

City of Ramsey
Agenda
Regular City Council
Tuesday November 27, 2012

7:00 pm
Council Chambers, 7550 Sunwood Drive NW

- 1. Call to Order**
- 2. Presentation**
 - 1. Presentation and Swearing in of new Police Officer**
- 3. Citizen Input**
- 4. Consent Agenda**
 1. Receive October 2012 Financial Reports - General Fund and Enterprise Funds
 2. Note the Following Boards Meeting Minutes:

Environmental Policy Board Meeting Minutes Dated September 10, 2012
Economic Development Authority Meeting Minutes Dated October 11, 2012
 3. Approve License Applications
 4. Approve the Following Meeting Minutes:
 1. Special City Council - October 16, 2012
 2. City Council Work Session - October 23, 2012
 3. City Council Regular - October 23, 2012
 4. Canvassing Board - November 13, 2012
 5. City Council Work Session - November 13, 2012
 6. City Council Regular - November 13, 2012
 5. Approval for Exemption for a Gambling License for Simply Outdoor Experiences to Conduct a Raffle at 8019 - 146th Avenue NW - Holte Contracting Office
 6. Approve Highway 10 access easement to benefit property located at 6781 Highway 10 NW.
 7. Approve Joint Powers Agreement between the City of Ramsey and County of Anoka for Assessment of Property
 8. Authorize Entering into a Joint Powers Agreement for Conduct of Elections between Anoka County and the Municipalities, Townships and School Districts in Anoka County to Allocate Costs for Election Expenses

9. Adopt Resolution #12-11-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of November 8, 2012 through November 20, 2012
 10. Adopt Resolution #12-11-XXX Authorizing Partial Payment to North Pine Aggregate for IP 12-20 Armstrong/Sunwood Drive
 11. Adopt Resolution #12-11-XXX Authorizing Partial Payment to North Valley Inc. for IP 12-23 Alpine Drive Reconstruction.
5. **Approve Agenda**
 6. **Public Hearing**
 7. **Council Business**
 1. Adopt Ordinance #12-XX Adopting 2013 Schedule of Rates, Fees & Charges
 2. Adopt Ordinance to Amend City Code Section 117-118 (The COR) Approving Amendment to The COR Design Framework Related to Off-Street Parking, Signs, Parks and Trails, and Sub-District Boundaries
 3. Adopt Ordinance to Amend City Code Chapter 117, Article II, Division 8 (Signs) Related to Temporary, Construction, Real Estate, and Off-Premise Signs
 4. Adopt Ordinance Vacating Portions of Drainage and Utility Easement at 6012 146th Ave NW; Case of Jesse and Dana VerBeek
 5. Consider entering into a purchase agreement for the property located at 8019 -146th Avenue NW, Minnesota owned by USCO Corp. -**PORTIONS OF THIS DISCUSSION MAY BE CLOSED TO THE PUBLIC.**
 6. Attorney-Client Privileged Discussion Regarding the pending Isanti County District Court lawsuit titled "County of Isanti, Plaintiff, vs. Keith Kiefer, Defendant, vs. City of Ramsey and City Administrator, Kurtis Ulrich, in his official capacity and individually, Third Party Defendants."
 8. **Mayor/Council/Staff Input**
 - 1) **There will be no Council Work Session on Tuesday, December 4, 2012**
 9. **Adjournment**

CC Regular Session

4. 1.

Meeting Date: 11/27/2012

By: Diana Lund, Finance

Information

Title:

Receive October 2012 Financial Reports - General Fund and Enterprise Funds

Background:

Brief summary of actual revenues and expenditures to date in comparison to adopted budget for the funds of General, Water, Sewer, Street Lighting, Recycling and Storm Drainage.

Council Action:

No Action Required. Informational Only.

Attachments

October 2012 Financials General Fund - Budget to Actual

October 2012 Financials Enterprise Funds - Budget to Actual

Form Review

Inbox

Kurt Ulrich

Reviewed By

Kurt Ulrich

Date

11/21/2012 11:36 AM

Form Started By: Diana Lund

Started On: 11/19/2012 01:17 PM

Final Approval Date: 11/21/2012

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

GENERAL FUND EXPENDITURES - BY DEPARTMENT -		
Dept	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Admin	1,423,788.00	1,165,587.45
Com Dev	528,852.00	392,960.75
Contingency	230,648.00	103,253.89
Council	133,951.00	103,157.66
Finance	454,044.00	445,097.25
Fire	872,656.00	507,910.26
Legal	121,000.00	96,456.53
Police	3,121,261.00	2,347,282.21
Public Works	2,761,875.00	1,631,703.99
Grand Total	9,648,075.00	6,793,409.99

GENERAL FUND EXPENDITURES - BY CATEGORY -		
Category	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Capital Outlay	340,252.00	185,712.80
Debt Service	213,113.00	107,458.02
Other Services & Charges	2,094,613.00	1,134,217.48
Personal Services	6,232,372.00	4,863,677.58
Supplies	767,725.00	502,344.11
Grand Total	9,648,075.00	6,793,409.99

GENERAL FUND REVENUES - BY CATEGORY -		
Category	-2012 ADOPTED BUDGET-	-2012 YTD GENERAL LEDGER-
Taxes	7,090,150.00	3,604,160.87
Charges for Services	543,189.00	348,731.79
Business Licenses/Permits	72,020.00	63,944.83
Fines and Forfeits	108,000.00	55,794.64
Federal Intergovernmental	7,000.00	(7,485.38)
State Intergovernmental	298,300.00	301,131.53
Interest	80,000.00	-
Miscellaneous	30,500.00	9,328.15
Non-Business Licenses/Permits	364,300.00	420,257.84
Transfers in	1,054,616.00	1,054,616.00
Grand Total	9,648,075.00	5,850,480.27

**CITY OF RAMSEY
FINANCIAL STATEMENT**



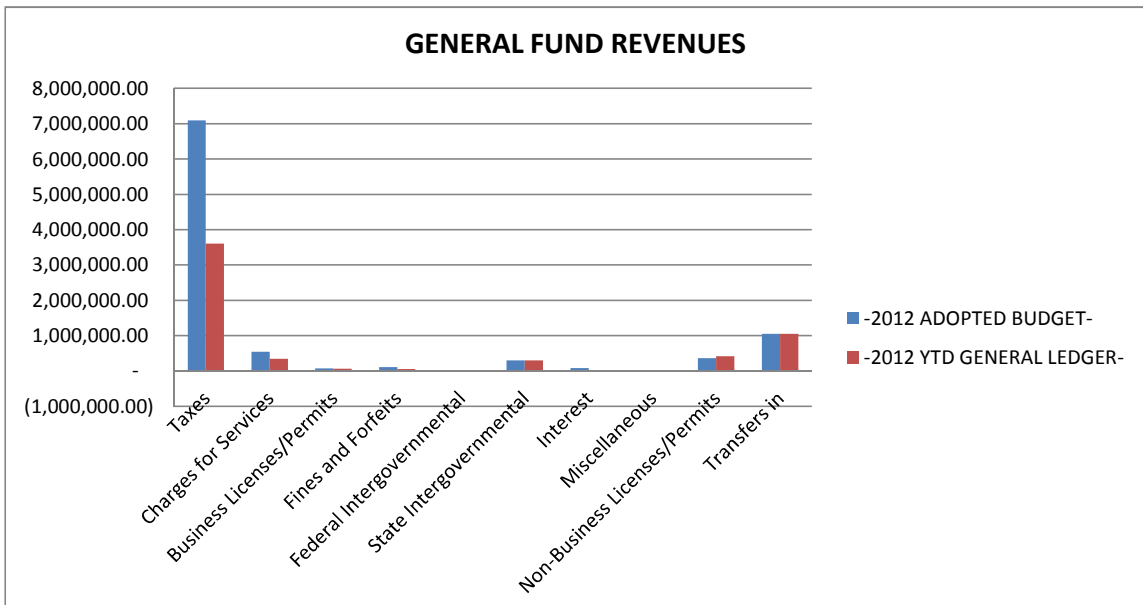
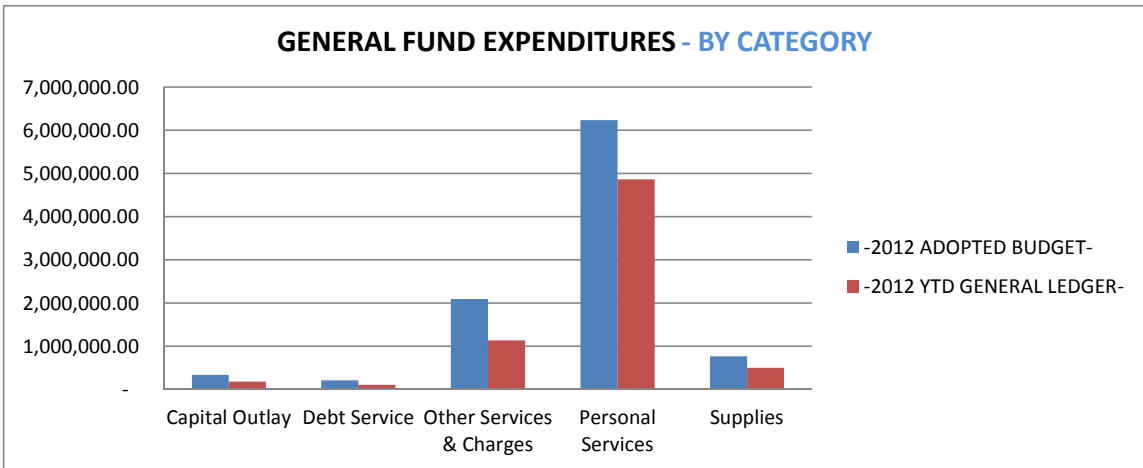
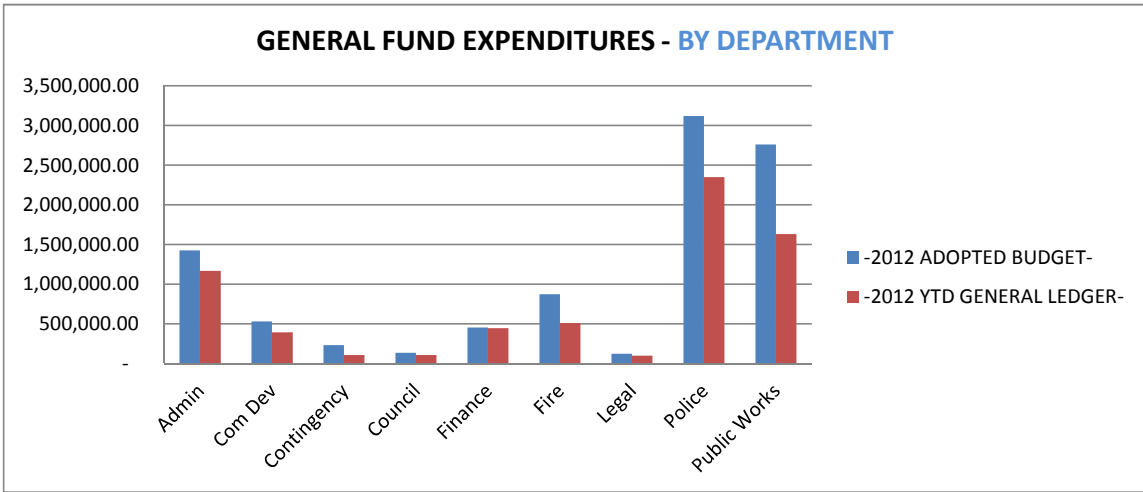
JANUARY 1, 2012 THROUGH PERIOD ENDING October 31, 2012

GENERAL FUND REVENUES			
BUSINESS UNIT	(Multiple Items)		
GENERAL LEDGER ACCOUNT	Sum of 2012 REQUESTED BUDGET	Sum of 2012 YTD GENERAL LEDGER	
4011 CURRENT-AD VALOREM TAXES	5,947,150.00		2,851,157.37
4012 DELINQUENT-AD VALOREM TAXES	60,000.00		68,046.52
4014 FISCAL DISPARITIES	1,050,000.00		639,151.88
4015 EXCESS TAX INCREMENTS	28,000.00		38,284.72
4018 PENALTY/INT-AD VALOREM TAXES	5,000.00		7,520.38
4140 CREDIT CARD PROCESSING FEES	-		(2,704.82)
4155 LIQUOR-ON SALE	40,000.00		42,660.00
4156 LIQUOR-OFF SALE	1,920.00		1,120.00
4157 NON-INTOXICATING LIQUOR	400.00		-
4159 MECHANICAL LICENSE	7,500.00		4,850.00
4163 PAWNSHOP LICENSE	8,000.00		7,725.65
4164 CIGARETTE SALES LICENSE	4,500.00		1,850.00
4165 REFUSE HAULERS LICENSE	400.00		400.00
4166 MOTOR VEHICLES LICENSE	5,000.00		4,100.00
4168 PEDDLERS LICENSE	1,000.00		1,155.00
4169 GASOLINE SALES LICENSE	1,800.00		1,500.00
4170 OTHER BUSINESS LICENSES & PERM	1,500.00		575.00
4171 INVESTIGATIVE FEES	-		714.00
4205 BUILDING PERMIT	254,600.00		267,636.75
4206 PLUMBING PERMIT	20,000.00		32,960.68
4207 ANIMAL LICENSE	700.00		1,299.75
4208 HEATING PERMIT	20,000.00		32,402.25
4209 CONDITIONAL USE PERMIT	5,000.00		8,630.00
4211 SIGN PERMITS	2,000.00		1,825.00
4212 RENTAL LICENSE	1,000.00		-
4213 FIRE PERMIT	2,000.00		5,202.75
4214 ELECTRICAL INSPECTION PERMIT	30,000.00		36,079.00
4220 SEPTIC SYSTEM PERMIT	15,000.00		24,167.00
4221 URBAN SEWER PERMIT	7,000.00		3,900.00
4222 URBAN WATER PERMIT	7,000.00		3,900.00
4230 OTHER NON-BUSINESS LIC & PERM	-		2,254.66
4253 FEDERAL EXCISE TAX REFUND	7,000.00		(7,485.38)
4268 MSA FOR STREETS	140,000.00		140,000.00
4269 POLICE - INSURANCE PREMIUM TAX	150,000.00		149,407.00
4271 POST BOARD REIMBURSEMENT	8,000.00		7,638.06
4272 STATE EXCISE TAX REFUND	300.00		-
4273 OTHER STATE GRANTS & AIDS	-		2,784.00
4304 RENTAL FEES - REAL PROPERTY			8,397.96
4305 RENTAL FEES	105,000.00		74,966.25
4306 ZONING & SUBDIVISION FEES	-		1,250.00
4307 PLAN CHECKING FEES	125,020.00		146,785.19
4308 SALES OF MAPS & PUBLICATIONS	200.00		747.00
4309 ASSESSMENT SEARCHES	3,000.00		3,445.00
4312 GENERAL GOVERNMENT STAFF TIME	9,661.00		4,124.50
4326 SPECIAL POLICE SERVICES	60,000.00		41,250.00
4327 SPECIAL FIRE PROTECTION SERVIC	20,000.00		32,811.04
4328 ACCIDENT REPORTS	1,200.00		1,199.25
4329 OPEN BURN PERMIT FEES	1,000.00		1,140.00
4330 OTHER PUBLIC SAFETY	10,000.00		6,235.10
4337 ENGINEERING	196,108.00		11,754.00
4338 PLAN & SPECIFICATION FEES	4,000.00		1,650.00
4339 OTHER PUBLIC WORKS	8,000.00		4,796.50
4452 COURT FINES	105,000.00		49,506.69
4454 ADMINISTRATIVE FINES	3,000.00		6,204.12
4604 SURCHARGES	500.00		628.47
4609 OTHER MISCELLANEOUS REVENUES	30,000.00		8,629.68
4701 INTEREST ON INVESTMENTS	80,000.00		-
4901 TRANSFER IN FROM OTHER FUNDS	1,054,616.00		1,054,616.00
4605 ELECTION FILING FEES	-		70.00
4347 OTHER CULTURE-RECREATION	-		8,180.00
4453 OTHER FINES & FORFEITS	-		83.83
4263 MARKET VALUE HOMESTEAD CREDIT	-		1,302.47
Grand Total	9,648,075.00		5,850,480.27

**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012



**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

REVENUES				
BUSINESS UNIT	9601	WATER UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
4140 CREDIT CARD PROCESSING FEES		(3,285.30)		0.00%
4273 OTHER STATE GRANTS & AIDS		7,074.00		0.00%
4609 OTHER MISCELLANEOUS REVENUES	61,853.00	64,754.74		104.69%
4651 WATER REVENUE		957.39		0.00%
4652 WATER SALES - RESIDENTIAL	963,401.00	1,015,807.79		105.44%
4653 WATER SALES-COMMERCIAL	676,702.00	701,209.72		103.62%
4654 WATER PENALTIES	32,802.00	22,365.32		68.18%
4655 WATER METER INSTALLATION	5,000.00	7,560.00		151.20%
4656 WATER METERS	14,000.00	18,328.64		130.92%
4657 CONNECTION/RECONNECTION FEES	4,000.00	2,000.00		50.00%
4701 INTEREST ON INVESTMENTS	150,000.00	-		0.00%
4506 PREPAID INTEREST		3,512.83		0.00%
4702 MISCELLANEOUS INTEREST		12,882.67		0.00%
Grand Total	1,907,758.00	1,853,167.80		

EXPENSES				
BUSINESS UNIT	9601	WATER UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
6102 F.T. REGULAR-WAGES & SALARIES	204,354.00	114,038.37		55.80%
6103 FULL TIME-REGULAR-OVERTIME	12,500.00	10,566.57		84.53%
6105 TEMPORARY-WAGES & SALARIES	17,000.00	9,452.89		55.61%
6121 PERA CONTRIBUTIONS	15,722.00	9,527.72		60.60%
6122 FICA/MEDICARE CONTRIBUTIONS	17,890.00	10,645.57		59.51%
6131 GROUP INSURANCE	20,470.00	16,957.00		82.84%
6133 WORKERS COMP INSURANCE PREMIUM	7,436.00	38.76		0.52%
6208 MISCELLANEOUS OFFICE SUPPLIES	1,000.00	-		0.00%
6223 GASOLINE	5,000.00	2,874.84		57.50%
6225 DIESEL FUEL	7,000.00	6,587.74		94.11%
6229 SHOP MATERIALS	750.00	118.11		15.75%
6231 UNIFORMS & TURN-OUT GEAR	1,500.00	664.84		44.32%
6249 MISCELLANEOUS OPERATING SUPPLY	13,000.00	12,521.39		96.32%
6257 OTHER VEHICLE PARTS	2,500.00	862.99		34.52%
6273 UTILITY SYSTEM MAINT SUPPLIES	70,000.00	61,754.00		88.22%
6281 SMALL TOOLS & MINOR EQUIPMENT	110,000.00	3,413.83		3.10%
6292 WATER METERS FOR RESALE	20,000.00	3,161.56		15.81%
6315 MISCELLANEOUS PROFESSIONAL SER	54,330.00	26,754.82		49.25%
6322 POSTAGE	2,000.00	1,015.32		50.77%
6323 CELLULAR PHONES	2,400.00	1,907.77		79.49%
6335 TRAINING	1,600.00	1,055.72		65.98%
6352 GENERAL NOTICE & PUBLIC INFOR	600.00	153.75		25.63%
6361 GENERAL LIABILITY/PROPERTY INS	24,000.00	-		0.00%
6371 ELECTRIC UTILITIES	125,000.00	88,514.01		70.81%
6372 WATER/IRRIGATION	800.00	-		0.00%
6373 GAS	4,000.00	1,044.37		26.11%
6374 REFUSE/RECYCLING	600.00	275.32		45.89%
6381 BUILDING & STRUCTURE REPAIR	500.00	-		0.00%
6439 OTHER MISCELLANEOUS	17,000.00	11,641.75		68.48%
6451 MEMBERSHIP DUES	800.00	1,089.50		136.19%
6489 OTHER CONTRACTED SERVICES	27,000.00	14,143.91		52.38%
6722 DEPRECIATION	623,308.00	-		0.00%
6820 OPERATING TRANSFERS TO OTHER F	34,000.00	34,000.00		100.00%
Grand Total	1,444,060.00	444,782.42		

Note: The Finance Department has highlighted line items that may be trending towards exceeding budget OR not may not have been included in the adopted budget.

**CITY OF RAMSEY
FINANCIAL STATEMENT**



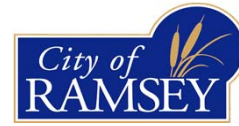
JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

REVENUES			
BUSINESS UNIT	9602	SEWER UTILITY	
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-
4140 CREDIT CARD PROCESSING FEES		(2,592.76)	0.00%
4356 SEWER AVAILABILITY CHARGE-ADM	1,000.00	6,574.70	657.47%
4609 OTHER MISCELLANEOUS REVENUES	18,546.00	-	0.00%
4661 RESIDENTIAL-SEWER CHARGES	925,057.00	709,341.17	76.68%
4662 COMMERCIAL-SEWER CHARGES	311,381.00	237,867.25	76.39%
4663 SEWER PENALTIES	24,729.00	21,876.67	88.47%
4701 INTEREST ON INVESTMENTS	50,000.00	-	0.00%
4506 PREPAID INTEREST		1,223.27	0.00%
4702 MISCELLANEOUS INTEREST		18,545.98	0.00%
Grand Total	1,330,713.00	992,836.28	

EXPENSES			
BUSINESS UNIT	9602	SEWER UTILITY	
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-
6102 F.T. REGULAR-WAGES & SALARIES	100,563.00	34,559.87	34.37%
6103 FULL TIME-REGULAR-OVERTIME	2,000.00	1,014.89	50.74%
6105 TEMPORARY-WAGES & SALARIES	5,500.00	2,549.65	46.36%
6121 PERA CONTRIBUTIONS	7,436.00	2,600.18	34.97%
6122 FICA/MEDICARE CONTRIBUTIONS	8,267.00	2,556.45	30.92%
6133 WORKERS COMP INSURANCE PREMIUM	2,798.00	38.76	1.39%
6225 DIESEL FUEL	2,500.00	1,043.10	41.72%
6249 MISCELLANEOUS OPERATING SUPPLY	10,000.00	5,883.24	58.83%
6257 OTHER VEHICLE PARTS		202.18	0.00%
6275 OTHER EQUIPMENT PARTS	2,500.00	312.53	12.50%
6281 SMALL TOOLS & MINOR EQUIPMENT		221.25	0.00%
6315 MISCELLANEOUS PROFESSIONAL SER	16,000.00	31,078.94	194.24%
6335 TRAINING	1,500.00	646.00	43.07%
6361 GENERAL LIABILITY/PROPERTY INS	9,500.00	-	0.00%
6371 ELECTRIC UTILITIES	9,500.00	6,612.86	69.61%
6373 GAS	2,400.00	658.98	27.46%
6374 REFUSE/RECYCLING	500.00	275.32	55.06%
6375 SEWER		45,089.42	0.00%
6377 SEWER SERVICE CHARGE	541,073.00	450,894.20	83.33%
6489 OTHER CONTRACTED SERVICES	22,000.00	28,320.12	128.73%
6722 DEPRECIATION	497,434.00	-	0.00%
6820 OPERATING TRANSFERS TO OTHER F	28,000.00	28,000.00	100.00%
Grand Total	1,269,471.00	642,557.94	

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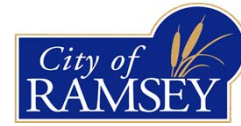
JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

REVENUES				
BUSINESS UNIT	9603	STREET LIGHT UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
4140 CREDIT CARD PROCESSING FEES		(330.18)	0.00%	
4681 CHARGES FOR STREET LIGHTS	168,312.00	96,798.04	57.51%	
4682 ST LIGHT O/M CHARGE	360.00	-	0.00%	
4683 STREET LIGHTING PENALTIES	3,366.00	3,744.82	111.25%	
4701 INTEREST ON INVESTMENTS	20,000.00	-	0.00%	
4684 PRIORITY STREET LIGHT		33,404.92	0.00%	
Grand Total	192,038.00	133,617.60		

EXPENSES				
BUSINESS UNIT	9603	STREET LIGHT UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
6122 FICA/MEDICARE CONTRIBUTIONS		326.61	0.00%	
6271 SIGN REPAIR MATERIALS	-	-	0.00%	
6371 ELECTRIC UTILITIES	122,000.00	92,114.03	75.50%	
6489 OTHER CONTRACTED SERVICES	12,328.00	8,226.45	66.73%	
6722 DEPRECIATION	33,997.00	-	0.00%	
6820 OPERATING TRANSFERS TO OTHER F	14,000.00	14,000.00	100.00%	
Grand Total	182,325.00	114,667.09		

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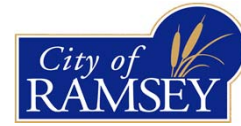
JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

REVENUES				
BUSINESS UNIT	9604	RECYCLING UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
4140 CREDIT CARD PROCESSING FEES		(458.36)	0.00%	
4287 OTHER LOCAL GOVERNMENT GRANTS	50,165.00	50,165.00	100.00%	
4609 OTHER MISCELLANEOUS REVENUES	-	696.85	0.00%	
4671 RECYCLING CHARGES	280,000.00	215,955.77	77.13%	
4672 RECYCLING PENALTIES	5,600.00	5,678.92	101.41%	
4701 INTEREST ON INVESTMENTS	200.00	-	0.00%	
Grand Total	335,965.00	272,038.18		

EXPENSES				
BUSINESS UNIT	9604	RECYCLING UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
6102 F.T. REGULAR-WAGES & SALARIES	5,143.00	4,192.68	81.52%	
6103 FULL TIME-REGULAR-OVERTIME	-	559.70	0.00%	
6121 PERA CONTRIBUTIONS	373.00	351.07	94.12%	
6122 FICA/MEDICARE CONTRIBUTIONS	393.00	294.44	74.92%	
6133 WORKERS COMP INSURANCE PREMIUM	87.00	-	0.00%	
6249 MISCELLANEOUS OPERATING SUPPLY	7,000.00	5,000.22	71.43%	
6322 POSTAGE	250.00	150.60	60.24%	
6489 OTHER CONTRACTED SERVICES	299,000.00	242,346.78	81.05%	
6820 OPERATING TRANSFERS TO OTHER F	8,500.00	8,500.00	100.00%	
Grand Total	320,746.00	261,395.49		

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**CITY OF RAMSEY
FINANCIAL STATEMENT**



JANUARY 1, 2012 THROUGH PERIOD ENDING: October 31, 2012

REVENUES				
BUSINESS UNIT	9605	STORM WATER UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
4140 CREDIT CARD PROCESSING FEES		(474.75)	0.00%	
4693 STORM WATER-RESIDENTIAL	305,790.00	234,337.10	76.63%	
4694 STORM WATER-COMMERCIAL	302,629.00	240,277.30	79.40%	
4695 STORM WATER-PENALTIES	12,168.00	10,324.54	84.85%	
4701 INTEREST ON INVESTMENTS	5,000.00	-	0.00%	
Grand Total	625,587.00	484,464.19		

EXPENSES				
BUSINESS UNIT	9605	STORM WATER UTILITY		
GENERAL LEDGER ACCOUNT	-2012 REQUESTED BUDGET-	-2012 YTD GENERAL LEDGER-	-% of Budget-	
6102 F.T. REGULAR-WAGES & SALARIES	86,991.00	51,556.93	59.27%	
6103 FULL TIME-REGULAR-OVERTIME	-	103.64	0.00%	
6105 TEMPORARY-WAGES & SALARIES	-	977.25	0.00%	
6121 PERA CONTRIBUTIONS	6,307.00	3,745.87	59.39%	
6122 FICA/MEDICARE CONTRIBUTIONS	6,655.00	3,925.46	58.99%	
6133 WORKERS COMP INSURANCE PREMIUM	2,894.00	80.76	2.79%	
6223 GASOLINE	2,000.00	1,104.18	55.21%	
6225 DIESEL FUEL	300.00	1,142.99	381.00%	
6249 MISCELLANEOUS OPERATING SUPPLY	13,000.00	4,703.19	36.18%	
6257 OTHER VEHICLE PARTS	7,000.00	2,108.00	30.11%	
6315 MISCELLANEOUS PROFESSIONAL SER	42,000.00	45,801.08	109.05%	
6361 GENERAL LIABILITY/PROPERTY INS	5,000.00	-	0.00%	
6371 ELECTRIC UTILITIES	2,420.00	1,512.98	62.52%	
6373 GAS	2,500.00	658.93	26.36%	
6374 REFUSE/RECYCLING	500.00	275.27	55.05%	
6451 MEMBERSHIP DUES	39,162.00	39,162.00	100.00%	
6489 OTHER CONTRACTED SERVICES	15,200.00	36,637.17	241.03%	
6722 DEPRECIATION	235,517.00	-	0.00%	
6820 OPERATING TRANSFERS TO OTHER F	23,000.00	23,000.00	100.00%	
Grand Total	490,446.00	216,495.70		

Note: The Finance Department has highlighted line items that may be trending towards exceeding budget OR not may not have been included in the adopted budget.

CC Regular Session

4. 2.

Meeting Date: 11/27/2012

By: JoAnn Shaw, Community Development

Information

Title:

Note the Following Boards Meeting Minutes:

Environmental Policy Board Meeting Minutes Dated September 10, 2012
Economic Development Authority Meeting Minutes Dated October 11, 2012

Attachments

[EPB 9.10.12](#)

[EDA 10.11.12](#)

Form Review

Inbox
Kurt Ulrich

Form Started By: JoAnn Shaw

Reviewed By
Kurt Ulrich

Final Approval Date: 11/21/2012

Date
11/21/2012 11:13 AM
Started On: 11/14/2012 09:28 AM

**ENVIRONMENTAL POLICY BOARD
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

On Monday, September 10, 2012, the Environmental Policy Board (EPB) met in the Rum River Conference Room at the Ramsey Municipal Center, 7550 Sunwood Drive N.W., Ramsey, Minnesota.

Members Present: Chairperson Michael Max
 Board Member Bob Bentz
 Board Member Michael Hiatt
 Board Member Larry Lewis (arrived at 6:37 p.m.)
 Board Member Michael Valentine

Members Absent: Board Member John Enstrom
 Board Member Thomas Stodola

Also Present: Associate Planner/Environmental Coordinator Chris Anderson

1. CALL TO ORDER

Chairperson Max called the meeting to order at 6:30 p.m.

2. CITIZEN INPUT

None.

3. APPROVE AGENDA

Motion by Board Member Hiatt and seconded by Board Member Valentine to approve the agenda as submitted.

Motion carried. Voting Yes: Chairperson Max, Board Members Hiatt, Valentine, and Bentz.
Voting No: None. Absent: Board Members Enstrom, Lewis and Stodola.

4. APPROVE MINUTES

4.01: Approve Meeting Minutes Dated August 6, 2012

Motion by Board Member Valentine and seconded by Board Member Hiatt to approve the regular meeting minutes dated August 6, 2012.

Motion carried. Voting Yes: Chairperson Max, Board Members Valentine, Hiatt, and Bentz.
Voting No: None. Absent: Board Members Enstrom, Lewis, and Stodola.

5. POLICY BOARD BUSINESS

5.01: Review and Consider Potential Icons or Symbols to Brand Environmental Information

Associate Planner/Environmental Coordinator Anderson presented the staff report. He provided his input on each of the options, noting that he would not prefer the brown oak leaf as he believed that would make people think of oak wilt. He stated that he likes the magnifying glass but expressed concern that the picture would not be clear when reduced down.

Board Member Lewis arrived at 6:37 p.m.

Chairperson Max commented that he likes the R symbol included in the packet.

Board Member Lewis provided some conceptual ideas that a friend of his drafted for the Board, which would work well in small or large scale. He stated that these ideas are simply concepts and would like to give his friend more time to draft some more ideas for the Board to review. He stated that while he liked the ideas brought forward from staff, he did not believe the symbols would translate well to different forms of media or handouts.

Associate Planner/Environmental Coordinator Anderson stated that he would not be opposed to that idea and noted that while he would like the symbol to be chosen prior to the next issue of the newsletter; the new symbol could also be used in the first newsletter of 2013.

Board Member Bentz commented that the symbol should not need an explanation and should translate well into black and white, as he believed that would be the form used most often.

Chairperson Max questioned if any other Boards have their own symbol or identification.

Associate Planner/Environmental Coordinator Anderson confirmed that there is not another Board with a symbol or identifier. He explained that he thought the purpose of the logo or icon was not to identify the Board but to identify environmental items in things such as the newsletter.

Board Member Lewis commented that he did not believe there is anything problematic with the Board developing a symbol such as this to identify things the Board is involved in, such as the tree sale.

Board Member Hiatt suggested that the symbol focus more on the "E", for environment, with the "PB", for Policy Board, much smaller.

Board Member Lewis confirmed that the logo would include the "PB", whereas the icon would focus solely on the "E". He confirmed that the "E" would stand out, as to not confuse people into thinking the "E" is the start letter for a paragraph. He confirmed that the Board does not object to giving his friend some additional time to further work on the icon/logo for the Board. He noted that his friend is donating his time, as a designer, which is a significant cost savings.

5.02: Article and Space Filler Discussion

Associate Planner/Environmental Coordinator Anderson presented the staff report. He advised that the draft article requested by the LRRWMO was drafted by Board Member Stodola. Associate Planner/Environmental Coordinator Anderson also noted that at last month's meeting there was a request for everyone to develop some "space fillers" for use in the newsletter and that he has provided a half dozen or so for review by the Board.

Board Member Lewis commented that he was able to find some interesting environmental facts on the internet.

Chairperson Max believed that it is good for these "space fillers" to be available to the editor, as open space in the newsletter could then easily be filled with environmental information.

Board Member Lewis noted that the article could be highlighted by the icon, once developed, which would easily mark the item as environmental information.

Associate Planner/Environmental Coordinator Anderson further discussed the recycling article and the recycling goals set by Anoka County each year.

Board Member Lewis stated that it might be beneficial to highlight the reimbursement, which the City receives from the County by meeting those goals. He explained that residents might be willing to put more effort into recycling if they knew that the City is able to receive funding from the County for reaching those recycling goals.

Associate Planner/Environmental Coordinator Anderson commented that approximately 90 percent of residents are recycling to some extent. He stated that the greater gain would be to focus on those who are already recycling and educate them on additional items that can be recycled, rather than trying to recruit those who do not recycle.

Board Member Bentz advised that some residents on his block have downgraded to a smaller garbage can because of the amount they are recycling, which is a tangible benefit of recycling. He noted that his neighbors would really like the City to contract with one hauler for garbage services to avoid the number of different garbage trucks running through the neighborhoods.

Associate Planner/Environmental Coordinator Anderson stated that he did not believe that is likely to happen. He commented that he would incorporate some of the items discussed tonight into the articles and questioned if the Board feels that any other issues should be spotlighted in the upcoming issue of the newsletter.

Chairperson Max commented that he believed it would be beneficial to have a standalone article that focuses on holiday recycling. He also noted that it could be beneficial to have an article, which focuses on the many items that can be recycled, and some of the creative local re-uses for those materials.

Associate Planner/Environmental Coordinator Anderson confirmed that the November/December article would focus on holiday recycling. He confirmed that the Board would meet again prior to the deadline for the January/February newsletter.

Board Member Lewis commented that it would be beneficial to have a few articles stockpiled to use.

Chairperson Max confirmed that because the next EPB article for the Ramsey newsletter will focus on holiday recycling, another community within the LRRWMO could print the article before Ramsey does.

Associate Planner/Environmental Coordinator Anderson stated that he would be comfortable drafting the holiday recycling article, as he is proficient in the knowledge of the City program. He noted that the Board may not be able to review the article prior to the deadline for the article but advised that the Board would be able to review the article at the next meeting, prior to resident receipt.

6. BOARD / STAFF INPUT

- **Community Parks Map**

Associate Planner/Environmental Coordinator Anderson advised that he was asked to draft a map, which identifies the larger community parks and has done so and distributed a copy to each Board Member.

- **Fall Recycling Day**

Associate Planner/Environmental Coordinator Anderson reported that the Fall Recycling Day will take place on September 29th. He noted that mattresses and box springs will be accepted for a charge of \$15 each, but advised that no other furniture will be accepted. He noted that the day of the event they will also be taking in used motor oil and antifreeze.

- **Shoreland Ordinance Amendment**

Associate Planner/Environmental Coordinator Anderson advised of an overlap in regulations, which identified the Mississippi River along Ramsey's border as part of both the Wild and Scenic and Critical Area Overlay Districts. He advised that there were legislative changes this year that exempts the Mississippi River from the Wild and Scenic regulations. He explained that the ordinance amendment would bring all the shoreland regulations into one spot within the City Code, as well as make it clear that the Wild and Scenic regulations would apply to the Rum River while the Critical River regulations would apply to the Mississippi River. He referenced a State Supreme Court ruling which stated that the DNR had overstepped their boundaries in regard to certification of variances and other land use applications and so the amendment will also clarify that the DNR only has certification authority for ordinance amendments. He provided further clarification and noted that once staff clarifies additional information within the ordinance to address these changes, he would be happy to draft something to update residents of the changes. He reviewed the timeline for these changes and hoped to bring this item forward at the October meeting of the Planning Commission.

- **Rain Garden Demonstration Project**

Associate Planner/Environmental Coordinator Anderson advised of a possible site for a rain garden demonstration project, noting that possible cost-share funds may be available for the project.

- **Other**

Associate Planner/Environmental Coordinator Anderson stated that with the reorganization of staff, about 90 percent of his time has been focused on planning activities rather than environmental duties. He apologized for the small agenda and asked if any Board Members would like to review the Work Plan to focus priorities, or even review items in Subcommittee format. Due to a lack of available staff time, he asked that the Board assist in prioritizing the Work Plan. He stated that it would be beneficial if an interested Board Member wanted to focus their attention on a Work Plan item and gather background information.

Chairperson Max suggested that the October Board meeting focus on prioritizing the Work Plan and discussing this in more detail.

Associate Planner/Environmental Coordinator Anderson confirmed that the Board would still like to focus on the demonstration projects.

Chairperson Max commented that October would be a good time to make the decision on the tree sale for the spring, and to line up vendors.

Board Member Lewis believed that the Board could do some “housecleaning” and could possibly develop the agendas in the future.

The Board also discussed the possibility of transferring the Environmental Expo from the usual date to take place in conjunction with the Happy Days event. Associate Planner/Environmental Coordinator Anderson explained that this would allow the event to take advantage of the high level of foot traffic from the Happy Days events.

Board Member Valentine stated that when he joined the Board, his expectation was that the members would most likely handle much of the planning and research items. He thought that the Board Members could begin to take a more active role in those actions.

7. ADJOURNMENT

Motion by Board Member Lewis and seconded by Board Member Hiatt to adjourn the meeting.

The meeting adjourned at 8:00 p.m.

Respectfully submitted,

Chris Anderson

Associate Planner/Environmental Coordinator

ATTEST:

JoAnn Shaw
Community Development Secretary

Drafted by Amanda Staple
TimeSaver Off Site Secretarial, Inc.

**ECONOMIC DEVELOPMENT AUTHORITY
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The City of Ramsey Economic Development Authority (EDA) conducted a regular meeting on Thursday, October 11, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Chairperson Chris Riley
 Member John LeTourneau
 Member Colin McGlone
 Member Wayne Skaff
 Member Jim Steffen
 Member Kristine Williams
 Member Jeffrey Wise

Members Absent: None

Also Present: Patrick Brama, Management Analyst
 Tim Gladhill, Development Services Manager
 Mike Mulrooney, ACG

Guests: Brian Burandt, Connexus Energy
 Jim Gromberg, State of Minnesota DEED

1. CALL TO ORDER

Chairperson Riley called the Economic Development Authority meeting to order at 7:35 a.m.

2. APPROVE AGENDA

Motion by Member Skaff, seconded by Member LeTourneau, to approve the agenda.

Motion carried. Voting Yes: Chairperson Riley, Members Skaff, LeTourneau, McGlone, Steffen, Williams, and Wise. Voting No: None. Absent: None.

3. APPROVE MINUTES

3.01: Approve Meeting Minutes Dated September 6, 2012, and September 13, 2012

Motion by Member LeTourneau, seconded by Member Skaff, to approve the minutes dated September 6, 2012, and September 13, 2012.

Motion carried. Voting Yes: Chairperson Riley, Members LeTourneau, Skaff, McGlone, Steffen, Williams, and Wise. Voting No: None. Absent: None.

4. EDA BUSINESS

4.01: Guest Speaker – Don Haller, Connexus Energy

Management Analyst Brama introduced Mr. Brian Burandt from Connexus Energy.

Mr. Burandt presented a diagram showing the Connexus Energy service area and stated he has met with several cities in the territory to discuss available land for a data center and the best place in Anoka County is the former municipal center in terms of land, available fiber, and electrical capabilities. He advised that the planned upgrade of the Crooked Lake to Enterprise Park transmission line will upgrade this line from 69,000 volts to 150,000 volts and will easily serve a data center on this site. He discussed rates, rebates, and reliable electric service and stated that Connexus Energy will provide a discounted rate with a five-year guaranteed discount for the site similar to the discounted rate provided to Vision-Ease. He indicated that rebates are also available including Shovel Ready rebates where Connexus Energy would cover the application expense of \$3,000 payable to DEED in order to have the site certified as Shovel Ready. He reviewed the SAIDI electrical interruption index and stated Connexus Energy customers experience far shorter interruptions on average than other utilities across the country, which is important to a data center. He stated that data centers are one of the most highly requested sites and Connexus Energy is excited about this opportunity to partner with the City.

4.02: 15153 Nowthen Blvd, Old City Hall Site, Consider Future Development

Management Analyst Brama explained this site includes three parcels owned by the City and is worth approximately \$1.5 million, is zoned quasi-public, and is surrounded by residential properties. He indicated that at its last meeting, the EDA directed staff to research options for the site and this research was conducted by Mr. Mike Mulrooney.

Mr. Mulrooney stated that this site is 20.86 acres, which could accommodate a data center of approximately 250,000 square feet either as a single story facility or multiple buildings. He stated that a data center would provide a significant positive impact to the local tax base and most data centers have an average estimated market value of \$120 per square foot or \$30 million in the case of a 250,000 square foot facility. He advised that the commercial/industrial property tax impact from this would be approximately \$1.19 million annually with the City's share being \$268,000; conversely, a residential housing development with 60-80 homes on the site would have an approximate \$15-20 million market value with tax revenue of approximately \$150,000-\$200,000 annually with the City's share being \$35,000-\$50,000. He encouraged the City to also consider the costs associated with serving a residential development versus a commercial/industrial property, e.g., public safety costs.

Management Analyst Brama pointed out that things can change and a 250,000 square foot facility is not guaranteed but represents an estimation of what could happen at full capacity on this site. He also pointed out that a data center is not a high job producer and data center sites are frequently classified as storage units instead of commercial businesses. He stated that funding packages for data centers available in the City include SBA loans, tax abatements, subsidized

power rates, and/or the Minnesota Investment Fund (MIF) and provided examples of similar projects in Chaska and Duluth. He then introduced Mr. Jim Gromberg from the State of Minnesota DEED office.

Mr. Gromberg explained that the Shovel Ready program is a DEED program that certifies a site as "shovel ready" and certifies that the site has clear title, correct zoning, and is ready for sale and construction of a building.

Management Analyst Brama stated that the Fire Station on the site will need to be addressed; in addition, the City will need to locate relevant historical documents regarding the site in order to become a shovel ready site. He advised that the City's future plans include moving the Fire Station to a new location but the timeline for relocating the Fire Station is not yet known. He stated the Fire Chief has indicated that the Fire Station could move to a temporary location if necessary. He stated the shovel ready certification requires historical surveys, documents, and maps so that when a developer steps forward they know the site is shovel ready. He indicated that Hakanson Anderson did a lot of the work on this site in the past and staff is working with them to locate these historical documents but if the documents cannot be located, some work will need to be redone and will take some time to complete. He advised that the cost estimate for the shovel ready certification is \$20,000.

Member McGlone expressed support for pursuing a data center on the site and felt this would fit well with the City's plan to handle its excess land inventory.

Member Wise stated if a proposal for residential development comes in, he does not want to hold the site for a data center. He added it will be important to figure out the impact of a data center to the surrounding neighbors and to consider the costs of demolition and building a new fire station. He asked if a data center would always be a stand-alone building or if it could be attached to other users.

Mr. Gromberg explained that a couple of data centers in St. Paul are co-located noting that this can result in security issues for the data centers themselves. He stated the City may wish to consider leaving the Fire Station on the site for now and build a data center on the triangular piece of property at the southern corner. He described how the MIF fund could be used to help a company develop the site and indicated the State has created certain criteria for data centers to meet that allows data centers to receive exemption from sales tax for the next 20 years and they can also apply for a rebate of sales tax on energy costs.

Member Skaff expressed support for pursuing a data center on the site and asked if there were any zoning issues with the property.

Development Services Manager Gladhill stated the City will likely have to use some employment type zoning and requested that staff be allowed to formulate a zoning plan for the site and work with the neighborhood so there are no surprises.

Motion by Member Skaff, seconded by Member LeTourneau, to recommend that the City Council move forward with the Shovel Ready certification of the old City Hall site.

Motion carried. Voting Yes: Chairperson Riley, Members Skaff, LeTourneau, McGlone, Steffen, Williams, and Wise. Voting No: None. Absent: None.

It was the consensus of the Economic Development Authority to move forward with completion of shovel ready certification, to rezone and apply for Comprehensive Plan amendment through the Met Council, to develop sketch plans/options for development of the site, to conduct an abatement assessment, and to update the City Council on the intentions of the EDA at a work session and ask for support moving forward.

Management Analyst Brama advised that staff will be working with Connexus Energy and other partners, including Great River Energy and Anoka County, and will provide more information regarding the data center prospect to the EDA in the near future.

4.03: Revolving Loan Fund Application (RLF)

Mr. Mulrooney indicated this item was discussed at the last EDA meeting and the owners have now reached a successful conclusion with the landlord for this fitness facility. He stated that total costs are \$177,000 with \$54,000 used for working capital, \$54,000 used for equipment, and \$69,000 used for leasehold improvements. He indicated the landlord has agreed to finance the majority of the leasehold improvement costs and the owners received a financing commitment from Bremer Bank subject to City participation. He stated the owners are requesting a \$34,500 RLF loan at 3.25% fixed for ten years, which is also the term of their lease. He stated that from a collateral standpoint, the loan would be in a junior position to the Bremer SBA loan and the ability of the guarantors to repay the debt was closely reviewed. He indicated that the owners have excellent credit scores and in his professional opinion, the likelihood of repayment on the RLF loan in the event the business is not successful is strong and he regards this as a low risk loan on the part of the EDA. He recommended that the EDA approve the RLF request subject to Bremer Bank approval, clear title to the assets, filing a UCC on the fitness equipment, execution of the loan agreement, and personal guarantees from the owners in the project.

Chairperson Riley questioned the ten-year term of the RLF.

Mr. Mulrooney stated that the ten-year term of the RLF matches the term of the SBA loan from Bremer Bank.

Member Wise stated that fitness equipment is replaced frequently and expressed concern that the EDA would be borrowing them money on equipment that will be sitting in recycling.

Mr. Mulrooney explained that the equipment being used in the business is not typical fitness equipment such as that found at Lifetime Fitness. He stated the owners are putting in substantial equity of \$34,500, which is 20% of project costs, adding that the owners have additional equity that could be put into the project if needed but they want to preserve cash equity for operating purposes recognizing that this is a start-up business.

Member McGlone disagreed that this loan was low risk and stated the nature of the business makes this high risk, the fitness equipment will be worthless which makes it high risk, and the term of the loan makes it high risk. He stated for these reasons he will oppose this loan and felt this was not a wise loan to be making.

Member Steffen asked how difficult it would be to enforce the personal guarantee.

Mr. Mulrooney felt that if the business failed the EDA would not have to enforce the personal guarantee and the loan would be paid off in full.

Member LeTourneau asked if this loan request met all of the RLF criteria.

Mr. Mulrooney replied in the affirmative.

Motion by Member LeTourneau, seconded by Member Skaff, to approve the proposed City of Ramsey Economic Development Authority Revolving Loan Fund Application.

Motion carried. Voting Yes: Chairperson Riley, Members LeTourneau, Skaff, Steffen, and Williams. Voting No: Members McGlone and Wise. Absent: None.

4.04: Prospect Update, Mike Mulrooney

Mr. Mulrooney reported he has been working with a manufacturing prospect on expansion of its current 40,000 square foot facility to a 60,000-80,000 square foot facility. He stated there are two sites available and further information will be provided at the next EDA meeting.

4.05: Anoka County HRA, Economic Development Activities

Chairperson Riley stated the EDA received direction from the City Council on Tuesday that the City will not be asking for EDA participation; as a result, the EDA does not need to discuss this item.

4.06: Business Retention & Expansion Program Introduction, Continued

Chairperson Riley asked if the EDA was in agreement that staff should proceed as outlined in the staff memo.

It was the consensus of the Economic Development Authority to approve the proposed plan for a business retention and expansion program.

4.07: 2013 EDA Budget

Chairperson Riley asked if there was anything in the 2013 EDA budget that is contrary to the EDA work plan. He also asked if the EDA was interested in further discussing the 2013 EDA budget as presented.

Management Analyst Brama felt staff was in a position to move forward with the goals listed.

It was the consensus of the Economic Development Authority that no further discussion regarding the 2013 EDA budget was warranted at this time.

4.08: Ramsey EDA Business Networking Breakfast Event Update

Management Analyst Brama requested that Chairperson Riley present introductory remarks and emcee the event. He also requested that Member LeTourneau assist with the group discussions.

4.09: Demolition of Buildings Located on 8020 147 Avenue NW, Update

Chairperson Riley stated this item would not be discussed at this time.

4.10: City Owned RALF Properties, Leasable Space Update

This item was not discussed due to lack of time.

5. Member/Staff Input

Member Steffen requested an update regarding the job posting for the economic development position and asked if the position will be filled in January.

Management Analyst Brama advised that the position will be posted at the start of the year and the position will not likely be filled until March. He added it will depend on how the position shakes out and will depend on decisions made at the City Council and HRA levels.

6. Adjournment

Motion by Member LeTourneau, seconded by Member Steffen, to adjourn the meeting.

Motion carried. Voting Yes: Chairperson Riley, Members LeTourneau, Steffen, Williams, LeTourneau, McGlone, and Skaff. Voting No: None. Absent: Member Wise.

The regular meeting of the Economic Development Authority adjourned at 9:04 a.m.

Respectfully submitted,

Kurtis G. Ulrich, City Administrator

ATTEST:

Patrick Brama, Management Analyst

Draft by Barbara Hughes, *TimeSaver Off Site Secretarial, Inc.*

CC Regular Session

4.3.

Meeting Date: 11/27/2012

Submitted For: Jo Thieling

By: Jo Thieling, Administrative Services

Information

Title:

Approve License Applications

Background:

Attached is a list of license requests for Council approval.

Council Action:

Motion to approve the license applications.

Attachments

Licenses for Approval

Form Review

Inbox	Reviewed By	Date
Kurt Ulrich	Jo Thieling	11/20/2012 11:40 AM
Jo Thieling	Jo Thieling	11/20/2012 11:50 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:37 AM

Form Started By: Jo Thieling Started On: 11/20/2012 11:26 AM

Final Approval Date: 11/21/2012

License Applications
11/27/2012

*Transient Merchant	Abundant Life Ministries	Box 1061	Anoka, MN 55303
**Temporary Intoxicating	St. Katharine Drexel	7101 – 143 rd Avenue NW G	Ramsey, MN 55303

*Abundant Life Ministries has made a request for a Transient Merchant License to sell Christmas Trees and Wreaths as a fundraiser for the youth group. The trees will be sold at Do-All Printing located at 6360 Highway #10 NW in Ramsey. Staff is requesting Council waive the fee as has been done in the past as it relates to fundraisers.

**No fee – the fee was removed for the temporary intoxicating licenses as only non-profits may apply.

CC Regular Session

4. 4.

Meeting Date: 11/27/2012

Submitted For: Jo Thieling

By: Jo Thieling, Administrative Services

Information

Title:

Approve the Following Meeting Minutes:

1. Special City Council - October 16, 2012
2. City Council Work Session - October 23, 2012
3. City Council Regular - October 23, 2012
4. Canvassing Board - November 13, 2012
5. City Council Work Session - November 13, 2012
6. City Council Regular - November 13, 2012

Background:

Meeting minutes are attached for Council review and approval.

Council Action:

Motion to Approve the Following Meeting Minutes:

1. Special City Council - October 16, 2012
 2. City Council Work Session - October 23, 2012
 3. City Council Regular - October 23, 2012
 4. Canvassing Board - November 13, 2012
 5. City Council Work Session - November 13, 2012
 6. City Council Regular - November 13, 2012
-

Attachments

Spec CC 101612

CCWS 102312

CC Regular 102312

Canvassing Bd 111312

CCWS 111312

CC Regular 111312

Form Review

Inbox

Kurt Ulrich

Reviewed By

Kurt Ulrich

Date

11/21/2012 11:34 AM

Form Started By: Jo Thieling

Started On: 11/20/2012 05:03 PM

Final Approval Date: 11/21/2012

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**CITY COUNCIL
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a Special Session meeting on Tuesday, October 16, 2012 at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Members Absent: None

Also Present: City Administrator Kurtis Ulrich
Finance Director Diana Lund
Public Works Superintendent Grant Riemer
Development Services Manager Timothy Gladhill
Assistant Planner/Environmental Coordinator Chris Anderson
City Attorney William Goodrich
Development Manager Darren Lazan
Interim Engineer Shane Nelson

1. CALL TO ORDER

Mayor Ramsey called the regular meeting of the Ramsey City Council to order at 7:00 p.m., followed by the Pledge of Allegiance led by Mayor Ramsey.

2. CITIZEN INPUT

None.

3. APPROVE AGENDA

Motion by Mayor Ramsey, seconded by Councilmember Strommen, to approve the agenda as revised to remove Case 4.02.

Further discussion: In response to the Council's questions relating to removal of Case 4.02, City Administrator Ulrich explained it is an HRA as well as City Council item and staff's request to

consider it during the HRA meeting had to do with availability of the consultants to address both items.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Strommen, Backous, McGlone, and Tossey. Voting No: Councilmember Elvig. Abstain: Councilmember Wise.

4. COUNCIL BUSINESS

4.01: Request for Site Plan and Conditional Use Permit Approval for Motor Fuel Sales and Car Wash for a SuperAmerica Located in the COR TWO Subdivision; Case of West Real Estate and Management, Inc.

Assistant Planner/Environmental Coordinator Anderson reviewed the staff report and recommendation of the Planning Commission for approval.

Motion by Councilmember Tossey, seconded by Mayor Ramsey, to adopt Resolution #12-10-154A adopting Findings of Fact #0907 favorable to the applicant and to adopt Resolution #12-10-154B approving the request for a Conditional Use Permit for motor fuel sales and an accessory car wash, contingent upon the completion of certain updates to the Design Framework and compliance with the Staff Review File dated September 28, 2012, revised October 12, 2012, and to approve the proposed site plan, contingent upon the completion of certain updates to the Design Framework and compliance with the Staff Review File dated September 28, 2012, revised October 12, 2012.

Further Discussion: The Council noted it had raised questions relating to the color of the canopy fascia, suggesting it be more color neutral, and the use of a standing seam roof. Assistant Planner/Environmental Coordinator Anderson clarified Amcon has indicated they would use a metal coated stone shingle instead of a standing seam roof due to snow and ice concerns. With regard to the color of the canopy fascia, Development Services Manager Gladhill indicated it could be included as a condition of approval. Development Manager Lazan explained there is no Code requirement relating to the color of the canopy but he can address that issue at his meeting with the developer to finalize this application. Gary Gabrielson, Pine Creek Associates representing KONA Properties, real estate broker for Amcon and SuperAmerica, stated this answer is out of his preview; however, SuperAmerica has done a great deal of research on signage and how it is presented in the marketplace so he assumes they would not want to change signage image or the canopy.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Tossey, Backous, Elvig, McGlone, and Strommen. Voting No: None. Abstain: Councilmember Wise.

4.02: Comparison of Final Pro-Forma and Current Draw Schedule in Regard to The Residence at The COR

This item was removed from the agenda upon adoption.

4.03: Approve Additional Services – Sunwood Drive Construction Administration

Development Manager Lazan reviewed the staff report.

Interim Engineer Nelson stated his recommendation that Landform continue to be involved through completion of this project.

Motion by Mayor Ramsey, seconded by Councilmember McGlone, to approve the modification to Landform's contract number RAM 12019 revising the maximum fee from \$15,000 to \$30,000 to cover projected efforts, as needed, through completion of the project.

Further Discussion: The Council discussed the work conducted by Landform under this contract to participate in weekly meetings, provide clarification on engineering drawings, and help with problem solving. Interim Engineer Nelson stated he does not anticipate a change order from WSB. The Council expressed concern that the modification request, if approved, would double the initial contract amount. Mayor Ramsey clarified the full \$15,000 may not be needed and his motion was that the increase be used as needed. Development Manager Lazan explained the City had asked Landform to react based on demands of the contractor and to anticipate \$15,000. It was not a "not-to-exceed" contract. He noted Landform does not control the scope and with this complicated project there have been far more answers and field solutions than anticipated by the former City Engineer. The Council determined it did not have enough information to make this decision and asked staff to provide a spreadsheet showing a running total of expenses, change orders to date, anticipated change orders, hours spent to date, and overall project scope as a whole. City Administrator Ulrich stated staff will bring forward that detail to the next meeting and asked if the Council would entertain approving an incremental increase of \$5,000 so Landform will remain engaged. Councilmember Strommen stated it is frustrating to be asked to take action at the "eleventh hour" without having the needed information and questioned whose responsibility it is to gauge when a contract is nearing its end and needs to be presented to the Council for extension. City Administrator Ulrich explained it is not unusual for this type of work, with an estimated amount, to reach that maximum and to have to request an additional amount. He stated Landform's contract is slightly over the maximum estimated and he should have notified the City it was nearing the maximum, so Landform would have to "eat" that amount for not bringing forward the extension request prior to exceeding that amount.

Mayor Ramsey withdrew his motion and Councilmember McGlone withdrew his second to the motion.

Motion by Mayor Ramsey, seconded by Councilmember Tossey, to postpone consideration of modification to Landform's contract number RAM 12019 to October 23, 2012.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Tossey, Backous, Elvig, McGlone, and Strommen. Voting No: None. Abstain: Councilmember Wise.

4.04: Review Utility Building / Historic Town Hall Relocation Project

City Administrator Ulrich reviewed the staff report and recommended location for the utility building on the south side without a vault.

Chris Rasmussen, Kimley-Horn, provided an update on how this building could be relocated and the determination that the building would have to be 2-4 feet lower than anticipated, requiring additional water proofing. Ms. Rasmussen described the location of existing utilities and the need to shore the north and south side of the building structure. Due to the challenges of this relocation and construction, the building is proposed to be located on the south side of the site. She displayed an artist rendition of the boiler plant with an arched roof to match the train station.

City Administrator Ulrich stated the land was purchased through RALF so an easement will be needed from the Metropolitan Council. He noted this project is well within the budget and this proposal is lower in cost than the original proposal.

Motion by Councilmember Elvig, seconded by Mayor Ramsey, to authorize staff to move the building to the south side of the tracks.

Further Discussion: The Council discussed the proposed location and asked questions of staff. Ms. Rasmussen stated she will confirm that the proposed building location and area of refuge will not conflict with the future realignment of Highway 10. She reviewed the location of utilities and stated a contractor had estimated the cost to locate the building on the north side would be greater than \$500,000, a significant increase due to timing of the year, location of utilities supply and return, potential need for directional boring, and a pump for the sanitary sewer and storm sewer if going below grade building. City Administrator Ulrich noted there is a detailed landscaping plan to assure appropriate screening and on the north side the City is not precluded from pursuing a building addition for a library kiosk, restrooms, trailhead, coffee shop, or other potential uses.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Backous, McGlone, Strommen, Tossey, and Wise. Voting No: None.

5. MAYOR / COUNCIL / STAFF INPUT

Councilmember McGlone reported on his conversation with Allen Young of Mid-America (property manager for Coborn's) who indicated they are thrilled with the project.

Councilmember Wise reported that several ATVs were ticketed for speeding and it was then determined the ATVs were stolen.

City Administrator Ulrich announced the train station ribbon cutting ceremony on November 8, 2012, and regular train service starting on November 14, 2012.

Development Services Manager Gladhill reported there is an opening on the Metropolitan Council Transportation Advisory Board (TAB) with applications closing on November 9, 2012.

6. ADJOURNMENT

Motion by Commissioner Wise, seconded by Commissioner Elvig, to adjourn the Special City Council Meeting.

Motion carried.

The Special City Council Meeting was adjourned at 7:57 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, October 23, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Finance Director Diana Lund
Human Resources Manager Colleen Lasher
Development Services Manager Timothy Gladhill
Management Analyst Patrick Brama
City Attorney William Goodrich
Development Manager Darren Lazan
Interim Engineer Shane Nelson

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 5:35 p.m.

2. TOPICS FOR DISCUSSION

2.01: Sunwood Drive Project – Review Funding Package, Expenditures to Date and Construction Challenges

Interim Engineer Nelson reviewed the staff report and described the funding scenarios for the Sunwood Drive and Armstrong Boulevard project. He stated he had reviewed the Council's meeting minutes and did not find any indication that the actual budget for this project had been approved but found The COR grading at Market Place was a separate line item. Interim Engineer Nelson reviewed the Council's past discussions related to this project's funding, potential shortfall of \$200,000, and funding via future land sales.

Councilmember Wise noted this project started initially as an HRA project with funding coming from the City.

Councilmember Strommen stated the budget was a Council consideration because it was discussed that the HRA would not be making a contribution.

City Administrator Ulrich stated that was also his recollection and that the Council had decided on the budget.

Mayor Ramsey noted the Council should have taken action to officially approve the budget for this project.

Councilmember Elvig stated he recalled a \$5.35 million budget number but that number had varied and there had been discussion whether the number would still be moving. He stated he thought the Council had approved a budget.

Mayor Ramsey stated the Council approved a project dollar amount, but perhaps not the actual budget.

Councilmember Wise noted the numbers were changing because the project went from a partial taking to full property acquisition.

City Attorney Ulrich advised that approval or non-approval of the project budget is not a legal issue.

Interim Engineer Nelson agreed and stated he had been asked whether the project was over or under budget so he had researched the project file to determine the budget that had been approved. He noted the budget was discussed several times, but a final contribution with funding sources had not been formally approved by Council action.

Councilmember McGlone stated if the numbers and potential funding sources were discussed, it is his opinion that the budget was approved if the project moved forward, so that is a moot issue.

City Administrator Ulrich stated the Council determined to eliminate land proceeds and decided to use grant funding along with MSA to fill the gap.

Finance Director Lund indicated the Equipment Revolving Fund was to also fill the gap.

Interim Engineer Nelson displayed a slide showing the bid with two schedules (Schedule A and B). He reviewed past City Council meeting minutes, noting there had been discussion; however, the motion approved was to enter a construction contract. He presented the final spreadsheet, noting land acquisition is not final but he thinks it will be within budget at \$2.7 million. The construction, after bids were received, with a \$2.35 million budget, would be about \$100,000 over the construction portion of the budget presented on June 26, 2012. On the expenditure side, the original budget estimate was at \$470,000 and is now at \$497,000 (including the requested \$15,000 of additional services).

Councilmember McGlone asked Interim Engineer Nelson whether his calculations factor in a project contingency when saying the project is over budget.

Interim Engineer Nelson explained it depends on how you look at it but if looking at the \$5.642 million revenue sources presented at the June 26, 2012, meeting, then there is a \$135,000 shortage. However, the \$210,000 was originally included as a separate line item. At one time, \$1.2 million was budgeted from the revolving equipment fund resulting in a funding overage of \$74,000.

City Administrator Ulrich asked whether there was a budgeted contingency of 10%.

Interim Engineer Nelson stated there was not a contingency budget line item. However, the initial budget included higher costs that would have allowed for a contingency but if using a \$5.642 million budget then there is a shortage.

Councilmember Elvig asked to see the slide of the meeting minutes, noting it indicates the former City Engineer had reviewed a \$5.35 million funding package in March.

Interim Engineer Nelson stated this reflects discussion at a meeting and he had also viewed this meeting video to be sure the budget was not separately approved. He indicated the Council's motion was to enter into a construction contract, not to approve the budget. The \$5.35 million budget was from an early meeting in March and he had found 8-9 versions of the project budget so it is hard to determine the actual approved budget.

Councilmember Strommen stated she still has many of the paper budget versions but when the construction project was approved and moved forward, that was the basis on which she made her decision.

Interim Engineer Nelson reviewed the revenue sources presented of \$5.642 million at the meeting of the bid opening. He explained the \$5.642 million budget had not been shown on the screen; however, the former City Engineer had indicated it was passed out to the Council before the meeting. The bids were opened after the case reports were written and that distributed budget was at \$5.642 million.

Mayor Ramsey stated if using the \$5.642 million budget, then the project is at a positive \$74,000.

Councilmember Strommen stated that is not correct because the project is at \$5,777,990 and the \$210,000 was wiped out when the bids came in.

Finance Director Lund explained the original documents pre-bid included an additional \$210,000 and then it was included in the bid document. The original equipment revolving fund had an additional \$210,000. The prebid amount included an additional \$210,000 for the Northstar Marketplace grading, but was included in the construction bid with North Pine. If using the project construction bid, the overall project was estimated at \$5,642,000, leaving the current project budget short by \$135,990. If the Council chose to use the amount that had originally been incorporated to come from the equipment revolving fund (prior to actual bids received and when it was believed that the Northstar Market Place grading would be extra) the equipment

revolving fund would contribute a total of \$1,206,697 and leave a fund balance estimate at year end 2012 of \$1,762,050. Using the extra \$210,000 from the equipment fund would make the project budget positive by \$74,000.

City Administrator Ulrich asked what is the industry standard for a project contingency.

Interim Engineer answered it is usually 5-10% but he has seen it as high as 15%.

City Administrator Ulrich noted if there would have been a budget, it would have included a contingency.

Interim Engineer Nelson stated that is typical and most always something is encountered during the project that you didn't anticipate.

Councilmember Elvig stated as he recalls, the budget was \$5.35 million and it looks like this project is a "run away freight train" with \$400,000 in overages and more add-ons are being asked for tonight.

Mayor Ramsey asked why the project is over budget.

Councilmember Wise stated it sounds like the engineer presented \$5.642 million, not \$5.35 million, so the project is within 10%.

Councilmember McGlone stated everybody approved what the former City Engineer handed out so the project is not over by \$400,000 and is still within the contingency and standard protocol.

Interim Engineer Nelson displayed a map exhibit depicting areas where changes occurred during construction and described each modification.

Mayor Ramsey asked whether the cost of Change Order #4 is included in these figures.

Interim Engineer Nelson stated it is within the calculations he presented but not yet approved by the Council. Change Order #4 is on the Council's agenda for tonight.

Councilmember Wise asked whether the issue with the drain field at 8019 146th Avenue was a survey error and should be covered by the contractor, not the City.

Interim Engineer Nelson stated he is not sure any of the fault is with the contractor but questioned whether the design engineer should have been aware of the drain field in that area. He explained if the contractor is culpable, that would have to be pursued elsewhere.

Councilmember McGlone asked if the City Engineer approved the ultimate plans and how long it took Interim Engineer Nelson to discover the issue and figure it out.

Interim Engineer Nelson stated they found the location of the drain field in about 10 minutes.

Councilmember McGlone asked if City engineering staff missed the drain field so now the Council has to consider a change order.

Interim Engineer Nelson stated there are other drainage issues on the Billmark Electric property. He presented an overview of issues relating to the Sunwood Drive project: a trail conflicts near Round Lake Boulevard; installation of the signal cabinets, trail bid at 8-foot width when Code requires 10-foot width; utility conflict with the signal base; Connexus transformer; discrepancies between COR TWO retail plans of Landform versus WSB plans; irrigation box is damaged; storm sewer was stubbed to the north side of Sunwood and located within a building site; existing utilities conflicting with road realignment and need to be relocated; ownership of the street lights; crosswalk location; storm sewer pipe was stubbed for future development that was not needed; signage for COR TWO retail area conflicts; Connexus transformer locations; proposed route for Connexus to install cables to serve properties but the COR TWO plat has not yet been recorded; and issues with easements for utility companies.

Mayor Ramsey asked who paid to take down and reinstall a fence.

Interim Engineer Nelson indicated there have been no additional charges itemized for that work.

2.02: Sunwood Drive Project – Consider Change Order #4

Interim Engineer Nelson reviewed the staff report and presented Change Order #4.

Mayor Ramsey asked whether WSB should have known about the drain field location.

Interim Engineer Nelson stated he does not think so unless WSB had knowledge of the City's records for review. He explained there were no pipes showing the location of a drain field. However, the City did take easements and between the City and WSB, the drain field could have been caught in the design but it was not.

Mayor Ramsey asked if the City is expected to "eat" this cost because someone neglected to check this.

Interim Engineer Nelson answered in the affirmative.

Councilmember Wise stated he does not want the City to pay for it if it is not the City's fault. He noted this project has budget overruns and this is the County's part of the project so it should be on WSB, not at the City's expense.

Development Services Manager Gladhill indicated WSB requested records on what parcels had septic systems and he would have to research whether there are missing records where it was not filed with the City.

City Administrator Ulrich asked whether the City has the ability to retain fees from WSB at a certain point.

Interim Engineer Nelson stated the City can do so with the construction contract. He asked the Council whether it wanted to proceed with the change in the construction contract, noting that is the discussion. Interim Engineer Nelson stated he had asked WSB to comment on the two drainage issues and provided an e-mail response on Monday. He indicated he could follow up whether the City had provided that information.

City Attorney Goodrich stated he can review contract specifications to determine whether WSB has obligation to determine easements.

City Administrator Ulrich explained that if the drain field had been found, the City would have done that work anyway so “damages” are considered to be the extra charges for a Change Order. That additional expense is the issue.

Interim Engineer Nelson stated the original project design would have been different if the drain field location had been known. He displayed pictures of the project area depicting the excavation on the Wiser Choice Liquor property and described the change needed to avoid the drain field. He also displayed pictures of large utility cabinets located on a private easement, resulting in the need for the project to go behind the cabinets. Interim Engineer Nelson explained that depending on the purchase terms, the project could proceed with a ditch to wipe out the drainfield and take it out of service.

Councilmember Wise stated his understanding from the owner is that he would be interested in leasing back the property until the building needed to be removed so taking the septic is not an option. He noted the change order cost is not significant enough to offset the rent.

Interim Engineer Nelson estimated about \$8,000 for that piece. He displayed a slide depicting the Billmark Electric parking lot, noting there is no room for a ditch so it is proposed to install a draintile around the parking lot that discharges into a ditch north of the Billmark Electric property.

Councilmember Wise stated the recent rain left Arnie Billmark’s parking lot under water. He asked if the drain tile was enough to address the issue in the spring.

Interim Engineer Nelson agreed that draintiles are not the most ideal drainage solution and more subject to freezing than a storm sewer pipe. Ideally, an open cut ditch would have worked better; however, there is not sufficient room to install a ditch. He explained an overflow could be installed. Interim Engineer Nelson displayed pictures of the irrigation box, noting areas of damage, and indicated the plans call for the contractor to pick up the box and move it to a new location. This would increase the project cost so if not approved, the existing box would be relocated to the new site. Interim Engineer Nelson reviewed the construction signage issue that was an oversight in the construction plans. The plans allowed for road closed advance warning signs before the contractor started work but contained no provision to change out those signs after construction. Additional signage had to be ordered with a changed message that eliminated the “follow detour” wording from the bottom plate. Interim Engineer Nelson stated those items are included in Change Order #4 that is on the Council’s agenda. He stated he just found out about another item relating to relocation of utilities into a new easement along Sunwood.

Connexus is planning its new relocation, and the telephone cable will be relocated; however, Zayo was unresponsive and has now informed him that it has a contract with the City requiring the City to pay to relocate their fiber optic lines. Interim Engineer Nelson stated he reviewed the final contract and found it is not specific to that relocation but does discuss that for non-routine maintenance, the City is responsible for paying two-thirds of their cost to relocate the fiber optic line. Interim Engineer Nelson stated he does not have an estimate on how much it will cost but it needs to be resolved because the existing alignment conflicts with the approved development plans.

Councilmember Elvig asked if it will be considered routine maintenance.

Interim Engineer Nelson indicated it would not.

Finance Director Lund stated the City entered into that agreement in April 2012 and asked whether Sunwood Drive had been realigned at that time.

Development Manager Lazan stated we made some changes to the actual right-of-way width and radius points were moved marginally but the realignment to Sunwood Drive with a roundabout to the north was known at that time. Development Manager Lazan stated he had substantial discussion with the former City Engineer at the time Zayo was urgently trying to get this piece in. They knew the road would be realigned and he had told the former City Engineer it would be in conflict with any future development. Development Manager Lazan stated the former City Engineer considered providing a new realignment and his solution was to allow Zayo to install with the understanding that when Sunwood Drive was realigned, Zayo would have to move the cable at that future date. Development Manager Lazan indicated this is not documented in the right-of-way permits although other things were also not documented and poorly described in alignment documents. He stated this issue was discussed, he had advised the former City Engineer that it would be a conflict, and it had been his understanding that the former City Engineer coordinated with Zayo and Zayo knew the cable would have to move once Sunwood Road was realigned.

Mayor Ramsey asked if there was no documentation of that conversation.

Interim Engineer Nelson stated he reviewed the project file and found no discussion about future relocation of this cable.

City Attorney Goodrich recommended contacting the former City Engineer to gain his recollection as well as e-mail correspondence.

Interim Engineer Nelson stated Change Order #4 includes the 146th storm sewer item, daintile swale related to 14700 Armstrong Boulevard (Billmark Electricity), irrigation service related to the new irrigation system on Sunwood Drive, additional construction signage, irrigation box relocation, and additional pipe removal. Interim Engineer Nelson explained that the City has the choice whether or not to approve the irrigation box relocation. He stated he had discussed with the City Attorney regarding municipal bid laws. For smaller projects, the City can enter a contract by obtaining quotes, unless not practical. In this case it is not practical to obtain quotes

because the City already has traffic control, right-of-way permits, and NPDES Permits that are under the prime contractor's name. In this case, he recommended the Council approve a change order instead of going out for quotes.

Councilmember Wise asked about the project cost savings since there will not be a cul-de-sac north of his store.

Interim Engineer Nelson stated there will be a project cost savings for not constructing the cul-de-sac of about \$5,000 to \$6,000.

2.03: Consider Change Order #5 – Sunwood Drive

Development Manager Lazan reviewed the staff report and previous HRA approval of soft costs associated with this effort and direction to staff and the Development Team to outline and present hard costs at a future meeting. The work included in Change Order #5 is public watermain, public sanitary sewer, and common stormwater mains (exclusive of infiltration facilities). This work is covered under the current NPDES permit so it is geographically part of the same project area. If approved, construction of these deep utilities will commence almost immediately and be available for connection by Lots 4 and 5 per the HRA agreement with those buyers. Development Manager Lazan noted the bid law precludes the City from adding to their contract or creating a new project in excess of \$100,000 without going out for bids. He had asked the contractor whether utilities could be included under this project and a quote was received of \$99,999.70. These costs are in line with the estimates prepared and provided to the buyer of Lots 4 and 5, who will be responsible for their pro rata share. The funding would be accomplished by having the HRA pay and add it to the project cost within COR TWO, as originally contemplated. Or, there could be an agreement reached between the City and HRA to barter as previously done with the soil provided from HRA property to the project resulting in significant cost savings (\$11/yard versus \$2/yard). Development Manager Lazan stated a second option is to consider this utility work in exchange for the soil provided for Armstrong Boulevard. He explained that since receiving the quote, he talked to North Pine Aggregate and developed an alternate proposal in case the Council feels this bid is too close to \$100,000 maximum. Development Manager Lazan stated in the alternate proposal, he had removed the storm sewer work, which resulted in a deduct of \$34,225 and bid estimate of \$65,774. If approved, it would enable deep utility work to be completed quickly, allows Lot 5 to start construction without having to tear up the site, and stay on schedule for construction this fall.

