13	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	2	\$3,000
14	CONCRETE CURB AND GUTTER DESIGN B618	LIN FT	\$20.00	60	\$1,200
15	TRAFFIC CONTROL	LUMP SUM	\$2,000.00	1	\$2,000
16	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.3	\$450

Estimated Construction Cost	\$34,270
Contingency (10%)	\$3,427
Total Estimated Construction Cost	<u>\$37,697</u>

**TABLE 26
156TH LANE
ALTERNATIVE 5**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$3,750.00	1	\$3,750
2	REMOVE STORM SEWER	LIN FT	\$4.00	72	\$288
3	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
4	36" METAL APRON	EACH	\$325.00	1	\$325
5	36" RC PIPE APRON	EACH	\$450.00	1	\$450
6	TRASH GUARD FOR 36" PIPE APRON	EACH	\$400.00	1	\$400
7	36" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$38.00	110	\$4,180
8	36" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$280.00	420	\$117,600
9	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	1	\$2,000
10	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$129,793
Contingency (10%)	\$12,979
Total Estimated Construction Cost	<u>\$142,772</u>

Section 6

Sodium Street

Sodium Street

Description

The house at 16756 Sodium Street has been experiencing water in the basement. Exhibit 17 shows the existing area. One reason for water in the basement could be that stormwater fills the ditch on the east side of Sodium Street, overtops the road and if the driveway culvert is blocked, the water fills up the ditch on the west side of Sodium Street and drains toward the house. The water then seeps along the basement wall and eventually into the basement. Another reason for water in the basement may be due to a high groundwater elevation in the area.

Alternatives

The following alternatives address the issue of water entering the basement from the road ditch.

Alternative 1

Alternative 1 will include installing a culvert under Sodium Street, regrading the west ditch of Sodium Street, constructing a berm to keep the water in the ditch, and replacing the existing driveway culvert. The ditch would be graded to drain to the south property line of 16756 Sodium Street and a culvert would be installed to drain the stormwater to the swale on the west side of the lot. Exhibit 18 shows the proposed construction. The culvert along the south property line is needed since a ditch cannot be graded without impacting the existing septic drainfield.

To be able to install the pipe along the south property line to the swale on the west side of the lot, a utility pole and a utility pedestal will have to be relocated from the southeast corner of 16756 Sodium Street. Moving the utilities will allow the area to be graded as needed to construct the ditch and the culvert. Regrading the ditch will prevent the stormwater from draining toward the house and reducing the chances of having water in the basement.

The estimated cost for this alternative is \$12,225. Table 27 includes the individual costs for this alternative.

Alternative 2

Alternative 2 is similar to Alternative 1, but instead of installing a culvert along the south property line to the swale, a retaining wall would be constructed. This alternative would only be required if the utilities in the southeast corner of the lot were unable to be relocated.

The estimated cost for this alternative is \$18,847. Table 28 includes the individual costs for this alternative.

Both alternatives would require that the property owner give an easement along the south property line. Also, temporary easements will likely be required to grade the ditch and construct the berm on the west side of Sodium Street.

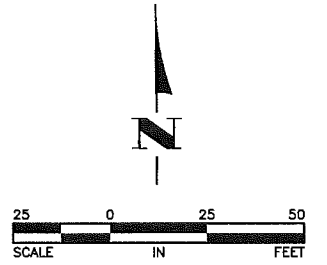


EXHIBIT 17
SODIUM STREET EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

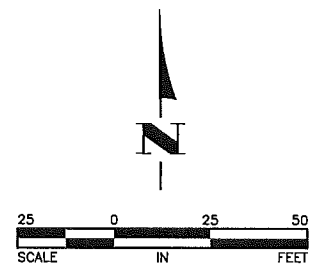


EXHIBIT 18
SODIUM STREET PROPOSED CONSTRUCTION
CITY OF RAMSEY, MINNESOTA

**TABLE 27
SODIUM STREET
ALTERNATIVE 1**

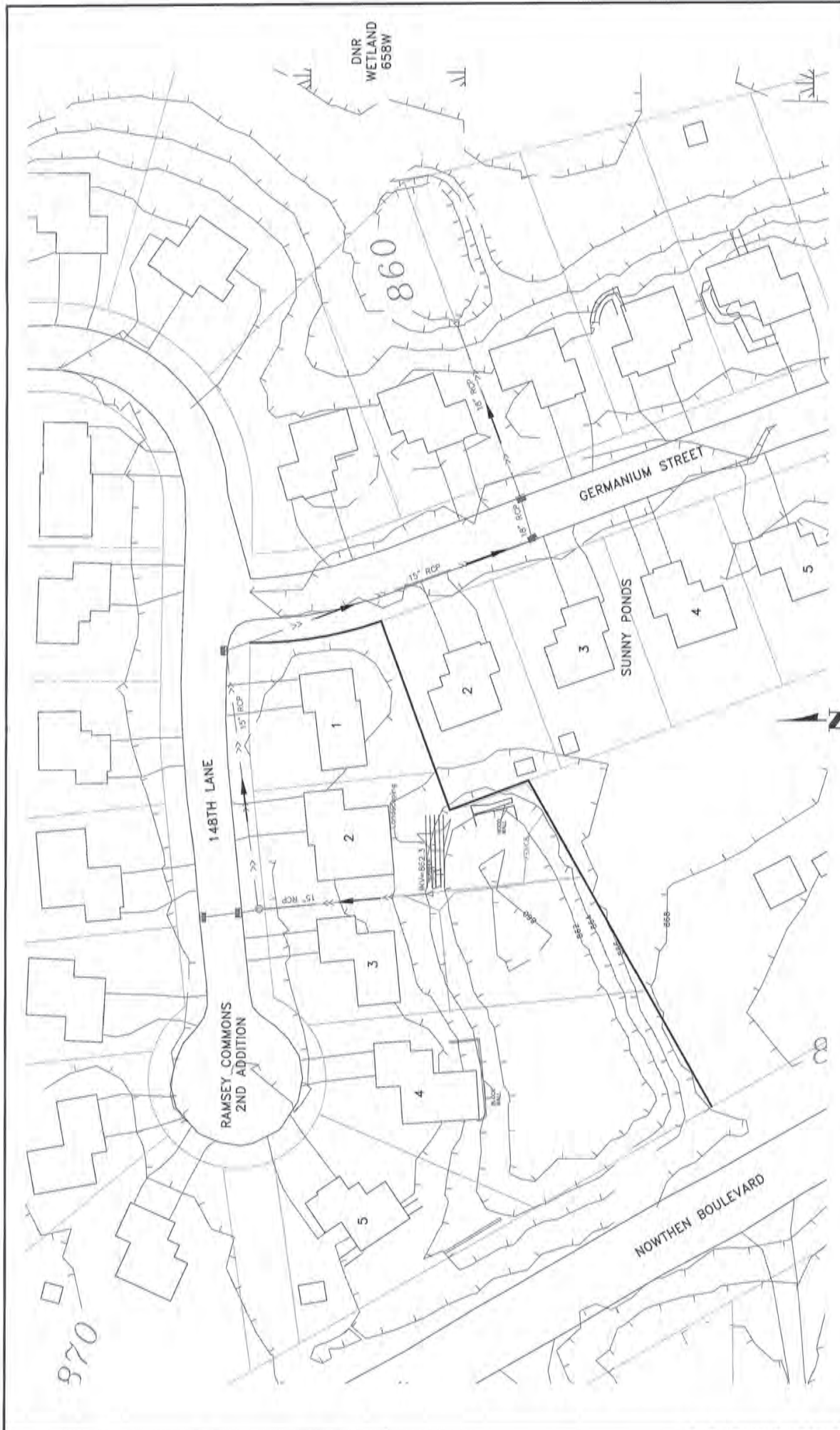
ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$300.00	1	\$300
2	REMMOVE STORM SEWER	LIN FT	\$5.00	25	\$125
3	REMOVE CONCRETE PAVEMENT	SQ YD	\$6.00	45	\$270
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	49	\$245
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	44	\$132
6	COMMON EXCAVATION	CU YD	\$6.00	400	\$2,400
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	94	\$705
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	49	\$1,372
9	4" CONCRETE DRIVEWAY PAVEMENT	SQ YD	\$13.00	45	\$585
10	15" METAL APRON	EACH	\$225.00	2	\$450
11	12" RC PIPE APRON	EACH	\$275.00	4	\$1,100
12	15" CP PIPE SEWER	LIN FT	\$20.00	44	\$880
13	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$20.00	40	\$800
14	TRAFFIC CONTROL	LUMP SUM	\$1,000.00	1	\$1,000
15	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$11,114
Contingency (10%)	\$1,111
Total Estimated Construction Cost	<u>\$12,225</u>

**TABLE 28
SODIUM STREET
ALTERNATIVE 2**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$300.00	1	\$300
2	REMMOVE STORM SEWER	LIN FT	\$5.00	25	\$125
3	REMOVE CONCRETE PAVEMENT	SQ YD	\$6.00	45	\$270
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	49	\$245
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	44	\$132
6	COMMON EXCAVATION	CU YD	\$6.00	475	\$2,850
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	94	\$705
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	49	\$1,372
9	4" CONCRETE DRIVEWAY PAVEMENT	SQ YD	\$13.00	45	\$585
10	12" RC PIPE APRON	EACH	\$275.00	4	\$1,100
11	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$20.00	40	\$800
12	MODULAR BLOCK RETAINING WALL	SQ FT	\$23.00	300	\$6,900
13	TRAFFIC CONTROL	LUMP SUM	\$1,000.00	1	\$1,000
14	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$17,134
Contingency (10%)	\$1,713
Total Estimated Construction Cost	<u>\$18,847</u>



DNR
WETLAND
658W

860

GERMANIUM STREET

SUNNY PONDS

148TH LANE

RAMSEY COMMONS
2ND ADDITION

NOWTHEN BOULEVARD

870

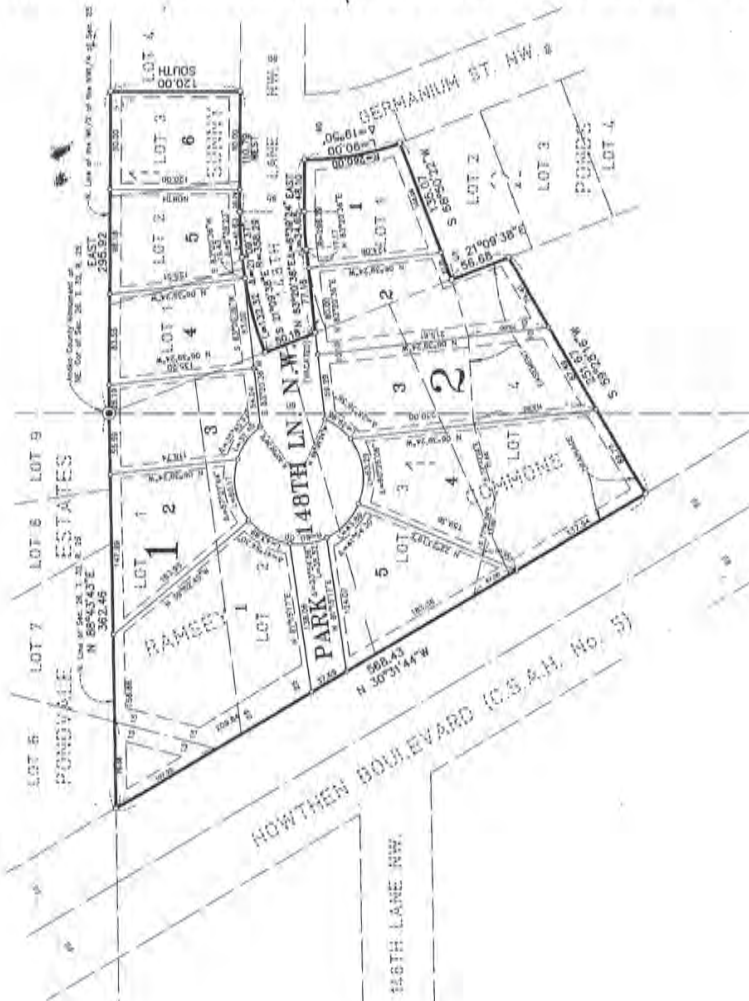
EXHIBIT 1
148TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA



RAMSEY COMMONS 2ND ADDITION

CITY OF RAMSEY

COUNTY OF ANOKA



1049281

OFFICE OF COUNTY REGISTER
STATE OF MINNESOTA, COUNTY OF ANOKA
I HEREBY CERTIFY THAT THIS PLAT
WAS FILED IN MY OFFICE FOR RECORD
ON THE 21ST DAY OF MAY, A.D. 1933
AT 10:49 A.M.
In Public Hearing at
St. Paul, Minnesota

Edmund J. Tink
County Register



3 DENOTES 1/2 INCH FROM PIPE SEE
PLAN FOR DETAILS
NOTE: TOP OF PARAPETS OF THIS PLAT, THE
NORTH LINE OF THE 1/2 OF THE 180' x 4
TO HAVE A GRADING OF 5.0'

SPACINGS AND UTILITY EASEMENTS SHOWN THIS
BEING TO FEET WIDE AND ADJOINING ALL STREET
RIGHT-OF-WAY LINES AND REAR LOT LINES AND
FRONT LOT LINES UNLESS OTHERWISE SHOWN IN THE PLAT

CAINE & ASSOCIATES
LAND SURVEYORS, INC.

KNOW ALL PERSONS BY THESE PRESENTS, that the following described property situated in the County of Anoka, State of Minnesota, to-wit: A Minnesota Corporation, owner and proprietor, and successors, heirs, assigns, executors, administrators and assigns of the County of Anoka, State of Minnesota, do hereby certify that the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Block 1, 2 and 3, Block 1, and Block 2, all in SEWER FIELDS, according to the recorded plat thereof, Anoka County, Minnesota, and the addition to the plat of SEWER FIELDS, according to the recorded plat thereof, Anoka County, Minnesota, Block north of the second line of Lot 1, Block 2, in said SEWER FIELDS and being north of the following described lines:

Commencing at the southeast corner of said Lot 1, Block 2, thence as an assumed bearing of West, along the north line of said Lot 1, Block 2, to the northeast corner of said Lot 1, Block 2, a distance of 110.00 feet; thence as an assumed bearing of North 70° 20' East and a distance of 110.00 feet to a station of 110.00 feet to the point of beginning of the line to be described; thence along the north line of said Lot 1, Block 2, and said line thence connecting.

AND that North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Dallas Smith, et al., successors and the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Block 1, 2, 3 and 4, Block 2, SEWER COMMONS, according to the recorded plat thereof, Anoka County, Minnesota.

KNOW ALL PERSONS BY THESE PRESENTS, that the following described property situated in the County of Anoka, State of Minnesota, to-wit: A Minnesota Corporation, owner and proprietor, and successors, heirs, assigns, executors, administrators and assigns of the County of Anoka, State of Minnesota, do hereby certify that the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Block 1, 2, 3 and 4, Block 2, SEWER COMMONS, according to the recorded plat thereof, Anoka County, Minnesota, and the addition to the plat of SEWER COMMONS, according to the recorded plat thereof, Anoka County, Minnesota, Block north of the second line of Lot 1, Block 2, in said SEWER COMMONS and being north of the following described lines:

Commencing at the southeast corner of said Lot 1, Block 2, thence as an assumed bearing of West, along the north line of said Lot 1, Block 2, to the northeast corner of said Lot 1, Block 2, a distance of 110.00 feet; thence as an assumed bearing of North 70° 20' East and a distance of 110.00 feet to a station of 110.00 feet to the point of beginning of the line to be described; thence along the north line of said Lot 1, Block 2, and said line thence connecting.

AND that North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Dallas Smith, et al., successors and the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Block 1, 2, 3 and 4, Block 2, SEWER COMMONS, according to the recorded plat thereof, Anoka County, Minnesota.

WITNESSE MY HAND AND SEAL OF OFFICE, this 21ST day of May, 1933.

Edmund J. Tink
County Register

STATE OF MINNESOTA
COUNTY OF ANOKA
I HEREBY CERTIFY THAT THIS PLAT
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St. Paul, Minnesota

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INSTALL 14" DIA. TIDEFLEX CHECKMATE INLINE VALVE IN SOUTH PIPE SEE SPECIFICATIONS

CB RIM=866.62 INV=853.23

RIM=865.74
INV NE=861.45
INV W=861.40
INV SE=861.43

148TH LANE

GERMANIUM STREET

BITUMINOUS TRAIL

NOWTHEN BOULEVARD (CSAH 5)

REGRADE AREA ALLOW DRAINAGE BETWEEN LOTS

GRADE NEW 860 CONTOUR 10 FT FROM 861 CONTOUR
REMOVE TREES AS DIRECTED
RESTORE DISTURBED AREA WITH SEED

CONSTRUCTION ACCESS FROM NOWTHEN BLVD, LOCATION SELECTED IN FIELD
OBTAIN ANOKA COUNTY PERMIT
REPLACE TRAIL IN KIND IF DAMAGED
RESTORE SLOPES WITH TOPSOIL AND SEED

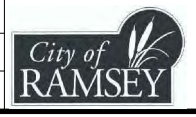
REMOVE AND REPLACE 80' FENCE

DATE	REVISION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

LEONARD L. LINTON
Date 6/1/12 Lic. No. 21112

DESIGNED BY: LLL
DRAWN BY: LLL
CHECKED BY: LLL
DATE: 6/1/12
FILE NO.: 12-24



CITY OF RAMSEY
7550 SUNWOOD DRIVE
RAMSEY, MN 55303
(763) 427-1410 FAX (763) 433-9898

5600 148TH LANE POND

2012 STORM SEWER CHANGE ORDER
RAMSEY CITY PROJECT NO. 12-24
CITY OF RAMSEY, MINNESOTA

SHEET 1

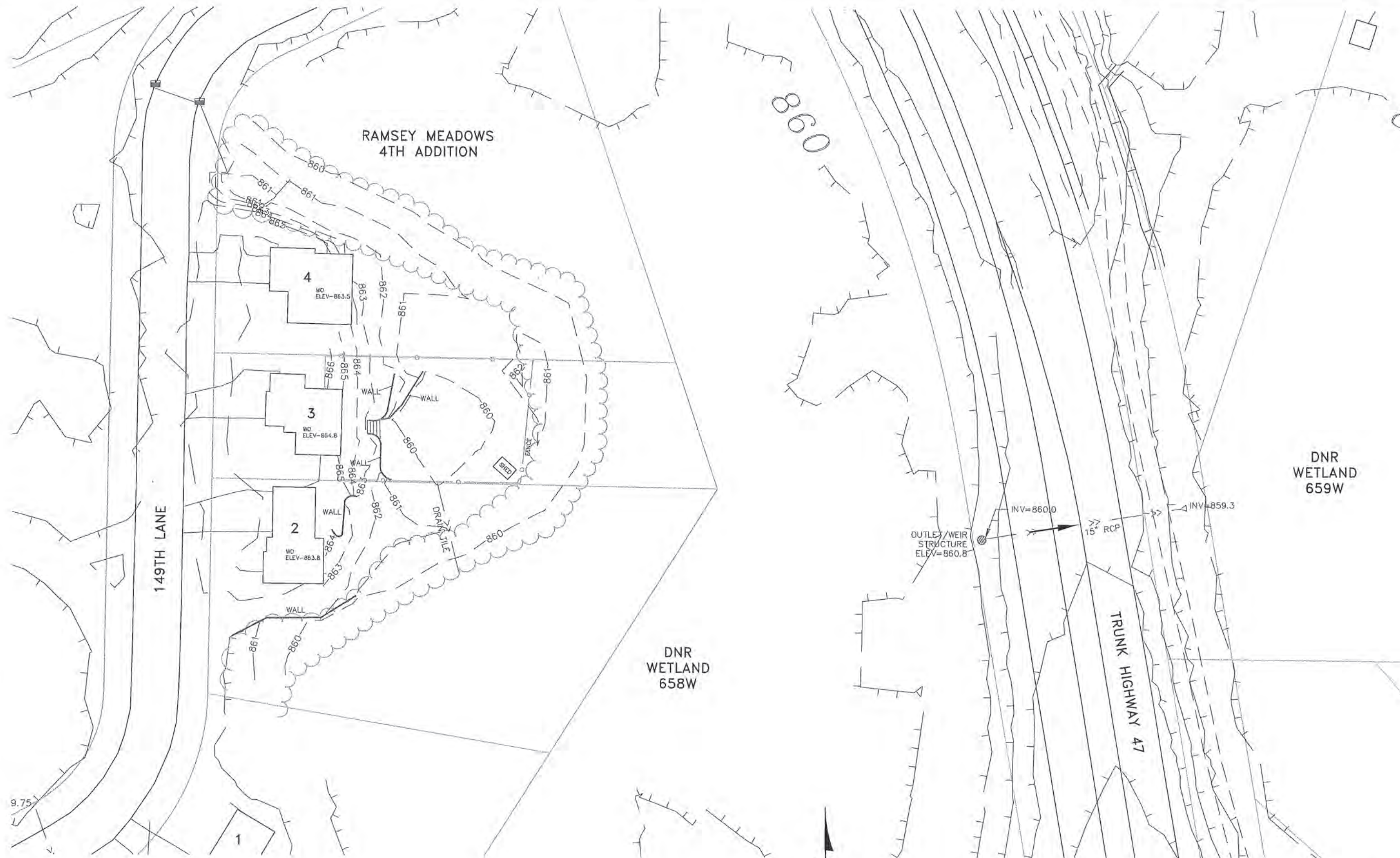
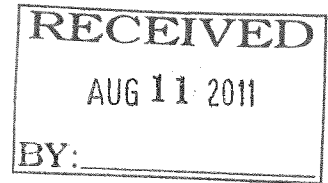


EXHIBIT 5
149TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

11 pages
total

Petition



Target: Review drainage problems in the city of Ramsey to cope with the volume of water entering our wetland and to lower the culvert that drains to Rum River.

Background:

We have lived in our homes for over seven years and some residents many more. We have never seen water even close to this amount in our yards. We have little children that this could cause injury or a health threat to. Many areas are filled over their capacity and we feel this is dangerous for our children to play near or around.

Petition:

Many of the houses that are backed up to the wetlands off of 149th lane NW ; Sunwood drive; 148th lane; Fluorine street; Germanium and Erkium are in the process or have lost their backwards due to the overflow of these wetlands . There is as much as 2 ½ feet of standing water and large portions of are yards, gardens, and sheds under water. The flooding started in May, before the recent rainfalls, and has continued to rise.

We as residents of the neighborhood believe that due to the new construction of houses and roads in the neighborhood this has changed the natural drainage process that was originally planned for this area and needs to be addressed by the city of Ramsey. The wetland we are told is still another foot or so away from reaching the overflow that eventually drains to the Rum River.

We are concerned now for the health and the safety of our children (newborns, special needs children), ourselves, and our pets due to the bacteria produced from the standing water along with the high concentration of mosquitoes that spread disease and we are obviously concerned of the flooding of our homes. The trees that are the noise barrier on highway 47 are starting to die. The market value of our homes has dropped considerably over the last few years and the standing stagnant water in our yards will definitely make it decrease even more. We need this problem to be addressed A.S.A.P.

Contact Person

First Name: Jessica P Rocky

Last Name: Belmonte

Email Address: j.belmonte@hotmail.com

Street Address: 5410 149th lane NW

City: Ramsey State: MN Zip Code: 55303

Signature: *Jessica Belmonte* Date 8.5.11

First Name: RANDALL Crumpler

Last Name: Crumpler

Email Address: randallcrumpler@hotmail.com

Street Address: 14780 Fickion St NW

City: Ramsey State: MN Zip Code: 55303

Signature: *Randall Crumpler* Date 8-6-11

1/

First Name: Doug Donald Turbett
Last Name: Jaguer
Email Address: DJRollin33@yahoo
Street Address: 5440 149th Ln NW
City: Ramsey State: MN Zip Code: 55303
Signature: [Signature]

First Name: Patrick
Last Name: Daly
Email Address: PJ Daly 7@yahoo.com
Street Address: 5450 149 LN NW
City: Ramsey State: MN Zip Code: 55303
Signature: [Signature]

First Name: LEO
Last Name: MAJENSKI
Email Address: leo.majenki@aol.com
Street Address: 5480 149TH LANE NW
City: RAMSEY State: MN Zip Code: 55303
Signature: [Signature]

2

First Name: TEJ

Sherry Klaus

Last Name: CRUTE

Email Address: TS CRUTE@MSN.COM

Street Address: 14780 FLUORINE ST. N.W.