Councilmember Wise asked if Scenario #2 would cost another \$55,000.

Development Manager Lazan explained it would provide sanitary sewer, watermain, and storm sewer piping as shown on the exhibit being displayed. He had arrived at the lower price of \$65,774 by removing storm sewer from the project, which could be installed by the Lot 5 user and credited to closing costs.

City Attorney Goodrich asked Development Manager Lazan whether he will get another quote.

Development Manager Lazan stated he held discussion with City Attorney Goodrich about needing two quotes, but believed there was some “gray area” in the League’s interpretation about whether this was an add-on project or a new project. He explained the difficulty with bringing another contractor into the first contractor’s bid area under which the permits were issued. Development Manager Lazan pointed out this is a public improvement project and continuation of the two stubs so he found it was a continuation, not a new project. He stated he can’t imagine that a second quote to mobilize would be less expensive than this quote.

City Attorney Goodrich advised that Bond Counsel Tom Bray had stated it is not a “gray area” and if the project cost \$100,000, it is mandatory to obtain two quotes. He explained that change orders with special assessment projects are in a different category and can be up to 25%. This is a different contract so two quotes are required.

Councilmember Strommen agreed this is a separate project, it was originated separately, and she supports getting a second quote.

Councilmember McGlone agreed with the need for a second quote, noting it will probably be higher but is required by the law.

Councilmember Wise questioned whether a contractor will be willing to put the work into preparing a quote knowing it is probably a waste of time.

City Attorney Goodrich advised if the City cannot obtain a second quote, it will have to bid the project.

Development Manager Lazan noted it is possible to approve a change order subject to the second quote being at or lower than this amount.

City Attorney Goodrich stated he can craft motion language to address contingent approval, if desired.

Councilmember Strommen expressed concern with that option and stated it is a question of how the City does business and it should not participate in “gray areas” around the law.

Councilmember Backous noted the price of this quote has already been stated into the record.

Mayor Ramsey stated he values the legal opinion and agrees with the need for a second quote.

Councilmember Elvig suggested repackaging the project so it can be competitively bid since the amount of the current quote is known.

Development Manager Lazan stated it can be bid for spring work or these items can be included with Lot 5 to do the work.

Councilmember McGlone stated he does not advocate rebidding the project but to follow the City Attorney’s recommendation and seek a second quote in the market.

City Administrator Ulrich stated if the Council wants to keep the project on track as scheduled, staff can obtain a quote within the next week and a special meeting can be called for next Tuesday.

Councilmember Backous asked City Attorney Goodrich when he had advised Development Manager Lazan of the requirement for a second quote.

City Attorney Goodrich stated he got an e-mail last week.

Councilmember Backous noted this issue is already a week down the road and the Development Manager is overruling the opinion of the legal counsel.

Development Manager Lazan clarified he did not overrule the opinion of legal counsel and had reached out to two other contractors to get quotes but they were not interested. He noted there are practical problems with putting another engineer within this contractor's project.

Councilmember Strommen stated she does not think this is a policy decision the Council can make tonight and she is not supportive of scheduling another special meeting if staff could have obtained the required second quote within the last week. She stated this situation is frustrating because if staff had obtained that second quote this past week, the Council could have taken action tonight.

Councilmember Wise asked if SuperAmerica can take on this work as part of its project. He noted if SuperAmerica can take the lead on this project, it would be a simple solution.

Development Manager Lazan advised that has always been an option but his suggestion presented itself as a way to install the deepest utilities quicker and still meet the schedule.

Councilmember Wise noted that is the option that had been discussed when Wiser Choice Liquor was going to lead the project.

Councilmember Tossey asked whether there was a legal problem with SuperAmerica doing the work.

City Attorney Goodrich stated there is not and that option has been approved with other developers. The developer would take on the project, guarantee it, and the City would accept the project at a later date. However, that is an HRA decision.

Development Manager Lazan agreed that is an option and his suggestion is for a parallel option to install the deepest utilities but if the Council is not comfortable with it, then it does not need to move forward.

Councilmember Strommen clarified it is not an issue of what the Council is comfortable with, legal counsel had advised it is not legal.

Councilmember McGlone noted if the City gets a second quote, it is legal.

Councilmember Strommen pointed out that tonight staff has presented only one quote and that is not legal.

Mayor Ramsey stated this case will be removed from the Council agenda.

3. FUTURE TOPICS FOR DISCUSSION

3.01: Review Future Work Session Topics / Calendar

Councilmember Strommen requested discussion of the Anoka dam prior to the December LRRWMO meeting. She indicated she will be leaving the LRRWMO after the November meeting.

4. MAYOR / COUNCIL / STAFF INPUT

None

5. ADJOURNMENT

Mayor Ramsey adjourned the Work Session of the City Council at 6:50 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

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**CITY COUNCIL
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a regular meeting on Tuesday, October 23, 2012 at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise

Members Absent: None.

Also Present: City Administrator Kurtis Ulrich
Fire Chief Dean Kapler
Human Resources Manager Colleen Lasher
Development Services Manager Timothy Gladhill
Assistant Planner/Environmental Coordinator Chris Anderson
Management Analyst Patrick Brama
City Attorney William Goodrich
Development Manager Darren Lazan
Interim Engineer Ron Wagner
Interim Engineer Shane Nelson

1. CALL TO ORDER

Mayor Ramsey called the regular meeting of the Ramsey City Council to order at 7:00 p.m., followed by the Pledge of Allegiance led by Mayor Ramsey.

2. PRESENTATION

2.01: Resolution Proclaiming Homelessness Awareness Month

Mayor Ramsey read in full a Resolution proclaiming November as Homelessness Awareness Month in the City of Ramsey.

3. CITIZEN INPUT

Patrick Surma, 164th Avenue NW, expressed his high respect for the Republic, Flag, and encouraged all to participate in the upcoming election.

4. CONSENT AGENDA

Motion by Councilmember Elvig, seconded by Councilmember Backous, to approve the following items on the Consent Agenda:

- 4.01: Receive Cash and Investments for Period Ending September 30, 2012
- 4.02: Note the following Boards and Commission Meeting Minutes:
 - 1) Planning Commission Meeting Minutes Dated September 6, 2012
 - 2) Economic Development Authority Special Meeting Minutes Dated September 2, 2012
 - 3) Economic Development Authority Meeting Minutes Dated September 13, 2012
- 4.03: Approve Request to Declare Surplus Property
- 4.04: Approve the Following Meeting Minutes:
 - 1) City Council Work Session – October 2, 2012
 - 2) City Council Work Session – October 9, 2012
 - 3) City Council Regular Session – October 9, 2012
- 4.05: Adopt Resolution #12-10-155 authorizing making application for Shared Fire Service Grant Opportunity
- 4.06: Schedule Canvassing Board for Tuesday, November 13, 2012
- 4.07: Adopt Resolution #12-10-155A Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of October 5, 2012, through October 16, 2012
- 4.08: Adopt Resolution #12-10-156 Authorizing Partial Payment to Knutson Construction for IP 10-22 Ramsey Municipal Parking Ramp, Phase II
- 4.09: Adopt Resolution #12-10-157 Authorizing Partial Payment to Julian M. Johnson Construction for IP 12-24; 2012 Storm Sewer Improvements Division B
- 4.10: Adopt Resolution #12-10-158 Authorizing Partial Payment to North Valley, Inc. for IP 12-23; Alpine Drive Reconstruction
- 4.11: Adopt Resolution #12-10-159 Authorizing Partial Payment to North Pine Aggregate, Inc. for IP 12-20; CSAH 83 (Armstrong Boulevard) – Sunwood Drive
- 4.12: Adopt Resolution #12-10-160 Adopting Findings of Fact #0906 Relating to a Request from Lake Region RV to Permit Motor Vehicle Sales in the B-2 Highway Business District at the Properties Located at 7815 Riverdale Drive NW; and Adopt Resolution #12-10-161 Approving the Issuance of the CUP and Declaring Terms of Same.
- 4.13: Adopt Resolution #12-10-162 Proclaiming November Homelessness Awareness Month
- 4.14: Adopt Resolution #12-10-163 Appointing Election Judges for the General Election on November 6, 2012
- 4.15: Report from the Personnel Committee Meeting dated October 9, 2012:
 - 1) Consider a Resolution Ending the Probation Period of the Management Analyst – *Ratify the recommendation of the Personnel Committee to Adopt Resolution #12-10-164 Ending the Probationary Period for the Management Analyst.*

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Backous, McGlone, Strommen, Tossey, and Wise. Voting No: None.

5. APPROVE AGENDA

Motion by Councilmember Backous, seconded by Mayor Ramsey, to approve the agenda as revised to remove Case 7:05.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Backous, Elvig, McGlone, Strommen, Tossey, and Wise. Voting No: None.

6. PUBLIC HEARING

6.01: Public Hearing (continued) to Consider Extension or Revocation of a Conditional Use Permit for Motor Vehicle Sales at 8175 Riverdale Drive NW; Case of Quality RV

Mayor Ramsey closed the regular portion of the City Council meeting at 7:05 p.m. in order to conduct a public hearing.

Public Hearing

Mayor Ramsey called the public hearing to order at 7:05 p.m.

Presentation

Assistant Planner/Environmental Coordinator Anderson reviewed the staff report. He reported the applicant had submitted revised grading plans that addressed staff's concerns and the Lower Rum River Water Management Organization approved the permit, so staff recommends the Conditional Use Permit be continued contingent upon required improvements being completed prior to December 31, 2012.

Citizen Input

There was none.

Motion by Councilmember Elvig, seconded by Councilmember Wise, to close the public hearing.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Wise, Backous, McGlone, Strommen, and Tossey. Voting No: None.

The public hearing was closed at 7:07 p.m.

Council Business

Mayor Ramsey called the regular City Council meeting back to order at 7:07 p.m.

Motion by Councilmember McGlone, seconded by Councilmember Wise, to Adopt Resolution #12-10-165 adopting Findings of Fact #0908 to support continuation of the Conditional Use Permit and confirm the continuation of the Conditional Use Permit, contingent upon receipt of their WMO Permit and installation of required improvements by December 31, 2012.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers McGlone, Wise, Backous, Elvig, Strommen, and Tossey. Voting No: None.

7. COUNCIL BUSINESS

7.01: Consider Selection of Consultant Firm for Analysis of a Full Service, Year-Round Recycling Drop-Off Center and Non-Site Specific Plan Development

Assistant Planner/Environmental Coordinator Anderson reviewed the staff report, presented the two Request for Proposals (RFPs) that were received and recommendation to award to SAIC at a flat rate of \$30,000 based on meeting the completion date of December 31, 2012. He also presented the request of Ace Solid Waste to table this consideration.

Motion by Councilmember McGlone, seconded by Councilmember Wise, to table indefinitely consideration of a recycling drop-off center.

Further discussion Councilmember McGlone stated his rationale for not supporting the use of grant funds to study a recycling drop-off center, noting the close location of other such utilities that can be viewed, and since Ace Solid Waste is willing to put a full-time staffer in this realm at no cost to the City. The Council directed staff to ask Anoka County whether the grant can be extended and/or used for hard costs. City Administrator Ulrich noted the advantage of using the grant to hire a consultant is that it will take work from staff so they can do other work. Assistant Planner/Environmental Coordinator Anderson stated other work tasks would include code enforcement, planning responsibilities, preparation for Tree City USA, County recycling reports, and working with the EPB on its initiatives. He stated he does not dispute the knowledge of Ace relating to solid waste management or recycling but both consultants provided extensive nationwide knowledge of developing these types of facilities from design through construction phases. Strommen stated she is swayed, in part, by the argument it takes some work off staff's "plate" to gather this information and supported moving forward with the initial scoping and to develop a specific way to engage the public and Ace, a local company with significant expertise. Councilmember Wise stated his preference to extend the schedule so the Foth RFP, which is \$5,000 lower, can be considered and that he would rather see the grant funds used for hard costs. Assistant Planner/Environmental Coordinator Anderson advised the County solid waste management staff assisted with review of the proposals and identified the timeline provided by Foth as a concern based on program guidelines for spending funds. In addition, SAIC was a better match for what Ramsey wants to accomplish. Councilmember McGlone advocated for his motion to table indefinitely, stating this is a private sector endeavor and should not be a burden on City staff. With regard to timeliness of this consideration, Assistant Planner/Environmental Coordinator Anderson reviewed dates of past consideration and explained part of the delay had to do with drafting a thoughtful RFP and meeting with Anoka County, Andover, and the Coon Rapids Recycling Coordinator, and scheduling for Council consideration. Councilmembers Backous and Tossey voiced support for the motion because it would allow time to explore

utilizing Ace as a partner and use private dollars for this purpose. Councilmember Strommen noted the Council authorized the grant application, accepted the grant, and authorized staff to send out an RFP so she questioned why this is the first time these concerns are being raised. Councilmember McGlone stated in the translation between staff and him, he thought the grant could be used to build a facility, fencing, or signage. Councilmember Elvig stated it makes sense to get Ace involved but he does not support the motion if postponing a private/public effort and building without public input. Mayor Ramsey clarified the motion by Councilmember McGlone to postpone is because he does not want to move forward at this time. City Administrator Ulrich recommended action to table to November 27, 2012, a date specific, to allow time for staff to research grant funding with Anoka County and pursue an Ace Solid Waste recommendation to prepare a better proposal. Councilmember McGlone declined the suggestion and indicated the motion stands to table indefinitely. Assistant Planner/Environmental Coordinator Anderson confirmed the grant was to gather information necessary to identify space needs, equipment needs, lot size, and building setup to accommodate materials. There may be a similar grant opportunity in 2013 for up to \$50,000 without a match requirement to build on what is learned over the next few months. It was his understanding that Ace Solid Waste would provide planning and hoped the grant money could be shifted towards hard costs. Assistant Planner/Environmental Coordinator Anderson indicated Ace Solid Waste has expertise in providing hauling and recycling services; however, does not match up in terms of expertise with project design and site layout.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers McGlone, Wise, Backous, and Tossey. Voting No: Councilmembers Elvig and Strommen.

7.02: Request for Final Plat Review of NORTHGATE ADDITION (Minor Plat) and Request for Site Plan Review and Conditional Use Permit for Northgate Performing Arts Center at the North East Intersection of Sunwood Drive and Peridot Street; Case of PSD, LL and Northgate Church

Development Services Manager Gladhill reviewed the staff report and presented the site plan components and parking calculations.

City Administrator Ulrich stated there would be no TIF impact because this is tax exempt property.

The Council discussed the availability of parking and opportunity for shared parking. It was noted if the church grows and wants to expand, underground or ramp parking, or proof of parking could be addressed at that time. In addition, there is a City parking lot in the area. The Council indicated it wanted to assure that following the Planning Commission's recommendation for a Conditional Use Permit (CUP) allows opportunity to address parking at the time of a future expansion.

Development Services Manager Gladhill noted consideration is for Phase 1. Phase 2 would require site plan review and a revised CUP, resulting in City consideration. He explained that should staff identify an issue, even if no expansion, it will be brought before the Council. Development Services Manager Gladhill indicated the City's analysis did not consider the PACT

Charter School as part of this request and the applicant is aware they have to solve parking if requesting an expansion.

The Council continued its discussion of parking and agreed the site holds opportunity to build a parking structure, if needed.

Motion by Councilmember Elvig, seconded by Councilmember Backous, to Adopt the Resolution Granting Final Plat Approval of NORTHGATE ADDITION; to Adopt the Resolution Adopting the Findings of Fact Related to the Approval of a Conditional Use Permit for the Operation of Northgate Performing Arts Center; to Adopt the Resolution Granting Approval of a Conditional Use Permit for the Operation of Northgate Performing Center; and, to Approve the Site Plan of the Northgate Performing Arts Center, all contingent upon compliance with the City Staff Review letter dated September 28, 2012, with direction by the Planning Commission on the Proposed Proof of Parking Plan to Support the use of Proof of Parking as written in the Resolution.

Further discussion: City Attorney Goodrich responded to questions of the Council and indicated there is no legal issue with the City's ability to address parking in the future with a CUP change as long as it is understood that no action tonight will give them more than the ordinance allows at this time. City Administrator suggested the motion include specificity in what defines a "parking problem" so the City has the authority to revoke the CUP should it occur. City Attorney Goodrich concurred and suggested the CUP language be subject to legal review.

Substitute motion by Councilmember Elvig, seconded by Councilmember Backous, to Adopt Resolution #12-10-166 Granting Final Plat Approval of NORTHGATE ADDITION; to Adopt Resolution #12-10-167 Adopting Findings of Fact #0909 Related to the Approval of a Conditional Use Permit for the Operation of Northgate Performing Arts Center; to Adopt Resolution #12-10-168 Granting Approval of a Conditional Use Permit for the Operation of Northgate Performing Center; and, to Approve the Site Plan of the Northgate Performing Arts Center, all contingent upon compliance with the City Staff Review letter dated September 28, 2012, with direction by the Planning Commission on the Proposed Proof of Parking Plan to Support the use of Proof of Parking as written in the Resolution, and that it is at the Council's sole discretion to determine whether there is a parking problem, and subject to review and modification by legal counsel as to legal form.

Further discussion: The Council agreed the performance standards should be tied to Code parking requirements.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Backous, McGlone, Strommen, Tossey, and Wise. Voting No: None.

Councilmember Elvig left the meeting at 8:07 p.m.

7.03: Receive Bids and Authorize Contract for City Project #12-28 Rum River Hills Storm Sewer Improvements

Interim Engineer Nelson reviewed the staff report and staff's recommendation to award the bid to North Pine Aggregate.

Motion by Councilmember Backous, seconded by Councilmember McGlone, to award the alternate bid to the low bidder, North Pine Aggregate, in the amount of \$98,211.40.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Backous, McGlone, Strommen, Tossey, and Wise. Voting No: None. Absent: Councilmember Elvig.

7.04: Sunwood Drive Project – Consider Change Order #4

Interim Engineer Nelson reviewed the staff report and recommendation to approve Change Order #4.

Motion by Councilmember McGlone, seconded by Mayor Ramsey, to approve Change Order #4 in the amount of \$22,645.79.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers McGlone, Backous, Strommen, Tossey, and Wise. Voting No: None. Absent: Councilmember Elvig.

7.05: Consider Change Order #5 – Sunwood Drive (Sunwood Retail Improvements)

This item was removed upon adoption of the agenda.

7.06: Consider Ordinance to Amend City Code Section 117-118 (The COR) Approving Amendment to The COR Design Framework Related to Off-Street Parking, Signs, Parks and Trails, and Sub-District Boundaries

Development Services Manager Gladhill reviewed the staff report.

Motion by Mayor Ramsey, seconded by Councilmember McGlone, to introduce Ordinance Amending Section 117-118 "The COR Districts" of Chapter 117 of the City Code of Ramsey, Minnesota to Amend Sub-District Boundaries, Parking Regulations, Parks Section, and Master Sign Plan.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers McGlone, Backous, Strommen, Tossey, and Wise. Voting No: None. Absent: Councilmember Elvig.

7.07: Approve Additional Services – Sunwood Drive Construction Administration

City Administrator Ulrich reviewed the staff report and recommended approval of the request by Landform for an additional \$15,000 for the Sunwood Drive construction administration.

Councilmember Wise stated he would not vote on this issue, due to a potential conflict, but wanted to raise the issue that his business had been dramatically impacted during this road

project. He asked the Council to hold a future discussion about writing contracts more tightly, possibly offering an incentive for the developer to finish early, because “time is money.”

The Council discussed the Landform request and how the contract is overseen, noting this is an after-the-fact funding request to double the cost for construction administration.

City Administrator Ulrich indicated if the Council does not approve the additional amount, it would be at contractor’s risk if it did not receive Council approval prior to expending the work. However, it is somewhat fluid as to when the contractor knows the costs are reaching the maximum and needs to schedule an extension request on the Council’s agenda. City Administrator Ulrich explained it is primarily the responsibility of the City Engineer but also the responsibility of the independent contractor to bring it to the City’s attention when costs are at the limit of what was approved.

Councilmember Strommen stated her concern with considering payment after-the-fact and pointed out the Council had established precedent in other cases to not approve retroactive amounts that had not come before the Council for preapproval or there had been no written documentation related to payment. She urged the Council to be consistent in how it handles similar requests.

Councilmember Tossey stated his concerns relate to the perception of inefficient government and lack of accountability.

Development Manager Lazan maintained there were important distinctions since this is not a contract where Landform controls the scope or price to which it is committed. He explained the former City Engineer determined the scope and every day staff determines the scope when they call Landform for work. By way of baseline, he pointed out inspection for WSB is \$70,000 including survey time and WSB can cross bill time between survey and inspection. City inspection is also \$70,000 yet the City picked \$15,000 for Landform. Development Manager Lazan stated Landform has spent a considerable amount of time on this project beyond what was anticipated and that extra time has been driven by City staff, not Landform. He requested the Council approve the budget and stated he would provide more information, if requested.

Councilmember McGlone noted there had been an extraordinary number of issues with this project and many involved Landform. In addition, with winter coming, staff wanted this road project completed prior to winter weather.

Mayor Ramsey submitted that this situation is the product of a poor decision made when this project started. He noted while some on the Council had advocated to hire a professional it was decided to have City staff run this project and now it has come back to “bite us.” He found the past City Engineer had under-anticipated the work of Landform and it was not right or proper to not pay a contractor who has provided a service requested by staff.

Motion by Mayor Ramsey, seconded by Councilmember McGlone, to approve the modification to Landform’s contract number RAM 12019 revising the maximum fee from \$15,000 to \$30,000 to cover projected efforts through completion of the project.

Further discussion: Councilmember Backous asked Development Manager Lazan whether he had objected to the \$15,000 estimate when it was put forward. Development Manager Lazan indicated at one time it was understood that WSB or Landform would do the inspection. After it was decided to have City staff inspect the project, he had noted his objection twice, in writing. Councilmember Strommen stated she thinks it would have been reasonable for Landform to present this request the end of September. She stated she does not want to stop the project, but finds this to be an issue of timeliness dealing with the public's money and she intends to vote against the motion. Development Manager Lazan reviewed Landform's billing process, noting it bills two to three weeks in arrears. He had been alerted when the costs reached \$17,000 and brought it before the Council on September 25, 2012, but it had been tabled to tonight. Development Manager Lazan stated he wanted to keep the project moving forward and worked in good faith. Councilmember Backous clarified the motion is for a maximum (not to exceed) of \$30,000.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers McGlone, Backous, and Tossey. Voting No: Councilmember Strommen. Abstain: Councilmember Wise. Absent: Councilmember Elvig.

8. MAYOR, COUNCIL AND STAFF INPUT

Councilmember Tossey expressed his increasing concern related to tax increment financing (TIF) and belief that it leads to unfairness.

Motion Councilmember Tossey, seconded by Councilmember Backous, to instruct staff to start the process to decertify properties within TIF 14.

City Administrator Ulrich indicated a decertification will need additional research and public hearings.

Councilmember McGlone raised the issue of fairness to the rest of the City should TIF be eliminated.

The Council discussed whether the concept of TIF results in "handcuffing" the City's general funds. It was noted that most TIF Districts will be coming off line within a year or two.

City Administrator Ulrich reviewed the current obligation for TIF districts.

Councilmembers Tossey and Backous revised the motion to instruct staff to start the process to decertify properties within TIF 14 that are not currently obligated.

The Council continued its discussion on the impact of TIF and resulting impact to annual property taxes, costs for administration of the contract, staff time, and intangibles.

Councilmember McGlone and Mayor Ramsey indicated that while they support, in theory, eliminating TIF, it can be beneficial when in the right hands and used correctly. They, along

with Councilmember Strommen indicated additional study and information was needed prior to considering decertification.

Motion carried. Voting Yes: Councilmembers Tossey and Backous. Voting No: Mayor Ramsey, Councilmembers McGlone, Strommen, and Wise. Absent: Councilmember Elvig.

City Administrator Ulrich announced upcoming meetings and events, noting the next scheduled Council meeting is November 13, 2012.

Mayor Ramsey announced that early voting is open at City Hall.

Councilmember McGlone commented on inaccuracies in a recent *StarTribune* article relating to the City of Ramsey and The COR.

John Enstrom, 8702 – 181st Avenue NW, invited everyone visit Veterans Park on November 11, 2012.

9. ADJOURNMENT

Motion by Councilmember Backous, seconded by Councilmember Wise, to adjourn the meeting.

Motion carried.

The regular meeting of the City Council adjourned at 8:50 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL SPECIAL MEETING
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a Canvassing Board Meeting on Tuesday, November 13, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey

Members Absent: Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Fire Chief Dean Kapler
Police Chief James Way
Human Resources Manager Colleen Lasher
City Clerk Jo Ann Thieling
Development Services Manager Timothy Gladhill
Councilmember-Elect Mark Kuzma
Councilmember-Elect Chris Riley

1. CALL TO ORDER

Mayor Ramsey called the Canvassing Board meeting to order at 5:00 p.m.

2. CITIZEN INPUT

There was none.

3. APPROVE AGENDA

The agenda was approved as submitted.

4. COMMITTEE BUSINESS

4.01: Declare the Results of the November 6, 2012, General Election

City Clerk Thieling presented the results of the November 6, 2012, General Election local races: 13,791 registered voters; 2,589 voters registered on Election Day, 16,380 total registered voters. The City accepted 1,231 absentee ballots with over 600 being received at City hall. City Clerk Thieling reported that 13,586 residents voted, equaling 82.9%.

The City Council proceeded to canvass the City election returns from all City precincts, reporting ballots cast as follows:

Office of Mayor	Ballot Count
Bob Ramsey	5,492
Sarah Strommen	6,879
Write-in	33
Office of Councilmember Ward 2	Ballot Count
Colin McGlone	1,099
Mark Kuzma	2,053
Write-in	10
Office of Councilmember Ward 4	Ballot Count
Chris Riley	1,502
Wayne Buchholz	1,227
Write-in	6
Office of Councilmember At Large	Ballot Count
John LeTourneau	7,291
Joe Field	4,036
Write-in	51

Motion by Councilmember Elvig, seconded by Councilmember Tossey, to accept the results of the 2012 General Election.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Tossey, Backous, McGlone, and Strommen. Voting No: None. Absent: Councilmember Wise.

5. MAYOR/COUNCIL/STAFF INPUT

None.

6. ADJOURNMENT

Motion by Commissioner Backous, seconded by Mayor Ramsey, to adjourn the Special City Council Meeting.

Motion carried.

The Special City Council Meeting was adjourned at 5:05 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL WORK SESSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, November 13, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey
Councilmember Jeffrey Wise (arrived at 5:18 p.m.)

Also Present: City Administrator Kurtis Ulrich
Finance Director Diana Lund
Fire Chief Dean Kapler
Police Chief James Way
Human Resources Manager Colleen Lasher
City Clerk Jo Ann Thieling
Development Services Manager Timothy Gladhill
Management Analyst Patrick Brama
City Attorney William Goodrich
Development Manager Darren Lazan
Mike Mulrooney, ACG
Councilmember-Elect Mark Kuzma
Councilmember-Elect Chris Riley

1. CALL TO ORDER

Mayor Ramsey called the City Council Work Session to order at 5:08 p.m.

2. TOPICS FOR DISCUSSION

2.01: Consider Modification of TIF District 14

City Administrator Ulrich reviewed the staff report and advised it has been determined that tax-exempt portions can be removed from the District without a public hearing. He reviewed the encumbrances against Tax Increment Financing (TIF) District #14, which had been established for The COR and expanded to include properties on both sides of Highway 10 to allow maximum flexibility to capture revenue to pay for infrastructure improvements. He advised that

keeping TIF District #14 in place allows maximum flexibility for the City to pay off current and future obligations.

Councilmember McGlone clarified that TIF did not fund the loan to Flaherty & Collins but was used to back the loan. At this point, no TIF funds have been used.

City Administrator Ulrich agreed that TIF is a backing to that loan.

Councilmember Wise arrived at 5:18 p.m.

Councilmember Tossey stated he made the motion at the last meeting to remove all unencumbered properties from this TIF District because he believed that TIF plays favorites and is the impetus for government-pushed development, as shown when he tried to lower the HRA levy by one-half. Councilmember Tossey suggested TIF is the “feeding tube” for that monster and decertification of this TIF District, whenever it occurs, is the right thing to do. He felt Ramsey should compete against other municipalities with its tax rate and not through the use of TIF. Councilmember Tossey stated cities are not using TIF as the Legislature intended so he thinks TIF Districts have hurt the taxpayer.

City Administrator Ulrich stated TIF has a subsidy side and a public improvement side. He pointed out that in Ramsey all TIF, except for \$5 million, is towards the public side. The Council has the choice to use TIF for public improvements or private development and has limited private subsidies.

Finance Director Lund explained the City has a JPA with Anoka County to construct Armstrong Boulevard with funding backed by TIF District 14. That is the only funding mechanism for that project or else the City would have to pass a bond or increase the levy. That is an outstanding issue. Finance Director Lund listed the encumbrances related to TIF District 14.

Councilmember Strommen stated this is good discussion and she appreciates Councilmember Tossey bringing it up. She indicated she is supportive of having staff gather financial information but believed TIF use is nested within the central issue that the Council has to address in January, the role of the City in The COR, the nature of that role, and the tools the City uses. Councilmember Strommen felt now is the time to launch an information gathering process, but not to make a decision on TIF use.

Finance Director Lund answered questions of the Council related to funding for road projects and the AUAR JPA.

Councilmember Tossey asked whether any funding for the overpass scenario used TIF backing.

Finance Director Lund explained that when Anoka County did the JPA, Armstrong was part of the funding mechanism and that is why Anoka County placed a \$4 million bond for their share of the project cost.

Mayor Ramsey stated the Council needs to be aware it is \$68 million but TIF is an arbitrary number and it could be much higher depending on how fast development happens and how much increment is captured over 25 years. He noted it was stated there is \$8 million for incentive of private projects but the City has a spending plan it agreed to when it got the legislation for this District that can be modified without asking for Legislature approval. There is no law saying Ramsey cannot do that and that is a danger in this TIF District. Mayor Ramsey stated he is concerned about this TIF District since the citizens of Ramsey spoke very clearly in the last election when the people who ran said the City should not be involved in this development. He stated he wanted discussion on this issue to be at the Council level when it is on camera.

Councilmember Strommen stated she agreed this is a central issue that needs to be addressed but the Council does not need to rush to that conclusion and she hopes the intent is not to make a political statement that would be disingenuous to the residents.

Mayor Ramsey stated he and Councilmember McGlone had worked very hard to get this TIF District and this discussion needs to be brought up on camera.

Councilmember Tossey stated this is why he did not think TIF was a good idea; it is a “political football.” He objected to playing with people’s money and supported decertifying the TIF District so the School District and Anoka County can each get their taxes. He felt enough is enough.

2.02: Consider Lease Agreement with Independent Auto Service, Inc. for use of Cold Storage Area; and Consider Sublease Agreement with First Choice Towing (6745 Highway 10)

Management Analyst Brama reviewed the staff report and presented the terms of the proposed sublease agreement with First Choice Towing. He noted this item is on tonight’s Consent Agenda.

Councilmember Wise asked about the square foot cost of the lease with Denny Sharp.

Management Analyst Brama stated the previous lease was a higher per square foot charge for this property than the property to the east but that conflicts with economic reality because the other building is in better condition and has a higher economic value. So, staff flipped the rates to better reflect the market. Management Analyst Brama stated he had also worked with Anoka County on the tax per square foot difference and found one was taxed at a higher rate, again being different from the economic reality since one building is in better condition than the other. He indicated the lease is about \$1.90/sq. ft. on this property and is now at \$1.75/sq. ft. The rate for the other property was flipped with this one to better reflect the economic reality.

Councilmember Wise asked if staff sees any problems that could come up with a sublease on a City-owned property.

City Attorney Goodrich advised the lease provides that the tenant can sublease. The City has the right to review but if the uses are consistent with the main tenant and lawful commercial uses,

the City could not unreasonably withhold its approval. He explained that a concern may be related to traffic or pollution, or hazardous activities (fireworks/explosives), but it appears there are no issues with this tenant.

Councilmember McGlone stated the need for fairness had been raised earlier tonight and that is his concern with RALF properties that the City leases, at a cost to the City, and creates an unfairness in the market. He noted it is not fair for a business to operate in a building for tax value or less when the next building has a mortgage in addition to taxes. Councilmember McGlone felt this issue dovetails with the previous TIF conversation about fairness. He noted that when properties are acquired with RALF, it also comes with demolition money so maybe the City should demolish this portion of the building so it would not need to hire a person to manage the buildings.

Mayor Ramsey asked whether there are any special requirements or permits for this property to serve as a vehicle impound lot.

Development Services Manager Gladhill explained the Code would restrict salvage operation or outdoor storage of inoperable vehicles. However, if the vehicle is towed for inside storage, it is a permitted use. Eddy's Auto Body has restrictions on outside storage, as an example.

City Attorney Goodrich advised that the current lease with the landlord, the master lease, says no outside storage is allowed. He indicated that any subtenant could not do more than allowed in the master lease.

Councilmember Strommen asked City Attorney Goodrich if he had reviewed the sublease and found that it is consistent with the master lease terms.

City Attorney Goodrich answered in the affirmative.

Management Analyst Brama stated the sublease alludes to the current lease of the master tenant and says it must follow those terms.

City Attorney Goodrich stated the potential option for two years has rent capped at 3% annually for those two years so if the term is going out five years, the two years after that cannot be increased by more than 3%. He stated the City should always assure the leases cover taxes.

Councilmember Tossey asked whether taxes would be covered.

Management Analyst Brama answered in the affirmative.

Councilmember Tossey asked if there is a way out of the master lease in case the Highway 10 project moves forward.

City Attorney Goodrich advised the lease contains a one-year provision with notice no matter the term of the lease.

Councilmember Elvig noted a question had been previously asked about charging administrative fees for RALF properties back to the Metropolitan Council.

City Administrator Ulrich stated that discussion occurred during the strategic plan, to look at special legislation to broaden what can be done with RALF proceeds when the properties are leased.

City Attorney Goodrich noted this item is on tonight's Consent Agenda.

The Council agreed to next consider Case 2.04 since Mr. Mulrooney was not yet in attendance.

2.04: Consider Development of 15153 Nowthen Boulevard, Old City Hall Site

Development Services Manager Gladhill reviewed the staff report.

Councilmember Backous asked for more background information on data centers, ownership, and if such a use would be good for Ramsey's businesses. He stated for him it comes down to cost benefit, benefit to Ramsey's residents, and if it will generate tax revenue compared to having homes.

Management Analyst Brama explained it would be a private business that typically serves private businesses, but could also serve public entities, regionally and perhaps nationally. A data center would be a large-sized building with a lot of servers for storage, a higher energy user, and generate a lot of property tax. In addition, a data center would be low operating from a City cost perspective for public services, yet have a high value and tax generation. Aesthetically, data centers resemble buildings in an industrial park.

Councilmember Backous felt it would be a good use for this area that is surrounded by homes and may provide the least amount of pushback from residents because it would not generate a lot of traffic.

Mayor Ramsey stated a data center will be very noisy.

Councilmember Wise suggested presenting the benefits of tax generation and low traffic to the public at the start of the process prior to City and County and Connexus investment.

Mayor Ramsey stated data centers generate a lot of noise to cool a huge amount of heat.

Councilmember Elvig stated that issue came up at the EDA meeting. He noted this site was slated, at one time, for a potential middle school and he would like staff to look into that again to be sure it is not still an option. Councilmember Elvig agreed that data centers generate a tremendous amount of heat and expressed concern should the City partner with Connexus, become obligated, and then find out the sound is not mitigated as much as the City would like. He stated he has been involved with putting a sound barrier on a data center located near a residential area and it did not work. Councilmember Elvig explained that data centers use diesel

generators with exhaust pipes that are run every day. He raised the option of using this site for density housing since it is across from an elementary school.

Mayor Ramsey stated a data center would be a great thing for the City as long as the residents around it buy into that use. He noted it would provide a great tax base and there is fiber optics in that location to utilize.

Management Analyst Brama advised that one of the concerns with a data center is the setback, which will need to be greater and have more screening. Staff is also aware of the need to address the issue of sound. He asked if there is general support for this use.

Councilmember-Elect and EDA Chair Riley stated the EDA had discussed the data center use, looked into it, and found there had been a lot of sound issues with first generation data centers but since that time, there have been a lot of improvements. Mr. Riley indicated sound is still a real issue to resolve but improvements have been made with data centers to contain more inside and there is now a second-generation design.

Don Haller, Connexus Energy, addressed the question raised relating to partnerships by explaining there would be no contracts, just an energy company with the local City saying there is potential for a data center. Mr. Haller stated Connexus has analyzed the electrical grid and determined this is the only site in Anoka County that is conducive for a data center so Connexus approached the City. He explained that data centers have come a long way in technology and the Councilmembers could visit two of the largest ones located in Elk River and the Anoka industrial park to see what it would look like and how much sound it generates. Mr. Haller stated it has been refreshing to work with Ramsey on an opportunity for a data center and discuss mutual benefits for both the City and Connexus.

Mayor Ramsey asked what is the anticipated price per foot for this land given the constraints with data centers and surrounding uses.

Management Analyst Brama referenced the staff report section containing a breakdown of the per square foot value of a data center. He pointed out that in comparison to a normal business, it would have a higher than normal value.

Mayor Ramsey asked what a data center would be willing to pay for this land.

Mr. Mulrooney advised that due to the uniqueness of sites that can accommodate data centers, they are willing to pay a higher per foot price than typical businesses. He stated he has seen prices double in south metro (Eden Prairie) locations. However, it can depend on the size of the facility and how well capitalized the user is. Mr. Mulrooney stated if the site works, data centers are usually willing to pay top dollar for the property.

Councilmember Tossey mentioned advise that he received that the City often times gets bogged down by saying “no” a lot. He stated support for approaching the citizens to get their buy in and if that happens, he thinks a data center would be an asset for Ramsey.

Mayor Ramsey suggested holding a public hearing or neighborhood meeting in the old City Hall to give residents information on this use.

City Administrator Ulrich suggested an informal open house during which Connexus can present this use.

The consensus of the Council was to direct staff to continue to explore developing the old City Hall site as a data center site; hold an informal open house with area residents; and, invite Connexus and the School District.

2.03: Consider EDA Revolving Loan Fund (RLF) Application

Mr. Mulrooney presented the loan request of Koru Fitness to develop a new fitness center in the City. Koru Fitness will be owned equally by Nicole Kutches and Gary Nereson. He explained the financing plan, noting it would include an EDA revolving loan at 3.25% with a seven-year term for equipment purchases. Mr. Mulrooney stated there is not an operating history to determine sufficient cash flow to support the proposed debt but an extensive business plan has been submitted and the worth and credit ratings of the borrowers would be considered. It was noted the EDA had recommended approval of this loan request.

Councilmember Wise asked who is providing the personal guarantees.

Mr. Mulrooney advised that each partner would provide a personal guarantee of 50%.

Councilmember Wise stated personal guarantees are required but oftentimes LLCs are created to protect personal assets. He asked what would happen to recover funds should this business fail. He stated his concern is that this is a non-brand fitness center operating with used equipment that does not hold resale value, which he thinks is a bigger risk.

Mr. Mulrooney explained that in the event the business defaults, the collateral would be liquidated and the shortage would be pursued from the personal guarantees. He assured the Council that he has complete confidence in the business owner being able to perform on the personal guarantee based on his worth and businesses formerly owned and sold. Mr. Mulrooney stated the owner is a very strong guarantor and not the typical business start up.

Councilmember Tossey asked about the life of the fitness equipment.

Mr. Mulrooney stated it depends on which piece of equipment and there are also staging and flooring. He stated this is not the type of equipment you would see at a LifeTime Fitness and should have a 7-10 year term prior to needing replacement.

Councilmember Tossey noted the loan term is seven years and if the equipment life is more than five years, it is not as much of a problem.

Councilmember McGlone stated he had opposed this loan request during the EDA meeting. He stated he had asked about the failure rate for this type of business but not received an answer so

he did some internet research and found it is about 90%. In addition, this proposes a new concept in that realm which, to him, makes it more of a risk. Councilmember McGlone stated at the EDA meeting, Mr. Mulrooney had said this was a business owner who wanted to teach his daughter business, but that is not what the City does and if the balance sheet of the silent partner is so great, he should add another \$30,000. Councilmember McGlone stated he is in the liquidation business, had sold four fitness businesses this year, and knows this equipment is valueless and there is no ability to recoup costs from the flooring or equipment. Councilmember McGlone stated he did not find this to be a good risk, and the City should not use public money to teach someone's daughter how to run a business.

Councilmember Tossey asked what is the failure rate and City's loan position.

Mr. Mulrooney advised the City's loan would be a junior position. He clarified that he is not saying this is a "daddy teaching daughter" situation. Rather, the father is guiding his daughter on business matters in things he is experienced in and that is why he is included in this business. Mr. Mulrooney noted that partner is financially invested in this transaction, providing capital and personal guarantee that any financing will be paid, placing confidence in his daughter's ability.

Councilmember Backous stated since the EDA recommends approval, he assumes this request meets RLF requirements so he will support that recommendation.

Mayor Ramsey stated the Council got "crucified" for loaning to Flaherty & Collins, a multimillion-dollar business that created tax base with a new building constructed and providing a benefit for 50 years or more. He noted this is an existing structure that is already paying taxes and is taking \$87,000 in working capital. Mayor Ramsey felt, basically, the City would be loaning them a portion of operating capital. He stated his position that this is a bad deal and not something the City should do for this type of a business so he will oppose it.

Councilmember Backous felt there were a couple differences between the Flaherty & Collins consideration and this application. He agreed there was history of LLC and bankruptcies and that some personal guarantees do not mean a lot as mentioned by Councilmember Wise. However, in this case, the applicants have an excellent credit history and the application fits inside the RLF guidelines.

Councilmember Strommen stated she does not put this request in the same league as Flaherty & Collins because this is a request for \$34,500 and the owners are putting in \$34,000, so it is almost equal. This loan would result in incubating a business with the hope it will grow and move out of a leased space. Councilmember Strommen pointed out that this is the purpose of the EDA Revolving Loan Program and asked EDA Chair Riley if the EDA had discussed any other issues related to this application.

Councilmember-Elect and EDA Chair Riley stated the EDA found this request did meet the program criteria. In addition, it falls under the mission of the EDA because it promotes economic development and the purpose of this fund, to help local businesses. He felt that was the big distinction between the two loans being compared. Councilmember-Elect Riley stated

the EDA recommended approval and he hopes the City Council will heed what that citizen group advised.

Councilmember McGlone stated this is a business with an outlandish failure rate being backed with valueless equipment. He thinks it is a bad deal.

Management Analyst Brama stated if this request is not supported by the Council, it should be pulled from tonight's Consent Agenda.

City Administrator Ulrich noted the loan agreement has been changed from ten years to seven years.

Councilmember Tossey stated the policy of the EDA loan fund is for only a seven-year term; however, the paperwork said it is a ten-year term, which does not fit into the guidelines. He agreed this application is not to the degree of monetary value of Flaherty & Collins, but principally it is similar. Councilmember Tossey stated he is concerned whether or not due diligence was completed to assure this application meets EDA policy since the term had to be changed from ten years to seven years.

Mayor Ramsey stated the application is to loan \$34,500 for a business that has no value.

Councilmember Elvig felt Mr. Mulrooney has a lot of "horsepower" behind his research, knows the applicants, and the strength of their financials. In addition, Mr. Mulrooney is the President and CEO of a financial company that is highly successful in its analysis. Councilmember Elvig stated he found this application to be a reasonable deal.

Mayor Ramsey stated it was not his intent to discredit the person making the request, but he does not support this loan.

Councilmember Tossey asked why one partner is not footing more of the bill.

Mr. Mulrooney stated the partners want to preserve capital for working purposes rather than putting all into assets, which is fairly typical in these types of loan structures. He explained you have to look at this as an unsecured transaction since the City would be in a subordinate position. In order to obtain any dollars back in the event of liquidation, the first lien holder would have to be paid off. Mr. Mulrooney advised if there was not a guarantor behind the deal, he may think differently since this is a start up business with a higher failure rate. However, the principal is a very strong guarantor, has a vast amount of experience in business, and had sold his company to a larger firm a few years ago. This guarantor is someone who has the "horsepower" to see this through and assure all of the lenders will be repaid in event there is a default.

Councilmember Tossey asked whether the applicants were notified that the term of the loan had to be reduced to seven years.

Mr. Mulrooney answered in the affirmative.

Councilmember Wise asked about the impact of the loan term change.

Mr. Mulrooney stated the debt service went from \$4,045 to \$5,015, or just over \$100/month.

Councilmember McGlone stated he intends to pull this item from the Consent Agenda.

2.04: Consider Development of 15153 Nowthen Boulevard, Old City Hall Site

This item was considered prior to Case 2.03.

3. FUTURE TOPICS FOR DISCUSSION

3.01: Review Future Work Session Topics / Calendar

City Administrator Ulrich advised of upcoming agenda topics and that the Kiefer law suit would be considered on November 27, 2012. He indicated there is currently no Work Session scheduled for December 4, 2012, and the December 25, 2012, meeting has been canceled.

4. MAYOR / COUNCIL / STAFF INPUT

None

5. ADJOURNMENT

Motion by Councilmember Backous, seconded by Councilmember Wise, to adjourn the Work Session meeting.

The Work Session of the City Council was adjourned at 6:40 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

**CITY COUNCIL SPECIAL MEETING
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey City Council conducted a Canvassing Board Meeting on Tuesday, November 13, 2012, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Bob Ramsey
Councilmember Randy Backous
Councilmember David Elvig
Councilmember Colin McGlone
Councilmember Sarah Strommen
Councilmember Jason Tossey

Members Absent: Councilmember Jeffrey Wise

Also Present: City Administrator Kurtis Ulrich
Fire Chief Dean Kapler
Police Chief James Way
Human Resources Manager Colleen Lasher
City Clerk Jo Ann Thieling
Development Services Manager Timothy Gladhill
Councilmember-Elect Mark Kuzma
Councilmember-Elect Chris Riley

1. CALL TO ORDER

Mayor Ramsey called the Canvassing Board meeting to order at 5:00 p.m.

2. CITIZEN INPUT

There was none.

3. APPROVE AGENDA

The agenda was approved as submitted.

4. COMMITTEE BUSINESS

4.01: Declare the Results of the November 6, 2012, General Election

City Clerk Thieling presented the results of the November 6, 2012, General Election local races: 13,791 registered voters; 2,589 voters registered on Election Day, 16,380 total registered voters. The City accepted 1,231 absentee ballots with over 600 being received at City hall. City Clerk Thieling reported that 13,586 residents voted, equaling 82.9%.

The City Council proceeded to canvass the City election returns from all City precincts, reporting ballots cast as follows:

Office of Mayor	Ballot Count
Bob Ramsey	5,492
Sarah Strommen	6,879
Write-in	33
Office of Councilmember Ward 2	Ballot Count
Colin McGlone	1,099
Mark Kuzma	2,053
Write-in	10
Office of Councilmember Ward 4	Ballot Count
Chris Riley	1,502
Wayne Buchholz	1,227
Write-in	6
Office of Councilmember At Large	Ballot Count
John LeTourneau	7,291
Joe Field	4,036
Write-in	51

Motion by Councilmember Elvig, seconded by Councilmember Tossey, to accept the results of the 2012 General Election.

Motion carried. Voting Yes: Mayor Ramsey, Councilmembers Elvig, Tossey, Backous, McGlone, and Strommen. Voting No: None. Absent: Councilmember Wise.

5. MAYOR/COUNCIL/STAFF INPUT

None.

6. ADJOURNMENT

Motion by Commissioner Backous, seconded by Mayor Ramsey, to adjourn the Special City Council Meeting.

Motion carried.

The Special City Council Meeting was adjourned at 5:05 p.m.

Respectfully submitted,

Kurtis G. Ulrich
City Administrator

ATTEST:

Jo Ann M. Thieling
City Clerk

Drafted by Carla Wirth
TimeSaver Off Site Secretarial, Inc.

CC Regular Session

4. 5.

Meeting Date: 11/27/2012

By: Diana Lund, Finance

Information

Title:

Approval for Exemption for a Gambling License for Simply Outdoor Experiences to Conduct a Raffle at 8019 - 146th Avenue NW - Holte Contracting Office

Background:

The attached application is from Simply Outdoor Experiences for exemption for a lawful gambling license to conduct a raffle at 8019 - 146th Avenue NW on December 31, 2012.

An exempt permit may be issued to a non-profit organization that conducts lawful gambling and awards than less than \$50,000 in prized during a calendar year. Simply Outdoor Experiences meets this requirement. The prized being raffled are under \$3,000 in total value.

Approval requires nothing more than a motion followed by a letter to the Gambling Control Board, while if the request is denied, a formal resolution would need to be prepared and submitted tot he Gambling Control Board.

Recommendation:

Staff recommends Council approve the exemption from a lawful gambling license for Simply Outdoor Experiences to conduct a raffle at 8019 - 146th Avenue NW - Holte Contracting Office on December 31, 2012.

Council Action:

Motion to recommend Council approve the exemption from a lawful gambling license for Simply Outdoor Experiences to conduct a raffle at 8019 - 146th Avenue NW - Holte Contracting Office on December 31, 2012.

Attachments

Simply Outdoors Application

Form Review

Inbox	Reviewed By	Date
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:46 AM
Form Started By: Diana Lund		Started On: 11/21/2012 07:50 AM
	Final Approval Date: 11/21/2012	

MINNESOTA LAWFUL GAMBLING
LG220 Application for Exempt Permit

6/12 Page 1 of 2

An exempt permit may be issued to a nonprofit organization that: - conducts lawful gambling on five or fewer days, and - awards less than \$50,000 in prizes during a calendar year. If total prize value for the year will be \$1,500 or less, contact the licensing specialist assigned to your county.	Application fee	
	If application posted or received: less than 30 days before the event \$100	more than 30 days before the event \$50

ORGANIZATION INFORMATION

Organization name: Simply Outdoor Experiences Previous gambling permit number: X-92919-12-003
 Minnesota tax ID number, if any: _____ Federal employer ID number (FEIN), if any: 45-4129783

Type of nonprofit organization. Check one.
 Fraternal Religious Veterans Other nonprofit organization

Mailing address: P.O. Box 7384 City: St. Cloud State: MN Zip code: 56302 County: Stearns
 Name of chief executive officer (CEO): Woody Sankey Daytime phone number: 612-770-3275 E-mail address: Woody.Sankey@gmail.com

NONPROFIT STATUS

Attach a copy of ONE of the following for proof of nonprofit status.

Nonprofit Articles of Incorporation OR a current Certificate of Good Standing.
 Don't have a copy? This certificate must be obtained each year from:
 Secretary of State, Business Services Div., 60 Empire Drive, Suite 100, St. Paul, MN 55103
 Phone: 651-296-2803

IRS income tax exemption [501(c)] letter in your organization's name.
 Don't have a copy? To obtain a copy of your federal income tax exempt letter, have an organization officer contact the IRS at 877-829-5500.

IRS - Affiliate of national, statewide, or international parent nonprofit organization [charter].
 If your organization falls under a parent organization, attach copies of both of the following:
 a. IRS letter showing your parent organization is a nonprofit 501(c) organization with a group ruling, and
 b. the charter or letter from your parent organization recognizing your organization as a subordinate.

GAMBLING PREMISES INFORMATION

Name of premises where the gambling event will be conducted. For raffles, list the site where the drawing will take place.
Holte Contracting Office

Address [do not use PO box]: 146th Ave NW City or township: Ramsey Zip code: 55303 County: Anoka
8019 Grandstrong Blvd

Date[s] of activity. For raffles, indicate the date of the drawing.
12/31/12

Check each type of gambling activity that your organization will conduct.

Bingo* Raffle Paddlewheels* Pull-tabs* Tipboards*

*Gambling equipment for bingo paper, paddlewheels, pull-tabs, and tipboards must be obtained from a distributor licensed by the Minnesota Gambling Control Board. EXCEPTION: Bingo hard cards and bingo number selection devices may be borrowed from another organization authorized to conduct bingo.

To find a licensed distributor, go to www.gcb.state.mn.us and click on **Distributors** under the **WHO'S WHO? LIST OF LICENSEES**, or call 651-639-4000.

LG220 Application for Exempt Permit

6/12 Page 2 of 2

LOCAL UNIT OF GOVERNMENT ACKNOWLEDGMENT

CITY APPROVAL for a gambling premises located within city limits

- The application is acknowledged with no waiting period.
- The application is acknowledged with a 30 day waiting period, and allows the Board to issue a permit after 30 days [60 days for a 1st class city].
- The application is denied.

Print city name City of Ramsey

Signature of city personnel [Signature]

Title Finance Director Date 11/21/12

Local unit of government must sign

COUNTY APPROVAL for a gambling premises located in a township

- The application is acknowledged with no waiting period.
- The application is acknowledged with a 30 day waiting period, and allows the Board to issue a permit after 30 days.
- The application is denied.

Print county name _____

Signature of county personnel _____

Title _____ Date _____

TOWNSHIP - If required by county. On behalf of the township, I acknowledge that the organization is applying for exempted gambling activity within the township limits.

[A township has no statutory authority to approve or deny an application, per Minnesota Statutes 349.166.]

Print township name _____

Signature of township officer _____

Title _____ Date _____

CHIEF EXECUTIVE OFFICER'S SIGNATURE

The information provided in this application is complete and accurate to the best of my knowledge. I acknowledge that the financial report will be completed and returned to the Board within 30 days of the event date.

Chief executive officer's signature [Signature]

Date 11/20/12

Print name Wade Seiber

Print form and have CEO sign

REQUIREMENTS

Reset form

Complete a separate application for:

- all gambling conducted on two or more consecutive days, or
 - all gambling conducted on one day.
- Only one application is required if one or more raffle drawings are conducted on the same day.

Send application with:

- a copy of your proof of nonprofit status, and
- application fee. Make check payable to "State of Minnesota."

To: Gambling Control Board
1711 West County Road B, Suite 300 South
Roseville, MN 55113

Financial report and recordkeeping required

A financial report form and instructions will be sent with your permit, or use the online fill-in form available at www.gcb.state.mn.us.

Within 30 days of the event date, complete and return the financial report form to the Gambling Control Board.

Questions?

Call the Licensing Section of the Gambling Control Board at 651-639-4000.

This form will be made available in alternative format (i.e. large print, Braille) upon request.

Data privacy notice: The information requested on this form (and any attachments) will be used by the Gambling Control Board (Board) to determine your organization's qualifications to be involved in lawful gambling activities in Minnesota. Your organization has the right to refuse to supply the information; however, if your organization refuses to supply this information, the Board may not be able to determine your organization's qualifications and, as a consequence, may refuse to issue a permit. If your organization supplies the information requested, the Board will be able to process the application. Your organization's name and address will be public information when received by the Board.

All other information provided will be private data about your organization until the Board issues the permit. When the Board issues the permit, all information provided will become public. If the Board does not issue a permit, all information provided remains private, with the exception of your organization's name and address which will remain public. Private data about your organization are available to: Board members, Board staff whose work requires access to the information; Minnesota's Department of Public Safety; Attorney

General; Commissioners of Administration, Minnesota Management & Budget, and Revenue; Legislative Auditor, national and international gambling regulatory agencies; anyone pursuant to court order; other individuals and agencies specifically authorized by state or federal law to have access to the information; individuals and agencies for which law or legal order authorizes a new use or sharing of information after this notice was given; and anyone with your written consent.

Meeting Date: 11/27/2012

Submitted For: Kurt Ulrich

By: Bill Goodrich, Administrative Services

Information

Title:

Approve Highway 10 access easement to benefit property located at 6781 Highway 10 NW.

Background:

The City through the RALF program purchased a vacant lot in 2009 located on Highway 10 and directly west of Pro Power Sports and Marine whose address is 6781 Highway 10 NW. The legal description of the City lot is Tract A, Registered Land Survey No. 249, Anoka County, Minnesota. Attached is a drawing depicting the lot.

The State of Minnesota, sometime ago, acquired the right-of-way access to the City lot and the lots lying directly east (6781 Hwy 10) and west of the City lot, except for a 40' opening located in the southeast corner of the City lot.

PSG, LLC the owner of 6781 Hwy 10 is now selling that lot and wants to confirm its Hwy 10 access by a recorded easement across the southerly 30 feet of the easterly 30 feet of the City lot. This area is already encumbered by a 40 foot wide service road easement which lies adjacent to Highway 10 and encumbers all three lots. Staff recommends granting the access easement. A copy of the proposed easement document is attached. Since this is a RALF acquired property, Metropolitan Council approval of the easement grant will be required.

Funding Source:

N/A

Council Action:

Motion to grant an ingress/egress permanent easement over and across Tract A, Registered Land Survey No. 249 which easement is more specifically described on the proposed easement document attached hereto. The grant of this easement is subject to the approval of the Metropolitan Council.

Attachments

RLS 249 and Easement

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
11/21/2012 01:23 PM
Started On: 11/21/2012 10:19 AM

Form Started By: Bill Goodrich

Final Approval Date: 11/21/2012

REGISTERED LAND SURVEY NO. 249

CITY OF RAMSEY
COUNTY OF ANOKA
SEC. 34, TWP. 32, RGE. 25

I, Craig E. Johnson, a duly licensed land surveyor under the laws of the State of Minnesota, do hereby certify that, in accordance with the provisions of Minnesota Statutes, Section 508.47, have surveyed the following described property situated in the County of Anoka, State of Minnesota:

Lots 1 and 3, Block 1, DEAL COMMERCIAL CENTER, according to the recorded plat thereof, Anoka County, Minnesota.

I hereby certify that this Registered Land Survey was prepared by me or under my direct supervision and is a correct delineation of Certificate(s) of Title No. 111501 & 111502.

Dated this 31st day of JULY, 2009

Craig E. Johnson
Craig E. Johnson, Land Surveyor
Minnesota License No. 44530

CITY OF RAMSEY

We do hereby certify that on the 31st day of July, 2009, the City Council of the City of Ramsey, Minnesota, has approved this Registered Land Survey No. 249

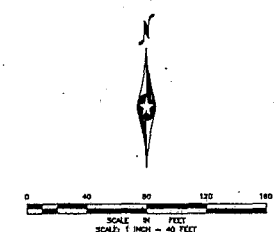
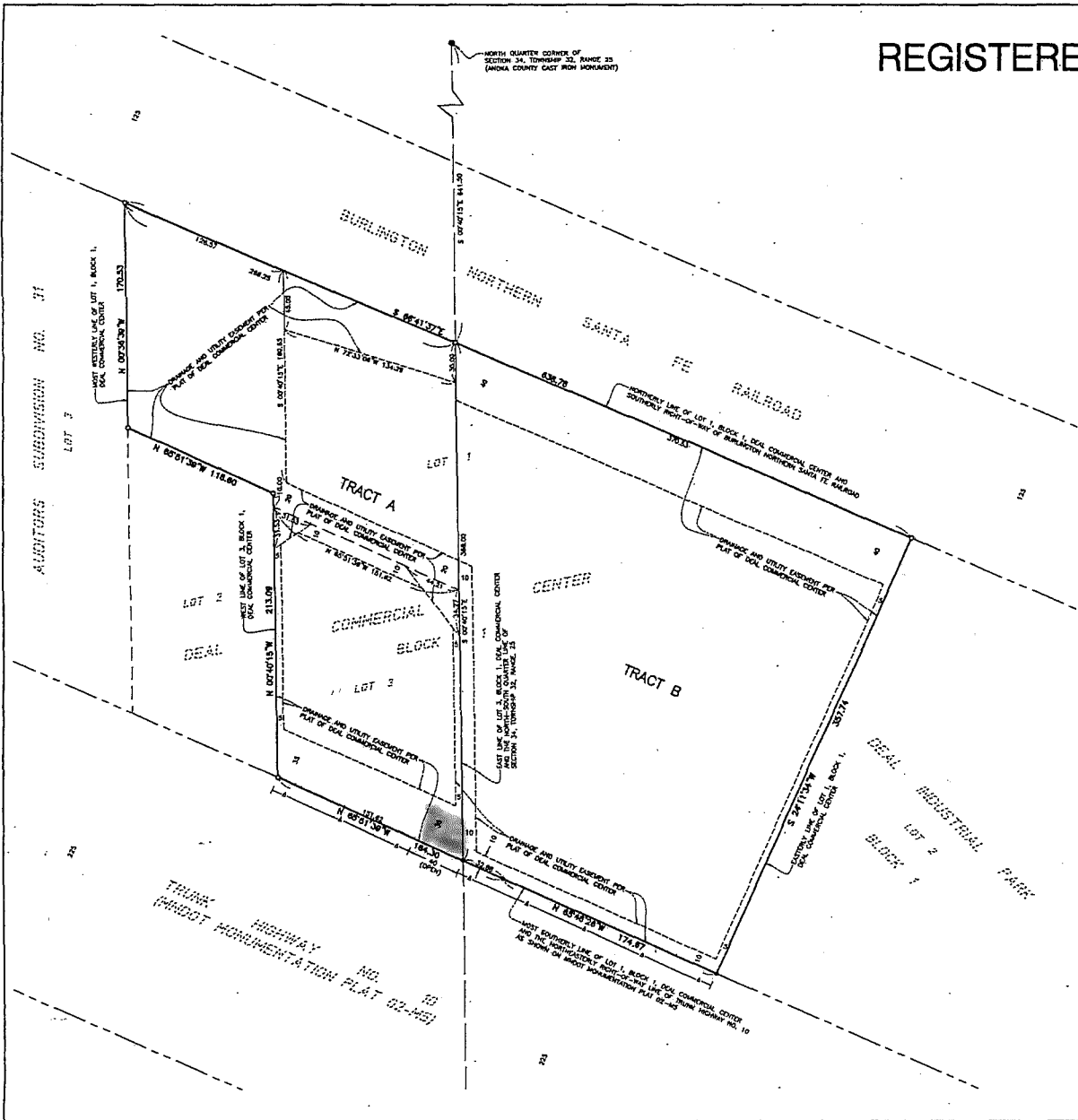
By: *Mark*
Mayor

By: *Charles F. Keaton*
City Clerk

ANOKA COUNTY SURVEYOR

Checked and approved this 1st day of August, 2009.

Larry D. Holium by *Charles F. Keaton*, Deputy
Larry D. Holium
Anoka County Surveyor



Orientation of the bearing system is based upon the North-South Quarter line of Section 34, Township 32, Range 25, Anoka County, Minnesota, which is assumed to bear South 00 degrees 40 minutes 15 seconds East.

- LEGEND**
- DENOTES 1/2" INCH x 1 1/4" INCH IRON PIPE MONUMENT SET AND MARKED BY REL. NO. 44320
 - DENOTES MONUMENT FOUND
 - DENOTES CAST IRON MONUMENT FOUND
 - DENOTES MOVE OF ACCESS DEPOSITED FOR PLAT OF DEAL COMMERCIAL CENTER

498961.002
Office of REGISTERED LAND SURVEYORS
COUNTY OF ANOKA
CRAIG E. JOHNSON
I hereby certify that the above described plat is a true and correct copy of the original plat as shown to me by the surveyor on this day of July 2009.
Craig E. Johnson
REGISTERED LAND SURVEYOR
MINNESOTA LICENSE NO. 44530

I HEREBY CERTIFY THAT THE CURRENT AND PREVIOUS SURVEYS ON THE LANDS DESCRIBED WERE MADE AND HAVE THE FOLLOWING IN COMMON: 498961.002
Charles F. Keaton
CITY CLERK
CITY OF RAMSEY, MINNESOTA

DEED OF EASEMENT

This Indenture, made and entered in this ____ day of _____, 20____, by and between the City of Ramsey, grantor, and PSG, LLC, a Minnesota limited liability company, grantee,

WHEREAS, said grantor is the owner of the tract of land in the County of Anoka, State of Minnesota, legally described as:

Tract A, Registered Land Survey No. 249, City of Ramsey, Anoka County, Minnesota.

WHEREAS, said grantee is the owner of **Tract B, Registered Land Survey No. 249, City of Ramsey, Anoka County, Minnesota.**

NOW, THEREFORE, the said grantor, in consideration of the mutual easements and other good and valuable consideration, hereby grants to said grantee, its successors and assigns, an easement for the purposes of ingress and egress over and across that part of said grantor's property described as follows:

The southerly 30.00 feet of the easterly 30.00 feet of Tract A, Registered Land Survey No. 249, as measured at right angles to the south and east lines thereof and adjoining the northerly line of State Trunk Highway No. 10.

TO HAVE AND TO HOLD THIS EASEMENT unto the grantee, its successors, and assigns, forever as appurtenant to grantee said land.

IN WITNESS WHEREOF, said grantor has hereunto set its hand the day and year first above written.

CITY OF RAMSEY

By _____
Its _____

STATE OF MINNESOTA)
) ss.
COUNTY OF ANOKA)

The foregoing was acknowledged before me this ____ day of _____, 20____, by the City of Ramsey, by _____, its _____.

Notarial Stamp or Seal

Signature of Person Taking
Acknowledgment

THIS INSTRUMENT WAS DRAFTED BY:
William A. Erhart, Esq. (ID#027066)

CC Regular Session

4. 7.

Meeting Date: 11/27/2012

By: Jo Thieling, Administrative Services

Information

Title:

Approve Joint Powers Agreement between the City of Ramsey and County of Anoka for Assessment of Property

Background:

As Council is aware, the City of Ramsey contracts with Anoka County for property assessment. It is once again time to renew the five-year contract. Attached is the letter from the Anoka County Assessor as well as a copy of the Joint Powers Agreement. It should be noted that the per parcel rate for each type of property will remain unchanged for the duration of this contract period.

Council Action:

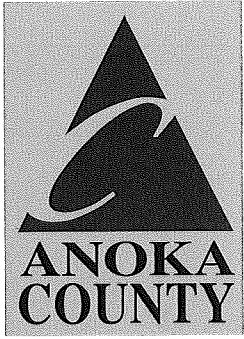
Motion to approve the Joint Powers Agreement with Anoka County for the 2014 through 2018 Assessment Contract.

Attachments

JPA with Anoka County for Property Assessment

Form Review

Inbox	Reviewed By	Date
Kurt Ulrich	Kurt Ulrich	11/15/2012 04:03 PM
Form Started By: Jo Thieling		Started On: 11/13/2012 03:32 PM
	Final Approval Date: 11/15/2012	



C O U N T Y O F A N O K A

DIVISION OF PROPERTY RECORDS AND TAXATION

GOVERNMENT CENTER • 2100 3RD AVENUE • ANOKA, MN 55303
FAX (763) 323-5421

www.AnokaCounty.us/prop-records-tax

- *Property Assessment*
- *Property Records and Public Service*
- *Property Tax Accounting and Research*

October 5, 2012

Kurt Ulrich
City Administrator
7550 Sunwood Dr. NW
Ramsey, MN 55303

Re: 2014 through 2018 Assessment Contract


Dear Mr. Ulrich,

Enclosed, please find a copy of our proposed Assessment Contract with your jurisdiction for the 2014 through 2018 Assessment years.

You will find that the per parcel rate for each type of property will remain unchanged for the duration of this contract period. We have been able to do this through some retirements and some alternate appraisal methods that have helped us do more work with the same or fewer people. Our plan at the county is to continue to take advantage of whatever technologies are available to get our work done at the most competitive rates possible.

It is our pleasure to be of service to you. If you have any questions or require my presence at any council or board meeting prior to adoption I am at your disposal.

Thank you for your kind attention.



Michael R. Sutherland SAMA
Anoka County Assessor
2100 3rd Ave, Anoka, MN 55303
763-323-5499
763-274-4411 Cell

Anoka County Contract No. _____

**JOINT POWERS AGREEMENT
BETWEEN THE CITY OF RAMSEY AND
COUNTY OF ANOKA FOR ASSESSMENT OF PROPERTY**

THIS AGREEMENT is made and entered into this ____ day of _____, 20__, by and between the City of Ramsey, 7550 Sunwood Drive Northwest, Ramsey, Minnesota 55303, hereinafter referred to as the "Municipality," and the County of Anoka, a political subdivision of the State of Minnesota, 2100 Third Avenue, Anoka, Minnesota 55303, hereinafter referred to as the "County."

WITNESSETH:

WHEREAS, pursuant to Minn. Stat. § 273.072, subd. 1, any county and city, lying wholly or partially within the county and constituting a separate assessment district, may, by agreement entered into under Minn. Stat. § 471.59, provide for the assessment of property in the city by the county assessor; and

WHEREAS, pursuant to Minn. Stat. § 273.072, subd. 2, the agreement may provide for the abolition of the office of the city assessor when the assessment of the property within the city is to be made under an agreement by the county assessor; and

WHEREAS, it is the wish of the County to cooperate with the Municipality to provide for a fair and equitable assessment of the property within the Municipality; and

WHEREAS, said work will be carried out in accordance with the provisions of Minn. Stat. § 471.59.

NOW, THEREFORE, in consideration of the mutual covenants herein contained, it is agreed as follows:

I. That said Municipality constitutes a separate assessment district which lies wholly within the County and that all property in the geographical boundaries of this district shall be assessed by the County Assessor of the County beginning with the assessment year 2014, provided this Agreement is approved by the Commissioner of Revenue.

II. Pursuant to Minn. Stat. § 273.072, subd. 2, the office of the city assessor in the Municipality shall cease to exist on January 16, 2013, but not before the end of the term of the current city assessor, if serving for a fixed term, or when an earlier vacancy occurs; provided, however, that upon the termination of this Agreement, the office of city assessor shall be automatically reestablished and shall be filled as provided by applicable law or charter in accordance with Minn. Stat. § 273.072.

III. The term of this Agreement shall be from January 16, 2013, through assessment services performed in the year 2018, unless earlier terminated as provided herein.

IV. In consideration of said assessment services, the Municipality shall pay to the County an annual payment as follows:

2014 Assessment services payable on or before January 15, 2013

2015 Assessment services payable on or before January 15, 2014

2016 Assessment services payable on or before January 15, 2015

2017 Assessment services payable on or before January 15, 2016

2018 Assessment services payable on or before January 15, 2017

V. For year 2014 assessment services, the Municipality shall pay the County as follows:

A. Twelve Dollars and 50/100s (\$12.50) for each improved parcel of residential, seasonal recreational residential and agricultural type property;

B. Four Dollars and 50/100s (\$4.50) for each unimproved parcel of residential, seasonal recreational residential and agricultural type property;

C. Eighty Dollars and 00/100s (\$80.00) for each improved and unimproved parcel of commercial, industrial and public utility type of property; and

D. Eighty Dollars and 00/100s (\$80.00) for each improved and unimproved parcel of apartment and mobile home park type property.

The same procedure shall be used, as described above, in the computation of the assessment payments due in 2015 through 2018, provided, however, that the cost for each type of parcel shall be increased according to the County Assessor's increase in operating costs, cost of living and merit adjustments established by the Anoka County Board of Commissioners for January 1 to December 31 in the year prior to the date of the assessment.

VI. Notwithstanding Section III. above, the County and/or Municipality shall have the right to terminate this Agreement by providing, at a minimum, two years written notice by certified mail, which notice shall be sent prior to January 2 of any year to terminate assessment services to be performed two years from such date (i.e. to terminate this Agreement for assessment services in the year 2015, the party must provide written notice of termination by not later than January 2, 2013).

IN WITNESS WHEREOF, the parties have hereunder set their hands on the dates written below.

COUNTY OF ANOKA

CITY OF RAMSEY

By: _____
Rhonda Sivarajah, Chair
Anoka County Board of Commissioners

By: _____
Bob Ramsey
Mayor

Dated: _____

Dated: _____

By: _____
Jerry Soma
County Administrator

By: _____
Title: _____

Dated: _____

Dated: _____

APPROVED AS TO FORM

By: _____
Dan Klint
Assistant County Attorney

By: _____
Title: _____

Dated: _____

Dated: _____

The above Agreement is hereby approved by the Commissioner of Revenue this ____ day of _____, _____.

By: _____
Minnesota Commissioner of Revenue

Meeting Date: 11/27/2012

Submitted For: Jo Thieling

By: Jo Thieling, Administrative Services

Information

Title:

Authorize Entering into a Joint Powers Agreement for Conduct of Elections between Anoka County and the Municipalities, Townships and School Districts in Anoka County to Allocate Costs for Election Expenses

Background:

As Council is aware, \$6,000 was approved in the 2013 budget for the "down payment" for purchase of new elections equipment, with the knowledge that if a payment and/or fee plan was determined, a similar amount would need to be budgeted over a period of years. Staff attended several meetings with Anoka County, other Anoka County municipalities, and Anoka-Hennepin School District to discuss purchase options. After extensive review of the funding options, the result of the discussions is a proposal to enter into a Joint Powers Agreement with Anoka County, Anoka County Municipalities and Anoka County School Districts, that establishes Anoka County as the sole owner of the voting equipment system and calls for an annual fee to be paid to the County by cities and school districts to supplement system costs. The City's payments are based on the 2010 census population number.

A memo from Cindy Reichert, Anoka County Elections Manager, is attached to this case. The memo gives much greater detail of the purchase discussions, options and the Joint Powers Agreement (JPA). A draft of the JPA is also attached for Council review. It is expected that the final JPA will be adopted by the Anoka County Board on the morning of November 27. If there are any amendments at all to the JPA, the final will be inserted into the agenda. Two spreadsheets - one showing the elections equipment system history and the other showing the budget, including the breakdown of the cost per City, are attached as well.

Added Language: On Tuesday morning, November 27, 2012, the Anoka County Board approved the Elections JPA with the following motion: Motion to approve Contract #2012-0362, a Joint Powers Agreement between Anoka County and the municipalities, townships, and school districts in Anoka County to allocate costs for election expenses, and authorizing the Management Committee Chair and County Board Chair to approve minor non-substantive contract changes through the approval process of the other governmental entities.

Anoka County Elections recommended that the City Councils approve the contract also allowing for administrative approval of non-substantive changes. Staff is adding that in the motion language below.

Funding Source:

Included in the proposed 2013 General Fund Budget

Council Action:

Motion to authorize entering into a Joint Powers Agreement for Conduct of Elections between Anoka County and the Municipalities, Townships and School Districts in Anoka County to Allocate Costs for Election Expenses and to authorize administrative approval of non-substantive changes to the contract.

Attachments

Memo from Anoka County Elections re JPA

Spreadsheet Elections Equipment Cost History

Spreadsheet Budget and Cost Allocation

Joint Powers Agreement

Form Review

Inbox	Reviewed By	Date
Diana Lund	Diana Lund	11/16/2012 07:48 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:17 AM
Form Started By: Jo Thieling		Started On: 11/15/2012 10:22 AM
	Final Approval Date: 11/21/2012	



ANOKA COUNTY ELECTIONS AND VOTER REGISTRATION

325 East Main Street

Anoka MN 55303

(763) 323-5275

(763) 422-7526 (fax)

elections@co.anoka.mn.us

MEMORANDUM

TO: Anoka County Management Committee
FROM: Cindy Reichert, Anoka County Elections Manager
SUBJECT: Joint Powers Agreement for Conduct of Elections
DATE: November 13, 2012

A voting equipment system is comprised of many components that work together to define ballots, cast and count votes, report, transmit, and display election results, and produce election audit information. The system includes ballot counters, ballot boxes, ADA compliant ballot markers, various software and hardware programs, servers, modems, and other ancillary system components.

The system currently in use in all precincts of Anoka County is the "Accuvote Precinct Optical Scan System" purchased in 2000 from Global Election Systems. That purchase was based on a shared ownership model and required each city in Anoka County to purchase ballot counters and ballot boxes for use in the city voting precincts. The County also purchased several ballot counters to use as back-ups should any of the city-owned equipment need replacement, and has since purchased several reconditioned ballot counters to ensure successful operation of elections through calendar year 2012. The county has continually maintained responsibility for components used county-wide, like system software, servers, and modems.

The time has come to replace the system. The equipment and its software run in a DOS platform that is no longer supported, and new units and parts are no longer available in the marketplace. The most unstable component is our system server which also operates on a DOS platform. That server cannot be replaced and the software that drives the system will not run in a more modern environment. It is imperative that the entire voting equipment system be replaced at the earliest opportunity.