City: RAMSEY State: MN. Zip Code: 55303

Signature: Tej Crute

First Name: KENNETH E SMITH

Last Name: _____

Email Address: KOSMITH@Q.COM

Street Address: 14771 FLUORINE ST NW

City: RAMSEY State: MN Zip Code: 55303

Signature: 

First Name: DANA

Julie Gardapee

Last Name: SPAH

Email Address: _____

Street Address: 14801 FLUORINE ST. NW

City: RAMSEY State: MN Zip Code: 55303

Signature: Dana Spah


First Name: Kami
Last Name: Schauer
Email Address: KSchauer@live.com
Street Address: 5428 149th Ln NW
City: Ramsey State: Mn Zip Code: 55303
Signature: Kami Schauer

same address


First Name: KRIS FITZGERALD
Last Name: C/O GRACELYN & ESTELLE FITZGERALD
Email Address: _____
Street Address: 5420 149th Ln NW
City: RAMSEY State: MN Zip Code: 55303
Signature: [Signature]

First Name: Charles
Last Name: Monette
Email Address: cmonette2004@msn.com
Street Address: 5423 149th Ln. N.W.
City: Ramsey State: Mn Zip Code: 55303
Signature: [Signature]

AAT Investments

First Name: John Gibson & Tepi Smith
Last Name: Gibson / Smith Phone 612.437.0705
Email Address: jgbsn@nym.hush.com fax
Street Address: 5400 149 Lane NW 1715-421-6044
City: Ramsey State: MN Zip Code: 55303
Signature:  POA Date 8/11

First Name: Garett & Dee Hanson
Last Name: Hanson
Email Address: dah75@netzero.net
Street Address: 14790 Fluorine St NW
City: Ramsey State: MN Zip Code: 55303
Signature: Dee Hanson / ~~Garett~~ Hanson

First Name: Youssif Elmagri
Last Name: Elmagri
Email Address: youssifelmagri@yahoo.com
Street Address: 5430 149th Lane NW
City: Ramsey State: MN Zip Code: 55303
Signature: 

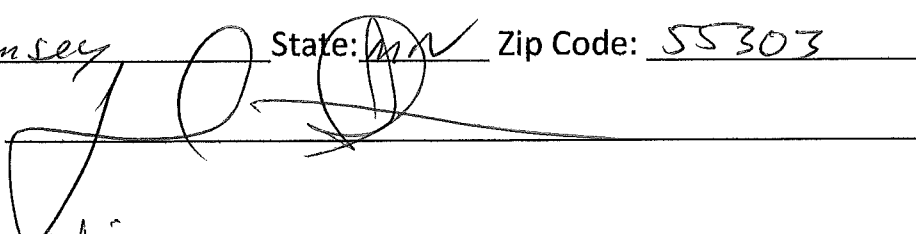
First Name: Joe Different Watershed

Last Name: Schaer

Email Address: joetanaers@hotmail.com

Street Address: 5429 145th Ave NW

City: Ramsey State: MN Zip Code: 55303

Signature: 

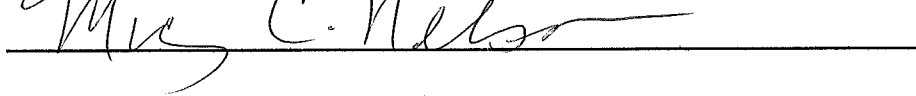
First Name: Mickey

Last Name: Nelson

Email Address: ~~mick~~ murranelsons@a.com

Street Address: 5470 149th Lane N.W.

City: Ramsey State: Mn. Zip Code: 55303

Signature: 

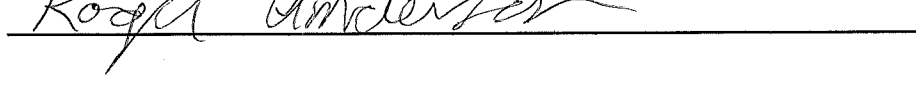
First Name: Roger

Last Name: Anderson

Email Address: RWANDERSON36@comcast.net

Street Address: 5490 149th LN. N.W.

City: Ramsey State: MN Zip Code: 55303

Signature: 

First Name: Somchith

Last Name: Chitmany

Email Address: _____

Street Address: 5460 149 LN MN

City: Ramsey State: MN Zip Code: 55308

Signature: Somchith Chitmany

✓ In to
Master
Lock

First Name: Pam

Last Name: Varner

Email Address: pjvpamela10@gmail

Street Address: 14931 Germanium St NW

City: Ramsey State: mn Zip Code: 55303

Signature: Pam V

First Name: RICK L. BERGLBBN

Last Name: _____

Email Address: TRANSAM ONE THREE @ AOL.COM

Street Address: 14851 GERMANIUM ST.

City: RAMSEY State: MN Zip Code: 55303-5113

Signature: Rick L. Berglbbn

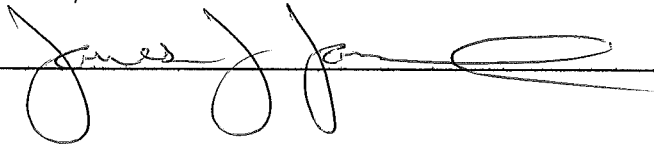
First Name: JAMES JAM

Last Name: JAMINSKI

Email Address: JJAMINSKI@COMCAST.NET

Street Address: 14827 GERMANIUM ST NW

City: RAMSEY State: MN Zip Code: 55303

Signature: 

First Name: Chad

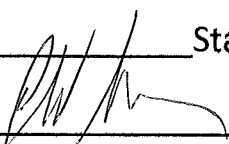
*Anna Hejchar
Chad Selger
O.K.*

Last Name: Selger

Email Address: Cselger@winroc.com

Street Address: 14801 Germanium St

City: Ramsey State: MN Zip Code: 55303

Signature: 


First Name: Jerry

Last Name: Smith

Email Address: _____

Street Address: 14790 Erbium St. NW

City: Ramsey State: MN Zip Code: 55303

Signature: 

First Name: TRISH David & Tricia
Last Name: BATES Osborn
Email Address: 14711 ERKIN ST NW
Street Address: NANTRISH@HOTMAIL.COM
City: Ramsey State: MN Zip Code: 55303
Signature: Trish Bates

First Name: Kenny Strobush
Last Name: Strobush
Email Address: DICKSTROBUSH@COMCAST.NET
Street Address: 5409 145th Ln NW
City: Ramsey State: MN Zip Code: 55303
Signature: Kenny Strobush

First Name: Ryan & Katre Lang
Last Name: _____
Email Address: Rlang1018@gmail.com
Street Address: 5115 149th Lane NW
City: Ramsey State: MN Zip Code: 55303
Signature: Ryan & Katre Lang AAT Investments

First Name: Tracey

Last Name: Dowd

Email Address: dowdt@comcast.net

Street Address: 5500 149th Lane NW

City: Ramsey State: MN Zip Code: 55303

Signature: Tracey Dowd

First Name: Cheri + Mike

Last Name: Johnson

Email Address: cheriandmike@usfamily.net

Street Address: 14791 Fluorine St NW

City: Ramsey State: MN Zip Code: 55303

Signature: Cheri + John

First Name: Chris + Alexandra

Last Name: Bertz

Bertolas

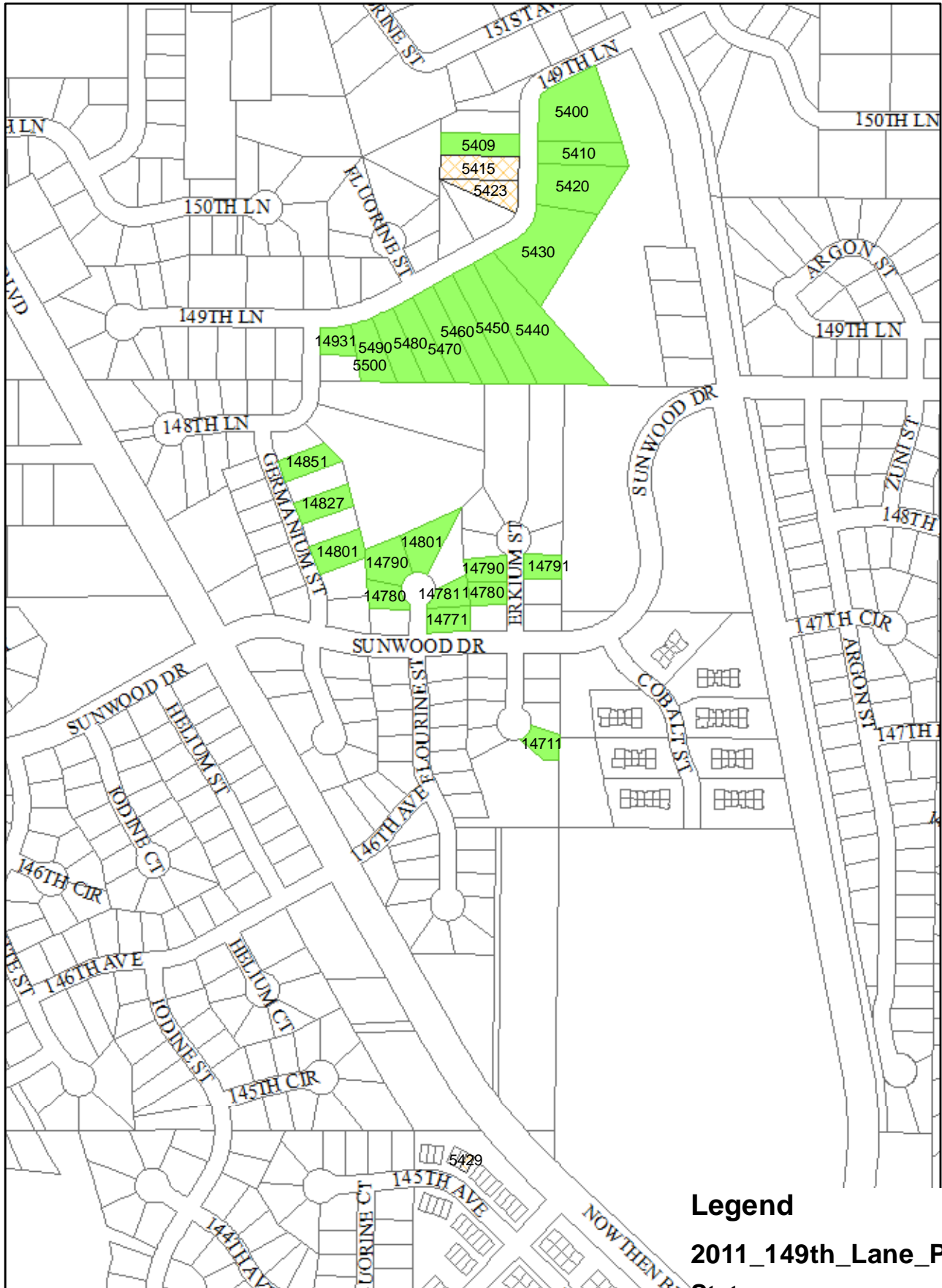
Email Address: bertz.chris@gmail.com

Street Address: 14781 Fluorine St NW

City: Ramsey State: MN Zip Code: 55303

Signature: Chris Bertz

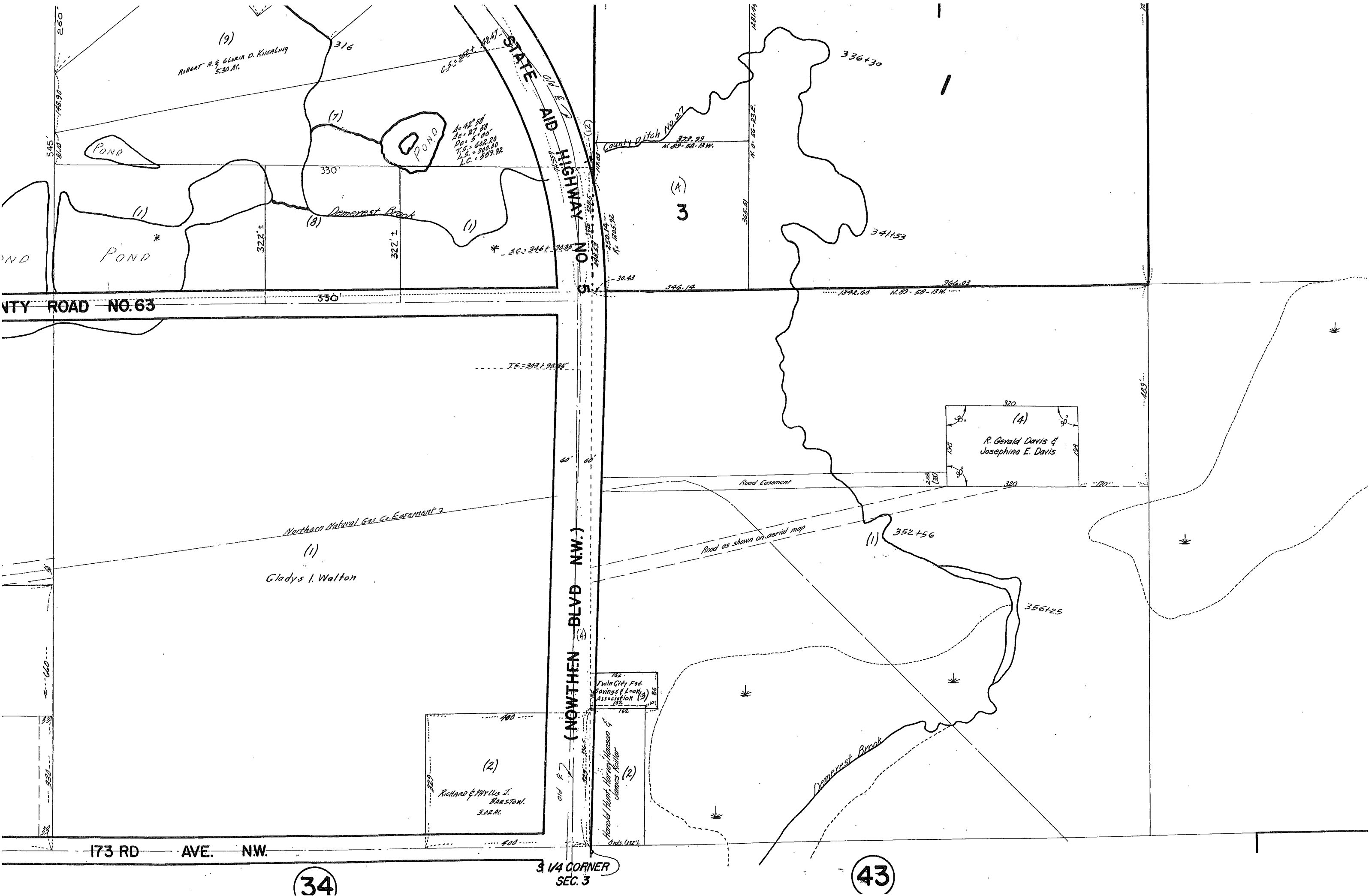
2011 149th Lane Petitioning Properties



Legend

2011_149th_Lane_Petition
Status

-  Occupant
-  Owner



(9)
ROBERT R. & GLORIA D. KEENLING
5.30 AC.

(7)
POND
A=42°58'
B=27°00'
C=5°00'
D=602.20
E=300.20
F.C.=557.92

(4)
3

(4)
R. Gerald Davis &
Josephine E. Davis

(2)
RICHARD & PHYLLIS J.
HANSON.
3.02 AC.

(2)
Twin City Fed.
Savings & Loan
Association (2)

(2)
Harold H. & Mary Hanson &
James Neillor
Divs. (12)

COUNTY ROAD NO. 63

STATE AID HIGHWAY NO. 63

NOWTHEN BLVD. NW.

173 RD AVE. NW.

34

3/4 CORNER
SEC. 3

43

**Dryden Excavating, Inc
6700 Viking Blvd NW
Anoka, MN 55303
Phone 763-753-7784
Fax 763-753-3247**

Date: 5/30/2012
To: Mike – City of Ramsey Engineering Dept
Project: 2012 Storm Sewer Improvements

DEDUCT

Item #10 Floation Silt Curtain-----	\$1008.00
Item #23 18" RCP-----	\$2461.00
Item #25 18" Bypass Line-----	\$2550.00
Item #30 FES 18"-----	\$850.00
TOTAL DEDUCT-----	\$6869.00

ADDITION

42" Bypass Line	
Lowboy-----	\$600.00
Backhoe-----	\$810.00
Dewatering-----	\$1500.00
Install Pipe and Remove Pipe-----	\$3840.00
TOTAL ADDITION-----	\$6750.00

Note that there is no charge for the 42" bypass pipe as we have it in our yard at this time. Due to the nature of the job there is a very good chance that some of this pipe will be broken during this process

**Randy Dryden
Dryden Excavating, Inc**

Meeting Date: 06/12/2012

Submitted For: Tim Himmer

By: Len Linton, Engineering/Public Works

Information**Title:**

Consider Scope of Services and Authorize the Preparation of Plans and Specifications for Stormwater Improvements at Rum River Hills Golf Course

Background:

Last summer the City experienced several significant rainfall events that lead to many localized flooding concerns, and resident complaints. The large volumes of precipitation that occurred over a short period of time appears to have elevated the groundwater within areas of the City, and prohibited the generous rate of infiltration that typically takes place in the Anoka sandplain. Throughout the summer and fall staff worked hard at registering and responding to the calls, and evaluating the situations on an individual basis to determine whether quick fixes could be implemented (culvert obstructions, re-ditching, etc.) to alleviate the immediate concerns.

This item was discussed at the Public Works Committee on August 15, 2011, and at that time staff summarized the areas of concern that were being investigated based upon citizen complaints received. A summary was presented that outlined the concern identified, actions steps to evaluate the concern, additional investigations that would be necessary to fully understand the situation, and recommendations for specific projects that could be implemented rather quickly and inexpensively. At that time staff classified the issues into 3 categories:

1. Those that required no further action. They were evaluated and corrected, or did not need correcting because the water was fully contained within dedicated drainage & utility easements (functioning as designed).
2. Those that required additional investigation and evaluation before deciding on a long term solution, and implementing corrective actions.
3. Those that had an identified recommendation for immediate action.

At that meeting the Committee briefly discussed the areas of concern, and directed staff to prepare plans and specifications for the items identified in category 3. Once this direction was ratified by the City Council on September 13, 2011, and the plans completed, it was too late in the season to secure bids and complete the improvements in 2011. It was then decided to delay construction until 2012, where some of the improvements could be completed by inclusion in a larger stormwater improvement plan set. Staff finalized plans for all these items, including incorporation of additional items that were approved at the Public Works Committee meeting in March of 2012, and bid them under two separate contracts, which were awarded by the City Council on May 22, 2012.

The final three areas of flooding concern from 2011 were discussed by the Public Works Committee on May 15, 2012, with the recommendation being to perform corrective actions on all three this year. These items were included in the report from Public Works on the consent agenda earlier this evening, and include the Rum River Hills Gold Course, 148th Lane NW, and 149th Lane NW. City staff has prepared plan sheets for the corrective actions contemplated for both 148th Lane and 149th Lane, which will be discussed by the Council as a separate case. This case is to discuss proposed improvements to the Rum River Hills Gold Course.

Notification:

The Golf Course representatives have been notified of the meeting and have received a copy of the agenda.

Observations:

In the fall of 2011, City staff contracted out the investigation of all the significant areas of flooding concern to Hakanson Anderson. The goal was to have an independent third party evaluate the areas, provide options for corrective actions, and associated estimates to implement the work. They completed their analysis and compiled a report that references potential solutions for each area identified (see attached final report). This report was introduced in general terms at the February 2012 Public Works Committee meeting, with the direction being that staff would come back before the committee over the next several months to discuss the merits of each potential improvement in more detail. This case will focus on section three of the report; the Rum River Hills Golf Club.

Rum River Hills Golf Club is located east of Trunk Highway 47 at 167th Avenue NW. Several areas were evaluated and summarized in the report. The golf course was platted in the early 1980's, and at that time linear drainage and utility easements were secured on the site to encumber areas where off-site stormwater was routed through the site. Many areas of the site experienced flooding concerns last summer, and they are summarized as follows:

Hole #1 (Area 1)

There is a large pond located adjacent to Hole #1, which has a 12" plastic pipe that serves as the outlet. This outlet pipe was televised after the flooding was brought to the attention of the City last year, and the video revealed that there were several sags in the line, and many joints have been compromised with root intrusion. This pond was evaluated with the City storm water model, and it was determined that the 12" pipe was the limiting factor and slightly undersized to accommodate the flows. Up-sizing this line to an 18" pipe, and installing a manhole at the outfall would improve the efficiency of the stormwater routing. A small area drain will also be incorporated into the storm sewer design along this pipe run to capture runoff from a low area at the bend point.

It should be noted that the City has always worked with the golf course on the outlet to this pond and previously installed a removable weir so they could control the flow leaving the pond with a removable weir. We are proposing to maintain this pond elevation adjustment for the golf course by installing a new manhole structure with a similar feature, as they currently utilize this pond as a course amenity and irrigation supply. The golf course likes to retain as much water as possible in the pond, installing the removable weir will provide the flexibility to retain small storm events and store the water while allowing larger events to pass through the system quickly.

Cart path area near the club house (Area 2A, B)

There is a pond north of the club house which is connected to the pond on hole #1. There are several cart paths around this smaller pond which experience localized flooding on occasion. By up sizing the existing undersized culvert connections we can allow more water to pass through the system quicker. The elevations on certain areas of the paths are below the maximum elevation of the pond. The paths would need to be raised if the intent is to not have water on the paths during the larger storm events, but doing so would have ramifications on other areas of the site upstream.

Saturated soil along Hole #15 (Area 3)

Hole #15 is along the Rum River, and there is a rock dam across the main drainage way from the golf course down to the river. This dam creates a ponding area adjacent to the fairway, and the water elevation of this pool is very close to the fairway grades, resulting in saturated soils in the area. The golf course excavated an area adjacent to the fairway last fall in an effort to determine the source of the water and dry the area up. Several draitile lines were uncovered by the excavation and they appeared to be plugged and collapsed in several locations.

Staff visited the site May 10th and observed the conditions in this area. The golf course has excavated a low area and swale on the edge of the 15th fairway. Golf course representatives said the low area had open water in it that flowed through the swale all winter, but the ponding area behind the dam froze over. This excavated low area has standing water that flows through the swale to the river. The water level behind the dam is approximately 1 foot below the adjacent fairway grades.

The recommendations for this area are two-fold; the golf course will undertake work that is outside of the drainage and utility easement (regrading and/or draitile installation), and the City will provide improvements within the drainage and utility easement. The golf course will monitor the low area this summer and are thinking about improving it into another water feature on the course.

They will also contact a draitile company to evaluate the installation of a new system along the fairway. They are also interested in removing sediment from behind the dam so that they can re-install a floating fountain. Staff will assist by researching state requirements for disposal of pond dredge material.

The City will evaluate options for changing the control structure on the rock dam. Currently there is an open top metal channel approximately 2 feet wide, which directs water over the top of the dam. Rocks are placed against the edges of the channel and as the depth increases water flows over the rocks. There is a metal dam approximately 70 yards upstream which has a flow width of 4 feet. The control structure on the dam should be widened to match the upstream structure, and the elevation of the top may be lowered.

Hole #3 and #17 (Area 4 A, B, C)

The fairways of Hole #3 and #17 also experienced flooding last summer. These areas receive stormwater runoff from an area north of the golf course. The water passes through several ponds connected by culverts, which form a water hazard on the course. The pipes have shifted over the years, and there are several locations where the upstream invert is lower than the downstream invert, thereby decreasing the capacity of the pipe and holding back flow. The golf course is also interested in dredging a pond in this location. The idea in this location is to replace the pipes with slightly larger pipes to increase flow, and re-establish the proper invert elevations. This work will require coordination with the golf course so that alternate routes around the work areas can be set up before construction begins.