Anoka County has been working toward a complete system replacement scheduled for early 2013. Our staff has collaborated with Election Managers of several other MN Counties, and with the Office of the Secretary of State to determine our needs and options. We've also procured grant funds issued through the Help America Vote Act to partially offset the cost of the replacement. Changes in election law, technology and equipment certification standards have affected not only the technology we use, but the business model of system vendors. As a result, costs have risen considerably.

Our planning has required us to consider many factors and we've worked to engage our partner cities and school districts in discussion. The result is a proposal to enter into a Joint Powers Agreement which addresses not only the equipment purchase itself, but also establishes a model for delivery of election services within our county.

Process for Proposal Development

In 2010 County election staff organized the "ACE Work Group" comprised of city and school district election administrators. Together we performed a systematic examination of the processes, responsibilities and

obligations of each party to Anoka County's election system. We looked at administrative and technical processes, discussed how election services could be delivered in the most logical and cost-effective way, and implemented organizational and procedural improvements in our precincts and respective offices. Throughout the study and planning process the following basic premises were considered by our participants:

- The electoral process is the bedrock of democracy and must be administered with the utmost level of professionalism and integrity
- Government should provide necessary services in an efficient and effective manner at the lowest possible cost
- Solutions developed must ease administrative burden and control the cost of services
- The division of duties between county and local governmental units must be logical and effective
- System security and legal defensibility must be maintained
- Cost-sharing models to support the voting system must be equitable for all parties

We also developed a cost sharing proposal for the next equipment purchase that goes beyond the initial capital purchase and provides on-going support to ensure the future integrity of the system.

Following development of the ACE Work Group proposal, the "Anoka County Voting Equipment Task Force" was convened. This group, comprised of city and school administrators and board members, has met twice in 2012 to review, discuss, and adjust the plan. We are now ready to propose its adoption.

Proposed Multi-Jurisdiction Election Agreement

The ACE Workgroup and Anoka County Voting Equipment Task Force are recommending adoption of an agreement between election jurisdictions in Anoka County that establishes Anoka County as the sole owner of the voting equipment system and calls for an annual fee to be paid to the county by cities and school districts to supplement system costs.

The model used for our last system purchase separated ownership of the system into various pieces and parts, and did not address issues related to the system as a whole such as maintenance, required software and hardware upgrades, and replacement of non-functioning units. Establishing Anoka County as sole owner allows us to more effectively manage the vendor contracts and administrative tasks that apply county-wide.

Rather than transferring ownership of individual components to our cities and schools, the county would collect a fee to supplement system costs. The proposed fee will be based on actual (and defined) capital and operating costs of the voting equipment system with cities paying 30% of the total cost and schools paying 15%, leaving the county with responsibility for 55% of the total cost. Within those percentages, the cost to each individual city and school district will be based upon their population at the time of the 2010 census.

We are also recommending that the agreement include provisions to address other election duties and responsibilities.

Statutes require the Office of the Secretary of State to develop a cost sharing plan for jurisdictions that share the same ballot. That plan allows cities to invoice school districts according to a complex formula for services that are not well defined. Jurisdictions can choose to use either the OSS cost sharing plan or share costs according to a negotiated agreement, *as long as that agreement addresses all expenses included in the OSS plan.*

Our recommendation is to include language in our agreement that addresses each election duty spelled out in the OSS cost sharing plan. Both cities and schools would contribute their share to the county-wide system via

the annual fee outlined above. The contribution of 15% of the total cost of the election system from schools effectively reduces the amount cities would be responsible for from 45% to 30%. In turn, cities would be prohibited from invoicing the schools for the “other election costs” specified in the agreement. This formula recognizes each party’s statutory obligation to share costs, simplifies the fee structure, eliminates the flurry of potentially disparate invoices, and allows all parties to plan for a relatively stable budget expenditure each year.

2013 Voting Equipment Purchase

Instability in the marketplace has led to a crisis in development and manufacturing of voting equipment in the United States. New laws were put into place with the adoption of the Help America Vote Act and a new federal agency, the Election Assistance Commission (EAC), was created to oversee voting equipment certification and distribution of federal grants for voting equipment replacement. That agency proved ineffectual and certification of equipment was at a near stand-still for several years. In the meantime, a series of corporate acquisitions left just a handful of vendors in business, seriously reducing competition.

As a result, Minnesota (which requires all voting equipment to meet federal certification standards) has not certified “next generation” voting equipment. We are currently in a certification application blackout time period that will lift on December 1, 2012. We expect to see applications to our state from at least two voting equipment manufacturers within the next few weeks. Once our choices have been established we must act quickly to make the purchase so that our new system can be fully operational in time for the fall 2013 local elections. Deployment of the system for 2013 local elections is highly desirable so that election administrators, poll workers, and voters can become familiar with the system well in advance of the State General Election of 2014.

Purchase and Agreement Approval Timeline

Timing of the certification and subsequent equipment purchase affects our agreement approval timeline. Though our cities and schools have expressed agreement in concept, specific system costs will not be known until early 2013. In the meantime, detailed estimates have been prepared and forwarded to each city and school district in Anoka County to include in their 2013 budget.

Members of the Anoka County Board of Commissioners have been consulted throughout the process and are supportive of the efforts and recommendation made by the Work Group and Task Force. The County budget has also been structured to incorporate all elements of the equipment purchase and cost-sharing proposal.

The contract itself has been reviewed by Task Force members and received unanimous approval at the Management Committee of the Anoka County Board on Monday, November 13th. Full board approval is expected on November 27th. The Anoka County Elections Manager is scheduled to attend several city/school meetings and work sessions to answer questions and address concerns during the months of November and December and we hope to have approval by all parties to the agreement no later than December 31, 2012.

Attachments: Draft Agreement

Anoka County Contract No. 2012- _____
JOINT POWERS AGREEMENT BETWEEN
ANOKA COUNTY AND THE MUNICIPALITIES, TOWNSHIPS AND SCHOOL DISTRICTS
IN ANOKA COUNTY
TO ALLOCATE COSTS FOR ELECTION EXPENSES

This is a joint Powers Agreement (“JPA”) between the County of Anoka (“County”) and THE MUNICIPALITIES, TOWNSHIPS AND SCHOOL DISTRICTS IN ANOKA COUNTY (“Governmental Entities”) entered into pursuant to Minn. Stat. § 471.59, for the purchase, maintenance and use of election equipment, including conducting elections, by the County on behalf of the County and the Governmental Entities.

Section 1
Term

1. This JPA shall be in effect for a four year term, beginning January 1, 2013 until December 31, 2016, subject to automatic renewal on January 1 of each subsequent calendar year beginning January 1, 2017.

Section 2
Contract Termination

2. During the initial four year term, this JPA may only be terminated by written agreement of the County with the effected Governmental Entity. Beginning January 1, 2017, a Governmental Entity’s participation in this agreement may be terminated by that Governmental Entity providing written notice to the remaining parties no later than June 1 of any year, effective on January 1 of the following year.

Upon termination of the agreement, all right title and interest in any election equipment purchased by the County under the terms of this agreement for use by the Governmental Entity shall remain with the County. Any Governmental Entity withdrawing from this agreement assumes all costs, responsibilities and liabilities related to the purchase, maintenance and use of voting equipment in the conduct of elections in that jurisdiction. Any amounts of the Governmental Entity’s share of the of the initial cost of procurement of the Voting Equipment System and their proportional share of any other costs incurred by the County on their behalf that remain unpaid as of the date of termination shall become immediately due and payable by the Governmental Entity to the County.

Section 3
Voting Equipment System Definition

3. For purposes of this agreement, the Anoka County Voting Equipment System means a system in which the voter records votes by means of marking a ballot, so that votes may be counted by automatic tabulating equipment in the polling place where the ballot is cast or at a counting center. An electronic voting system includes automatic tabulating equipment; non-electronic ballot markers; electronic ballot markers, including electronic ballot display, audio ballot reader, and devices by which the voter will register the voter's voting intent; software used to program automatic tabulators and layout ballots; computer programs used to accumulate precinct results; ballots; secrecy folders; system documentation; and system testing as well as software used to manage the assignment, deployment, chain of custody, and associated logistical operations of said equipment in Anoka County.

Section 4 Applicability

4. This agreement, and the use of the Voting Equipment System defined herein, between the County and the Governmental Entities is applicable for any election at which offices or questions for the following categories are voted on:

Category A: Federal Offices
State Offices or Constitutional Amendments
Judicial Offices
County Offices or Ballot Questions
Soil and Water District Offices or Ballot Questions

Category B: Municipal (Township) Offices or Ballot Questions

Category C: School District Offices or Ballot Questions

Category D: Hospital District Offices or Ballot Questions

Section 5 County Responsibilities

5. Except as otherwise provided in this contract or required by statute or state or federal rule, the County shall be responsible for preparing the specifications for the purchase and maintenance of the Voting Equipment System as defined herein and for the purchase and maintenance of the system, including making all payments and expenditures for capital and on-going operating costs related to the voting equipment system. In addition, for all Category A, B, C and D Elections, Anoka County shall:
- 5.1. Perform voting equipment system programming including ballots, ballot counters, ballot markers, and other components of the voting equipment system used to mark, count, record or report election returns and statistics.
 - 5.2. Perform programming and testing of the State Election Reporting System interface, subject to policies of the State.
 - 5.3. Program and develop a voting equipment testing plan for each election according to statutory requirements.
 - 5.4. Provide ballot design and layout services, and arrange for the printing of ballots to be used in the elections.

Section 6 Governmental Entities' Responsibilities

6. Except as otherwise provided in this contract, each individual Governmental Entity shall be responsible for and shall perform all duties and assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of precinct voting equipment for each election and shall utilize county provided software, as determined necessary by the County, to track the testing, assignment, deployment, chain of custody, and associated logistical operations of said equipment in Anoka County. as follows:

6.1. When Category A and/or B or D offices or questions appear on the ballot:

- 6.1.1. The municipality shall be responsible for and assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of precinct voting equipment for all elections which include a Category A and/or B or D office or question.
- 6.1.2. The municipality shall assume all costs required to arrange for the use of polling places in the manner required by the Minnesota election law, for ensuring the physical set up of rooms and furnishings are conducive to the voting process, and for ensuring that all necessary equipment and supplies are delivered to the polling place for use on Election Day.
- 6.1.3. The municipality shall assume all costs related to picking up ballots, supplies and equipment from the Anoka County Elections and Voter Registration Office in Anoka and other storage locations that may be arranged from time to time, and transporting them to and from the polling place.
- 6.1.4. The municipality shall assume all costs related to issuing, receiving and processing absentee ballots cast by in-person absentee voters in that municipality including procurement and preparation of physical spaces, equipment, and staff needed to administer the process, and costs for delivery of voted ballots to the Anoka County Central Count Absentee Precinct.
- 6.1.5. The municipality shall assume all costs related to recruiting, hiring, and paying Election Judges for all hours served including, training, testing, election day assignments, and any other work assignments associated with the election.

6.2. When only Category C offices or questions appear on the ballot:

- 6.2.1. The School District shall be responsible shall assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of precinct voting equipment for all elections which include only Category C offices or questions.
- 6.2.2. The school district shall assume all costs required to arrange for the use of polling places in the manner required by law, for ensuring the physical set up of rooms and furnishings are conducive to the voting process, and for ensuring that all necessary equipment and supplies are delivered to the polling place for use on Election Day.
- 6.2.3. The school district shall assume all costs related to picking up ballots, supplies and equipment from the Anoka County Elections and Voter Registration Office in Anoka and other storage locations that may be arranged from time to time, and transporting them to and from the polling place.
- 6.2.4. The school district shall assume all costs related to issuing, receiving and processing absentee ballots cast by in-person absentee voters in the school district including procurement and preparation of physical spaces, equipment, and staff needed to administer the process, and costs for delivery of voted ballots to the Anoka County Central Count Absentee Precinct.

- 6.2.5. The school district shall assume all costs related to recruiting, hiring, and paying Election Judges for all hours served including, training, testing, election day assignments, and any other work assignments associated with the election

Section 7 Allocation of Election Expenses

7. Except as already specifically provided for herein, the Voting Equipment System procurement, maintenance and support cost shall be divided between the county, its municipalities, and school districts as follows:
- 7.1. The County shall incur 55% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.
- 7.2. Municipalities located wholly or in part in Anoka County shall, collectively, incur 30% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.
- 7.3. School Districts located wholly or in part in Anoka County shall incur 15% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.
- 7.4. Anoka County shall make all payments and expenditures for capital and on-going operating and maintenance costs related to the system throughout the duration of this contract.
- 7.5. The annual fee for each jurisdiction shall be established as follows:
- 7.5.1. Each individual municipality shall pay a fee equal to that percentage of the total Anoka County population residing in that municipality at the time of the 2010 census multiplied by the municipal share (30%) of the actual cost of procurement (prorated over ten annual installments), plus the actual cost of operation and maintenance of the system, as solely determined by the County, calculated annually throughout the duration of the contract
- 7.5.2. Each individual school district shall pay a fee equal to that percentage of the total Anoka County population residing in that school district at the time of the 2010 census multiplied by the school district share (15%) of the actual cost of procurement (prorated over ten annual installments), plus the actual cost of operation and maintenance of the system, as solely determined by the County, calculated annually throughout the duration of the contract.
- 7.5.3. Each Governmental Entity shall be invoiced annually on June 1 for each calendar year of the agreement for the above referenced fees. Said fees shall be due and payable within thirty (30) calendar days of invoicing.
- 7.5.4. The Governmental Entities hereby agree that they will not reallocate any of the costs incurred herein.
- 7.6. For each governmental entity, determine that proportion of the ballot devoted to offices and questions for that entity as a percentage of the total number of column inches on the ballot, and

provide an invoice to the governmental entity for that share of the cost of ballot printing, paper and normal delivery charges.

- 7.7. Pay the cost of postage for all domestic mailed absentee ballots cast in the county and absentee ballots cast under the Uniformed Overseas Citizens Absentee Voting Act (UOCAVA) except those absentee postage costs incurred by Municipalities designated to administer absentee voting laws under M.S. 203B.05.

Section 8 Documentation of Election Expenses

8. Documentation of actual expenditures as required by the County is required for the allocation of election expenses pursuant to this agreement. Invoices or billing statements are acceptable documentation for goods or services purchased for vendors.

Section 9 Ownership

9. The Governmental Entities acknowledge that the County owns the Voting Equipment System and that the Governmental Entities are authorized to use said Voting Equipment System for official election related purposes. Use of the Voting Equipment System by the Governmental Entities for any other purpose is strictly prohibited absent express written consent of the County. The Governmental Entities hereby acknowledge and agree that the Voting Equipment System may contain proprietary and trade secret information that is owned by a third party and is protected under federal copyright law or other laws, rules, regulations and decisions. The Governmental Entities shall protect and maintain the proprietary and trade secret status of the Voting Equipment System in their possession.

Section 10 Handling Of Equipment and Insurance

10. Each municipality shall be responsible for storage of elections equipment assigned by the county to that municipality. Municipalities shall make all necessary elections equipment in its possession available to other entities as directed by the county.

Each Governmental Entity acknowledges that it shall be responsible for the Voting Equipment System while it is in the Governmental Entity's custody. Each Governmental Entity, either through insurance or a self-insurance program, shall be responsible for all costs, fees, damages and expenses including but not limited to personal injury, storage, damage, repair and/or replacement of the Voting Equipment System while it's in the Governmental Entity's custody and this contract is in effect. The Governmental Entities shall be responsible for, provide coverage for and shall provide proof of general liability and worker's compensating insurance (Hold Harmless Agreement) for all individuals providing services required by this contract. In addition to the foregoing, the Governmental Entities shall, during the term of this contract, maintain, through commercially available insurance or on a self-insured basis, property insurance coverage on all of the voting systems used or intended for use in this agreement to cover all repairs or replacement of the voting equipment if damaged or stolen. The Governmental Entities are responsible for any deductible under their policy.

Section 11 Independent Contractor

11. It is agreed that nothing in this contract is intended or should be construed as creating the relationship of agents, partners, joint ventures, or associates between the parties hereto or as constituting the County or the Governmental Entities as the employee of the other entity for any purpose or in any manner whatsoever. The County is an independent contractor and neither it, its employees, agents, nor its representatives are employees of the Governmental Entities. From any amounts due the County, there shall be no deductions for federal income tax or FICA payments, nor for any state income tax, nor for any other purposes which are associated with an employer-employee relationship unless required by law.

Section 12
Data Practices

12. All data created, collected, received, maintained, or disseminated for any purpose in the course of this contract is governed by the Minnesota Government Data Practices Act, any other applicable statute, or any rules adopted to implement the Act or statute, as well as federal statutes and regulations on data privacy.

Section 13
No Waiver

13. No delay or omission by either party hereto to exercise any right or power occurring upon any noncompliance or default by the other party with respect to any of the terms of this Agreement shall impair any such right or power or be construed to be a waiver thereof unless the same is consented to in writing. A waiver by either of the parties hereto of any of the covenants, conditions, or agreements to be observed by the other shall not be construed to be a waiver of any succeeding breach thereof or of any covenant, condition, or agreement herein contained. All remedies provided for in this Agreement shall be cumulative and in addition to, and not in lieu of, any other remedies available to either party at law, in equity, or otherwise.

Section 14
Governing Law

14. This Agreement shall be governed by and construed in accordance with the laws of the State of Minnesota.

Section 15
Entire Agreement

15. It is understood and agreed that the entire Agreement between the parties is contained herein and that this Agreement supersedes all oral agreements and negotiations between the parties relating to the subject matter hereof and hereby rescinds and replace all prior Agreements with the respective Governmental Entities with this Agreement. All items referred to in this Agreement are incorporated or attached and are deemed to be part of this Agreement. Any alterations, variations, modifications, or waivers of provisions of this Agreement shall only be valid when they have been reduced to writing as an amendment to this Agreement signed by the parties hereto.

Section 16
No Assignment

16. Neither party shall assign, sublet or transfer this Agreement, either in whole or in part, without the prior written consent of the other party, and any attempt to do so shall be void and of no force and effect.

Section 17
No Warranty

17. The Governmental Entities agree that the County is furnishing the Voting Equipment System on an "as is" basis, without representation or any express or implied warranties, other than those provided by any maintenance agreement entered into by the County for the maintenance of the Voting Equipment System, including but not limited to, fitness for particular purpose, merchantability or the accuracy and completeness of the Voting Equipment System.

The Governmental Entity's exclusive remedy and the County's sole liability for any substantial defect which impairs the use of the Voting Equipment System for the purposes stated herein shall be the right to terminate this agreement.

The County does not warrant that the Election Voting Equipment System will be error free.

The County disclaims any other warranties, express or implied, respecting this agreement or the Voting Equipment System.

In no event shall the County be liable for actual, direct, indirect, special, incidental, consequential damages (even if the County has been advised of the possibility of such damage) or loss of profit, loss of business or any other financial loss or any other damage arising out of performance or failure of performance of this Agreement by the County. Except as otherwise specifically provided for in this agreement, County and the Governmental Entities agree each will be responsible for their own acts and omissions under this Agreement and the results thereof and shall to the extent authorized by law defend, indemnify and hold harmless the other party for such acts. Each party shall not be responsible for the acts, errors or omissions of any other party under the Agreement and the results thereof. The parties' respective liabilities shall be governed by the provisions of the Municipal Tort Claims Act, Minnesota Statutes Chapter 466, and other applicable law. This paragraph shall not be construed to bar legal remedies one party may have for the other party's failure to fulfill its obligations under this Agreement. Nothing in this Agreement constitutes a waiver by the Governmental Entities or County of any statutory or common law defenses, immunities, or limits on liability.

Section 18
Notice

18. Any notice or demand shall be in writing and shall be sent registered or certified mail to the other party addressed as follows:

To the Governmental Entity: To the person and address designated by each Governmental Entity in writing.

To the County: Anoka County Administrator
ADDRESS

Copy to: Anoka County Elections Manager
ADDRESS

**Section 19
Audit Provision**

19. Both parties agree that either party, the State Auditor, or any of their duly authorized representatives at any time during normal business hours, and as often as they may reasonably deem necessary, shall have access to and the right to examine, audit, excerpt, and transcribe any books, documents, papers, records, etc., which are pertinent to the accounting practices and procedures of the other party and involve transactions relating to this Agreement. Such materials shall be maintained and such access and rights shall be in force and effect during the period of the contract and for six (6) years after its termination or cancellation.

**Section 20
Survival of Provisions**

20. It is expressly understood and agreed that the obligations and warranties of the Governmental Entity and County hereof shall survive the completion of performance and termination or cancellation of this Agreement.

**Section 21
Whereas Clauses**

21. The matters set forth in the "Whereas" clauses on page one of this Agreement are incorporated into and made a part hereof by this reference.

**Section 22
Authority**

22. The person or persons executing this Lease Agreement on behalf of the Governmental Entity and County represent that they are duly authorized to execute this Lease Agreement on behalf of the Governmental Entity and the County and represent and warrant that this Lease Agreement is a legal, valid and binding obligation and is enforceable in accordance with its terms.

IN WITNESS WHEREOF, the parties hereto have hereunto set their hands.

COUNTY OF ANOKA

By: _____
Rhonda Sivarajah, Chair,
Anoka County Board of Commissioners

Dated: _____

By: _____

Jerry Soma,
Anoka County Administrator

Dated: _____

APPROVED AS TO FORM:

By: _____
Thomas Haluska
Assistant Anoka County Attorney

Dated: _____

CITY OF ***

By: _____
****, Its Mayor

Dated: _____

By: _____
***, Its City Clerk

Dated: _____

***** SCHOOL DISTRICT *****

By: _____
****, Its Superintendent

Dated: _____

By: _____
***, Its ***

Dated: _____

Anoka County Election Equipment System History

09/11/2012

Event		Yr	Grant Capital	Grant Operating	County Capital	County Operating	County Revenue	City Capital	City Operating	Schools Capital	Schools Operating
County-wide Equipment Purchase	Actual	2000	\$ -	\$ -	\$ 70,975	\$ -	\$ -	\$ 543,223	\$ -	\$ -	\$ -
	Actual	2001	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2002	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2003	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2004	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2005	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
AutoMARKS purchased	Actual	2006	\$ 663,570	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2007	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2008	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
	Actual	2009	\$ -	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
AutoMarks covered by maint plan	Actual	2010	\$ -	\$ 25,000	\$ 20,306	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -
6 Reconditioned Accuvotes Purchased											
Modus - County Use	Actual	2011	\$ -	\$ 25,000	\$ -	\$ 49,500	\$ -	\$ -	\$ -	\$ -	\$ -
Modus - ALL jurisdictions Use	Actual	2012	\$ -	\$ 25,000	\$ -	\$ 79,000	\$ -	\$ -	\$ -	\$ -	\$ -
			\$ 663,570	\$ 75,000	\$ 91,281	\$ 248,500	\$ -	\$ 543,223	\$ -	\$ -	\$ -
			GRANT NET	\$ 738,570		COUNTY NET	\$ 339,781	MUNICIPAL NET	\$ 543,223	SCHOOL NET	\$ -

Event		Yr	Grant Capital	Grant Operating	County Capital	County Operating	County Revenue	City Capital	City Operating	Schools Capital	Schools Operating
County-wide Equipment Purchase	Projected	2013	\$ 310,836	\$ 17,000	\$ 871,572	\$ 74,300	\$ 72,656	\$ -	\$ 48,437	\$ -	\$ 24,219
	Projected	2014	\$ -	\$ -	\$ -	\$ 155,460	\$ 109,178	\$ -	\$ 72,785	\$ -	\$ 36,393
	Projected	2015	\$ -	\$ -	\$ -	\$ 158,810	\$ 110,685	\$ -	\$ 73,790	\$ -	\$ 36,895
	Projected	2016	\$ -	\$ -	\$ -	\$ 163,233	\$ 112,676	\$ -	\$ 75,117	\$ -	\$ 37,559
	Projected	2017	\$ -	\$ -	\$ -	\$ 166,751	\$ 114,258	\$ -	\$ 76,172	\$ -	\$ 38,086
	Projected	2018	\$ -	\$ -	\$ -	\$ 171,395	\$ 116,348	\$ -	\$ 77,566	\$ -	\$ 38,783
	Projected	2019	\$ -	\$ -	\$ -	\$ 175,088	\$ 118,010	\$ -	\$ 78,674	\$ -	\$ 39,337
	Projected	2020	\$ -	\$ -	\$ -	\$ 179,964	\$ 120,205	\$ -	\$ 80,136	\$ -	\$ 40,068
	Projected	2021	\$ -	\$ -	\$ -	\$ 183,842	\$ 121,950	\$ -	\$ 81,300	\$ -	\$ 40,650
	Projected	2022	\$ -	\$ -	\$ -	\$ 188,963	\$ 124,254	\$ -	\$ 82,836	\$ -	\$ 41,418
			\$ 310,836	\$ 17,000	\$ 871,572	\$ 1,617,806	\$ 1,120,220	\$ -	\$ 746,813	\$ -	\$ 373,407
			GRANT NET	\$ 327,836		COUNTY NET	\$ 1,369,158	MUNICIPAL NET	\$ 746,813	SCHOOL NET	\$ 373,407

	Actual 2000-2012		Projected 2013-2022	
Grants	\$ 738,570	46%	\$ 327,836	12%
County	\$ 339,781	21%	\$ 1,369,158	49%
Municipalities	\$ 543,223	33%	\$ 746,813	27%
School Districts	\$ -	0%	\$ 373,407	13%
Totals	\$ 1,621,574	100%	\$ 2,817,213	100%

Projected 2013-2022	
After HAVA Funds Used	
\$ 1,369,158	55%
\$ 746,813	30%
\$ 373,407	15%
\$ 2,489,377	100%

VOTING EQUIPMENT SYSTEM - REV AND EXP 29024													
Code	Resp	Type	Title	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
REVENUE													
4301-7430-61251-29024		Rev	Federal Grants - Equip Grant	\$ 310,836									
4301-7430-61251-29010		Rev	Federal Grants - 2006 Grant	\$ 17,000									
Capital Fund		Rev	Refunds and Reimbursements (Op Exp)	\$ 72,656	\$ 109,178	\$ 110,685	\$ 112,676	\$ 114,258	\$ 116,348	\$ 118,010	\$ 120,205	\$ 121,950	\$ 124,254
			TOTAL REVENUES	\$ 400,492	\$ 109,178	\$ 110,685	\$ 112,676	\$ 114,258	\$ 116,348	\$ 118,010	\$ 120,205	\$ 121,950	\$ 124,254
Capital Fund	County	Capital	DS200 Ballot Counters (+ship)	\$ 927,408									
Capital Fund	County	Capital	AutoMARK Ballot Markers	deferred									
Capital Fund	County	Capital	Electronic Poll Books (county expense)	deferred									
Capital Fund	County	Capital	Server Operating System Upgrade	deferred									
Capital Fund	County	Capital	Dedicated System Server	\$ 50,000									
Capital Fund	County	Capital	Central Count Unit (purchase)	\$ 120,000									
Capital Fund	County	Capital	Programming Software	\$ 85,000									
			TOTAL CAPITAL	\$ 1,182,408	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
61251-29024	Govt Partners	Operating	DS200 Hardware Maintenance		\$ 23,625	\$ 23,625	\$ 24,806	\$ 24,806	\$ 26,047	\$ 26,047	\$ 27,349	\$ 27,349	\$ 28,716
61251-29024	Govt Partners	Operating	DS200 Firmware Maintenance		\$ 10,125	\$ 10,125	\$ 10,631	\$ 10,631	\$ 11,163	\$ 11,163	\$ 11,721	\$ 11,721	\$ 12,307
61251-29024	Govt Partners	Operating	DS850 Hardware Maintenance		\$ 4,600	\$ 4,600	\$ 4,830	\$ 4,830	\$ 5,072	\$ 5,072	\$ 5,325	\$ 5,325	\$ 5,591
61251-29010	Govt Partners	Operating	AutoMARK Maintenance	\$ 24,300	\$ 24,300	\$ 24,300	\$ 25,515	\$ 25,515	\$ 26,791	\$ 26,791	\$ 28,130	\$ 28,130	\$ 29,537
61251-29024	Govt Partners	Operating	Electronic Poll Book Maintenance	deferred									
61251-29024	Govt Partners	Operating	Modus Elections Manager	\$ 67,000	\$ 67,000	\$ 70,350	\$ 70,350	\$ 73,868	\$ 73,868	\$ 77,561	\$ 77,561	\$ 81,439	\$ 81,439
61251-29024	Govt Partners	Operating	Electionware License		\$ 25,810	\$ 25,810	\$ 27,101	\$ 27,101	\$ 28,456	\$ 28,456	\$ 29,878	\$ 29,878	\$ 31,372
			TOTAL OPERATING	\$ 74,300	\$ 155,460	\$ 158,810	\$ 163,233	\$ 166,751	\$ 171,395	\$ 175,088	\$ 179,964	\$ 183,842	\$ 188,963
			TOTAL SYSTEM COST 55-30-15 SPLIT										
			Net Capital (after HAVA)	\$ 871,572									
			Operating (10 years)	\$ 1,617,806									
			Total cost	\$ 2,489,377									

			System	System	System	System	System	System	System	System	System	System	System	Total 10 Year	Purchase
City	2010 Census Population	Proportion of Population	Support Fee Year 2013	Support Fee Year 2014	Support Fee Year 2015	Support Fee Year 2016	Support Fee Year 2017	Support Fee Year 2018	Support Fee Year 2019	Support Fee Year 2020	Support Fee Year 2021	Support Fee Year 2022	Contribution	2000	
Andover	30,587	9.25%	\$ 4,478	\$ 6,729	\$ 6,822	\$ 6,945	\$ 7,042	\$ 7,171	\$ 7,273	\$ 7,409	\$ 7,516	\$ 7,658	\$ 69,044	\$ 43,773	
Anoka	17,153	5.18%	\$ 2,511	\$ 3,774	\$ 3,826	\$ 3,895	\$ 3,949	\$ 4,021	\$ 4,079	\$ 4,155	\$ 4,215	\$ 4,295	\$ 38,719	\$ 33,748	
Bethel	488	0.15%	\$ 71	\$ 107	\$ 109	\$ 111	\$ 112	\$ 114	\$ 116	\$ 118	\$ 120	\$ 122	\$ 1,102	\$ -	
Blaine	57,186	17.28%	\$ 8,372	\$ 12,581	\$ 12,755	\$ 12,984	\$ 13,166	\$ 13,407	\$ 13,599	\$ 13,851	\$ 14,053	\$ 14,318	\$ 129,086	\$ 94,665	
Ctrville	3,792	1.15%	\$ 555	\$ 834	\$ 846	\$ 861	\$ 873	\$ 889	\$ 902	\$ 918	\$ 932	\$ 949	\$ 8,560	\$ 4,218	
Cir Pines	4,918	1.49%	\$ 720	\$ 1,082	\$ 1,097	\$ 1,117	\$ 1,132	\$ 1,153	\$ 1,169	\$ 1,191	\$ 1,209	\$ 1,231	\$ 11,101	\$ 8,437	
Col Hgts	19,486	5.89%	\$ 2,853	\$ 4,287	\$ 4,346	\$ 4,424	\$ 4,486	\$ 4,568	\$ 4,634	\$ 4,720	\$ 4,788	\$ 4,879	\$ 43,986	\$ 36,771	
Columbus	3,914	1.18%	\$ 573	\$ 861	\$ 873	\$ 889	\$ 901	\$ 918	\$ 931	\$ 948	\$ 962	\$ 980	\$ 8,835	\$ 4,005	
Coon Rpds	61,476	18.58%	\$ 9,000	\$ 13,525	\$ 13,711	\$ 13,958	\$ 14,154	\$ 14,413	\$ 14,619	\$ 14,891	\$ 15,107	\$ 15,392	\$ 138,770	\$ 109,893	
E Bethel	11,603	3.51%	\$ 1,699	\$ 2,553	\$ 2,588	\$ 2,634	\$ 2,671	\$ 2,720	\$ 2,759	\$ 2,810	\$ 2,851	\$ 2,905	\$ 26,191	\$ 3,637	
Fridley	27,208	8.22%	\$ 3,983	\$ 5,986	\$ 6,068	\$ 6,177	\$ 6,264	\$ 6,379	\$ 6,470	\$ 6,590	\$ 6,686	\$ 6,812	\$ 61,417	\$ 63,064	
Ham Lake	15,296	4.62%	\$ 2,239	\$ 3,365	\$ 3,412	\$ 3,473	\$ 3,522	\$ 3,586	\$ 3,637	\$ 3,705	\$ 3,759	\$ 3,830	\$ 34,528	\$ 27,275	
Hilltop	754	0.23%	\$ 110	\$ 166	\$ 168	\$ 171	\$ 174	\$ 177	\$ 179	\$ 183	\$ 185	\$ 189	\$ 1,702	\$ 4,218	
Lexington	2,049	0.62%	\$ 300	\$ 451	\$ 457	\$ 465	\$ 472	\$ 480	\$ 487	\$ 496	\$ 504	\$ 513	\$ 4,625	\$ 4,218	
Lino Lakes	20,216	6.11%	\$ 2,960	\$ 4,447	\$ 4,509	\$ 4,590	\$ 4,654	\$ 4,740	\$ 4,807	\$ 4,897	\$ 4,968	\$ 5,062	\$ 45,634	\$ 27,274	
Linwood	5,123	1.55%	\$ 750	\$ 1,127	\$ 1,143	\$ 1,163	\$ 1,180	\$ 1,201	\$ 1,218	\$ 1,241	\$ 1,259	\$ 1,283	\$ 11,564	\$ 4,005	
Nowthen	4,443	1.34%	\$ 650	\$ 977	\$ 991	\$ 1,009	\$ 1,023	\$ 1,042	\$ 1,057	\$ 1,076	\$ 1,092	\$ 1,112	\$ 10,029	\$ 4,218	
Oak Grove	8,031	2.43%	\$ 1,176	\$ 1,767	\$ 1,791	\$ 1,823	\$ 1,849	\$ 1,883	\$ 1,910	\$ 1,945	\$ 1,973	\$ 2,011	\$ 18,128	\$ 8,224	
Ramsey	23,668	7.15%	\$ 3,465	\$ 5,207	\$ 5,279	\$ 5,374	\$ 5,449	\$ 5,549	\$ 5,628	\$ 5,733	\$ 5,816	\$ 5,926	\$ 53,426	\$ 36,481	
Sp Lk Pk	6,234	1.88%	\$ 913	\$ 1,371	\$ 1,390	\$ 1,415	\$ 1,435	\$ 1,462	\$ 1,482	\$ 1,510	\$ 1,532	\$ 1,561	\$ 14,072	\$ 15,679	
St. Francis	7,219	2.18%	\$ 1,057	\$ 1,588	\$ 1,610	\$ 1,639	\$ 1,662	\$ 1,692	\$ 1,717	\$ 1,749	\$ 1,774	\$ 1,807	\$ 16,295	\$ 9,419	
TOTAL	330,844	100.00%	\$ 48,437	\$ 72,785	\$ 73,790	\$ 75,117	\$ 76,172	\$ 77,566	\$ 78,674	\$ 80,136	\$ 81,300	\$ 82,836	\$ 746,813	\$ 543,223	

			System	System	System	System	System	System	System	System	System	System	System	Total 10 Year	Purchase
City	2010 Census Population	Proportion of Population	Support Fee Year 2013	Support Fee Year 2014	Support Fee Year 2015	Support Fee Year 2016	Support Fee Year 2017	Support Fee Year 2018	Support Fee Year 2019	Support Fee Year 2020	Support Fee Year 2021	Support Fee Year 2022	Contribution	2000	
Dist 11	177,169	53.55%	\$ 12,969	\$ 19,488	\$ 19,758	\$ 20,113	\$ 20,395	\$ 20,768	\$ 21,065	\$ 21,457	\$ 21,768	\$ 22,180	199,962	\$ -	
Dist 12	32,249	9.75%	\$ 2,361	\$ 3,547	\$ 3,596	\$ 3,661	\$ 3,712	\$ 3,780	\$ 3,834	\$ 3,906	\$ 3,962	\$ 4,037	36,398	\$ -	
Dist 13	25,195	7.62%	\$ 1,844	\$ 2,771	\$ 2,810	\$ 2,860	\$ 2,900	\$ 2,953	\$ 2,996	\$ 3,051	\$ 3,096	\$ 3,154	28,436	\$ -	
Dist 14	15,504	4.69%	\$ 1,135	\$ 1,705	\$ 1,729	\$ 1,760	\$ 1,785	\$ 1,817	\$ 1,843	\$ 1,878	\$ 1,905	\$ 1,941	17,499	\$ -	
Dist 15	29,168	8.82%	\$ 2,135	\$ 3,208	\$ 3,253	\$ 3,311	\$ 3,358	\$ 3,419	\$ 3,468	\$ 3,533	\$ 3,584	\$ 3,652	32,920	\$ -	
Dist 16	30,548	9.23%	\$ 2,236	\$ 3,360	\$ 3,407	\$ 3,468	\$ 3,517	\$ 3,581	\$ 3,632	\$ 3,700	\$ 3,753	\$ 3,824	34,478	\$ -	
Dist 728	4057	1.23%	\$ 297	\$ 446	\$ 452	\$ 461	\$ 467	\$ 476	\$ 482	\$ 491	\$ 498	\$ 508	4,579	\$ -	
Dist 624	1722	0.52%	\$ 126	\$ 189	\$ 192	\$ 195	\$ 198	\$ 202	\$ 205	\$ 209	\$ 212	\$ 216	1,944	\$ -	
Dist 831	15,232	4.60%	\$ 1,115	\$ 1,676	\$ 1,699	\$ 1,729	\$ 1,753	\$ 1,786	\$ 1,811	\$ 1,845	\$ 1,872	\$ 1,907	17,192	\$ -	
Total	330,844	100.00%	\$ 24,219	\$ 36,393	\$ 36,895	\$ 37,559	\$ 38,086	\$ 38,783	\$ 39,337	\$ 40,068	\$ 40,650	\$ 41,418	\$ 373,407	\$ -	

Anoka County Contract No. 2012- _____
JOINT POWERS AGREEMENT BETWEEN
ANOKA COUNTY AND THE MUNICIPALITIES, TOWNSHIPS AND SCHOOL DISTRICTS
IN ANOKA COUNTY
TO ALLOCATE COSTS FOR ELECTION EXPENSES

This is a joint Powers Agreement (“JPA”) between the County of Anoka (“County”) and THE MUNICIPALITIES, TOWNSHIPS AND SCHOOL DISTRICTS IN ANOKA COUNTY (“Governmental Entities”) entered into pursuant to Minn. Stat. § 471.59, for the purchase, maintenance and use of election equipment, including conducting elections, by the County on behalf of the County and the Governmental Entities.

Section 1
Term

1. This JPA shall be in effect for a four year term, beginning January 1, 2013 until December 31, 2016, subject to automatic renewal on January 1 of each subsequent calendar year beginning January 1, 2017.

Section 2
Contract Termination

2. During the initial four year term, this JPA may only be terminated by written agreement of the County with the effected Governmental Entity. Beginning January 1, 2017, a Governmental Entity’s participation in this agreement may be terminated by that Governmental Entity providing written notice to the remaining parties no later than June 1 of any year, effective on January 1 of the following year.

Upon termination of the agreement, all right title and interest in any election equipment purchased by the County under the terms of this agreement for use by the Governmental Entity shall remain with the County. Any Governmental Entity withdrawing from this agreement assumes all costs, responsibilities and liabilities related to the purchase, maintenance and use of voting equipment in the conduct of elections in that jurisdiction. Any amounts of the Governmental Entity’s share of the of the initial cost of procurement of the Voting Equipment System and their proportional share of any other costs incurred by the County on their behalf that remain unpaid as of the date of termination shall become immediately due and payable by the Governmental Entity to the County.

Section 3
Voting Equipment System Definition

3. For purposes of this agreement, the Anoka County Voting Equipment System means a system in which the voter records votes by means of marking a ballot, so that votes may be counted by automatic tabulating equipment in the polling place where the ballot is cast or at a counting center. An electronic voting system includes automatic tabulating equipment; non-electronic ballot markers; electronic ballot markers, including electronic ballot display, audio ballot reader, and devices by which the voter will register the voter's voting intent; software used to program automatic tabulators and layout ballots; computer programs used to accumulate precinct results; ballots; system documentation; and system testing as well as software used to manage the

assignment, deployment, chain of custody, and associated logistical operations of said equipment in Anoka County.

Section 4 Applicability

4. This agreement, and the use of the Voting Equipment System defined herein, between the County and the Governmental Entities is applicable for any election at which offices or questions for the following categories are voted on:

Category A: Federal Offices
State Offices or Constitutional Amendments
Judicial Offices
County Offices or Ballot Questions
Soil and Water District Offices or Ballot Questions

Category B: Municipal (Township) Offices or Ballot Questions

Category C: School District Offices or Ballot Questions

Category D: Hospital District Offices or Ballot Questions

Section 5 County Responsibilities

5. Except as otherwise provided in this contract or required by statute or state or federal rule, the County shall be responsible for preparing the specifications for the purchase and maintenance of the Voting Equipment System as defined herein and for the purchase and maintenance of the system, including making all payments and expenditures for capital and on-going operating costs related to the voting equipment system. In addition, for all Category A, B, C and D Elections, Anoka County shall:
 - 5.1. Perform voting equipment system programming including ballots, ballot counters, ballot markers, and other components of the voting equipment system used to mark, count, record or report election returns and statistics.
 - 5.2. Perform programming and testing of the State Election Reporting System interface, subject to policies of the State.
 - 5.3. Program and develop a voting equipment testing plan for each election according to statutory requirements.
 - 5.4. Provide ballot design and layout services, and arrange for the printing of ballots to be used in the elections.

Section 6 Governmental Entities' Responsibilities

6. Except as otherwise provided in this contract, each individual Governmental Entity shall be responsible for and shall perform all duties and assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of

precinct voting equipment for each election and shall utilize county provided software, as determined necessary by the County, to track the testing, assignment, deployment, chain of custody, and associated logistical operations of said equipment in Anoka County, as follows:

6.1. When Category A and/or B or D offices or questions appear on the ballot:

- 6.1.1. The municipality shall be responsible for and assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of precinct voting equipment for all elections which include a Category A and/or B or D office or question.
- 6.1.2. The municipality shall assume all costs required to arrange for the use of polling places in the manner required by the Minnesota election law, for ensuring the physical set up of rooms and furnishings are conducive to the voting process, and for ensuring that all necessary equipment and supplies are delivered to the polling place for use on Election Day.
- 6.1.3. The municipality shall assume all costs related to picking up ballots, supplies and equipment from the Anoka County Elections and Voter Registration Office in Anoka and other storage locations that may be arranged from time to time, and transporting them to and from the polling place.
- 6.1.4. The municipality shall assume all costs related to issuing, receiving and processing absentee ballots cast by in-person absentee voters in that municipality including procurement and preparation of physical spaces, equipment, and staff needed to administer the process, and costs for delivery of voted ballots to the Anoka County Central Count Absentee Precinct.
- 6.1.5. The municipality shall assume all costs related to recruiting, hiring, and paying Election Judges for all hours served including, training, testing, election day assignments, and any other work assignments associated with the election.

6.2. When only Category C offices or questions appear on the ballot:

- 6.2.1. The School District shall be responsible and shall assume all costs associated with the production of test decks, and conduct of pre-election and post-election tests and audits of precinct voting equipment for all elections which include only Category C offices or questions.
- 6.2.2. The school district shall assume all costs required to arrange for the use of polling places in the manner required by law, for ensuring the physical set up of rooms and furnishings are conducive to the voting process, and for ensuring that all necessary equipment and supplies are delivered to the polling place for use on Election Day.
- 6.2.3. The school district shall assume all costs related to picking up ballots, supplies and equipment from the Anoka County Elections and Voter Registration Office in Anoka and other storage locations that may be arranged from time to time, and transporting them to and from the polling place.

6.2.4. The school district shall assume all costs related to issuing, receiving and processing absentee ballots cast by in-person absentee voters in the school district including procurement and preparation of physical spaces, equipment, and staff needed to administer the process, and costs for delivery of voted ballots to the Anoka County Central Count Absentee Precinct.

6.2.5. The school district shall assume all costs related to recruiting, hiring, and paying Election Judges for all hours served including, training, testing, election day assignments, and any other work assignments associated with the election.

Section 7 Allocation of Election Expenses

7. Except as already specifically provided for herein, the Voting Equipment System procurement, maintenance and support cost shall be divided between the county, its municipalities, and school districts as follows:

7.1. The County shall incur 55% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.

7.2. Municipalities located wholly or in part in Anoka County shall, collectively, incur 30% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.

7.3. School Districts located wholly or in part in Anoka County shall incur 15% of the actual cost of procurement, operation and maintenance of the system over the duration of this contract.

7.4. Anoka County shall make all payments and expenditures for capital and on-going operating and maintenance costs related to the system throughout the duration of this contract.

7.5. The annual fee for each jurisdiction shall be established as follows:

7.5.1. Each individual municipality shall pay a fee equal to that percentage of the total Anoka County population residing in that municipality at the time of the 2010 census multiplied by the municipal share (30%) of the actual cost of procurement (prorated over ten annual installments), plus the actual cost of operation and maintenance of the system, as solely determined by the County, calculated annually throughout the duration of the contract

7.5.2. Each individual school district shall pay a fee equal to that percentage of the total Anoka County population residing in that school district at the time of the 2010 census multiplied by the school district share (15%) of the actual cost of procurement (prorated over ten annual installments), plus the actual cost of operation and maintenance of the system, as solely determined by the County, calculated annually throughout the duration of the contract.

7.5.3. Each Governmental Entity shall be invoiced annually on June 1 for each calendar year of the agreement for the above referenced fees. Said fees shall be due and payable within thirty (30) calendar days of invoicing.

7.5.4. The Governmental Entities hereby agree that they will not reallocate any of the costs incurred herein.

7.6. For each governmental entity, the County shall determine that proportion of the ballot devoted to offices and questions for that entity as a percentage of the total number of column inches on the ballot, and provide an invoice to the governmental entity for that share of the cost of ballot printing, paper and normal delivery charges.

7.7. The County shall pay the cost of postage for all domestic mailed absentee ballots cast in the county and absentee ballots cast under the Uniformed Overseas Citizens Absentee Voting Act (UOCAVA) except those absentee postage costs incurred by Municipalities designated to administer absentee voting laws under M.S. 203B.05.

Section 8 Documentation of Election Expenses

8. Documentation of actual expenditures as required by the County is required for the allocation of election expenses pursuant to this agreement. Invoices or billing statements are acceptable documentation for goods or services purchased for vendors.

Section 9 Ownership

9. The Governmental Entities acknowledge that the County owns the Voting Equipment System and that the Governmental Entities are authorized to use said Voting Equipment System for official election related purposes. Use of the Voting Equipment System by the Governmental Entities for any other purpose is strictly prohibited absent express written consent of the County. The Governmental Entities hereby acknowledge and agree that the Voting Equipment System may contain proprietary and trade secret information that is owned by a third party and is protected under federal copyright law or other laws, rules, regulations and decisions. The Governmental Entities shall protect and maintain the proprietary and trade secret status of the Voting Equipment System in their possession.

Section 10 Handling Of Equipment and Insurance

10. Each municipality shall be responsible for storage of elections equipment assigned by the county to that municipality. Municipalities shall make all necessary elections equipment in its possession available to other entities as directed by the county.

Each Governmental Entity acknowledges that it shall be responsible for the Voting Equipment System while it is in the Governmental Entity's custody. Each Governmental Entity, either through insurance or a self-insurance program, shall be responsible for all costs, fees, damages and expenses including but not limited to personal injury, storage, damage, repair and/or replacement of the Voting Equipment System while it's in the

Governmental Entity's custody and this contract is in effect unless such costs, fees, damages and expenses are then currently covered under a manufacturer warranty covering said equipment. The Governmental Entities shall be responsible for, provide coverage for and shall provide proof of general liability and worker's compensating insurance (Hold Harmless Agreement) for all individuals providing services required by this contract. In addition to the foregoing, the Governmental Entities shall, during the term of this contract, maintain, through commercially available insurance or on a self-insured basis, property insurance coverage on all of the voting systems used or intended for use in this agreement to cover all repairs or replacement of the voting equipment if damaged or stolen. The Governmental Entities are responsible for any deductible under their policy.

Section 11 Independent Contractor

11. It is agreed that nothing in this contract is intended or should be construed as creating the relationship of agents, partners, joint ventures, or associates between the parties hereto or as constituting the County or the Governmental Entities as the employee of the other entity for any purpose or in any manner whatsoever. The County is an independent contractor and neither it, its employees, agents, nor its representatives are employees of the Governmental Entities. From any amounts due the County, there shall be no deductions for federal income tax or FICA payments, nor for any state income tax, nor for any other purposes which are associated with an employer-employee relationship unless required by law.

Section 12 Data Practices

12. All data created, collected, received, maintained, or disseminated for any purpose in the course of this contract is governed by the Minnesota Government Data Practices Act, any other applicable statute, or any rules adopted to implement the Act or statute, as well as federal statutes and regulations on data privacy.

Section 13 No Waiver

13. No delay or omission by either party hereto to exercise any right or power occurring upon any noncompliance or default by the other party with respect to any of the terms of this Agreement shall impair any such right or power or be construed to be a waiver thereof unless the same is consented to in writing. A waiver by either of the parties hereto of any of the covenants, conditions, or agreements to be observed by the other shall not be construed to be a waiver of any succeeding breach thereof or of any covenant, condition, or agreement herein contained. All remedies provided for in this Agreement shall be cumulative and in addition to, and not in lieu of, any other remedies available to either party at law, in equity, or otherwise.

Section 14 Governing Law

14. This Agreement shall be governed by and construed in accordance with the laws of the State of Minnesota.

Section 15
Entire Agreement

15. It is understood and agreed that the entire Agreement between the parties is contained herein and that this Agreement supersedes all oral agreements and negotiations between the parties relating to the subject matter hereof and hereby rescinds and replace all prior Agreements with the respective Governmental Entities with this Agreement. All items referred to in this Agreement are incorporated or attached and are deemed to be part of this Agreement. Any alterations, variations, modifications, or waivers of provisions of this Agreement shall only be valid when they have been reduced to writing as an amendment to this Agreement signed by the parties hereto.

Section 16
No Assignment

16. Neither party shall assign, sublet or transfer this Agreement, either in whole or in part, without the prior written consent of the other party, and any attempt to do so shall be void and of no force and effect.

Section 17
No Warranty

17. The Governmental Entities agree that the County is furnishing the Voting Equipment System on an "as is" basis, without representation or any express or implied warranties, other than those provided by any maintenance agreement entered into by the County for the maintenance of the Voting Equipment System, including but not limited to, fitness for particular purpose, merchantability or the accuracy and completeness of the Voting Equipment System.

The Governmental Entity's exclusive remedy and the County's sole liability for any substantial defect which impairs the use of the Voting Equipment System for the purposes stated herein shall be the right to terminate this agreement.

The County does not warrant that the Election Voting Equipment System will be error free.

The County disclaims any other warranties, express or implied, respecting this agreement or the Voting Equipment System.

In no event shall the County be liable for actual, direct, indirect, special, incidental, consequential damages (even if the County has been advised of the possibility of such damage) or loss of profit, loss of business or any other financial loss or any other damage arising out of performance or failure of performance of this Agreement by the County. Except as otherwise specifically provided for in this agreement, County and the Governmental Entities agree each will be responsible for their own acts and omissions under this Agreement and the results thereof and shall to the extent authorized by law defend, indemnify and hold harmless the other party for such acts. Each party shall not be responsible for the acts, errors or omissions of any other party under the Agreement and the results thereof. The parties' respective liabilities shall be governed by the provisions of the Municipal Tort Claims Act, Minnesota Statutes Chapter 466, and other applicable law. This paragraph shall not be construed to bar legal remedies one party may have for the other party's failure to fulfill its obligations under this Agreement. Nothing in this Agreement

constitutes a waiver by the Governmental Entities or County of any statutory or common law defenses, immunities, or limits on liability.

**Section 18
Notice**

18. Any notice or demand shall be in writing and shall be sent registered or certified mail to the other party addressed as follows:

To the Governmental Entity: To the person and address designated by each Governmental Entity in writing.

To the County: Anoka County Administrator
ADDRESS

Copy to: Anoka County Elections Manager
ADDRESS

**Section 19
Audit Provision**

19. Both parties agree that either party, the State Auditor, or any of their duly authorized representatives at any time during normal business hours, and as often as they may reasonably deem necessary, shall have access to and the right to examine, audit, excerpt, and transcribe any books, documents, papers, records, etc., which are pertinent to the accounting practices and procedures of the other party and involve transactions relating to this Agreement. Such materials shall be maintained and such access and rights shall be in force and effect during the period of the contract and for six (6) years after its termination or cancellation.

**Section 20
Survival of Provisions**

20. It is expressly understood and agreed that the obligations and warranties of the Governmental Entity and County hereof shall survive the completion of performance and termination or cancellation of this Agreement.

**Section 21
Authority**

21. The person or persons executing this Joint Powers Agreement on behalf of the Governmental Entity and County represent that they are duly authorized to execute this Joint Powers Agreement on behalf of the Governmental Entity and the County and represent and warrant that this Joint Powers Agreement is a legal, valid and binding obligation and is enforceable in accordance with its terms.

(Rest of page left intentionally blank)

IN WITNESS WHEREOF, the parties hereto have hereunto set their hands.

COUNTY OF ANOKA

By: _____
Rhonda Sivarajah, Chair,
Anoka County Board of Commissioners

Dated: _____

By: _____
Jerry Soma,
Anoka County Administrator

Dated: _____

APPROVED AS TO FORM:

By: _____
Thomas Haluska
Assistant Anoka County Attorney

Dated: _____

CITY OF ***

By: _____
****, Its Mayor

Dated: _____

By: _____
***, Its City Clerk

Dated: _____

***** SCHOOL DISTRICT *****

By: _____
****, Its Superintendent

Dated: _____

By: _____
***, Its ***

Dated: _____

CC Regular Session

4. 9.

Meeting Date: 11/27/2012

By: Jackie Lipski, Finance

Information

Title:

Adopt Resolution #12-11-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of November 8, 2012 through November 20, 2012

Funding Source:

N/A

Council Action:

Motion to Adopt Resolution #12-11-XXX Approving Cash Disbursements Made and Authorizing Payment of Accounts Payable Invoicing Received During the Period of November 8, 2012 through November 20, 2012.

Attachments

Bills List 11/27/2012

Resolution 11/27/2012

Form Review

Inbox	Reviewed By	Date
Diana Lund	Diana Lund	11/21/2012 08:37 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:44 AM
Form Started By: Jackie Lipski		Started On: 11/20/2012 02:58 PM
	Final Approval Date: 11/21/2012	

RAMSEY CITY COUNCIL MEETING
11/27/2012
BILLS LIST

DISBURSEMENTS TO BE APPROVED THIS MEETING:

DISBURSEMENT TYPE:	<u>SUBMITTED FOR APPROVAL</u>
Purchase Journal:	
Prepays 11/8/12-11/20/12	2,008,571.12
Accounts Payable 11/8/12-11/20/12	229,969.41
Payroll 11/8/12	124,292.05
Pay Estimates- Projects	746,472.27

TOTAL SUBMITTED FOR APPROVAL THIS MEETING

\$ 3,109,304.85

<u>DISBURSEMENTS PREVIOUSLY APPROVED AND PAID:</u>	<u>APPROVED PREV. MTG</u>	<u>2012 Y.T.D.</u>
NET PAYROLL TOTAL	\$ 157,713.03	\$ 2,602,892.31
- CORRECTION TO PAYROLL		
PREPAIDS	145,545.09	7,520,724.89
- PREPAID ADJUSTMENTS		
WIRE TRANSFERS FOR DEBT SERVICE		665,030.83
- CORRECTION TO D.S.		
ACCOUNTS PAYABLE INVOICING - PREVIOUS MEETING:		
- BILLS LIST SUBMITTED	279,457.66	5,254,863.43
ADD (DELETE) BILLS LIST SUBMITTED		
PAY ESTIMATE(S)		4,027,233.01
- CHECKS VOIDED	0.00	0.00

TOTAL CASH DISBURSEMENTS PREVIOUSLY APPROVED

\$ 582,715.78

\$ 20,070,744.47

CITY OF RAMSEY

Council Check Register

11/8/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
89734	11/8/2012		100948 ANOKA COUNTY LICENSE CENTER						
		875.62	REG/TITLE/PLATES FOR 376		64968	110512	0230.6249	00000376	MISCELLANEOUS OPERATING
		<u>875.62</u>							
89735	11/8/2012		110734 CITY OF RAMSEY						
		2.01	ACCT # 69045916		64971	110812	9601.4651		WATER REVENUE
		20.00	ACCT # 44943511		64971	110812	9601.4651		WATER REVENUE
		21.00	ACCT # 55994616		64971	110812	9601.4651		WATER REVENUE
		21.53	ACCT # 711708649		64971	110812	9601.4651		WATER REVENUE
		21.67	ACCT # 56054622		64971	110812	9601.4651		WATER REVENUE
		31.00	ACCT # 718833		64971	110812	9601.4651		WATER REVENUE
		39.54	ACCT # 48903907		64971	110812	9601.4651		WATER REVENUE
		43.21	ACCT # 678110935		64971	110812	9601.4651		WATER REVENUE
		43.52	ACCT # 51094126		64971	110812	9601.4651		WATER REVENUE
		85.00	ACCT # 60725089		64971	110812	9601.4651		WATER REVENUE
		100.00	ACCT # 704554054		64971	110812	9601.4651		WATER REVENUE
		113.26	ACCT 494777006		64971	110812	9601.4651		WATER REVENUE
		124.00	ACCT # 718070404		64971	110812	9601.4651		WATER REVENUE
		132.86	ACCT # 689292819		64971	110812	9601.4651		WATER REVENUE
		133.00	ACCT # 720532		64971	110812	9601.4651		WATER REVENUE
		133.00	ACCT # 71892199		64971	110812	9601.4651		WATER REVENUE
		188.16	ACCT # 720802		64971	110812	9601.4651		WATER REVENUE
		305.10	ACCT # 383602534		64971	110812	9601.4651		WATER REVENUE
		379.40	ACCT # 720257		64971	110812	9601.4651		WATER REVENUE
		600.00	ACCT # 655531873		64971	110812	9601.4651		WATER REVENUE
		1,000.00	ACCT # 671198103		64971	110812	9601.4651		WATER REVENUE
		1,170.00	ACCT # 640542237		64971	110812	9601.4651		WATER REVENUE
		<u>4,707.26</u>							
89736	11/8/2012		105628 WELLS CATERING SERVICE						
		123.85	CC MTG 7/10/2012		64970	26660	0111.6249		MISCELLANEOUS OPERATING
		<u>123.85</u>							
89737	11/9/2012		107962 GENESIS EMPLOYEE BENEFITS						
		3,883.50			64956	11071216002510	9101.2176		LIFE/HEALTH-EMPLOYEE
		<u>3,883.50</u>							
89738	11/13/2012		110734 CITY OF RAMSEY						
		5,018.25	14779 ZEOLITE ST- NORTH COMMON		64972	722164 3RD QTR 2012	0452.6372		WATER/IRRIGATION
		19.77	ACCT 65285539		64973	111212	9601.4651		WATER REVENUE
		20.00	ACCT 398002818		64973	111212	9601.4651		WATER REVENUE
		28.78	ACCT 722400		64973	111212	9601.4651		WATER REVENUE
		34.64	ACCT 718954		64973	111212	9601.4651		WATER REVENUE
		38.61	ACCT 502484		64973	111212	9601.4651		WATER REVENUE
		107.64	ACCT 68975909		64973	111212	9601.4651		WATER REVENUE
		300.00	ACCT 718954		64973	111212	9601.4651		WATER REVENUE
		610.00	ACCT 719231		64973	111212	9601.4651		WATER REVENUE
		<u>6,177.69</u>							
89739	11/13/2012		113357 SANDQUIST, ANGELA						
		100.00	DAM DEP REFUND RC # 31381		64974	111312	9804.1160		KEY & DAMAGE DEPOSIT
		<u>100.00</u>							
89740	11/13/2012		105628 WELLS CATERING SERVICE						
		414.00	EDA FALL EVENT- DEPOSIT 1/2 PY		64975	26944	9230.6249		MISCELLANEOUS OPERATING
		<u>414.00</u>							

CITY OF RAMSEY
Council Check Register

11/8/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
89835	11/15/2012		100404 CENTURYLINK						
		310.52	NOV 2012 SERVICE		64978	612 E34-0544	0192.6321		TELEPHONE
						018 NOV 12			
		310.52	NOV 2012 SERVICE		64979	612 E34-0549	0192.6321		TELEPHONE
						596 NOV 12			
		310.52	NOV 12 SERVICES		64980	612 E34-0550	0192.6321		TELEPHONE
						637 NOV12			
		<u>931.56</u>							
89836	11/15/2012		100678 PETTY CASH						
		5.00	J WAY ZIPPER REPAIR		64981	111412	0211.6231		UNIFORMS & TURN-OUT GEAR
		19.29	N. MARAS TRAINING		64981	111412	0311.6335		TRAINING
		26.64	LAASEBY -MILEAGE PLEAA MTG		64981	111412	0211.6334		MILEAGE REIMBURSEMENT
		<u>50.93</u>							
89837	11/20/2012		100012 ACE SOLID WASTE INC						
		27.19	MISC CITY TRASH ACCOUNTS		65108	0009898960	9601.6374		REFUSE/RECYCLING
		27.19	MISC CITY TRASH ACCOUNTS		65108	0009898960	9602.6374		REFUSE/RECYCLING
		27.20	MISC CITY TRASH ACCOUNTS		65108	0009898960	9605.6374		REFUSE/RECYCLING
		30.76	MISC CITY TRASH ACCOUNTS		65108	0009898960	0220.6374		REFUSE/RECYCLING
		81.58	MISC CITY TRASH ACCOUNTS		65108	0009898960	0311.6374		REFUSE/RECYCLING
		154.49	MISC CITY TRASH ACCOUNTS		65108	0009898960	0194.6374		REFUSE/RECYCLING
		228.68	MISC CITY TRASH ACCOUNTS		65108	0009898960	0452.6374		REFUSE/RECYCLING
		26.75	FD TRASH		65109	0009899030	0220.6374		REFUSE/RECYCLING
		<u>603.84</u>							
89838	11/20/2012		100056 ANOKA, CITY OF						
		13,230.25	50% ENG. WORK MISS TRAIL PROJ.		65121	201211092041	0452.6315		MISCELLANEOUS PROFESSIO
		<u>13,230.25</u>							
89839	11/20/2012		100113 BANK OF THE WEST						
		100.00	TO OPEN ACCOUNT		65110	111912	9101.1010		CASH IN BANK
		<u>100.00</u>							
89840	11/20/2012		106564 BLUE CROSS BLUE SHIELD						
		46,670.00	DEC 2012 HEALTH INS		65111	111612	9101.2176		LIFE/HEALTH-EMPLOYEE
		<u>46,670.00</u>							
89841	11/20/2012		110734 CITY OF RAMSEY						
		14.51	722449		65122	112012	9601.4651		WATER REVENUE
		19.77	55244541		65122	112012	9601.4651		WATER REVENUE
		19.77	40323049		65122	112012	9601.4651		WATER REVENUE
		20.00	628051531		65122	112012	9601.4651		WATER REVENUE
		20.00	52894306		65122	112012	9601.4651		WATER REVENUE
		132.86	485784521		65122	112012	9601.4651		WATER REVENUE
		155.09	484615404		65122	112012	9601.4651		WATER REVENUE
		162.66	721662		65122	112012	9601.4651		WATER REVENUE
		200.00	663106169		65122	112012	9601.4651		WATER REVENUE
		289.90	721584		65122	112012	9601.4651		WATER REVENUE
		401.94	700617252		65122	112012	9601.4651		WATER REVENUE
		<u>1,436.50</u>							
89842	11/20/2012		100116 CONNEXUS ENERGY						
		39.54	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	9410.6371	00041018	ELECTRIC UTILITIES
						T12			
		92.84	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	9230.6371		ELECTRIC UTILITIES

CITY OF RAMSEY
Council Check Register

11/8/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
						T12			
		101.79	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	0295.6371		ELECTRIC UTILITIES
						T12			
		152.67	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	0194.6371		ELECTRIC UTILITIES
						T12			
		602.56	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	9410.6371	00041012	ELECTRIC UTILITIES
						T12			
		1,308.31	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	0220.6371		ELECTRIC UTILITIES
						T12			
		3,241.42	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	9240.6371		ELECTRIC UTILITIES
						T12			
		7,633.69	OCT 12 MISC CITY ACCOUNTS		65112	759126-303107OC	0194.6371		ELECTRIC UTILITIES
						T12			
		13,172.82							
89843	11/20/2012		100291 MET COUNCIL SAC						
		1,016.95-	OCT 12 SAC CHARGES		65113	111612	9602.4356		SEWER AVAILABILITY CHARGE
		101,695.00	OCT 12 SAC CHARGES		65113	111612	9602.2083		SAC CHARGES
		100,678.05							
89844	11/20/2012		100413 RANDALL AND GOODRICH, P L C						
		2,893.53	OCT/NOV 2012 PROSECUTION BILL		65114	111512	0161.6304		LEGAL FEES
		2,893.53							
89845	11/20/2012		113360 STERNE AGEE AND LEACH INC						
		.42-	ACCT 6355-4042 CITY OF RAMSEY		65115	111512	9701.4701		INTEREST ON INVESTMENTS
		23.01	ACCT 6355-4042 CITY OF RAMSEY		65115	111512	9701.4701		INTEREST ON INVESTMENTS
		200,000.00	ACCT 6355-4042 CITY OF RAMSEY		65115	111512	9701.1038		JURAN & MOODY - INVESTMEI
		200,022.59							
89846	11/20/2012		113191 UBS FINANCIAL SERVICES INC						
		500.00-	FHLB 313381/AC9/BT1/BD6		65116	110712	9701.4701		INTEREST ON INVESTMENTS
		500,000.00	FHLB 313381/AC9/BT1/BD6		65116	110712	9701.1035		PRUDENTIAL - INVESTMENTS
		500,000.00	FHLB 313381/AC9/BT1/BD6		65116	110712	9701.1035		PRUDENTIAL - INVESTMENTS
		500,000.00	FHLB 313381/AC9/BT1/BD6		65116	110712	9701.1035		PRUDENTIAL - INVESTMENTS
		1,499,500.00							
89847	11/20/2012		100290 USA MOBILITY INC						
		6.36	NOV 2012 BILLING		65117	V0317755K	0311.6323		CELLULAR PHONES
		168.87	NOV 2012 BILLING		65117	V0317755K	0220.6249		MISCELLANEOUS OPERATING
		175.23							
89848	11/20/2012		111137 WRIGHT HENNEPIN COOPERATIVE EL						
		26.67	7550 SUNWOOD DR		65118	150-1681-6340	0194.6489		OTHER CONTRACTED SERVIC
						OCT 12			
		26.67	7650 SUNWOOD DR		65119	150-1681-4280	9240.6315		MISCELLANEOUS PROFESSIO
						OCT 12			
		29.87	7550 SUNWOOD DR PD		65120	150-1682-6501	0211.6489		OTHER CONTRACTED SERVIC
						OCT 12			
		83.21							
90229316	11/8/2012		100398 PUBLIC EMPLOYEES RETIREMENT AS						
		12,536.54			64962	1107121600254	9101.2174		PERA-EMPLOYEE
		16,761.22			64963	1107121600255	9101.2183		PERA-EMPLOYER
		29,297.76							
92406912	11/8/2012		100601 MN DEPT OF REV WH						

CITY OF RAMSEY
Council Check Register

11/8/2012 -- 12/31/2012

Check #	Date	Amount	Supplier / Explanation	PO #	Doc No	Inv No	Account No	Subledger	Account Description
		7,306.60			64964	1107121600256	9101.2172		STATE WITHHOLDING
		<u>7,306.60</u>							
95121956	11/8/2012		107784 VILLAGE BANK						
		17,456.63			64965	1107121600257	9101.2171		FEDERAL WITHHOLDING
		6,035.04			64966	1107121600258	9101.2173		FICA & MEDICARE-EMPLOYEE
		<u>7,828.60</u>			64967	1107121600259	9101.2182		FICA & MEDICARE-EMPLOYER
		<u>31,320.27</u>							
99103112	11/16/2012		100219 HOME DEPOT COMMERCIAL ACCT PRO						
		35.92	OCT 2012 BILLING		65033	102812	0194.6259		BUILDING MAINT/REPAIR SUPI
		107.74	OCT 2012 BILLING		65033	102812	0194.6381		BUILDING & STRUCTURE REP/
		<u>307.00</u>	OCT 2012 BILLING		65033	102812	0452.6249		MISCELLANEOUS OPERATING
		<u>450.66</u>							
99110851	11/8/2012		111465 STATE STREET BANK						
		417.44			64959	11071216002513	9101.2176		LIFE/HEALTH-EMPLOYEE
		<u>417.44</u>							
99110854	11/8/2012		111465 STATE STREET BANK						
		2,579.00			64958	11071216002512	9101.2175		DEFERRED COMPENSATION
		<u>2,579.00</u>							
99111512	11/15/2012		108768 COMDATA NETWORK INC						
		2.61	AMAZON MKTPLACE PMTS , GLADHIL		64976	OCT12 COMDATA PCARDS	0240.6471		BOOKS & PAMPHLETS
		2.97	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		3.07	TARGET , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		3.99	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		4.26	AMAZON MKTPLACE PMTS , GLADHIL		64976	OCT12 COMDATA PCARDS	0240.6471		BOOKS & PAMPHLETS
		4.00	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		5.14	ARAMARK PHOENIX CONV , ULRICH		64976	OCT12 COMDATA PCARDS	0130.6331		TRAVEL & LODGING
		5.98	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		6.47	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		6.50	VCN*MN STATE PATROL , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		6.99	MENARDS , KAPLER		64976	OCT12 COMDATA PCARDS	0220.6249		MISCELLANEOUS OPERATING
		7.62	THE HARVEST GRILL , ULRICH		64976	OCT12 COMDATA PCARDS	0130.6249		MISCELLANEOUS OPERATING
		10.39	TARGET , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		10.39	TARGET , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		10.39	TARGET , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		10.39	TARGET , RIEMER		64976	OCT12 COMDATA PCARDS	0311.6249		MISCELLANEOUS OPERATING
		12.27	MENARDS , KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING

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						PCARDS			
		14.31	PERKINS FAMILY RESTAUR , KAPLE		64976	OCT12 COMDATA	0220.6335		TRAINING
						PCARDS			
		15.79	STEVES GRILL , ULRICH		64976	OCT12 COMDATA	0130.6331		TRAVEL & LODGING
						PCARDS			
		16.10	COBORN'S SUPERSTORE , KOHNER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		16.25	LITTLE ANGIE'S CANTINA , KAPLE		64976	OCT12 COMDATA	0220.6335		TRAINING
						PCARDS			
		16.88	JIMMY JOHN'S , DWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		17.38	WALMART , DWYER		64976	OCT12 COMDATA	0211.6249		MISCELLANEOUS OPERATING
						PCARDS			
		17.99	MENARDS KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		18.03	HYATT HOTELS PHOENIX F , ULRIC		64976	OCT12 COMDATA	0130.6331		TRAVEL & LODGING
						PCARDS			
		19.71	BUFFALO WILD WINGS , RIEMER		64976	OCT12 COMDATA	0311.6335		TRAINING
						PCARDS			
		19.95	SURVEYMONKEY.COM , LUND		64976	OCT12 COMDATA	0192.6405		OFFICE & DATA PROCESSING
						PCARDS			
		20.97	MENARDS , KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		21.15	HOLIDAY INN AND SUITES , RIEME		64976	OCT12 COMDATA	0311.6335		TRAINING
						PCARDS			
		21.44	COBORN'S SUPERSTORE , DWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		23.56	COBORN'S SUPERSTORE , DWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		25.00	DELTAULRICH		64976	OCT12 COMDATA	0130.6331		TRAVEL & LODGING
						PCARDS			
		27.94	MENARDS , KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		29.99	MENARDS , KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		34.29	NORTHERN TOOL EQUIPMNT , RIEME		64976	OCT12 COMDATA	0311.6249		MISCELLANEOUS OPERATING
						PCARDS			
		36.65	AMAZON MKTPLACE PMTS , GLADHIL		64976	OCT12 COMDATA	0240.6471		BOOKS & PAMPHLETS
						PCARDS			
		37.99	LA CASITA , DWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		42.35	COBORN'S SUPERSTORE , DWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		44.99	MENARDS , KAPLER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		45.69	WALMART , BRAY		64976	OCT12 COMDATA	0111.6249		MISCELLANEOUS OPERATING
						PCARDS			
		56.32	MOCEAN HOLDING LLC , DWYER		64976	OCT12 COMDATA	0211.6231		UNIFORMS & TURN-OUT GEAF
						PCARDS			
		59.61	BILL'S SUPERETTE , KAPLER		64976	OCT12 COMDATA	0220.6225		DIESEL FUEL
						PCARDS			
		76.77	MAMA DE LUCAS PIZZERIA , ULRIC		64976	OCT12 COMDATA	0111.6249		MISCELLANEOUS OPERATING
						PCARDS			
		76.77	MAMA DE LUCAS PIZZERIA , ULRIC		64976	OCT12 COMDATA	0111.6249		MISCELLANEOUS OPERATING
						PCARDS			
		85.00	ECONAR GEOSYSTEMS LLC , BRAY		64976	OCT12 COMDATA	0194.6259		BUILDING MAINT/REPAIR SUPI

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						PCARDS			
		91.92	AMAZON MKTPLACE PMTS , GLADHIL		64976	OCT12 COMDATA	0240.6471		BOOKS & PAMPHLETS
						PCARDS			
		87.40	BURGER KING , ANDERSON		64976	OCT12 COMDATA	0191.6451		MEMBERSHIP DUES
						PCARDS			
		100.00	HOLIDAY STNSTORE , KOHNER		64976	OCT12 COMDATA	0220.6223		GASOLINE
						PCARDS			
		138.40	AMAZON.COM , WIEMANN		64976	OCT12 COMDATA	0211.6249		MISCELLANEOUS OPERATING
						PCARDS			
		138.41	AMAZON.COM , WIEMANN		64976	OCT12 COMDATA	0211.6249		MISCELLANEOUS OPERATING
						PCARDS			
		139.96	REACTGEAR LLC , DWYER		64976	OCT12 COMDATA	0211.6231		UNIFORMS & TURN-OUT GEAF
						PCARDS			
		134.51	FIRST STATE TIRE DISPO , RIEME		64976	OCT12 COMDATA	9604.6249		MISCELLANEOUS OPERATING
						PCARDS			
		150.00	U OF M CCE NONCREDIT , RIEMER		64976	OCT12 COMDATA	0311.6335		TRAINING
						PCARDS			
		175.57	CALENDARS , WIEMANN		64976	OCT12 COMDATA	0211.6208		MISCELLANEOUS OFFICE SUF
						PCARDS			
		208.41	GLOCK PROFESSIONAL INCDWYER		64976	OCT12 COMDATA	0211.6335		TRAINING
						PCARDS			
		204.55	COMFORT INNS , KAPLER		64976	OCT12 COMDATA	0220.6335		TRAINING
						PCARDS			
		225.71	COBORN'S SUPERSTORE , KOHNER		64976	OCT12 COMDATA	0220.6249		MISCELLANEOUS OPERATING
						PCARDS			
		268.54	MENARDS , RIEMER		64976	OCT12 COMDATA	0452.6249		MISCELLANEOUS OPERATING
						PCARDS			
		285.00	APA - MEMBERSHIP ONLIN , ANDER		64976	OCT12 COMDATA	9604.6249		MISCELLANEOUS OPERATING
						PCARDS			
		346.31	CENTRAL HYDRAULICS INC , RIEME		64976	OCT12 COMDATA	0311.6257		OTHER VEHICLE PARTS
						PCARDS			
		495.83	CENTRAL HYDRAULICS INC , RIEME		64976	OCT12 COMDATA	0452.6257		OTHER VEHICLE PARTS
						PCARDS			
		642.88	AAA CLUB/INSUR , WIEMANN		64976	OCT12 COMDATA	0280.6241		COMMUNITY POLICING SUPPL
						PCARDS			
		659.22	HYATT HOTELS PHOENIX , ULRICH		64976	OCT12 COMDATA	0130.6331		TRAVEL & LODGING
						PCARDS			
		61.32-			64976	OCT12 COMDATA	9101.2082		SALES/USE TAX PAYABLE
						PCARDS			
		9.95	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		12.44	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		13.89	LITTLE DUKES , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		14.47	LITTLE DUKES , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		14.86	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		15.00	BILL'S SUPERETTE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		15.68	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		16.00	HOLIDAY STNSTORE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		16.12	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE

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						FUEL			
		16.90	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		16.91	HOLIDAY STNSTORE , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		17.00	BILL'S SUPERETTE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		17.70	LITTLE DUKES , WEBB		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		17.77	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		17.82	LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		18.00	HOLIDAY STNSTORE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		19.28	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		19.33	LITTLE DUKES , VOIT		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		19.81	LITTLE DUKES , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		20.01	HOLIDAY STNSTORE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000664	GASOLINE
						FUEL			
		20.83	LITTLE DUKES , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		21.14	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		21.64	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		21.77	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		21.79	LITTLE DUKES , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		21.82	LITTLE DUKES , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		22.30	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		22.68	LITTLE DUKES , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		23.58	LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		23.58	HOLIDAY STNSTORE , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		23.71	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		23.79	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		23.87	BILL'S SUPERETTE , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		24.00	HOLIDAY STNSTORE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		24.80	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		24.95	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		25.00	BILL'S SUPERETTE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE

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						FUEL			
		25.01	BILL'S SUPERETTE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000678	GASOLINE
						FUEL			
		25.15	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		26.00	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		26.14	HOLIDAY STNSTORE , RIEMER		64977	OCT12 COMDATA	0311.6223	00000405	GASOLINE
						FUEL			
		26.21	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		26.25	LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		26.42	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		26.51	HOLIDAY STNSTORE , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		26.95	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		27.02	CASEYS GEN STORE , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		27.29	LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		27.31	BILL'S SUPERETTE , WIELER		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		27.66	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		27.74	LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
		27.79	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		27.79	LITTLE DUKES , RESERVES		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		27.99	SUPERAMERICA , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		28.06	SUPERAMERICA , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		28.21	LITTLE DUKES , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		28.32	HOLIDAY STNSTORE , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		28.38	RAMSEY MARKET , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		28.83	LITTLE DUKES , WSTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		28.96	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		29.00	HOLIDAY STNSTORE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		29.01	HOLIDAY STNSTORE , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		29.20	LITTLE DUKES , RESERVES		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		29.41	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		29.60	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE

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						FUEL			
		29.74	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		29.75	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		29.79	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		29.93	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		30.00	BILL'S SUPERETTE , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
		30.35	BILL'S SUPERETTE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000675	GASOLINE
						FUEL			
		30.46	LITTLE DUKES , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		30.59	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		30.74	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		30.83	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000364	GASOLINE
						FUEL			
		31.15	HOLIDAY STNSTORE , MARAS		64977	OCT12 COMDATA	0311.6223	00000362	GASOLINE
						FUEL			
		31.44	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		31.45	LITTLE DUKES , RIEMER		64977	OCT12 COMDATA	0311.6223	00000405	GASOLINE
						FUEL			
		31.73	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		31.77	LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
		31.90	LITTLE DUKES , PAPHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		32.62	SUPERAMERICA , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		32.73	LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		32.73	LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
		32.78	LITTLE DUKES , DWYER		64977	OCT12 COMDATA	0211.6223	00000364	GASOLINE
						FUEL			
		32.79	BILL'S SUPERETTE , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		33.40	LITTLE DUKES , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		33.69	BILL'S SUPERETTE , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		34.00	HOLIDAY STNSTORE , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		34.17	LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		34.46	HOLIDAY STNSTORE , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		34.75	HOLIDAY STNSTORE , LINTON		64977	OCT12 COMDATA	0301.6223	00000407	GASOLINE
						FUEL			
		34.77	LITTLE DUKES , WEBB		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE

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						FUEL			
34.84			LITTLE DUKES , WEBB		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
35.83			RAMSEY MARKET , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000627	GASOLINE
						FUEL			
36.00			EXXONMOBIL , KATERS		64977	OCT12 COMDATA	0211.6223	00000352	GASOLINE
						FUEL			
36.11			LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
36.31			LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
36.53			LITTLE DUKES , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
36.70			LITTLE DUKES , FRANKFURTH		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
37.53			LITTLE DUKES , HEMMERICH		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
37.53			CASEYS GEN STORE , WIELER		64977	OCT12.COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
37.67			LITTLE DUKES , RESERVES		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
37.69			LITTLE DUKES , LUECK		64977	OCT12 COMDATA	0211.6223	00000318	GASOLINE
						FUEL			
38.19			BILL'S SUPERETTE , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
38.51			LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
38.56			LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
39.20			LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
39.52			LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
39.70			LITTLE DUKES , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000641	GASOLINE
						FUEL			
39.80			LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
40.02			LITTLE DUKES , WAY		64977	OCT12 COMDATA	0211.6223	00000351	GASOLINE
						FUEL			
40.07			LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
40.18			LITTLE DUKES , WEBB		64977	OCT12 COMDATA	0211.6223	00000301	GASOLINE
						FUEL			
40.22			LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
40.35			HOLIDAY STNSTORE , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
40.46			LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
40.79			LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
40.82			LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
42.15			LITTLE DUKES , MEYENBURG		64977	OCT12 COMDATA	0194.6223	00000403	GASOLINE
						FUEL			
42.20			LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE

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						FUEL			
		43.11	LITTLE DUKES , GLADHILL		64977	OCT12 COMDATA	0194.6223	00000404	GASOLINE
						FUEL			
		43.30	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		43.34	SUPERAMERICA , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		43.50	HOLIDAY STNSTORE , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		43.60	LITTLE DUKES , BLUML		64977	OCT12 COMDATA	0211.6223	00000365	GASOLINE
						FUEL			
		43.65	LITTLE DUKES , INSPECTRON		64977	OCT12 COMDATA	0240.6223	00000406	GASOLINE
						FUEL			
		43.88	LITTLE DUKES , VOIT		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		44.25	SUPERAMERICA , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		44.62	LITTLE DUKES , DWYER		64977	OCT12 COMDATA	0211.6223	00000364	GASOLINE
						FUEL			
		44.64	LITTLE DUKES , BLUML		64977	OCT12 COMDATA	0211.6223	00000365	GASOLINE
						FUEL			
		44.75	HOLIDAY STNSTORE , ROSSUM		64977	OCT12 COMDATA	0211.6223	00000352	GASOLINE
						FUEL			
		44.78	LITTLE DUKES , WAY		64977	OCT12 COMDATA	0211.6223	00000351	GASOLINE
						FUEL			
		44.83	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		44.86	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		45.33	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		45.51	SUPERAMERICA , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		45.58	LITTLE DUKES , KATERS		64977	OCT12 COMDATA	0211.6223	00000352	GASOLINE
						FUEL			
		45.60	CASEYS GEN STORE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000654	GASOLINE
						FUEL			
		45.62	LITTLE DUKES , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
		45.78	LITTLE DUKES , STOLP		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		46.20	LITTLE DUKES , MOLDENHAUER		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		46.39	LITTLE DUKES , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		46.58	LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		46.60	SUPERAMERICA , SEASONAL		64977	OCT12 COMDATA	0194.6223	00000312	GASOLINE
						FUEL			
		46.91	LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		47.08	LITTLE DUKES , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
		47.95	SUPERAMERICA , DIXON		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		48.00	HOLIDAY STNSTORE , BLUML		64977	OCT12 COMDATA	0211.6223	00000365	GASOLINE

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						FUEL			
		48.00	BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
		48.26	SUPERAMERICA , STOLP		64977	OCT12 COMDATA	0211.6223	00000364	GASOLINE
						FUEL			
		48.47	LITTLE DUKES , VOIT		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		48.51	LITTLE DUKES , HASSEL		64977	OCT12 COMDATA	0211.6223	00000317	GASOLINE
						FUEL			
		48.54	LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		48.60	LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000562	GASOLINE
						FUEL			
		48.81	SUPERAMERICA , VOIT		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		49.43	BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000557	GASOLINE
						FUEL			
		49.87	BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6225	00000560	DIESEL FUEL
						FUEL			
		49.99	LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		50.09	LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
		50.74	LITTLE DUKES , ROSSUM		64977	OCT12 COMDATA	0211.6223	00000375	GASOLINE
						FUEL			
		50.75	LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000392	GASOLINE
						FUEL			
		50.82	LITTLE DUKES , BLUML		64977	OCT12 COMDATA	0211.6223	00000365	GASOLINE
						FUEL			
		51.62	SUPERAMERICA , VOIT		64977	OCT12 COMDATA	0211.6223	00000303	GASOLINE
						FUEL			
		52.00	BILL'S SUPERETTE , BYRON		64977	OCT12 COMDATA	0452.6223	00000654	GASOLINE
						FUEL			
		52.01	SUPERAMERICA , KATERS		64977	OCT12 COMDATA	0211.6223	00000352	GASOLINE
						FUEL			
		52.33	LITTLE DUKES , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		52.60	LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		52.82	HOLIDAY STNSTORE , WIELER		64977	OCT12 COMDATA	0211.6223	00000393	GASOLINE
						FUEL			
		53.01	SUPERAMERICA , MARAS		64977	OCT12 COMDATA	0311.6223	00000405	GASOLINE
						FUEL			
		53.52	LITTLE DUKES , WISTROM		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
		53.70	BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
		54.07	BILL'S SUPERETTE , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE
						FUEL			
		54.12	LITTLE DUKES , WAY		64977	OCT12 COMDATA	0211.6223	00000351	GASOLINE
						FUEL			
		54.14	LITTLE DUKES , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
		54.56	LITTLE DUKES , CURTIS		64977	OCT12 COMDATA	0211.6223	00000391	GASOLINE
						FUEL			
		54.56	LITTLE DUKES , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE

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						FUEL			
55.17			LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
55.53			LITTLE DUKES , MEYENBURG		64977	OCT12 COMDATA	0211.6223	00000388	GASOLINE
						FUEL			
55.80			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000559	GASOLINE
						FUEL			
56.48			LITTLE DUKES , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000678	GASOLINE
						FUEL			
56.48			LITTLE DUKES , RESERVES		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
57.02			SUPERAMERICA , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6225	00000565	DIESEL FUEL
						FUEL			
57.18			LITTLE DUKES , PAPLHAM		64977	OCT12 COMDATA	0211.6223	00000316	GASOLINE
						FUEL			
57.33			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6225	00000565	DIESEL FUEL
						FUEL			
58.14			LITTLE DUKES , ROSSUM		64977	OCT12 COMDATA	0211.6223	00000375	GASOLINE
						FUEL			
58.90			LITTLE DUKES , INSPECTRON		64977	OCT12 COMDATA	0240.6223	00000401	GASOLINE
						FUEL			
59.00			HOLIDAY STNSTORE , DAHLBERG		64977	OCT12 COMDATA	0211.6223	00000302	GASOLINE
						FUEL			
60.03			BILL'S SUPERETTE , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
61.35			LITTLE DUKES , INSPECTRON		64977	OCT12 COMDATA	0240.6223	00000401	GASOLINE
						FUEL			
61.56			LITTLE DUKES , INSPECTRON		64977	OCT12 COMDATA	0240.6223	00000401	GASOLINE
						FUEL			
62.44			LITTLE DUKES , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE
						FUEL			
62.71			LITTLE DUKES , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
63.01			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
63.18			LITTLE DUKES , BONINE		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
63.47			LITTLE DUKES , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE
						FUEL			
63.87			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
64.49			LITTLE DUKES , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE
						FUEL			
64.59			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
65.24			RAMSEY MARKET , WIELER		64977	OCT12 COMDATA	0211.6223	00000386	GASOLINE
						FUEL			
65.38			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000558	GASOLINE
						FUEL			
66.41			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
66.48			LITTLE DUKES , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE
						FUEL			
67.02			BILL'S SUPERETTE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000641	GASOLINE
						FUEL			
68.31			KINGS COUNTY MARKET , KAPLER		64977	OCT12 COMDATA	0220.6223	00000564	GASOLINE

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						FUEL			
68.79			HOLIDAY STNSTORE , BERGE		64977	OCT12 COMDATA	0452.6223	00000664	GASOLINE
						FUEL			
69.00			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000563	GASOLINE
						FUEL			
69.44			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6223	00000558	GASOLINE
						FUEL			
69.87			LITTLE DUKES , SCHIFERLI		64977	OCT12 COMDATA	0211.6223	00000321	GASOLINE
						FUEL			
72.29			HOLIDAY STNSTORE , NIELSEN		64977	OCT12 COMDATA	0452.6223	00000675	GASOLINE
						FUEL			
73.85			LITTLE DUKES , TALBOT		64977	OCT12 COMDATA	0311.6223	00000353	GASOLINE
						FUEL			
73.93			SHELL OIL , SEASONAL		64977	OCT12 COMDATA	0194.6223	00000312	GASOLINE
						FUEL			
74.15			LITTLE DUKES , ERICKSON		64977	OCT12 COMDATA	9601.6223	00000667	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6225	00000565	DIESEL FUEL
						FUEL			
75.00			BILL'S SUPERETTE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000675	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000678	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , FIREFIGHTE		64977	OCT12 COMDATA	0220.6223	00000566	GASOLINE
						FUEL			
75.00			BILL'S SUPERETTE , MARAS		64977	OCT12 COMDATA	0311.6223	00000353	GASOLINE
						FUEL			
75.05			HOLIDAY STNSTORE , NELSON		64977	OCT12 COMDATA	0311.6223	00000676	GASOLINE
						FUEL			
75.34			LITTLE DUKES , TALBOT		64977	OCT12 COMDATA	0311.6223	00000353	GASOLINE
						FUEL			
76.37			HOLIDAY STNSTORE , FLEET		64977	OCT12 COMDATA	0452.6223	00000675	GASOLINE
						FUEL			
79.02			SUPERAMERICA , MADSEN		64977	OCT12 COMDATA	0301.6223	00000402	GASOLINE
						FUEL			
79.15			CASEYS GEN STORE , SEASONAL		64977	OCT12 COMDATA	0194.6223	00000312	GASOLINE
						FUEL			
79.52			SUPERAMERICA , MCDOWALL		64977	OCT12 COMDATA	0301.6223	00000638	GASOLINE
						FUEL			
80.34			LITTLE DUKES , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6225	00000556	DIESEL FUEL
						FUEL			
81.37			LITTLE DUKES , ERICKSON		64977	OCT12 COMDATA	9601.6223	00000667	GASOLINE
						FUEL			
82.53			HOLIDAY STNSTORE , BYRON		64977	OCT12 COMDATA	0452.6223	00000654	GASOLINE
						FUEL			
83.77			LITTLE DUKES , BYRON		64977	OCT12 COMDATA	0452.6223	00000664	GASOLINE
						FUEL			
84.27			KWIK TRIP , MARAS		64977	OCT12 COMDATA	0311.6223	00000676	GASOLINE
						FUEL			
84.30			LITTLE DUKES , BRAY		64977	OCT12 COMDATA	0194.6223	00000410	GASOLINE
						FUEL			
84.75			SUPERAMERICA , MCDOWALL		64977	OCT12 COMDATA	0301.6223	00000638	GASOLINE

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						FUEL			
		85.63	SUPERAMERICA , MCDOWALL		64977	OCT12 COMDATA	0301.6223	00000638	GASOLINE
						FUEL			
		87.48	LITTLE DUKES , ERICKSON		64977	OCT12 COMDATA	9601.6223	00000667	GASOLINE
						FUEL			
		88.10	HOLIDAY STNSTORE , RIVERBLOOD		64977	OCT12 COMDATA	0452.6223	00000664	GASOLINE
						FUEL			
		89.28	LITTLE DUKES , ERICKSON		64977	OCT12 COMDATA	9601.6223	00000667	GASOLINE
						FUEL			
		96.27	LITTLE DUKES , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000678	GASOLINE
						FUEL			
		96.73	LITTLE DUKES , BRAY		64977	OCT12 COMDATA	0194.6223	00000410	GASOLINE
						FUEL			
		97.78	HOLIDAY STNSTORE , BERGE		64977	OCT12 COMDATA	0452.6223	00000664	GASOLINE
						FUEL			
		102.46	LITTLE DUKES , ERICKSON		64977	OCT12 COMDATA	9601.6223	00000667	GASOLINE
						FUEL			
		103.00	HOLIDAY STNSTORE , BYRON		64977	OCT12 COMDATA	0452.6223	00000653	GASOLINE
						FUEL			
		104.00	SUPERAMERICA , FIREFIGHTERS		64977	OCT12 COMDATA	0220.6225	00000560	DIESEL FUEL
						FUEL			
		109.70	HOLIDAY STNSTORE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000675	GASOLINE
						FUEL			
		114.70	HOLIDAY STNSTORE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000653	GASOLINE
						FUEL			
		125.00	HOLIDAY STNSTORE , SEASONAL		64977	OCT12 COMDATA	0452.6223	00000627	GASOLINE
						FUEL			
		140.81	LITTLE DUKES , STENQUIST		64977	OCT12 COMDATA	0311.6223	00000676	GASOLINE
						FUEL			
		16,955.78							
99111912	11/19/2012		100629 MN DEPT OF REV SALES TX						
		.61	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9101.4609		OTHER MISCELLANEOUS REV
						SALES/USE/TRANS TAX			
		1.51	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9101.4308		SALES OF MAPS & PUBLICATI
						SALES/USE/TRANS TAX			
		3.71	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9101.4305		RENTAL FEES
						SALES/USE/TRANS TAX			
		103.55	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9499.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			
		215.49	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9601.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			
		489.03	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9101.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			
		767.95	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9601.2085		ANOKA COUNTY TRANSIT TAX
						SALES/USE/TRANS TAX			
		21,067.15	OCT12 SALES/USE/TRANS TAX		65083	OCT12	9601.2082		SALES/USE TAX PAYABLE
						SALES/USE/TRANS TAX			

CITY OF RAMSEY
Council Check Register

11/8/2012 – 12/31/2012

<u>Check #</u>	<u>Date</u>	<u>Amount</u>	<u>Supplier / Explanation</u>	<u>PO #</u>	<u>Doc No</u>	<u>Inv No</u>	<u>Account No</u>	<u>Subledger</u>	<u>Account Description</u>
		22,649.00							
99681850	11/8/2012	1,764.18	100223 ICMA RETIREMENT TRUST 457		64955	1107121600251	9101.2175		DEFERRED COMPENSATION
		1,764.18							
		<u>2,008,571.12</u>	Grand Total						

Payment Instrument Totals

Check Total	1,895,830.43
Transfer Total	112,740.69
Total Payments	<u>2,008,571.12</u>

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CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Number	Payee Name / Mailing Address	Stub Message	Ty	Document Number	Item	Co	Due Date	Invoice Number	Payment Amount
111385	ACHIEVE SERVICES INC 1201 89TH AVENUE NE BLAINE MN 55434	SHREDDING	PV	65036	001	09101	11/14/2012	7143	62.40
								Summary Total	62.40
								Payment Amount	62.40
100017	AIRGAS NORTH CENTRAL AIRGAS NORTH CENTRAL PO BOX 802588 CHICAGO IL 60680-2588	FIRE -TEST CYLINDER GASES	PV	64982	001	09101	10/23/2012	9009634176	38.52
								Summary Total	38.52
			PV	65037	001	09101	10/31/2012	9905131043	23.20
								Summary Total	23.20
								Payment Amount	61.72
107485	ALEXANDRA HOUSE INC ALEXANDRA HOUSE INC P O BOX 49039 BLAINE MN 55449-0039	2012 SERVICES	PV	65008	001	09101	11/2/2012	110212	5,000.00
								Summary Total	5,000.00
								Payment Amount	5,000.00
110744	ALLINA HEALTH SYSTEM ALLINA HEALTH SYSTEM LAWSON MISC BILLING NW 7710 P O BOX 9383 MINNEAPOLIS MN 55440-9383	4TH QTR OCT-DEC 2012	PV	65009	001	09101	10/1/2012	110016212	1,346.25
								Summary Total	1,346.25
								Payment Amount	1,346.25
107526	ANCOM TECHNICAL CENTER ANCOM TECHNICAL CENTER 1800 EAST CLIFF ROAD SUITE 17A BURNSVILLE MN 55337	PAGER REPAIR	PV	65042	001	09101	10/29/2012	33051	105.00
								Summary Total	105.00
		PAGER REPAIR	PV	65043	001	09101	10/29/2012	33054	95.00
								Summary Total	95.00
		PAGER REPAIR	PV	65044	001	09101	10/29/2012	33055	95.00
								Summary Total	95.00
		PAGER REPAIR	PV	65045	001	09101	10/29/2012	33056	95.00
								Summary Total	95.00
		PAGER REPAIR	PV	65046	001	09101	10/29/2012	33053	95.00
								Summary Total	95.00
		PAGER REPAIR	PV	65047	001	09101	10/29/2012	33052	95.00

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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee	Stub	Document	Due	Invoice	Payment				
Number	Name / Mailing Address	Message	Ty	Number	Item	Co	Date	Number	Amount
Summary Total									95.00
Payment Amount									580.00
100043	ANOKA COUNTY PROPERTY RECORDS TAXATION	00066-2001/7039 HWY 10	PV	65048	001	09410	11/2/2012	110212	7,812.14
Summary Total									7,812.14
	ANOKA COUNTY PROPERTY RECORDS TAXATION	2100 - 3RD AVENUE	PV	65089	001	09101	11/1/2012	534 OCT 12	46.00
Summary Total									46.00
Payment Amount									7,858.14
111377	ANOKA MUNICIPAL UTILITY	14034 DYSPROSIUM ST	PV	65038	001	09603	11/13/2012	22-610280-00NOV12	66.43
Summary Total									66.43
	ANOKA MUNICIPAL UTILITY	CITY HALL	PV	65039	001	09603	11/13/2012	22-612000-01 NOV 12	31.25
Summary Total									31.25
	2015 FIRST AVENUE NO	ANOKA MN 55303	PV	65040	001	09603	11/13/2012	22-990005-01NOV 12	58.35
Summary Total									58.35
		CTY RD 116 & HWY 47-SIGNAL	PV	65041	001	09603	11/13/2012	22-613120-01 NOV 12	101.96
Summary Total									101.96
Payment Amount									257.99
100058	ANOKA RAMSEY FARM AND GARDEN	MINK MULCH	PV	64983	001	09101	10/24/2012	7576-5	94.05
Summary Total									94.05
Payment Amount									94.05
107141	APPLIED CONCEPTS INC	ANTENNA/MOUNT	PV	65049	001	09101	11/1/2012	227437	491.63
Summary Total									491.63
Payment Amount									491.63

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
100063	ASPEN MILLS	FD T. FLICKINGER UNIFORM	PV	65010	001	09101	10/30/2012	127332	486.34
	ASPEN MILLS			Summary Total					486.34
	8201 C CENTRAL AVE NE	S. WIDSTROM BOOTS	PV	65011	001	09101	10/30/2012	127330	219.50
	SPRING LAKE PARK MN 55432			Summary Total					219.50
		J. EDWARDS FD BOOTS	PV	65012	001	09101	10/30/2012	127331	219.50
				Summary Total					219.50
		D. MOLDENHAUER- BOOTS	PV	65050	001	09101	10/30/2012	127333	129.50
				Summary Total					129.50
				Payment Amount					1,054.84
100095	BRIGGS AND MORGAN PA INC	COR TWO	PV	64984	001	09468	10/31/2012	509964	5,828.76
	BRIGGS AND MORGAN			Summary Total					5,828.76
	P O BOX 64591	RE: MASTER DECLARATION	PV	64985	001	09235	10/31/2012	509963	1,732.50
	ST PAUL MN 55164-0591			Summary Total					1,732.50
		RAMSEY TOWN CENTER	PV	64986	001	09295	10/31/2012	509961	4,786.38
				Summary Total					4,786.38
		RE: EDGEWOOD MANAGEMENT GROUP	PV	64987	001	09468	10/31/2012	509965	5,418.00
				Summary Total					5,418.00
		RE F AND C APARTMENTS	PV	65051	001	09214	11/16/2012	511207	1,785.00
				Summary Total					1,785.00
		RTC PROJECT	PV	65090	001	09295	11/16/2012	511206	210.00
				Summary Total					210.00
		COR TWO	PV	65091	001	09468	11/16/2012	511209	10,720.28
				Summary Total					10,720.28
		EDGEWOOD MANAGE. GROUP	PV	65092	001	09468	11/16/2012	511210	297.50
				Summary Total					297.50
		RE: MASTER DELJUDGEMENT ACT.	PV	65093	001	09295	11/16/2012	511208	2,820.36
				Summary Total					2,820.36
				Payment Amount					33,598.78
112663	CAPSTONE HOMES INC	ERO CON REFUND 7489	PV	65107	001	09804	11/20/2012	113277	1,500.00

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
Payment Amount								6,750.00	
100144	DEHN OIL COMPANY	DIESEL FUEL	PV	65013	001	09101	11/1/2012	19958	2,331.00
	DEHN OIL COMPANY			Summary Total					2,331.00
	6735 141ST AVENUE NW	DYED PREF GOLD- 450 GAL	PV	65055	001	09101	11/1/2012	19957	1,731.38
	RAMSEY MN 55303			Summary Total					1,731.38
Payment Amount								4,062.38	
104026	DIVERSIFIED PAVING INC	OVERLAY OF TRAIL	PV	64989	001	09101	10/24/2012	5591	18,237.70
	DIVERSIFIED PAVING INC			Summary Total					18,237.70
	8019 146TH AVENUE NW			Payment Amount					18,237.70
	RAMSEY MN 55303			Payment Amount					18,237.70
100158	ECM PUBLISHERS INC	PUBLIC HEARING	PV	65014	001	09101	11/2/2012	01806796	56.38
	ECM PUBLISHERS INC			Summary Total					56.38
	4095 COON RAPIDS BLVD			Payment Amount					56.38
	COON RAPIDS MN 55433			Payment Amount					56.38
100160	EH RENNER AND SONS INC	REMOVE/INSTALL PUMP	PV	65056	001	09601	11/5/2012	000127590000	3,480.00
	EH RENNER AND SONS INC			Summary Total					3,480.00
	15688 JARVIS STREET			Payment Amount					3,480.00
	ELK RIVER MN 55330			Payment Amount					3,480.00
104205	EXPRESS SIGNS AND BALLOONS INC	FIRE OPEN HOUSE SIGN RENTAL	PV	64990	001	09101	10/11/2012	8256	138.94
	EXPRESS SIGNS AND BALLOONS INC			Summary Total					138.94
	PO BOX 475			Payment Amount					138.94
	ANOKA MN 55303			Payment Amount					138.94
113321	FACTORY MOTOR PARTS	BRAKES AND ROTORS 364	PV	64991	001	09101	11/2/2012	6-1304035	224.90
	FACTORY MOTOR PARTS			Summary Total					224.90

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
	3731 THURSTON AVENUE	BRAKE LINING/ROTORS 376	PV	64992	001	09101	10/31/2012	6-1303793	356.30
	ANOKA MN 55303								Summary Total 356.30
									Payment Amount 581.20
107099	FASTENAL	MISC PARTS	PV	64993	001	09101	10/30/2012	MNTC891989	26.19
	FASTENAL COMPANY								Summary Total 26.19
	P O BOX 978	MISC PARTS	PV	64994	001	09101	10/29/2012	MNTC891939	12.54
	WINONA MN 55987								Summary Total 12.54
		MISC PARTS	PV	64995	001	09101	10/29/2012	MNTC891925	35.84
									Summary Total 35.84
									Payment Amount 74.57
107101	FIRE LOSS MANAGEMENT LLC	REVIEW- THE RESIDENCE	PV	65015	001	09101	10/29/2012	102912	500.00
	FIRE LOSS MANAGEMENT LLC								Summary Total 500.00
	678 PINWOOD DRIVE								
	SHOREVIEW MN 55126								Payment Amount 500.00
110760	FIRE SAFETY USA, INC	MISC SUPPLIES	PV	65058	001	09101	11/12/2012	55957	4,785.00
	FIRE SAFETY USA, INC								Summary Total 4,785.00
	3253 19TH STREET NW								
	ROCHESTER MN 55901								Payment Amount 4,785.00
112421	FIRST SCRIBE	ROW WEB APP	PV	65057	001	09101	11/1/2012	2458400	200.00
	FIRST SCRIBE								Summary Total 200.00
	110 CHESHIRE LANE SUITE 105								
	MINNETONKA MN 55305								Payment Amount 200.00
111709	FIRST TRANSIT INC	OCT 12 RAMSEY STAR EXPRESS	PV	64996	001	09803	11/1/2012	558091012R	56,124.15
	FIRST TRANSIT INC								Summary Total 56,124.15
	22192 NETWORK PLACE								
	CHICAGO IL 60673-1221								Payment Amount 56,124.15

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
Payment Amount								37.16	
109262	GREENBERG IMPLEMENT INC	FUEL CAP	PV	65064	001	09101	10/18/2012	PS79925	7.89
	GREENBERG IMPLEMENT INC 19745 NOWTHEN BLVD NW RAMSEY MN 55303			Summary Total					7.89
Payment Amount								7.89	
100209	HAKANSON ANDERSON ASSOC INC	RUM RIVER GOLF COURSE-DRAINAGE	PV	65017	001	09605	10/25/2012	30888	8,500.00
	HAKANSON ANDERSON ASSOC INC 3601 THURSTON AVENUE ANOKA MN 55303-1063			Summary Total					8,500.00
Payment Amount								8,500.00	
104027	INK WIZARDS INC	WINDSHIRTS	PV	65098	001	09101	11/6/2012	63105	239.75
	INK WIZARDS INC 9958 HIGHWAY 10 NW ELK RIVER MN 55330			Summary Total					239.75
Payment Amount								239.75	
106324	INSPECTRON INC	OCT 2012 BILLING	PV	65099	001	09101	11/15/2012	111512	11,175.00
	INSPECTRON INC CODE COMPLIANCE INSPECTIONS 15120 CHIPPENDALE AVE SUITE 104 ROSEMOUNT MN 55068			Summary Total					11,175.00
Payment Amount								11,175.00	
100240	K MART 3897	RE SCHOOL SAFETY PATROL	PV	65018	001	09101	11/8/2012	110812	31.95
	K MART 3897 1100 WEST HIGHWAY 10 ANOKA MN 55303			Summary Total					31.95
		MISC SUPPLIES	PV	65019	001	09101	11/1/2012	110112	23.56
				Summary Total					23.56
Payment Amount								55.51	
100722	LATOUR VINYL	HELMET NAMES (3)	PV	65100	001	09101	11/6/2012	2013-39	22.44
	LATOUR VINYL 7747 158TH AVENUE NW			Summary Total					22.44
		HELMET NAMES (8)	PV	65101	001	09101	10/8/2012	2013-39A	44.89

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CITY OF RAMSEY
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Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
RAMSEY MN 55303				Summary Total					44.89
				Payment Amount					67.33
107807	LIND, GREG	BOOTS G. LIND REIMB 12	PV	65065	001	09101	11/27/2012	111612	100.00
GREG LIND 1099 WESTCLIFF CURVE SHOREVIEW MN 55120-1112				Summary Total					100.00
				Payment Amount					100.00
112622	LOFFLER COMPANIES INC	TONER	PV	65022	001	09101	10/23/2012	1475682	213.75
LOFFLER COMPANIES INC 1101 EAST 78TH STREET # 200		RETURN- TONER	PD	65023	001	09101	10/26/2012	1475682A	213.75-
BLOOMINGTON MN 55420				Summary Total					213.75-
*****	3717	Amount Under Payment Limit		Payment Amount					
100266	LOGIS	OCTOBER 2012 BILLING	PV	65024	001	09101	10/31/2012	35855	9,982.00
LOGIS 5750 DULUTH STREET		NETWORK	PV	65025	001	09101	10/29/2012	35823	48.00
GOLDEN VALLEY MN 55422-4036				Summary Total					48.00
				Payment Amount					10,030.00
106741	LSA DESIGN INC	RAMSEY DECK BIDDING	PV	65020	001	09468	11/7/2012	12151	1,266.84
LSA DESIGN INC 219 N 2ND STREET SUITE 302 MINNEAPOLIS MN 55401-1454				Summary Total					1,266.84
				Payment Amount					1,266.84
100271	MAIN MOTORS	REPAIR ON 364	PV	64999	001	09101	10/26/2012	378021	302.94
MAIN MOTORS		REPAIR ON 364	PV	64999	002	09101	10/26/2012	378021	765.00
435 WEST MAIN STREET ANOKA MN 55303				Summary Total					1,067.94
				Payment Amount					1,067.94
111999	MEADOW CREEK BUILDERS INC	ERO CON REFUND 15231 HELIUM ST	PV	65102	001	09804	11/20/2012	113197	1,500.00

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Payee	Stub	Document	Due	Invoice	Payment				
Number	Name / Mailing Address	Message	Ty	Number	Item	Co	Date	Number	Amount
	MEADOW CREEK BUILDERS INC 6382 BALSAM LANE MAPLE GROVE MN 55369				Summary Total				1,500.00
					Payment Amount				1,500.00
100283	MENARDS COON RAPIDS MENARDS COON RAPIDS 3045 MAIN STREET COON RAPIDS MN 55433	FD MISC SUPPLIES	PV	65021	001	09101	11/7/2012	8179	140.39
					Summary Total				140.39
					Payment Amount				140.39
100284	MENARDS ELK RIVER MENARDS ELK RIVER 19521 EVANS STREET NW ELK RIVER MN 55330-1077	MISC SUPPLIES	PV	65066	001	09101	11/7/2012	8228	237.48
					Summary Total				237.48
					Payment Amount				237.48
100287	METRO FIRE INC METRO FIRE INC 13915 LINCOLN ST NE SUITE D HAM LAKE MN 55304	MISC FD SUPPLIES	PV	65067	001	09101	11/8/2012	45628	5,139.66
					Summary Total				5,139.66
					Payment Amount				5,139.66
100289	METRO SALES INC METRO SALES INC 1620 EAST 78TH STEET MINNEAPOLIS MN 55423	QTR BILLING NOV 12/FEB 12	PV	65026	001	09101	11/6/2012	485410	1,229.29
		QTR BILLING NOV 12/FEB 12	PV	65026	002	09101	11/6/2012	485410	367.19
					Summary Total				1,596.48
		QTR BILLING OCT 12 TO JAN 13	PV	65027	001	09101	10/30/2012	484486	1,291.99
		QTR BILLING OCT 12 TO JAN 13	PV	65027	002	09101	10/30/2012	484486	385.92
					Summary Total				1,677.91
					Payment Amount				3,274.39
108201	METRO TENT RENTAL METRO TENT RENTAL 9273 188TH AVE NW	RAIL EVENT 11/8 BALANCE DUE	PV	65000	001	09230	10/10/2012	3461A	11.76
		RAIL EVENT 11/8 BALANCE DUE	PV	65000	002	09230	10/10/2012	3461A	11.76
					Summary Total				23.52

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Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
ELK RIVER MN 55330									
Payment Amount								23.52	
100293	MIDC ENTERPRISES	20% DOWN- WATER PUMP	PV	65103	001	09101	11/9/2012	10031460	2,115.00
MIDC ENTERPRISES 1450 WEST COUNTY ROAD C ROSEVILLE MN 55113									
Summary Total								2,115.00	
Payment Amount								2,115.00	
109247	MIDWEST TESTING	REBUILT KIT	PV	65069	001	09101	10/15/2012	2605	332.35
MIDWEST TESTING 6401 CAMBRIDGE STREET SAINT LOUIS PARK MN 55426									
Summary Total								332.35	
Payment Amount								332.35	
106555	MINNEAPOLIS, CITY OF	TRANSACTION FEE OCT 2012	PV	65070	001	09101	11/8/2012	400413003356	118.20
MINNEAPOLIS FINANCE DEPT P O BOX 77038 MINNEAPOLIS MN 55480-7738									
Summary Total								118.20	
Payment Amount								118.20	
102216	MN DEPT OF TRANSPORTATION	INSPECTIONS	PV	65068	001	09497	10/26/2012	P00000766	978.27
MINNESOTA DEPT OF TRANSPORTATION		INSPECTIONS	PV	65068	002	09497	10/26/2012	P00000766	131.68
COMMISSIONER OF TRANSPORTATION CASH ACCOUNTING -MS 215 395 JOHN IRELAND BLVD ST PAUL MN 55155-1899									
Summary Total								1,109.95	
Payment Amount								1,109.95	
100224	MN OFFICE OF ENTERPRISE TECH SHARED SER.	OCT 2012 WAN CHARGES	PV	65028	001	09101	11/5/2012	DV12100444	176.38
MN OFFICE OF ENTERPRISE TECH SHARED TECH ROOM 510 658 CEDAR STREET ST PAUL MN 55155									
Summary Total								176.38	
Payment Amount								176.38	

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Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
100345	NAPAAUTO PARTS ELK RIVER	BATTERY/WIPER BLADES 376	PV	65001	001	09101	10/31/2012	651424	145.61
	NAPAAUTO PARTS ELK RIVER 17137 YALE STREET NW P O BOX 1041 ELK RIVER MN 55330								Summary Total 145.61
									Payment Amount 145.61
100363	NORTHERN SANITARY SUPPLY CO	MISC SUPPLIES	PV	65072	001	09101	10/30/2012	158063	338.06
	NORTHERN SANITARY SUPPLY CO 341 COON RAPIDS BLVD MINNEAPOLIS MN 55433								Summary Total 338.06
									Payment Amount 338.06
110547	NORTHWEST LIGHTING SYSTEMS CO.	BULBS/LAMPS	PV	65071	001	09101	10/29/2012	71657	86.90
	NORTHWEST LIGHTING SYSTEMS CO. 9405 HOLLY STREET NW COON RAPIDS MN 55433								Summary Total 86.90
									Payment Amount 86.90
109908	NOVAK FLECK INC	ERO CON REFUND 7888 149TH AVE	PV	65104	001	09804	11/20/2012	113207	1,500.00
	NOVAK FLECK INC 8857 ZEALAND AVENUE NORTH BROOKLYN PARK MN 55445								Summary Total 1,500.00
									Payment Amount 1,500.00
110480	OPUS 21 MANAGEMENT SOLUTIONS	OCT 2012 SERVICES	PV	65002	001	09601	11/3/2012	121052	3,130.18
	OPUS 21 MANAGEMENT SOLUTIONS	OCT 2012 SERVICES	PV	65002	002	09601	11/3/2012	121052	2,504.14
	680 COMMERCE DRIVE SUITE 160	OCT 2012 SERVICES	PV	65002	003	09601	11/3/2012	121052	2,504.14
	WOODBURY MN 55125	OCT 2012 SERVICES	PV	65002	004	09601	11/3/2012	121052	1,878.10
		OCT 2012 SERVICES	PV	65002	005	09601	11/3/2012	121052	2,504.14
									Summary Total 12,520.70
									Payment Amount 12,520.70

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
111320	PARAMOUNT AUTO SERVICE	TIRES FOR 376	PV	65003	001	09101	11/1/2012	032937	491.37
	PARAMOUNT AUTO SERVICE 7151 RIVERDALE DRIVE RAMSEY MN 55303			Summary Total					491.37
				Payment Amount					491.37
111488	POPP.COM INC	OCT 2012 BILLING	PV	65073	001	09101	10/31/2012	992064050	112.29
	POPP.COM INC	OCT 2012 BILLING	PV	65073	002	09101	10/31/2012	992064050	40.10
	P O BOX 27110	OCT 2012 BILLING	PV	65073	003	09101	10/31/2012	992064050	40.10
	GOLDEN VALLEY MN 55427-0110	OCT 2012 BILLING	PV	65073	004	09101	10/31/2012	992064050	232.60
		OCT 2012 BILLING	PV	65073	005	09101	10/31/2012	992064050	40.10
		OCT 2012 BILLING	PV	65073	006	09101	10/31/2012	992064050	56.15
		OCT 2012 BILLING	PV	65073	007	09101	10/31/2012	992064050	32.08
		OCT 2012 BILLING	PV	65073	008	09101	10/31/2012	992064050	8.27
		OCT 2012 BILLING	PV	65073	009	09101	10/31/2012	992064050	90.36
		OCT 2012 BILLING	PV	65073	010	09101	10/31/2012	992064050	90.50
		OCT 2012 BILLING	PV	65073	011	09101	10/31/2012	992064050	120.48
		OCT 2012 BILLING	PV	65073	012	09101	10/31/2012	992064050	61.68
				Summary Total					924.71
				Payment Amount					924.71
112959	PREMIUM WATERS INC	WATER FD	PV	65074	001	09101	10/31/2012	621332-10-12	16.61
	PREMIUM WATERS INC			Summary Total					16.61
	P O BOX 9128	WATER FD	PV	65075	001	09101	10/31/2012	621331-10-12	10.79
	MINNEAPOLIS MN 55480-9128			Summary Total					10.79
				Payment Amount					27.40
100403	QUICKSILVER EXPRESS COURIER	RE MCDONALDS MIDWEST	PV	65076	001	09468	10/31/2012	6465314	118.46
	QUICKSILVER EXPRESS COURIER			Summary Total					118.46
	PO BOX 64417			Payment Amount					118.46
	ST PAUL MN 55164-0417								
107880	RICK JOHNSON DEER AND BEAVER INC	OCT 2012 BILLING	PV	65004	001	09101	11/1/2012	2012 OCT	360.00
	RICK JOHNSON DEER AND BEAVER INC			Summary Total					360.00
	18595 XENOLITH STREET NW								

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Number	Payee Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
BURNS TOWNSHIP MN 55303									360.00
Payment Amount									360.00
112475	S AND T OFFICE PRODUCTS INC	OFFICE SUPPLIES	PV	65005	001	09101	10/11/2012	01PK9204	116.16
	S AND T OFFICE PRODUCTS INC	OFFICE SUPPLIES	PV	65005	002	09101	10/11/2012	01PK9204	13.47
	1000 KRISTEN COURT	OFFICE SUPPLIES	PV	65005	003	09101	10/11/2012	01PK9204	52.50
	ST PAUL MN 55110	OFFICE SUPPLIES	PV	65005	004	09101	10/11/2012	01PK9204	51.18
		OFFICE SUPPLIES	PV	65005	005	09101	10/11/2012	01PK9204	41.32
		OFFICE SUPPLIES	PV	65005	006	09101	10/11/2012	01PK9204	170.16
Summary Total									444.79
Payment Amount									444.79
106398	S W WOLD CONSTRUCTION INC	ERO CON REFUND 7516 163RD AVE	PV	65105	001	09804	11/20/2012	113230	1,500.00
	S W WOLD CONSTRUCTION INC	9457 HIGHWAY 10 NW SUITE 200							1,500.00
		ERO CON REFUND 7508 161ST AVE	PV	65106	001	09804	11/20/2012	113229	1,500.00
	RAMSEY MN 55303								1,500.00
Summary Total									1,500.00
Payment Amount									3,000.00
100431	SAFETY KLEEN CORPORATION	RECYCLE OIL	PV	65077	001	09101	10/31/2012	927882016	101.00
	SAFETY KLEEN CORPORATION	PO BOX 382066 PITTSBURGH PA 15250-8066							101.00
Summary Total									101.00
Payment Amount									101.00
107711	SCHINDLER ELEVATOR	QTR NOV 12- JAN 13	PV	65078	001	09101	11/2/2012	8103321105	522.27
	SCHINDLER ELEVATOR	P O BOX 93050 CHICAGO IL 60673-3050							522.27
Summary Total									522.27
Payment Amount									522.27
100920	SHERWIN WILLIAMS	PAINT	PV	65079	001	09101	10/23/2012	9324-6	110.80
	SHERWIN WILLIAMS	3564 MAIN STREET NW MINNEAPOLIS MN 55448-1002							110.80
Summary Total									110.80
Payment Amount									110.80

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee Number	Name / Mailing Address	Stub Message	Document Ty	Number	Itm	Co	Due Date	Invoice Number	Payment Amount
Payment Amount									98.84
105628	WELLS CATERING SERVICE	CC MEETING 11/13/12	PV	65031	001	09101	11/13/2012	27137	123.85
	WELLS CATERING SERVICE 7533 SUNWOOD DRIVE SUITE 108 RAMSEY MN 55303								Summary Total 123.85
Payment Amount									123.85
100529	WENDELL'S INC	RED- DRY PAD PRINTY	PV	65032	001	09101	11/2/2012	1750090	11.86
	WENDELL'S INC 6601 BUNKER LAKE BLVD PO BOX 458 RAMSEY MN 55303-0458								Summary Total 11.86
Payment Amount									11.86
100539	WRIGHT TIRE SERVICE INC	TIRES FOR 642	PV	65006	001	09605	11/1/2012	86048	903.55
	WRIGHT TIRE SERVICE INC 710 WEST MAIN STREET ANOKA MN 55303								Summary Total 903.55
Payment Amount									903.55
100541	ZARNOTH BRUSH WORKS INC	BROOM REFILL/ GUTTER BROOM	PV	65007	001	09605	10/17/2012	0141175	1,598.85
	ZARNOTH BRUSH WORKS INC PO BOX 141 CHILTON WI 53014-0141								Summary Total 1,598.85
Payment Amount									1,598.85
Total Amount to be Processed									229,969.41
Total Number of Payments to be Processed									78

R04570

CITY OF RAMSEY
Create Payment Control Groups

Bank Account 00002224 CASH IN BANK
Version LOGIS003V
Originator JLIPSKI
Payment Instrument Check Payment
Pay Through Date 12/31/2012

Payee		Stub	Document			Due	Invoice	Payment	
Number	Name / Mailing Address	Message	Ty	Number	Itm	Co	Date	Number	Amount
113206	NORTH PINE AGGREGATE INC	PAY EST 4 SUNWOOD REALIGN.	PV	65034	001	09496	11/16/2012	111612	521,084.66
	NORTH PINE AGGREGATE INC 14551 LAKE DRIVE FOREST LAKE MN 55025							Summary Total	521,084.66
								Payment Amount	521,084.66
101234	NORTH VALLEY, INC	PAY EST 2 ALPINE DR RECONST	PV	65035	001	09204	11/16/2012	111612	180,442.66
	NORTH VALLEY, INC	PAY EST 2 ALPINE DR RECONST	PV	65035	002	09204	11/16/2012	111612	44,944.95
	20015 IGUANA STREET NW NOWTHEN MN 55330							Summary Total	225,387.61
								Payment Amount	225,387.61
								Total Amount to be Processed	746,472.27
								Total Number of Payments to be Processed	2

Councilmember _____ introduced the following resolution and moved for its adoption:

RESOLUTION #12-11-XXX

RESOLUTION APPROVING CASH DISBURSEMENTS MADE AND AUTHORIZING PAYMENT OF ACCOUNTS PAYABLE INVOICING RECEIVED DURING THE PERIOD OF NOVEMBER 8, 2012 THROUGH NOVEMBER 20, 2012.

WHEREAS, the City of Ramsey Finance Department has made cash disbursements and received accounts payable invoicing during the period of November 8, 2012, through November 20, 2012, in the amount of \$3,109,304.85; and

WHEREAS, the City Council of the City of Ramsey is required to authorize payment for all disbursement transactions.

NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:

- 1) That the Ramsey City Council hereby approves the cash disbursements made and authorizes payment of the accounts payable invoices as detailed in the attached Bills List for the period November 8, 2012, through November 20, 2012, in the amount of \$3,109,304.85.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember _____, and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 27th day of November 2012.

Mayor

ATTEST:

City Clerk

CC Regular Session

4. 10.

Meeting Date: 11/27/2012

Submitted For: MaryJo Warner

By: MaryJo Warner, Engineering/Public Works

Information

Title:

Adopt Resolution #12-11-XXX Authorizing Partial Payment to North Pine Aggregate for IP 12-20 Armstrong/Sunwood Drive

Background:

Resolution and Pay Request attached.

Recommendation:

Staff recommends authorizing partial payment to North Pine Aggregate, for IP 12-20 Armstrong/Sunwood in the amount of \$521,084.66.

Council Action:

Motion to adopt Resolution #12-11-XXX authorizing partial payment to North Pine Aggregate for IP 12-20 Armstrong/Sunwood in the amount of \$521,084.66.

Attachments

Resolution 11.27.12

Pay Request 11.27.12

Form Review

Inbox	Reviewed By	Date
Hakanson Anderson Engineering	Shane Nelson	11/21/2012 11:05 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:36 AM
Form Started By: MaryJo Warner		Started On: 11/19/2012 09:20 AM

Final Approval Date: 11/21/2012

Councilmember introduced the following resolution and moved for its adoption:

RESOLUTION #12-11-XXX

**RESOLUTION AUTHORIZING PARTIAL PAYMENT TO NORTH PINE
AGGREGATE FOR CITY IMPROVEMENT
PROJECT #12-20; SUNWOOD DRIVE REALIGNMENT**

WHEREAS, on June 28, 2011 the Ramsey Housing and Redevelopment Authority awarded an engineering services contract to Landform Professional Services to complete the necessary Ramsey Town Center AUAR updates, and prepare of a feasibility study for the realignment of Sunwood Drive NW; and

WHEREAS, on July 26, 2011 the Ramsey City Council awarded an engineering services contract to WSB & Associates to prepare a feasibility study for the portion of the Sunwood Drive realignment contained within Anoka County right-of-way, and assist in completing updates to the existing Ramsey Town Center AUAR; and

WHEREAS, on January 24, 2012 the Ramsey City Council accepted the feasibility studies and called for the preparation of plans and specifications for City Improvement Project #12-20; Sunwood Drive Realignment; and

WHEREAS, on May 22, 2012 the Ramsey City Council accepted the plans and specifications and authorized the solicitation of bids; and

WHEREAS, North Pine Aggregates, Inc is the lowest responsible bidder; and

WHEREAS, as of November 27, 2012 \$1,445,325.28 has been paid to date; and

WHEREAS, Engineering Technician IV has inspected the completed work and recommends partial payment to North Pine Aggregate, Inc. in the amount of \$521,084.66.

NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:

- 1) That the City Council hereby authorizes partial payment to North Pine Aggregates, Inc. for Improvement Project #12-20; CSAH 83(Armstrong Boulevard) – Sunwood Drive in the amount of \$521,084.66.
- 2) That the City Council hereby accepts the project and authorizes the Mayor or City Administrator to sign the release form for this payment.
- 3) That the total amount of this payment is not included in resolutions approving payment of bills for the date of November 27, 2012.
- 4) That the City of Ramsey Finance Department will be provided a signed copy of this resolution.

RESOLUTION #12-11-XXX

Page 1 of 2

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 27th day of November 2012.

Mayor

ATTEST:

City Clerk

Pay Estimate Summary Sheet
IP 12-20; CSAH 83 (ARMSTRONG BOULEVARD)- SUNWOOD DRIVE
SAP 199-020-010 199-104-010
City of Ramsey

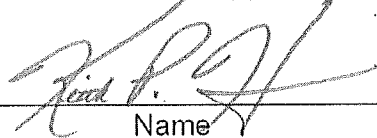
Estimate no 4

TOTAL CONTRACT	\$ 2,342,825.81
CHANGE ORDERS (#1,#2,#3,#4)	\$ 76,940.23
TOTAL WITH CHANGE ORDERS	\$ 2,419,766.05
STORED MATERIALS	
TOTAL, COMPLETED WORK TO DATE	\$ 2,069,905.20
TOTAL WORK COMPLETED PLUS STORED MATERIALS	\$ 2,069,905.20
RETAINED PERCENTAGE 5%	\$ 103,495.26
NET AMOUNT DUE TO CONTRACTOR TO DATE	\$ 1,966,409.94
TOTAL AMOUNT PAID ON PREVIOUS ESTIMATES	\$ 1,445,325.28
PAY CONTRACTOR PER ESTIMATE NO. 4	\$ 521,084.66

Certificate for Partial Payment

I hereby certify that, to the best of my knowledge and belief, all items, quantities and prices of work and material shown on the this Estimate are correct and that all work has been performed in full accordance with the terms and conditions of the Contract for this project between owner and the undersigned Contractor, and as amended by any authorized changes and the foregoing is a true and correct statement of the contract amount for the period covered by this estimate.

Contractor: North Pine Aggregates, Inc.

By  Project Manager
Name Title

Date 11/19/12

CHECKED AND APPROVED AS TO QUANTITIES AND AMOUNT:

ENGINEER: CITY OF RAMSEY

By  Civil Engineer II
Leonard Linton, PE

Date 11/19/12

APPROVED FOR PAYMENT:

OWNER: City of Ramsey

By  INTERIM CITY ENGINEER 11/19/12
Name Title Date

CC Regular Session

4. 11.

Meeting Date: 11/27/2012

Submitted For: MaryJo Warner

By: MaryJo Warner, Engineering/Public Works

Information

Title:

Adopt Resolution #12-11-XXX Authorizing Partial Payment to North Valley Inc. for IP 12-23 Alpine Drive Reconstruction.

Background:

Resolution and Pay Request attached.

Recommendation:

Staff recommends partial payment to North Valley Inc. for IP 12-23 Alpine Drive in the amount of \$225,387.61.

Council Action:

Motion to adopt Resolution #12-11-XXX authorizing partial payment to North Valley Inc. for IP 12-23 Alpine Drive in the amount of \$225,387.61.

Attachments

Resolution 11.27.12

Pay Request 11.27.12

Form Review

Inbox	Reviewed By	Date
Hakanson Anderson Engineering	Shane Nelson	11/21/2012 11:04 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:35 AM
Form Started By: MaryJo Warner		Started On: 11/19/2012 09:16 AM
	Final Approval Date: 11/21/2012	

Councilmember introduced the following resolution and moved for its adoption:

RESOLUTION #12-11-XXX

**RESOLUTION APPROVING 2nd PARTIAL PAYMENT TO NORTH VALLEY INC FOR CITY
PROJECT 12-23,
ALPINE DRIVE RECONSTRUCTION**

WHEREAS, on July 24, 2012 the Ramsey City Council accepted the plans and authorized the solicitation of bids; and

WHEREAS, pursuant to an advertisement for bids for Improvement Project 12-23; Alpine Drive Reconstruction, bids were received, opened and tabulated according to law; and

WHEREAS, North Valley Inc. is the lowest responsible bidder; and

WHEREAS, as of November 27, 2012 \$34,161.08 has been paid to date; and

WHEREAS, Civil Engineer II has inspected the completed work and recommends partial payment to North Valley Inc. in the amount of \$225,387.61.

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF
RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:**

- 1) That the City Council hereby authorizes partial payment to North Valley Inc. for Improvement Project 12-23; Alpine Drive Reconstruction in the amount of \$225,387.61.
- 2) That the City Council hereby accepts the project and authorizes the Mayor or City Administrator to sign the release form for this payment.
- 3) That the total amount of this payment is not included in resolutions approving payment of bills for the date of November 27, 2012.
- 4) That the City of Ramsey Finance Department will be provided a signed copy of this resolution.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

Whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 27th day of November 2012.

Mayor

ATTEST:

City Clerk

Pay Estimate Summary Sheet
IP 12-23; Alpine Drive Reconstruction
SAP 199-107-009
City of Ramsey

Estimate no 2

TOTAL CONTRACT \$ 277,861.67

TOTAL CONTRACT \$ 277,861.67

STORED MATERIALS

TOTAL, COMPLETED WORK TO DATE \$ 273,209.15

TOTAL WORK COMPLETED PLUS STORED MATERIALS \$ 273,209.15

RETAINED PERCENTAGE 5% \$ 13,660.46

NET AMOUNT DUE TO CONTRACTOR TO DATE \$ 259,548.69

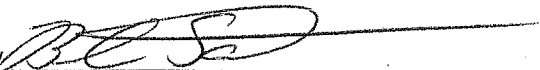
TOTAL AMOUNT PAID ON PREVIOUS ESTIMATES \$ 34,161.08

PAY CONTRACTOR PER ESTIMATE NO. 2 \$ 225,387.61

Certificate for Partial Payment

I hereby certify that, to the best of my knowledge and belief, all items, quantities and prices of work and material shown on the this Estimate are correct and that all work has been performed in full accordance with the terms and conditions of the Contract for this project between owner and the undersigned Contractor, and as amended by any authorized changes and the foregoing is a true and correct statement of the contract amount for the period covered by this estimate.

Contractor: North Valley, Inc

By  President
Name Title

Date 11/15/2012

CHECKED AND APPROVED AS TO QUANTITIES AND AMOUNT:


ENGINEER: CITY OF RAMSEY

By  Civil Engineer II
Leonard Linton, PE

Date 11/15/12

APPROVED FOR PAYMENT:

OWNER: City of Ramsey

By  INTERIM CITY ENGINEER 11/19/12
Name Title Date

CC Regular Session

7. 1.

Meeting Date: 11/27/2012

By: Diana Lund, Finance

Information

Title:

Adopt Ordinance #12-XX Adopting 2013 Schedule of Rates, Fees & Charges

Background:

Effective June 30, 1991, Ordinance #91-08 amended S7.01.10 of the City Code to allow the City Council to annually set fees.

Attached is the proposed 2013 Schedule of Rates, Fees and Charges. The attached schedule shows most fees remaining constant with those adopted in 2012.

Most changes are within the utility section. The sewer and water connection rates have been reduced to reflect the recommendation of the Comprehensive Sewer and Water study that was prepared and adopted in the late spring of this year.

The proposed 2013 schedule was presented at a worksession on October 9, 2012 and the recommendation was to bring forward as is for adoption in November.

This Ordinance was introduced by the City Council at their regular meeting on November 13, 2012.

The 2013 schedule will become effective January 1, 2013.

Recommendation:

Adopt Ordinance #12-XX - 2013 Schedule of Rates, Fees and Charges.

Funding Source:

None Required.

Council Action:

Motion to waive the requirement of the City Charter to read the Ordinance aloud and adopt Ordinance #12-XX Adopting the 2013 Schedule of Rates, Fees and Charges.

Attachments

2013 Schedule of Rates, Fees & Charges

Ordinance for 2013 Fees & Charges

Form Review

Inbox

Kurt Ulrich

Reviewed By

Kurt Ulrich

Date

11/21/2012 11:20 AM

Form Started By: Diana Lund

Started On: 11/16/2012 09:47 AM

Final Approval Date: 11/21/2012

**CITY OF RAMSEY
2013 PROPOSED SCHEDULE OF RATES, FEES AND CHARGES**

	2012 Adopted	2013 Proposed
Gas Water Heater Replacement	47.00	47.00
Lawn Irrigation-Residential	47.00	47.00
Lawn Irrigation-Commercial (Multi)	60.00	60.00
Mechanical Permit-Residential	150.00	150.00
	94.00	94.00
Mechanical permit-Commercial	1% of Job Value/Minimum \$200.00	1% of Job Value/Minimum \$200.00
Remodel/Addition	1% of Job Value/Minimum \$100.00	1% of Job Value/Minimum \$100.00
Plan Check fee (65% of commercial mechanical permit)	65% of Comm Mech Permit fee	65% of Comm Mech Permit fee
Moving Structure Permit	94.00	94.00
Moving Structure Permit Site Inspection	94.00	94.00
Mobile Home Tie Down	45.00	45.00
Mobile Home Water Hookup	60.00	60.00
Outside Consultants-Plan check and/or inspections	Actual Costs	Actual Costs
Permit/Inspection Card-Replacement or Duplicate	5.00	5.00
Plumbing permit- Residential/Single Family	200.00	200.00
	94.00	94.00
Remodel/Addition	1% of Job Value/Minimum \$300.00	1% of Job Value/Minimum \$300.00
Plumbing permit- Commercial	1% of Job Value/Minimum \$150.00	1% of Job Value/Minimum \$150.00
Remodel/Addition	47.00 per trip	47.00 per trip
Relinspection Fees	94.00	94.00
Re-Roofing/Shingling Permit-Residential	LMC Fee Table	LMC Fee Table
Re-Roofing/Shingling Permit-Commercial/Multi-Family	94.00	94.00
Re-Siding Permit	94.00	94.00
Replacement Windows	94.00	94.00
Septic system permit-residential	150.00	150.00
Septic system permit-commercial	250.00	250.00
Septic Tank Installation Permit-Residential	150.00	150.00
Service Availability Charge (SAC)	2,365.00	2,435.00
SAC Handling Fee - per address	25.00	25.00
Septic Tank Pumping Permit	10.00	10.00
Sewer permit (Utility Connection)-Residential	75.00	75.00
Sewer permit (Utility Connection)-Commercial	2% of Job Value/Minimum \$250.00	2% of Job Value/Minimum \$250.00
Site Evaluation Fee	45.00	45.00
Swimming pool permit	125.00	125.00
Vacuum Breaker Verification for Irrigation System (RPZ)-Residential	47.00	47.00
Vacuum Breaker Verification for Irrigation System (RPZ)-Commercial	60.00	60.00
Water Availability Charge (WAC) - per address	1,640.00	1,148.00
Water Softener Permit-residential	15.00	15.00
Water permit (Utility Connection)-Residential	75.00	75.00
Water permit (Utility Connection)-Commercial	2% of Job Value/Minimum \$250.00	2% of Job Value/Minimum \$250.00
Business:		
Administrative Fee - Process Returned Business Permits	25.00	25.00
Business Registration Certificate (BRC)	30.00	30.00
Contractor License	50.00	50.00
Contractor License	50.00	50.00
Garbage & refuse hauler	50.00	50.00
Gasoline sales	250.00	250.00
License Late Fee	15% of license fee/month	15% of license fee/month

	2012 Adopted	2013 Proposed
Gas Water Heater Replacement	47.00	47.00
Lawn Irrigation-Residential	47.00	47.00
Lawn Irrigation-Commercial (Multi)	60.00	60.00
Mechanical Permit-Residential	150.00	150.00
	94.00	94.00
Mechanical permit-Commercial	1% of Job Value/Minimum \$200.00	1% of Job Value/Minimum \$200.00
Remodel/Addition	1% of Job Value/Minimum \$100.00	1% of Job Value/Minimum \$100.00
Plan Check fee (65% of commercial mechanical permit)	65% of Comm Mech Permit fee	65% of Comm Mech Permit fee
Moving Structure Permit	94.00	94.00
Moving Structure Permit Site Inspection	94.00	94.00
Mobile Home Tie Down	45.00	45.00
Mobile Home Water Hookup	60.00	60.00
Outside Consultants-Plan check and/or inspections	Actual Costs	Actual Costs
Permit/Inspection Card-Replacement or Duplicate	5.00	5.00
Plumbing permit- Residential/Single Family	200.00	200.00
	94.00	94.00
Remodel/Addition	1% of Job Value/Minimum \$300.00	1% of Job Value/Minimum \$300.00
Plumbing permit- Commercial	1% of Job Value/Minimum \$150.00	1% of Job Value/Minimum \$150.00
Remodel/Addition	47.00 per trip	47.00 per trip
Relinspection Fees	94.00	94.00
Re-Roofing/Shingling Permit-Residential	LMC Fee Table	LMC Fee Table
Re-Roofing/Shingling Permit-Commercial/Multi-Family	94.00	94.00
Re-Siding Permit	94.00	94.00
Replacement Windows	94.00	94.00
Septic system permit-residential	150.00	150.00
Septic system permit-commercial	250.00	250.00
Septic Tank Installation Permit-Residential	150.00	150.00
Service Availability Charge (SAC)	2,365.00	2,435.00
SAC Handling Fee - per address	25.00	25.00
Septic Tank Pumping Permit	10.00	10.00
Sewer permit (Utility Connection)-Residential	75.00	75.00
Sewer permit (Utility Connection)-Commercial	2% of Job Value/Minimum \$250.00	2% of Job Value/Minimum \$250.00
Site Evaluation Fee	45.00	45.00
Swimming pool permit	125.00	125.00
Vacuum Breaker Verification for Irrigation System (RPZ)-Residential	47.00	47.00
Vacuum Breaker Verification for Irrigation System (RPZ)-Commercial	60.00	60.00
Water Availability Charge (WAC) - per address	1,640.00	1,148.00
Water Softener Permit-residential	15.00	15.00
Water permit (Utility Connection)-Residential	75.00	75.00
Water permit (Utility Connection)-Commercial	2% of Job Value/Minimum \$250.00	2% of Job Value/Minimum \$250.00
Business:		
Administrative Fee - Process Returned Business Permits	25.00	25.00
Business Registration Certificate (BRC)	30.00	30.00
Contractor License	50.00	50.00
Contractor License	50.00	50.00
Garbage & refuse hauler	50.00	50.00
Gasoline sales	250.00	250.00
License Late Fee	15% of license fee/month	15% of license fee/month

CITY OF RAMSEY
2013 PROPOSED SCHEDULE OF RATES, FEES AND CHARGES

	2013 Proposed
Plan Review	Minimum of 23.50 65% of permit fee
	\$5.00 Minimum State Surcharge
Fire Alarm Fees:	
Permit	Valuation based/1997 UBC Fee Table 1-A
Plan Review	Minimum of 23.50 65% of permit fee
	\$5.00 Minimum State Surcharge
Temporary Assembly/Tent Permit(Greater than 100 sq. ft.)	50.00/per tent
Aboveground Tank Storage	100.00/per tank
Underground Tank Storage	100.00/per tank
Fuel Tank Storage Removal	100.00/per tank
Daycare Inspection Fee	50.00
Permit Re-Inspection Fee	50.00
Fireworks/Pyrotechnic Display Permit	200.00
Fireworks-Retail Sale Permit - Retailer only selling fireworks	
Fireworks-Retail Sale Permit-All other retailers	
Double Permit Fee	2 x normal permit fee
Lock Box Fee:	
Surface Mount	215.00
Recessed Box	255.00
All Others	Actual cost + 10% Admin fee
Fire Code Re-Inspection Fee	140.00
Gas Line Hft By Contractors	300.00
Open Burn Permits:	
Open burn permit application	25.00
	50.00
Open burn permit fire suppression escort	500.00
Open burn permit fire suppression services	300.00/hr - min 300.00
Illegal Burning	200.00
Subsequent Site Evaluations	45.00/visit
Fire Service Fee:	
Disaster Assistance	300.00/hr per truck
Ordinance Violations	300.00/hr
Victim Service Fee	300.00/hr per truck
Hunting Permit (Bow) per person	5.00
Hunting Permit (Shotgun) per person	5.00
Hunting Permit (Target Shooting) per person	5.00
Letter of Good Standing	10.00
Personal Injury Accident/Extrication Charge for Non-Residents	500.00
Photo CD	20.00/cd
Photo Electronic Transfers	10.00/transmission
Photo Reprints	5.00 min chg + 1.00/print
Vehicle Lockout	20.00

	2012 Adopted	SPECIAL NOTES
Plan Review	Minimum of 23.50 65% of permit fee	
	\$5.00 Minimum State Surcharge	
Fire Alarm Fees:		
Permit	Valuation based/1997 UBC Fee Table 1-A	
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	\$5.00 Minimum State Surcharge	
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Fireworks-Retail Sale Permit-All other retailers		
Double Permit Fee	2 x normal permit fee	
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Surface Mount	215.00	
Recessed Box	255.00	
All Others	Actual cost + 10% Admin fee	
Fire Code Re-Inspection Fee	140.00	
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Open Burn Permits:		
Open burn permit application	25.00	
	50.00	
Open burn permit fire suppression escort	500.00	
Open burn permit fire suppression services	300.00/hr - min 300.00	
Illegal Burning	200.00	
Subsequent Site Evaluations	45.00/visit	
Fire Service Fee:		
Disaster Assistance	300.00/hr per truck	
Ordinance Violations	300.00/hr	
Victim Service Fee	300.00/hr per truck	
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Hunting Permit (Shotgun) per person	5.00	
Hunting Permit (Target Shooting) per person	5.00	
Letter of Good Standing	10.00	
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Photo CD	20.00/cd	
Photo Electronic Transfers	10.00/transmission	
Photo Reprints	5.00 min chg + 1.00/print	
Vehicle Lockout	20.00	

CITY OF RAMSEY
2013 PROPOSED SCHEDULE OF RATES, FEES AND CHARGES

SERVICE OR LICENSE	SPECIAL NOTES	2012 Adopted	2013 Proposed
Vehicle Storage		10.00/day	10.00/day
Room Dismountments			
Mileage/personal vehicle (IRS allowable rate)	Always follow IRS adopted rate	.555/mile	.555/mile
Right-of-Way			
Permit Fee - Boring/Open Trench		\$95+\$.15/Driveway, \$20/Road Closing + \$10,000 Bond per mile	\$95+\$.15/Driveway, \$20/Road Closing + \$10,000 Bond per mile
Boulevard Tree Replacement		\$700/Tree	\$700/Tree
Permit Fee - Overhead		\$75+\$.05/linear foot	\$75+\$.05/linear foot
Street and Traffic Changes			
Developer contribution:			
street and/or traffic signs	Developer Installed		
Bituminous Paving/Patching	25.00/sq yard		
Culverts	Price + Tax		
Street Sweeping: Equipment Only:	74.00/Hr		
Tobacco	Per FEMA Schedule of Equipment Rates		
Cigarette Sales (vending and over-the counter)		250.00	250.00
Cigarette Sales with age verification technology		150.00	150.00
UTILITY RATES			
Assessment Charge for Unpaid Items Assessed	Administration fee	\$75/ACCT	\$76/ACCT
Interest Rate on Unpaid Items Assessed	Past due on current billing	10%	10%
Penalty/late payment			
Recycling Rates	Per Ace Contract	8.85/qr	8.85/qr
Residential Curb-side Program fee/qr			
Sewer Rates			
Residential/Single dwelling usage fee/qr	Per 2012 Comprehensive Water System Study	66.79	66.79
Multi-family/apt) & commercial usage fee/qr		66.79+2.89/1000 gal. in excess of 20,000 gal.	66.79+2.89/1000 gal. in excess of 20,000 gal.
Connection charge/res. equivalent	Per 2012 Comprehensive Water System Study	1,271.00	1,099.00
Connection charge/acre comm./ind.	Per 2012 Comprehensive Water System Study	3,824.00	3,398.00
Lateral Benefit Charges	Per 2012 Comprehensive Water System Study	3,847.00	3,328.00
Street Lights			
Developer contribution:			
installation of Cobra street light	1300.00/light		
traditional subdivision street light (w/100 ft of wire)	Developer Installed		
New ROW COR Lights	\$2,600/light		
street light O & M 3 year @ \$98/year	\$294/light		
Residential billing:			
urban subdivision street light rate per lot (>7/92)		9.01/qr	9.01/qr
rural subdivision street light rate per lot		14.85/qr	14.85/qr
priority street light rate per residential lot		1.37/qr	1.37/qr

SERVICE OR LICENSE	SPECIAL NOTES	2012 Adopted	2013 Proposed
Vehicle Storage		10.00/day	10.00/day
Room Dismountments			
Mileage/personal vehicle (IRS allowable rate)	Always follow IRS adopted rate	.555/mile	.555/mile
Right-of-Way			
Permit Fee - Boring/Open Trench		\$95+\$.15/Driveway, \$20/Road Closing + \$10,000 Bond per mile	\$95+\$.15/Driveway, \$20/Road Closing + \$10,000 Bond per mile
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Street and Traffic Changes			
Developer contribution:			
street and/or traffic signs	Developer Installed		
Bituminous Paving/Patching	25.00/sq yard		
Culverts	Price + Tax		
Street Sweeping: Equipment Only:	74.00/Hr		
Tobacco	Per FEMA Schedule of Equipment Rates		
Cigarette Sales (vending and over-the counter)		250.00	250.00
Cigarette Sales with age verification technology		150.00	150.00
UTILITY RATES			
Assessment Charge for Unpaid Items Assessed	Administration fee	\$75/ACCT	\$76/ACCT
Interest Rate on Unpaid Items Assessed	Past due on current billing	10%	10%
Penalty/late payment			
Recycling Rates	Per Ace Contract	8.85/qr	8.85/qr
Residential Curb-side Program fee/qr			
Sewer Rates			
Residential/Single dwelling usage fee/qr	Per 2012 Comprehensive Water System Study	66.79	66.79
Multi-family/apt) & commercial usage fee/qr		66.79+2.89/1000 gal. in excess of 20,000 gal.	66.79+2.89/1000 gal. in excess of 20,000 gal.
Connection charge/res. equivalent	Per 2012 Comprehensive Water System Study	1,271.00	1,099.00
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Residential billing:			
urban subdivision street light rate per lot (>7/92)		9.01/qr	9.01/qr
rural subdivision street light rate per lot		14.85/qr	14.85/qr
priority street light rate per residential lot		1.37/qr	1.37/qr

FEMA's SCHEDULE OF EQUIPMENT RATES

**DEPARTMENT OF HOMELAND SECURITY
FEDERAL EMERGENCY MANAGEMENT AGENCY
RECOVERY DIRECTORATE
PUBLIC ASSISTANCE DIVISION
WASHINGTON, D.C. 20472**

The rates on this Schedule of Equipment Rates are for applicant-owned equipment in good mechanical condition, complete with all required attachments. Each rate covers all costs eligible under the Robert T. Stafford Disaster Relief and Emergency Assistance Act, 42 U.S.C. § 5121, et seq., for ownership and operation of equipment, including depreciation, overhead, all maintenance, field repairs, fuel, lubricants, tires, OSHA equipment and other costs incidental to operation. Standby equipment costs are not eligible.

Equipment must be in actual operation performing eligible work in order for reimbursement to be eligible. LABOR COSTS OF OPERATOR ARE NOT INCLUDED in the rates and should be approved separately from equipment costs.

Information regarding the use of the Schedule is contained in 44 CFR § 206.228 Allowable Costs. Rates for equipment not listed will be furnished by FEMA upon request. Any appeals shall be in accordance with 44 CFR § 206.208 Appeals.

THESE RATES ARE APPLICABLE TO MAJOR DISASTERS AND EMERGENCIES DECLARED BY THE PRESIDENT ON OR AFTER SEPTEMBER 15, 2010.