Funding Source:

The funding source for these improvements is proposed to be the Stormwater Utility Fund. The opinion of probable construction costs, as prepared by Hakanson Anderson, are estimated at \$49,000, with a breakdown as follows:

Area 1 \$ 32,000
Area 2 \$ 7,000
Area 3 \$ 5,000
Area 4 \$ 5,000
Total \$ 49,000

There are also engineering costs associated with surveying, design, bidding, staking, inspections, and construction administration, and Hakanson Anderson has submitted a proposal for this work in an amount not to exceed \$8,500.

Staff Recommendation:

Staff recommends authorizing the proposed storm sewer improvements for the Rum River Hills Golf Club; including a contract with Hakanson Anderson for the preparation of plans and specifications, surveying, staking, inspections, and construction administration in an amount not to exceed \$57,500.

-and-

Authorization to solicit quotes to complete the improvements.

Committee Action:

Motion to authorize the proposed storm sewer improvements for the Rum River Hills Golf Club; including a contract with Hakanson Anderson for the preparation of plans and specifications, surveying, staking, inspections, and construction administration in an amount not to exceed \$57,500.

-and-

Motion to authorize soliciting quotes to complete the improvements.

Hakanson Anderson Report
Golf Course Drainage Map
Existing Dedicated Easements on Site
Proposed Improvements

Form Review

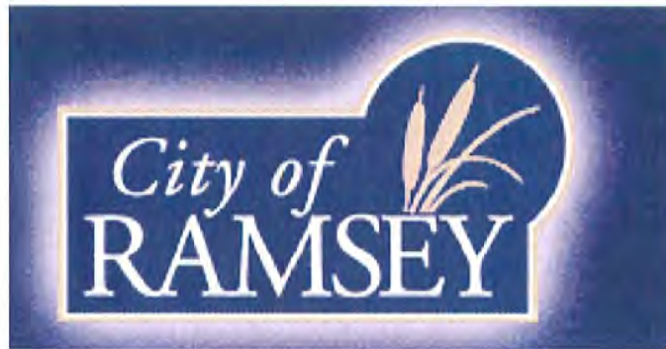
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Kurt Ulrich	Tim Himmer	06/06/2012 12:41 PM
Len Linton (Originator)	Len Linton	06/06/2012 12:43 PM
Tim Himmer	Tim Himmer	06/06/2012 12:46 PM
Kurt Ulrich	Kurt Ulrich	06/07/2012 02:41 PM
Form Started By: Len Linton		Started On: 06/04/2012

Final Approval Date: 06/07/2012

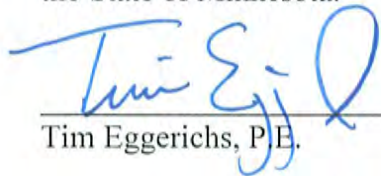
INVESTIGATION OF 2011 FLOODING CONCERNS

FOR THE

City of Ramsey
Anoka County, Minnesota



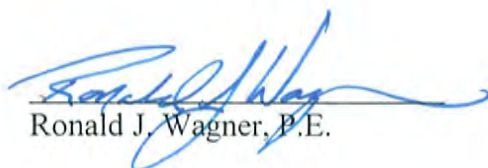
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.



Tim Eggerichs, P.E.

43362
License No.

February 14, 2012
Date



Ronald J. Wagner, P.E.

26052
License No.

February 14, 2012
Date

TABLE OF CONTENTS

SUMMARIES AND RECOMMENDATIONS

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SECTION 4	163 rd Lane
SECTION 5	156 th Lane
SECTION 6	Sodium Street

SUMMARIES AND RECOMMENDATIONS

148th Lane

There is an existing low area on the south side of 148th Lane. During the wet spring of 2011, stormwater would pond for extended periods in this low area. A 15" outlet exists approximately 2.5 feet above the bottom of the low area that drains the area to the east. The stormwater below the outlet pipe infiltrates into the soil. A drainage easement exists over this low area.

Different sized outlet pipes at different elevations were analyzed. These different sized outlets did not have a significant impact on the high water level in the area. Since the low area is within an existing drainage easement, it is our recommendation that nothing be changed in the area.

149th Lane

There is an existing low area on the east side of Lot 3, Block 1 of Ramsey Meadows 4th Addition (5410 149th Lane NW). This low area is connected to the large DNR Wetland to the east with a drain tile. During the wet spring of 2011, stormwater would back up through this drain tile and flooded the low area. It appears that this low area may have been constructed as part of a wetland mitigation plan and may be controlled by Wetland Conservation Act rules. A drainage easement exists over this low area.

The outlet from the DNR Wetland, which drains east under Trunk Highway 47 (TH 47), was analyzed. The current outlet has a weir structure that is approximately 0.8 feet above the invert of the 15" pipe that crosses TH 47. Additional culverts under TH 47 were analyzed, but the additional outlets did not have a significant impact on the high water levels in the DNR Wetland. Removing the weir structure was also analyzed.

It is our recommendation that the weir structure be removed from the outlet. This will not have a significant impact on the high water level of the wetland; however, the wetland will drain down to an elevation near the elevation of the bottom of the low area at 5410 149th Lane. The estimated cost to remove the weir structure is \$1,265.

Rum River Hills Golf Club

Rum River Hills Golf Club has been experiencing flooding issues throughout the golf course. The large ponds near the clubhouse drain to the east through an existing outlet structure and 12" pipe. This stormwater then drains over an existing steel weir structure and over a rock dam prior to discharging to the Rum River.

Area 1

The ponds near the clubhouse have been flooding and the outlet does not seem to drain the ponds effectively. The existing 12" outlet from the large ponds near the clubhouse has several sags in it and several joints have been compromised.

It is our recommendation to replace this pipe and install a new outlet structure with removable planks that will give the golf course more flexibility in controlling the water

elevation in the ponds. This includes an 18" HDPE outlet and precast concrete outlet structure with a removable grate for access to the planks. It is proposed to leave the overflow at the same elevation as existing, which will have minimal impacts on the elevation of the standing water and the high water levels of the ponds. The estimated cost to install this new outlet is \$29,853.

Area 2

Based on the high water levels in the ponds near the clubhouse, it appears flooding of the cart paths near Hole #1 may be an issue during large storm events. The existing culverts consist of a 12" CMP and 15" HDPE.

To reduce the frequency of the flooding, we recommend installing 24" diameter culverts under the two existing cart paths. The paths would continue to be inundated during storm events greater than 3.5", but would not be inundated during storm events less than 3.5". The estimated cost to replace the culverts is \$6,642. If the golf course does not feel that the cart path flooding is a concern, replacing the culverts is not a necessity.

Area 3

The soil in the fairway of Hole #15 has become saturated. There is an existing rock dam southeast of this fairway and it appears that water being contained by the rock dam may be infiltrating into the soil and saturating the fairway.

We recommend lining the creek upstream of the rock dam with an impermeable liner and replacing and/or installing new drain tile in the fairway of Hole #15. Lining the creek will eliminate the infiltration into the soil and the drain tile will keep the soil from becoming saturated. The estimated cost for these improvements is \$13,530.

Area 4

Holes #3 and #17 have had flooding issues. The ponds and swales near the two holes are drained through three 15" CMP culverts.

Different sized culverts at different elevations were analyzed. These different sized culverts did not have a significant impact on the high water levels in the area. Without lowering the entire swale and creating more storage, it does not appear that replacing the culverts would have a significant impact. It is our recommendation that the three culverts not be replaced.

163rd Lane

There is an existing, landlocked low area south of 163rd Lane and east of Wolfram Street. During a majority of the year, stormwater runoff infiltrates into the soil. However, during early spring when the ground is frozen and during periods of heavy rainfall, water levels have risen to levels that cause flooding of adjacent properties.

Different sized outlet pipes were analyzed to drain the low area. Installing an outlet will have a significant impact on the high water levels in the area and, most importantly, the length of inundation will be greatly reduced. We recommend directionally drilling an

18" HDPE pipe south to County Ditch #3 with an invert elevation of 872.0. Installing the pipe at an elevation of 872.0 will continue to allow 3" storm events to infiltrate into the soil. The estimated cost to install this outlet is \$80,795.

156th Lane

During large storm events, the wetland in Woodland Green Park ponds water in the backyard of 5220 156th Lane. The area where water ponds was platted with a 75-foot drainage and utility easement, however, this easement has been vacated. The wetland discharges east through an existing storm sewer system to a low area and then north through another storm sewer system to the Rum River.

Different sized ponds and outlet configurations were analyzed. One alternative to reduce the high water level of the wetland in Woodland Green Park included constructing a new outlet pipe from the wetland to the low area to the east. This alternative would also require constructing a new outlet at a lower elevation from the low area directly to the Rum River. This would require the approval of the Minnesota Department of Natural Resources and does not appear viable at this time.

We recommend filling the backyard of 5220 156th Lane to an elevation equal to the 100-year high water level. Stormwater would then be contained within Woodland Green Park and would not impact this homeowner. Filling the backyard would require grading in the Woodland Green Park site to create storage to compensate for the lost volume. The estimated cost to complete the grading is \$21,175.

Sodium Street

The house at 16756 Sodium Street has been experiencing water in the basement. One reason for having water in the basement could be caused by the road ditch in front of the house filling and then overflowing toward the house. The water then seeps along the basement wall and eventually into the basement. Another reason for water in the basement could be caused by a high groundwater elevation in the area.

We recommend installing a culvert under Sodium Street, regrading the west ditch of Sodium Street, constructing a berm to keep the water in the ditch, and replacing the existing driveway culvert. The ditch would be graded to drain south and a culvert would be installed near the south property line to drain the water to the west. The estimated cost to complete this project is \$12,225.

Section 1
148th Lane

148th Lane

Description

As shown on Exhibit 1, a low area exists south of 148th Lane. A 15" outlet pipe drains the low area north and then east to DNR Wetland 658W. The invert of the outlet pipe is at elevation 862.3 and the bottom of the low area is at elevation 859.8. Water in the low area has to rise 2.5 feet prior to discharging. The water below the outlet infiltrates into the soil, which may take days depending on the condition of the soil.

The existing 100-year high water level (HWL) for the low area is 865.5. As shown on Exhibit 2, a drainage easement exists in the rear of Lots 2 through 5, Block 2 of Ramsey Commons 2nd Addition.

Alternatives

The following alternatives address the water elevation in the existing low area.

Alternative 1

In this alternative, a new outlet pipe would be installed between Lot 1 of Ramsey Commons 2nd Addition and Lot 2 of Sunny Ponds, as shown on Exhibit 3. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.6. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.4	\$16,264
18"	865.1	\$31,566 *
24"	864.3	\$45,381 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

Tables 1 through 3 include the individual costs for this alternative.

Alternative 2

In this alternative, a new outlet pipe would be installed between Lots 2 and 3 of Sunny Ponds, as shown on Exhibit 4. It was assumed that the existing outlet pipe to the north would be removed. By installing an outlet pipe in this location, the invert of the outlet pipe can be lowered from 862.3 to 861.4. This alternative would require additional grading and the acquisition of a permanent easement on the property south of Ramsey Commons 2nd Addition and west of Sunny Ponds. The following table summarizes the 100-year HWL's and estimated costs for each outlet pipe:

Outlet Pipe Size	100-Year HWL	Estimated Cost
Existing	865.5	N/A
15"	865.3	\$13,098
18"	865.0	\$18,846 *
24"	864.3	\$32,543 *

* The existing pipe being connected to in Germanium Street is a 15" diameter. Installing the 18" and 24" outlets will require the removal and replacement of this pipe to match the size of the pipe being installed.

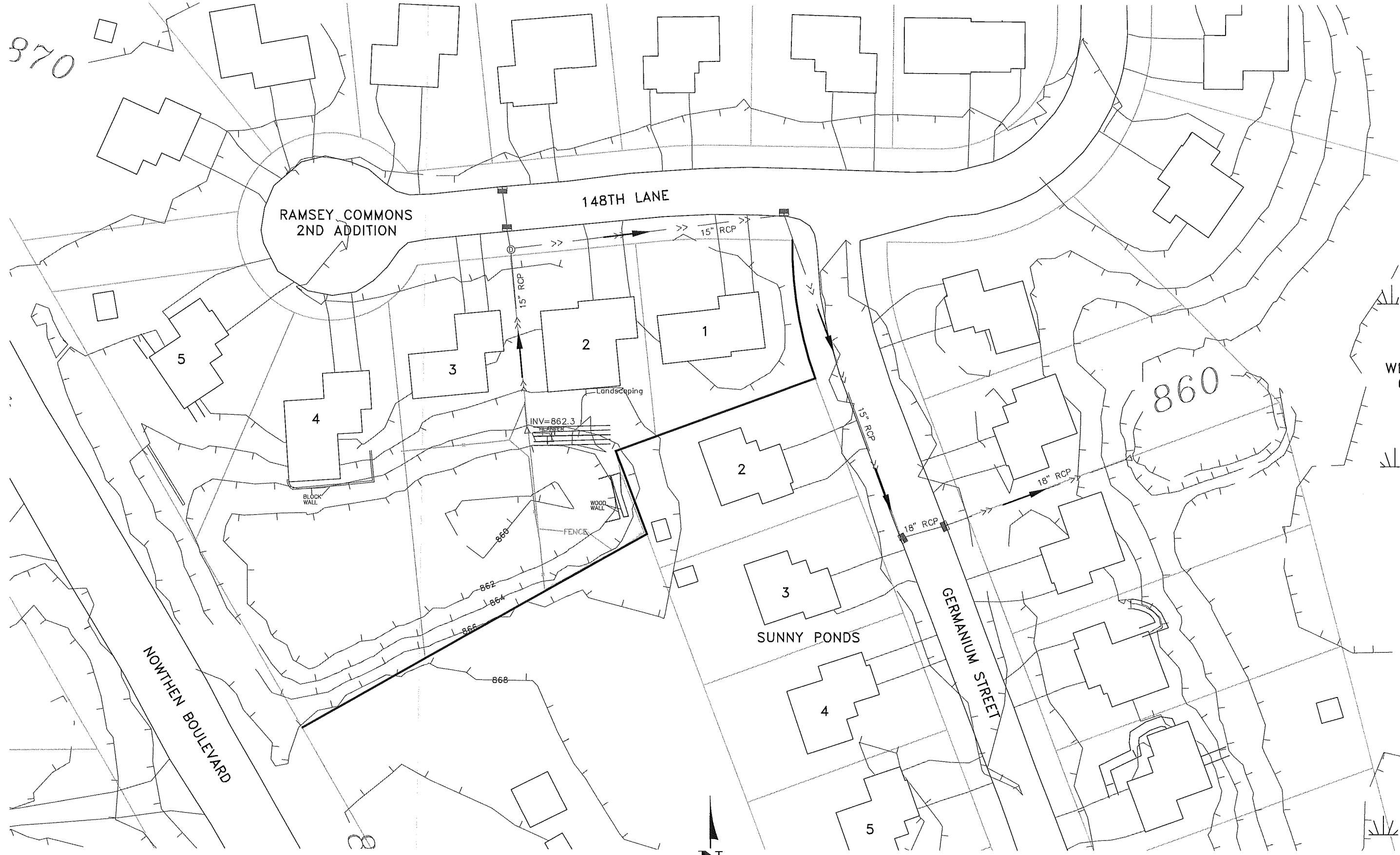
Tables 4 through 6 include the individual costs for this alternative.

Alternative 3

In this alternative, the bottom of the low area would be filled to the same elevation as the outlet pipe invert. The pond would then drain dry and would not be sitting with water until it infiltrated. By filling the bottom of the low area, the resultant 100-year HWL will be 866.2, 0.7 feet higher than the existing HWL. This higher HWL would end up outside the existing drainage and utility easement, creating the need for additional drainage easement.

The estimated cost to fill the low area is \$15,321. Table 7 includes the individual costs for this alternative.

Further research is required to determine if this low area was designed to treat a water quality volume. The volume required would dictate if Alternatives 1 and 2 are viable options. Alternative 3 would not be viable, because the water quality volume is being eliminated in this alternative.



870

RAMSEY COMMONS
2ND ADDITION

148TH LANE

DNR
WETLAND
658W

860

SUNNY PONDS

GERMANIUM STREET

NORTHEN BOULEVARD

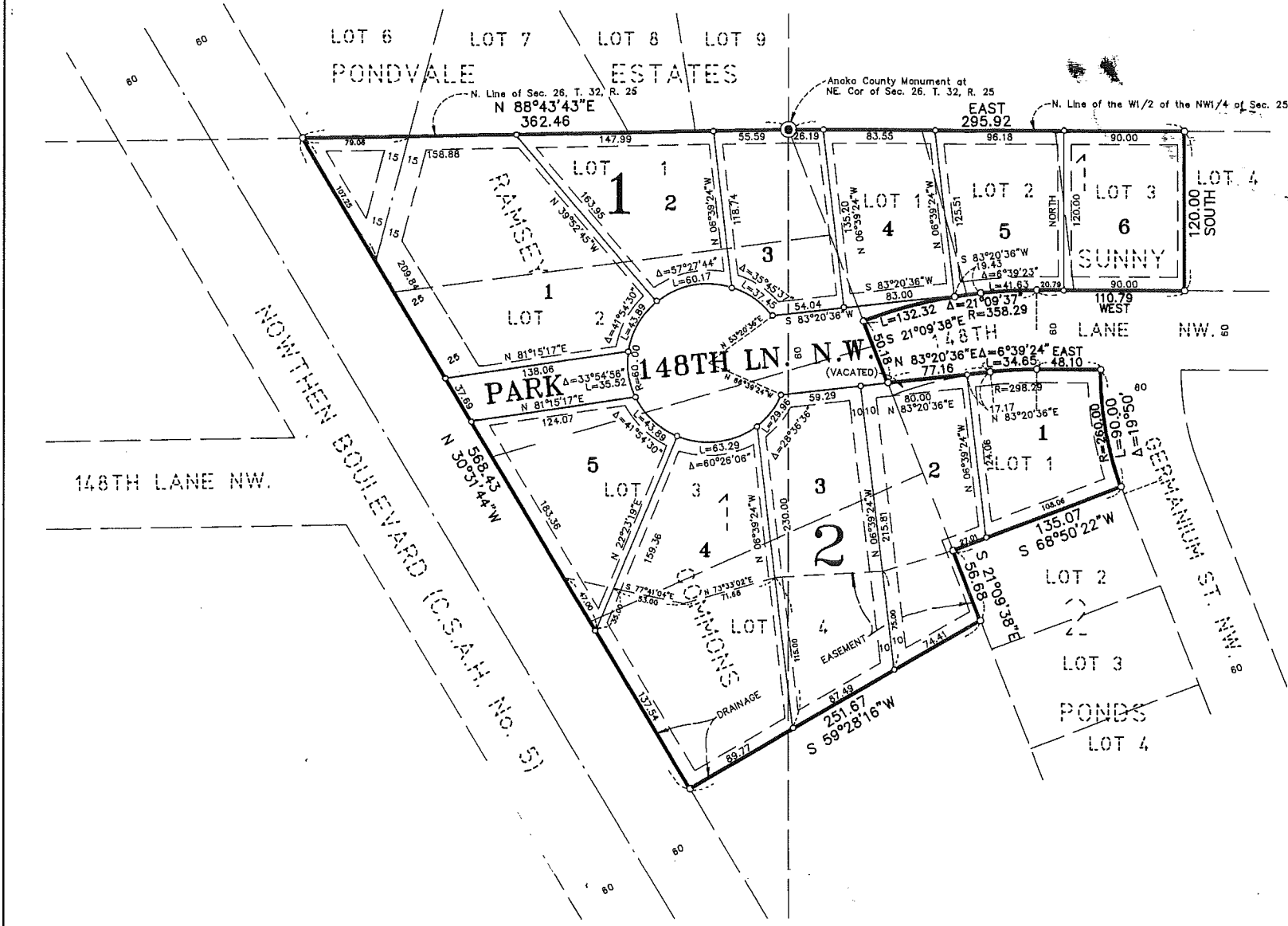


EXHIBIT 1
148TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY COMMONS 2ND ADDITION

CITY OF RAMSEY

COUNTY OF ANOKA



KNOW ALL PERSONS BY THESE PRESENTS: That North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Dolores S. Fleischer, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2 and 3, Block 1, and Lot 1, Block 2, all in SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota,
And that part of vacated 148th Lane N.W., as dedicated in the plat of SUNNY PONDS, according to the recorded plat thereof, Anoka County, Minnesota, lying north of the north line of Lot 1, Block 2, in said SUNNY PONDS and lying south of the following described line:

Commencing at the northeast corner of said Lot 1, Block 2; thence on an assumed bearing of West, along the north line of said Lot 1, Block 2, a distance of 48.10 feet; thence westerly continuing along said north line and along a tangential curve, concave to the south having a radius of 298.29 feet and a central angle of 6 degrees 39 minutes 24 seconds, a distance of 34.65 feet to the point of beginning of the line to be described; thence South 83 degrees 20 minutes 36 seconds West, a distance of 77.16 feet to the intersection with the northerly extension of the west line of said Lot 1, Block 2, and said line there terminating.

AND that North Suburban Development, Inc., a Minnesota Corporation, owner and proprietor, and Delano Skeim, single, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

Lots 1, 2, 3 and 4, Block 1, RAMSEY COMMONS, according to the recorded plat thereof, Anoka County, Minnesota.
Have caused the same to be surveyed and platted as RAMSEY COMMONS 2ND ADDITION and do hereby donate and dedicate to the public for public use forever the lane/as shown on the plat. Also dedicating the drainage and/or utility easements as shown on the plat. Also dedicating to the County of Anoka the right of access onto County State Aid Highway No. 5 from Lot 1, Block 1 and from Lots 4 and 5, Block 2. In witness whereof said North Suburban Development, Inc. has caused these presents to be signed by its proper officer this 26th day of MAY, 1993. Also in witness whereof said Dolores S. Fleischer has hereunto set her hand this 26th day of MAY, 1993. Also in witness whereof said Delano Skeim has hereunto set his hand this 26th day of MAY, 1993.

NORTH SUBURBAN DEVELOPMENT, INC.
[Signature]
A. Henkveld, as President

SIGNED:
[Signature]
Dolores S. Fleischer

SIGNED:
[Signature]
Delano Skeim

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 26th day of MAY, 1993, by J. A. COUNTY OF ANOKA) Henkveld, President of North Suburban Development, Inc., a Minnesota corporation, on behalf of the corporation.

SHIRLEY D. CHENOWETH
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 26th day of MAY, 1993, by Dolores COUNTY OF ANOKA) S. Fleischer, single.

SHIRLEY D. CHENOWETH
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires 6-24-96

[Signature]
Notary Public, ANOKA County, Minnesota
My Commission expires 6-24-96

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this 21st day of May, 1993, by Delano COUNTY OF ANOKA) Skeim, single.

MARGARET A. McINERNEY
NOTARY PUBLIC-MINNESOTA
ANOKA COUNTY
My Commission Expires FEB. 22, 1998

[Signature]
Notary Public, Anoka County, Minnesota
My Commission expires 2/22/96

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY COMMONS 2ND ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wet lands or public highways to be designated on said plat other than as shown thereon.

[Signature]
Jeffrey N. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyors certificate was acknowledged before me a Notary Public, this 19th day of May, 1993, COUNTY OF ANOKA) by Jeffrey N. Caine, Land Surveyor.