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8490	Aerial Lift, Self-Propelled	Max. Platform Height	37 ft	to 15	Articulated, Telescoping, Scissor.	hour	\$8.25
8491	Aerial Lift, Self-Propelled	Max. Platform Height	60 ft	to 30	Articulated, Telescoping, Scissor.	hour	\$12.25
8492	Aerial Lift, Self-Propelled	Max. Platform Height	70 ft	to 50	Articulated, Telescoping, Scissor.	hour	\$21.00
8493	Aerial Lift, Self-Propelled	Max. Platform Height	125 ft	to 85	Articulated and Telescoping.	hour	\$55.00
8494	Aerial Lift, Self-Propelled	Max. Platform Height	150 ft	to 130	Articulated and Telescoping.	hour	\$87.00
8486	Aerial Lift, Truck Mntd	Max. Platform Height	40 ft		Articulated and Telescoping. Add to Truck rate for total rate.	hour	\$6.75
8487	Aerial Lift, Truck Mntd	Max. Platform Height	61 ft		Articulated and Telescoping. Add to Truck rate for total rate.	hour	\$12.25
8488	Aerial Lift, Truck Mntd	Max. Platform Height	80 ft		Articulated and Telescoping. Add to Truck rate for total rate.	hour	\$23.50
8489	Aerial Lift, Truck Mntd	Max. Platform Height	100 ft		Articulated and Telescoping. Add to Truck rate for total rate.	hour	\$34.00
8010	Air Compressor	Air Delivery	41 cfm	to 10	Hoses included.	hour	\$1.50
8011	Air Compressor	Air Delivery	103 cfm	to 30	Hoses included.	hour	\$7.00
8012	Air Compressor	Air Delivery	130 cfm	to 50	Hoses included.	hour	\$9.25
8013	Air Compressor	Air Delivery	175 cfm	to 90	Hoses included.	hour	\$20.00
8014	Air Compressor	Air Delivery	400 cfm	to 145	Hoses included.	hour	\$27.50
8015	Air Compressor	Air Delivery	575 cfm	to 230	Hoses included.	hour	\$45.50
8016	Air Compressor	Air Delivery	1100 cfm	to 355	Hoses included.	hour	\$51.00
8017	Air Compressor	Air Delivery	1600 cfm	to 500	Hoses included.	hour	\$80.00
8040	Ambulance			to 150		hour	\$25.50
8041	Ambulance			to 210		hour	\$32.50
8060	Auger, Portable	Hole Diameter	16 in	to 6		hour	\$1.30
8061	Auger, Portable	Hole Diameter	18 in	to 13		hour	\$3.50
8062	Auger, Tractor Mntd	Max. Auger Diameter	36 in	to 13	Includes digger, boom and mounting hardware. Add to Tractor rate for total rate.	hour	\$1.30
8063	Auger, Truck Mntd	Max. Auger Size	24 in	to 100	Includes digger, boom and mounting hardware. Add to Truck rate for total rate.	hour	\$29.00
8070	Automobile			to 130	Transporting people.	mile	\$0.50
8071	Automobile			to 130	Transporting cargo.	hour	\$13.00
8072	Automobile, Police			to 250	Patrolling.	mile	\$0.60
8073	Automobile, Police			to 250	Stationary with engine running.	hour	\$16.25
8110	Barge, Deck	Size	50'x35'x7.25'			hour	\$34.00
8111	Barge, Deck	Size	50'x35'x9'			hour	\$49.00
8112	Barge, Deck	Size	120'x45'x10'			hour	\$60.00
8113	Barge, Deck	Size	160'x45'x11'			hour	\$75.00
8050	Board, Arrow			to 8	Trailer Mounted.	hour	\$3.15
8051	Board, Message			to 5	Trailer Mounted.	hour	\$8.50
8133	Boat, Push	Size	45'x21'x6'	to 435	Flat hull.	hour	\$150.00
8134	Boat, Push	Size	54'x21'x8'	to 525	Flat hull.	hour	\$200.00
8135	Boat, Push	Size	58'x24'x7.5'	to 705	Flat hull.	hour	\$250.00
8136	Boat, Push	Size	64'x25'x8'	to 870	Flat hull.	hour	\$300.00

FEMA's SCHEDULE OF EQUIPMENT RATES

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8130	Boat, Row				Heavy duty.	hour	\$0.85
8131	Boat, Runabout	Size	13'x5'	to 50	Outboard.	hour	\$14.00
8132	Boat, Tender	Size	14'x7'	to 100	Inboard with 360 degree drive.	hour	\$28.00
8120	Boat, Tow	Size	55'x20'x5'	to 870	Steel.	hour	\$250.00
8121	Boat, Tow	Size	60'x21'x5'	to 1050	Steel.	hour	\$300.00
8122	Boat, Tow	Size	70'x30'x7.5'	to 1350	Steel.	hour	\$450.00
8123	Boat, Tow	Size	120'x34'x8'	to 2000	Steel.	hour	\$830.00
8140	Boat, Tug	Length	16 ft	to 100		hour	\$33.50
8141	Boat, Tug	Length	18 ft	to 175		hour	\$53.00
8142	Boat, Tug	Length	26 ft	to 250		hour	\$65.00
8143	Boat, Tug	Length	40 ft	to 380		hour	\$150.00
8144	Boat, Tug	Length	61 ft	to 700		hour	\$225.00
8419	Breaker, Pavement, Hand-Held	Weight	25-90 lb			hour	\$0.65
8420	Breaker, Pavement			to 70		hour	\$31.25
8150	Broom, Pavement	Broom Length	72 in	to 35		hour	\$12.30
8151	Broom, Pavement	Broom Length	86 in	to 100		hour	\$19.75
8153	Broom, Pavement, Mntd	Broom Length	72 in	to 18	Add to Prime Mover rate for total rate.	hour	\$8.00
8154	Broom, Pavement, Pull	Broom Length	84 in	to 20	Add to Prime Mover rate for total rate.	hour	\$10.25
8270	Bucket, Clamshell	Capacity	1.0 cy		Includes teeth. Does not include Clamshell & Dragline.	hour	\$3.60
8271	Bucket, Clamshell	Capacity	2.5 cy		Includes teeth. Does not include Clamshell & Dragline.	hour	\$6.75
8272	Bucket, Clamshell	Capacity	5.0 cy		Includes teeth. Does not include Clamshell & Dragline.	hour	\$11.25
8273	Bucket, Clamshell	Capacity	7.5 cy		Includes teeth. Does not include Clamshell & Dragline.	hour	\$14.50
8275	Bucket, Dragline	Capacity	2.0 cy		Does not include Clamshell & Dragline.	hour	\$2.80
8276	Bucket, Dragline	Capacity	5.0 cy		Does not include Clamshell & Dragline.	hour	\$6.60
8277	Bucket, Dragline	Capacity	10 cy		Does not include Clamshell & Dragline.	hour	\$10.50
8278	Bucket, Dragline	Capacity	14 cy		Does not include Clamshell & Dragline.	hour	\$13.60
8180	Bus			to 150		hour	\$20.00
8181	Bus			to 210		hour	\$23.00
8182	Bus			to 300		hour	\$27.00
8190	Chain Saw	Bar Length	18 in			hour	\$1.75
8191	Chain Saw	Bar Length	25 in			hour	\$3.20
8192	Chain Saw, Pole	Bar Size	18 in			hour	\$1.60
8200	Chipper, Brush	Chipping Capacity	6 in	to 35	Trailer Mounted.	hour	\$7.50
8201	Chipper, Brush	Chipping Capacity	9 in	to 65	Trailer Mounted.	hour	\$16.00
8202	Chipper, Brush	Chipping Capacity	12 in	to 100	Trailer Mounted.	hour	\$21.75
8203	Chipper, Brush	Chipping Capacity	15 in	to 125	Trailer Mounted.	hour	\$30.75
8204	Chipper, Brush	Chipping Capacity	18 in	to 200	Trailer Mounted.	hour	\$45.50
8210	Clamshell & Dragline, Crawler		149,999 lb	to 235	Bucket not included in rate.	hour	\$86.00
8211	Clamshell & Dragline, Crawler		250,000 lb	to 520	Bucket not included in rate.	hour	\$121.00
8212	Clamshell & Dragline, Truck			to 240	Bucket not included in rate.	hour	\$130.00
8712	Cleaner, Sewer/Catch Basin	Hopper Capacity	5 cy		Truck Mounted. Add to Truck rate for total rate.	hour	\$16.00
8713	Cleaner, Sewer/Catch Basin	Hopper Capacity	14 cy		Truck Mounted. Add to Truck rate for total rate.	hour	\$21.50
8220	Compactor			to 10		hour	\$11.00
8221	Compactor, Towed, Vibratory Drum			to 45		hour	\$17.50
8222	Compactor, Vibratory, Drum			to 75		hour	\$25.00
8223	Compactor, Pneumatic, Wheel			to 100		hour	\$29.00
8225	Compactor, Sanitation			to 300		hour	\$86.00
8226	Compactor, Sanitation			to 400		hour	\$183.00
8227	Compactor, Sanitation			to 535		hour	\$225.00
8228	Compactor, Towed, Pneumatic, Wheel		10000 lb		Add to Prime Mover rate for total rate.	hour	\$7.50

FEMA's SCHEDULE OF EQUIPMENT RATES

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8229	Compactor, Towed, Drum Static		20000 lb		Add to Prime Mover rate for total rate.	hour	\$12.25
8500	Crane	Max. Lift Capacity	8 MT	to 80		hour	\$27.00
8501	Crane	Max. Lift Capacity	15 MT	to 150		hour	\$55.00
8502	Crane	Max. Lift Capacity	50 MT	to 200		hour	\$95.00
8503	Crane	Max. Lift Capacity	70 MT	to 300		hour	\$155.00
8504	Crane	Max. Lift Capacity	110 MT	to 350		hour	\$220.00
8498	Crane, Truck Mntd	Max. Lift Capacity	24000 lb		Add to Truck rate for total rate.	hour	\$10.00
8497	Crane, Truck Mntd	Max. Lift Capacity	36000 lb		Add to Truck rate for total rate.	hour	\$18.00
8498	Crane, Truck Mntd	Max. Lift Capacity	60000 lb		Add to Truck rate for total rate.	hour	\$30.00
8195	Cutter, Brush	Cutter Size	8 ft	to 150		hour	\$90.00
8196	Cutter, Brush	Cutter Size	8 ft	to 190		hour	\$100.00
8197	Cutter, Brush	Cutter Size	10 ft	to 245		hour	\$120.00
8670	Derrick, Hydraulic Digger	Max. Boom Length	60 ft		Includes hydraulic pole alignment attachment. Add to Truck rate.	hour	\$21.00
8671	Derrick, Hydraulic Digger	Max. Boom Length	90 ft		Includes hydraulic pole alignment attachment. Add to Truck rate.	hour	\$39.00
8580	Distributor, Asphalt	Tank Capacity	500 gal		Insulated tank, and circulating spray bar.	hour	\$12.00
8581	Distributor, Asphalt	Tank Capacity	1000 gal		Truck Mounted. Includes burners, insulated tank, and circulating spray bar. Add to Truck rate.	hour	\$13.00
8582	Distributor, Asphalt	Tank Capacity	4000 gal		Truck Mounted. Includes burners, insulated tank, and circulating spray bar. Add to Truck rate.	hour	\$25.00
8250	Dozer, Crawler			to 75		hour	\$31.00
8251	Dozer, Crawler			to 105		hour	\$40.00
8252	Dozer, Crawler			to 160		hour	\$85.00
8253	Dozer, Crawler			to 250		hour	\$80.00
8254	Dozer, Crawler			to 360		hour	\$135.00
8255	Dozer, Crawler			to 565		hour	\$250.00
8256	Dozer, Crawler			to 850		hour	\$340.00
8260	Dozer, Wheel			to 300		hour	\$55.00
8261	Dozer, Wheel			to 400		hour	\$110.00
8262	Dozer, Wheel			to 500		hour	\$150.00
8263	Dozer, Wheel			to 625		hour	\$200.00
8280	Excavator, Hydraulic	Bucket Capacity	0.5 cy	to 45	Crawler, Truck & Wheel. Includes bucket.	hour	\$18.00
8281	Excavator, Hydraulic	Bucket Capacity	1.0 cy	to 90	Crawler, Truck & Wheel. Includes bucket.	hour	\$39.00
8282	Excavator, Hydraulic	Bucket Capacity	1.5 cy	to 160	Crawler, Truck & Wheel. Includes bucket.	hour	\$65.00
8283	Excavator, Hydraulic	Bucket Capacity	2.5 cy	to 265	Crawler, Truck & Wheel. Includes bucket.	hour	\$120.00
8284	Excavator, Hydraulic	Bucket Capacity	4.5 cy	to 420	Crawler, Truck & Wheel. Includes bucket.	hour	\$200.00
8285	Excavator, Hydraulic	Bucket Capacity	7.5 cy	to 650	Crawler, Truck & Wheel. Includes bucket.	hour	\$240.00
8286	Excavator, Hydraulic	Bucket Capacity	12 cy	to 1000	Crawler, Truck & Wheel. Includes bucket.	hour	\$400.00
8240	Feeder, Grizzly			to 35		hour	\$17.00
8241	Feeder, Grizzly			to 55		hour	\$30.00
8242	Feeder, Grizzly			to 75		hour	\$44.00
8300	Fork Lift	Capacity	6000 lb	to 60		hour	11.75
8301	Fork Lift	Capacity	12000 lb	to 90		hour	\$17.00
8302	Fork Lift	Capacity	18000 lb	to 140		hour	\$23.00
8303	Fork Lift	Capacity	50000 lb	to 215		hour	\$50.00
8310	Generator	Prime Output	5.5 kW	to 10		hour	\$3.25
8311	Generator	Prime Output	16 kW	to 25		hour	\$8.00
8312	Generator	Prime Output	43 kW	to 65		hour	\$17.00
8313	Generator	Prime Output	100 kW	to 125		hour	\$34.00
8314	Generator	Prime Output	160 kW	to 240		hour	\$50.00
8315	Generator	Prime Output	210 kW	to 300		hour	\$60.00
8316	Generator	Prime Output	280 kW	to 400		hour	\$85.00

FEMA's SCHEDULE OF EQUIPMENT RATES

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8317	Generator	Prime Output	350 kW	to 500		hour	\$95.00
8318	Generator	Prime Output	530 kW	to 750		hour	\$150.00
8319	Generator	Prime Output	710 kW	to 1000		hour	\$200.00
8320	Generator	Prime Output	1100 kW	to 1500		hour	\$375.00
8321	Generator	Prime Output	2500 kW	to 3000		hour	\$500.00
8755	Golf Cart	Capacity	2 person			hour	\$3.20
8330	Graders	Moldboard Size	10 ft	to 110	Includes Rigid and Articulate	hour	\$34.50
8331	Graders	Moldboard Size	12 ft	to 150	Includes Rigid and Articulate	hour	\$58.00
8332	Graders	Moldboard Size	14 ft	to 225	Includes Rigid and Articulate	hour	\$70.00
8350	Hose, Discharge	Diameter	3 in		Per 25 foot length. Includes couplings.	hour	\$0.13
8351	Hose, Discharge	Diameter	4 in		Per 25 foot length. Includes couplings.	hour	\$0.19
8352	Hose, Discharge	Diameter	6 in		Per 25 foot length. Includes couplings.	hour	\$0.50
8353	Hose, Discharge	Diameter	8 in		Per 25 foot length. Includes couplings.	hour	\$0.75
8354	Hose, Discharge	Diameter	12 in		Per 25 foot length. Includes couplings.	hour	\$1.35
8355	Hose, Discharge	Diameter	16 in		Per 25 foot length. Includes couplings.	hour	\$2.20
8356	Hose, Suction	Diameter	3 in		Per 25 foot length. Includes couplings.	hour	\$0.23
8357	Hose, Suction	Diameter	4 in		Per 25 foot length. Includes couplings.	hour	\$0.43
8358	Hose, Suction	Diameter	6 in		Per 25 foot length. Includes couplings.	hour	\$0.90
8359	Hose, Suction	Diameter	8 in		Per 25 foot length. Includes couplings.	hour	\$1.35
8360	Hose, Suction	Diameter	12 in		Per 25 foot length. Includes couplings.	hour	\$2.45
8361	Hose, Suction	Diameter	16 in		Per 25 foot length. Includes couplings.	hour	\$3.90
8517	Jackhammer (Dry)	Weight Class	25-45 lb			hour	\$1.00
8518	Jackhammer (Wet)	Weight Class	30-55 lb			hour	\$1.15
8380	Loader, Crawler	Bucket Capacity	0.5 cy	to 32	Includes bucket.	hour	\$11.50
8381	Loader, Crawler	Bucket Capacity	1 cy	to 60	Includes bucket.	hour	\$19.00
8382	Loader, Crawler	Bucket Capacity	2 cy	to 118	Includes bucket.	hour	\$42.00
8383	Loader, Crawler	Bucket Capacity	3 cy	to 178	Includes bucket.	hour	\$78.00
8384	Loader, Crawler	Bucket Capacity	4 cy	to 238	Includes bucket.	hour	\$115.00
8540	Loader, Skid-Steer	Operating Capacity	1000 lb	to 35		hour	\$11.00
8541	Loader, Skid-Steer	Operating Capacity	2000 lb	to 65		hour	\$18.00
8542	Loader, Skid-Steer	Operating Capacity	3000 lb	to 85		hour	\$22.00
8401	Loader, Tractor, Wheel			to 81		hour	\$25.00
8390	Loader, Wheel	Bucket Capacity	0.5 cy	to 38		hour	\$15.50
8391	Loader, Wheel	Bucket Capacity	1 cy	to 60		hour	\$21.50
8392	Loader, Wheel	Bucket Capacity	2 cy	to 105		hour	\$28.75
8393	Loader, Wheel	Bucket Capacity	3 cy	to 152		hour	\$40.00
8394	Loader, Wheel	Bucket Capacity	4 cy	to 200		hour	\$52.00
8395	Loader, Wheel	Bucket Capacity	5 cy	to 250		hour	\$68.00
8396	Loader, Wheel	Bucket Capacity	6 cy	to 305		hour	\$82.00
8397	Loader, Wheel	Bucket Capacity	7 cy	to 360		hour	\$95.00
8398	Loader, Wheel	Bucket Capacity	8 cy	to 530		hour	\$140.00
8570	Loader-Backhoe, Wheel	Loader Bucket Capacity	0.5 cy	to 40	Loader and Backhoe Buckets Included.	hour	\$14.75
8571	Loader-Backhoe, Wheel	Loader Bucket Capacity	1 cy	to 70	Loader and Backhoe Buckets Included.	hour	\$23.50
8572	Loader-Backhoe, Wheel	Loader Bucket Capacity	1.5 cy	to 95	Loader and Backhoe Buckets Included.	hour	\$33.00
8573	Loader-Backhoe, Wheel	Loader Bucket Capacity	1.75 cy	to 115	Loader and Backhoe Buckets Included.	hour	\$38.00
8410	Mixer, Concrete Portable	Batching Capacity	10 cft			hour	\$3.25
8411	Mixer, Concrete Portable	Batching Capacity	12 cft			hour	\$4.25
8412	Mixer, Concrete, Trailer Mntd	Batching Capacity	11 cft	to 10		hour	\$8.75
8413	Mixer, Concrete, Trailer Mntd	Batching Capacity	16 cft	to 25		hour	\$15.25
8075	Motorcycle, Police					mile	\$0.35
8633	Mulcher, Trailer Mntd	Working Capacity	7 tph	to 35		hour	\$10.25
8634	Mulcher, Trailer Mntd	Working Capacity	10 tph	to 55		hour	\$15.75
8635	Mulcher, Trailer Mntd	Working Capacity	20 tph	to 120		hour	\$24.75
8430	Paver, Asphalt, Towed				Does not include Prime Mover.	hour	\$7.00
8431	Paver, Asphalt			to 50	Includes wheel and crawler equipment.	hour	\$65.00
8432	Paver, Asphalt			to 125	Includes wheel and crawler equipment.	hour	\$115.00
8433	Paver, Asphalt			to 175	Includes wheel and crawler equipment.	hour	\$125.00
8434	Paver, Asphalt			to 250	Includes wheel and crawler equipment.	hour	\$140.00
8436	Pick-up, Asphalt			to 110		hour	\$55.00
8437	Pick-up, Asphalt			to 150		hour	\$83.00
8438	Pick-up, Asphalt			to 200		hour	\$110.00
8439	Pick-up, Asphalt			to 275		hour	\$140.00
8660	Plow, Cable	Plow Depth	24 in	to 30		hour	\$10.25

FEMA's SCHEDULE OF EQUIPMENT RATES

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8681	Plow, Cable	Plow Depth	36 in	to 65		hour	\$27.75
8682	Plow, Cable	Plow Depth	48 in	to 110		hour	\$31.75
8450	Plow, Snow, Grader Mntd	Width	to 10 ft		Add to Grader for total rate.	hour	\$18.00
8451	Plow, Snow, Grader Mntd	Width	to 14 ft		Add to Grader for total rate.	hour	\$24.00
8452	Plow, Snow, Truck Mntd	Width	to 15 ft		Add to Truck rate for total rate.	hour	\$10.75
8453	Plow, Snow, Truck Mntd	Width	to 15 ft		With leveling wing. Add to Truck rate for total rate.	hour	\$18.50
8470	Pump			to 4	Does not include Hoses.	hour	\$2.16
8471	Pump			to 6	Does not include Hoses.	hour	\$3.20
8472	Pump			to 10	Does not include Hoses.	hour	\$4.10
8473	Pump			to 15	Does not include Hoses.	hour	\$7.75
8474	Pump			to 25	Does not include Hoses.	hour	\$9.25
8475	Pump			to 40	Does not include Hoses.	hour	\$16.00
8476	Pump			to 60	Does not include Hoses.	hour	\$18.75
8477	Pump			to 95	Does not include Hoses.	hour	\$28.50
8478	Pump			to 140	Does not include Hoses.	hour	\$31.00
8479	Pump			to 200	Does not include Hoses.	hour	\$36.00
8480	Pump			to 275	Does not include Hoses.	hour	\$80.00
8481	Pump			to 350	Does not include Hoses.	hour	\$95.00
8482	Pump			to 425	Does not include Hoses.	hour	\$120.00
8483	Pump			to 500	Does not include Hoses.	hour	\$135.00
8484	Pump			to 575	Does not include Hoses.	hour	\$155.00
8485	Pump			to 650	Does not include Hoses.	hour	\$180.00
8510	Saw, Concrete	Blade Diameter	14 in	to 14		hour	\$6.00
8511	Saw, Concrete	Blade Diameter	26 in	to 35		hour	\$13.50
8512	Saw, Concrete	Blade Diameter	48 in	to 65		hour	\$23.00
8513	Saw, Rock			to 100		hour	\$30.00
8514	Saw, Rock			to 200		hour	\$60.00
8521	Scraper	Scraper Capacity	16 cy	to 250		hour	\$90.00
8522	Scraper	Scraper Capacity	23 cy	to 365		hour	\$130.00
8523	Scraper	Scraper Capacity	34 cy	to 475		hour	\$200.00
8524	Scraper	Scraper Capacity	44 cy	to 600		hour	\$240.00
8560	Snow Blower	Capacity	2,000 tph	to 400		hour	\$140.00
8561	Snow Blower	Capacity	2,500 tph	to 500		hour	\$160.00
8562	Snow Blower	Capacity	3,500 tph	to 600		hour	\$180.00
8550	Snow Blower, Truck Mntd	Capacity	600 tph	to 75	Does not include Truck.	hour	\$37.50
8551	Snow Blower, Truck Mntd	Capacity	1400 tph	to 200	Does not include Truck.	hour	\$70.00
8552	Snow Blower, Truck Mntd	Capacity	2000 tph	to 340	Does not include Truck.	hour	\$110.00
8553	Snow Blower, Truck Mntd	Capacity	2500 tph	to 400	Does not include Truck.	hour	\$120.00
8558	Snow Thrower, Walk Behind	Cutting Width	25 in	to 5		hour	\$3.25
8559	Snow Thrower, Walk Behind	Cutting Width	60 in	to 15		hour	\$7.00
8630	Sprayer, Seed	Working Capacity	750 gal	to 30	Trailer & Truck mounted. Does not include Prime Mover.	hour	\$9.75
8631	Sprayer, Seed	Working Capacity	1250 gal	to 50	Trailer & Truck mounted. Does not include Prime Mover.	hour	\$15.00
8632	Sprayer, Seed	Working Capacity	3500 gal	to 115	Trailer & Truck mounted. Does not include Prime Mover.	hour	\$25.75
8458	Spreader, Chemical	Capacity	5 cy	to 4	Trailer & Truck mounted. Does not	hour	\$4.00
8423	Spreader, Chip	Spread Hopper Width	12.5 ft	to 152		hour	\$50.00
8424	Spreader, Chip	Spread Hopper Width	16.5 ft	to 215		hour	\$80.00
8425	Spreader, Chip, Mntd	Hopper Size	8 ft	to 8	Trailer & Truck mounted.	hour	\$3.30
8455	Spreader, Sand	Mounting	Tallgate, Chassis			hour	\$3.30
8456	Spreader, Sand	Mounting	Dump Body			hour	\$5.50
8457	Spreader, Sand	Mounting	Truck (10 yd)			hour	\$7.50
8440	Striper	Paint Capacity	40 gal	to 22		hour	\$8.75
8441	Striper	Paint Capacity	90 gal	to 60		hour	\$19.00
8442	Striper	Paint Capacity	120 gal	to 122		hour	\$37.00
8445	Striper, Truck Mntd	Paint Capacity	120 gal	to 460		hour	\$70.00
8446	Striper, Walk-behind	Paint Capacity	12 gal			hour	\$3.35
8157	Sweeper, Pavement			to 110		hour	\$59.00
8158	Sweeper, Pavement			to 230		hour	\$74.00
8590	Trailer, Dump	Capacity	20 cy		Does not include Prime Mover.	hour	\$8.00

FEMA's SCHEDULE OF EQUIPMENT RATES

Cost Code	Equipment	Specification	Capacity/Size	HP	Notes	Unit	Rate
8591	Trailer, Dump	Capacity	30 cy		Does not include Prime Mover.	hour	\$14.00
8600	Trailer, Equipment	Capacity	30 ton			hour	\$10.25
8601	Trailer, Equipment	Capacity	40 ton			hour	\$12.50
8602	Trailer, Equipment	Capacity	60 ton			hour	\$15.00
8603	Trailer, Equipment	Capacity	120 ton			hour	\$25.00
8640	Trailer, Office	Trailer Size	8' x 24'			hour	\$1.70
8641	Trailer, Office	Trailer Size	8' x 32'			hour	\$1.75
8642	Trailer, Office	Trailer Size	10' x 32'			hour	\$2.60
8610	Trailer, Water	Tank Capacity	4000 gal		Includes a centrifugal pump with sump and a rear spraybar.	hour	\$11.00
8611	Trailer, Water	Tank Capacity	6000 gal		Includes a centrifugal pump with sump and a rear spraybar.	hour	\$14.00
8612	Trailer, Water	Tank Capacity	10000 gal		Includes a centrifugal pump with sump and a rear spraybar.	hour	\$16.50
8613	Trailer, Water	Tank Capacity	14000 gal		Includes a centrifugal pump with sump and a rear spraybar.	hour	\$20.50
8650	Trencher			to 40	Walk-behind, Crawler & Wheel Mounted. Chain and Wheel.	hour	\$11.75
8651	Trencher			to 85	Walk-behind, Crawler & Wheel Mounted. Chain and Wheel.	hour	\$25.00
8290	Trowel, Concrete	Diameter	48 in	to 12		hour	\$4.50
8680	Truck, Concrete Mixer	Mixer Capacity	13 cy	to 300		hour	\$75.00
8720	Truck, Dump	Struck Capacity	8 cy	to 220		hour	\$35.00
8721	Truck, Dump	Struck Capacity	10 cy	to 320		hour	\$45.00
8722	Truck, Dump	Struck Capacity	12 cy	to 400		hour	\$60.00
8723	Truck, Dump	Struck Capacity	18 cy	to 400		hour	\$65.00
8724	Truck, Dump, Off	Struck Capacity	28 cy	to 450		hour	\$105.00
8690	Truck, Fire	Pump Capacity	1000 gpm			hour	\$70.00
8691	Truck, Fire	Pump Capacity	1250 gpm			hour	\$80.00
8692	Truck, Fire	Pump Capacity	1500 gpm			hour	\$85.00
8693	Truck, Fire	Pump Capacity	2000 gpm			hour	\$90.00
8694	Truck, Fire Ladder	Ladder length	75 ft			hour	\$125.00
8695	Truck, Fire Ladder	Ladder length	150 ft			hour	\$150.00
8700	Truck, Flatbed	Maximum Gvw	15000 lb	to 200		hour	\$20.00
8701	Truck, Flatbed	Maximum Gvw	25000 lb	to 275		hour	\$22.00
8702	Truck, Flatbed	Maximum Gvw	30000 lb	to 300		hour	\$25.00
8703	Truck, Flatbed	Maximum Gvw	45000 lb	to 380		hour	\$43.00
8730	Truck, Garbage	Capacity	25 cy	to 255		hour	\$47.00
8731	Truck, Garbage	Capacity	32 cy	to 325		hour	\$55.00
8800	Truck, Pickup				Transporting people.	mile	\$0.50
8801	Truck, Pickup		½ ton			hour	\$14.00
8802	Truck, Pickup		1 ton			hour	\$20.00
8803	Truck, Pickup		1½ ton			hour	\$22.00
8804	Truck, Pickup		1½ ton			hour	\$25.00
8805	Truck, Pickup		1¾ ton			hour	\$30.00
8790	Truck, Tractor	4 x 2	30000 lb	to 220		hour	\$32.00
8791	Truck, Tractor	4 x 2	45000 lb	to 310		hour	\$45.00
8792	Truck, Tractor	6 x 4	50000 lb	to 400		hour	\$55.00
8780	Truck, Water	Tank Capacity	2500 gal	to 175	Include pump and rear spray system.	hour	\$31.00
8781	Truck, Water	Tank Capacity	4000 gal	to 250	Include pump and rear spray system.	hour	\$42.00
8620	Tub Grinder			to 440		hour	\$85.00
8621	Tub Grinder			to 630		hour	\$120.00
8622	Tub Grinder			to 760		hour	\$150.00
8623	Tub Grinder			to 1000		hour	\$270.00
8753	Vehicle, Recreational			to 10		hour	\$3.00
8750	Vehicle, Small			to 30		hour	\$7.00
8761	Vibrator, Concrete			to 4		hour	\$1.15
8770	Welder, Portable			to 16	Includes ground cable and lead cable.	hour	\$5.00
8771	Welder, Portable			to 34	Includes ground cable and lead cable.	hour	\$11.50
8772	Welder, Portable			to 50	Includes ground cable and lead cable.	hour	\$16.00
8773	Welder, Portable			to 80	Includes ground cable and lead cable.	hour	\$22.00

**ORDINANCE #12-XX
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

AN ORDINANCE ESTABLISHING PERMIT FEES, SERVICE CHARGES, AND VARIOUS OTHER FEES TO BE COLLECTED BY THE CITY OF RAMSEY.

The City of Ramsey ordains:

SECTION 1. AUTHORITY

This ordinance is adopted pursuant to and under the authority of the Charter of the City of Ramsey.

SECTION 2. PURPOSE

Per S701.10 (Fees) the purpose of this Ordinance is to establish the Permit Fees, service Charges, and Other Fees that will be collected by the City in year 2013. This is known as the Schedule of Rates, Fees and Charges.

SECTION 3. SCHEDULE

The attached permit fees and service charges are hereby established for the year 2013.

SECTION 4. EFFECTIVE DATE

This Ordinance becomes effective 30 days after its passage and publication, subject to City Charter Section 5.04.

PASSED by the City Council of the City of Ramsey, Minnesota, the 23rd day of November, 2012.

Mayor

ATTEST:

City Clerk

Introduction date: November 13, 2012
Posting dates: November 13, 2012-November 27, 2012
Adoption date: November 27, 2012
Publication date: November 30, 2012
Effective date: January 1, 2013

Meeting Date: 11/27/2012

By: Tim Gladhill, Community Development

Information

Title:

Adopt Ordinance to Amend City Code Section 117-118 (The COR) Approving Amendment to The COR Design Framework Related to Off-Street Parking, Signs, Parks and Trails, and Sub-District Boundaries

Background:

This case relates to requested amendments to The COR Design Framework for the following topics:

- Master Sign Plan and Private Signs
- Parking Standards for Restaurants
- Updated Development Plan and Sub-District Boundaries

Notification:

The Public Hearing was properly published in the Anoka County Union, the City's official newsletter for Public Hearings.

Observations:

Master Sign Plan

The original version of the Design Framework included provisions for a Master Sign Plan. At that time, only locations of these larger community and development signs had been determined. The design and bulk of those signs had not yet been determined. This amendment now creates Appendix B of the Design Framework that includes these design factors. Community Signs are those signs that are generally located along Highway 10 and are intended to capture motorists attention into the development. Community Signs may include tenant panels for users within the development signs. Specific standards are included in Appendix B, attached hereto, and include the following:

Sign Type	Allowable Size	Architecture	Allowable Height
Community Sign	300 Square Feet	Per Appendix B	30 Feet *
Gateway Sign	50 Square Feet	Per Appendix B	10 Feet
Retail Gateway Sign	150 Square Feet	Per Appendix B	12 Feet
Community Kiosk	32 Square Feet	Per Appendix B	10 Feet*
Parking Ramp Signage	100 Square Feet	Per Appendix B	(Wall Signage)

Gateway Signs are generally located at key nodes and intersections of the development at Armstrong Boulevard, Bunker Lake Boulevard, and Ramsey Boulevard. General Gateway Signs may not include tenant panels. Retail Gateway Signs may include tentant panels.

Community Kiosks are generally located in the center of the development with the highest emphasis on the pedestrian. As such, these are intended to be pedestrian scale and provide general information about the development and wayfinding information.

The revised text also allows additional flexibility for Project (private signs) at shared entrances in the retail sub-district. City Staff recommends additional flexibility in size in order to allow users to consolidate signs at shared entrances, thereby potentially reducing the clutter of signs along Sunwood Drive in this retail area.

The Master Sign Plan has the effect of being the Sign Code for The COR. As such amendments must be processed by Ordinance as any other type of Zoning Amendment would. A final version of the site location map related to the Master Sign Plan to provide distinction between General Gateway and Retail Gateway Signs.

Please note that Staff discovered a few wall sign types that had not been adjusted to the intended 15% of the facade area. Those sign types have been adjusted in the current draft.

Parks

The City is actively working towards a future pedestrian overpass over Highway 10 known as Mississippi Skway. A component of that request is to relocate the designation of the Central Anoka County Regional Trail through The COR along Center Street. The preferred alignment is indicated in the revised draft.

Sub-District Boundaries

As the City of Ramsey Housing and Redevelopment Authority has approved a revised Development Plan, the Planning Commission must consider updates to the City's Official Controls (Zoning Ordinance). The Sub-District Map has been revised to reflect the preferred Development Plan (Version 6.1). The only change is to amend the COR TWO Plat from COR-2 Retail Sub-District to COR-2b Sub-District based on preferred land uses and parking needs.

Parking

Based on experience with existing users in the retail area of the development, Staff recommends amending parking regulations for restaurant users in areas without planned parking ramps to allow additional parking spaces beyond the maximum allowed. Rather than basing parking requirements on square footage, the revised draft proposes to base requirements on number of seats.

One map within the Framework, the Master Sign Plan, will need to be updated upon adoption of the ordinance to reflect distinction in location for General Gateway and Retail Gateway signs.

The ordinance has been introduced and is now eligible for adoption.

Funding Source:

Preparation of the amendments are being handled by a combination of regular Staff duties and professional services. The City of Ramsey Housing and Redevelopment Authority (HRA) approved a scope of services to complete the project.

Staff Recommendation:

The Planning Commission met on Thursday, October 4, 2012 to hold a Public Hearing and recommended approval of the proposed ordinance, which includes the revised Design Framework and Master Sign Plan with the following amendments, with adjustments to the Draft Master Sign Plan to reflect distinction between General Gateway and Retail Gateway Signs.

Committee Action:

Motion to waive the City Charter requirement to read the ordinance aloud;

-AND-

Motion to adopt the proposed ordinance, which includes the revised Design Framework and Master Sign Plan.

Roll Call Vote:

Councilmember McGlone
Councilmember Tossey
Councilmember Elvig
Councilmember Strommen
Councilmember Wise
Councilmember Backous

Attachments

REVISED Design Framework

Proposed Development Plan Version 6.1

Site Location Map - Master Sign Plan

Proposed Appendix B to Design Framework

Proposed Ordinance

DRAFT Planning Commission Minutes dated October 4, 2012

Form Review

Inbox	Reviewed By	Date
Tim Gladhill (Originator)	Tim Gladhill	11/20/2012 08:29 AM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:12 AM
Form Started By: Tim Gladhill		Started On: 10/25/2012
	Final Approval Date: 11/21/2012	



Design Framework



February 28, 2012

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Appendix A: Specifications and Details

Design Framework – The COR

Purpose

The primary purpose of Design Framework is to set basic parameters, describe preferences and illustrate design intent. These framework standards serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

Application

The format and content of this framework are specifically tailored for use as a supplement to the Zoning Ordinance. This document is organized into two sections. The first section outlines issues and recommendations that apply to the entire COR. The second section highlights specific framework that apply to each sub-district within The COR. The City of Ramsey has adopted Development Plan 5.03 as the Master Plan for The COR. This master plan provides illustrative guidance for building locations and orientation. Any changes to this approved master plan must be reviewed and approved by the City Council as part of the development application. This Development Plan will be updated by the City as changes are approved.

Implied Responsibility

All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

Part One: Overall Framework

There are a number of framework standards that apply to the entire COR, including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall COR framework standards overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Framework:

- Context – local, city, regional
- Street Hierarchy--access, circulation, arrangement
- Streetscape—preferred design, location
- Parking -- quantity, location, type
- Building Design-- preferred uses, horizontal/vertical mixed use, built form, character of development
- Signage—design, location, quantity, type
- Stormwater Management-- surface water features, stormwater management
- Parks/Public Spaces – parks, trails and open space

It is essential that proposed design solutions for development projects and other improvements within The COR demonstrate an understanding of the interplay between these layers.

Overall Framework - Context

Overview

The COR is a unique area within the City; but it is a part of, and connected with, a variety of local, city-wide and regional systems (See Figure 1: Development Master Plan 5.03). Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

Guideline Recommendations

To ensure that The COR takes full advantage of local and regional systems, development should:

- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/stormwater management plan

Objectives

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

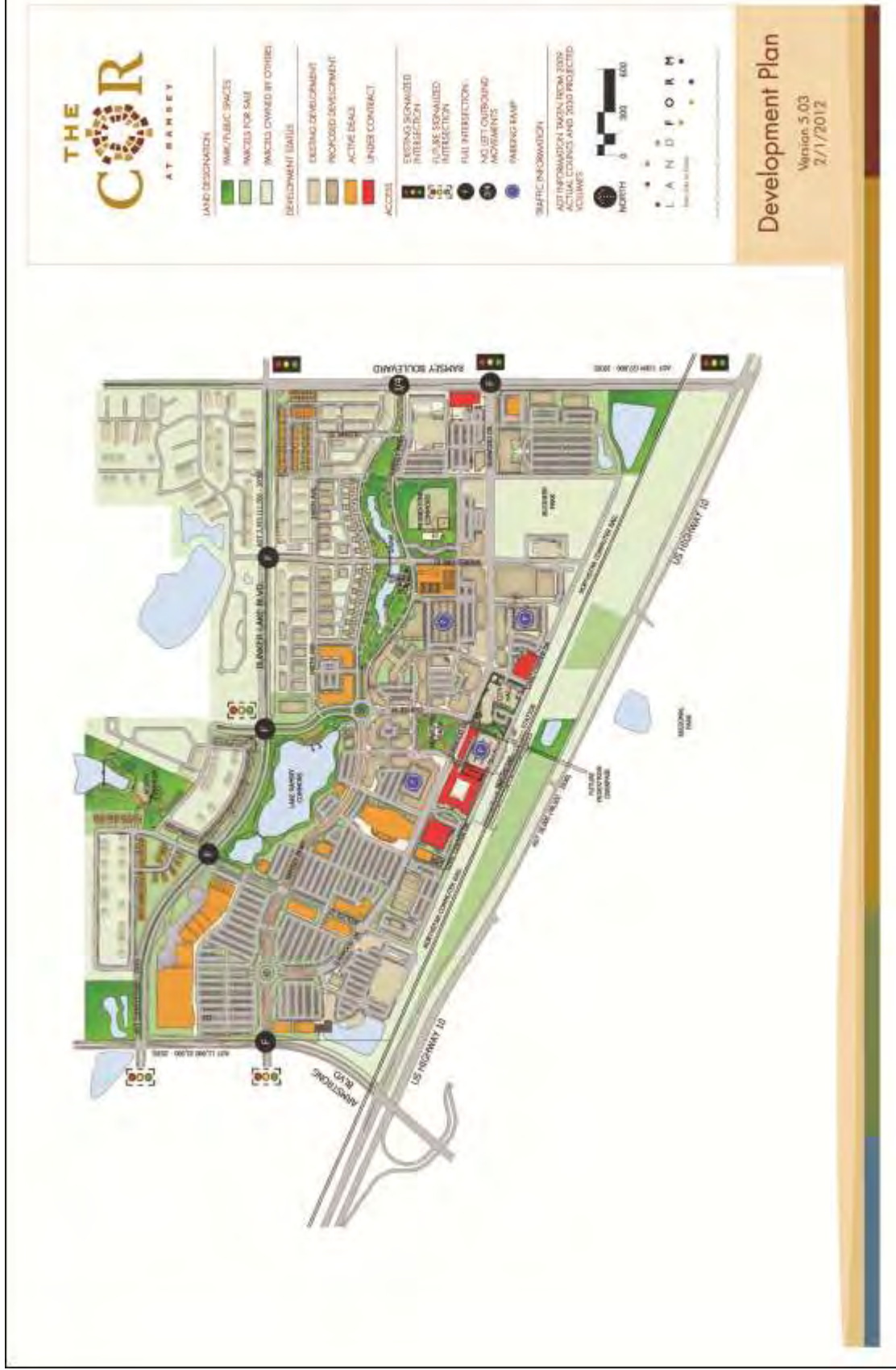


Figure 1: Development Master Plan 5.03

Overall Framework – Street Hierarchy

Overview

The Development Master Plan defines a specific strategy for the layout of streets and blocks within, and around The COR. The size, type and configuration of this street hierarchy (See Figure 2: Street Hierarchy) is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample space for street-side activities, strolling promenades, pockets of green and other amenities is critical to the success of The COR. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as a primary way to move about and conduct business in the core area.

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in The COR and encourages a more dispersed and less congested traffic flow (many choices or travel paths).
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout The COR.
- Work with Anoka County to convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches.
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street).



Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Arterial street – these streets provide the boundary of The COR and all access into The COR will be from one of these three streets:
 - Bunker Lake Boulevard
 - Armstrong Boulevard
 - Ramsey Boulevard
- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities
 - Sunwood Drive



- Parkway – premier ‘green streets’ adjacent to primary public park / green space, with more landscaping than other streets in The COR
 - Ramsey Parkway
 - Center Street (north of the roundabout)
- Connector street – primary routes that link various districts within The COR, usually providing the most direct means of getting from point A to point B
 - Rhinestone Street
 - Zeolite Street
 - Veterans Drive
- Downtown street – the most urban of street sections where high pedestrian traffic is intended and traffic calming measures such as on street parking, bump outs and other methods will be employed
 - Center Street (south of the roundabout)
 - Sapphire Street (north of Sunwood)
 - Other streets as shown on the Street Hierarchy Map
- Local street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout The COR.



These street hierarchy classifications are intended to provide guidance regarding the streetscape, but it should be noted that within a street classification, there may be construction or design distinctions, based on the sub-district classification. For example, Sunwood Drive is defined as a destination street throughout The COR, but the design may be different between the COR1 and COR2 sub-districts. The Master Streetscape Map will provide design direction.

Transit Overview

The COR is designed to be a Transit Oriented Development (TOD) in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multimodal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. The COR is the Twin Cities' first and only transit-oriented development along the new Northstar Commuter Rail Line. Ramsey Station at The COR will open for riders boarding the train in November 2012. As Northstar's most visible station, The COR will immediately bolster the ridership on the line and continue to build recognition for this regional service.



Transit components include the following:

- Ramsey Star Express service provides peak period, peak direction bus service between downtown Minneapolis and Ramsey until the Northstar Commuter Rail station stop is complete
- Northstar Commuter Rail provides train service between Big Lake and downtown Minneapolis
- 350 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within The COR and into the surrounding neighborhoods
- The COR can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station



The COR also encourages developers to participate in a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. The TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions.

- Developers shall submit a TDM plan with their site plan application for new development and work with the City to implement their plans.

Transit Guideline Recommendations

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in The COR, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following framework:

- Understand and take advantage of opportunities to use existing and proposed transit components

- Provide transit related facilities specifically linked to uses and character within each COR district, including signage and lighting for way-finding and bicycle amenities
- Building future Northstar Commuter Rail riders through the Ramsey Star Express bus service

Transit Objectives

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. The framework for The COR promotes public transportation as part of the solution and suggests a variety of street improvements and other amenities to invite increased use. Photographs throughout the framework suggest some of the many possibilities.

Overall Framework - Streetscape

Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within and around The COR. The amenities within the public right-of-way -and immediately adjacent -play an important role in the aesthetics of The COR. Great streets are essential components of an attractive and inviting public realm. Developing a streetscape that is comfortable and inviting to pedestrians is important to realizing the vision for The COR. The framework will encourage interesting and inviting places for the public.

The Streetscape framework defines the visual character and physical improvements for all public spaces within The COR. The document also provides the general framework and examples for design of proposed quasi-public and private space located within each development parcel.

Public realm/streetscape framework focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space



The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. The City has developed a series of street cross-sections to illustrate these design expectations. Typical street sections are provided in Appendix A as a reference.

Destination and Connector streets must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)



The COR includes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.

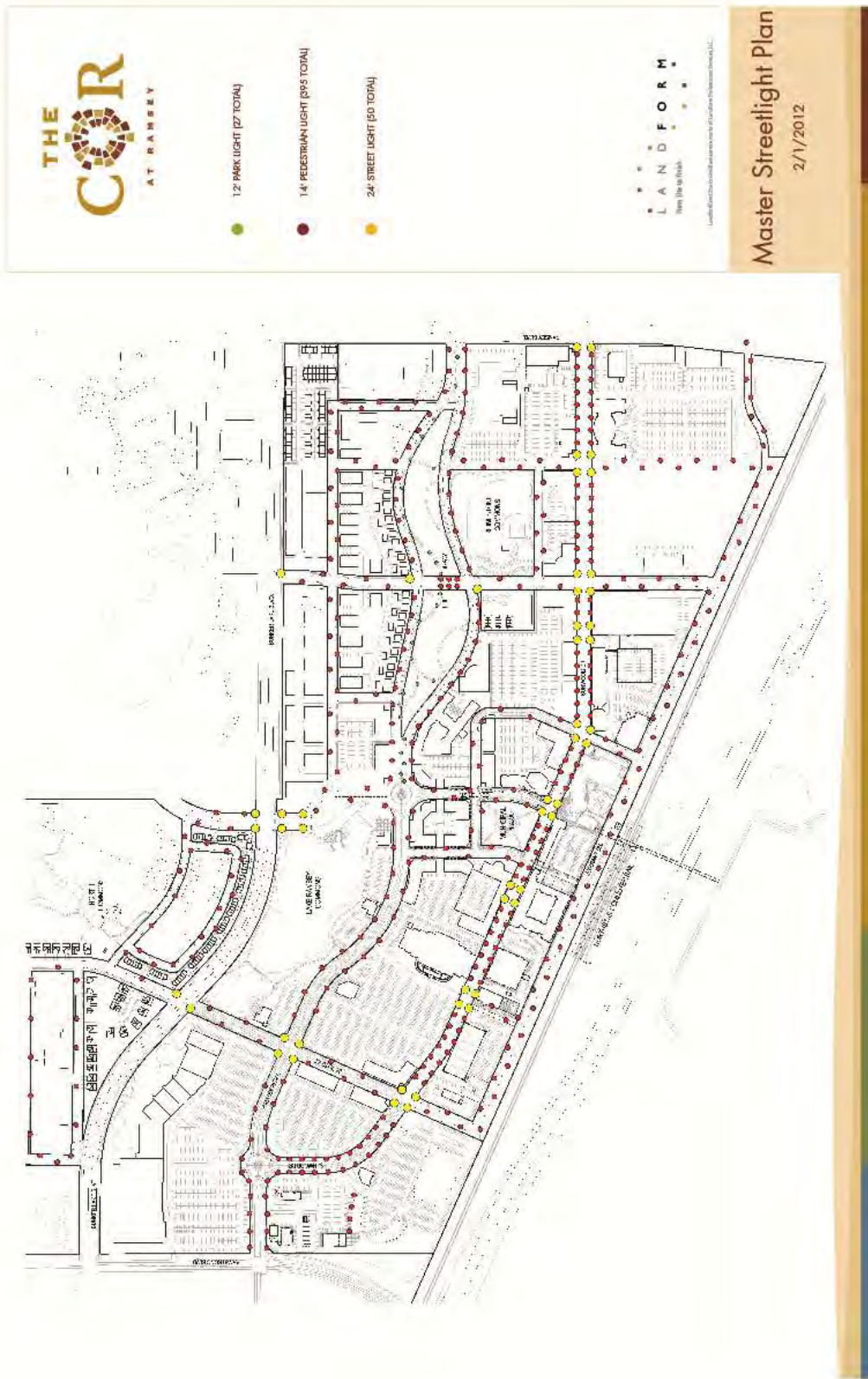


Figure 3: Master Lighting Plan

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, including the following:

- Bike racks interspersed throughout The COR to encourage cycling as an alternative mode of transportation.
- Benches on the public sidewalks to encourage people to linger in The COR
- Trees and planters along the streets to create an inviting space
- Wide sidewalks and both on- and off-street bicycle trails
- Kiosks and other way finding tools for visitors and residents in The COR
- Outdoor dining is encouraged on the street side of buildings within all districts.
- Public plaza space is strongly encouraged to be located adjacent to the streets



Objectives

Creative design solutions for streetscape improvements should include:

- Streetscape improvements that provide traffic calming, particularly along Destination and Downtown streets
- Inviting streets that draw you into and through The COR
- A marriage of building design and streetscape design that encourages active spaces near the streets



Street Trees and Plantings

Street trees and planter boxes are a critical element in the unique character of The COR and will be used to help distinguish between different street types and sub-districts within The COR. However, there may be variety within the same street when the street crosses into sub-districts.

- Street trees will be chosen to ensure that the species allow for clear pedestrian access along the streets and require minimal maintenance.
- Along Destination, Downtown, Parkway and Local streets, where regularly spaced blocks exists, one (1) tree species shall be provided per block. In order to provide variety and protect against disease, adjacent blocks shall not be allowed the same street tree. Initial development along a block shall establish the tree species for that block, subject to City review and approval.
- Along streets without regular block space or longer blocks, such as Connector and Arterial streets, the City may approve groupings of 3-6 trees of one (1) species in lieu of the regular block spacing.
- Alternatives to the above mentioned planting plan shall be subject to review and approval by the City Council.

Destination Street Trees:

- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Bicolor Oak (*Quercus bicolor*)
- Sienna Glenn Maple (*Acer x freemanii* 'Sienna')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Ginko (*Ginkgo biloba*)
- Accolade Elm (*Ulmus x 'Accolade'*)

Parkway Street Trees:

- Valley Forge Elm (*Ulmus americana* 'Valley Forge')
- Bitternut Hickory (*Carya cordiformis*)
- Siouxlant Poplar (*Populus deltoides* 'Siouxlant')
- Sienna Glen Maple (*Acer x freemanii* 'Sienna')
- Boulevard Linden (*Tilia americana* 'Boulevard')
- Northern Catalpa (*Catalpa speciosa*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Korean Mountain Ash (*Sorbus alnifolia*)
- Ohio Buckeye (*Aesculus glabra*)
- Ginkgo (*Ginkgo biloba*)

Connector Street Trees:

- Bicolor Oak (*Quercus bicolor*)
- Northern Red Oak (*Quercus rubra*)
- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Downtown Street Trees:

- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Local and Arterial Street Trees:

Local and Arterial Street trees may be chosen from any species on the above lists.

Planter Beds/Boxes/Tree Grates

- Planter beds, tree grates and planters shall be as shown on the Streetscape Master Plan. Details and specification shall be added to Appendix A as developed and approved by the City. Appendix A shall be amended as needed.
- Irrigation shall be provided to ensure survival of plant materials. The City shall provide irrigation systems along the Parkway and Destination streets. Unless otherwise approved, all other irrigation shall be the responsibility of the landowner.

Street Furniture

Bike racks, trash enclosures and benches are encouraged on all streets, but will be required on Destination, Connector and Parkways and other streets as shown conceptually on the Streetscape Master Plan.

Bike racks shall be the standard bike rack (see Appendix A for specifications).

Trash enclosures shall be the standard trash enclosures (see Appendix A for specifications).

Benches shall be the standard bench (See Appendix A for specifications).

Benches shall be grouped in twos that face each other and provide an opportunity for an outdoor conversation space.

Alternative bike rack, trash enclosures and bench designs will be considered on a case-by-case basis. The City will review alternative designs as part of the site plan review and will evaluate these alternatives based on the following:

- Does the design maintain the character of the district?
- Does the alternative design maintain the quality of the standard feature?
- Does the design provide a relationship between the public elements of the streetscape and the building that would not otherwise be possible?



Sidewalks and Boulevard

In the design framework, the term “boulevard” is defined as “the space between the sidewalk and the curb.” The boulevard area provides the required space for public features such as street lights, benches, fire hydrants, street signs, etc. The boulevard will have different characteristics based on the street hierarchy.

The street width and layout within different zones may affect the ability of a particular development to provide the minimum boulevard width and the minimum sidewalk width within the existing public right-of-way. In those cases, the landowner shall provide a permanent public easement over the portion of the sidewalk that is located on private property.

Street Lights

Street lights are owned and installed by Connexus Energy. The streetlight design was developed in conjunction with the City of Ramsey and Connexus (See Figure 3: Master Lighting Plan). The standard street lights are designed to accommodate banner arms should banner signage be desired. The banner arms should be the manufacturer's standard break-away banner arms to ensure safety and minimize maintenance costs. Connexus provides street light maintenance for the City. Connexus bills the City of Ramsey for the cost of installation and maintenance.

The choice of street light was intended to emphasize the pedestrian scale of The COR. Light poles within the public right-of-way should be planned to accommodate banners, flowers, hanging artwork and electrical outlets.

There are three (3) different streetlight types within The COR:

- 24-foot high poles at intersections
- 14-foot high poles at mid-block
- 12-foot high poles at The Draw



The City will ensure that the appropriate streetlights are installed on all public streets as described by the Design Framework. Private developers shall provide parking lot or other project lighting that retains these design

objectives. All lighting shall comply with City Code performance standards for lighting. All lighting shall be reviewed by City staff to ensure compliance with the Design Framework and City Code standards described above.

Standards for Outdoor Dining.

Outdoor dining is encouraged as an attractive addition to a pedestrian- and retail-friendly, vital COR. Such dining is encouraged subject to the standards in Section 117-118 of the Zoning Ordinance.

Overall Framework - Parking

Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of The COR is a significant challenge. The goal is to provide adequate parking; both quantity and location, while promoting new strategies that support the overall needs of The COR. Parking will be provided through a combination of on street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of The COR. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street level uses should be integrated into ramp design along important street frontages. The City also encourages developers to wrap the ramp with multi-story uses that will provide commercial and residential opportunities and help to define the street edge.

Guideline Recommendations

Parking guidelines encourage the following:

- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views, soften parking area edges, provide shade, integrate native plantings, offset islands and reduce to 'sea of parking' image
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering
- Provide parallel parking on both public and private streets throughout The COR, to add parking and create a traffic calming, pedestrian buffer; Bumpouts are encouraged as a traffic calming measure on all streets within the COR.
- Explore options to integrate stormwater management requirements into parking area design
- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on street, small surface lot, underground and ramped parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area

- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Discourage driveway access to parking facilities along Arterial, Destination and Parkway streets, in favor of side streets.

Objectives

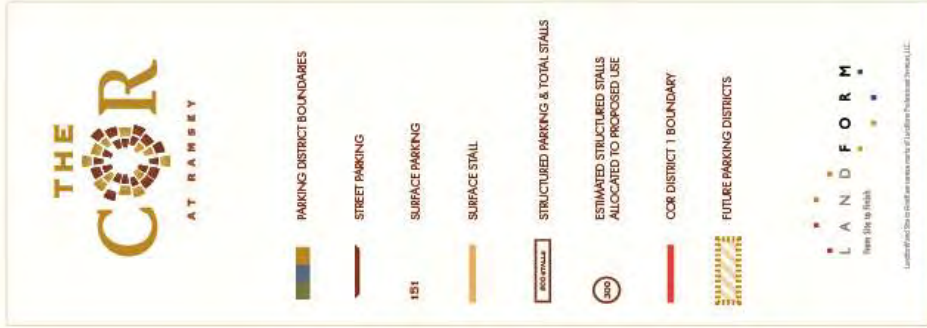
The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in The COR. The Design Framework anticipates the construction of a total of 3 parking structures in COR1 to serve the needs of the area by providing structured parking that can be shared among users, allow developers to maximize the development on each individual lot.

As discussed later in this section, sizing and timing of the structured parking will be finalized as the area develops and parking demand grows. In addition to providing adequate space, excellent design is a major factor as well. The framework for The COR encourages creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs throughout the Design Framework suggest some of the many possibilities.





Figure 4: Streetscape Master Plan



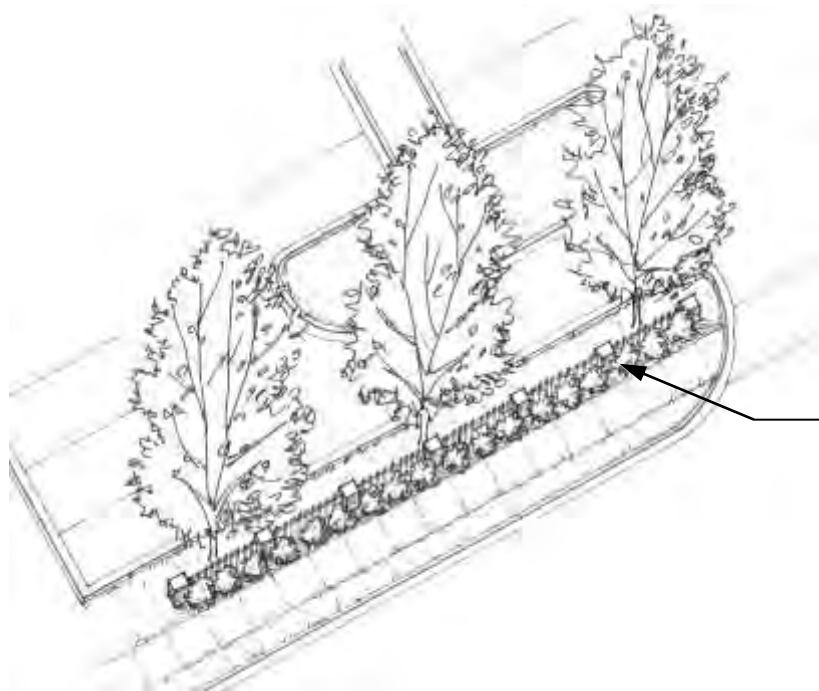
Parking Districts
 VERSION 5.03
 9/27/11

Figure 5: Parking District Map

Minimum Standards

Parking in The COR is intended to be shared to the greatest extent practicable in all mixed-use areas. The parking standards are intended to provide a practical basis for providing adequate parking within the COR District through a careful analysis of uses, shared parking arrangements, use of public street parking and reduction allowances for the proximity to the transit station. A parking plan has been prepared to identify the planned number, location, sharing arrangements and public use (see Figure 4: Streetscape Master Plan and Figure 5: Parking District Map). The parking plan provides information that can be used to help address longer term parking arrangements where the possibility of use conversion or building expansion exists. Parking shall be provided under any of the following arrangements:

- A. No parking shall be located between the front of the principal structure and the street on parcels abutting Destination and Downtown streets in COR1.
- B. Not more than fifty (50%) percent of the parking shall be located in front of the principal structure and the street on parcels abutting Arterial, Parkway, Connector and Local streets.
 - 1. In the COR2 district, developments may be allowed more than fifty (50%) percent parking in the front yard, provided all other design standards have been met, including the use of a wall, railing, hedge or combination to create a street edge and screen parking.
- C. Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4¹/₂') above the level of the parking lot, at the build-to line required for the property. The amount of required screening may vary by sub-district and street frontage and is described in Part Two of this document.



Landscaped edge that combines fence with trees, low shrubs and ground cover for parking lot screening and buffering along sidewalk to screen parking

- D. No parking spaces shall be located on corner lots at the point of street intersections.
- E. Shared parking in COR1 will be accomplished through a Parking Use and Maintenance Agreement (PUMA).
- F. Off-street parking in the COR2 and COR3 sub-districts shall be secured for public use through parking easements and other appropriate conveyances. Shared parking arrangements between nearby uses are encouraged in both sub-districts.
 - 1. The City may approve joint parking for one or more businesses where the total number of parking stalls provided for joint use is less than the sum of the total required for each business should they provide them separately. The applicant must demonstrate the feasibility of the arrangement in a written report. Such a permit shall not be granted except when the following conditions are found to exist:
 - i. Proximity. The building or use for which application is being made to utilize the off-street parking facilities provided by another building or use shall be located within three hundred (300') feet of such parking facilities, excluding public rights-of-way.
 - ii. Conflict in Hours. The applicant shall demonstrate in documented fashion that there is no substantial conflict in the principal operating hours of the two (2) buildings or uses for which joint use of off-street parking facilities is proposed.
 - iii. Written Consent and Agreement. A legally binding instrument, executed by the parties concerned, for joint use of off-street parking facilities, duly approved as

to title of grantors or lessors, and in a form and manner of execution approved by the City Attorney, shall be filed with the City Clerk and recorded with the Anoka County Recorder or Registrar of Titles, and a certified copy of the recorded document shall be filed with the City within sixty (60) days after approval of the joint parking use by the City or the interim use permit shall be considered null and void.

- G. On-street parking adjacent to buildings shall not be used for the purposes of calculating parking requirements.
- H. Within the COR-4 sub-district, parking on individual parcels serving individual uses may be provided if designated and approved as part of the master plan.
- I. Within all sub-districts, a minimum driveway length of twenty-five (25') feet is required to limit conflicts between vehicles parked in the driveway and street or sidewalk users.
- J. In order to ensure the pedestrian orientation of the COR Districts, minimum/maximum on-site parking standards are set based on the following table:

Table 1

Use	Minimum number of parking stalls	Maximum number of parking stalls*
Retail	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Restaurants	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Offices	2 per 1,000 sq. ft.	3 per 1,000 sq. ft.
Medical offices, clinics	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Health clubs	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Theaters, places of assembly	1 per 5 seats	1 per 3 seats
Residential		
Attached or detached	1 per unit	2 per unit
Multifamily units	1 per unit	2 per unit

*If a parking structure is provided on site, maximum parking stalls do not apply.

- K. Non-Specified Uses. For uses not specifically listed above, off-street parking requirements shall be computed by the Zoning Administrator on the same basis as required for the most similar listed uses. In such cases, the Zoning Administrator shall also consult off-street parking reference materials including, but not limited to, manuals prepared by the American Planning Association and Institute of Transportation Engineers.
- L. Handicap Parking. The size, number, and location of stalls reserved for handicapped parking shall be provided and identified as required by applicable regulations. These spaces are included in the computation for the minimum parking space requirement.
- M. Change in Land Use. When the site intensity or use of a building and/or property is increased with consequential effect upon the parking requirements as prescribed in this Section, the

parking requirements as prescribed herein shall be used to provide for such increase in the site intensity and/or use.

- N. Use of required parking spaces. Required parking spaces must be available for the use of residents, customers or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.
- O. Transit Service Reduction. The minimum number of parking stalls required on site may be reduced by 10% for any parcel located within ¼ mile of a transit stop. To qualify for this reduction, the transit stop must provide regular service on all days of the week and direct pedestrian access must be provided between the building and the transit stop.
- P. Bike Racks. In addition to the bike racks provided as part of the streetscape, individual businesses are encouraged to provide bike racks for customers. Bike rack locations will be reviewed and approved by the City as part of the site plan review.
- Q. Increase to Maximum Required Parking. If additional parking is sought to exceed these maximum requirements, a conditional use permit can be sought to increase maximums up to twenty-five (25%) percent.
 - 1. In the COR-2 sub-district, an increase of twenty-five (25%) percent in the maximum number of parking stalls is permitted without the issuance of a conditional use permit if approved as part of the site plan.
- R. In the COR1 sub-district, a PUMA (Parking Use and Maintenance Agreement) shall be applied to all development as follows:
 - 1. It is the City's intent to maximize development on individual lots. In order to help landowners accomplish this goal, the City will develop public parking ramps, which will be available for public parking within The COR. In order to minimize the amount of the site required for parking, the landowner may choose to enter into the PUMA for utilization of the public parking ramps to meet all or a portion of their minimum parking requirements.
 - 2. Each development shall be subject to a PUMA that specifies that each property in the COR1 shall be financially responsible for its proportionate share of a shared parking facility. The proportionate share shall be determined on the basis of the property's off-street parking needs, as determined by the parking requirements of Item J above.

3. The City shall establish fees by ordinance. The developer shall be responsible for payment in an amount equal to the value of the required parking on a per-stall price basis for ramp parking through the PUMA. The City will determine the appropriate contribution.
4. Funds collected by the City shall be deposited in a special fund used only to acquire and/or develop off-street parking facilities for the COR. Financial responsibility shall cover the construction and continuing maintenance of the parking facility.
5. The parking facility may be constructed and maintained by the City or by a private management entity acceptable to the City.

Private Streets

Private streets may be allowed within residential subdivisions, subject to the standards in Section 117-112 (e)3.b. of the Zoning Ordinance.

Loading Areas

- A. Loading areas and docks shall be located in the rear of the building.
- B. Loading areas shall not be located in the front yard and shall be fully screened from public streets, public parks and residential areas through landscaping and building design measures.



Overall Framework – Building Design

Overview

The COR Development Plan 5.03 identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, this Design Framework focuses on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

Guideline Recommendations

The mix of land uses in The COR will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, The COR resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of The COR, anchored by City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.

Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street, particularly in the center of the district.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, ‘big box’ retailing and auto-oriented uses within the COR2 sub-district only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a ‘family’ or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district.
- All uses should reinforce the street edge.



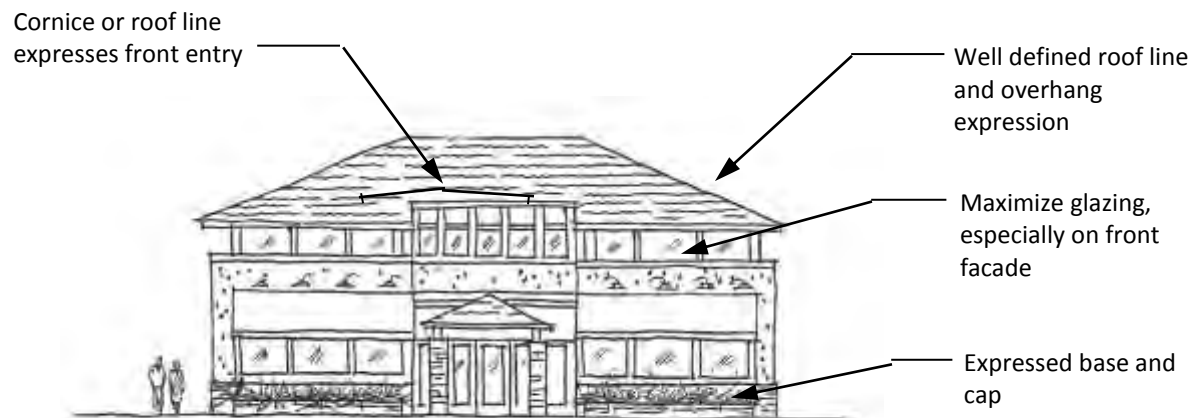
Architectural Overview

High quality architecture is an essential component in fulfilling the objectives of the Design Framework. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

Architecture Recommendations

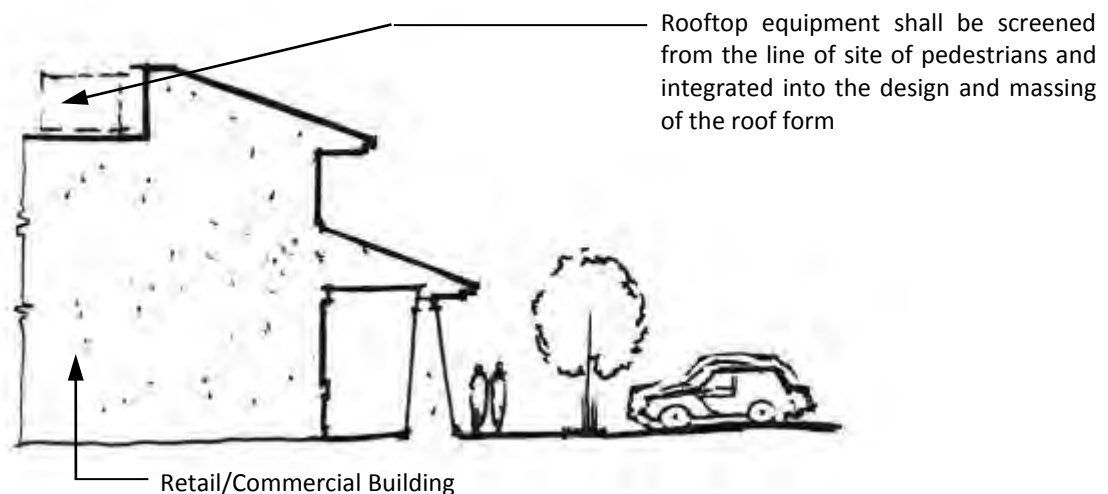
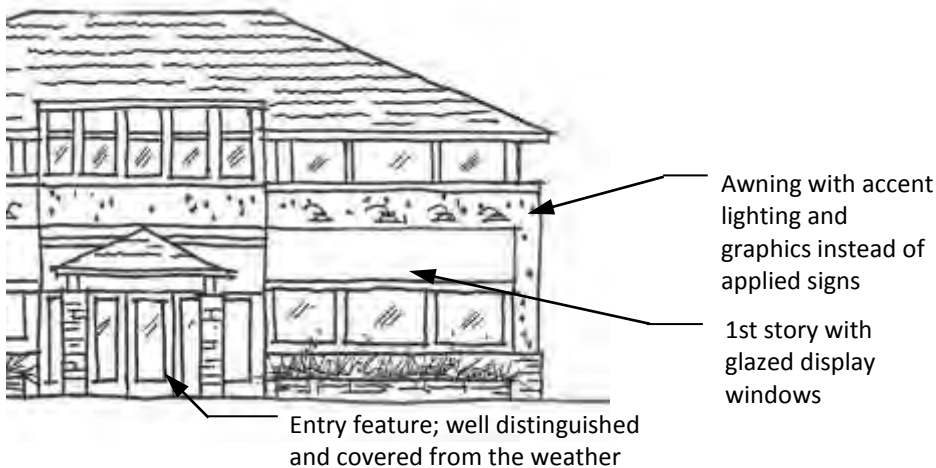
Of all the layers that combine to form The COR Development Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for The COR. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- An animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience

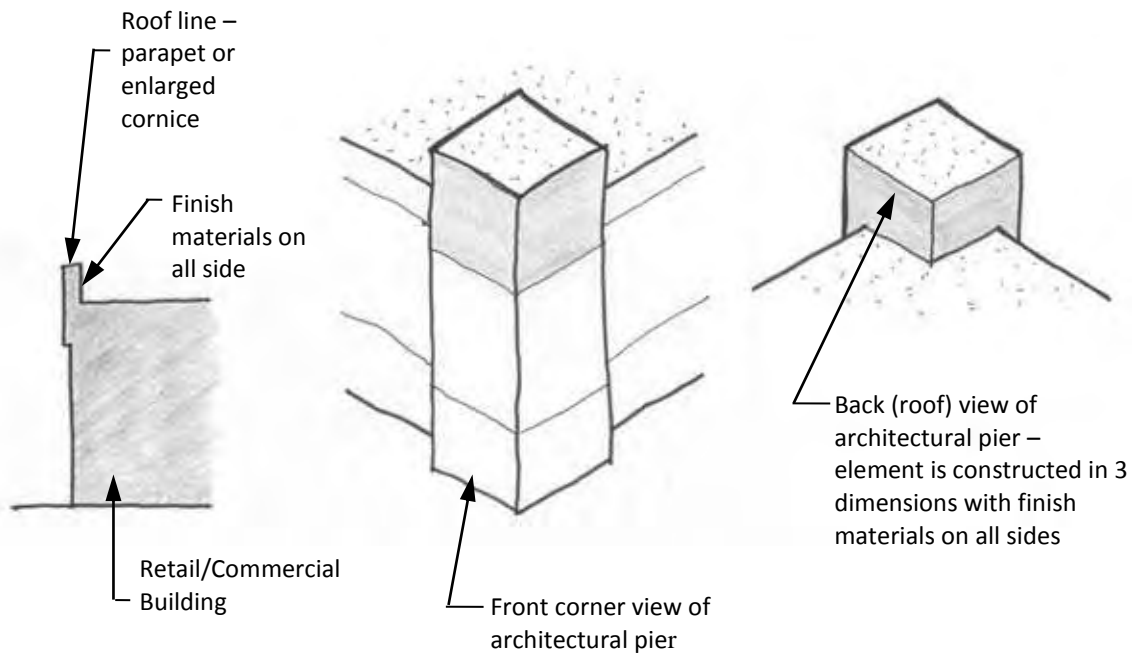


- Variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other COR uses
- Use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets and important crossroads throughout The COR
- Buildings should always be located at all four corners of intersections.
 - In the COR2 sub-district, if the City finds that there are unique circumstances where buildings cannot be located on the corner, the City may approve the use of fences, walls or other design elements to create that "street edge"

- Buildings will be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, or where streets intersect with parks, such as the streets that front on or end near the Municipal Plaza. These locations can often be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as The COR develops
- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.
- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Use recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage



- Equipment, mechanical systems, transformers, etc. must be screened from view in a way that is integral to the architecture of the building. Screening should be accomplished with materials similar to or compatible with materials used on the main structure; metal fencing or mechanical vent screens alone are not sufficient.
- Roof lines and cornice details shall be completed in a 3-dimensional manner so that the back of roof features or similar unfinished areas are not visible.



Overall Framework - Signage

Overview

Signage within The COR should provide a system for clear wayfinding for all modes of transportation and should ensure successful business operation while maintaining the design aesthetic of this unique project. These sign standards are intended to allow flexibility and creativity while maintaining the design concepts of The COR. Due to the distinctive nature of The COR, the sign standards from the City Code are not applicable and signage shall be allowed as prescribed by the Design Framework. A Master Sign Plan identifies the location of key sign locations (see Figure 6: Signage Plan).

Guideline Recommendations

The COR development manager shall prepare a Master Sign Plan for the public elements of the project, which are in compliance with the design framework and will be adopted by reference. The Master Sign Plan will specifically address public signs including:

- The three (3) off-site community signs
- Banner signs on light poles
- Signage for public parks
- Signage for public parking ramps
- Signage for public buildings
- Community kiosks



Objectives

Signage should be used as a tool to help identify businesses and neighborhoods with The COR and should have elements that are focused on the pedestrian nature of The COR as well as the signage needs for businesses.

- Signage and lighting which is integrated into the design of the building is encouraged.

Definitions

Awning Sign means a sign incorporated into or attached to an awning.

Community sign means an off-site sign identifying the development name and key tenants. Community signs may also include public information. Reader boards are allowed to be incorporated into community signs within The COR.

Temporary Sign means a sign which is erected or displayed for a limited period of time and not affixed to a Minnesota State Building Code approved structure.

Project sign means a freestanding area identification sign which identifies a single-family or multifamily residential subdivision, a commercial development or an industrial park or office park and which is located on the same parcel as the development it identifies.

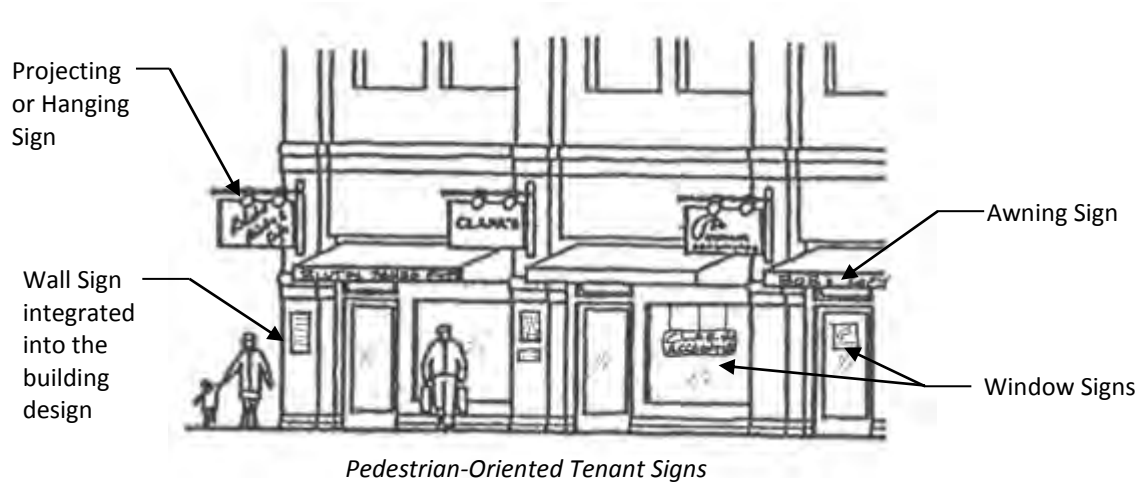
Projecting sign means any sign, all or any part of which extends beyond the surface of the building or wall by more than 16 inches.

Public Realm/Streetscape sign means any sign erected and maintained by public officials or public agencies, or approved and authorized for use by state or local governmental authorities.

Sandwich Board Signs means a self-supporting, freestanding temporary sign with only two (2) sides that are situated adjacent to a business with the intent to attract pedestrian traffic to businesses. Sandwich board signs are not meant to be read by vehicular traffic.

Wall sign means any sign which is affixed to a wall of any building. This definition includes individual letter signs and signs on mansards.

Window sign means a sign installed inside a window, or any sign placed within a building for the purpose of being visible from the public right-of-way. This does not include merchandise on display.



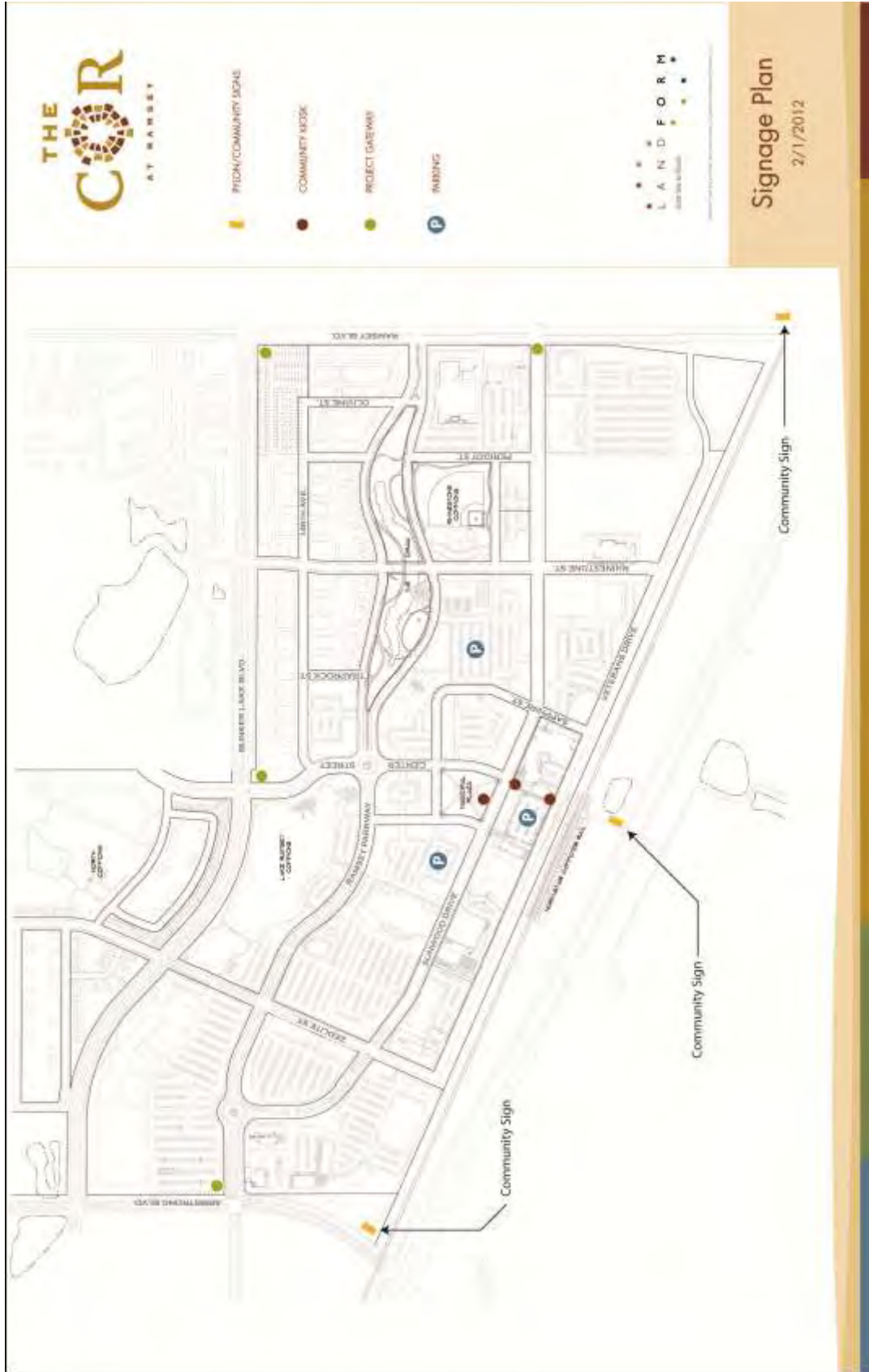


Figure 6: Signage Plan

Standards

Public Realm/Streetscape Signs

The pedestrian oriented nature of The COR requires unique signage that will build on the design concepts for The COR. These types of signs include banner signs on the streetlights, kiosks, street signs, wayfinding signs, signage for public parking ramps, etc. As the design concept for The COR has been refined, so has the signage plan. The COR development manager shall develop standards for these public realm signs as part of the Master Sign Plan. The Master Sign Plan shall be incorporated by reference and may be updated from time to time. Such signage shall be allowed on public spaces as needed to provide information and wayfinding. No advertising shall be allowed on Public Realm/Streetscape Signs except for community events.

Community signs

Community signs include are planned to include three (3) signs on Highway 10, as shown on the Signage Plan (see Figure 6: Signage Plan). The signs include the existing Community Sign near the future transit plaza, the sign at the northwest corner of Highway 10 and Ramsey Boulevard and the planned sign at the northwest corner of Highway 10 and Armstrong Boulevard.

These off-site signs are allowed as shown on the signage plan and shall have consistent materials and colors. The Master Sign Plan shall detail the materials, colors and standards for these three (3) community signs. The Master Sign Plan shall include standards for which tenants shall be allowed on the community signs.



Desirable signs are designed to be architecturally compatible with buildings within the development and other signage within the development

Project Signs

Project signs are permitted within all COR sub-districts.

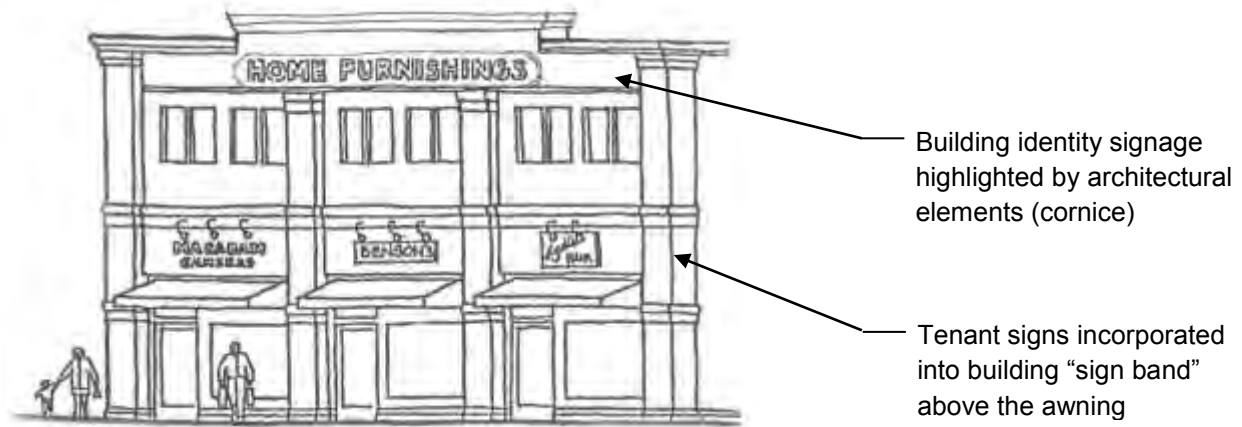
One (1) freestanding sign is allowed per building, provided that the following standards are met:

- A. The building in which the advertising business is located, shall be set back a minimum of six (6) feet from a public street right-of-way.
- B. The area of each face of the signboard shall not exceed six (6) square feet and the signboard shall not have more than two (2) readable faces
- C. The height of the top of the signboard, or of any posts, brackets, or other supporting elements shall not exceed six (6') feet from the ground.

- D. The signboard shall be constructed of wood, acrylic, aluminum or metal and shall be architecturally compatible with the style, composition, materials, colors and details of the building.
- E. No part of the sign shall encroach on the right-of-way and its location shall not interfere with pedestrian or vehicular circulation.
- F. Limited to one (1) sign per building and shall not be in addition to wall-mounted, applied letter or projecting signs.
- G. The readable faces of the sign shall be perpendicular to the adjacent street.

In the COR2 District, the following signage shall be allowed:

- A. Ground sign. There shall not be more than one (1) ground sign for each parcel. The gross surface area of a ground sign shall not exceed one hundred (100) square feet for each exposed face nor exceed an aggregate gross surface area of two hundred (200) square feet.
- B. Menu board. One (1) on-site menu board per drive-up or walk-up lane of a drive-in restaurant up to a maximum of thirty-two (32) square feet each and a maximum height of three (3) feet. Menu boards are allowed a message on one (1) side only and cannot contain an advertising message.
- C. Directional signs.
 - 1. Directional or instructional signs are permitted in accordance with Section 117-463(I).
 - 2. Parking lot directional signs designating parking area entrances and exits are limited to one (1) sign for each entrance and/or exit and shall not exceed four (4) square feet for each exposed face. Parking lot directional signs shall not project higher than five feet (5') in height, as measured from the established grade of the parking area to which such signs are accessory.
 - 3. Parking lot instructional signs designating the conditions of use or identification parking areas shall not exceed eight (8) square feet and shall not project higher than ten (10) feet in height for wall signs and seven (7) feet in height for ground signs, as measured from the established grade of the parking area to which such signs are accessory.



Wall Signs

Wall signs are permitted within all COR sub-districts.

Wall signs shall be permitted on one (1) wall, except that lots with frontage on more than one (1) street may have signage on one (1) wall per street frontage.

Within the COR1, COR3, COR4 and COR5 districts, the following standards apply:

- A. Wall-mounted or painted signs, provided the following standards are met:
 1. The sign shall be affixed to the front facade of the building, and shall project outward from the wall to which it is attached no more than six inches (6").
 2. Single Tenant Buildings. Each building shall be allowed one (1) wall sign per street frontage. Signage must be located on the street elevation. The area of the sign shall not exceed five (5%) percent of the ground floor building facade area or fifty (50) square feet, whichever is less.
 3. Multi-Tenant Buildings.
 - a. Each building shall be allowed wall signage on the street frontage elevation. For buildings with multiple street frontages, the allowable signage is per street frontage. Signage must be located on the street elevation.
 - b. Each building is allowed wall signage that shall not exceed 5% of the wall area of the building façade adjacent to the street or 50 square feet, whichever is less.
 - c. Tenant signage must be located on the tenant lease space.
 - d. At least fifty (50%) percent of the allowed signage must be allocated to ground floor tenants and located on the ground floor.
 - e. Signage is limited to a maximum of one (1) sign per business/tenant, except that tenants with frontage on multiple streets may be permitted to have signage on each street frontage.
 - f. Multi-tenant buildings must have wall signs of similar design. Sign permits for tenants in multi-tenant buildings shall only be permitted by the City after the

building owner has submitted a comprehensive sign plan approved by the Zoning Administrator. The comprehensive sign plan for the building shall include similar design standards including sign material, color, style, spacing and size.

4. Signs for buildings facing Highway 10. Single or multi-tenant buildings that have frontage on Highway 10 or are separated from Highway 10 only by other public right-of-way, shall be allowed to have signage on that street elevation that is up to five (5%) percent of the wall area of that building façade with no maximum square footage.
- B. Wall-mounted building directory signs identifying the occupants of a commercial building, including upper story business uses, provided the following standards are met:
1. The sign is located next to the entrance.
 2. The sign shall project outward from the wall to which it is attached no more than six inches (6").
 3. The sign shall not extend above the parapet, eave, or building facade.
 4. The area of the signboard shall not exceed three (3) square feet, with each tenant limited to one (1) square foot.
 5. The height of the lettering, numbers, or graphics shall not exceed four inches (4").
 6. One (1) such sign is allowed per public building entrance and is allowed in addition to other permitted wall signage.
- C. Applied letters may substitute for wall-mounted signs, if constructed of painted wood, painted cast metal, bronze, brass, acrylic or black anodized aluminum. The height of applied letters shall not exceed 12 inches.
- D. Logos are considered signs and shall be included in the maximum allowable sign area.
- E. Window or door signs, provided that the following standards are met:
1. The sign shall not exceed ten percent of the window or door area or four (4) square feet, whichever is less.
 2. The sign shall be silk screened, hand painted, applied letters/graphics, neon tubing or other sign technologies that meet these standards.
 3. Limited to one (1) sign per business, applied on either the window or the door, but not on both.
 4. The sign shall not have an opaque backing of any type although smoked glass is allowed.

5. Window signage (includes graphics) shall be permitted on the same building walls that have, or are allowed to have, wall signage pursuant to Item A above. Window signage shall be deducted from the allowable wall signage area.

Within the COR2 district, where more conventional suburban development is expected, rather than the neo-traditional development planned for the other COR sub-districts, the following standards apply:

- A. Wall, canopy or marquee sign. Total sign area may not exceed 15 percent of the front building facade. At least 50 percent of the signage area must be placed on the measured wall with remaining signage area, if desired, distributed on any other wall. Sign height shall not exceed the top of the parapet wall or, if no parapet wall, sign height shall not exceed the height of the eaves. The gross surface area of a wall, canopy or marquee sign may be increased by ten percent if such wall sign:
 1. Consists only of individual, outlined alphabetic, numeric and/or symbolic characters without background except that provided by the building surface to which the sign is to be affixed;
 2. Illumination, if any, is achieved through shielded illumination, shielded silhouette lighting, or shielded spot lighting but not any lighting where the light source is visible or exposed on the face or sides of the characters; and
 3. A wall, canopy or marquee sign may be located on the outermost wall of any principle building but shall not project more than 16 inches from the wall to which the sign is to be affixed. A wall sign shall not project higher than the parapet line of the wall to which the sign is to be affixed.
- B. Window signs are restricted to 30 percent of the area of the window in which the sign is to be displayed.

In all COR sub-districts, restaurants and cafes shall be permitted one (1) wall-mounted display featuring the actual menu as used at the dining table, to be contained within a shallow wood or metal case and clearly visible through a glass front. The display case shall be attached to the building wall, next to the main entrance, at a height of approximately five feet (5'), shall not exceed a total area of two square feet, and may be lighted. This signage is allowed in addition to other permitted wall signage.

Projecting Signs

Projecting signs are permitted within all COR sub-districts.

Projecting signs, including graphics or icon signs, mounted perpendicular to the building wall, are encouraged in all sub-districts, provided the following standards are met:

- A. The sign area shall not exceed six (6) square feet.
- B. The distance from the ground to the lower edge of the signboard shall be ten feet or greater.



- C. The height of the top edge of the signboard shall not exceed the height of the wall from which the sign projects, if attached to a single story building, or the height of the sill or bottom of any second story window, if attached to a multistory building.
- D. The distance from the building wall to the signboard shall not exceed six inches (6").
- E. The width of the signboard shall not exceed three (3) feet.
- F. Limited to one (1) sign per business. Projecting sign area shall be deducted from the allowable wall signage area.
- G. Granted to ground floor commercial uses only.

Awning Signs

Awning signs are allowed in all COR sub-districts. Awning signs are allowed for ground floor uses only, provided that the following standards are met:

- A. If acting as the main business sign, it shall not exceed 24 square feet in area, and the height of the lettering, numbers, or graphics shall not exceed 12 inches.
- B. If acting as an auxiliary business sign, it shall be located on the valance only, shall not exceed four (4) square feet in area, and the height of the lettering, numbers, or graphics shall not exceed four inches (4").
- C. Limited to two such signs per business.
- D. If acting as the main business sign, it shall not be in addition to a wall-mounted or applied letter sign.

Sandwich Board Signs

Sandwich board signs are permitted within all COR sub-districts.

- A. One (1) sandwich board sign per business is permitted in any business, commercial and mixed use district and shall be located within five feet (5') of the main building entrance to the business it advertises.
- B. Sandwich board signs shall be displayed only during open business hours and must be removed daily.
- C. Sandwich board signs shall be no more than a total of two feet (2') in width and three feet (3') in height



- D. Sandwich board signs must leave a minimum of five feet (5') of clearance for pedestrian access if placed on a public or private sidewalk. Sandwich board signs may not hinder the ability of persons to access vehicles parked at the curb and/or access to a building.
- E. Acceptable materials for sandwich board signs shall include the following: metal, wood synthetic materials such as a chalk board and whiteboard. Sandwich board signs shall not be illuminated, nor shall they contain moving parts, or have balloons, streamers, stringers, pennants or similar adornments attached to them. Sandwich board signs shall be maintained in a good appearance at all times.
- F. No sandwich board sign shall be secured, tethered or installed on traffic devices, utility equipment, street furniture, street lights, or any other public fixture.
- G. Sandwich board signs are temporary signs and shall not be counted towards the total sign area of the site for permanent signage.

Temporary Signs

Temporary signs are allowed per Section 117-465 (Temporary signs) of the Zoning Ordinance, except as noted below:

- A. Freestanding temporary signs shall only be allowed in the COR1 District.
- B. In all other COR districts, temporary signs shall be limited to temporary sandwich board signs or temporary wall signage.

Off-Site Signs

Off-site signs are prohibited except for wayfinding and community signage specifically allowed by the Design Framework.

Table 2

Sign Standards					
Development Standard	COR1	COR2	COR3	COR4	COR5
Project Signs					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign (maximum)	6 sq. ft./6 feet high	100 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high
Menu Board**	N/A	1 per drive-through/36 sq. ft. max.	N/A	N/A	N/A
Directional Sign	N/A	Per 117-463(l)	N/A	N/A	N/A
Wall Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign					
Single Tenant Building	50 sq. ft. or 5% of the ground floor façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less
Multi-Tenant Buildings	50 sq. ft. or 5% of the façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less
Building Facing Highway 10	5% of the façade area	15% of façade area	5% of the façade area	5% of the façade area	5% of the façade area
Window Sign					
Number of signs allowed	1	1	1	1	1
Size of Sign	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	30% of area of window. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.
Directory Sign					
Number of signs allowed	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance
Size of Sign	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum
Projecting Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide
Awning Sign					
Number of signs allowed	2 per business	2 per business	2 per business	2 per business	2 per business
Size of Sign	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.

*One (1) sign is allowed per street frontage (either wall or project)

**Restaurants and cafes in all districts are allowed one (1) wall mounted menu board not to exceed 2 sq. ft. in addition to other permitted wall signage.

Additional standards

- A. Businesses with service entrances may identify these with one (1) wall-mounted or applied letter sign not exceeding two square feet.

- B. One (1) directional sign, facing a rear parking lot. This sign may be any type of permitted sign other than a freestanding sign, but shall be limited to four (4) square feet in area.

Creative Sign Standards

- A. *Purpose.* This section establishes standards and procedures for the design, review, and approval of creative signs. The purposes of this creative sign program are to:
 - 1. Encourage signs of unique design, and that exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - 2. Provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- B. *Applicability.* An applicant may request approval of a sign permit for a creative sign to authorize onsite signs that employ standards that differ from the other provisions of this chapter but comply with the provisions of this section.
- C. *Application Requirements.* A sign permit application for a creative sign shall include all information and materials required by the City.
- D. *Procedure.* A sign permit application for a creative sign shall be subject to review and approval by the City as part of the Site Plan review process. When the creative sign is proposed after site plan review is complete, a creative sign may be approved by the Zoning Administrator when the proposed sign is fifty square feet or less, and shall be approved by the City Council when the sign is larger than fifty square feet.
- E. *Design Criteria.* In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
 - 1. Design Quality. The sign shall:
 - a. Constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area;
 - b. Be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - c. Provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
 - 2. Contextual Criteria. The sign shall contain at least one (1) of the following elements:
 - a. Classic historic design style;
 - b. Creative image reflecting current or historic character of the City;
 - c. Inventive representation of the use, name, or logo of the structure or business.

3. Architectural Criteria. The sign shall:
 - a. Utilize or enhance the architectural elements of the building; and
 - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
4. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

Prohibited signs

The following signs are prohibited:

- A. Box signs or cabinet signs, whether on a wall, projecting or on canopies are prohibited except for logo signs permitted as part of an overall sign plan.
- B. Signs employing mercury vapor, low pressure and high pressure sodium and metal halide lighting; plastic panel rear-lighted signs.
- C. Signs on roofs, dormers, and balconies.
- D. Billboards.
- E. Signs painted or mounted upon the exterior side or rear walls on any principle or accessory building or structure, except as otherwise permitted hereunder.
- F. Free standing pylon signs over six (6) feet in height, except community signs.
- G. Back-lit awnings.
- H. Interchangeable letter boards or panels.
- I. Flashing signs.
- J. Off-premises signs, except community signs.

Overall Framework – Stormwater Management

Overview

Water features and drainage systems are essential components of The COR Master Plan. The mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize ‘best management practices’ (BMP) to pretreat run-off, reduce erosion and encourage infiltration, with the understanding that regional treatment will be provided. Water elements are not intended to be separate stand-alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for The COR. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.

Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- ‘Green architecture’, expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



Guideline Recommendations

This framework not only defines the overall role and character of The COR, but also encourages each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout The COR
- **Promote** high quality, creative and appealing aesthetics for all system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support COR development. While this overall strategy addresses the water quantity issue for the entire COR by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.

Overall Framework – Parks/Public Spaces

Overview

The overall design framework of The COR is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout The COR. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

Guideline Recommendations

The Design Framework encourages the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements



Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.

Objectives

Based on the Development Master Plan for The COR, a variety of public and private green space is proposed including:

- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities.
- Playgrounds, game fields and park buildings are park amenities that will target a wider user demand from the surrounding community.
- The Municipal Plaza will connect with all other parks and open space elements, providing a focal point for activity and a sense of place. Gardens, public pavilions and shelters, play lots and open play fields are amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to The COR.



Part Two: Sub-district Framework

Overview

The Master Plan and Zoning Ordinance identify five (5) distinct sub-districts within The COR. Each of these sub-districts is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. The sub-districts are shown the attached Sub-district Map (Figure 7: Sub-district Map). They include:

- **COR1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **COR2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses
- **COR3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **COR4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **COR5 Park and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout The COR

Development plans must respond to the overall COR Design Framework described in Part One of this document, but also must address the following detailed framework standards for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Framework
- Site Design Framework
- Architectural Framework

In all cases, the sub-district framework must be applied in harmony with the overall COR framework and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development. However, within each district, development standards shall be based, in part, on the street hierarchy established in The COR.

Public Realm/Streetscape

- A. The developer shall be responsible for construction of all streetscape improvements along adjacent streets. These improvements include all hardscape improvements, such as trails, sidewalks, benches, bike racks, etc., as well as street trees and other greenscape.
- B. The landowner shall be responsible for maintenance of the streetscape improvements along adjacent streets. Such maintenance may be managed as part of a Special Service District (SSD) or similar.

Plazas

- A. All properties with principal buildings in excess of 30,000 square feet shall be required to have a public plaza space.
- B. The public plaza space shall include benches, bike racks trash receptacles, lighting fixtures and other amenities to create a welcoming space for tenants of the building and members of the public.

C. These plaza spaces should be visible from the public street.

Architecture

These architectural framework standards provide an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



Auto Versus Human Design: Providing rear alleys or side loaded garages enables the home to present itself to the street, which promotes sociability and walkability.

Minimum Standards

Definitions

Build to line: The line at which construction of a building facade is to occur on a lot. The build to line provides a maximum setback for all building facades from the right-of-way or inside edge of the sidewalk, whichever is greater. The build to line applies to all facades abutting a street right of way. Build to line standards are based on the sub-district the parcel is located and the street in which the building is located upon.

Setback: The minimum required distance between a parking lot or the vertical wall of a building and a lot line. Setbacks are typically more flexible than build to lines and allow a building to be set further back from the lot line and street right-of-way.

Requirements:

- A. Additional development standards shall be defined within each sub-district.
- B. Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street.

C. Buildings should be articulated to break up the mass of the building façade on all elevations. Large blank exterior walls shall be prohibited. Windows are encouraged to be used to break up the mass of the building. Rooflines and building elevations should be articulated to break up the mass of buildings. Buildings must be broken at a minimum of every fifty (50) feet. This variation can be accomplished through one (1) or more of the following:

1. Wall face variations (minimum depth of two (2) feet)
2. Pilasters or columns (minimum depth of two (2) feet)
3. Upper wall break in color and/or materials (minimum of 2/3 up the height of the wall)
4. Wainscot (minimum of four (4) feet high)
5. Canopies (minimum four (4) foot width)
6. Corner and demising wall or building separations



More Desirable

D. Each development application must list building materials, roofing materials and building colors on the plans. Building articulation, mechanical screening by architectural elements and design elements on all building elevations must be identified on the plans. The plans shall be subject to City review and approval to ensure that the design intent of the Framework has been met.



Less Desirable

E. Building materials shall be high-quality durable materials, such as glass, stone, brick, windows, canvas awnings, etc.

1. Use local sources if available
2. Siding is not allowed (vinyl, metal, wood) except as a complimentary use
3. Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
4. Metal and wood components are acceptable as a complimentary, not primary, building material
5. COR1 sub-district buildings shall be primarily brick or a comparable alternative and shall comprise a minimum of fifty (50%) percent of the non-glazed wall area

F. Residential development should provide variety within the subdivision to avoid monotony. Developers will include front porches and windows that face all streets. Corner units shall have more than one (1) window per side of building to help create a sense of place in the community and promote safety on the street.

G. Garage forward or “snout houses” are highly discouraged on all residential development as they dominate the view from the street and sidewalk.

H. All sides of buildings shall have an equal finish in terms of materials and general design.

I. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.

- J. Building massing shall be oriented parallel to the street frontage of the lot.
- K. Trash and recycling areas shall be designed internal to the building.
- L. Provide recessed entries wherever possible.
- M. Buildings shall have a base and top to the architecture.
- N. Multi-story buildings are preferred in the COR1 sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of twenty (20) feet to the roofline.
- O. Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – fifty (50%) minimum of total ground level façade, or demonstrate great design through other means.
- P. Provide real window openings for all street facing façades above ground level in all districts—a minimum of forty (40%) percent of total façade.



Residential uses in COR4 and COR5 must also comply with the following:

- A. *Location of main entrance*
 1. A building must include a front porch or covered balcony/patio at all main entrances that face a street. The porch or stoop shall adjoin the main entrance and the main entrance shall be accessible from the porch.
 2. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.
- B. *Porches:* Porches used to satisfy the design criteria shall comply with the following:
 1. Porches shall be covered by a solid roof. The roof shall not be located more than 12 feet above the floor of the porch. If the roof of a required porch is developed as a deck or balcony, it may be flat.
 2. The porch shall have minimum dimensions of 6 feet by 6 feet. (6' x 6'). For single-family detached dwelling units, the covered area provided by the porch must be at least



forty-eight (48) square feet and a minimum of eight (8) feet wide. If the main entrance is for more than one (1) dwelling unit, the covered area provided by the porch must be at least sixty-three (63) square feet and a minimum of nine (9) feet wide.



C. *Covered balconies/patio*: The covered area must be at least forty-eight (48) square feet, a minimum of eight (8) feet wide, and no more than fifteen (15) feet above grade. The covered area must be accessible from the interior living space of the house.

D. *Openings between porch floor and ground*: Openings of more than one (1) foot between the porch floor and the ground must be covered with a solid material or lattice.

E. *Roofs*

1. *Slope*: Principal structures must have a roof that is sloped, with a pitch that is no flatter than six (6) units of horizontal run to twelve (12) units of horizontal rise.
2. *Architectural features*: The roof of a principal structure shall include the following architectural details:
 - a. At least one (1) dormer facing the street. If only one (1) dormer is included, it shall be at least five (5) feet wide and shall be centered horizontally between each end of the front elevation. If more than one (1) dormer is provided, a dormer at least four (4) feet wide must be provided on each side of the front elevation;
or
 - b. A gabled end, or a gabled end of a roof projection, facing the street.
3. *Roof eaves*: Roof eaves must project from the building wall at least twelve (12) inches, measured horizontally, on at least the front and side elevations.



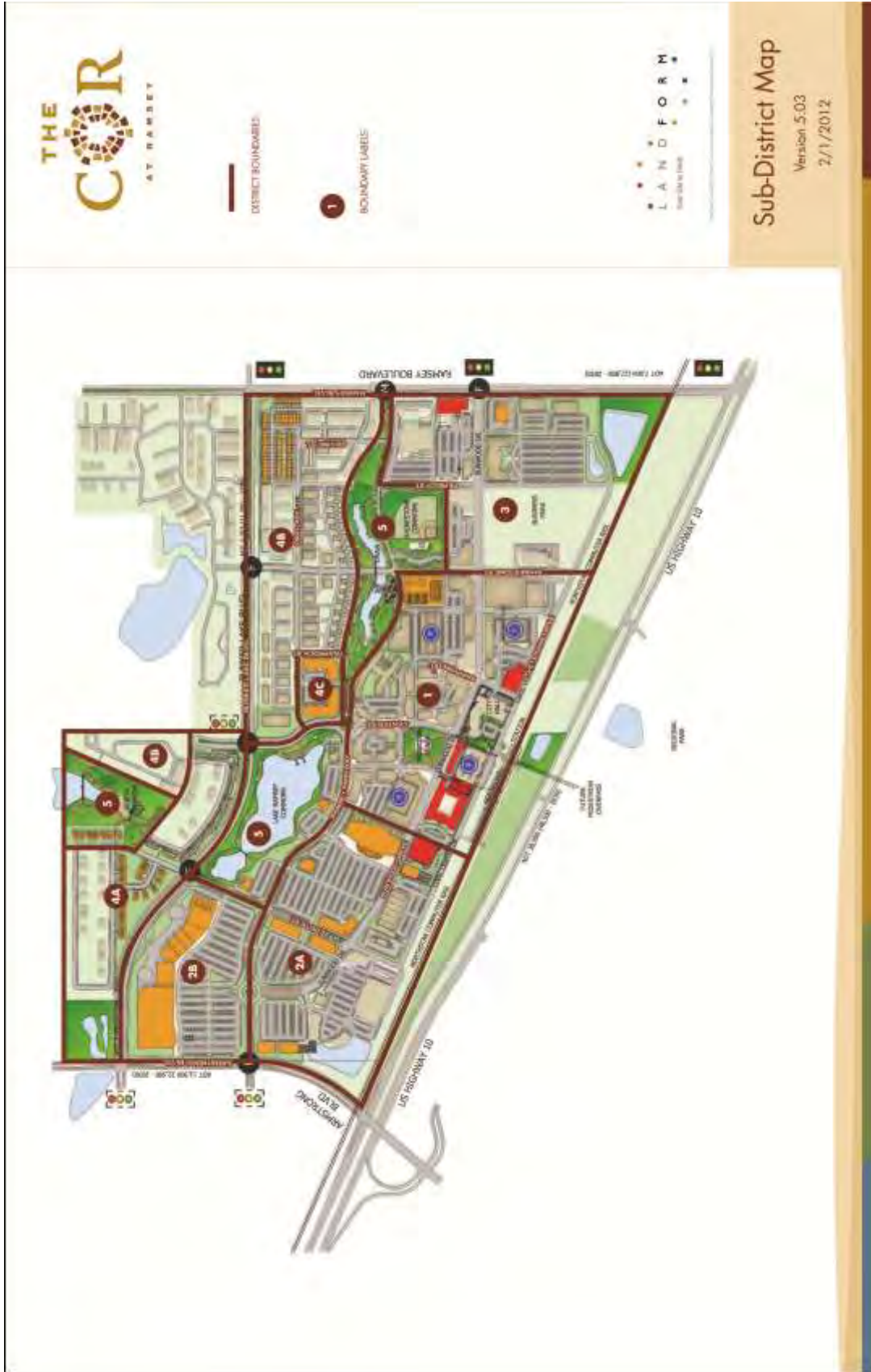


Figure 7: Sub-district Map

Sub-District Framework - COR1 Mixed Use Core

Public Realm / Streetscape

Public realm and streetscape improvements define the character of The COR and create a memorable signature for the Mixed Use Core sub-district. This is the most urban of all sub-districts in The COR and will be developed with a very urban pattern. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year.

Site Development

Framework standards that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, the Design Framework also describes the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within The COR.

Architecture

Buildings in the Mixed Use Core will play a major role in defining the overall character of The COR. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 3

COR1 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.75	.75	.75	.75	.75	.75
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR2 Commercial

Public Realm / Streetscape

While the Commercial Sub-district encourages a mix of large and small floor plates accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential.

Site Development

Site Design Framework for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for The COR. Specifically, this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. However, the market realities suggest that the types of auto-oriented users in this district will have the potential for larger parking fields and drive-throughs that will make a true urban model challenging. The City will hold all users to this design vision while modifying the COR2 standards slightly to accommodate these types of more typical suburban land uses. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours.

Architecture

Buildings in the Commercial Sub-district make a significant contribution to first impressions of The COR. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make The COR unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 4

COR2 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Minimum lot depth	100 feet	100 feet	100 feet	100 feet	100 feet	100 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Boulevard Sod	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	6 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

- A. At least forty (40%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply only to one (1) street frontage.
 1. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
 2. The City may approve a variation from the required build to line if the applicant provides a street edge consisting of fencing, decorative wall and/or landscaping with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
- B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the

level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the forty (40%) percent required on all street frontages.

Sub-District Framework - COR3 Workplace

Public Realm/Streetscape

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of The COR is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. The Draw, an active, neighborhood scale, recreational play space has been included in this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts.

Site Development

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts.

Architecture

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter The COR via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this sub-district suggest a variety of potential building types – blending medical campus with small office, storefront and some housing -- demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 5

COR3 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	40 feet	40 feet	40 feet	40 feet	40 feet	40 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	none	none	none	none	none	none
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR4 Neighborhood

Public Realm/Streetscape

Two prominent public parks supply much of the informal green space for The COR, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Commons surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. The sub-district is adjacent to The Draw, which is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout The COR.

Site Development

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of The COR. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The arterial streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of The COR.

Architecture

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of The COR property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for The COR. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 6

COR4 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR5 Parks & Open Space

Public Realm/Streetscape

The overall structure of The COR is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles.

Site Development

The COR Master Plan illustrates a strong framework of green space, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in The COR.

Architecture

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for The COR. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Additionally, limited retail is planned along the perimeter of the park.

Table 7

COR5 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	0.5	N/A	0.5	0.5	0.5	0.5
Minimum lot width	N/A	N/A	N/A	N/A	N/A	N/A
Minimum lot depth	N/A	N/A	N/A	N/A	N/A	N/A
Allowable residential density in dwelling units per acre ⁽¹⁾	N/A	N/A	N/A	N/A	N/A	N/A
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	N/A	N/A	N/A	N/A	N/A	N/A
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	N/A	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	N/A	no req.	no req.	no req.	no req.
Rear yard	no req.	N/A	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet	N/A	25 feet	25 feet	25 feet	25 feet
Building height (min. - max.)	1-2 stories.	N/A	1-2 stories.	1-2 stories.	1-2 stories.	1-2 stories.
Planter Style	N/A	N/A	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	N/A	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	N/A	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	N/A	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Where commercial buildings are planned, at least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Implementation

Introduction

This framework provides distinct strategies for guiding The COR's form and appearance. This framework's vision for the COR will take several years to implement. Successful implementation will require consistent application of the Design Framework by City staff. Zoning enforcement and design framework standards will not succeed without the cooperation and commitment of landowners, development interests, and the rest of the community.

Design Review

The Design Framework will be administered by staff as part of the development review process. Staff will incorporate design review to ensure compliance with the Design Framework as a standard step in the development review process.

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APPENDIX A: SPECIFICATIONS AND DETAILS

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Appendix A: Specifications and Details

Design Framework – The COR

Purpose

The primary purpose of Design Framework is to set basic parameters, describe preferences and illustrate design intent. These framework standards serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

Application

The format and content of this framework are specifically tailored for use as a supplement to the Zoning Ordinance. This document is organized into two sections. The first section outlines issues and recommendations that apply to the entire COR. The second section highlights specific framework that apply to each sub-district within The COR. The City of Ramsey has adopted Development Plan 5.03 as the Master Plan for The COR. This master plan provides illustrative guidance for building locations and orientation. Any changes to this approved master plan must be reviewed and approved by the City Council as part of the development application. This Development Plan will be updated by the City as changes are approved.

Implied Responsibility

All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

Part One: Overall Framework

There are a number of framework standards that apply to the entire COR, including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall COR framework standards overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Framework:

- Context – local, city, regional
- Street Hierarchy--access, circulation, arrangement
- Streetscape—preferred design, location
- Parking -- quantity, location, type
- Building Design-- preferred uses, horizontal/vertical mixed use, built form, character of development
- Signage—design, location, quantity, type
- Stormwater Management-- surface water features, stormwater management
- Parks/Public Spaces – parks, trails and open space

It is essential that proposed design solutions for development projects and other improvements within The COR demonstrate an understanding of the interplay between these layers.

Overall Framework - Context

Overview

The COR is a unique area within the City; but it is a part of, and connected with, a variety of local, city-wide and regional systems (~~See Figure 1: Development Master Plan 5.03~~). Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

Guideline Recommendations

To ensure that The COR takes full advantage of local and regional systems, development should:

- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/stormwater management plan

Objectives

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

- DEVELOPMENT STATUS**
- EXISTING DEVELOPMENT
 - PROPOSED DEVELOPMENT
 - ACTIVE DEALS
 - UNDER CONTRACT
 - COMPLETED

- ACCESS**
- EXISTING SIGNALIZED INTERSECTION
 - FUTURE SIGNALIZED INTERSECTION
 - FULL INTERSECTION
 - NO LEFT OUTBOUND MOVEMENTS
 - PARKING GARAGE

- TRAFFIC INFORMATION**
- ADT INFORMATION TAKEN FROM 2009 ACTUAL COUNTS AND 2030 PROJECTED VOLUMES



Overall Framework – Street Hierarchy

Overview

The Development Master Plan defines a specific strategy for the layout of streets and blocks within, and around The COR. The size, type and configuration of this street hierarchy (See [Figure 1: Street Hierarchy](#)[Figure 2: Street Hierarchy](#)) is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample space for street-side activities, strolling promenades, pockets of green and other amenities is critical to the success of The COR. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as a primary way to move about and conduct business in the core area.

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in The COR and encourages a more dispersed and less congested traffic flow (many choices or travel paths).
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout The COR.
- Work with Anoka County to convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches.
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street).



Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Arterial street – these streets provide the boundary of The COR and all access into The COR will be from one of these three streets:
 - Bunker Lake Boulevard
 - Armstrong Boulevard
 - Ramsey Boulevard
- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities
 - Sunwood Drive



- Parkway – premier ‘green streets’ adjacent to primary public park / green space, with more landscaping than other streets in The COR
 - Ramsey Parkway
 - Center Street (north of the roundabout)
- Connector street – primary routes that link various districts within The COR, usually providing the most direct means of getting from point A to point B
 - Rhinestone Street
 - Zeolite Street
 - Veterans Drive
- Downtown street – the most urban of street sections where high pedestrian traffic is intended and traffic calming measures such as on street parking, bump outs and other methods will be employed
 - Center Street (south of the roundabout)
 - Sapphire Street (north of Sunwood)
 - Other streets as shown on the Street Hierarchy Map
- Local street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout The COR.



These street hierarchy classifications are intended to provide guidance regarding the streetscape, but it should be noted that within a street classification, there may be construction or design distinctions, based on the sub-district classification. For example, Sunwood Drive is defined as a destination street throughout The COR, but the design may be different between the COR1 and COR2 sub-districts. The Master Streetscape Map will provide design direction.

Transit Overview

The COR is designed to be a Transit Oriented Development (TOD) in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multimodal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. The COR is the Twin Cities' first and only transit-oriented development along the new Northstar Commuter Rail Line. Ramsey Station at The COR will open for riders boarding the train in November 2012. As Northstar's most visible station, The COR will immediately bolster the ridership on the line and continue to build recognition for this regional service.



Transit components include the following:

- Ramsey Star Express service provides peak period, peak direction bus service between downtown Minneapolis and Ramsey until the Northstar Commuter Rail station stop is complete
- Northstar Commuter Rail provides train service between Big Lake and downtown Minneapolis
- 350 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within The COR and into the surrounding neighborhoods
- The COR can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station



The COR also encourages developers to participate in a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. The TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions.

- Developers shall submit a TDM plan with their site plan application for new development and work with the City to implement their plans.

Transit Guideline Recommendations

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in The COR, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following framework:

- Understand and take advantage of opportunities to use existing and proposed transit components

- Provide transit related facilities specifically linked to uses and character within each COR district, including signage and lighting for way-finding and bicycle amenities
- Building future Northstar Commuter Rail riders through the Ramsey Star Express bus service

Transit Objectives

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. The framework for The COR promotes public transportation as part of the solution and suggests a variety of street improvements and other amenities to invite increased use. Photographs throughout the framework suggest some of the many possibilities.

Overall Framework - Streetscape

Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within and around The COR. The amenities within the public right-of-way -and immediately adjacent -play an important role in the aesthetics of The COR. Great streets are essential components of an attractive and inviting public realm. Developing a streetscape that is comfortable and inviting to pedestrians is important to realizing the vision for The COR. The framework will encourage interesting and inviting places for the public.

The Streetscape framework defines the visual character and physical improvements for all public spaces within The COR. The document also provides the general framework and examples for design of proposed quasi-public and private space located within each development parcel.

Public realm/streetscape framework focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space



The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. The City has developed a series of street cross-sections to illustrate these design expectations. Typical street sections are provided in Appendix A as a reference.

Destination and Connector streets must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)



The COR includes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.



Figure 23: Master Lighting Plan

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, including the following:

- Bike racks interspersed throughout The COR to encourage cycling as an alternative mode of transportation.
- Benches on the public sidewalks to encourage people to linger in The COR
- Trees and planters along the streets to create an inviting space
- Wide sidewalks and both on- and off-street bicycle trails
- Kiosks and other way finding tools for visitors and residents in The COR
- Outdoor dining is encouraged on the street side of buildings within all districts.
- Public plaza space is strongly encouraged to be located adjacent to the streets



Objectives

Creative design solutions for streetscape improvements should include:

- Streetscape improvements that provide traffic calming, particularly along Destination and Downtown streets
- Inviting streets that draw you into and through The COR
- A marriage of building design and streetscape design that encourages active spaces near the streets



Street Trees and Plantings

Street trees and planter boxes are a critical element in the unique character of The COR and will be used to help distinguish between different street types and sub-districts within The COR. However, there may be variety within the same street when the street crosses into sub-districts.

- Street trees will be chosen to ensure that the species allow for clear pedestrian access along the streets and require minimal maintenance.
- Along Destination, Downtown, Parkway and Local streets, where regularly spaced blocks exist, one (1) tree species shall be provided per block. In order to provide variety and protect against disease, adjacent blocks shall not be allowed the same street tree. Initial development along a block shall establish the tree species for that block, subject to City review and approval.
- Along streets without regular block space or longer blocks, such as Connector and Arterial streets, the City may approve groupings of 3-6 trees of one (1) species in lieu of the regular block spacing.
- Alternatives to the above mentioned planting plan shall be subject to review and approval by the City Council.

Destination Street Trees:

- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Bicolor Oak (*Quercus bicolor*)
- Sienna Glenn Maple (*Acer x freemanii* 'Sienna')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Ginkgo (*Ginkgo biloba*)
- Accolade Elm (*Ulmus x 'Accolade'*)

Parkway Street Trees:

- Valley Forge Elm (*Ulmus americana* 'Valley Forge')
- Bitternut Hickory (*Carya cordiformis*)
- Siouxlant Poplar (*Populus deltoides* 'Siouxlant')
- Sienna Glen Maple (*Acer x freemanii* 'Sienna')
- Boulevard Linden (*Tilia americana* 'Boulevard')
- Northern Catalpa (*Catalpa speciosa*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Korean Mountain Ash (*Sorbus alnifolia*)
- Ohio Buckeye (*Aesculus glabra*)
- Ginkgo (*Ginkgo biloba*)

Connector Street Trees:

- Bicolor Oak (*Quercus bicolor*)
- Northern Red Oak (*Quercus rubra*)
- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Downtown Street Trees:

- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Local and Arterial Street Trees:

Local and Arterial Street trees may be chosen from any species on the above lists.

Planter Beds/Boxes/Tree Grates

- Planter beds, tree grates and planters shall be as shown on the Streetscape Master Plan. Details and specification shall be added to Appendix A as developed and approved by the City. Appendix A shall be amended as needed.
- Irrigation shall be provided to ensure survival of plant materials. The City shall provide irrigation systems along the Parkway and Destination streets. Unless otherwise approved, all other irrigation shall be the responsibility of the landowner.

Street Furniture

Bike racks, trash enclosures and benches are encouraged on all streets, but will be required on Destination, Connector and Parkways and other streets as shown conceptually on the Streetscape Master Plan.

Bike racks shall be the standard bike rack (see Appendix A for specifications).

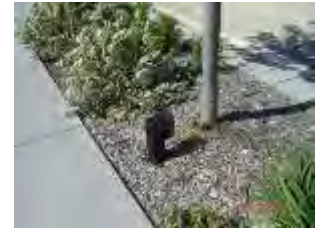
Trash enclosures shall be the standard trash enclosures (see Appendix A for specifications).

Benches shall be the standard bench (See Appendix A for specifications).

Benches shall be grouped in twos that face each other and provide an opportunity for an outdoor conversation space.

Alternative bike rack, trash enclosures and bench designs will be considered on a case-by-case basis. The City will review alternative designs as part of the site plan review and will evaluate these alternatives based on the following:

- Does the design maintain the character of the district?
- Does the alternative design maintain the quality of the standard feature?
- Does the design provide a relationship between the public elements of the streetscape and the building that would not otherwise be possible?



Sidewalks and Boulevard

In the design framework, the term “boulevard” is defined as “the space between the sidewalk and the curb.” The boulevard area provides the required space for public features such as street lights, benches, fire hydrants, street signs, etc. The boulevard will have different characteristics based on the street hierarchy.

The street width and layout within different zones may affect the ability of a particular development to provide the minimum boulevard width and the minimum sidewalk width within the existing public right-of-way. In those cases, the landowner shall provide a permanent public easement over the portion of the sidewalk that is located on private property.

Street Lights

Street lights are owned and installed by Connexus Energy. The streetlight design was developed in conjunction with the City of Ramsey and Connexus (See [Figure 2: Master Lighting Plan](#)~~Figure 3: Master Lighting Plan~~). The standard street lights are designed to accommodate banner arms should banner signage be desired. The banner arms should be the manufacturer's standard break-away banner arms to ensure safety and minimize maintenance costs. Connexus provides street light maintenance for the City. Connexus bills the City of Ramsey for the cost of installation and maintenance.

The choice of street light was intended to emphasize the pedestrian scale of The COR. Light poles within the public right-of-way should be planned to accommodate banners, flowers, hanging artwork and electrical outlets.

There are three (3) different streetlight types within The COR:

- 24-foot high poles at intersections
- 14-foot high poles at mid-block
- 12-foot high poles at The Draw



The City will ensure that the appropriate streetlights are installed on all public streets as described by the Design Framework. Private developers shall provide parking lot or other project lighting that retains these design

objectives. All lighting shall comply with City Code performance standards for lighting. All lighting shall be reviewed by City staff to ensure compliance with the Design Framework and City Code standards described above.

Standards for Outdoor Dining.

Outdoor dining is encouraged as an attractive addition to a pedestrian- and retail-friendly, vital COR. Such dining is encouraged subject to the standards in Section 117-118 of the Zoning Ordinance.

Overall Framework - Parking

Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of The COR is a significant challenge. The goal is to provide adequate parking; both quantity and location, while promoting new strategies that support the overall needs of The COR. Parking will be provided through a combination of on street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of The COR. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street level uses should be integrated into ramp design along important street frontages. The City also encourages developers to wrap the ramp with multi-story uses that will provide commercial and residential opportunities and help to define the street edge.

Guideline Recommendations

Parking guidelines encourage the following:

- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views, soften parking area edges, provide shade, integrate native plantings, offset islands and reduce to 'sea of parking' image
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering
- Provide parallel parking on both public and private streets throughout The COR, to add parking and create a traffic calming, pedestrian buffer; Bumpouts are encouraged as a traffic calming measure on all streets within the COR.
- Explore options to integrate stormwater management requirements into parking area design
- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on street, small surface lot, underground and ramped parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area

- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Discourage driveway access to parking facilities along Arterial, Destination and Parkway streets, in favor of side streets.

Objectives

The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in The COR. The Design Framework anticipates the construction of a total of 3 parking structures in COR1 to serve the needs of the area by providing structured parking that can be shared among users, allow developers to maximize the development on each individual lot.

As discussed later in this section, sizing and timing of the structured parking will be finalized as the area develops and parking demand grows. In addition to providing adequate space, excellent design is a major factor as well. The framework for The COR encourages creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs throughout the Design Framework suggest some of the many possibilities.





Figure 34: Streetscape Master Plan

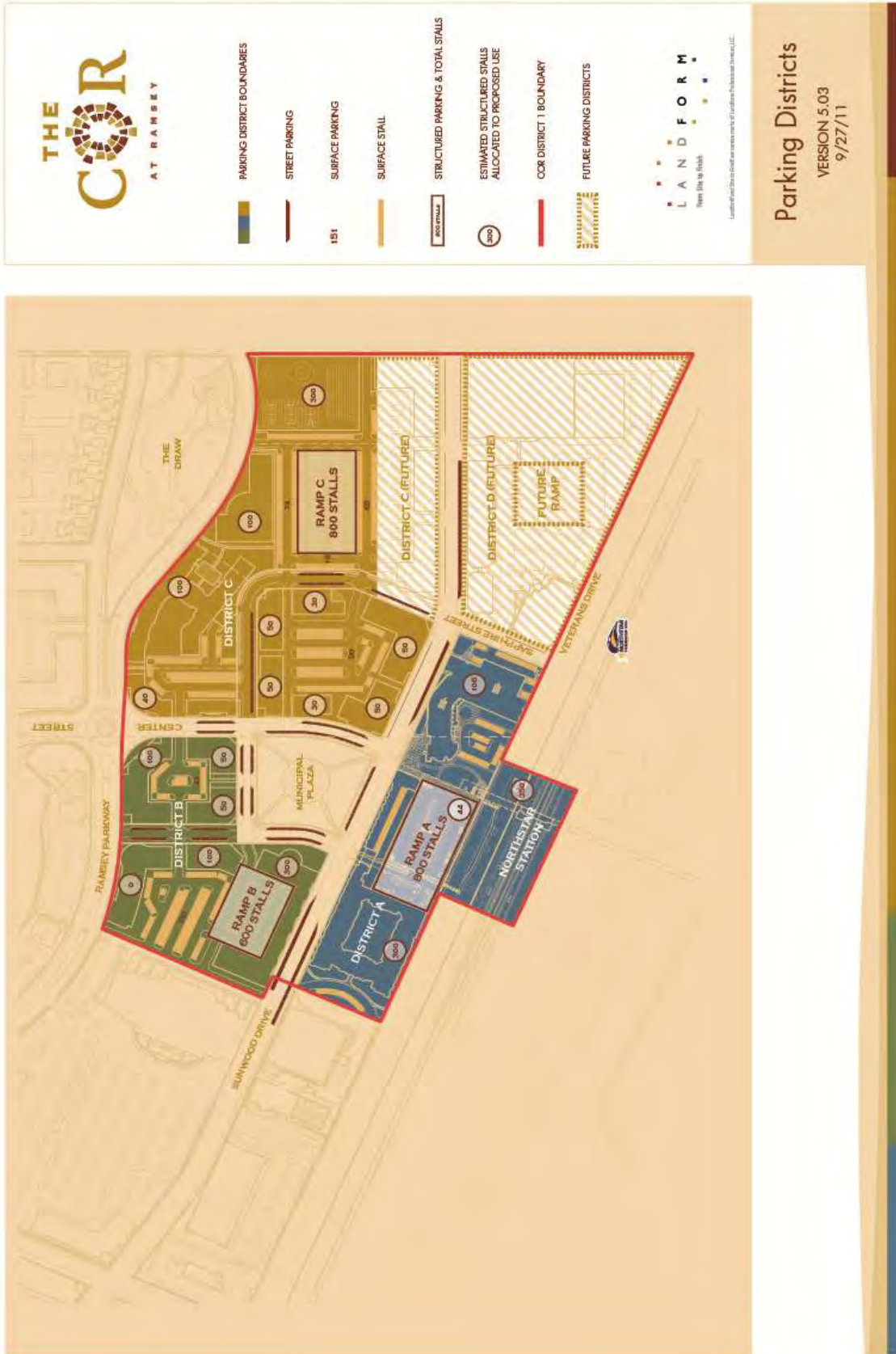
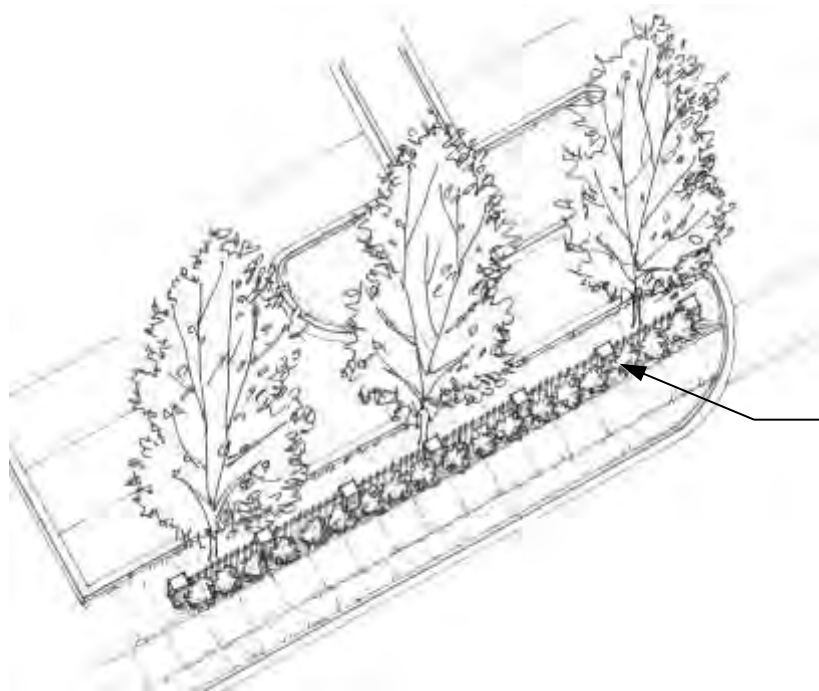


Figure 45: Parking District Map

Minimum Standards

Parking in The COR is intended to be shared to the greatest extent practicable in all mixed-use areas. The parking standards are intended to provide a practical basis for providing adequate parking within the COR District through a careful analysis of uses, shared parking arrangements, use of public street parking and reduction allowances for the proximity to the transit station. A parking plan has been prepared to identify the planned number, location, sharing arrangements and public use (see [Figure 3: Streetscape Master Plan](#), ~~Figure 4: Streetscape Master Plan~~ and ~~Figure 4: Parking District Map~~ [Figure 5: Parking District Map](#)). The parking plan provides information that can be used to help address longer term parking arrangements where the possibility of use conversion or building expansion exists. Parking shall be provided under any of the following arrangements:

- A. No parking shall be located between the front of the principal structure and the street on parcels abutting Destination and Downtown streets in COR1.
- B. Not more than fifty (50%) percent of the parking shall be located in front of the principal structure and the street on parcels abutting Arterial, Parkway, Connector and Local streets.
 1. In the COR2 district, developments may be allowed more than fifty (50%) percent parking in the front yard, provided all other design standards have been met, including the use of a wall, railing, hedge or combination to create a street edge and screen parking.
- C. Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet ($4\frac{1}{2}'$) above the level of the parking lot, at the build-to line required for the property. The amount of required screening may vary by sub-district and street frontage and is described in Part Two of this document.



Landscaped edge that combines fence with trees, low shrubs and ground cover for parking lot screening and buffering along sidewalk to screen parking

- D. No parking spaces shall be located on corner lots at the point of street intersections.
- E. Shared parking in COR1 will be accomplished through a Parking Use and Maintenance Agreement (PUMA).
- F. Off-street parking in the COR2 and COR3 sub-districts shall be secured for public use through parking easements and other appropriate conveyances. Shared parking arrangements between nearby uses are encouraged in both sub-districts.
 - 1. The City may approve joint parking for one or more businesses where the total number of parking stalls provided for joint use is less than the sum of the total required for each business should they provide them separately. The applicant must demonstrate the feasibility of the arrangement in a written report. Such a permit shall not be granted except when the following conditions are found to exist:
 - i. Proximity. The building or use for which application is being made to utilize the off-street parking facilities provided by another building or use shall be located within three hundred (300') feet of such parking facilities, excluding public rights-of-way.
 - ii. Conflict in Hours. The applicant shall demonstrate in documented fashion that there is no substantial conflict in the principal operating hours of the two (2) buildings or uses for which joint use of off-street parking facilities is proposed.
 - iii. Written Consent and Agreement. A legally binding instrument, executed by the parties concerned, for joint use of off-street parking facilities, duly approved as

to title of grantors or lessors, and in a form and manner of execution approved by the City Attorney, shall be filed with the City Clerk and recorded with the Anoka County Recorder or Registrar of Titles, and a certified copy of the recorded document shall be filed with the City within sixty (60) days after approval of the joint parking use by the City or the interim use permit shall be considered null and void.

- G. On-street parking adjacent to buildings shall not be used for the purposes of calculating parking requirements.
- H. Within the COR-4 sub-district, parking on individual parcels serving individual uses may be provided if designated and approved as part of the master plan.
- I. Within all sub-districts, a minimum driveway length of twenty-five (25') feet is required to limit conflicts between vehicles parked in the driveway and street or sidewalk users.
- J. In order to ensure the pedestrian orientation of the COR Districts, minimum/maximum on-site parking standards are set based on the following table:

Table 1

Use	Minimum number of parking stalls	Maximum number of parking stalls*
Retail	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Restaurants	3 per 1,000 sq. ft. 1 per each 2 seats	5 per 1,000 sq. ft. 1 per each 1 seats.
Offices	2 per 1,000 sq. ft.	3 per 1,000 sq. ft.
Medical offices, clinics	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Health clubs	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Theaters, places of assembly	1 per 5 seats	1 per 3 seats
Residential		
Attached or detached	1 per unit	2 per unit
Multifamily units	1 per unit	2 per unit

*If a parking structure is provided on site, maximum parking stalls do not apply.

- K. Non-Specified Uses. For uses not specifically listed above, off-street parking requirements shall be computed by the Zoning Administrator on the same basis as required for the most similar listed uses. In such cases, the Zoning Administrator shall also consult off-street parking reference materials including, but not limited to, manuals prepared by the American Planning Association and Institute of Transportation Engineers.
- L. Handicap Parking. The size, number, and location of stalls reserved for handicapped parking shall be provided and identified as required by applicable regulations. These spaces are included in the computation for the minimum parking space requirement.
- M. Change in Land Use. When the site intensity or use of a building and/or property is increased with consequential effect upon the parking requirements as prescribed in this Section, the

parking requirements as prescribed herein shall be used to provide for such increase in the site intensity and/or use.

- N. Use of required parking spaces. Required parking spaces must be available for the use of residents, customers or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.
- O. Transit Service Reduction. The minimum number of parking stalls required on site may be reduced by 10% for any parcel located within ¼ mile of a transit stop. To qualify for this reduction, the transit stop must provide regular service on all days of the week and direct pedestrian access must be provided between the building and the transit stop.
- P. Bike Racks. In addition to the bike racks provided as part of the streetscape, individual businesses are encouraged to provide bike racks for customers. Bike rack locations will be reviewed and approved by the City as part of the site plan review.
- Q. Increase to Maximum Required Parking. If additional parking is sought to exceed these maximum requirements, a conditional use permit can be sought to increase maximums up to twenty-five (25%) percent.
 - 1. In the COR-2 sub-district, an increase of twenty-five (25%) percent in the maximum number of parking stalls is permitted without the issuance of a conditional use permit if approved as part of the site plan.
- R. In the COR1 sub-district, a PUMA (Parking Use and Maintenance Agreement) shall be applied to all development as follows:
 - 1. It is the City's intent to maximize development on individual lots. In order to help landowners accomplish this goal, the City will develop public parking ramps, which will be available for public parking within The COR. In order to minimize the amount of the site required for parking, the landowner may choose to enter into the PUMA for utilization of the public parking ramps to meet all or a portion of their minimum parking requirements.
 - 2. Each development shall be subject to a PUMA that specifies that each property in the COR1 shall be financially responsible for its proportionate share of a shared parking facility. The proportionate share shall be determined on the basis of the property's off-street parking needs, as determined by the parking requirements of Item J above.

3. The City shall establish fees by ordinance. The developer shall be responsible for payment in an amount equal to the value of the required parking on a per-stall price basis for ramp parking through the PUMA. The City will determine the appropriate contribution.



4. Funds collected by the City shall be deposited in a special fund used only to acquire and/or develop off-street parking facilities for the COR. Financial responsibility shall cover the construction and continuing maintenance of the parking facility.
5. The parking facility may be constructed and maintained by the City or by a private management entity acceptable to the City.

Private Streets

Private streets may be allowed within residential subdivisions, subject to the standards in Section 117-112 (e)3.b. of the Zoning Ordinance.

Loading Areas

- A. Loading areas and docks shall be located in the rear of the building.
- B. Loading areas shall not be located in the front yard and shall be fully screened from public streets, public parks and residential areas through landscaping and building design measures.



B.

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Overall Framework – Building Design

Overview

The COR Development Plan 5.03 identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, this Design Framework focuses on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

Guideline Recommendations

The mix of land uses in The COR will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, The COR resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of The COR, anchored by City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.

Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street, particularly in the center of the district.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, ‘big box’ retailing and auto-oriented uses within the COR2 sub-district only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a ‘family’ or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district.
- All uses should reinforce the street edge.



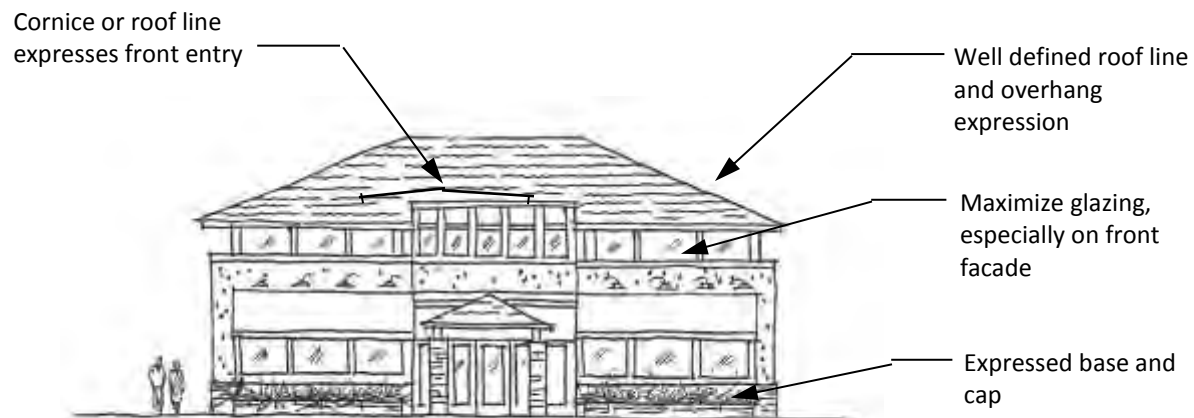
Architectural Overview

High quality architecture is an essential component in fulfilling the objectives of the Design Framework. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

Architecture Recommendations

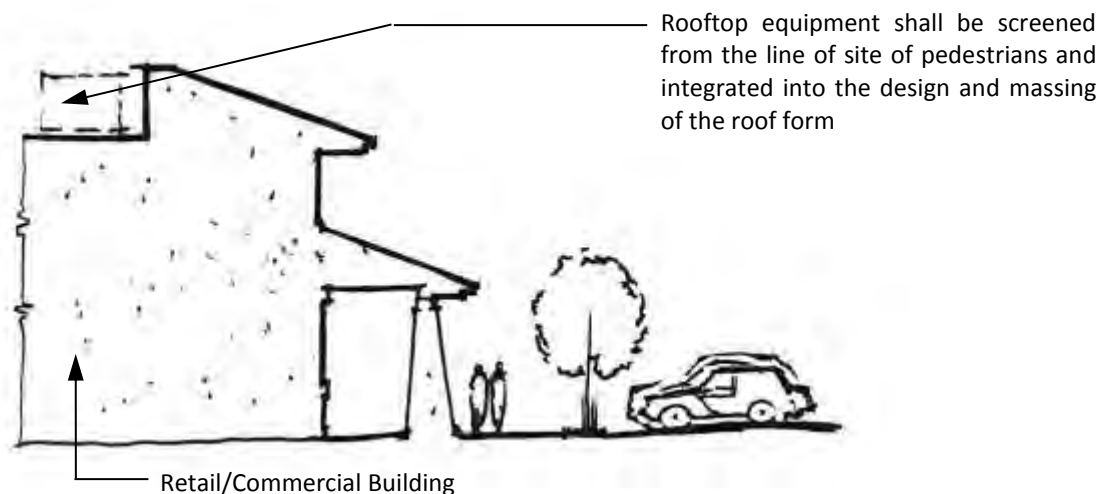
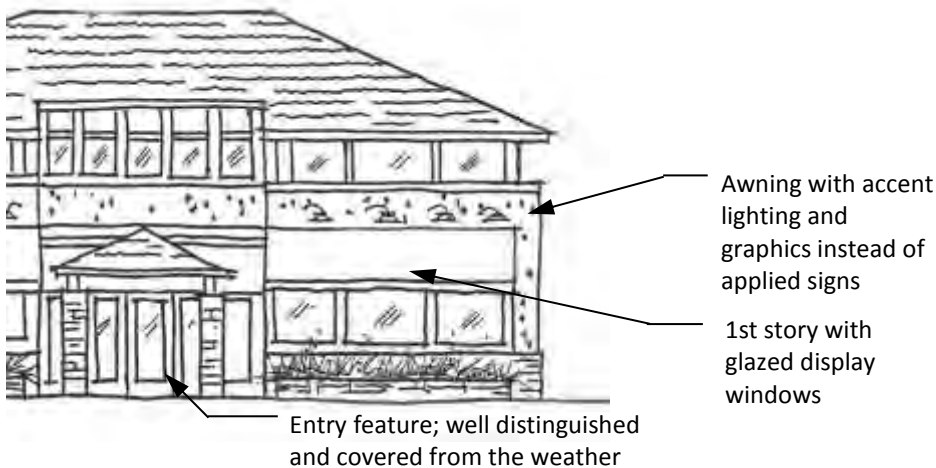
Of all the layers that combine to form The COR Development Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for The COR. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- An animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience

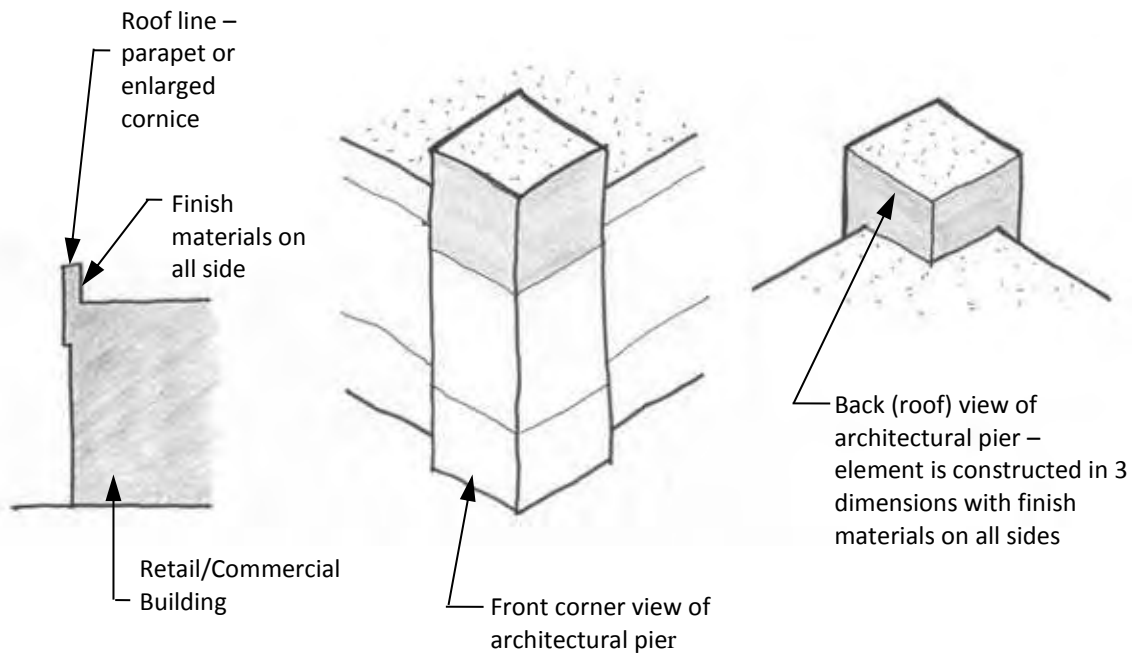


- Variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other COR uses
- Use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets and important crossroads throughout The COR
- Buildings should always be located at all four corners of intersections.
 - In the COR2 sub-district, if the City finds that there are unique circumstances where buildings cannot be located on the corner, the City may approve the use of fences, walls or other design elements to create that "street edge"

- Buildings will be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, or where streets intersect with parks, such as the streets that front on or end near the Municipal Plaza. These locations can often be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as The COR develops
- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.
- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Use recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage



- Equipment, mechanical systems, transformers, etc. must be screened from view in a way that is integral to the architecture of the building. Screening should be accomplished with materials similar to or compatible with materials used on the main structure; metal fencing or mechanical vent screens alone are not sufficient.
- Roof lines and cornice details shall be completed in a 3-dimensional manner so that the back of roof features or similar unfinished areas are not visible.



Overall Framework - Signage

Overview

Signage within The COR should provide a system for clear wayfinding for all modes of transportation and should ensure successful business operation while maintaining the design aesthetic of this unique project. These sign standards are intended to allow flexibility and creativity while maintaining the design concepts of The COR. Due to the distinctive nature of The COR, the sign standards from the City Code are not applicable and signage shall be allowed as prescribed by the Design Framework. A Master Sign Plan identifies the location of key sign locations (see [Figure 5: Signage Plan](#)~~Figure 6: Signage Plan~~).

Guideline Recommendations

The COR development manager shall prepare a Master Sign Plan for the public elements of the project, which are in compliance with the design framework and will be adopted by reference. The Master Sign Plan will specifically address public signs including:

- The three (3) off-site community signs
- Banner signs on light poles
- Signage for public parks
- Signage for public parking ramps
- Signage for public buildings
- Community kiosks



Objectives

Signage should be used as a tool to help identify businesses and neighborhoods with The COR and should have elements that are focused on the pedestrian nature of The COR as well as the signage needs for businesses.

- Signage and lighting which is integrated into the design of the building is encouraged.

Definitions

Awning Sign means a sign incorporated into or attached to an awning.

Community sign means an off-site sign identifying the development name and key tenants. Community signs may also include public information. Reader boards are allowed to be incorporated into community signs within The COR. [Gateway and kiosk signs are part of this category.](#)

Temporary Sign means a sign which is erected or displayed for a limited period of time and not affixed to a Minnesota State Building Code approved structure.

Project sign means a freestanding area identification sign which identifies a single-family or multifamily residential subdivision, a commercial development or an industrial park or office park and which is located on the same [parcel-site](#) as the development it identifies. [A site shall be identified as the block in which the use is located.](#)

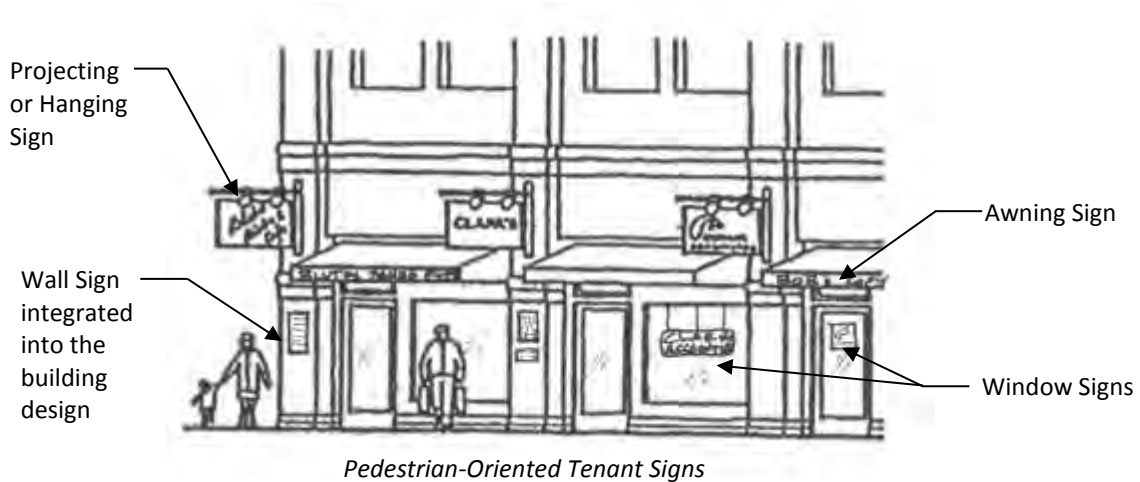
Projecting sign means any sign, all or any part of which extends beyond the surface of the building or wall by more than 16 inches.

Public Realm/Streetscape sign means any sign erected and maintained by public officials or public agencies, or approved and authorized for use by state or local governmental authorities.

Sandwich Board Signs means a self-supporting, freestanding temporary sign with only two (2) sides that are situated adjacent to a business with the intent to attract pedestrian traffic to businesses. Sandwich board signs are not meant to be read by vehicular traffic.

Wall sign means any sign which is affixed to a wall of any building. This definition includes individual letter signs and signs on mansards.

Window sign means a sign installed inside a window, or any sign placed within a building for the purpose of being visible from the public right-of-way. This does not include merchandise on display.