MOLLY W. CAINE
Notary Public-Minnesota
Anoka County
My Commission Expires 6-13-96

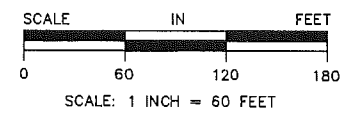
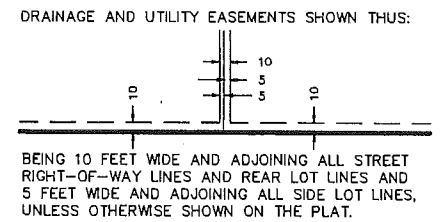
[Signature]
Notary Public, Anoka County, Minnesota
My Commission expires 6-13-96

CITY OF RAMSEY
We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY COMMONS 2ND ADDITION at a regular meeting held this 11th day of May, 1993. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 509.03, Subd. 2.
By *[Signature]* Mayor By *[Signature]* Clerk

Checked and approved this 29th day of June, 1993

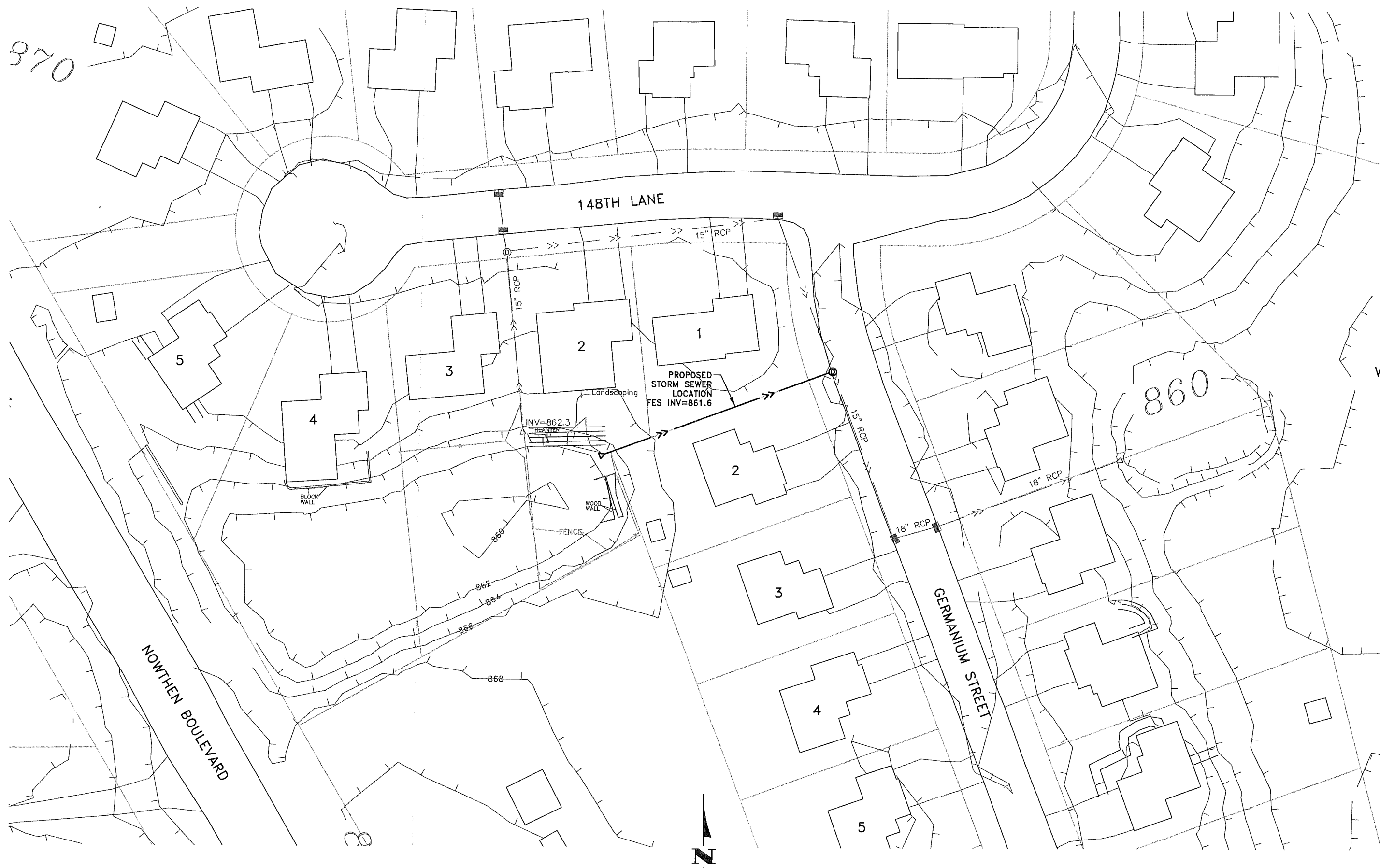
By *[Signature]*
Anoka County Surveyor

EXHIBIT 2.
FINAL PLAT-RAMSEY COMMONS 2ND ADD.
CITY OF RAMSEY, MINNESOTA



NOTE: DENOTES 1/2 INCH IRON PIPE SET.
DENOTES ANOKA COUNTY MONUMENT.
FOR THE PURPOSES OF THIS PLAT, THE NORTH LINE OF THE W1/2 OF THE NW1/4 OF SEC. 25, T. 32, R. 25 IS ASSUMED TO HAVE A BEARING OF EAST.

CAINE & ASSOCIATES
LAND SURVEYORS, INC.



870

860

DNR
WETLAND
658W

NORTHEN BOULEVARD

GERMANIUM STREET

148TH LANE

PROPOSED
STORM SEWER
LOCATION
FES INV=861.6

INV=862.3

Landscaping

FENCES

WOOD WALL

BLOCK WALL

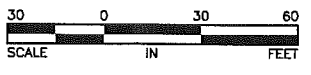


EXHIBIT 3
148TH LANE ALTERNATIVE 1 STORM SEWER
CITY OF RAMSEY, MINNESOTA



DNR
WETLAND
658W

NORTHEN BOULEVARD

GERMANIUM STREET

148TH LANE

860

PROPOSED
PERMANENT
DRAINAGE
EASEMENT

PROPOSED
STORM SEWER
LOCATION
FES INV=861.4

INV=862.3

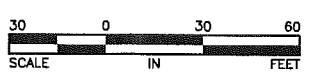


EXHIBIT 4
148TH LANE ALTERNATIVE 2 STORM SEWER
CITY OF RAMSEY, MINNESOTA

**TABLE 1
148TH LANE
ALTERNATIVE 1A - 15" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	15" RC PIPE APRON	EACH	\$300.00	1	\$300
12	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
17	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$14,786
Contingency (10%)	\$1,479
Total Estimated Construction Cost	<u>\$16,264</u>

**TABLE 2
148TH LANE
ALTERNATIVE 1B - 18" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$5.00	233	\$1,165
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	135	\$1,350
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	150	\$750
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	155	\$465
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	150	\$1,125
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	150	\$4,200
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	18" RC PIPE APRON	EACH	\$350.00	1	\$350
12	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
14	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	267	\$6,141
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	135	\$2,700
17	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
18	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
19	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$28,696
Contingency (10%)	\$2,870
Total Estimated Construction Cost	<u>\$31,566</u>

TABLE 3
148TH LANE
ALTERNATIVE 1C - 24" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,300.00	1	\$1,300
2	CLEARING	TREE	\$100.00	12	\$1,200
3	GRUBBING	TREE	\$100.00	12	\$1,200
4	REMOVE STORM SEWER	LIN FT	\$4.00	388	\$1,552
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	155	\$775
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	195	\$975
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	175	\$525
8	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	195	\$1,463
9	4" BITUMINOUS PATCH	SQ YD	\$28.00	195	\$5,460
10	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
11	24" RC PIPE APRON	EACH	\$450.00	2	\$900
12	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
13	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
14	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	414	\$12,006
15	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	155	\$3,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	12	\$2,400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$41,256
Contingency (10%)	\$4,126
Total Estimated Construction Cost	<u>\$45,381</u>

TABLE 4
148TH LANE
ALTERNATIVE 2A - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	118	\$590
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	20	\$200
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	23	\$115
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	40	\$120
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	23	\$173
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	23	\$644
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	15" RC PIPE APRON	EACH	\$300.00	1	\$300
13	TRASH GUARD FOR 15" PIPE APRON	EACH	\$150.00	1	\$150
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	15" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	152	\$3,344
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	20	\$400
18	TRAFFIC CONTROL	LUMP SUM	\$300.00	1	\$300
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$11,186
Contingency (10%)	\$1,119
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$13,098</u>

TABLE 5
148TH LANE
ALTERNATIVE 2B - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$5.00	153	\$765
5	REMOVE CONCRETE CURB	LIN FT	\$10.00	55	\$550
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	61	\$305
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	75	\$225
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	61	\$458
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	61	\$1,708
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	18" RC PIPE APRON	EACH	\$350.00	1	\$350
13	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
15	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	187	\$4,301
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	55	\$1,100
18	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
19	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
20	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$16,412
Contingency (10%)	\$1,641
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$18,846</u>

TABLE 6
148TH LANE
ALTERNATIVE 2C - 24" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$900.00	1	\$900
2	CLEARING	TREE	\$100.00	2	\$200
3	GRUBBING	TREE	\$100.00	2	\$200
4	REMOVE STORM SEWER	LIN FT	\$4.00	306	\$1,224
5	REMOVE CONCRETE CURB	LIN FT	\$5.00	75	\$375
6	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	106	\$530
7	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	95	\$285
8	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
9	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	106	\$795
10	4" BITUMINOUS PATCH	SQ YD	\$28.00	106	\$2,968
11	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
12	24" RC PIPE APRON	EACH	\$450.00	2	\$900
13	TRASH GUARD FOR 24" PIPE APRON	EACH	\$300.00	2	\$600
14	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
15	24" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$29.00	334	\$9,686
16	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	1	\$1,500
17	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	2	\$4,000
18	CONCRETE CURB & GUTTER DESIGN B618	LIN FT	\$20.00	75	\$1,500
19	TRAFFIC CONTROL	LUMP SUM	\$500.00	1	\$500
20	CONIFEROUS TREE 4' HT B&B	TREE	\$200.00	2	\$400
21	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$28,863
Contingency (10%)	\$2,886
Permanent Easement (\$1.15/ sq ft)	\$794
Total Estimated Construction Cost	<u>\$32,543</u>

**TABLE 7
148TH LANE
ALTERNATIVE 3**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	CLEARING	TREE	\$100.00	10	\$1,000
3	GRUBBING	TREE	\$100.00	10	\$1,000
4	COMMON EXCAVATION	CU YD	\$5.00	270	\$1,350
5	GRANULAR BORROW	CU YD	\$8.00	1166	\$9,328
6	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$13,928
Contingency (10%)	\$1,393
Total Estimated Construction Cost	<u>\$15,321</u>

Section 2
149th Lane

149th Lane

Description

As shown on Exhibit 5, a low area exists on Lot 3, Block 1 of Ramsey Meadows 4th Addition. There is a drain tile that drains the low area to DNR Wetland 658W. During large storm events, DNR Wetland 658W backs up through the drain tile and into the low area. DNR Wetland 658W drains east under Trunk Highway 47 (TH 47) through a small weir structure and 15" pipe to DNR Wetland 659W. The weir structure is at an elevation of 860.9 and drains to the 15" pipe at an elevation of 860.0.

The current outlet elevation is approximately two feet above the bottom of Wetland 658W. The existing 100-year high water level (HWL) for DNR Wetland 658W is 862.1 and for DNR Wetland 659W is 860.4. As shown on Exhibit 6, there is an existing drainage and utility easement over a majority of Block 1 of Ramsey Meadows 4th Addition including the low area in question.

Alternatives

The following alternatives address the water elevation in DNR Wetland 658W.

Alternative 1

Alternative 1 will include removing the existing weir structure and leaving only the 15" culvert as the outlet from DNR Wetland 658W. By removing the weir structure, the wetland will begin to discharge at an elevation of 860.0 as opposed to 860.8.

Removing the weir structure, resulting in a lower normal water level in the wetland, will result in a 100-year HWL for DNR Wetland 658W of 862.0. The 100-year HWL for DNR Wetland 659W did not change. The estimated cost to remove the weir structure is \$1,265. Table 8 includes the individual costs for this alternative.

It is our understanding that any work proposed below an elevation of 860 will need the approval of the Minnesota Department of Natural Resources (DNR). This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. It appears the weir structure is part of the TH 47 storm sewer system. Removing the weir structure may require Mn/DOT's approval.

Alternative 2

Alternative 2 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at the same elevation as the existing outlet, 860.0. It was assumed the additional pipe would have to be jacked under TH 47.

Three different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + 15"	861.8	861.2	\$15,290
Existing + 18"	861.7	861.3	\$18,755
Existing + 24"	861.6	861.4	\$29,040

Tables 9 through 11 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative will not have any effect below an elevation of 860 and is not anticipated to need the approval of the DNR. Removing the weir structure and constructing a culvert under TH 47 will require Mn/DOT's approval.

Alternative 3

Alternative 3 will include removing the existing weir structure and constructing an additional outlet from DNR Wetland 658W under TH 47. The proposed additional outlet would be at an elevation of 859.0, one foot lower than the existing outlet. It was assumed the additional pipe would have to be jacked under TH 47.

Two different sized additional outlet pipes were analyzed. The following table summarizes the 100-year HWL's for DNR Wetlands 658W and 659W and the estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL (658W)	100-Year HWL (659W)	Estimated Cost
Existing	862.1	860.4	N/A
Existing + lower 18"	861.6	861.3	\$18,755
Existing + lower 24"	861.6	861.5	\$29,040

Tables 12 and 13 include the individual costs for this alternative.

As mentioned above, it is our understanding that any work proposed below an elevation of 860 will need the approval of the DNR. This alternative has work proposed below an elevation of 860 and will need the approval of the DNR. Removing the weir structure and constructing a culvert under Trunk Highway 47 will require Mn/DOT's approval.

Alternative 4

Alternative 4 will include filling the low area on Lot 3, Block 1 of Ramsey Meadows 4th Addition. Filling the low area will reduce the frequency of the backyard flooding. During large storm events, the area may be inundated by water, but will likely be less frequently and for a shorter duration than under existing conditions.

The estimated cost to fill the low area is \$4,681. Table 14 includes the individual costs for this alternative.

As shown on Exhibits 7 and 8, this low area appears to have been designed as wetland mitigation area. Further research may be necessary to determine if this area is protected by the Wetland Conservation Act (WCA). If the area is protected by the WCA, filling this low area will not be a viable alternative.

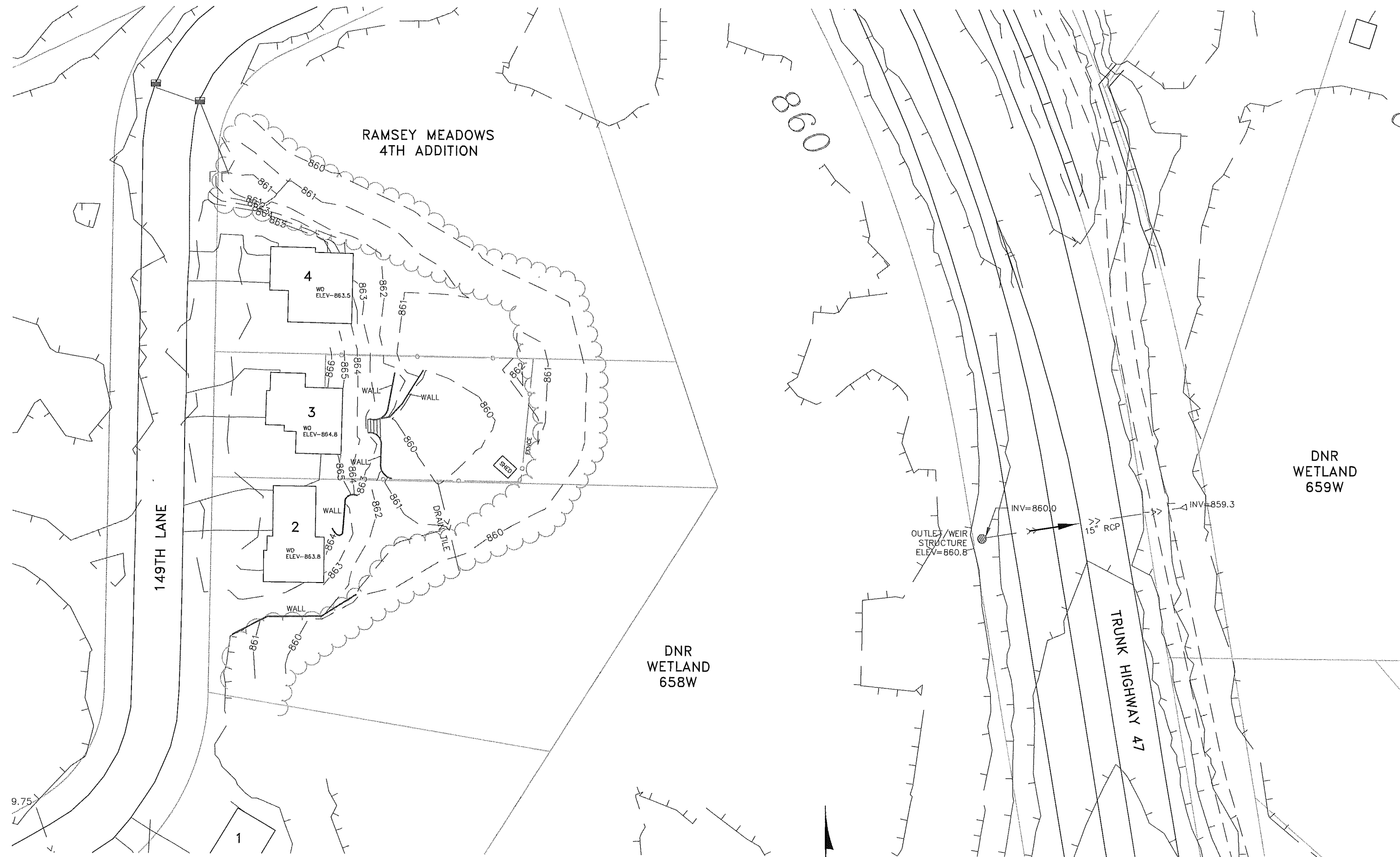


EXHIBIT 5
149TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

RAMSEY MEADOWS 4TH ADDITION

CITY OF RAMSEY COUNTY OF ANOKA

pg 25

KNOW ALL PERSONS BY THESE PRESENTS: That J. A. Menkveld & Associates, Inc., a Minnesota corporation, owner and proprietor and Builders Mortgage Corporation, a Minnesota corporation, mortgagee of the following described property situated in the County of Anoka, State of Minnesota, to-wit:

That part of the Southwest Quarter of Section 24, Township 32, Range 25, Anoka County, Minnesota, described as follows:

Beginning at the northeast corner of Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota; thence South 89 degrees 07 minutes 51 seconds East, assumed bearing, parallel with the south line of said Southwest Quarter, a distance of 208.50 feet to the center line of State Trunk Highway No. 47, per the plat of AMBER RIDGE, according to the recorded plat thereof, Anoka County, Minnesota; thence northerly along said center line and along the center line of said State Trunk Highway No. 47, per the plat of WILLOW RIDGE, according to the recorded plat thereof, Anoka County, Minnesota, a distance of 789.96 feet to the intersection with the northeasterly extension of the following described line:

Beginning at a point on the center line of State Trunk Highway No. 47, per the plat of GORHAM'S ADDITION, according to the recorded plat thereof, Anoka County, Minnesota, said point being distant 93.00 feet southeasterly of the northeasterly extension of the southeasterly line of Block 3, said GORHAM'S ADDITION, as measured along said center line; thence South 64 degrees 18 minutes West, parallel with the southeasterly line of Block 3, said GORHAM'S ADDITION, a distance of 376.78 feet, and said line there terminating;

thence South 64 degrees 18 minutes 00 seconds West, along said last described line, a distance of 376.83 feet to the point of termination of said line; thence southwesterly along a tangential curve concave to the southeast, having a radius of 103.25 feet and a central angle of 63 degrees 14 minutes 20 seconds, a distance of 113.96 feet; thence South 1 degree 03 minutes 40 seconds West, tangent to said curve, a distance of 345.42 feet; thence southwesterly along a tangential curve concave to northwest, having a radius of 115.75 feet and a central angle of 60 degrees 08 minutes 15 seconds, a distance of 121.49 feet; thence South 61 degrees 11 minutes 55 seconds West, tangent to said curve, a distance of 53.09 feet to the intersection with the westerly extension of the north line of said Outlot A, RAMSEY MEADOWS 3RD ADDITION; thence South 87 degrees 06 minutes 09 seconds East, along said north line and its westerly extension, a distance of 517.62 feet to the point of beginning.

AND

Outlot A, RAMSEY MEADOWS 3RD ADDITION, according to the recorded plat thereof, Anoka County, Minnesota.

Have caused the same to be surveyed and platted as RAMSEY MEADOWS 4TH ADDITION and do hereby dedicate to the public for public use forever the boulevard, lane and drainage and utility easements as shown on the plat. In witness whereof said J. A. Menkveld & Associates, Inc., a Minnesota corporation, has caused these presents to be signed by its proper officer this 7th day of OCT, 1996. Also in witness whereof said Builders Mortgage Corporation has caused these presents to be signed by its proper officer this 7th day of OCT, 1996.

SIGNED:

J. A. MENKVELD & ASSOCIATES, INC.:

J. A. Menkveld
J. A. Menkveld, President



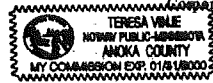
CAINE & ASSOCIATES
LAND SURVEYORS, INC.

1275655
OFFICE OF COUNTY RECORDS
STATE OF MINNESOTA, COUNTY OF ANOKA
I hereby certify that the within instrument was filed in this office for record on the 7th day of MAY, 1997.
Book block AM, and was duly recorded in book 54, page 25.
Edward M. Truka
County Recorder
By *KHG*
Deputy

BUILDERS MORTGAGE CORPORATION:

Ronald Stratton
Ronald Stratton, as President

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by J. A. Menkveld, President of J. A. Menkveld & Associates, Inc., a Minnesota Corporation, on behalf of the Corporation.



Teresa Vinje
Notary Public, Anoka County, Minnesota
My Commission expires 1-31-00

STATE OF MINNESOTA) The foregoing instrument was acknowledged before me this COUNTY OF ANOKA) 7th day of October, 1996, by Ronald Stratton, President of Builders Mortgage Corporation, a Minnesota corporation, on behalf of the corporation.

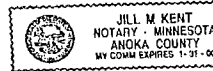


Kathleen R. Caven
Notary Public, Ramsey County, Minnesota
My Commission expires 1-31-2000

I hereby certify that I have surveyed and platted the land described in the dedication on this plat as RAMSEY MEADOWS 4TH ADDITION; that the plat is a correct representation of said survey; that all distances are correctly shown on said plat in feet and hundredths of a foot; that the monuments have been correctly placed in the ground as shown; that the outside boundaries are correctly designated on said plat; and that there are no wetlands or public highways to be designated on said plat other than as shown thereon.

Jeffrey N. Caine
Jeffrey N. Caine, Registered Land Surveyor
Minnesota Registration No. 12251

STATE OF MINNESOTA) The surveyors certificate was acknowledged before me a Notary COUNTY OF ANOKA) Public, this 12th day of October, 1996, by Jeffrey N. Caine, Land Surveyor.



Jill M. Kent
Notary Public, Anoka County, Minnesota
My Commission expires 01-31-00

CITY OF RAMSEY

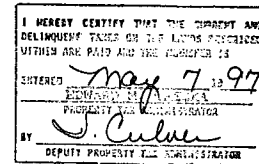
We hereby certify that the City Council of the City of Ramsey, Anoka County, Minnesota, duly accepted and approved the plat of RAMSEY MEADOWS 4TH ADDITION at a regular meeting held this 24th day of September, 1996. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the city or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 505.03, Subd. 2.

By *Ryan R. Schneider* Mayor By *Ryan R. Schneider* Clerk

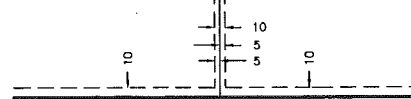
Checked and approved this 7th day of MAY, 1997.

By *Merlyn D. Anderson*
Anoka County Surveyor

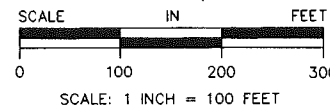
By *Larry S. Ham* deputy



DRAINAGE AND UTILITY EASEMENTS SHOWN THUS:



BEING 10 FEET WIDE AND ADJOINING ALL STREET RIGHT-OF-WAY LINES AND REAR LOT LINES AND 5 FEET WIDE AND ADJOINING ALL SIDE LOT LINES, UNLESS OTHERWISE SHOWN ON THE PLAT.