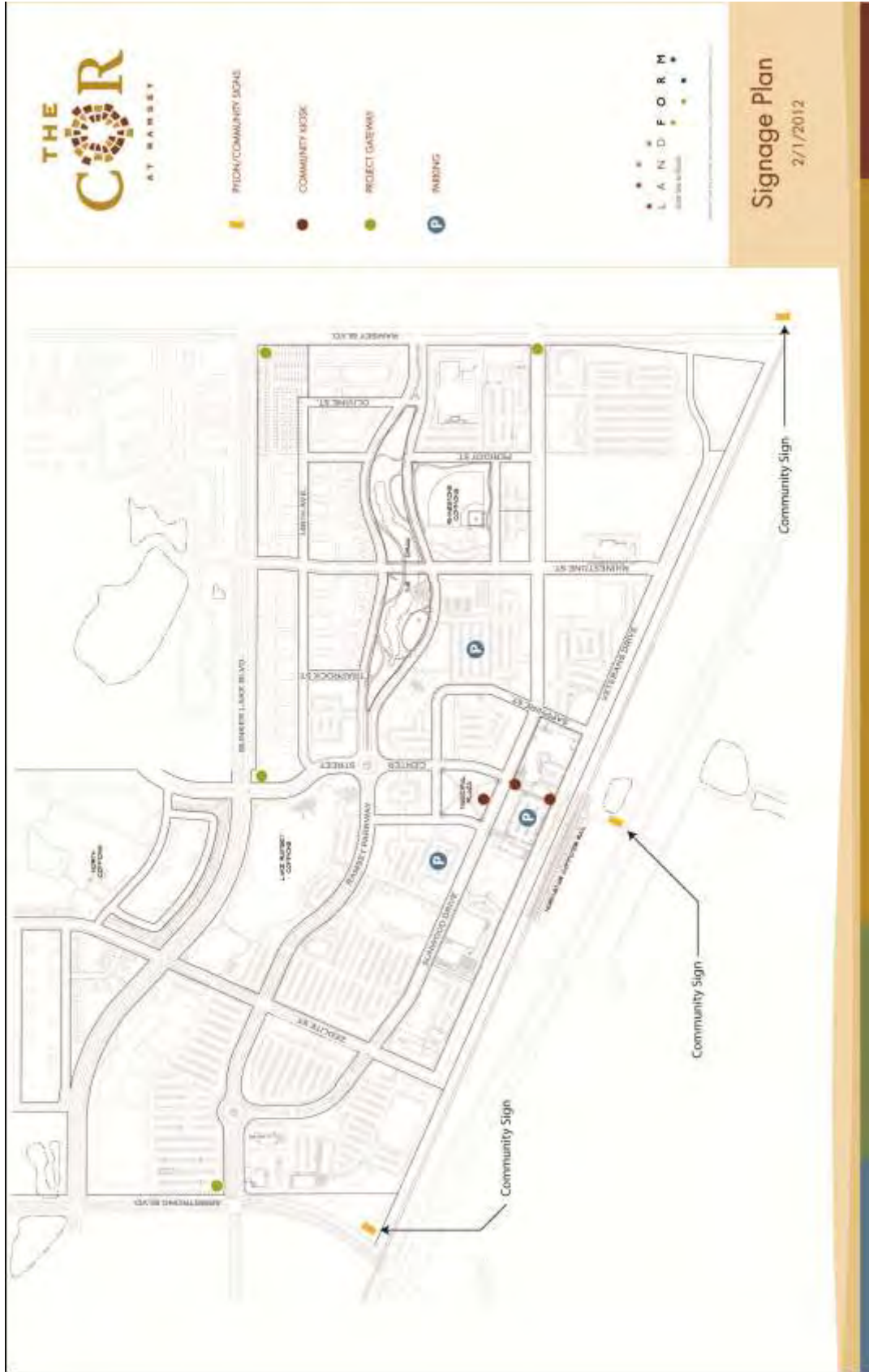


Figure 56: Signage Plan

Standards

Public Realm/Streetscape Signs

The pedestrian oriented nature of The COR requires unique signage that will build on the design concepts for The COR. These types of signs include banner signs on the streetlights, kiosks, street signs, wayfinding signs, signage for public parking ramps, etc. As the design concept for The COR has been refined, so has the signage plan. ~~The COR development manager shall develop standards for these public realm signs as part of the Master Sign Plan. The Master Sign Plan shall be incorporated by reference and may be updated from time to time. Such signage shall be allowed on public spaces as needed to provide information and wayfinding. No advertising shall be allowed on Public Realm/Streetscape Signs except for community events.~~ The Master Sign Plan is included as Appendix B. Size, location, and construction specifications of these signs shall be determined by the Master Sign Plan, which must be approved by the City Council, after a recommendation by the Planning Commission. Careful foresight has been given to size of signs based on needs of the adjacent roadways in order to maintain adequate safety standards that may otherwise be compromised by poor design. Roadways with higher speeds warrant larger lettering to maintain safe travel.

Community and Gateway signs

Community signs include are planned to include three (3) signs on Highway 10, as shown on the Signage Plan (see ~~Figure 5: Signage Plan~~ Figure 6: Signage Plan). The signs include the existing Community Sign near the future transit plaza, the sign at the northwest corner of Highway 10 and Ramsey Boulevard and the planned sign at the northwest corner of Highway 10 and Armstrong Boulevard.

Gateway signs are planned at multiple locations at entrances to The COR from major roadways, as shown on the Signage Plan. General gateway signs may identify The COR only. Retail Gateway signs may include provisions for off-premise, tenant sign panels.

These off-site signs, part of a unified development, are allowed as shown on the signage plan and shall have consistent materials and colors. ~~The Master Sign Plan shall detail the materials, colors and standards for these three (3) community signs. The Master Sign Plan shall include standards for which tenants shall be allowed on the community signs.~~ Community and Gateway Signs shall conform to the design of the Master Sign Plan. Tenants within the unified development shall be allowed to utilize tenant panels on the Community and Retail Gateway Signs.

Amendments to the Master Sign Plan and Approved Community Signs. As the Master Sign Plan is adopted as part of the zoning ordinance, and has the effect of being a zoning ordinance, amendments to the Master Sign Plan must be processed by Ordinance and approved by City Council after review and recommendation by the Planning Commission.

<u>Sign Type</u>	<u>Allowable Size</u>	<u>Architecture</u>	<u>Allowable Height</u>
<u>Community Sign</u>	<u>300 Square Feet</u>	<u>Per Appendix B</u>	<u>30 Feet *</u>
<u>Gateway Sign</u>	<u>50 Square Feet</u>	<u>Per Appendix B</u>	<u>10 Feet</u>
<u>Retail Gateway Sign</u>	<u>150 Square Feet</u>	<u>Per Appendix B</u>	<u>12 Feet</u>
<u>Community Kiosk</u>	<u>32 Square Feet</u>	<u>Per Appendix B</u>	<u>10 Feet*</u>
<u>Parking Ramp Signage</u>	<u>100 Square Feet</u>	<u>Per Appendix B</u>	<u>(Wall Signage)</u>

* From highest point of adjacent road centerline



Desirable signs are designed to be architecturally compatible with buildings within the development and other signage within the development

Project Signs

Project signs are permitted within all COR sub-districts.

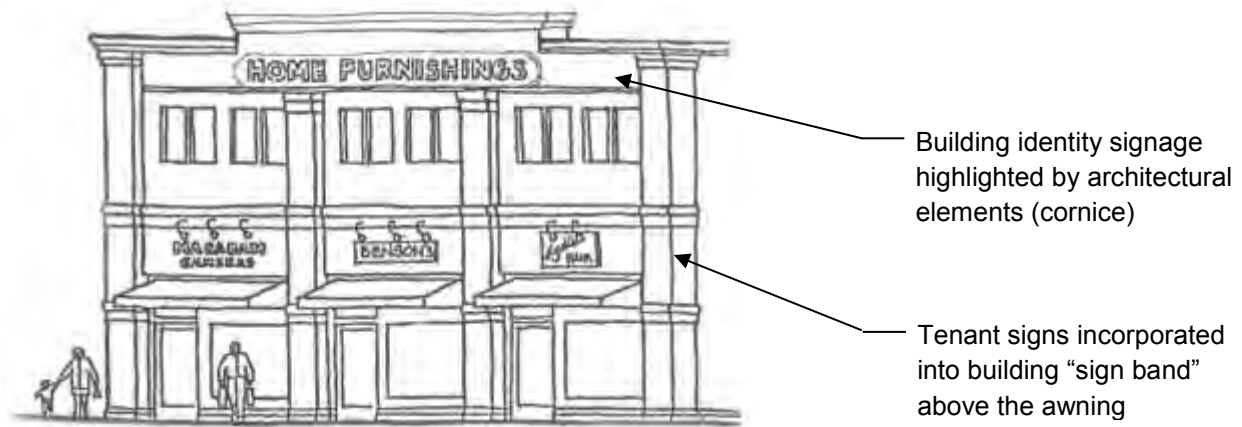
One (1) freestanding sign is allowed per building, provided that the following standards are met:

- A. The building in which the advertising business is located, shall be set back a minimum of six (6) feet from a public street right-of-way.
- B. The area of each face of the signboard shall not exceed six (6) square feet and the signboard shall not have more than two (2) readable faces. The signboard may exceed six (6) square feet, but not more than 32 square feet, provided that the sign consists of individual letters, without internally illuminated box letters. Lighting may be achieved through 'halo' lighting effects.
- C. The height of the top of the signboard, or of any posts, brackets, or other supporting elements shall not exceed six (6') feet from the ground.
- D. The signboard shall be constructed of wood, acrylic, aluminum or metal and shall be architecturally compatible with the style, composition, materials, colors and details of the building.
- E. No part of the sign shall encroach on the right-of-way and its location shall not interfere with pedestrian or vehicular circulation.
- F. Limited to one (1) sign per building and shall not be in addition to wall-mounted, applied letter or projecting signs.

- G. The readable faces of the sign shall be perpendicular to the adjacent street.

In the COR2 District, the following signage shall be allowed:

- A. Ground sign. There shall not be more than one (1) ground sign for each parcel. The gross surface area of a ground sign shall not exceed one hundred (100) square feet for each exposed face nor exceed an aggregate gross surface area of two hundred (200) square feet. Ground signs may be shared and combined at areas of shared entrances for multiple users. Shared ground signs shall not exceed 150 square feet. A shared sign shall be allowed for each entrance to the site (defined as bordered by public streets).
- B. Menu board. One (1) on-site menu board per drive-up or walk-up lane of a drive-in restaurant up to a maximum of thirty-two (32) square feet each and a maximum height of three (3) feet. Menu boards are allowed a message on one (1) side only and cannot contain an advertising message.
- C. Directional signs.
 - 1. Directional or instructional signs are permitted in accordance with Section 117-463(I).
 - 2. Parking lot directional signs designating parking area entrances and exits are limited to one (1) sign for each entrance and/or exit and shall not exceed four (4) square feet for each exposed face. Parking lot directional signs shall not project higher than five feet (5') in height, as measured from the established grade of the parking area to which such signs are accessory.
 - 3. Parking lot instructional signs designating the conditions of use or identification parking areas shall not exceed eight (8) square feet and shall not project higher than ten (10) feet in height for wall signs and seven (7) feet in height for ground signs, as measured from the established grade of the parking area to which such signs are accessory.



Wall Signs

Wall signs are permitted within all COR sub-districts.

Wall signs shall be permitted on one (1) wall, except that lots with frontage on more than one (1) street may have signage on one (1) wall per street frontage.

Within the COR1, COR3, COR4 and COR5 districts, the following standards apply:

- A. Wall-mounted or painted signs, provided the following standards are met:
 1. The sign shall be affixed to the front facade of the building, and shall project outward from the wall to which it is attached no more than six inches (6").
 2. Single Tenant Buildings. Each building shall be allowed one (1) wall sign per street frontage. Signage must be located on the street elevation. The area of the sign shall not exceed ~~five-fifteen (515%)~~ five-fifteen (515%) percent of the ground floor building facade area. Individual signs may not exceed or fifty (50) square feet, whichever is less.
 3. Multi-Tenant Buildings.
 - a. Each building shall be allowed wall signage on the street frontage elevation. For buildings with multiple street frontages, the allowable signage is per street frontage. Signage must be located on the street elevation.
 - b. Each building is allowed wall signage that shall not exceed 5% of the wall area of the building façade adjacent to the street. or Individual signs may not exceed 50 square feet, whichever is less.
 - c. Tenant signage must be located on the tenant lease space.
 - d. At least fifty (50%) percent of the allowed signage must be allocated to ground floor tenants and located on the ground floor.
 - e. Signage is limited to a maximum of one (1) sign per business/tenant, except that tenants with frontage on multiple streets may be permitted to have signage on each street frontage.
 - f. Multi-tenant buildings must have wall signs of similar design. Sign permits for

tenants in multi-tenant buildings shall only be permitted by the City after the building owner has submitted a comprehensive sign plan approved by the Zoning Administrator. The comprehensive sign plan for the building shall include similar design standards including sign material, color, style, spacing and size.

4. Signs for buildings facing Highway 10. Single or multi-tenant buildings that have frontage on Highway 10 or are separated from Highway 10 only by other public right-of-way, shall be allowed to have signage on that street elevation that is up to ~~five-fifteen~~ (5.15%) percent of the wall area of that building façade with no maximum square footage.
- B. Wall-mounted building directory signs identifying the occupants of a commercial building, including upper story business uses, provided the following standards are met:
1. The sign is located next to the entrance.
 2. The sign shall project outward from the wall to which it is attached no more than six inches (6").
 3. The sign shall not extend above the parapet, eave, or building facade.
 - ~~4. The area of the signboard shall not exceed three (3) square feet, with each tenant limited to one (1) square foot.~~
 - ~~5.4.~~ The height of the lettering, numbers, or graphics shall not exceed ~~four-twelve~~ inches (124").
 - ~~6.5.~~ One (1) such sign is allowed per public building entrance and is allowed in addition to other permitted wall signage.
- C. Applied letters may substitute for wall-mounted signs, if constructed of painted wood, painted cast metal, bronze, brass, acrylic or black anodized aluminum. The height of applied letters shall not exceed 12 inches.
- D. Logos are considered signs and shall be included in the maximum allowable sign area.
- E. Window or door signs, provided that the following standards are met:
1. The sign shall not exceed ten percent of the window or door area or four (4) square feet, whichever is less.
 2. The sign shall be silk screened, hand painted, applied letters/graphics, neon tubing or other sign technologies that meet these standards.
 3. Limited to one (1) sign per business, applied on either the window or the door, but not on both.

4. The sign shall not have an opaque backing of any type although smoked glass is allowed.

5. Window signage (includes graphics) shall be permitted on the same building walls that have, or are allowed to have, wall signage pursuant to Item A above. Window signage shall be deducted from the allowable wall signage area.

Within the COR2 district, where more conventional suburban development is expected, rather than the neo-traditional development planned for the other COR sub-districts, the following standards apply:

- A. Wall, canopy or marquee sign. Total sign area may not exceed 15 percent of the front building facade. At least 50 percent of the signage area must be placed on the measured wall with remaining signage area, if desired, distributed on any other wall. Sign height shall not exceed the top of the parapet wall or, if no parapet wall, sign height shall not exceed the height of the eaves. The gross surface area of a wall, canopy or marquee sign may be increased by ten percent if such wall sign:
 1. Consists only of individual, outlined alphabetic, numeric and/or symbolic characters without background except that provided by the building surface to which the sign is to be affixed;
 2. Illumination, if any, is achieved through shielded illumination, shielded silhouette lighting, or shielded spot lighting but not any lighting where the light source is visible or exposed on the face or sides of the characters; and
 3. A wall, canopy or marquee sign may be located on the outermost wall of any principle building but shall not project more than 16 inches from the wall to which the sign is to be affixed. A wall sign shall not project higher than the parapet line of the wall to which the sign is to be affixed.
- B. Window signs are restricted to 30 percent of the area of the window in which the sign is to be displayed.

In all COR sub-districts, restaurants and cafes shall be permitted one (1) wall-mounted display featuring the actual menu as used at the dining table, to be contained within a shallow wood or metal case and clearly visible through a glass front. The display case shall be attached to the building wall, next to the main entrance, at a height of approximately five feet (5'), shall not exceed a total area of two square feet, and may be lighted. This signage is allowed in addition to other permitted wall signage.

Projecting Signs

Projecting signs are permitted within all COR sub-districts.

Projecting signs, including graphics or icon signs, mounted perpendicular to the building wall, are encouraged in all sub-districts, provided the following standards are met:

- A. The sign area shall not exceed ~~six-thirty-two~~ (632) square feet.
- B. The distance from the ground to the lower edge of the signboard shall be ten feet or greater.



- C. The height of the top edge of the signboard shall not exceed the height of the wall from which the sign projects, if attached to a single story building, or the height of the sill or bottom of any second story window, if attached to a multistory building.
- D. The distance from the building wall to the signboard shall not exceed six inches (6").
- E. The width of the signboard shall not exceed three (3) feet.
- F. Limited to one (1) sign per business. Projecting sign area shall be deducted from the allowable wall signage area.
- G. Granted to ground floor commercial uses only.

Awning Signs

Awning signs are allowed in all COR sub-districts. Awning signs are allowed for ground floor uses only, provided that the following standards are met:

- A. If acting as the main business sign, it shall not exceed 24 square feet in area, ~~and the height of the lettering, numbers, or graphics shall not exceed 12 inches.~~
- B. If acting as an auxiliary business sign, it shall be located on the valance only, shall not exceed four (4) square feet in area, and the height of the lettering, numbers, or graphics shall not exceed four inches (4").
- C. Limited to two such signs per business, per frontage.
- D. If acting as the main business sign, it shall not be in addition to a wall-mounted or applied letter sign.

Sandwich Board Signs

Sandwich board signs are permitted within all COR sub-districts.

- A. One (1) sandwich board sign per business is permitted in any business, commercial and mixed use district and shall be located within five feet (5') of the main building entrance to the business it advertises.
- B. Sandwich board signs shall be displayed only during open business hours and must be removed daily.
- C. Sandwich board signs shall be no more than a total of two feet (2') in width and three feet (3') in height



- D. Sandwich board signs must leave a minimum of five feet (5') of clearance for pedestrian access if placed on a public or private sidewalk. Sandwich board signs may not hinder the ability of persons to access vehicles parked at the curb and/or access to a building.
- E. Acceptable materials for sandwich board signs shall include the following: metal, wood synthetic materials such as a chalk board and whiteboard. Sandwich board signs shall not be illuminated, nor shall they contain moving parts, or have balloons, streamers, stringers, pennants or similar adornments attached to them. Sandwich board signs shall be maintained in a good appearance at all times.
- F. No sandwich board sign shall be secured, tethered or installed on traffic devices, utility equipment, street furniture, street lights, or any other public fixture.
- G. Sandwich board signs are temporary signs and shall not be counted towards the total sign area of the site for permanent signage.

Temporary Signs

Temporary signs are allowed per Section 117-465 (Temporary signs) of the Zoning Ordinance, except as noted below:

- A. Freestanding temporary signs shall only be allowed in the COR~~2~~ and COR3 Districts.
- B. In all other COR districts, temporary signs shall be limited to temporary sandwich board signs or temporary wall signage, without interchangeable letters.
- ~~B-C.~~ Real estate and leasing signs shall be exempt from temporary and permanent sign regulations, provided that these signs do not exceed thirty-two (32) square feet.

Off-Site Signs

Off-site signs are prohibited except for wayfinding and community signage specifically allowed by the Design Framework.

Table 2

Sign Standards					
Development Standard	COR1	COR2	COR3	COR4	COR5
Project Signs					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign (maximum)	6 sq. ft./6 feet high***	100 sq. ft./6 feet high	6 sq. ft./6 feet high***	6 sq. ft./6 feet high	6 sq. ft./6 feet high
Menu Board**	N/A	1 per drive-through lane/36 40 sq. ft. max.	N/A	N/A	N/A
Directional Sign	N/A	Per 117-463(l)	N/A	N/A	N/A
Wall Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign					
Single Tenant Building	50 sq. ft. or 15% of the ground floor façade area, -or- 15% of the front façade area when facing Highway 10, whichever is less	15% of façade area	50 sq. ft. or 15% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less
Multi-Tenant Buildings	50 sq. ft. or 15% of the façade area, whichever is less	15% of façade area	50 sq. ft. or 15% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less
Building Facing Highway 10	15% of the façade area	15% of façade area	15% of the façade area	15% of the façade area	15% of the façade area
Window Sign					
Number of signs allowed	1	1	1	1	1
Size of Sign	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	30% of area of window. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.
Directory Sign					
Number of signs allowed	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance
Size of Sign	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum
Projecting Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide
Awning Sign					
Number of signs allowed	2 per business	2 per business	2 per business	2 per business	2 per business
Size of Sign	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.

*One (1) sign is allowed per street frontage (either wall or project)

**Restaurants and cafes in all districts are allowed one (1) wall mounted menu board not to exceed 2 sq. ft. in addition to other permitted wall signage.

***See Page 32, 'Project Signs' for allowances up to 32 square feet.

Additional standards

- A. Businesses with service entrances may identify these with one (1) wall-mounted or applied letter sign not exceeding two square feet.
- B. One (1) directional sign, facing a rear parking lot. This sign may be any type of permitted sign other than a freestanding sign, but shall be limited to four (4) square feet in area.

Creative Sign Standards

- A. *Purpose.* This section establishes standards and procedures for the design, review, and approval of creative signs. The purposes of this creative sign program are to:
 - 1. Encourage signs of unique design, and that exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - 2. Provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- B. *Applicability.* An applicant may request approval of a sign permit for a creative sign to authorize onsite signs that employ standards that differ from the other provisions of this chapter but comply with the provisions of this section.
- C. *Application Requirements.* A sign permit application for a creative sign shall include all information and materials required by the City.
- D. *Procedure.* A sign permit application for a creative sign shall be subject to review and approval by the City as part of the Site Plan review process. When the creative sign is proposed after site plan review is complete, a creative sign may be approved by the Zoning Administrator when the proposed sign is fifty square feet or less, and shall be approved by the City Council when the sign is larger than fifty square feet.
- E. *Design Criteria.* In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
 - 1. Design Quality. The sign shall:
 - a. Constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area;
 - b. Be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - c. Provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
 - 2. Contextual Criteria. The sign shall contain at least one (1) of the following elements:
 - a. Classic historic design style;

- b. Creative image reflecting current or historic character of the City;
 - c. Inventive representation of the use, name, or logo of the structure or business.
3. Architectural Criteria. The sign shall:
- a. Utilize or enhance the architectural elements of the building; and
 - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
4. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

Prohibited signs

The following signs are prohibited:

- A. Box signs or cabinet signs, whether on a wall, projecting or on canopies are prohibited except for logo signs permitted as part of an overall sign plan.
- B. Signs employing mercury vapor, low pressure and high pressure sodium and metal halide lighting; plastic panel rear-lighted signs.
- C. Signs on roofs, dormers, and balconies.
- D. Billboards.
- E. Signs painted or mounted upon the exterior side or rear walls on any principle or accessory building or structure, except as otherwise permitted hereunder.
- F. Free standing pylon signs over six (6) feet in height, except community signs.
- G. Back-lit awnings.
- H. Interchangeable letter boards or panels.
- I. Flashing signs.
- J. Off-premises signs, except community signs.

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Overall Framework – Stormwater Management

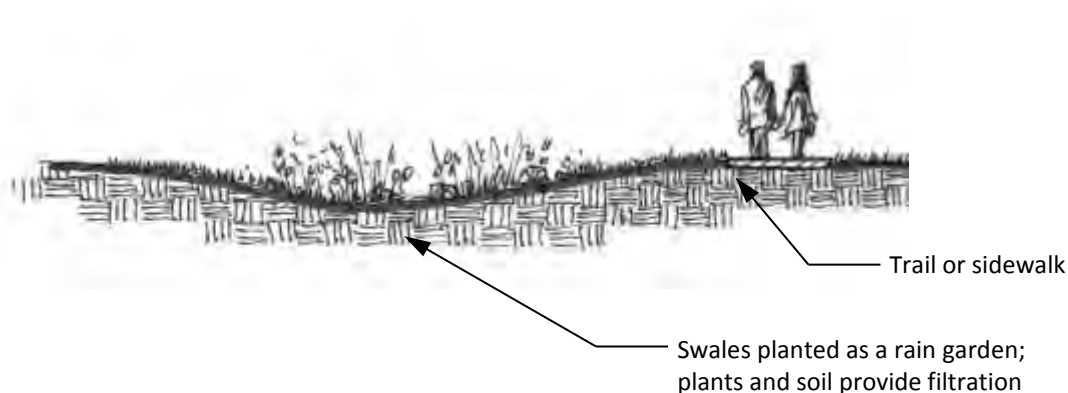
Overview

Water features and drainage systems are essential components of The COR Master Plan. The mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize ‘best management practices’ (BMP) to pretreat run-off, reduce erosion and encourage infiltration, with the understanding that regional treatment will be provided. Water elements are not intended to be separate stand-alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for The COR. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.

Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- ‘Green architecture’, expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



Guideline Recommendations

This framework not only defines the overall role and character of The COR, but also encourages each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout The COR
- **Promote** high quality, creative and appealing aesthetics for all system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support COR development. While this overall strategy addresses the water quantity issue for the entire COR by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.

Overall Framework – Parks/Public Spaces

Overview

The overall design framework of The COR is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout The COR. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

Guideline Recommendations

The Design Framework encourages the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements



Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.

Objectives

Based on the Development Master Plan for The COR, a variety of public and private green space is proposed including:

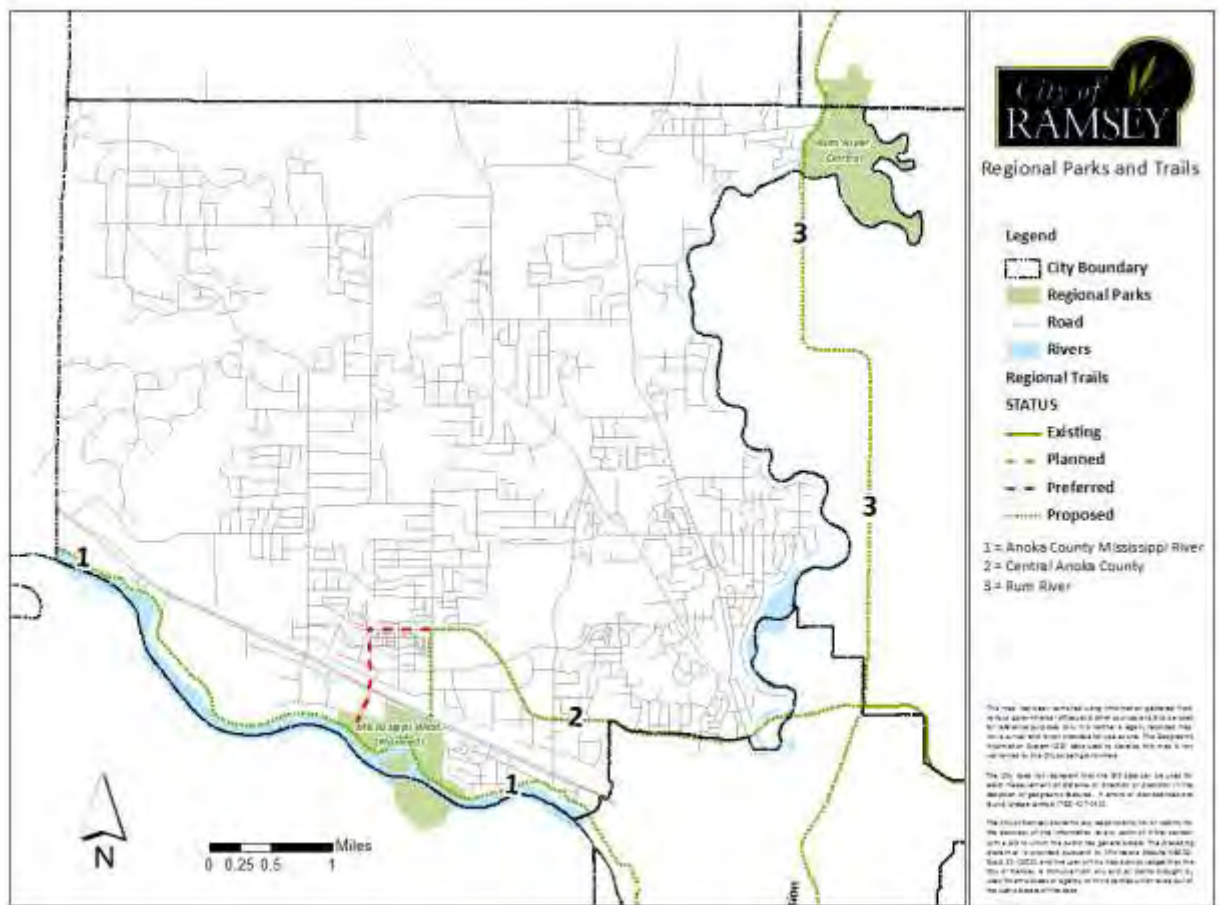
- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities.
- Playgrounds, game fields and park buildings are park amenities that will target a wider user demand from the surrounding community.
- The Municipal Plaza will connect with all other parks and open space elements, providing a focal point for activity and a sense of place. Gardens, public pavilions and shelters, play lots and open play fields are amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to The COR.



Regional Systems

The COR is the heart of several regional systems that are either located within or adjacent to The COR. These regional systems include the Northstar Commuter Rail, the Central Anoka County Regional Trail, the Mississippi West Regional Park, and the Mississippi River Trail.

A number of these systems have a pedestrian component. As such, Ramsey desires to locate the future Central Anoka County Regional Trail through the heart of The COR, along Center Street. The map below indicates Ramsey's preferred route for this trail. As this trail requires crossing Highway 10, it is imperative that a safe, grade-separated crossing is realized. This pedestrian overpass is envisioned to be connected to the Northstar Commuter Rail – Ramsey Station, and has been named the 'Mississippi Skyway'.



Part Two: Sub-district Framework

Overview

The Master Plan and Zoning Ordinance identify five (5) distinct sub-districts within The COR. Each of these sub-districts is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. The sub-districts are shown the attached Sub-district Map (~~Error! Reference source not found.Figure 7: Sub-district Map~~). They include:

- **COR1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **COR2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses
- **COR3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **COR4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **COR5 Park and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout The COR

Development plans must respond to the overall COR Design Framework described in Part One of this document, but also must address the following detailed framework standards for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Framework
- Site Design Framework
- Architectural Framework

In all cases, the sub-district framework must be applied in harmony with the overall COR framework and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development. However, within each district, development standards shall be based, in part, on the street hierarchy established in The COR.

Public Realm/Streetscape

- A. The developer shall be responsible for construction of all streetscape improvements along adjacent streets. These improvements include all hardscape improvements, such as trails, sidewalks, benches, bike racks, etc., as well as street trees and other greenscape.
- B. The landowner shall be responsible for maintenance of the streetscape improvements along adjacent streets. Such maintenance may be managed as part of a Special Service District (SSD) or similar.

Plazas

- A. All properties with principal buildings in excess of 30,000 square feet shall be required to have a public plaza space.

- B. The public plaza space shall include benches, bike racks trash receptacles, lighting fixtures and other amenities to create a welcoming space for tenants of the building and members of the public.
- C. These plaza spaces should be visible from the public street.

Architecture

These architectural framework standards provide an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



Auto Versus Human Design: Providing rear alleys or side loaded garages enables the home to present itself to the street, which promotes sociability and walkability.

Minimum Standards

Definitions

Build to line: The line at which construction of a building facade is to occur on a lot. The build to line provides a maximum setback for all building facades from the right-of-way or inside edge of the sidewalk, whichever is greater. The build to line applies to all facades abutting a street right of way. Build to line standards are based on the sub-district the parcel is located and the street in which the building is located upon.

Setback: The minimum required distance between a parking lot or the vertical wall of a building and a lot line. Setbacks are typically more flexible than build to lines and allow a building to be set further back from the lot line and street right-of-way.

Requirements:

- A. Additional development standards shall be defined within each sub-district.

- B. Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street.
- C. Buildings should be articulated to break up the mass of the building façade on all elevations. Large blank exterior walls shall be prohibited. Windows are encouraged to be used to break up the mass of the building. Rooflines and building elevations should be articulated to break up the mass of buildings. Buildings must be broken at a minimum of every fifty (50) feet. This variation can be accomplished through one (1) or more of the following:
 1. Wall face variations (minimum depth of two (2) feet)
 2. Pilasters or columns (minimum depth of two (2) feet)
 3. Upper wall break in color and/or materials (minimum of 2/3 up the height of the wall)
 4. Wainscot (minimum of four (4) feet high)
 5. Canopies (minimum four (4) foot width)
 6. Corner and demising wall or building separations



More Desirable

- D. Each development application must list building materials, roofing materials and building colors on the plans. Building articulation, mechanical screening by architectural elements and design elements on all building elevations must be identified on the plans. The plans shall be subject to City review and approval to ensure that the design intent of the Framework has been met.



Less Desirable

- E. Building materials shall be high-quality durable materials, such as glass, stone, brick, windows, canvas awnings, etc.
 1. Use local sources if available
 2. Siding is not allowed (vinyl, metal, wood) except as a complimentary use
 3. Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
 4. Metal and wood components are acceptable as a complimentary, not primary, building material
 5. COR1 sub-district buildings shall be primarily brick or a comparable alternative and shall comprise a minimum of fifty (50%) percent of the non-glazed wall area
- F. Residential development should provide variety within the subdivision to avoid monotony. Developers will include front porches and windows that face all streets. Corner units shall have more than one (1) window per side of building to help create a sense of place in the community and promote safety on the street.
- G. Garage forward or “snout houses” are highly discouraged on all residential development as they dominate the view from the street and sidewalk.
- H. All sides of buildings shall have an equal finish in terms of materials and general design.

I. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.



J. Building massing shall be oriented parallel to the street frontage of the lot.

K. Trash and recycling areas shall be designed internal to the building.

L. Provide recessed entries wherever possible.

M. Buildings shall have a base and top to the architecture.

N. Multi-story buildings are preferred in the COR1 sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of twenty (20) feet to the roofline.



O. Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – fifty (50%) minimum of total ground level façade, or demonstrate great design through other means.

P. Provide real window openings for all street facing façades above ground level in all districts—a minimum of forty (40%) percent of total façade.

Residential uses in COR4 and COR5 must also comply with the following:

A. *Location of main entrance*

1. A building must include a front porch or covered balcony/patio at all main entrances that face a street. The porch or stoop shall adjoin the main entrance and the main entrance shall be accessible from the porch.
2. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.



B. *Porches:* Porches used to satisfy the design criteria shall comply with the following:

1. Porches shall be covered by a solid roof. The roof shall not

be located more than 12 feet above the floor of the porch. If the roof of a required porch is developed as a deck or balcony, it may be flat.

2. The porch shall have minimum dimensions of 6 feet by 6 feet. (6' x 6'). For single-family detached dwelling units, the covered area provided by the porch must be at least forty-eight (48) square feet and a minimum of eight (8) feet wide. If the main entrance is for more than one (1) dwelling unit, the covered area provided by the porch must be at least sixty-three (63) square feet and a minimum of nine (9) feet wide.



- C. *Covered balconies/patio*: The covered area must be at least forty-eight (48) square feet, a minimum of eight (8) feet wide, and no more than fifteen (15) feet above grade. The covered area must be accessible from the interior living space of the house.

- D. *Openings between porch floor and ground*: Openings of more than one (1) foot between the porch floor and the ground must be covered with a solid material or lattice.

E. *Roofs*

1. *Slope*: Principal structures must have a roof that is sloped, with a pitch that is no flatter than six (6) units of horizontal run to twelve (12) units of horizontal rise.
2. *Architectural features*: The roof of a principal structure shall include the following architectural details:
 - a. At least one (1) dormer facing the street. If only one (1) dormer is included, it shall be at least five (5) feet wide and shall be centered horizontally between each end of the front elevation. If more than one (1) dormer is provided, a dormer at least four (4) feet wide must be provided on each side of the front elevation; or
 - b. A gabled end, or a gabled end of a roof projection, facing the street.
3. *Roof eaves*: Roof eaves must project from the building wall at least twelve (12) inches, measured horizontally, on at least the front and side elevations.





Sub-District Framework - COR1 Mixed Use Core

Public Realm / Streetscape

Public realm and streetscape improvements define the character of The COR and create a memorable signature for the Mixed Use Core sub-district. This is the most urban of all sub-districts in The COR and will be developed with a very urban pattern. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year.

Site Development

Framework standards that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, the Design Framework also describes the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within The COR.

Architecture

Buildings in the Mixed Use Core will play a major role in defining the overall character of The COR. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 3

COR1 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.75	.75	.75	.75	.75	.75
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR2 Commercial

Public Realm / Streetscape

While the Commercial Sub-district encourages a mix of large and small floor plates accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential.

Site Development

Site Design Framework for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for The COR. Specifically, this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. However, the market realities suggest that the types of auto-oriented users in this district will have the potential for larger parking fields and drive-throughs that will make a true urban model challenging. The City will hold all users to this design vision while modifying the COR2 standards slightly to accommodate these types of more typical suburban land uses. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours.

Architecture

Buildings in the Commercial Sub-district make a significant contribution to first impressions of The COR. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make The COR unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 4

COR2 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Minimum lot depth	100 feet	100 feet	100 feet	100 feet	100 feet	100 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Boulevard Sod	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	6 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

- A. At least forty (40%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply only to one (1) street frontage.
1. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
 2. The City may approve a variation from the required build to line if the applicant provides a street edge consisting of fencing, decorative wall and/or landscaping with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the

level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the forty (40%) percent required on all street frontages.

Sub-District Framework - COR3 Workplace

Public Realm/Streetscape

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of The COR is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. The Draw, an active, neighborhood scale, recreational play space has been included in this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts.

Site Development

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts.

Architecture

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter The COR via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this sub-district suggest a variety of potential building types – blending medical campus with small office, storefront and some housing -- demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 5

COR3 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	40 feet	40 feet	40 feet	40 feet	40 feet	40 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	none	none	none	none	none	none
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR4 Neighborhood

Public Realm/Streetscape

Two prominent public parks supply much of the informal green space for The COR, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Commons surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. The sub-district is adjacent to The Draw, which is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout The COR.

Site Development

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of The COR. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The arterial streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of The COR.

Architecture

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of The COR property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for The COR. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 6

COR4 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR5 Parks & Open Space

Public Realm/Streetscape

The overall structure of The COR is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles.

Site Development

The COR Master Plan illustrates a strong framework of green space, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in The COR.

Architecture

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for The COR. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Additionally, limited retail is planned along the perimeter of the park.

Table 7

COR5 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	0.5	N/A	0.5	0.5	0.5	0.5
Minimum lot width	N/A	N/A	N/A	N/A	N/A	N/A
Minimum lot depth	N/A	N/A	N/A	N/A	N/A	N/A
Allowable residential density in dwelling units per acre ⁽¹⁾	N/A	N/A	N/A	N/A	N/A	N/A
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	N/A	N/A	N/A	N/A	N/A	N/A
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	N/A	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	N/A	no req.	no req.	no req.	no req.
Rear yard	no req.	N/A	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet	N/A	25 feet	25 feet	25 feet	25 feet
Building height (min. - max.)	1-2 stories.	N/A	1-2 stories.	1-2 stories.	1-2 stories.	1-2 stories.
Planter Style	N/A	N/A	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	N/A	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	N/A	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	N/A	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Where commercial buildings are planned, at least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

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Implementation

Introduction

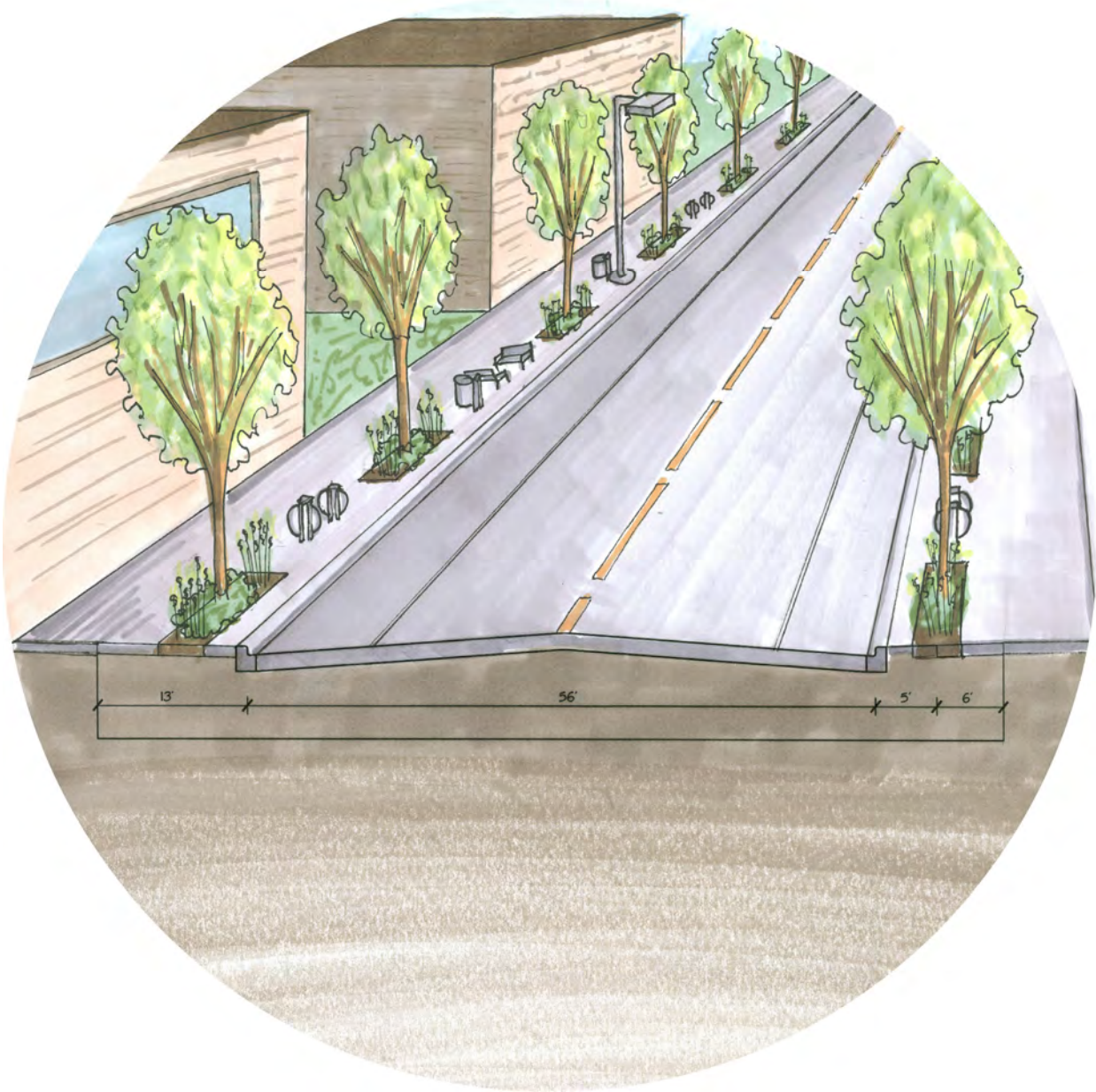
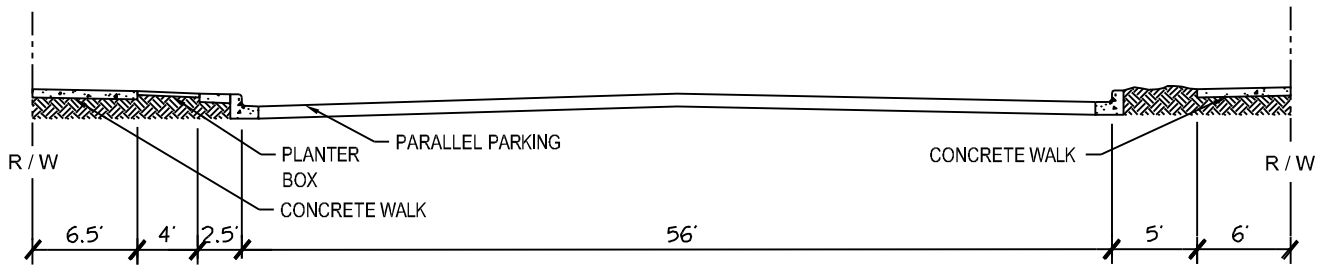
This framework provides distinct strategies for guiding The COR's form and appearance. This framework's vision for the COR will take several years to implement. Successful implementation will require consistent application of the Design Framework by City staff. Zoning enforcement and design framework standards will not succeed without the cooperation and commitment of landowners, development interests, and the rest of the community.

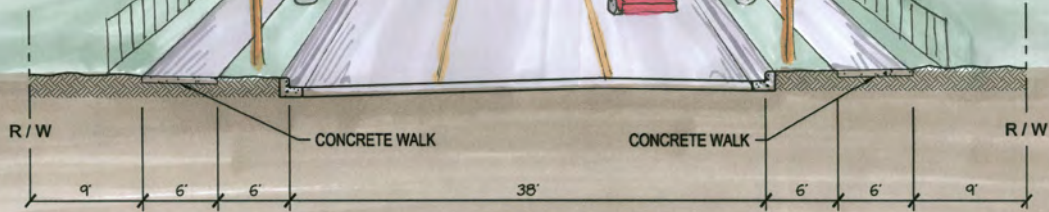
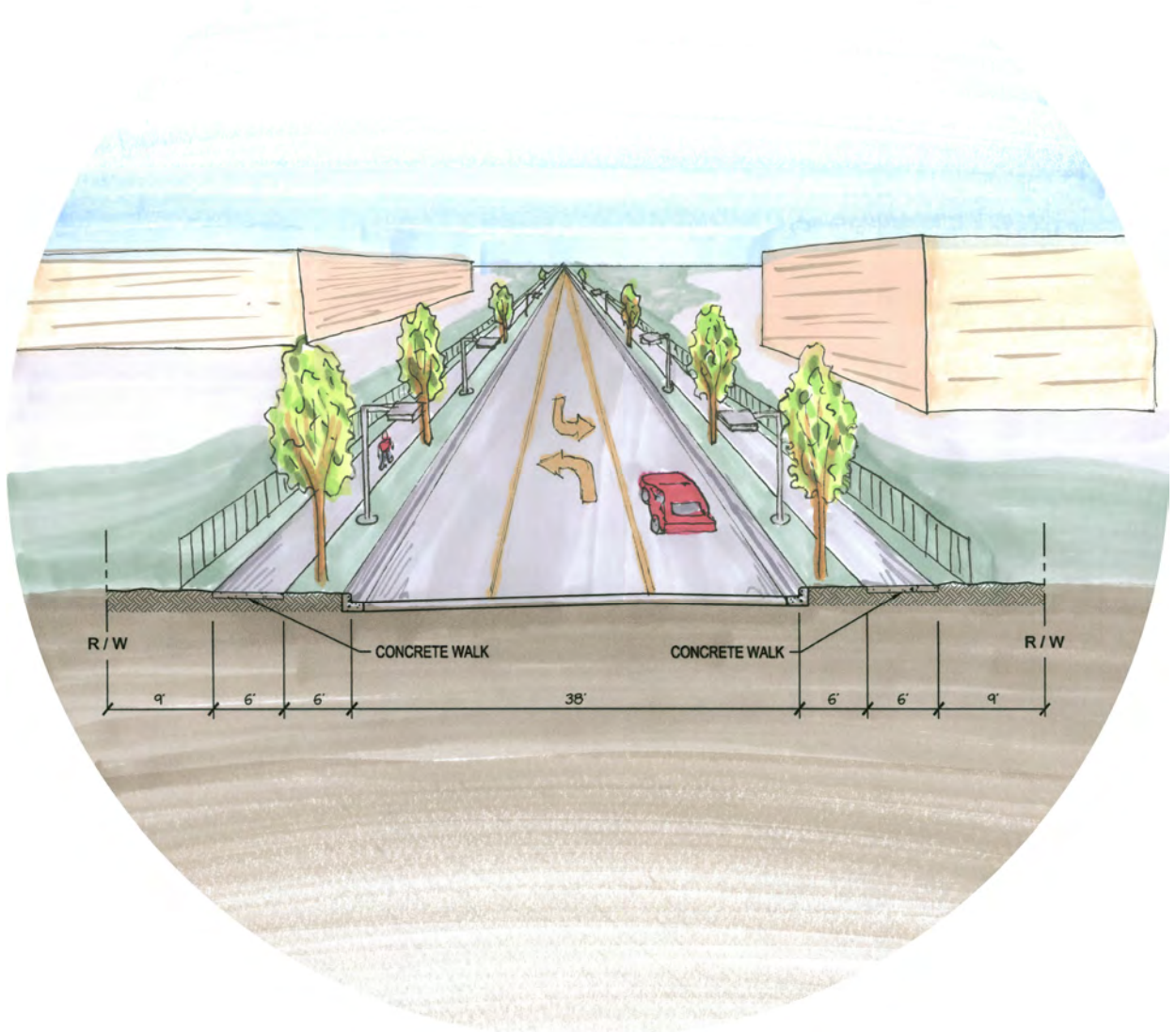
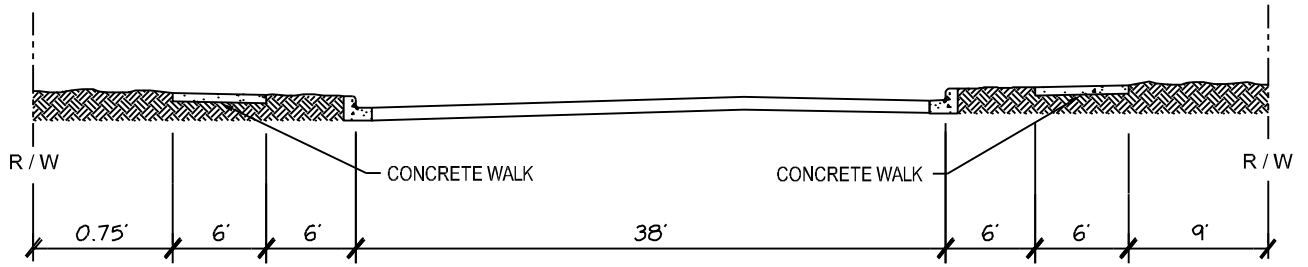
Design Review

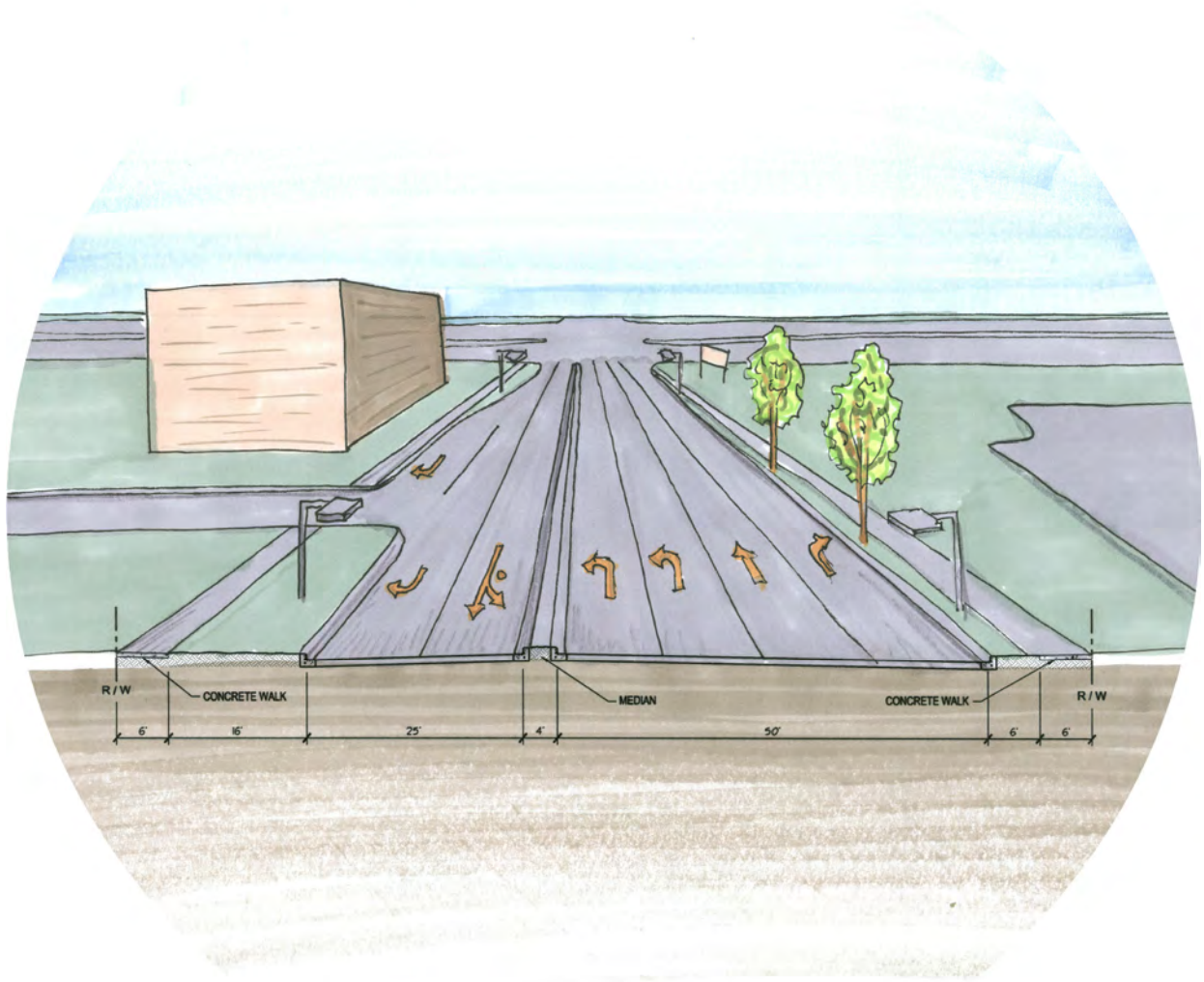
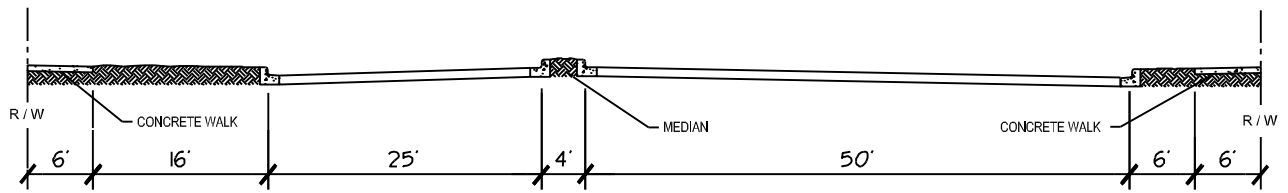
The Design Framework will be administered by staff as part of the development review process. Staff will incorporate design review to ensure compliance with the Design Framework as a standard step in the development review process.

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APPENDIX A: SPECIFICATIONS AND DETAILS







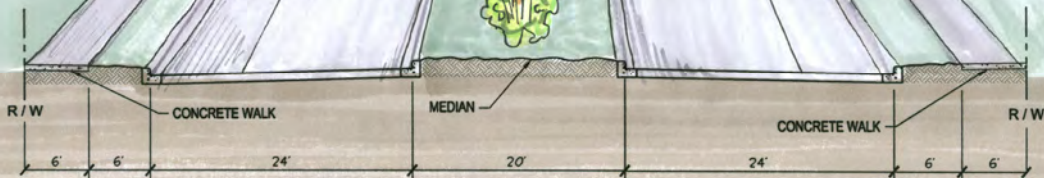
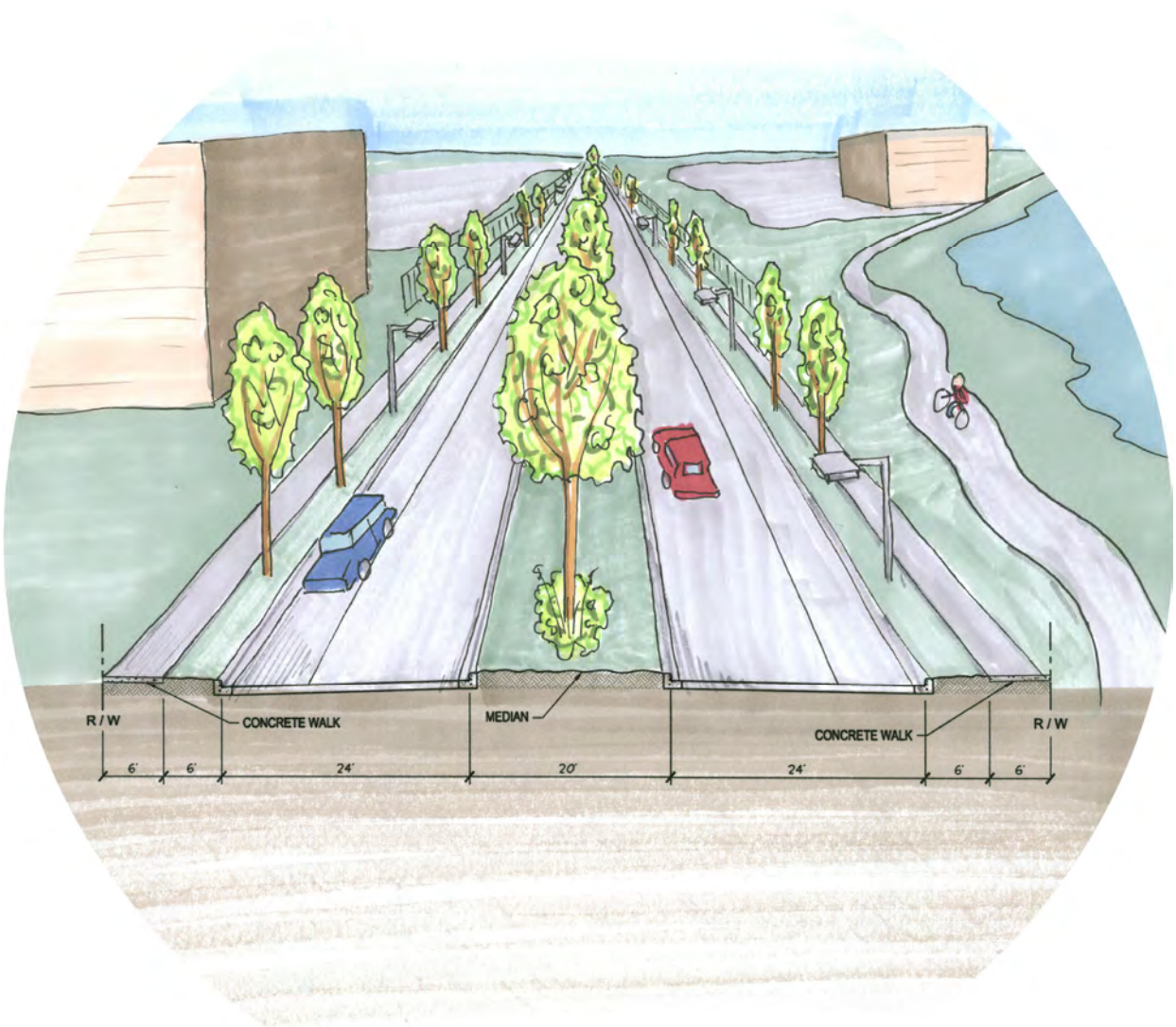
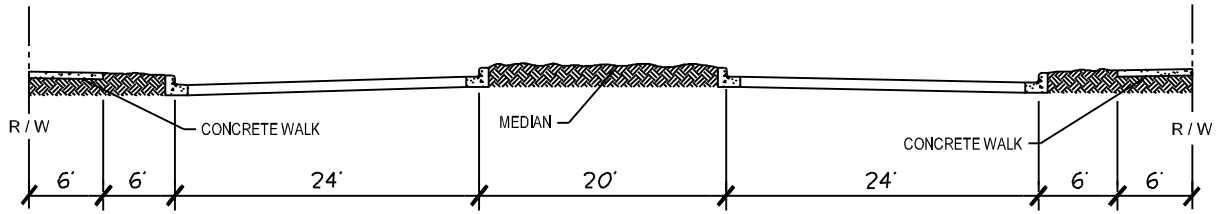
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●

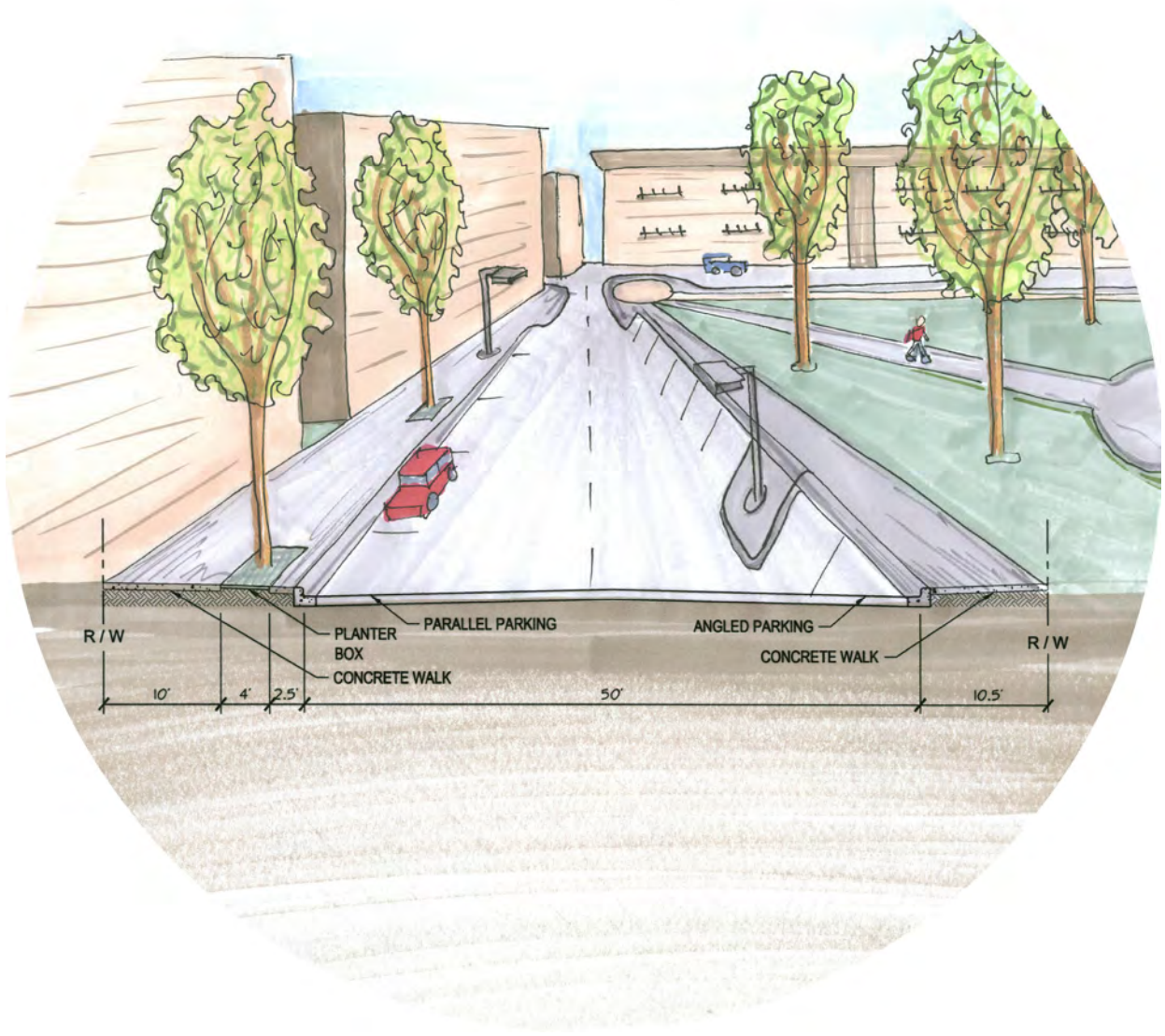
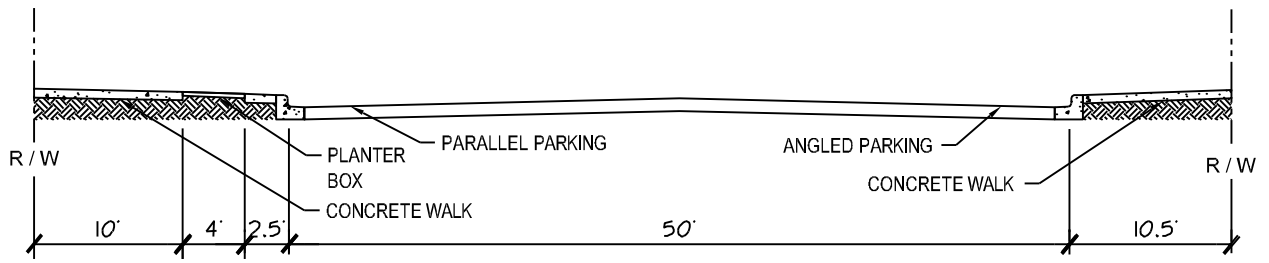
L A N D F O R M

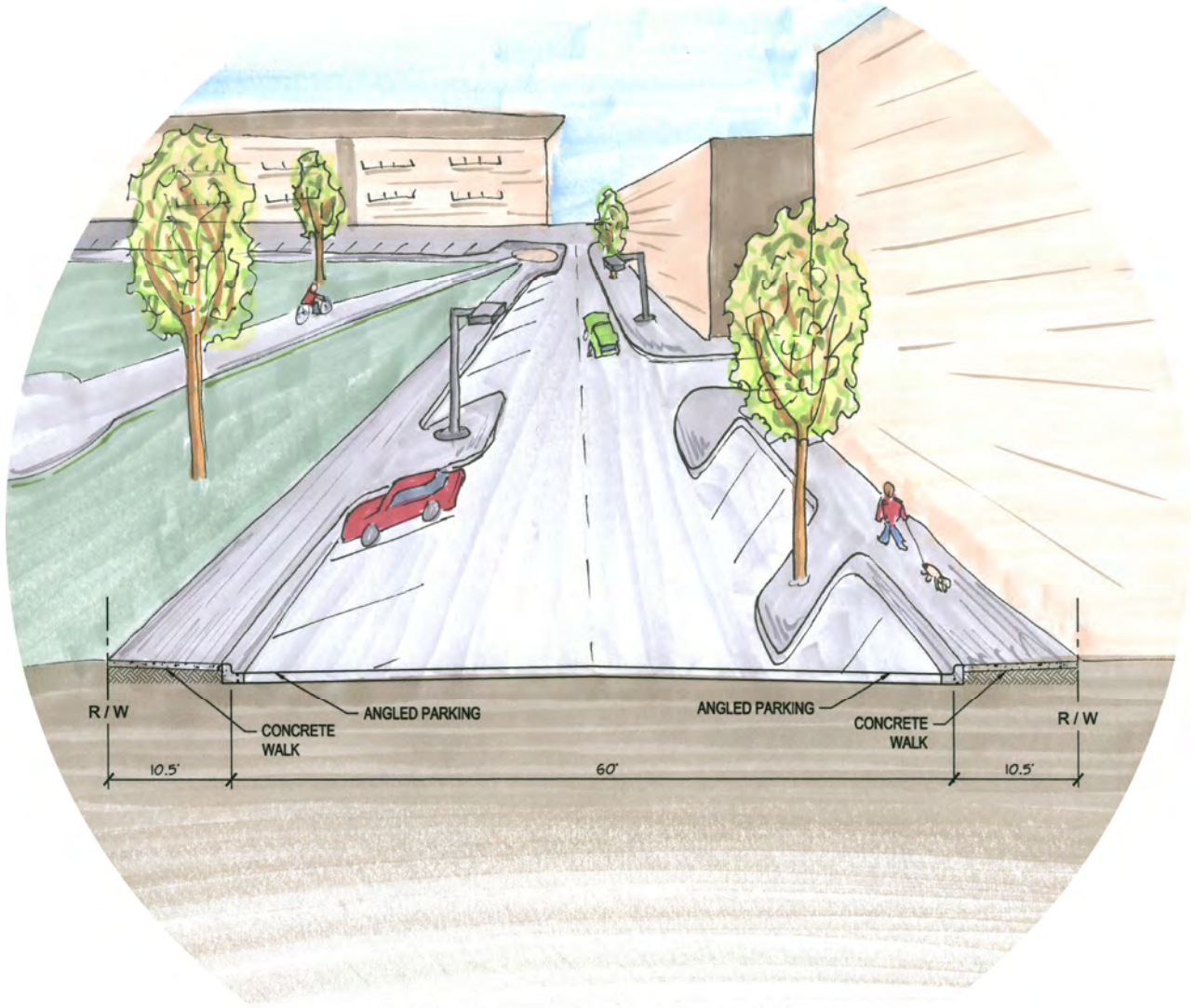
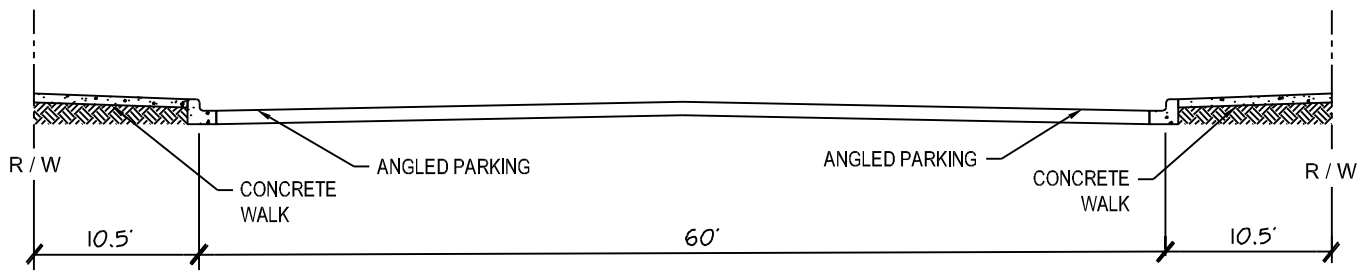
 From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
 PERSPECTIVE SECTION #3
 Ramsey Pkwy. West of Roundabout





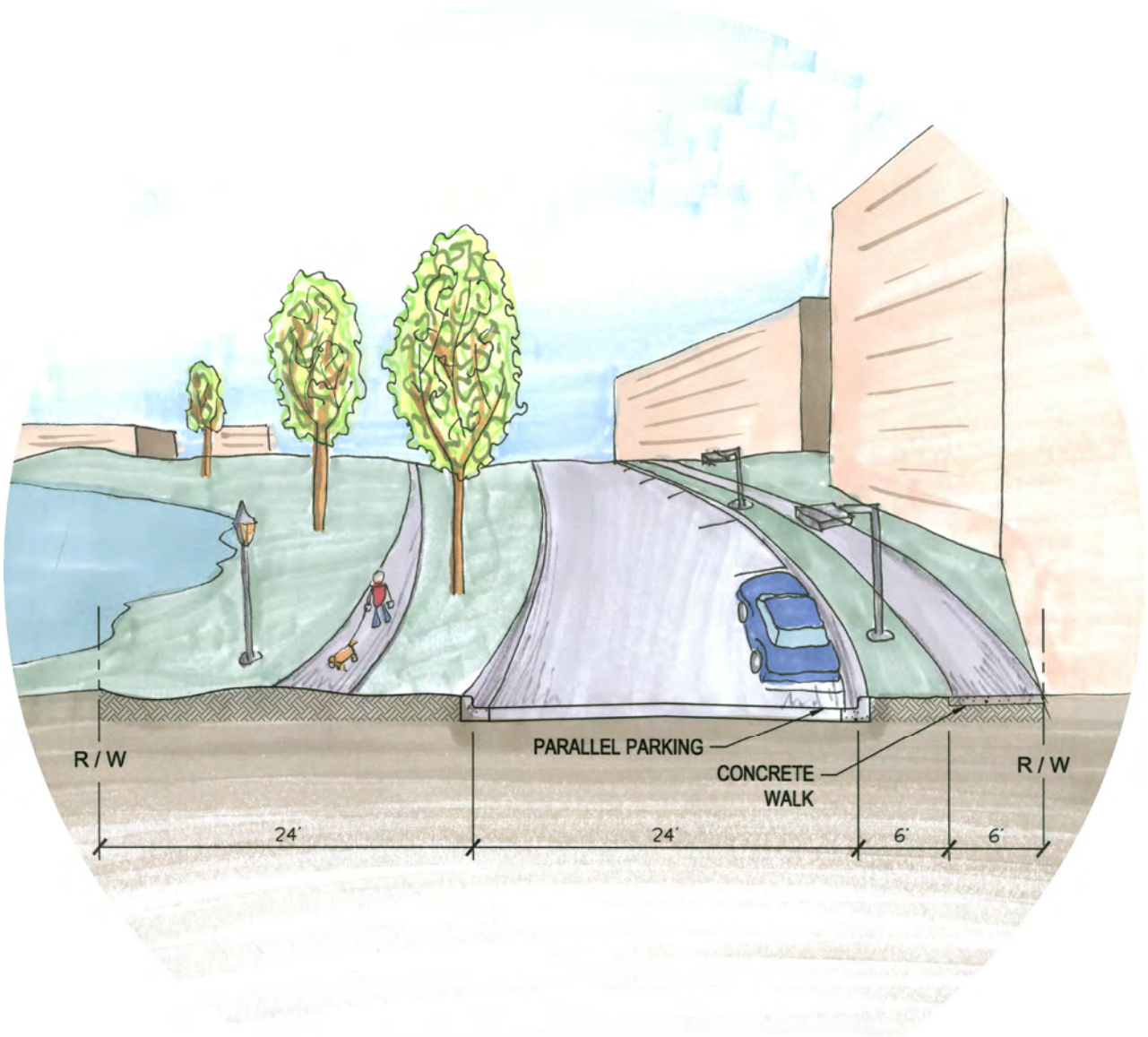
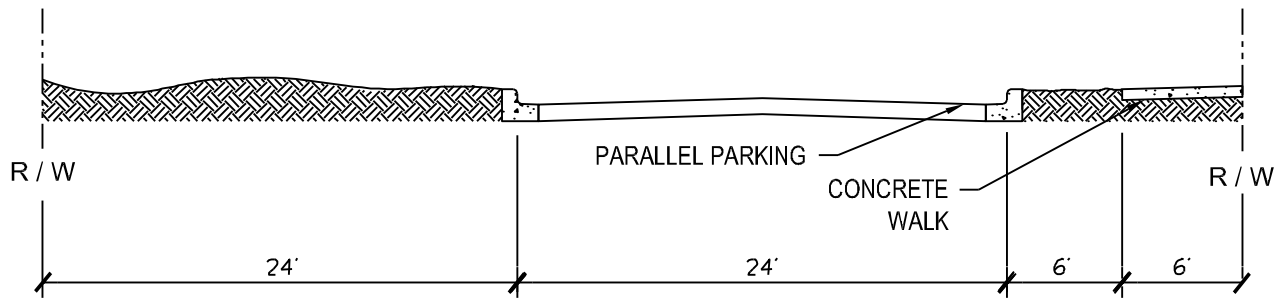




LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PERSPECTIVE SECTION #6
Center St. East of Municipal Plaza





• •
L A N D F O R M
 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
 PERSPECTIVE SECTION #7
 Ramsey Pkwy. South of The Draw



THE COR

AT RAMSEY

-  PYLON/COMMUNITY SIGNS
-  COMMUNITY KIOSK
-  PROJECT GATEWAY
-  PARKING

*See 'Appendix B' for sign details



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Signage Plan

09/06/2012





EXHIBIT B.1

<p>LANDFORM From Site to Finish</p>	<p>DESIGN FRAMEWORK The COR at Ramsey</p>	<p>THE COR RAMSEY, MINNESOTA</p>
	<p>PYLON/COMMUNITY SIGN 'A' Armstrong Blvd. & Highway 10</p>	



Eastbound



Westbound

EXHIBIT B.2



DESIGN FRAMEWORK
The COR at Ramsey

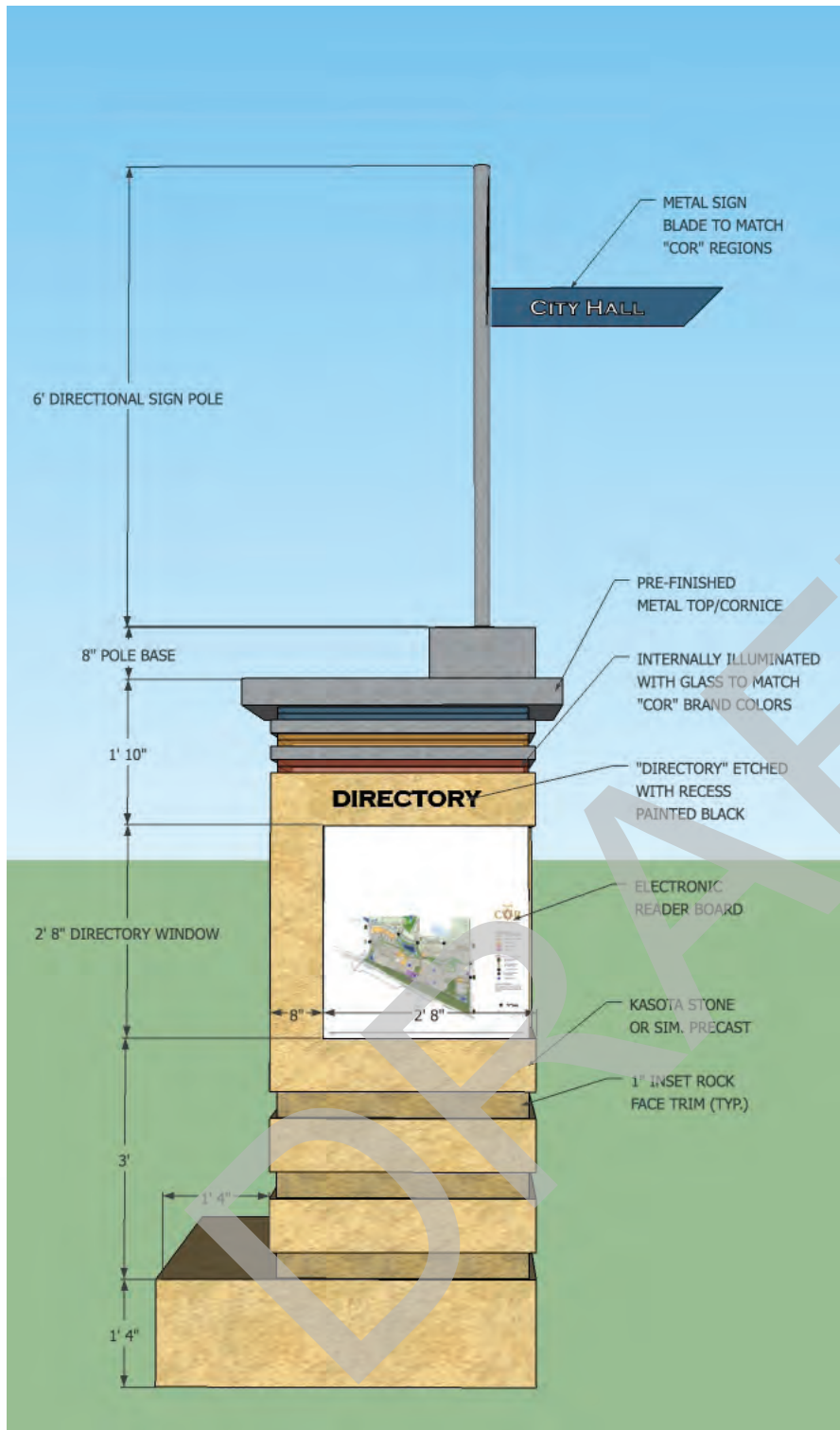
PYLON/COMMUNITY SIGN 'B'
Commuter Rail & Highway 10





EXHIBIT B.3

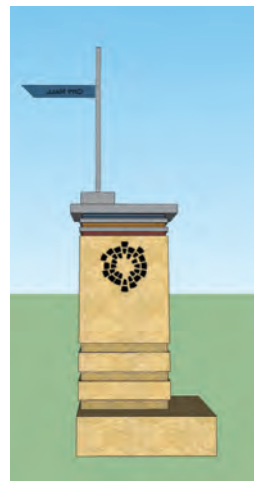
	<p>DESIGN FRAMEWORK The COR at Ramsey</p> <hr/> <p>PYLON/COMMUNITY SIGN 'C' Ramsey Blvd. & Highway 10</p>	
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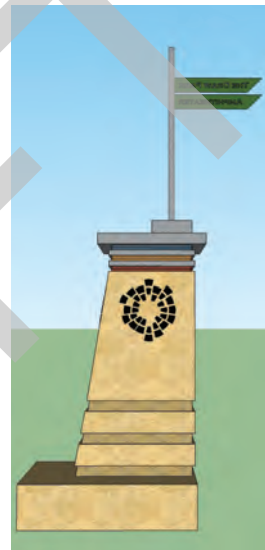
DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



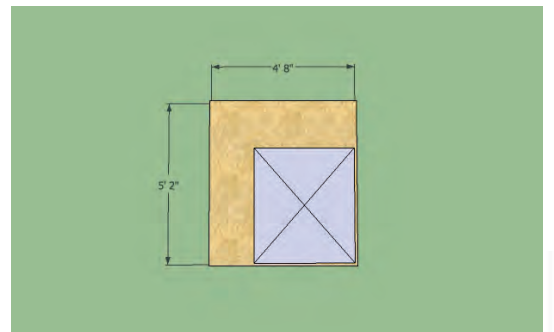
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

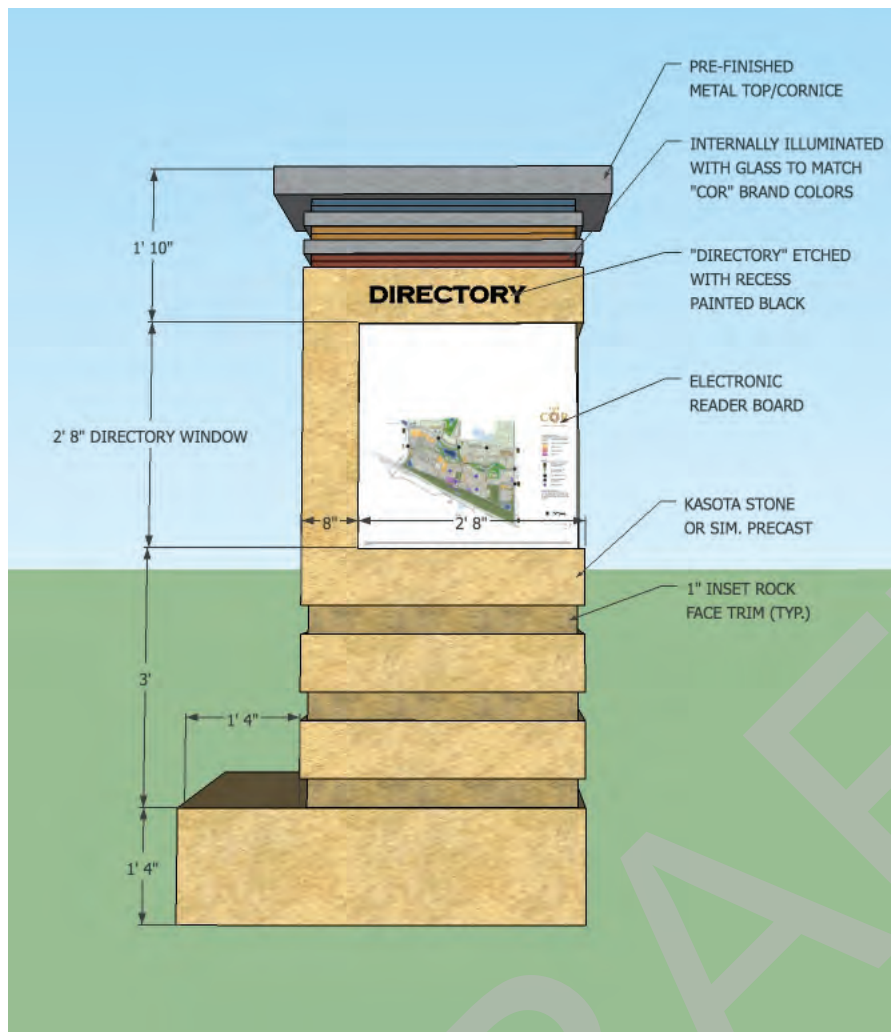
EXHIBIT B.4



DESIGN FRAMEWORK
The COR at Ramsey

COMMUNITY KIOSK 'A'
Commuter Rail & Veterans Dr.