- DENOTES IRON MONUMENT FOUND.
 - DENOTES 1/2 INCH IRON PIPE SET.
 - ◎ DENOTES ANOKA COUNTY MONUMENT.
- NOTE: FOR THE PURPOSES OF THIS PLAT, THE SOUTH LINE OF THE SW1/4 OF SEC. 24, T. 32, R. 25 IS ASSUMED TO BEAR S 89°07'51"E

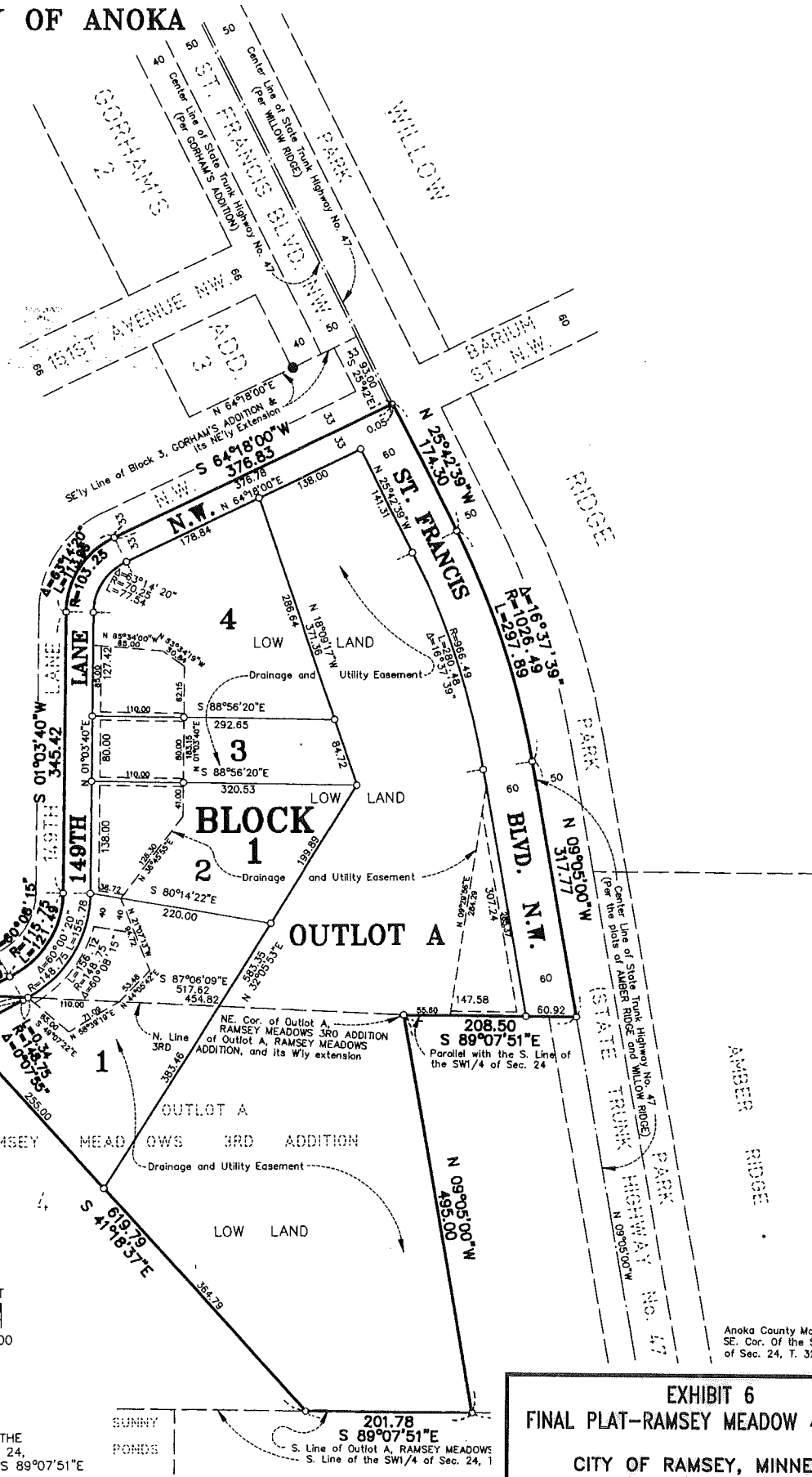
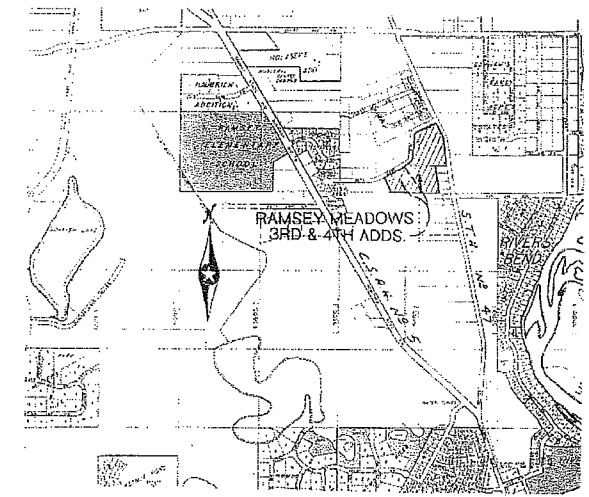
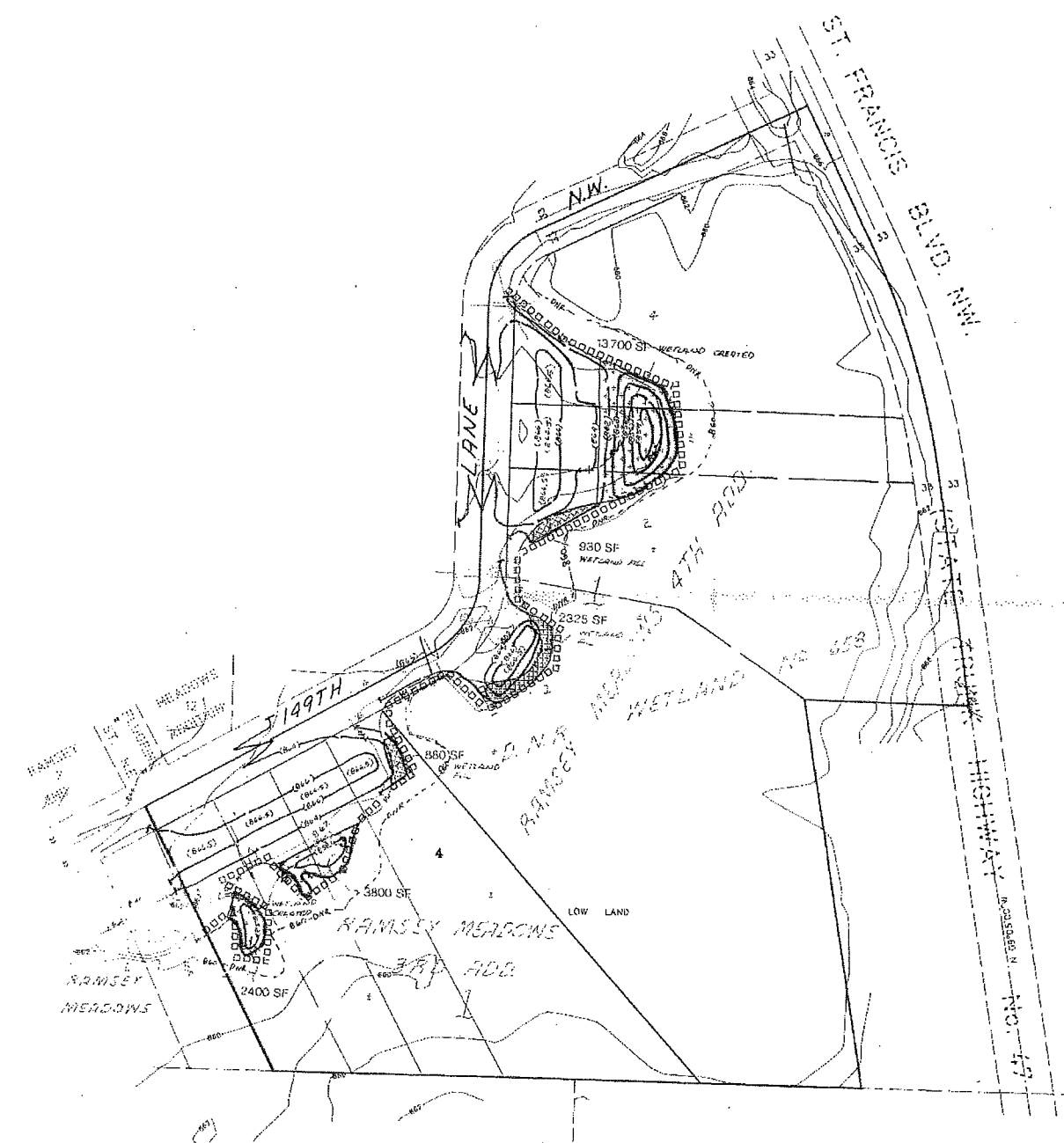


EXHIBIT 6
FINAL PLAT-RAMSEY MEADOW 4TH ADD.
CITY OF RAMSEY, MINNESOTA

Anoka County Monument at SE. Cor. Of the SW1/4 of Sec. 24, T. 32, R. 25

30997/824500

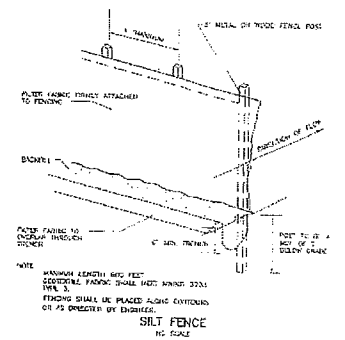
PRELIMINARY GRADING, DRAINAGE & EROSION CONTROL PLAN FOR: **RAMSEY MEADOWS 3RD & 4TH ADDITIONS**



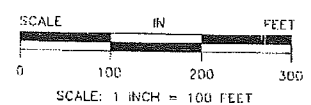
EROSION CONTROL DURING CONSTRUCTION

NOTES:

- All disturbed soils within 100 lineal feet from wetlands shall be covered with four inches (4") of topsoil and seeded with a minimum of seventy-five (75) pounds per acre of MNDOT Specification Section 3876, Mixture No. 12, with fertilizer 12-12-12 mixture applied at 600 pounds per acre with Mulch-Type 1. This work shall be constructed in accordance with MNDOT Specification Section 2575, within the following timeframe:
 Slopes steeper than 3:1 - 7 days
 Slopes 10:1 to 3:1 - 14 days
 Flatter than 10:1 - 21 days
- Excavate ponding areas before upland grading.
- The bottom of all drainage ditches shall be stabilized within 100 feet of all wetlands. Stabilization must be initiated within 24 hours of connection to wetlands and be completed within five days. All pipes connecting to drainage swales must be provided with energy dissipation structures prior to connecting to wetland.
- Sediment control structures must be in place prior to starting of grading and must be maintained until final stabilization has been established.
- Inspect and maintain after rainfall (as required). The inspector is to be
- Vehicle tracking onto unpaved surfaces must be minimized.
- All silt fence shall be removed after the site has undergone final stabilization.



	PROPOSED NON-DNR WETLAND DISTURBED	PROPOSED WETLAND CREATED
Ramsey Meadows 3rd Addition	880 S.F. ±	6,200 S.F. ±
Ramsey Meadows 4th Addition	4,135 S.F. ±	19,900 S.F. ±
TOTALS	5,015 S.F. ±	26,100 S.F. ±



● DENOTES IRON MONUMENT FOUND
 ○ DENOTES 1/2 INCH IRON PIPE SET.
 BEARINGS SHOWN ARE BASED ON ASSUMED DATUM

I HEREBY CERTIFY THAT THIS SURVEY, PLAN OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

Jeffrey M. Caine
 DATE: Jan. 23, 2015 REG. NO. 12251

--- 000.0 --- DENOTES EXISTING SPOT ELEVATION
 (000.0) " " PROPOSED " "
 --- 000 --- " " EXISTING CONTOUR
 --- (000) --- " " PROPOSED CONTOUR

--- DNR --- DENOTES EDGE OF D.N.R. WETLAND N.R. 658.
 --- W --- W --- DENOTES EDGE OF WETLAND AS LOCATED BY JOHN C. ANDERSON OF WETLANDS DATA, INC. & SURVEYED BY CAINE & ASSOCIATES, INC.
 □□□□□□□□□□ DENOTES PROPOSED SILT FENCE

CAINE & ASSOCIATES
LAND SURVEYORS, INC.
 17720 Highway 85 N.E. - Ham, Ia.
 634-7846

EXHIBIT 7
RAMSEY MEADOWS GRADING PLAN
 CITY OF RAMSEY, MINNESOTA

**TABLE 8
149TH LANE
ALTERNATIVE 1**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
5	TURF ESTABLISHMENT	LUMP SUM	\$50.00	1	\$50

Estimated Construction Cost	\$1,150
Contingency (10%)	\$115
Total Estimated Construction Cost	<u>\$1,265</u>

**TABLE 9
149TH LANE
ALTERNATIVE 2A - 15" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$400.00	1	\$400
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	3	\$900
4	15" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$100.00	118	\$11,800
5	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
6	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$13,900
Contingency (10%)	\$1,390
Total Estimated Construction Cost	<u>\$15,290</u>

**TABLE 10
149TH LANE
ALTERNATIVE 2B - 18" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	\$1,705
Total Estimated Construction Cost	<u>\$18,755</u>

**TABLE 11
149TH LANE
ALTERNATIVE 2C - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

TABLE 12
149TH LANE
ALTERNATIVE 3A - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$500.00	1	\$500
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	18" RC PIPE APRON	EACH	\$350.00	2	\$700
5	18" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$125.00	118	\$14,750
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$17,050
Contingency (10%)	\$1,705
Total Estimated Construction Cost	<u>\$18,755</u>

**TABLE 13
149TH LANE
ALTERNATIVE 3B - 24" OUTLET**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	REMOVE STORM STRUCTURE	EACH	\$400.00	1	\$400
3	15" RC PIPE APRON	EACH	\$300.00	1	\$300
4	24" RC PIPE APRON	EACH	\$450.00	2	\$900
5	24" RC PIPE SEWER DESIGN 3006, CL V - JACKED	LIN FT	\$200.00	118	\$23,600
6	CONNECT TO EXISTING STORM SEWER	EACH	\$300.00	1	\$300
7	TURF ESTABLISHMENT	LUMP SUM	\$100.00	1	\$100

Estimated Construction Cost	\$26,400
Contingency (10%)	\$2,640
Total Estimated Construction Cost	<u>\$29,040</u>

**TABLE 14
149TH LANE
ALTERNATIVE 4**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$100.00	1	\$100
2	COMMON EXCAVATION	CU YD	\$5.00	133	\$665
3	GRANULAR BORROW	CU YD	\$8.00	380	\$3,040
4	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.3	\$450

Estimated Construction Cost	\$4,255
Contingency (10%)	\$426
Total Estimated Construction Cost	<u>\$4,681</u>

Section 3

Rum River Hills Golf Club

Rum River Hills Golf Club

Summary

Rum River Hills Golf Club has recently experienced flooding on some fairways and cart paths. Exhibit 9 shows the discharge points and problem areas throughout the golf course. The following will address the problem areas throughout the golf course.

Flooding along Hole #1

The first area of concern includes flooding of the large pond along Hole #1. This pond discharges through an existing concrete outlet structure and a 12" plastic pipe to the east. The outlet is labeled as Area 1 on Exhibit 9. It is our understanding that after large storm events, the pond remains elevated for long periods of time. The 100-year high water level (HWL) is 870.3.

After reviewing video recordings of the outlet pipe, it appears that the pipe has several sags in it and several joints have been compromised. The outlet pipe is relatively shallow and may have been affected by frost heave. This outlet pipe is likely causing the pond to operate inefficiently.

We believe that the best alternative for this outlet is to remove the existing outlet structure and the 12" outlet pipe and replace them with a new 4-foot diameter concrete outlet structure and 18" high density polyethylene pipe outlet in the same location as the existing pipe. Exhibit 10 shows the proposed outlet structure. The new pipe would be installed at a lower elevation than it exists now, reducing the impact of frost heave on the pipe. The polyethylene pipe is also more rigid and is solid (no air voids) and therefore more resistant to frost heave or buoyancy when soils are saturated. A removable, weir wall would be installed in the new outlet structure. We would propose to leave the weir height at the same elevation as existing. By installing the weir wall, it will allow for greater flexibility in controlling the water elevations of the pond. Installing this outlet structure would result in a 100-year HWL of 870.0.

The estimated cost to construct the new outlet pipe and structure is \$29,853. Table 15 includes the individual costs for this alternative.

Cart path flooding near the clubhouse

Based on the HWL of the pond along Hole #1, it appears that there could be an issue with the cart paths flooding near the clubhouse, Areas 2A and 2B on Exhibit 9. The existing culverts under the cart paths are a 15" diameter and 12" diameter. If flooding the cart paths is an issue, the best alternative would be to install larger diameter culverts under the cart paths.

Installing 24" diameter culverts will reduce the flooding during small storm events. However, since the HWL elevation for the area is controlled by the downstream outlet structure discussed above, the paths will still flood during large storm events. To reduce the flooding during the large storm events, the cart paths would have to be raised

approximately 1.5 feet. This would, however, increase the HWL in the pond near the clubhouse, which would appear to adversely affect the fairway for Hole #18.

The estimated cost to replace the two culverts under the carts paths is \$6,642. Table 16 includes the individual costs this alternative.

Saturated soil along Hole #15

Another issue is occurring along Hole #15 near the Rum River. The soil in the area is saturated. There is an existing rock dam, Area 3 on Exhibit 19, southeast of the most saturated area of the fairway. Part of the fairway was excavated to help to determine the problem and drain the area.

Based on our review, it appears that the water being contained by the rock dam may be infiltrating into the soil and causing the saturation. There is a dropped of approximately seven feet from the rock dam to the bottom of the downstream channel. We also noticed that the existing drain tile that was excavated along Hole #15 was plugged with roots and soil.

We recommend two alternatives to address the soil saturation along Hole #15. First, the drain tile should be replaced to improve the drainage in the area. Second, the area of the creek that is being contained by the rock dam should be lined with an impermeable material to eliminate the water infiltrating through the soil. Lining the creek will eliminate the infiltration into the soil and the new drain tile will help to keep the existing soil dry.

The estimated cost to replace the drain tile and line the creek bed is \$13,530. Table 17 includes individual costs for this alternative.

Flooding of Hole #3 and #17

Flooding of Hole #3 and #17 is also an issue at the golf course. The stormwater drains south through three 15” culverts, labeled Area 4A, Area 4B and Area 4C on Exhibit 9, and then to the Rum River.

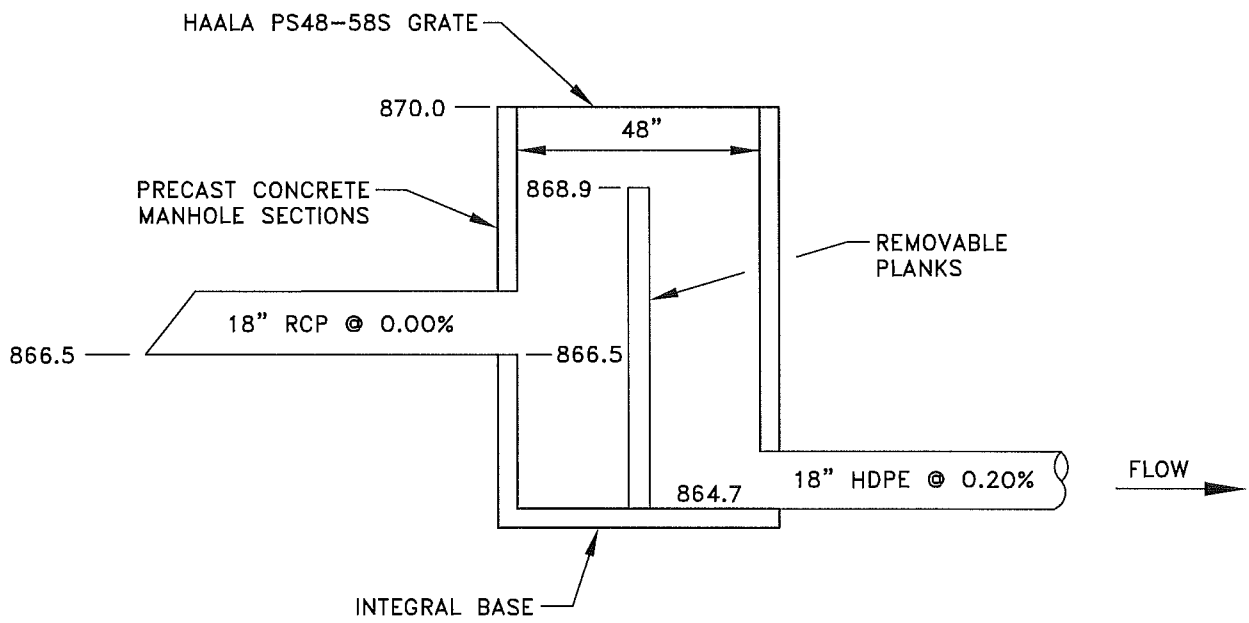
One alternative to lower the HWL’s in the area would be to install bigger culverts. A 21” culvert was modeled to replace the culverts at Area 4A and 4B and a 24” culvert was modeled to replace the culvert at 4C. The following table summarizes the 100-year HWL’s for the three areas:

Existing 100-Year HWL			Proposed 100-Year HWL		
Area 4A	Area 4B	Area 4C	Area 4A	Area 4B	Area 4C
859.3	859.3	858.5	859.1	858.7	858.3

The estimated cost to replace the three culverts is \$4,990. Table 18 includes individual costs for this alternative.



EXHIBIT 9
RUM RIVER HILLS GOLF CLUB
EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA



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EXHIBIT 10
RUM RIVER HILLS GOLF CLUB
PROPOSED OUTLET STRUCTURE
CITY OF RAMSEY, MINNESOTA

TABLE 15
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #1 - NEW OUTLET STRUCTURE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$800.00	1	\$800
2	CLEARING	ACRE	\$1,500.00	0.2	\$300
3	GRUBBING	ACRE	\$1,500.00	0.2	\$300
4	REMOVE STORM SEWER	LIN FT	\$1.50	856	\$1,284
5	18" METAL APRON	EACH	\$275.00	1	\$275
6	18" RC PIPE APRON	EACH	\$350.00	1	\$350
7	18" HDPE PIPE SEWER	LIN FT	\$22.00	850	\$18,700
8	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	10	\$230
9	OUTLET CONTROL STRUCTURE	EACH	\$4,000.00	1	\$4,000
10	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.6	\$900

Estimated Construction Cost	\$27,139
Contingency (10%)	\$2,714
Total Estimated Construction Cost	<u>\$29,853</u>

TABLE 16
RUM RIVER HILLS GOLF CLUB
CART PATH FLOODING NEAR CLUBHOUSE - NEW CULVERTS

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	96	\$384
3	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	30	\$150
4	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	48	\$144
5	4" AGGREGATE BASE CLASS 5	SQ YD	\$8.00	30	\$240
6	4" BITUMINOUS PAVEMENT	SQ YD	\$34.00	30	\$1,020
7	24" METAL APRON	EACH	\$325.00	4	\$1,300
8	24" CP PIPE CULVERT	LIN FT	\$25.00	96	\$2,400
9	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$6,038
Contingency (10%)	\$604
Total Estimated Construction Cost	<u>\$6,642</u>

TABLE 17
RUM RIVER HILLS GOLF CLUB
HOLE #15 SATURATION - CREEK LINING AND DRAIN TILE

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$350.00	1	\$350
2	4" PERF PIPE DRAIN	LIN FT	\$15.00	450	\$6,750
3	CREEK LINING	LUMP SUM	\$5,000.00	1	\$5,000
4	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$12,300
Contingency (10%)	\$1,230
Total Estimated Construction Cost	<u>\$13,530</u>

**TABLE 18
RUM RIVER HILLS GOLF CLUB
FLOODING AROUND HOLE #3 AND #17 - NEW CULVERTS**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$200.00	1	\$200
2	REMOVE STORM SEWER	LIN FT	\$4.00	72	\$288
3	21" METAL APRON	EACH	\$300.00	4	\$1,200
4	24" METAL APRON	EACH	\$325.00	2	\$650
5	24" CP PIPE CULVERT	LIN FT	\$26.00	46	\$1,196
6	24" CP PIPE CULVERT	LIN FT	\$27.00	26	\$702
7	TURF ESTABLISHMENT	LUMP SUM	\$300.00	1	\$300

Estimated Construction Cost	\$4,536
Contingency (10%)	\$454
Total Estimated Construction Cost	<u>\$4,990</u>

Section 4
163rd Lane

163rd Lane

Description

As shown on Exhibit 11, a low area exists south of 163rd Lane and east of Wolfram Street. The low area does not have a piped outlet to County Ditch #3. During a majority of the year, stormwater runoff infiltrates into the soil. However, during early spring when the ground is frozen and during periods of heavy rainfall, water levels have risen to levels that cause flooding of adjacent properties.

Alternatives

The following alternatives address the water elevation in the low area.

Alternative 1

In this alternative, an outlet pipe would be installed from the low area to County Ditch #3, as shown on Exhibit 12. The outlet pipe will not be installed at the bottom of the low area, rather, it will be installed at an elevation that would allow smaller storm events to continue to infiltrate into the soil. The outlet pipe invert is proposed at an elevation of 872.0, which is the approximate elevation of a 3-inch rainfall event. The following table summarizes the 100-year HWL's, the detention time above an elevation of 873.0 and estimated costs to construct each outlet pipe:

Outlet Pipe Size	100-Year HWL	Detention Time Above Elevation 873 During a 100-Year Storm Event (hours)	Estimated Cost
Existing	874.8	23.4	N/A
12"	874.3	6.2	\$68,640
15"	874.0	2.9	\$74,305
18"	873.8	1.5	\$80,795

As shown, the area may continue to flood during large storm events; however, the duration of flooding will be much shorter.

Tables 19 through 21 include the individual costs for this alternative. It is proposed to directionally drill the pipe as shown on Exhibit 12 as opposed to open cutting a trench. A trench would require excessive cuts in the surrounding area. The outlet pipe will be approximately 700 feet long. Cleaning the pipe will require access on both ends of the pipe.

Alternative 2

Alternative 2 included draining the low area east to a ditch system in Elmcrest Park. This alternative was reviewed and deemed not feasible. There is not enough difference in elevation from the low area to the ditch in Elmcrest Park.

Alternative 3

Alternative 3 included constructing a pipe west and then south along 163rd Lane and Wolfram Street to County Ditch #3. The length of this alternative is approximately 500 feet longer than Alternative 1, therefore decreasing the pipe slope and increasing the costs. The impact of constructing the pipe along the existing streets would also add to the costs of this alternative. For these reasons, this alternative was deemed not feasible.

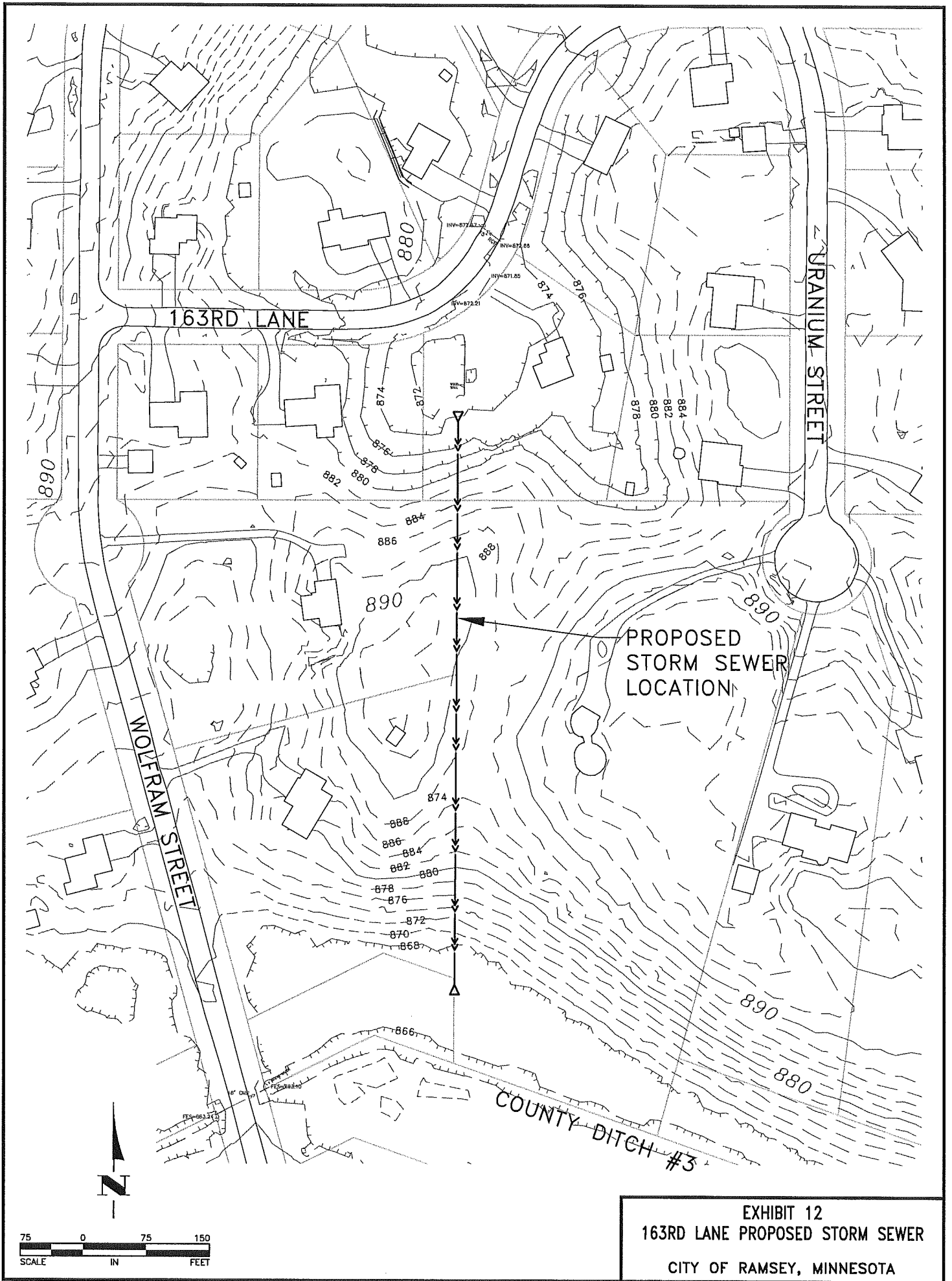


EXHIBIT 12
163RD LANE PROPOSED STORM SEWER
CITY OF RAMSEY, MINNESOTA

TABLE 19
163RD LANE
ALTERNATIVE 1A - 12" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,800.00	1	\$1,800
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	12" METAL APRON	EACH	\$200.00	2	\$400
4	12" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$85.00	700	\$59,500
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$62,400
Contingency (10%)	\$6,240
Total Estimated Construction Cost	<u>\$68,640</u>

TABLE 20
163RD LANE
ALTERNATIVE 1B - 15" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$2,000.00	1	\$2,000
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	15" METAL APRON	EACH	\$225.00	2	\$450
4	15" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$92.00	700	\$64,400
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$67,550
Contingency (10%)	\$6,755
Total Estimated Construction Cost	<u>\$74,305</u>

TABLE 21
163RD LANE
ALTERNATIVE 1C - 18" OUTLET

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$2,200.00	1	\$2,200
2	COMMON EXCAVATION	CU YD	\$5.00	100	\$500
3	18" METAL APRON	EACH	\$275.00	2	\$550
4	18" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$100.00	700	\$70,000
5	TURF ESTABLISHMENT	LUMP SUM	\$200.00	1	\$200

Estimated Construction Cost	\$73,450
Contingency (10%)	\$7,345
Total Estimated Construction Cost	<u>\$80,795</u>

Section 5
156th Lane

156th Lane

Description

As shown on Exhibit 13, a low area exists south of 156th Lane and west of Yakima Street, referred to as Depression 1 on Exhibit 13. Depression 1 drains south and east through an existing storm sewer system to another low area west of Juniper Ridge Drive, referred to as Depression 2 on Exhibit 13. From Depression 2 the stormwater drains north through an existing storm sewer system to the Rum River. The outlet elevations for both Depression 1 and Depression 2 are 859.8.

During storm events, water ponds in the backyard of 5220 156th Lane. The area where water ponds was platted with a 75-foot drainage and utility easement, however, this easement has been vacated. The existing 100-year high water level (HWL) for Depression 1 is 864.7.

Alternatives

The following alternatives address the stormwater in the area.

Alternative 1

Alternative 1 includes filling the backyard of 5220 156th Lane. Excavation would be required in Woodland Green Park to the south to compensate for the storage being lost by filling the backyard. The proposed grading is shown on Exhibit 14. In this alternative the proposed 100-year HWL would remain 864.7, but it would not encroach into the backyard to the extent it does under existing conditions.

The estimated cost for this alternative is \$21,175. Table 22 includes the individual costs for this alternative.

Alternative 2

Alternative 2 also includes filling the backyard of 5220 156th Lane and excavating Woodland Green Park to the south to compensate for the storage being lost by filling the backyard. As opposed to Alternative 1, this alternative proposes to over-excavate the area in Woodland Green Park, which will lower the 100-year HWL by adding storage. The proposed grading is shown on Exhibit 15. In this alternative the proposed 100-year HWL would drop to 864.1.

The estimated cost for this alternative is \$76,274. Table 23 includes the individual costs for this alternative.

Alternative 3

Alternative 3 includes constructing an additional 12" outlet pipe from Depression 2. As shown on Exhibit 16, the new outlet pipe would run east along 156th Lane and then north along Juniper Ridge Drive to an existing catch basin that drains to the Rum River. The

new outlet would be constructed at an elevation of 857.8, two feet lower than the existing outlet.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.6
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.9

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.0
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.8

As shown, adding this additional outlet pipe has very little effect on the high water levels of the two depressions. One reason for this is that the storm sewer system downstream of Depression 2 is at or above capacity and is flowing back into Depression 2 prior to draining downstream.

The estimated cost to construct this additional outlet pipe is \$158,609. Table 24 includes the individual costs for this alternative.

Alternative 4

Alternative 4 includes constructing an additional 18" outlet pipe from Depression 1. As shown on Exhibit 16, the new outlet pipe would run east from Depression 1 to Depression 2. The new outlet would be constructed at an elevation of 858.2; 1.6-feet lower than the existing outlet. This alternative assumes that the new outlet from Depression 2, as discussed in Alternative 3, would also be constructed.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	864.6
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.5

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	863.8
Depression 2 – Existing	866.0
Depression 2 – Proposed	865.3

As shown, adding this additional outlet pipe has very little effect on the high water levels of the two depressions. After adding the pipe, stormwater actually flows from Depression 2 back to Depression 1 prior to flowing downstream, lowering the HWL in Depression 2.

The estimated cost to construct this additional outlet pipe is \$37,697. Table 25 includes the individual costs for this alternative.

Alternative 5

Alternative 5 includes constructing a 36" outlet pipe from Depression 2. As shown on Exhibit 16, the new outlet pipe would run east from Depression 2 to the Rum River. The new outlet would be constructed at an elevation of 857.8. As part of this alternative, the existing outlet pipe from Depression 2 will be eliminated. This alternative also assumes that the new outlet from Depression 1, as discussed in Alternative 4, would also be constructed.

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 14, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	863.8
Depression 2 – Existing	866.0
Depression 2 – Proposed	859.9

The following table summarizes the 100-year HWL's for Depression 1, assuming the pond construction shown on Exhibit 15, and Depression 2:

Location	100-year HWL
Depression 1 – Existing	864.7
Depression 1 – Proposed	862.7
Depression 2 – Existing	866.0
Depression 2 – Proposed	859.8

As shown, adding this outlet pipe has a significant effect on the high water levels of the two depressions. This alternative would have to be approved by the Minnesota Department of Natural Resources.

The estimated cost to construct this additional outlet pipe is \$142,772. Table 26 includes the individual costs for this alternative.



EXHIBIT 13
156TH LANE EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

Feb 13, 2012 - 6:14pm
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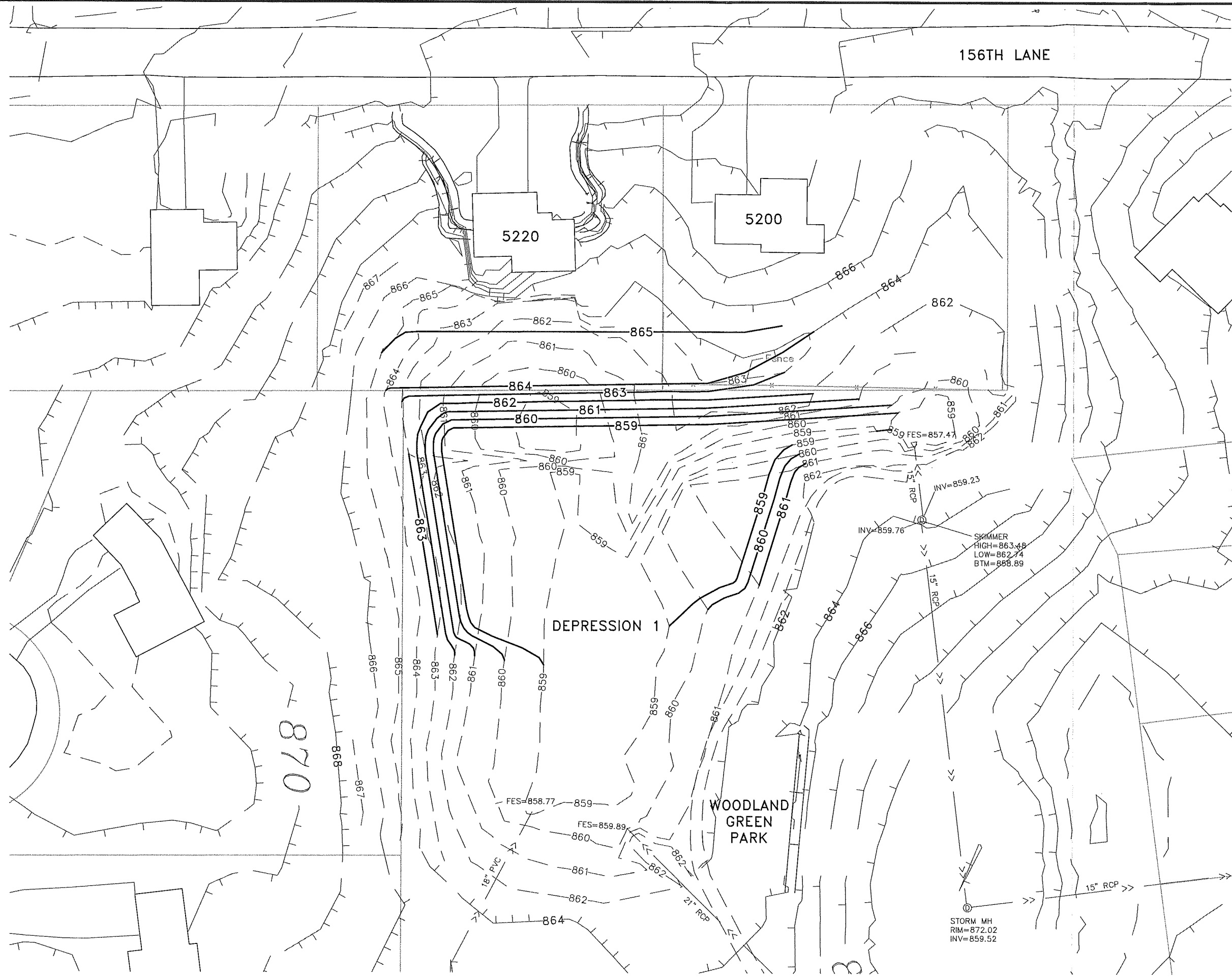


EXHIBIT 14
156TH LANE ALTERNATIVE 1 GRADING PLAN
CITY OF RAMSEY, MINNESOTA

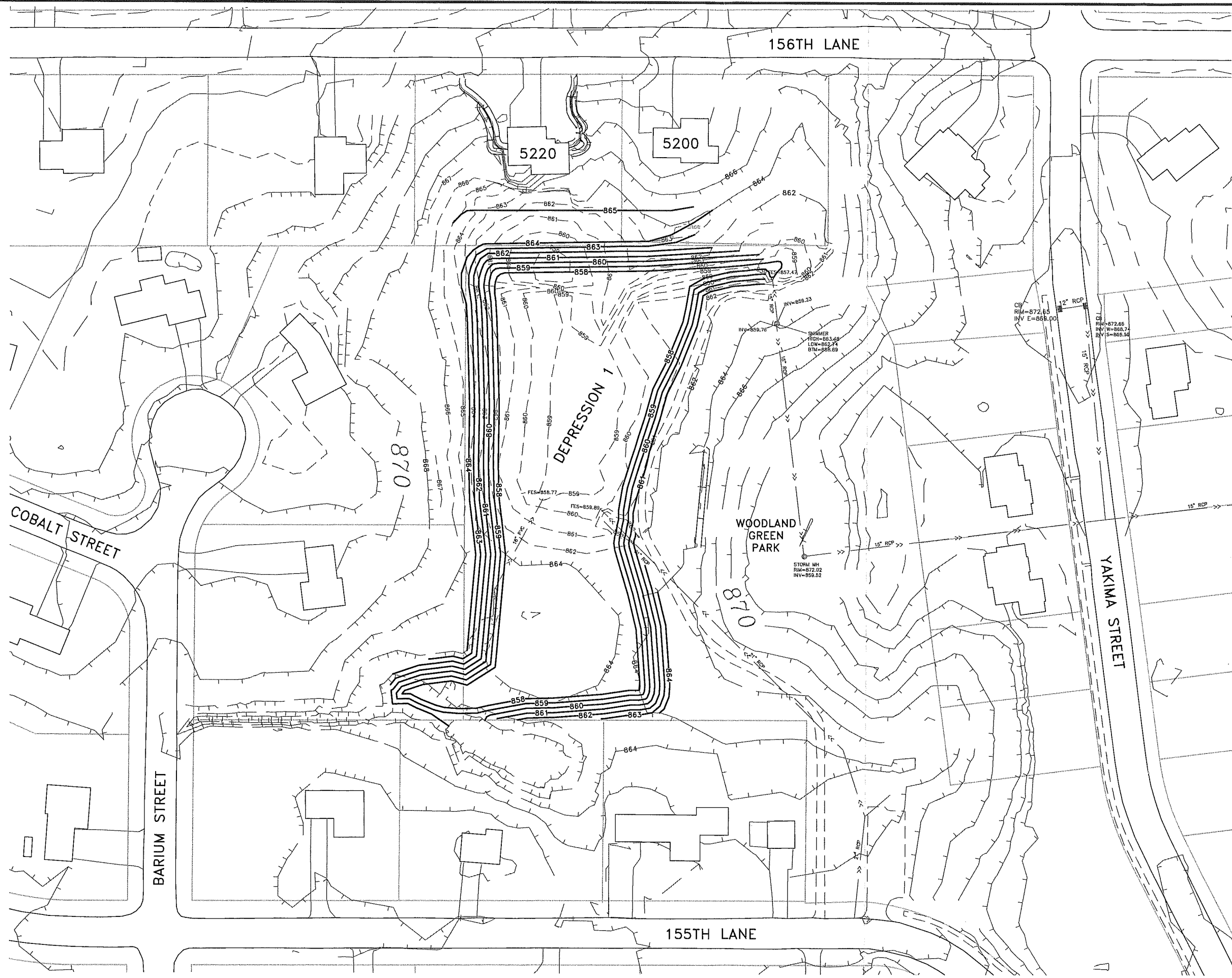


EXHIBIT 15
156TH LANE ALTERNATIVE 2 GRADING PLAN
CITY OF RAMSEY, MINNESOTA

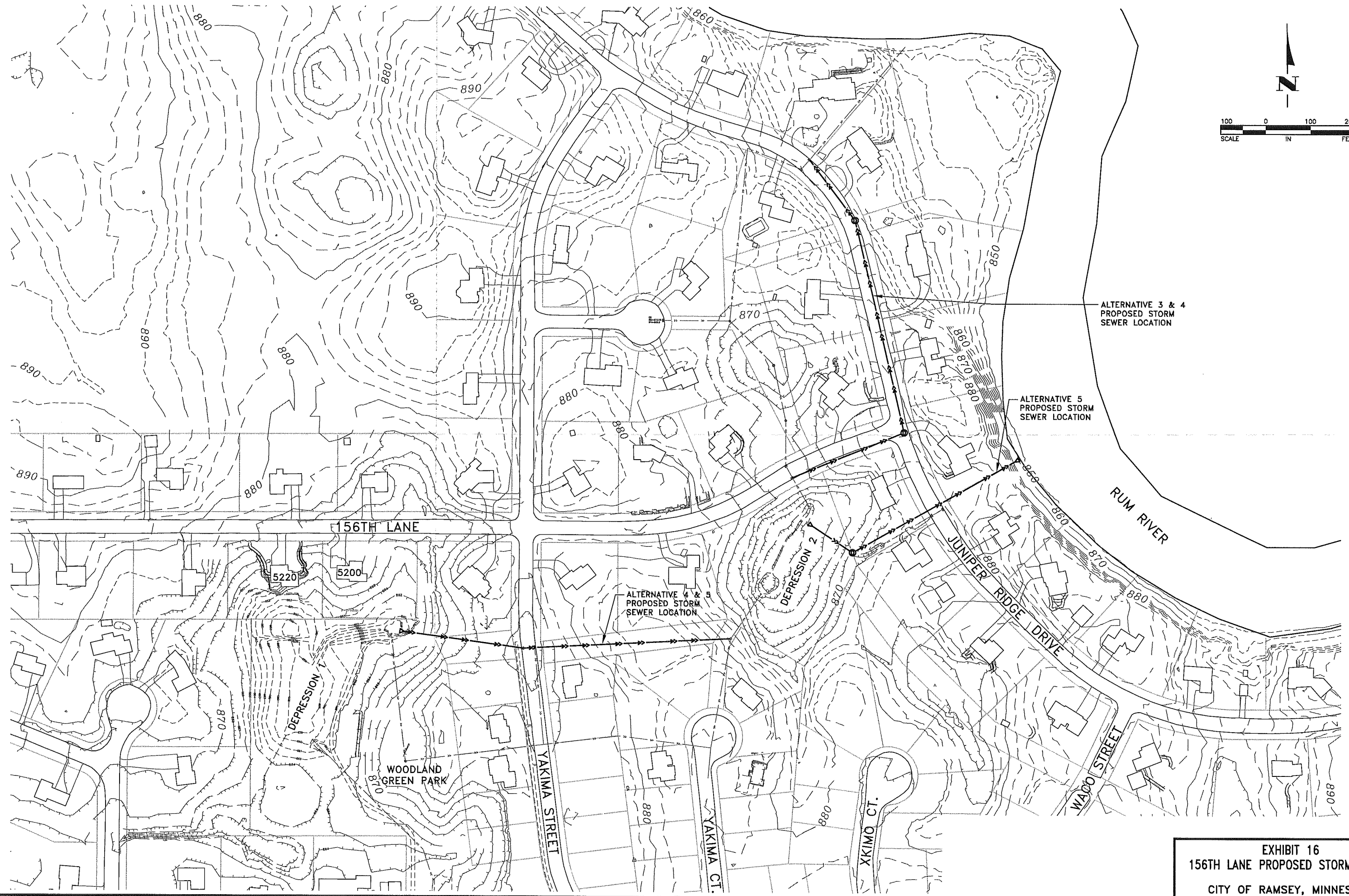


EXHIBIT 16
156TH LANE PROPOSED STORM SEWER
CITY OF RAMSEY, MINNESOTA

**TABLE 22
156TH LANE
ALTERNATIVE 1**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$600.00	1	\$600
2	CLEARING	ACRE	\$1,500.00	0.75	\$1,125
3	GRUBBING	ACRE	\$1,500.00	0.75	\$1,125
4	COMMON EXCAVATION	CU YD	\$5.00	1820	\$9,100
5	ADJUST SEWER MANHOLES	LUMP SUM	\$4,000.00	1	\$4,000
6	ADJUST SEWER CLEANOUTS	LUMP SUM	\$500.00	1	\$500
7	REINSTALL SPRINKLER SYSTEM	LUMP SUM	\$1,000.00	1	\$1,000
8	TURF ESTABLISHMENT	ACRE	\$1,500.00	1.2	\$1,800

Estimated Construction Cost	\$19,250
Contingency (10%)	\$1,925
Total Estimated Construction Cost	<u>\$21,175</u>

**TABLE 23
156TH LANE
ALTERNATIVE 2**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,900.00	1	\$1,900
2	CLEARING	ACRE	\$1,500.00	2.35	\$3,525
3	GRUBBING	ACRE	\$1,500.00	2.35	\$3,525
4	COMMON EXCAVATION	CU YD	\$5.00	10123	\$50,615
5	ADJUST SEWER MANHOLES	LUMP SUM	\$4,000.00	1	\$4,000
6	ADJUST SEWER CLEANOUTS	LUMP SUM	\$500.00	1	\$500
7	REINSTALL SPRINKLER SYSTEM	LUMP SUM	\$1,000.00	1	\$1,000
8	TURF ESTABLISHMENT	ACRE	\$1,500.00	2.9	\$4,275

Estimated Construction Cost	\$69,340
Contingency (10%)	\$6,934
Total Estimated Construction Cost	<u>\$76,274</u>

TABLE 24
156TH LANE
ALTERNATIVE 3

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$4,200.00	1	\$4,200
2	SALVAGE STORM SEWER	LIN FT	\$20.00	72	\$1,440
3	REMOVE MANHOLE	EACH	\$500.00	1	\$500
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$2.00	3,290	\$6,580
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	100	\$300
6	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.00	3,290	\$23,030
7	4" BITUMINOUS PAVEMENT	SQ YD	\$21.00	3,290	\$69,090
8	BITUMINOUS CURB	LIN FT	\$3.00	1,850	\$5,550
9	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	2	\$2,000
10	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$22.00	925	\$20,350
11	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$2,000.00	3	\$6,000
12	TRAFFIC CONTROL	LUMP SUM	\$5,000.00	1	\$5,000
13	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.1	\$150

Estimated Construction Cost	\$144,190
Contingency (10%)	\$14,419
Total Estimated Construction Cost	<u>\$158,609</u>

**TABLE 25
156TH LANE
ALTERNATIVE 4**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$1,000.00	1	\$1,000
2	SALVAGE STORM SEWER	LIN FT	\$20.00	120	\$2,400
3	REMOVE CONCRETE CURB	LIN FT	\$10.00	60	\$600
4	REMOVE MANHOLE	EACH	\$500.00	1	\$500
5	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	110	\$550
6	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	70	\$210
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	110	\$825
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	110	\$3,080
9	18" RC PIPE APRON	EACH	\$350.00	1	\$350
10	TRASH GUARD FOR 18" PIPE APRON	EACH	\$200.00	1	\$200
11	CONNECT TO EXISTING STORM SEWER	EACH	\$1,000.00	1	\$1,000
12	18" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$23.00	735	\$16,905
13	CONSTRUCT DRAINAGE STRUCTURE DESIGN 48 - 4020	EACH	\$1,500.00	2	\$3,000
14	CONCRETE CURB AND GUTTER DESIGN B618	LIN FT	\$20.00	60	\$1,200
15	TRAFFIC CONTROL	LUMP SUM	\$2,000.00	1	\$2,000
16	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.3	\$450

Estimated Construction Cost	\$34,270
Contingency (10%)	\$3,427
Total Estimated Construction Cost	<u>\$37,697</u>

**TABLE 26
156TH LANE
ALTERNATIVE 5**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$3,750.00	1	\$3,750
2	REMOVE STORM SEWER	LIN FT	\$4.00	72	\$288
3	BULKHEAD MANHOLE	EACH	\$500.00	1	\$500
4	36" METAL APRON	EACH	\$325.00	1	\$325
5	36" RC PIPE APRON	EACH	\$450.00	1	\$450
6	TRASH GUARD FOR 36" PIPE APRON	EACH	\$400.00	1	\$400
7	36" RC PIPE SEWER DESIGN 3006, CL III	LIN FT	\$38.00	110	\$4,180
8	36" HDPE PIPE SEWER (DIRECTIONALLY DRILLED)	LIN FT	\$280.00	420	\$117,600
9	CONSTRUCT DRAINAGE STRUCTURE DESIGN 60 - 4020	LIN FT	\$2,000.00	1	\$2,000
10	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.2	\$300

Estimated Construction Cost	\$129,793
Contingency (10%)	\$12,979
Total Estimated Construction Cost	<u>\$142,772</u>

Section 6

Sodium Street

Sodium Street

Description

The house at 16756 Sodium Street has been experiencing water in the basement. Exhibit 17 shows the existing area. One reason for water in the basement could be that stormwater fills the ditch on the east side of Sodium Street, overtops the road and if the driveway culvert is blocked, the water fills up the ditch on the west side of Sodium Street and drains toward the house. The water then seeps along the basement wall and eventually into the basement. Another reason for water in the basement may be due to a high groundwater elevation in the area.

Alternatives

The following alternatives address the issue of water entering the basement from the road ditch.

Alternative 1

Alternative 1 will include installing a culvert under Sodium Street, regrading the west ditch of Sodium Street, constructing a berm to keep the water in the ditch, and replacing the existing driveway culvert. The ditch would be graded to drain to the south property line of 16756 Sodium Street and a culvert would be installed to drain the stormwater to the swale on the west side of the lot. Exhibit 18 shows the proposed construction. The culvert along the south property line is needed since a ditch cannot be graded without impacting the existing septic drainfield.

To be able to install the pipe along the south property line to the swale on the west side of the lot, a utility pole and a utility pedestal will have to be relocated from the southeast corner of 16756 Sodium Street. Moving the utilities will allow the area to be graded as needed to construct the ditch and the culvert. Regrading the ditch will prevent the stormwater from draining toward the house and reducing the chances of having water in the basement.

The estimated cost for this alternative is \$12,225. Table 27 includes the individual costs for this alternative.

Alternative 2

Alternative 2 is similar to Alternative 1, but instead of installing a culvert along the south property line to the swale, a retaining wall would be constructed. This alternative would only be required if the utilities in the southeast corner of the lot were unable to be relocated.

The estimated cost for this alternative is \$18,847. Table 28 includes the individual costs for this alternative.

Both alternatives would require that the property owner give an easement along the south property line. Also, temporary easements will likely be required to grade the ditch and construct the berm on the west side of Sodium Street.



EXHIBIT 17
SODIUM STREET EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA

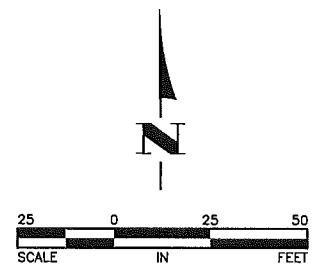


EXHIBIT 18
SODIUM STREET PROPOSED CONSTRUCTION
CITY OF RAMSEY, MINNESOTA

**TABLE 27
SODIUM STREET
ALTERNATIVE 1**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$300.00	1	\$300
2	REMMOVE STORM SEWER	LIN FT	\$5.00	25	\$125
3	REMOVE CONCRETE PAVEMENT	SQ YD	\$6.00	45	\$270
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	49	\$245
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	44	\$132
6	COMMON EXCAVATION	CU YD	\$6.00	400	\$2,400
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	94	\$705
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	49	\$1,372
9	4" CONCRETE DRIVEWAY PAVEMENT	SQ YD	\$13.00	45	\$585
10	15" METAL APRON	EACH	\$225.00	2	\$450
11	12" RC PIPE APRON	EACH	\$275.00	4	\$1,100
12	15" CP PIPE SEWER	LIN FT	\$20.00	44	\$880
13	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$20.00	40	\$800
14	TRAFFIC CONTROL	LUMP SUM	\$1,000.00	1	\$1,000
15	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$11,114
Contingency (10%)	\$1,111
Total Estimated Construction Cost	<u>\$12,225</u>

**TABLE 28
SODIUM STREET
ALTERNATIVE 2**

ITEM NO.	DESCRIPTION	UNIT	UNIT COST	TOTAL ESTIMATED QUANTITY	TOTAL ESTIMATED COST
1	MOBILIZATION	LUMP SUM	\$300.00	1	\$300
2	REMMOVE STORM SEWER	LIN FT	\$5.00	25	\$125
3	REMOVE CONCRETE PAVEMENT	SQ YD	\$6.00	45	\$270
4	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$5.00	49	\$245
5	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	\$3.00	44	\$132
6	COMMON EXCAVATION	CU YD	\$6.00	475	\$2,850
7	4" AGGREGATE BASE CLASS 5	SQ YD	\$7.50	94	\$705
8	4" BITUMINOUS PAVEMENT	SQ YD	\$28.00	49	\$1,372
9	4" CONCRETE DRIVEWAY PAVEMENT	SQ YD	\$13.00	45	\$585
10	12" RC PIPE APRON	EACH	\$275.00	4	\$1,100
11	12" RC PIPE SEWER DESIGN 3006, CL V	LIN FT	\$20.00	40	\$800
12	MODULAR BLOCK RETAINING WALL	SQ FT	\$23.00	300	\$6,900
13	TRAFFIC CONTROL	LUMP SUM	\$1,000.00	1	\$1,000
14	TURF ESTABLISHMENT	ACRE	\$1,500.00	0.5	\$750

Estimated Construction Cost	\$17,134
Contingency (10%)	\$1,713
Total Estimated Construction Cost	<u>\$18,847</u>



EXHIBIT 9
RUM RIVER HILLS GOLF CLUB
EXISTING CONDITIONS
CITY OF RAMSEY, MINNESOTA



EXISTING EASEMENTS

1 BARTHEL'S

2 2 14 13




3 2 3 RIVER

2012 RUM RIVER HILLS GOLF STORM IMPROVEMENTS



Legend

WORK DESCRIPTION

-  REPLACE CULVERT
-  REPLACE PIPE
-  REPLACE WEIR

CC Regular Session

7. 5.

Meeting Date: 06/12/2012

By: Mark Riverblood, Engineering/Public Works

Information

Title:

Approve Irrigation and Topsoil Test Plots for North Commons

Background:

On April 24th, 2012, City Council approved Park Trust Funds for grading and the application of topsoil for North Commons. At this same time, staff was directed to develop a park irrigation plan for bidding which included all portions of the topsoil test plots (metered irrigation system with 4 different topsoil matrices).

At the May 10th 2012 Park and Recreation Commission meeting, the Commission recommended that City Council consider up to \$40,000 in Park Trust Funds for first phase park improvements at North Commons - consisting primarily of irrigation and drinking fountain. (The drinking fountain would also have a pet attachment - and connection for a garden hose for the community garden.)

It may be noted that the irrigation project will serve the park, the topsoil test plots, the community garden, as well as providing the needed watering for the DNR grant funded trees to be planted later this year by neighborhood volunteers.

Notification:

Observations:

The irrigation plan and spec is attached for information and to detail some of the complexity of this system and associated costs. The park and topsoil test beds will consist of; pipe, sprinkler heads, soil moisture sensors, decoders, valves, valve boxes the sophisticated controller, 5 water meters, 2" copper pipe, many steel fittings, a reduced pressure back-flow preventer, garden shed (instead of traditional control cabinet) and the power supply.

A portion of the irrigation costs is being proposed as attributable to the City's Water Utility; as the Municipal Well #8 will now be irrigated, with this utility encumbered area of North Commons hereafter maintained by parks' operations and funding. This value is assessed as \$1,183, or 5% of the base irrigation cost plus the water supply piping. (Staff will also execute the watermain tapping and connection in-house thereby saving approximately \$3,000.)

The development cost study experiment (for the topsoil test beds) may also be assigned to the Water Utility as this cost is directly associated with addressing the question of the efficacy of the city's topsoil specification in water conservation - as it relates to on-going irrigation demand upon the utility. This value is assigned at \$13,716 (test bed irrigation cost including topsoil, meters and flanges).

The preponderance of the project costs however, are associated with the park and were recommended to be funded by the Park Trust Fund by the Park and Recreation Commission in May of 2012. Those costs are summarized as follows:

- Base Bid Irrigation \$19,627 (less the utility's contribution of \$1,183 above)
- Electrical Supply \$1,000
- Garden Shed (*in lieu of irrigation cabinet and slab at \$1,522*) 2,583
- Drinking Fountain with pet attachment and hose bib \$4,563
- All water supply fittings, meter and connection from watermain \$2,842
- Contingency \$3,000

• **Total \$33,615**

A Request for Proposal was issued to two irrigation wholesalers and several local irrigation contractors, with four quotes being received by sealed quotation. Anderson Irrigation Company Inc. was the lowest bidder for both the base bid and the test plot, as well as total project cost and is a qualified contractor. Additionally, Anderson Irrigation offered a \$400 co-sponsorship discount to partner with the City on the topsoil efficiency experiment, and this will be credited to the Municipal Utility side of the ledger in the final accounting.

Funding Source:

Funding for irrigation with drinking fountain for North Commons is from the Park Trust Fund (with a present balance in the amount of approximately \$1.9 million) in the amount of \$33,615; and the Municipal Water Utility in the amount of \$14,499 (including a \$400 co-sponsorship credit relating to the topsoil experiment).

Staff Recommendation:

Staff recommends proceeding with this project as detailed, with the topsoil test plots being in service by June 25th, 2012 and the park's irrigation immediately thereafter.

Commission Action:

Motion to approve a not-to-exceed amount of \$33,615 from the Park Trust Fund, and \$14,499 from the Municipal Water Utility for irrigation and topsoil test plot development and associated improvements at North Commons.

Attachments

[Irrigation Plan](#)

[Irrigation Spec](#)

[RFP Quotes](#)

[Anderson's Quote](#)

Form Review

Inbox	Reviewed By	Date
Tim Himmer	Tim Himmer	06/07/2012 10:57 AM
Kurt Ulrich	Kurt Ulrich	06/07/2012 03:13 PM
Form Started By: Mark Riverblood		Started On: 06/06/2012
Final Approval Date: 06/07/2012		

**CITY OF RAMSEY
NORTH COMMONS
IRRIGATION SPECIFICATIONS**

24 May 2012

PART 1 – GENERAL

1.01 RELATED DOCUMENTS:

A. Letter inviting quotations, Irrigation Plan, and bid form are hereby made part of this section by reference.

1.02 SCOPE:

A. Furnish and install the complete underground irrigation system including materials, labor, equipment and apparatus for the testing, re-testing and re-adjusting as required to place the system in an approved operational condition. The work shall include, but not be limited to, the following:

1. Supply all materials.
2. Arrange for, obtain and pay for necessary permits, bonds, and fees.
3. Excavating and backfill for all work as specified herein.
4. Install complete irrigation system including controllers, heads, automatic valves, soil moisture sensors, decoders and all related irrigation equipment included in design.
5. Program controller to fully incorporate irrigation system.
6. Test: The entire testing of the underground piping system.
7. Furnish, at no expense to the owner, a technician to instruct the Owner's operating personnel in the maintenance and operation of the system.

1.03 QUALIFICATIONS:

- A. Installer to be an irrigation contractor who has had at least 5 years continuous, satisfactory, and documented experience installing underground irrigation systems of a similar size and type.
- B. As a prerequisite of qualification to bid, the irrigation construction company shall provide verifiable documentation that such person or company is licensed by the Minnesota State Board of Electricity as a Technology Systems Contractor and that company employs not less than one Power Limited Technician and that such licenses are considered "in good standing" by the Minnesota State Board of Electricity. "Verifiable Documentation" shall include but not be limited to submission of copies of Technology System Contractor and Power Limited Technician credentials.

1.04 SUBMITTALS:

- A. Contractor shall furnish the articles, equipment, materials, or processes specified by name in the drawings and specifications. NO substitutions will be allowed .
- B. Contractor shall provide complete "As Built" drawings showing all changes from the original design and specifications and exact "as built" locations, sizes, and types of equipment. Prints of the original design are available from the Owner at cost.

1.05 EXAMINATION OF SITE:

- A. The submission of any proposal in respect to the above named project shall be considered evidence that the Contractor has examined the site:

1.06 CONDUCT OF WORK:

- A. The Contractor shall maintain a skilled journeyman on the jobsite during progress, together with a competent superintendent who has the authority to act in all matters pertaining to the work specified herein.
- B. The Contractor shall coordinate his work with other trades, if any.
- C. The Contractor shall confine operations to the areas to be improved and allotted for material and equipment storage.

1.07 HANDLING OF MATERIALS AND EQUIPMENT:

- A. The contractor shall provide and pay for all transportation required to deliver and remove from the site all materials and equipment required for all the work shown and specified.
- 1.08 **CONTRACTORS RESPONSIBILITY:**
- A. The Contractor shall be responsible for his work and for all materials, tools, and property of every description used in connection.
- B. The Contractor shall protect the Owner against all liabilities, claims, or demands for injuries or damages to any persons or property growing out of the performance of the work under this contract, except such liability, claims or demands for injuries or damages resulting from acts or failure to act by the Owner.
- 1.09 **WORK BY OTHERS:**
- A. Water service, including water meters, backflow prevention, concrete pad and steel enclosure or garden shed shall be by others.
- B. 110 VAC electrical service to the irrigation enclosure or shed shall be by others.
- 1.10 **CODES AND INSPECTION:**
- A. The entire installation shall fully comply with all local, state laws and ordinances and with all the established codes applicable thereto. The Contractor shall be responsible for all required permits, and arrange for all necessary inspection and pay for any and all expenses in conjunction with the irrigation portion of the work.
- 1.11 **FIELD MEASUREMENTS:**
- A. **Verify that field measurements are as shown on drawings and that area within limits are 100% covered.**
- 1.12 **CLEANING PROCESS:**
- A. The contractor shall at all times keep the premises on which the work is being done, and the adjoining premises, clean of rubbish caused by his work.
- B. Upon completion of the job, the Contractor shall clean up all debris caused by his/her work and leave the job in a neat and clean condition. All debris from the job shall be taken away from the premises.

PART 2 - PRODUCTS

All materials shall be new and especially designed for the intended use.

2.01 PRODUCTS:

Hunter Irrigation Products I 20's
ACCLIMA controller Model # CS3500
Acclima Decoders Model # ACC-DCR-002 or 001
Acclima Moisture Sensors Model # ACC-SEN-TDT
MP ROTATOR Matched Precipitation Rate Rotary Sprinklers W/ mpr-40-04 bodies

2.02 PIPING:

- A. Pressure Main Piping on the supply side of the zone valves shall be class 160 PVC, as indicated in drawings. Fittings shall be solvent welded, schedule 40 socket type PVC, rated for pressure applications. All Solvent cement shall meet ANSI/ASTM B2564 for PVC pipe and fittings.
- B. Lateral piping less than or equal to 1 1/4" shall be 100 psi rated polyethylene. Fittings shall be insert type secured with all stainless steel clamps (double clamped).
- C. Lateral piping greater than or equal to 1 1/2" shall be Class 160 PVC, solvent weld, bell end. Fittings shall be solvent welded, socket type schedule 40, rated for pressure applications.

- 2.03 **SLEEVES:**
A. Sleeves to be installed under drives and sidewalks shall be class 160 PVC of the appropriate size and depth to accommodate piping and low voltage wiring.
B. Sleeves shall extend beyond the edge of paving or construction by 12” minimum.
- 2.04 **VALVES:**
A. Automatic Remote Control Valves shall operate each zone and shall be Hunter PGV
- 2.05 **VALVE BOXES:**
A. Valve boxes for Automatic Remote Control Valves shall be NDS Pro Series model 214BC, 14” x 19” x 12 deep rectangular box. Valve boxes shall have bolt on covers. Valve boxes for Soil Moisture Sensor splices shall be 7” round “econo boxes”
- 2.06 **CONTROLS:**
A. Automatic Controller shall be Acclima CS3500 located per plan.
B. Soil Moisture Sensors shall be Acclima ACC- SEN-TDT sensors
C. Decoders shall be Acclima ACC-DCR-002 or 001
- 2.07 **OUTLETS:**
A. Gear drive sprinklers shall be Hunter I 20 and, nozzled per plan.
B. Fluid brake rotary sprinklers shall be MP Rotators, as indicated in plan.
- 2.08 **SWING JOINTS:**
A.
- 2.09 **WIRE:**
A. Wire shall be 14 gauge P.E. solid conductor approved for direct burial.
- 2.09
- 2.10 **GROUNDING AND GROUND RODS:**
A. Decoders must be grounded to manufacture specifications in the valve box with the decoders
B. The ground rods need to be properly ohm (10 or less) and decoder must be attached to the grounding rod with a ground clamp pursuant to Acclima specifications.
- 2.11 **PRESSURE REGULATORS:**
N.A.
- 2.12 **ENCLOSURE:**
By others
- 2.13 **BOOSTER PUMP:**
N.A.
- 2.14 **SOIL MOISTURE SENSOR:**
A. Acclima ACC-SEN-TDT Moisture Sensor (TWO each test bed, that read identically).
- 2.15 **ACCESSORIES:**
N.A.

PART 3 - EXECUTION

- 3.01 **PROTECTION:**
A. Protect trees, shrubs, lawn, sidewalks, drives, curbs, utilities, and other features on or adjacent to the site from damage from the work of this section. Contractor is liable for the repair of any and all damage.

3.02 **WORKMANSHIP:**

- A. All material shall be installed in accordance with local trade practices and codes and in accordance with approved design shop drawings and the manufacturer's recommendations.

3.03 **UNDERGROUND STRUCTURE:**

- A. The contractor shall contact the appropriate public utilities for location and marking of those utilities.
- B. The Owner/General Contractor shall make available to the Irrigation contractor, upon written request, all available information regarding the exact location of existing underground equipment including, but not limited to, owner owned security cables, electrical parking lot and signage wiring, etc.
- C. Except in the case of mismarked or unmarked buried equipment, the Contractor shall be liable for the damages to and the cost of repairing or replacing any buried conduit, cables or piping encountered during the installation of the work.

3.04 **EXAMINATION:**

- A. Verify that field conditions are acceptable and are ready for work.
- B. Verify that required utilities are installed and that adequate water pressure is available and in proper location and ready for use. A minimum static water pressure of 60 psi is required at the water source.
- C. Beginning of installation means installer accepts existing conditions.

3.05 **PREPARATION:**

- A. Layout work accurately according to the Drawing. The Drawings, though carefully drawn are generally diagrammatic and head and pipe locations may need moderate adjustment to complement actual field conditions.
- B. Layout and stake location of all system components.

3.06. **EXCAVATING AND BACKFILLING:**

- A. Excavation: Contractor shall do all necessary excavation required for the proper installation of his work. Trench to accommodate proposed grade changes and slope to drain when possible.
- B. Maintain trenches free of debris, material or obstructions that may damage pipe.
- C. Backfill: Excavated soil may be used to backfill the remainder of the trench. All Backfill material shall be free from rock, large stone or other unsuitable substances. Backfilling of trenches containing plastic piping shall be done when pipe is cool to avoid excessive contraction. All backfill material will be compacted in 6" layers as it is brought up to finish grade as to insure that no settling results. Replace topsoil and compact to consistency and level of adjacent topsoil.

3.07 **INSTALLATION OF PIPING:**

- A. All shall be installed in accordance with the manufacturer's recommendation.
- B. Mainline piping shall be installed with a minimum depth of cover of 18".
- C. Lateral piping shall be installed with a minimum depth of cover of 12".
- D. Piping shall be securely capped at the end of each day's work to prevent entrance of foreign material.

3.08 **INSTALLATION OF WIRING:**

- A. Wire shall be installed with main line piping in the same trench, but separate sleeve or conduit. At each valve, or valve box location, sufficient slack of at least 18" shall be provided to facilitate future service.
- B. There shall be no splices between valve boxes. All wire splices shall be enclosed in valve boxes. Splices shall be made by twisting conductors together with pliers and then securing with proper sized wire nut. Wire nut shall then be inserted into splice tube filled with waterproof

dielectric gel sealant. **Grease filled wire nuts shall not be allowed.**

3.09 **SLEEVING:**

- A. Pipe and wire shall be enclosed in separate Class 160 PVC sleeves at each walk, road or path crossing as shown on the irrigation plan. Coordinate locations of sleeves to accommodate system. Sleeves under existing roads, sidewalks, and paths shall be bored or cut & patched as indicated on plan..

3.10 **ROCK EXCAVATION:**

- A. If rock or other underground debris is encountered in the alignment and depth shown on the plan, the alignment and/ or depth shall be adjusted in order to avoid its excavation if at all possible. If alignment and depth adjustment cannot be made and it becomes necessary to remove the rock, the Contractor shall be paid for additional cost incurred in the handling of it.

3.11 **WATER SUPPLY:**

- A. Water source for irrigation shall be 1 ½' galvanized pipe stubbed at location indicated on plan. Contractor shall be responsible for all piping and appurtenances downstream of and including the point of connection.

3.12 **CONTROLLER INSTALLATION:**

- A. The controller shall be located and installed in irrigation enclosure. Connect to 110 VAC power supply at location. Controller must be grounded. All power, control and communication lines connected to controller shall be enclosed in conduit.
- B. TDT soil moisture sensors shall be installed at locations shown in plan. These locations are approximate and exact locations shall be determined by the City, the Irrigation Consultant and the Irrigation Contractor. The sensors shall be installed at a depth indicative of the root zone for the surrounding plant material (3" – 4" for turf). Sensors shall be installed horizontally and backfilled with original soil. Backfill shall be compacted to ensure that no voids exist around the sensor and to restore excavated soil as close as possible to original bulk density. **Install & Calibrate sensors per manufacturer's instructions.**
- C. Contractor shall configure Controller to utilize soil moisture sensors to efficiently irrigate the site. Confer with owner for any time restrictions.

3.13 **ELECTRICAL WORK:**

- A. Contractor shall be responsible for all low voltage wiring at controller location. Wiring installation shall comply with all state and local codes.

3.14 **AUTOMATIC CONTROL VALVE INSTALLATION:**

- A. All control valves shall be installed in valve boxes that provide adequate space for valve servicing without excavation. Provide waterproof connections where control wires attach to valve. Make connections using wire nuts and splice tubes filled with waterproof dielectric gel sealant. **Grease filled wire nuts shall not be allowed.** Center valve in an upright position within the valve box. **Valve boxes shall be set on a 6" base of crushed rock with brick supports on each corner.** (Buckshot or pea gravel is available free from the Owner.)

3.15 **ISOLATION VALVE INSTALLATION:**

- A. N.A.

3.16 **SPRINKLER HEAD INSTALLATION:**

- A. Sprinkler heads shall be set plumb to within 1/16" and level to finish grade and be re-adjusted and /or changed as directed after lawn areas are completed to assure proper operation

3.17 **DRIPPERLINE INSTALLATION:**

- A. N.A.

3.18 **RAIN SENSOR INSTALLATION:**

N.A.

3.19 **IMPROPERLY OPERATING OR LOCATED EQUIPMENT:**

- A. Any equipment, which fails to operate properly and/or is located incorrectly for 100% coverage shall be promptly relocated and/or removed at the direction of the Architect, Landscape Architect, Owner, and/or General Contractor, at the Irrigation Contractor's expense. Before any piping or equipment is located other than shown on the plans, permission to do so shall be obtained from the aforementioned authorities.

3.20 **PERFORMANCE TEST:**

- A. Contractor shall notify Owner that all systems are in place and ready for a performance test. The Contractor is cautioned not to notify the Owner until he, the contractor, has thoroughly observed the operation of the system and found it to be in good working order. In the event the performance test fails and additional visits are required by the Owner, the Irrigation Contractor may be liable to compensate the Owner for those additional visits.
- B. The Contractor shall provide all manpower necessary to conduct the performance test.
- C. The Contractor shall provide preliminary "As built" drawings, if different from those provided to the Irrigation Contractor, to the Owner for use during the performance test. The "As Built" drawings will be used to verify the location and operation of the equipment, if different from the original plans.

3.21 **SYSTEM IDENTIFICATION:**

- A. Location of the irrigation system heads, piping and valves shall be marked to avoid damage by others during completion of the landscaping work.

3.22 **SITE REESTABLISHMENT:**

- A. Replace planting and permanent features disturbed by installation of the underground irrigation system.

3.23 **INSTRUCTIONS:**

- A. After completion and testing of the system, the Contractor will instruct the Owner's personnel in the proper operation and maintenance of the system. Contractor shall also provide Owner, one (1) copy of the construction and maintenance manuals on all equipment installed and a manufacturer's parts catalog, as well as an as-built drawing in digital form.

3.24 **GUARANTEE:**

- A. For a period of one year from date of final acceptance of the work performed under this contract, the Contractor shall promptly furnish, without cost to the Owner, any and all parts and or labor required to correct materials or workmanship, which prove to be defective. In the fall, following the installation, the Irrigation Contractor shall drain the system by evacuating water with compressed air (including municipal side of the water supply). In the Spring, following the installation, the Irrigation Contractor shall recharge the system.

3.25 **EXCEPTIONS:**

It is recognized that the practice of Irrigation Design and installation varies considerably depending on the geographic location, prevalent soil and climate conditions and type of plants irrigated. The information contained herein is for the purpose of providing general guidelines to the contractor and inspector. Thus, specific conditions could require a deviation from these guidelines. The irrigation Contractor shall be responsible to provide all information on any deviation, regardless of size or scope. Irrigation Contractor shall provide the Owner information containing, (a) the nature of the deviation, (b) the reason for the deviation and (c) the justification for the standard used. The irrigation Contractor may make recommendations on the nature of the change, if any, however, the Owner will have the authority to make changes to the specifications. The Irrigation Contractor must obtain the written authority of the Owner before material and equipment may be changed.

North Commons Irrigation
2 PM June 6th 2012

Anderson Irrigation	Base	<u>\$ 20,809.⁶¹</u>
	Test	<u>\$ 12,822.⁶⁵</u>
	Total	<u>\$ 33,232.²⁶</u>

Crefree	Base	<u>\$ 36,015</u>
	Test	<u>\$ 20,550</u>
	Total	<u>\$ 54,565</u>

Great Northern	Base	<u>\$ 22,780</u>
	Test	<u>\$ 15,490</u>
	Total	<u>\$ 36,670</u>

Mickman Bros.	Base	<u>\$ 31,000</u>
	Test	<u>\$ 17,863</u>
	Total	<u>\$ 47,563</u>

BID FORM

IRRIGATION INSTALLATION FOR NORTH COMMONS

DESCRIPTION

UNIT PRICE

BASE QUOTE – ALL ELEMENTS OF IRRIGATION,
EXCEPT FOUR (4) TEST BEDS pursuant to specifications

\$ 20,809.61

TEST BED IRRIGATION (includes PVC supply pipe
to valve, valves, valve boxes and related parts. The City
to supply all meters with flanges with pipe thread
connection points) AND furnish **MN DOT 3877 soil**
pursuant to specifications

\$ 12,822.65

IRRIGATION CO-SPONSERSHIP DISCOUNT

(Total cost discount in favor of the City for
vendor/distributor partnership on water
conservation demonstration project)

Deduct -\$ 400.00

TOTAL NET BID (including tax and all costs, and deduct)

\$ 33,232.26

Contractor: Name Anderson Irrigation CO, INC

Address 3200 MAIN st NW ste 240 mpls mn 55448

Phone Office 763-233-0400 cell 612 282 6330

Fax 763-233-0404

E-mail Andersonirrigation@hotmail.com

Title and signature of Representative Gregory Anderson

President

Bidder agrees to complete all work under the contract by July, 2012
(Test Beds to be complete June 25th 2012)

The City reserves the right to accept or reject all bids and to award contract based upon the bid most advantageous to the City.

BID DUE BY 2:00 P.M., June 6th, 2012 at Ramsey Municipal Center, 7550 Sunwood Drive



Anderson Irrigation Acclima Projects

Anderson Irrigation pioneered the installation of "SMART" two-wire soil moisture sensing irrigation control systems in Minnesota with our first Acclima System in 2005. Since then we've established ourselves as the leaders in Soil Moisture Sensing based "SMART" irrigation control, saving our clients millions of gallons of water.

Two Wire

Rhinestone Commons Park, City of Ramsey	2005
Sunwood Drive, Ramsey Town Square	2005
Boston Scientific, Maple Grove Campus	2005
Beaudry, Kittredge and Preserve Parks, City of Otsego	2006
St. Louis Park Jr. High	2006
Boston Scientific, Maple Grove Campus	2007
The Core, Ramsey, MN	2010
North Education Center, ISD 287, New Hope, MN	2012
College Drive Improvements, City of Brainerd, MN	2012

Conventional

Since 2008 we've installed 50+ conventional Acclima Soil Moisture based systems.

3200 Main Street NW, Suite 240 Minneapolis, MN 55448

Office: 763-233-0400 Fax: 763-233-0404

Email: AndersonIrrigation@hotmail.com Website: AndersonIrrigation.com

Meeting Date: 06/12/2012

By: Colleen Lasher, Administrative Services

Information

Title:

Consider a Resolution to Reclassify Two Public Works Employees

Background:

In response to the recent changes in the Public Works Department, the City Council authorized the following changes:

- 1) Accept Mr. Brian Olson's resignation as Public Works Director.
- 2) Appoint Fire Chief Kapler to an interim assignment for up to a 3-month period to oversee the Public Works Building Maintenance, Parks, Streets, and Utilities divisions.
- 3) Appoint City Engineer Himmer to an assignment for up to a 3-month period to oversee the Engineering Division. (Mr. Himmer will oversee the Engineering until his last day of June 29th, and then a consultant engineer will be retained to manage the engineering operations.)
- 4) Separate the Engineering Division from the operations side of Public Works, creating two separate divisions; Engineering and Public Works Operations (Building Maintenance, Parks, Streets, and Utilities).
- 3) Recruit for a PW Superintendent position to oversee Building Maintenance, Parks, Streets, and Utilities; this position will report to the City Administrator.

At the June 5, 2012 meeting, the City Council directed staff to bring back a proposal related to a change wherein the City would appropriately structure the PW Operations side of the department as it pertains to the current Streets Supervisor and Parks Supervisor.

The proposal includes the following:

- 1) Reclassify the current Streets Supervisor position, held by Mr. Grant Riemer, to Public Works Superintendent. This position will report to the City Administrator and will be responsible for the oversight of the daily operations of Building Maintenance, Parks, Streets (including traffic engineering), and Utilities.
- 2) Reclassify the current Parks Supervisor position, held by Mr. Mark Riverblood, to Parks Supervisor & Assistant Public Works Superintendent. This position will report to the PW Superintendent and serve as a back-up when the PW Superintendent is away. This position will be responsible for the supervision of Parks, as well as special project management, serving as a community liason, staying abreast of legislative issues, responsible for seeking alternative funding sources (e.g., grants), and coordinating volunteer resources .
- 3) Consider any necessary backfilling or other position adjustments in approximately 9 to 18 months.

The reclassification would result in both employees receiving a pay increase of five percent.

A draft organizational chart is attached, alongwith both draft job descriptions.

Recommendation:

To authorize staff to reclassify the Streets Supervisor position to Public Works Superintendent position; and to reclassify the Parks Supervisor position to Parks Supervisor/Assistant Public Works Superintendent and adjust both employee's current hourly rate to the revised pay scale.

Funding Source:

The funding to reclassify the Parks Supervisor and Streets Supervisor as described above will require a total of \$8,800 annually.

Council Action:

Motion to approve resolution # 12-06-XXX to reclassify the Streets Supervisor to Public Works Superintendent and the Parks Supervisor to Parks Supervisor/Assistant Public Works Superintendent and adjust both employee's current hourly rate to the revised pay scale.

Attachments

Draft Org. Chart

PW Superintendent Responsibilities

PW Park Assist

Resolution

Form Review

Inbox

Kurt Ulrich

Form Started By: Colleen Lasher

Reviewed By

Kurt Ulrich

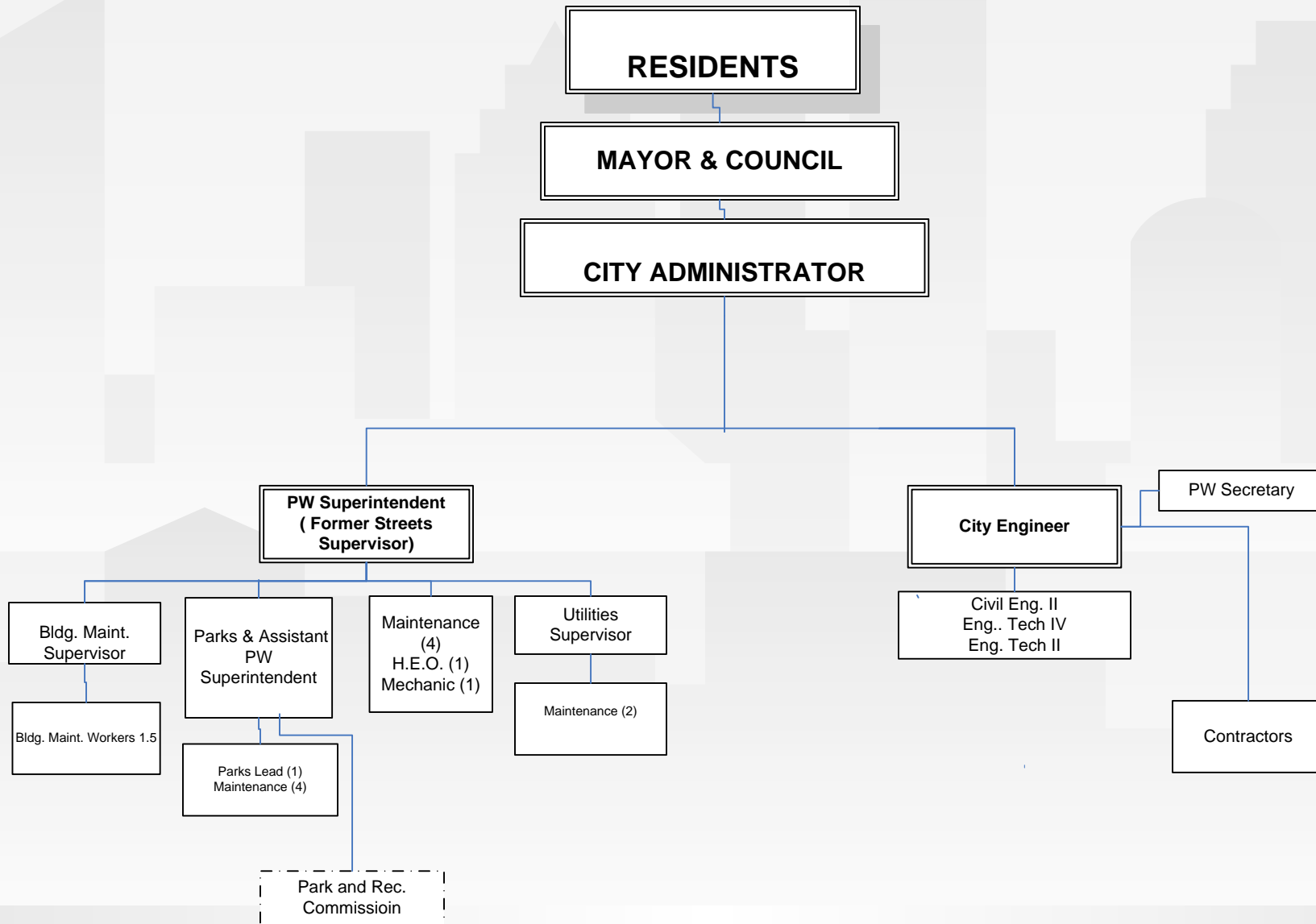
Date

06/07/2012 03:36 PM

Started On: 06/06/2012 01:53 PM

Final Approval Date: 06/07/2012

CITY OF RAMSEY PUBLIC WORKS DRAFT ORGANIZATIONAL CHART



Public Works Superintendent – Draft Job Responsibilities

This position reports to the City Administrator and is responsible for the oversight of the daily operations of Building Maintenance, Parks, Streets, and Utilities.

- Serves as a division head with responsibility to oversee the City's public works operations.
- Determines work schedules and assigns/prioritizes daily work tasks to department employees.
- Ensures employees obtain proper safety training and promotes safe work practices in the work place ensuring federal, state, and City requirements are followed.
- Undertakes or effectively recommends supervisory functions such as hiring, interviewing, evaluating performance, disciplining, and rewarding employees.
- Participates with the City Administrator in establishing goals, budget plans and implementation strategies to ensure the desired level of public works activities are being provided.
- In cooperation with the Parks/Assistant Public Works Superintendent, administers the department budgets and funds.
- Prepares and presents clear reports on public works related items to the Administrator and the City Council. Attends Public Works and Council meetings as needed.
- Represents the City in a tactful manner that commands respect of the public, contractors, developers and others.
- Plans for short- and long-term maintenance and repair work, developing a variety of maintenance programs; ensures resources are efficiently and effectively used; and maintains records for reporting and decision making purposes.
- Secures contracted services as necessary to perform a variety of maintenance activities.
- Responds to emergency situations by coordinating employees and other resources.
- Purchases supplies and materials, prepares specifications for capital items, and maintains public works inventory through continual monitoring and periodic checks.
- Develops departmental policies & procedures & makes recommendations to Administrator.
- Works in conjunction with engineers and consultants on public works projects including the design of roads, drainage, trails; and conducts inspections for City or other compliance.
- Oversees and participates in the creation of complete and accurate documentation and maintains all departmental files and records.
- Responds to and resolves citizen complaints.
- Attends relevant training to keep current on required knowledge areas and skills.
- Reviews new or different construction material and procedures; informs Administrator and Council of changes made or makes recommendations as appropriate.
- Inspects field operations to determine work progress, changing priorities, problems and material and equipment needs.
- Prepares/analyzes bid specifications for large purchases or obtains bids & quotes in accordance with statute; purchases equipment & supplies in accordance with budget & maintains related records.
- Participates in planning and zoning matters related to land use requests; reviews and provides comments on infrastructure or other public works considerations of new developments.
- Performs snow related activities.
- Perform a wide range of tasks (within the ability of the Public Works Superintendent at the oral or written direction of the City Administrator.
- Performs OSHA and Safety compliance for Public Works department
- Performs Maintenance/Inventory/Installation of all Traffic Control Devices
- Approve ROW permits for the City
- Develop and Administer Various Contracted Services for Public Works Functions
- Coordinate Oncall Program and Schedule for Public Works Employees
- Oversees the operations of the City's water facilities, wells, pumps, pump houses, lift stations and all other water or wastewater facilities.

Parks & Assistant Public Works Superintendent Responsibilities - Draft Job Responsibilities:

This position reports to the Public Works Superintendent and serves as a back-up when he/she is away.

Note: Due to limited preparation time, this list is not complete and should only be considered a draft.

- Assigns, directs, and supervises Parks employees in the operations necessary to achieve goals within available resources.
- Seeks alternative funding sources (e.g., grants) or other financial opportunities in an effort to maximize all appropriate and reasonable funding sources.
- Makes presentations to City Council, boards and commissions, civic groups and the general public.
- Acts as a community liaison in matters related to the Parks and Public Works in general.
- Manages special projects.
- Coordinates volunteers for individual and group projects related to city events and/or city parks and property while following city procedures and record keeping policies.
-
- Plans and organizes workloads and staff assignments; trains, advises, motivates and evaluates assigned staff; reviews progress and implements changes as needed within Parks operations in close coordination the Public Works Superintendent.
- Plans, supervises, and directs employees and resources for a comprehensive maintenance and development program for parks and other city owned public use areas.
- Plans and supervises projects and programs related to landscape, grounds and forestry, as well as naturalist activities.
- Communicates official plans, policies and procedures to staff and the general public.
- Performs a wide range of tasks (within the ability and resources of the Parks & Assistant Public Works Superintendent) at the oral or written direction of the Public Works Superintendent or in his/her absence, the City Administrator.

Councilmember introduced the following resolution and moved for its adoption:

RESOLUTION #

**CONSIDER A RESOLUTION TO
RECLASSIFY TWO PUBLIC WORKS EMPLOYEES**

WHEREAS, staff recommends reclassify the current Streets Supervisor position, to Public Works Superintendent. This position will report to the City Administrator and will be responsible for the oversight of the daily operations of Building Maintenance, Utilities, Streets (including traffic engineering) and high level oversight of Parks; and

WHEREAS, staff recommends reclassify the current Parks Supervisor position, to Parks & Assistant Public Works Superintendent. This position will report to the PW Superintendent and serve as a back-up when the PW Superintendent is away. This position will be responsible for the supervision of Parks, as well as PW project management, serving as a community liaison, staying abreast of legislative issues, and seeking any applicable/reasonable funding sources; and

WHEREAS, staff recommends considering any necessary backfilling or other position adjustments in approximately 9 to 18 months.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:

- 1) To reclassify the Streets Supervisor to Public Works Superintendent.
- 2) To reclassify the Parks Supervisor to Parks Supervisor & Assistant Public Works Superintendent.
- 3) Adjust both employees' current hourly rate to the revised pay scale.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember , and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 12th day of June 2012.

CC Regular Session

7.7.

Meeting Date: 06/12/2012

Submitted For: Kurt Ulrich

By: Bill Goodrich, Administrative Services

Information

Title:

Update on Required Land Acquisitions for the Sunwood Drive Realignment Project - **PORTIONS OF THIS DISCUSSION MAY BE CLOSED TO THE PUBLIC**

Background:

The property/easement acquisition for the Sunwood Drive Realignment Project is progressing. The status of the property acquisitions and the owners/property addresses are as follows:

<u>Property Address</u>	<u>Owner</u>	<u>Negotiation Status</u>
14590 Armstrong Blvd.	M&W Holding	Purchase agreement pending. Further discussion in closed session.
8019 – 146th Avenue NW	USCO Corp	Purchase agreement pending. Further discussion in closed session.
8020 -147th Avenue NW	Solveig B. Nordvik Trust	Terms of purchase agreement will be presented to City Council in closed session for final approval.
14700 Armstrong Blvd. (easement only)	Arnie R. Billmark	Council approved purchase terms at its May 8, 2012 meeting. Purchase price is \$44,255.00 plus City paid hook-up to sewer service, which brings the total cost to \$51,296.00.

It is estimated that the total acquisition cost of all the properties/easements to be acquired is \$2,700,000.00. Please note this amount does not include the potential proceeds from the sale of a lot in the COR from a property owner negotiating such a purchase from the City.

Observations:

A breakdown of all costs attributable to the Sunwood Realignment Project are as follows:

Land Acquisition	\$2,700,000
Armstrong Blvd	\$1,650,000
Sunwood Drive	\$1,075,000
COR Grading & Northstar Market Place Access Revisions	\$210,000
Streetlighting	\$112,000
Engineering/Construction Administration/Testing	\$470,000
TOTAL	\$6,217,000

The funding package for this project, as previously approved by the City Council is as follows:

Anoka County	\$1,700,000
LRIP Grant	\$500,000

Equipment Revolving Fund	\$1,056,450
TIF #2	\$500,000
Stormwater Utility Fund	\$255,484
Water Utility Fund	\$238,066
EDA (Nordvik acquisition)	\$600,000
Excess Rail Funding	\$500,000
TOTAL	\$5,350,000

A revised accounting, based upon the most current estimates, is as follows:

Anoka County	\$1,700,000
LRIP Grant	\$500,000
Equipment Revolving Fund	\$1,056,450
TIF #2	\$500,000
Stormwater Utility Fund	\$230,500
Water Utility Fund	\$210,000
Sanitary Sewer Utility Fund	\$19,000
EDA (Nordvik acquisition)	\$600,000
Excess Rail Funding	\$500,000
Streetlight Utility	\$112,000
COR land sale proceeds *	\$500,000
TOTAL	\$5,927,950

As can be seen above there is approximately \$289,000 that is unaccounted for with this project. This overage is primarily the costs for streetlighting and construction administration/testing. Staff intends to make adjustments for a portion of the soft costs (engineering/testing) such that the appropriate enterprise funds pay their pro-rata share. City staff was preparing to perform the construction administration on this project, but due to recent staffing changes we will have to review this matter to determine what can be accomplished in house, and what will have to be contracted. It is proposed that the Streetlight Utility will fund the streetlighting component of this project. The Housing and Redevelopment Authority (HRA) will have to discuss the COR land sale proceeds referenced above to determine whether they are interested in deviating from their land sale distribution policy, and apply the entire amount of a pending land sale to this improvement project. A final funding package will be brought forward, clarifying these matters, after the bids are opened and a project is proposed to be awarded (June 26, 2012).

Funding Source:

The funding for this project has been identified above.

Council Action:

This case is strictly informational in nature, no action is required.

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date

06/07/2012 03:40 PM

Form Started By: Bill Goodrich

Started On: 06/06/2012 02:17 PM

Final Approval Date: 06/07/2012