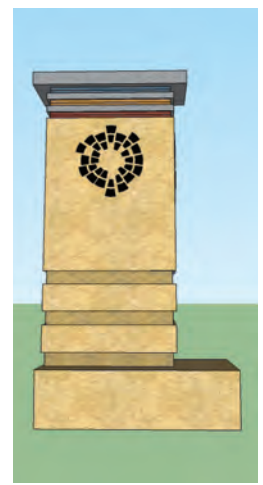




DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



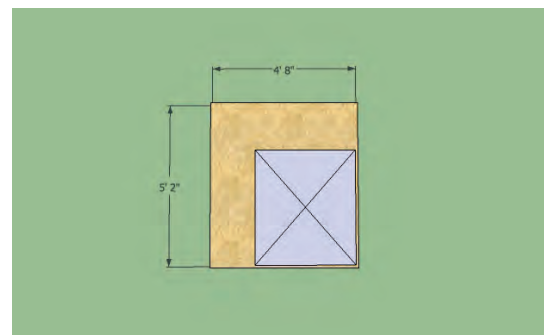
REAR VIEW



SIDE VIEW



PERSPECTIVE



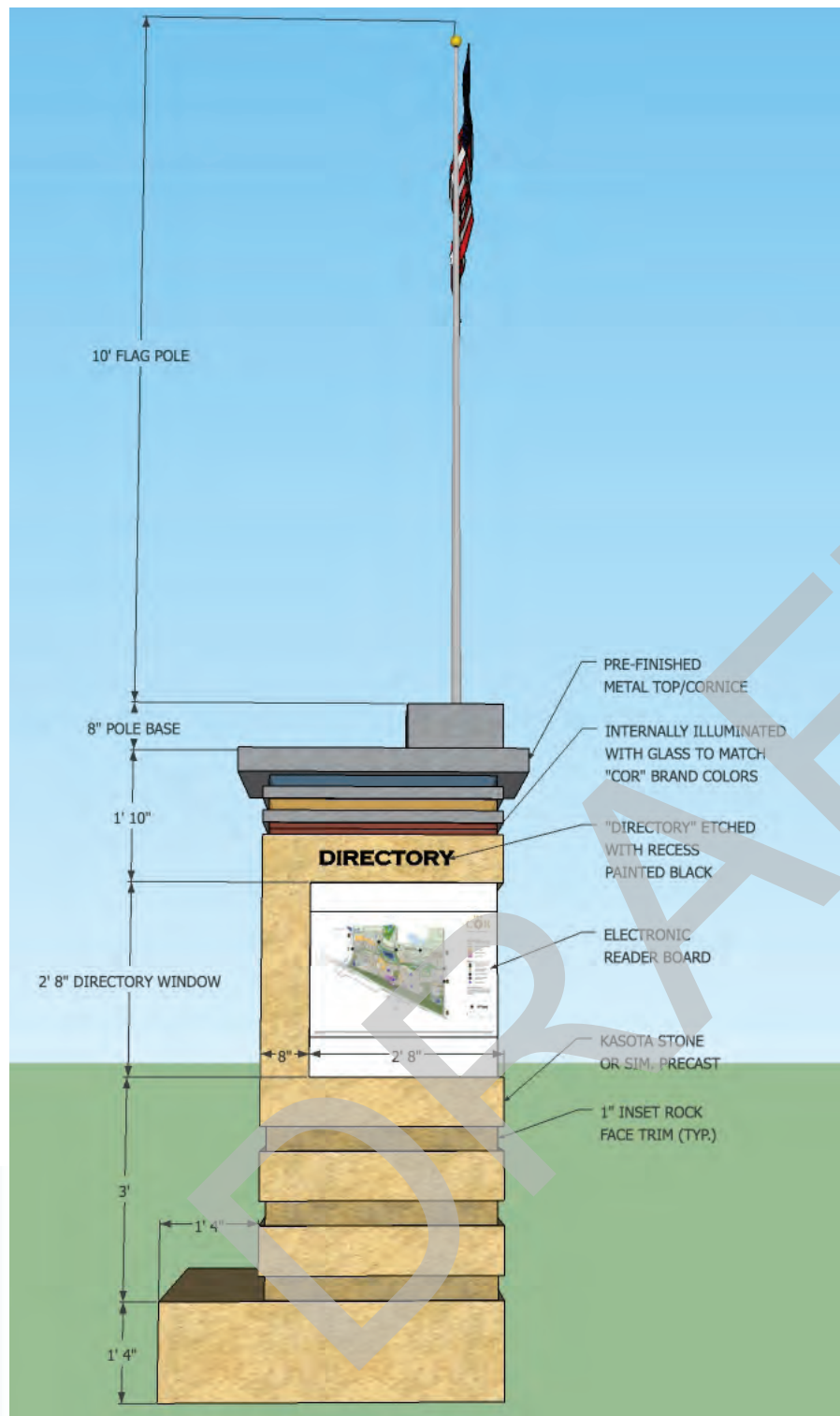
PLAN VIEW

EXHIBIT B.5

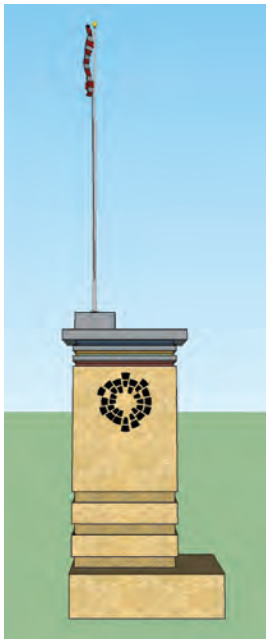


DESIGN FRAMEWORK
 The COR at Ramsey
 COMMUNITY KIOSK 'B'
 Sunwood Dr. & Center St.

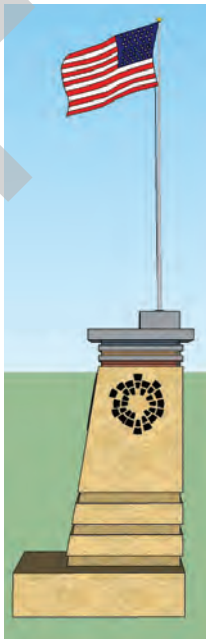




EVENTS PANEL VIEW



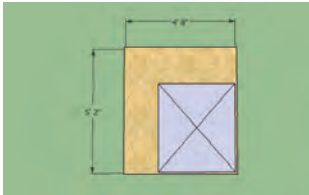
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

DIRECTORY PANEL VIEW

EXHIBIT B.6

LANDFORM

From Site to Finish

DESIGN FRAMEWORK

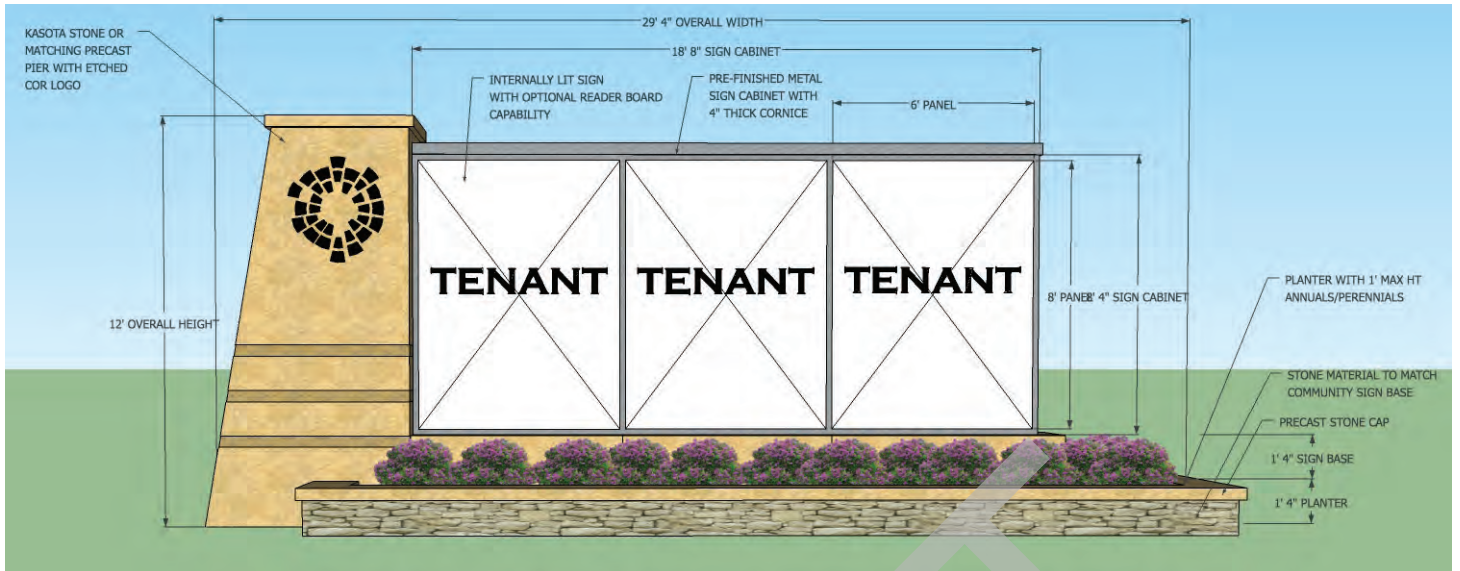
The COR at Ramsey

COMMUNITY KIOSK 'C'

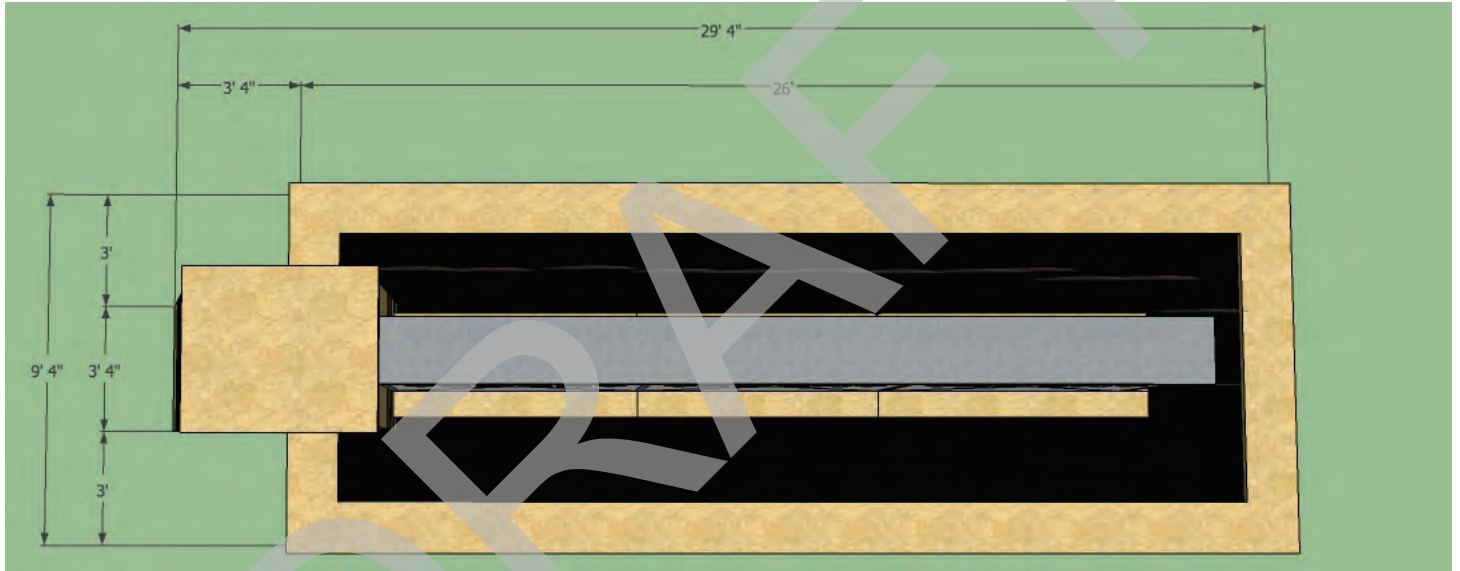
Sunwood Dr. at Municipal Plaza

THE COR

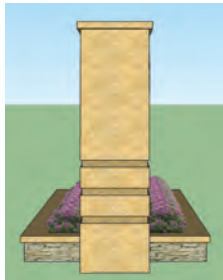
RAMSEY, MINNESOTA



ELEVATION AT SUNWOOD



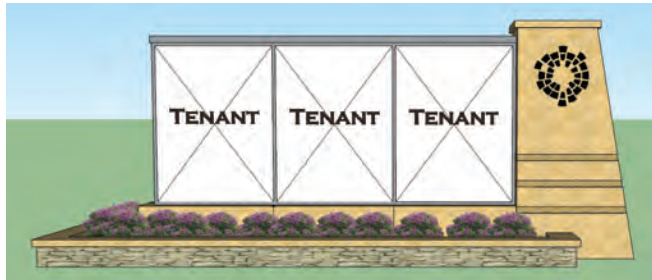
PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION

EXHIBIT B.7



DESIGN FRAMEWORK
The COR at Ramsey
 PROJECT GATEWAY SIGN 'A'
 Sunwood Dr. & Armstrong Blvd.



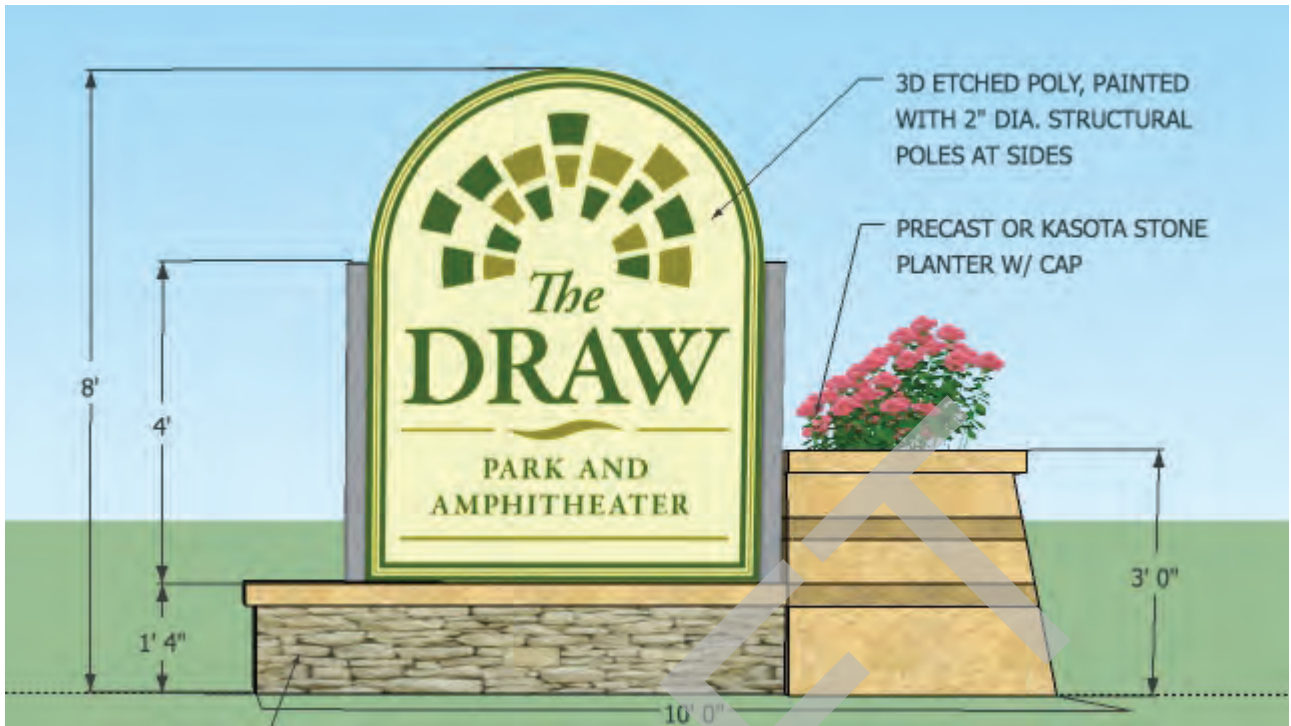
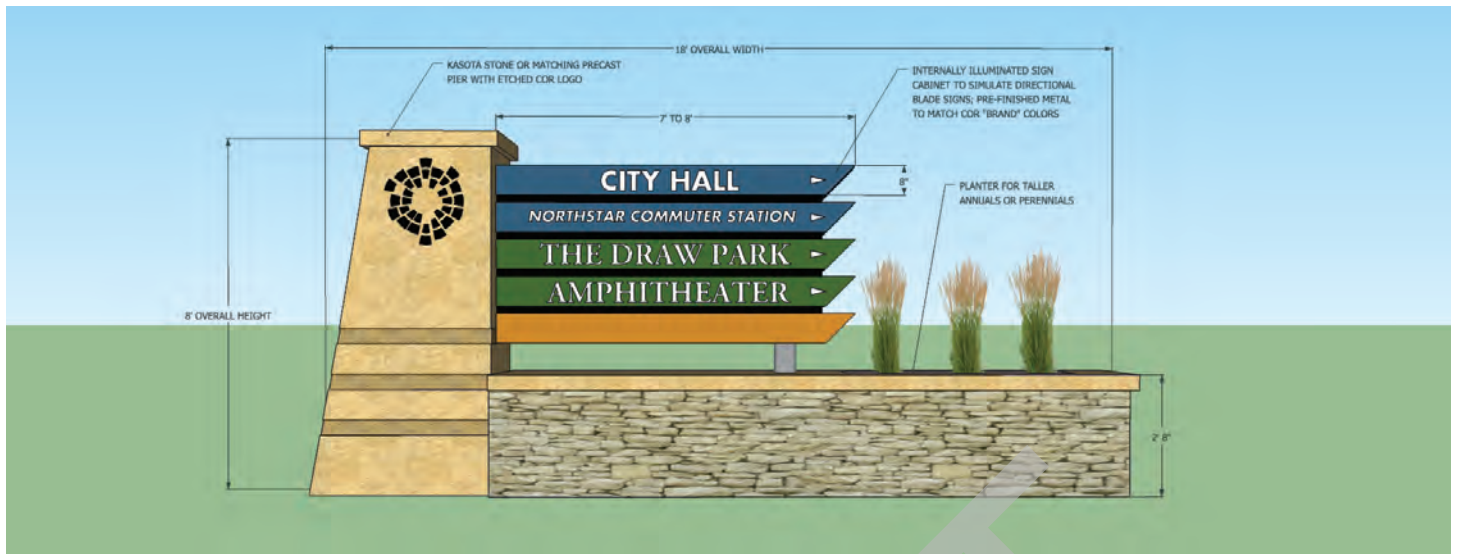
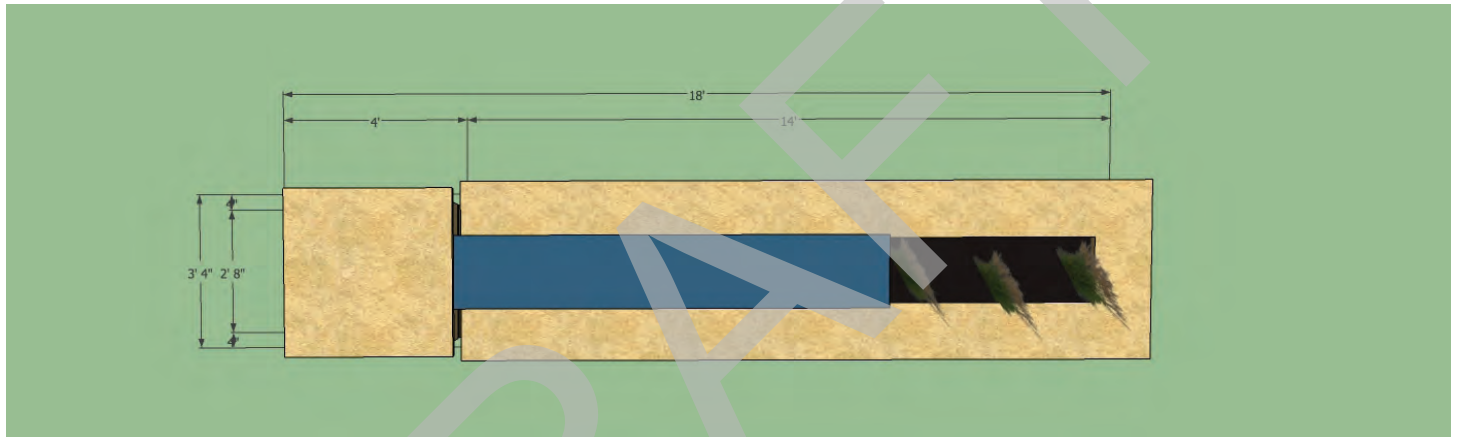


EXHIBIT B.8



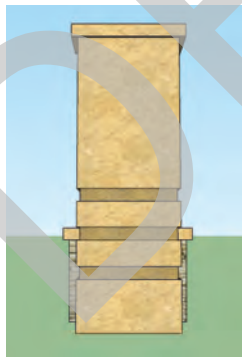
ELEVATION AT EASTBOUND BUNKER LAKE



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT WESTBOUND BUNKER LAKE

EXHIBIT B.9

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L A N D F O R M

From Site to Finish

DESIGN FRAMEWORK

The COR at Ramsey

PROJECT GATEWAY SIGN 'C'

Bunker Lake Blvd. & Center St.

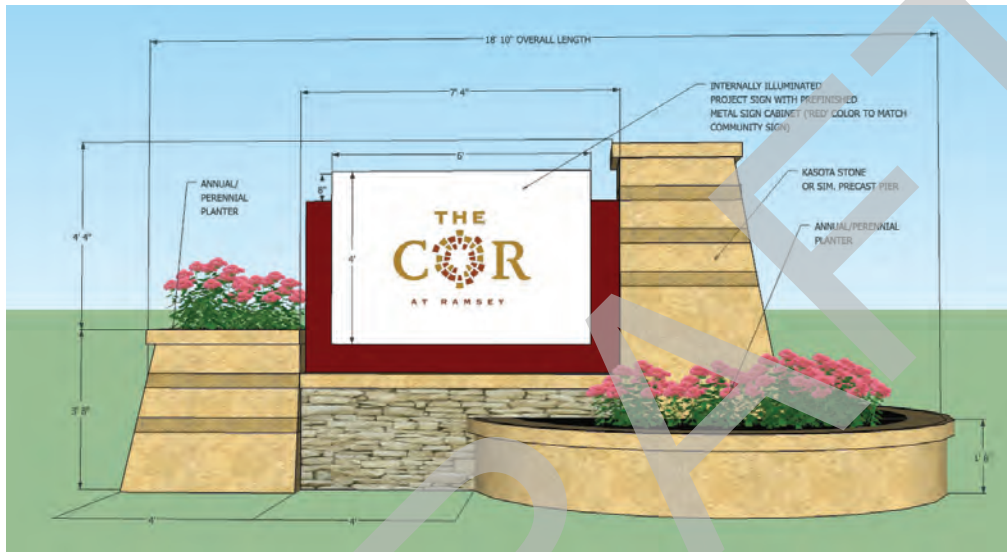
THE

COR

RAMSEY, MINNESOTA



ELEVATION AT SOUTH BOUND RAMSEY



ELEVATION AT WESTBOUND BUNKER LAKE



PERSPECTIVE

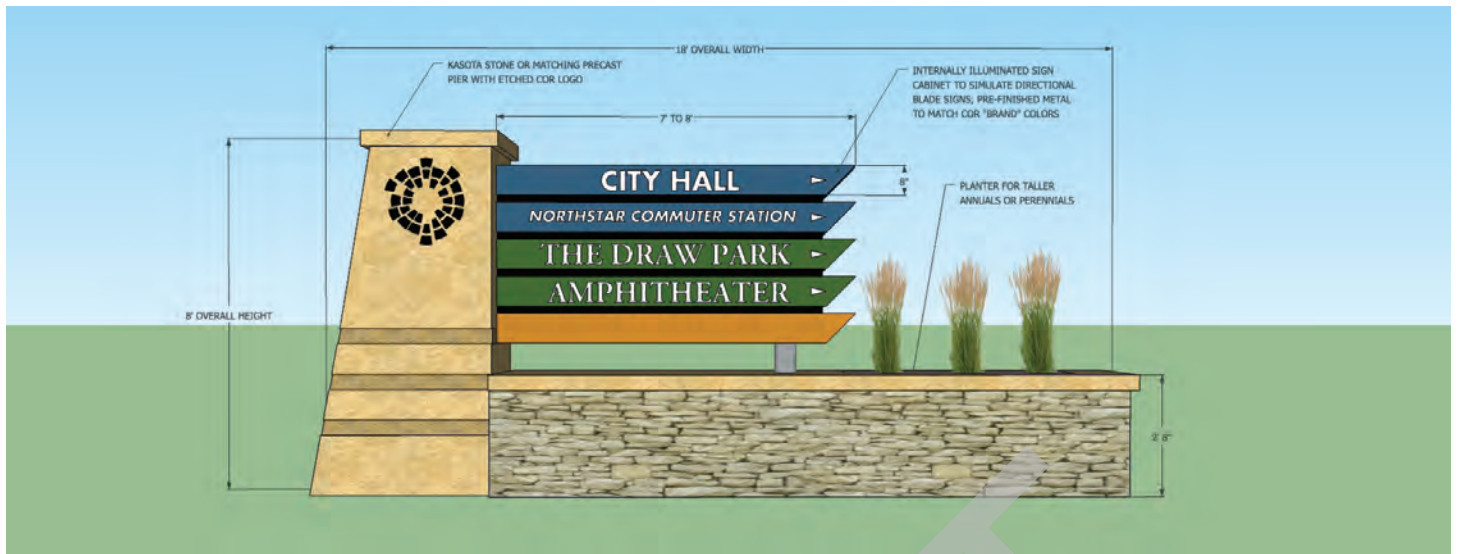
EXHIBIT B.10



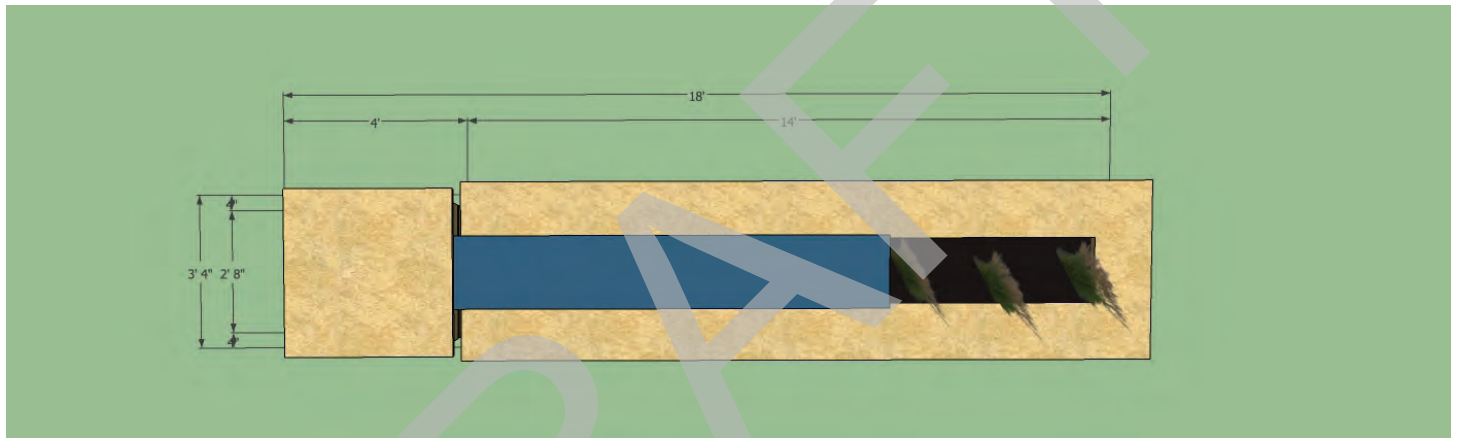
DESIGN FRAMEWORK
The COR at Ramsey

PROJECT GATEWAY SIGN 'D'
 Bunker Lake Blvd. & Ramsey Blvd.





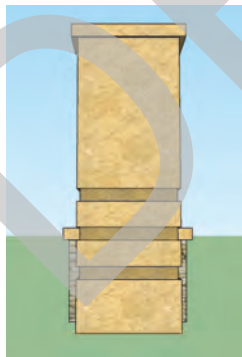
ELEVATION AT SOUTHBOUND RAMSEY



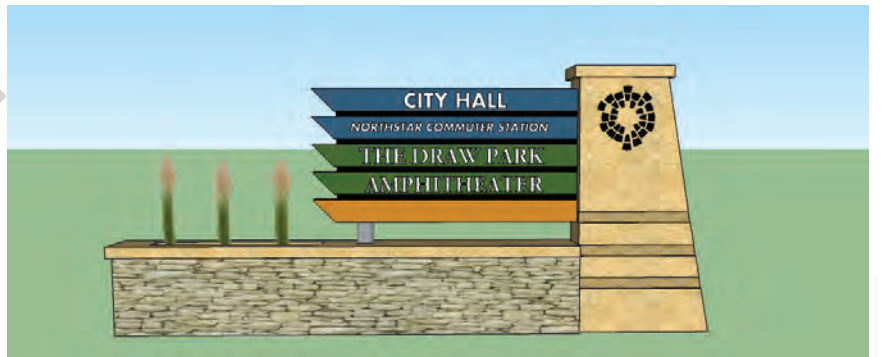
PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT NORTHBOUND RAMSEY

EXHIBIT B.11



DESIGN FRAMEWORK
The COR at Ramsey
 PROJECT GATEWAY SIGN 'E'
 Sunwood Dr. & Ramsey Blvd.





EXHIBIT B.12

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●

L A N D F O R M

 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
 PUBLIC PARKING SIGNAGE
 Standard Signage

THE
COR
 RAMSEY, MINNESOTA

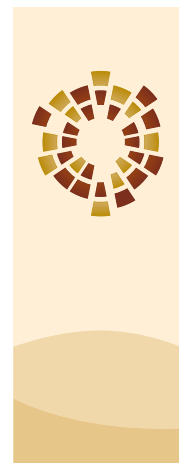
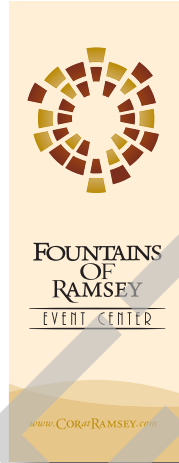
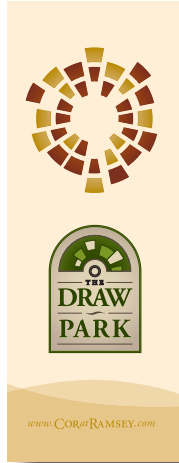


EXHIBIT B.13

LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
LIGHT POLE BANNER SIGNS
Standard Signage





PERMANENT TENANT SIGN 1.



RAMP MARQUEE SIGN 2.



INFILL BANNERS 3.



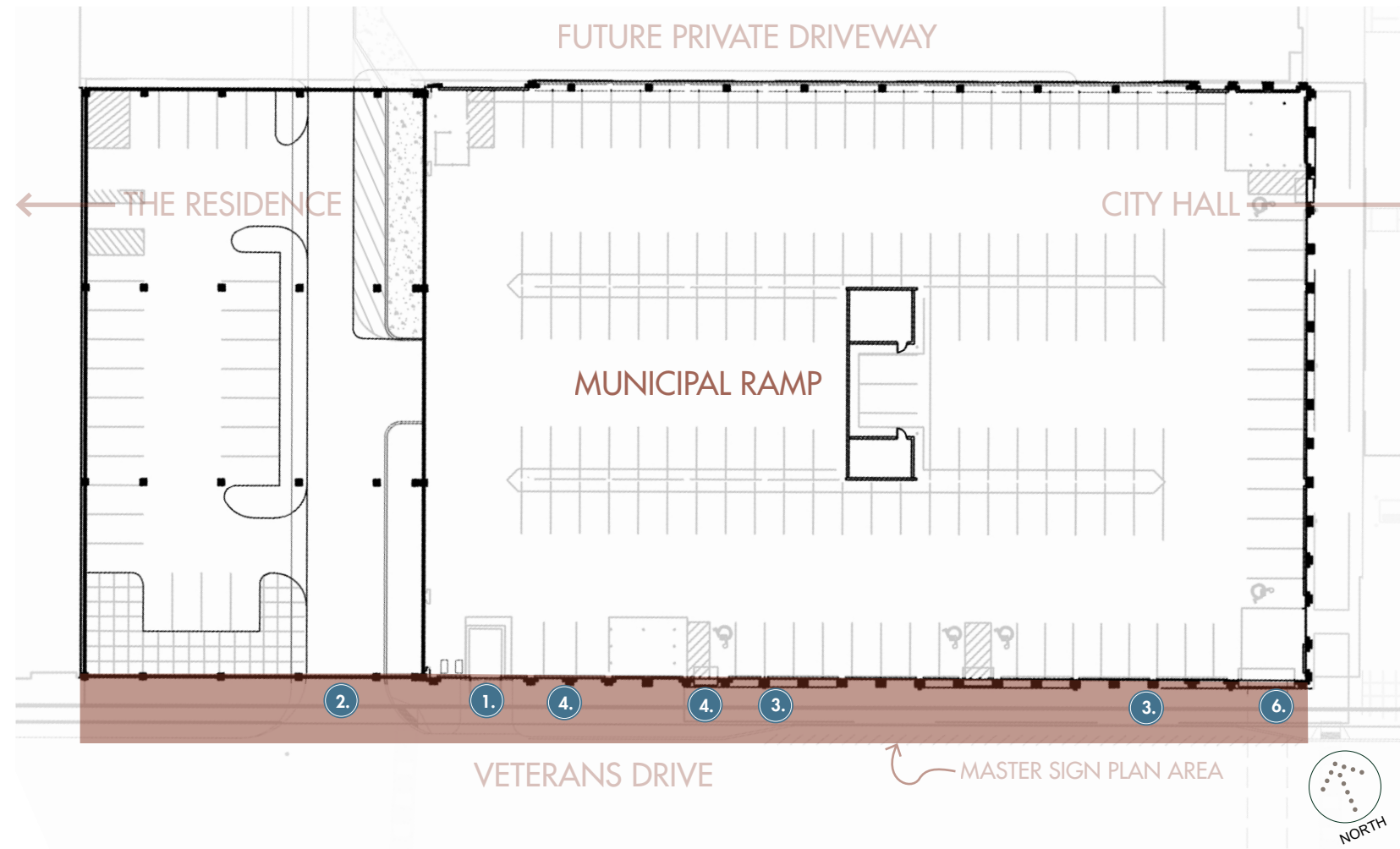
COLUMN BANNERS 4.



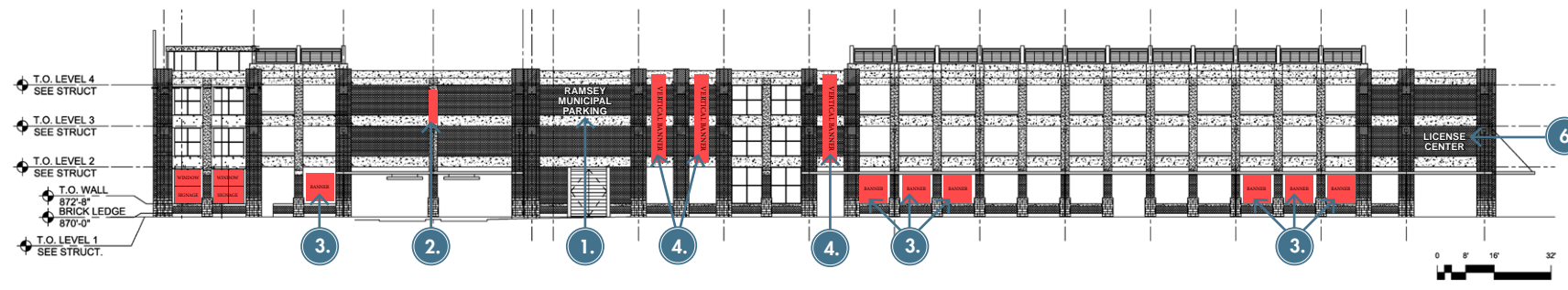
WINDOW FILM SIGN 5.



PERMANENT TENANT SIGN 6.



PLAN - N.T.S.



SOUTH ELEVATION - N.T.S.



Sign Descriptions:

1. 'RAMSEY MUNICIPAL PARKING' brushed aluminum letters, flood illuminated.
Typical Sign Area = ±66 S.F.
Qty. 1 Total South Elevation = 66 S.F.
2. Two-sided Internally illuminated marquee sign cabinet. Typical Sign Area = ±60 S.F.
Qty. 1 Total South Elevation = 60 S.F.
3. Typical banner 6'x 6' printed perforated canvas with finished seams; occurs at open bays.
Typical Sign Area = 36 S.F.
Qty. 6 Total South Elevation = 216 S.F.
4. Typical banner 4'x20' printed canvas x 3 sides.
Typical Sign Area = 240 S.F.
Qty. 3 Total South Elevation = 720 S.F.
5. Window film signs. Sizes vary.
Typical Sign Area = 64 S.F.
Qty. 2 Total South Elevation = 128 S.F.
6. "LICENSE CENTER" brushed aluminum letters
Typical Sign Area = 60 S.F.
Qty. 1 Total South Elevation = 60 S.F.

Ramp Elevation & Sign Area Calculations:






South Elevation:

Total Elevation Area = ±15,589 S.F.
 Total Allowable Sign Area (15% Elevation Area) = ±2,338 S.F.
 Total Proposed Sign Area Used = 1250 S.F.

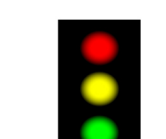
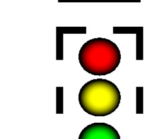





Landform® and Site to Finish® are registered service marks of Landform Professional Services, LLC.

DEVELOPMENT STATUS

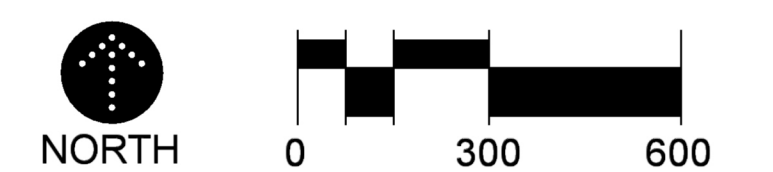
-  EXISTING DEVELOPMENT
-  PROPOSED DEVELOPMENT
-  ACTIVE DEALS
-  UNDER CONTRACT
-  COMPLETED

ACCESS

-  EXISTING SIGNALIZED INTERSECTION
-  FUTURE SIGNALIZED INTERSECTION
-  FULL INTERSECTION
-  NO LEFT OUTBOUND MOVEMENTS
-  PARKING RAMP

TRAFFIC INFORMATION

ADT INFORMATION TAKEN FROM 2009 ACTUAL COUNTS AND 2030 PROJECTED VOLUMES



DEVELOPMENT PLAN 6.1

09.06.2012



THE COR

AT RAMSEY

-  PYLON/COMMUNITY SIGNS
-  COMMUNITY KIOSK
-  PROJECT GATEWAY
-  PARKING

*See 'Appendix B' for sign details



Landform™ and Site to Finish™ are service marks of Landform Professional Services, LLC.

Signage Plan

09/06/2012





EXHIBIT B.1



Eastbound

Westbound

EXHIBIT B.2

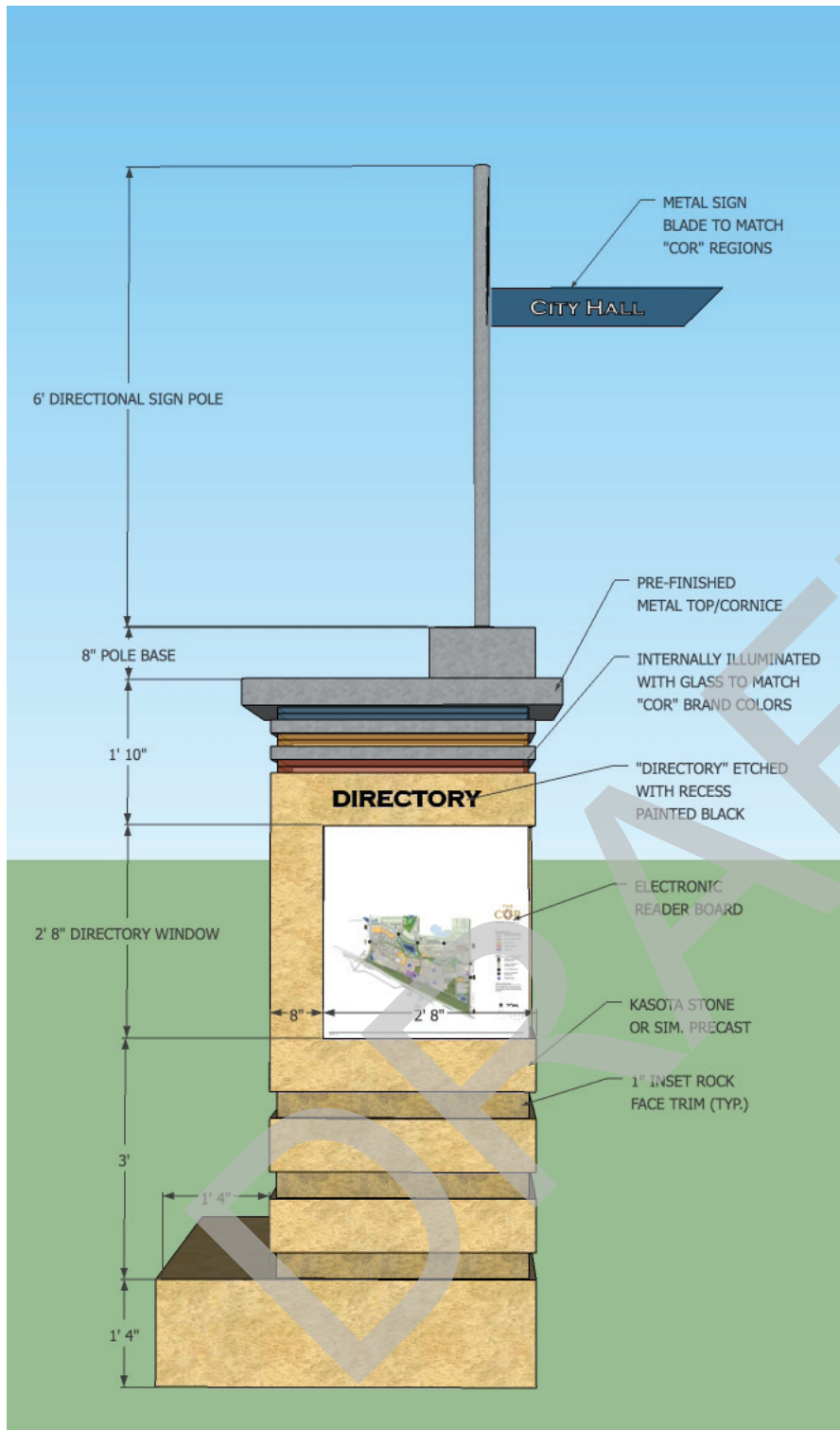
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PYLON/COMMUNITY SIGN 'B'
Commuter Rail & Highway 10





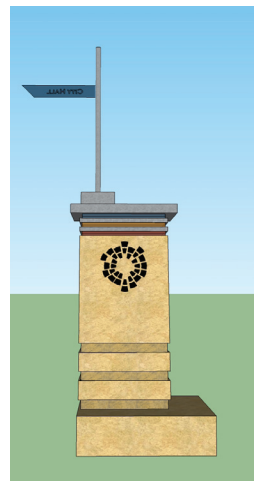
EXHIBIT B.3



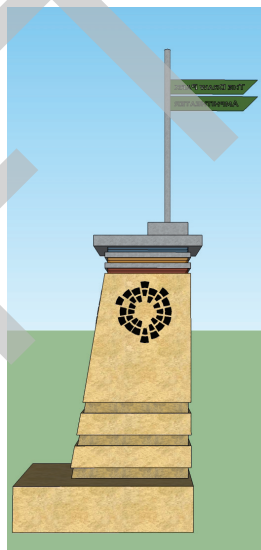
DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



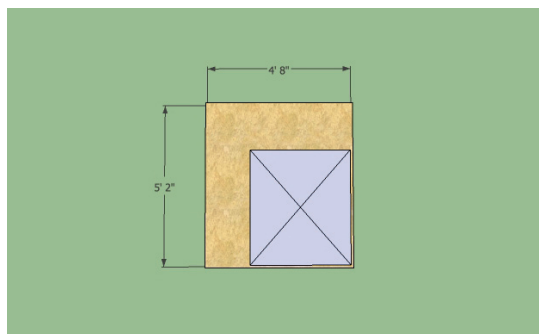
REAR VIEW



SIDE VIEW



PERSPECTIVE



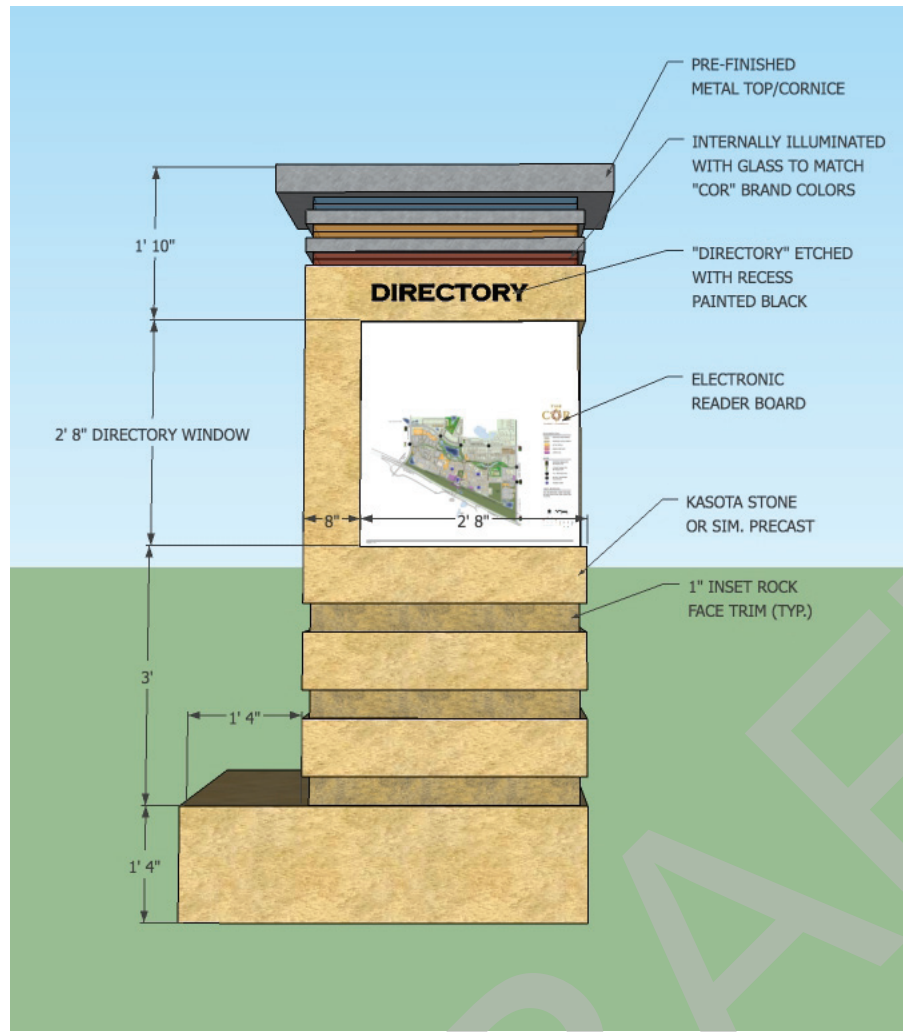
PLAN VIEW

EXHIBIT B.4

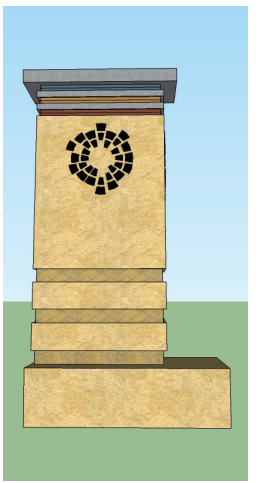
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
COMMUNITY KIOSK 'A'
Commuter Rail & Veterans Dr.

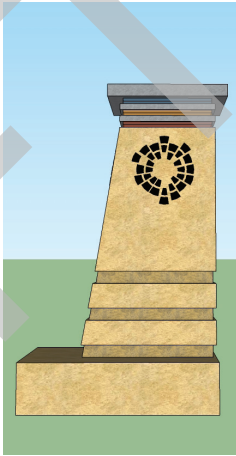




EVENTS PANEL VIEW



REAR VIEW

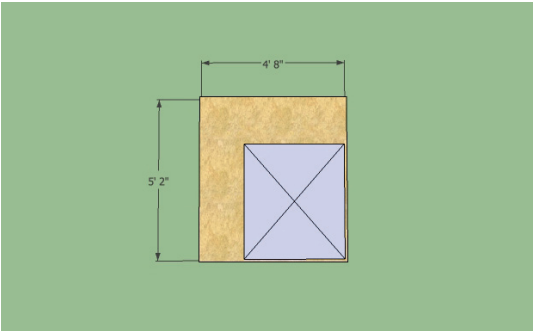


SIDE VIEW



PERSPECTIVE

DIRECTORY PANEL VIEW



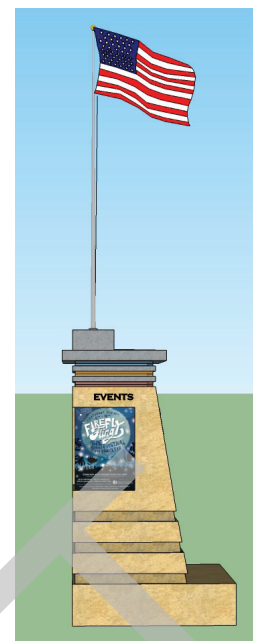
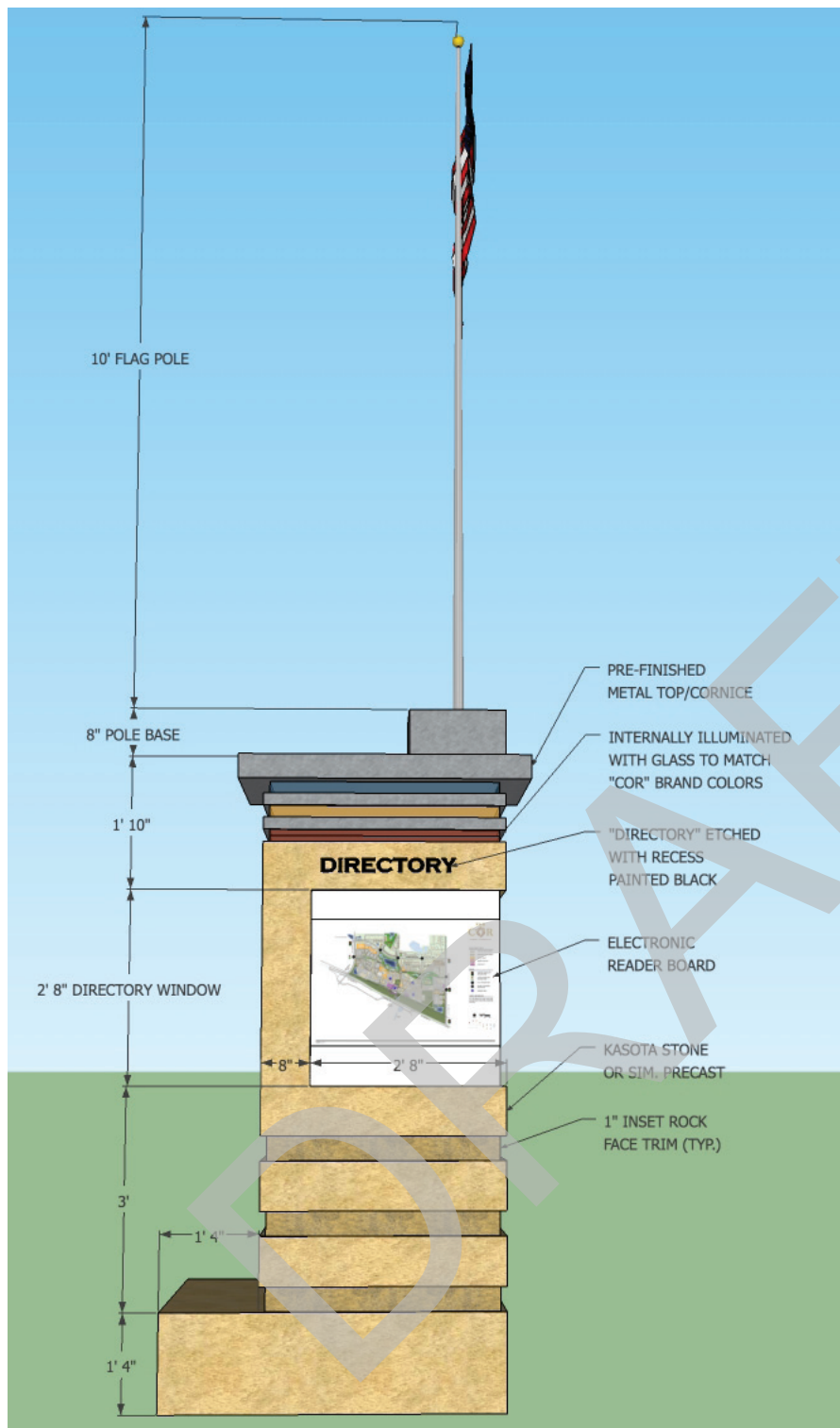
PLAN VIEW

EXHIBIT B.5

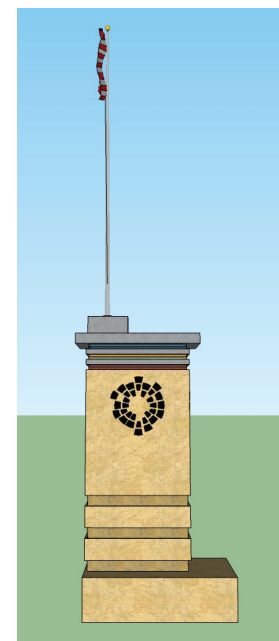
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
COMMUNITY KIOSK 'B'
Sunwood Dr. & Center St.

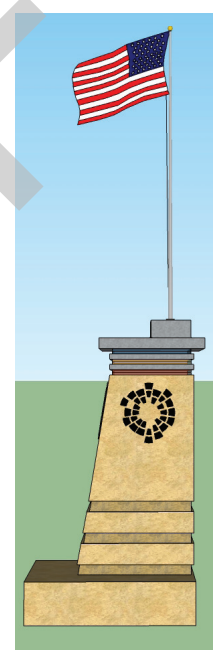




EVENTS PANEL VIEW



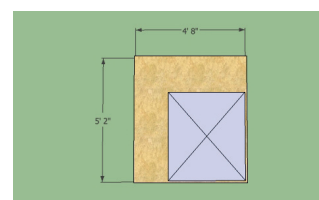
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

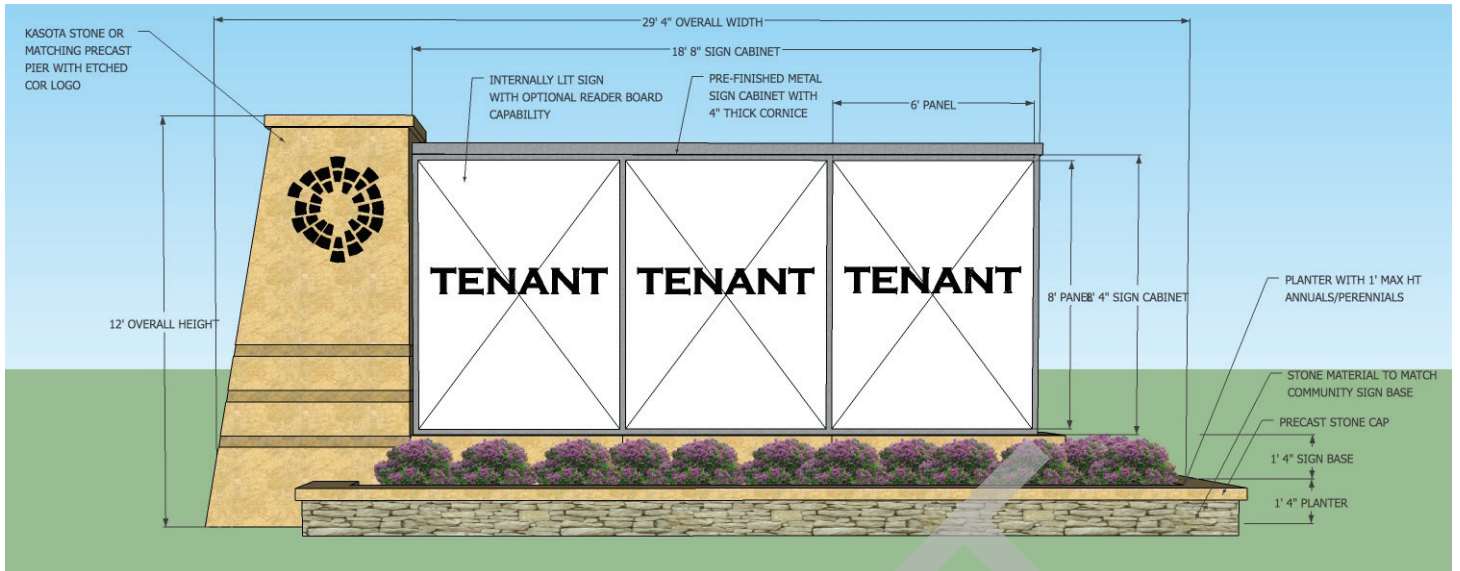
DIRECTORY PANEL VIEW

EXHIBIT B.6

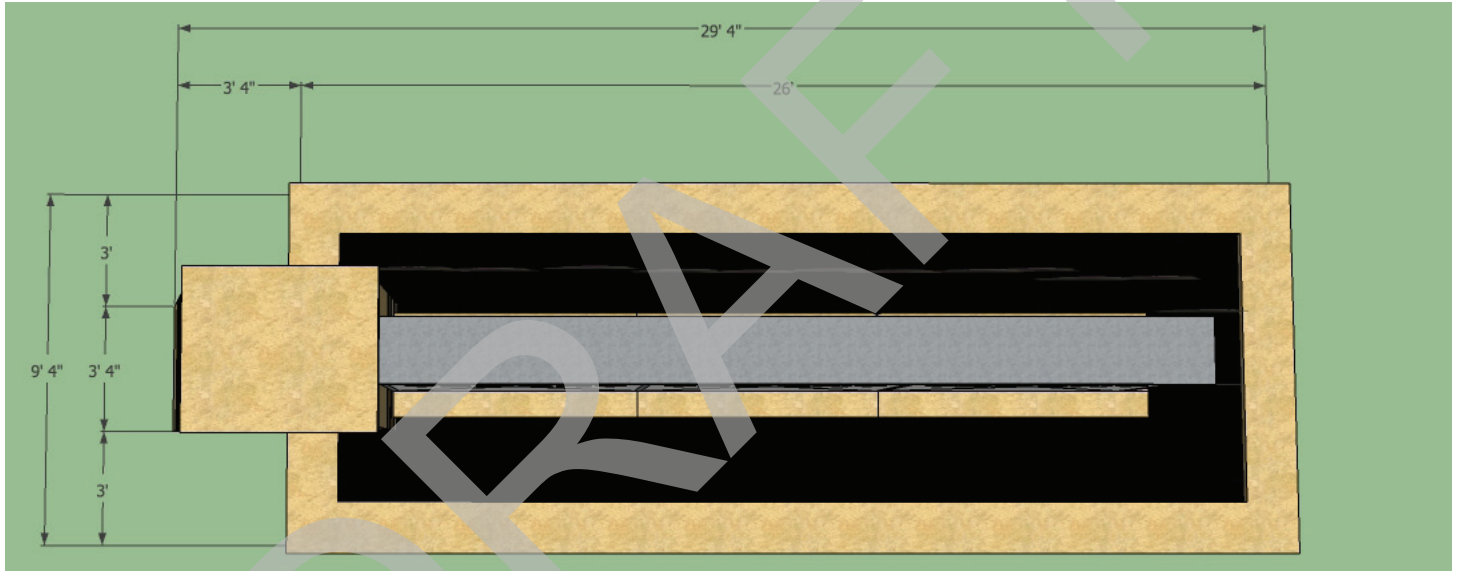


DESIGN FRAMEWORK
 The COR at Ramsey
 COMMUNITY KIOSK 'C'
 Sunwood Dr. at Municipal Plaza

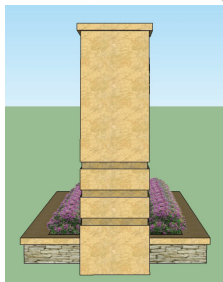




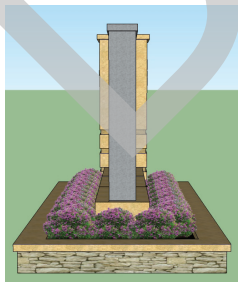
ELEVATION AT SUNWOOD



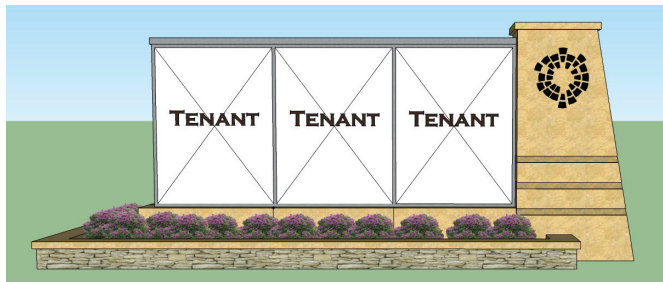
PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION

EXHIBIT B.7

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L A N D F O R M
 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
 PROJECT GATEWAY SIGN 'A'
 Sunwood Dr. & Armstrong Blvd.



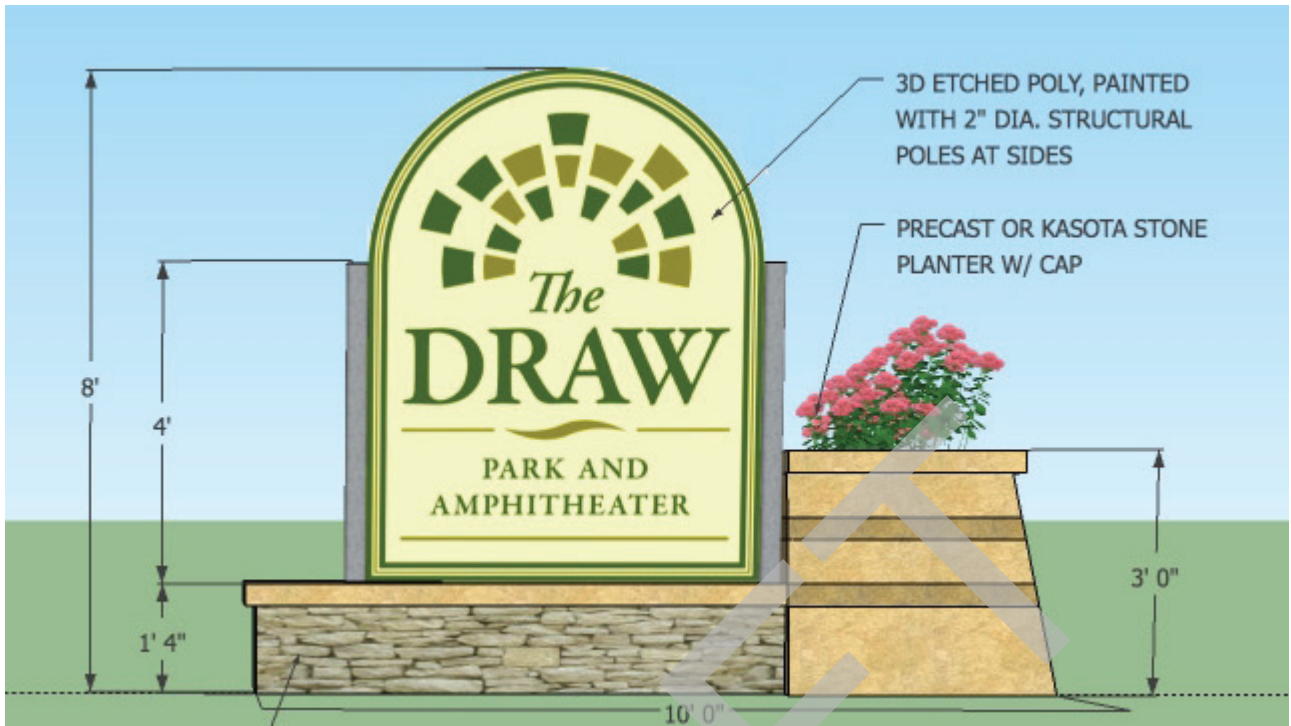


EXHIBIT B.8

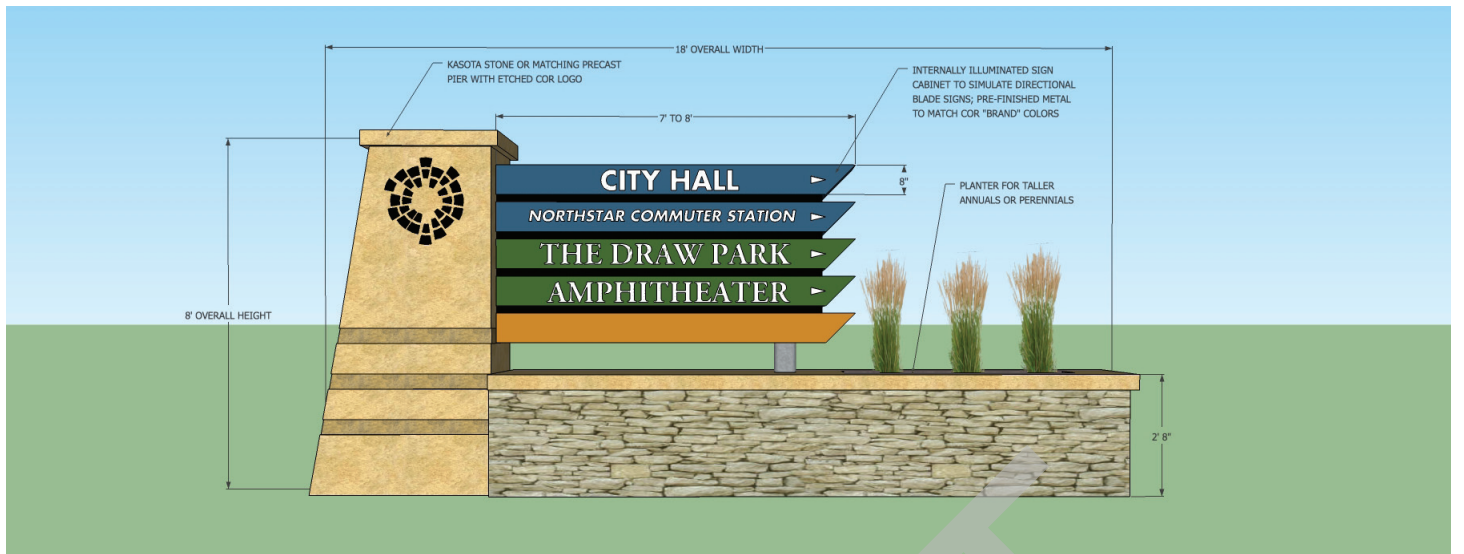
●
●

L A N D F O R M

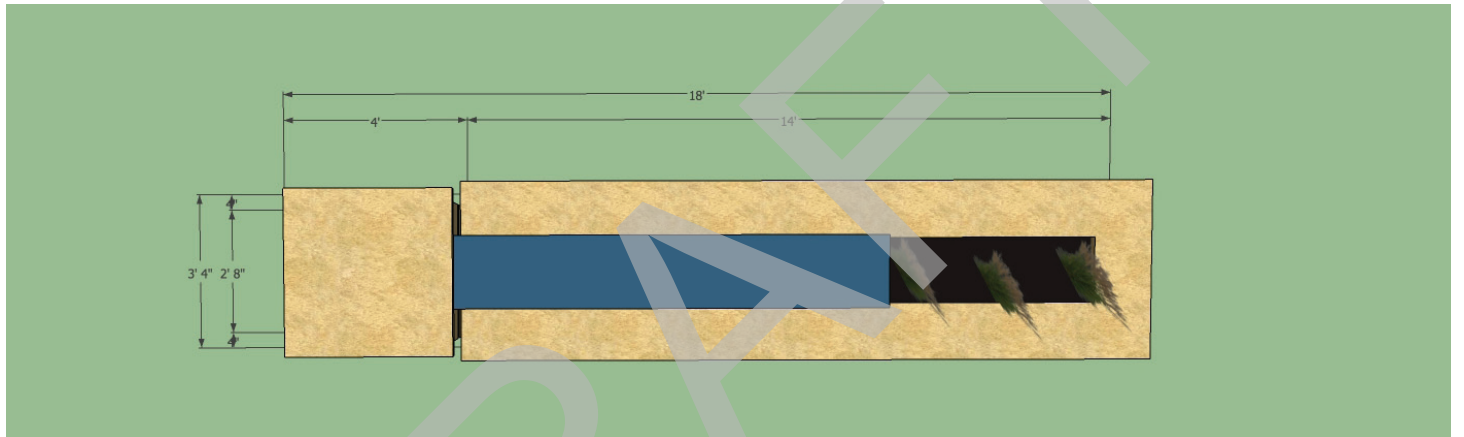
 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
PUBLIC PARK SIGNS





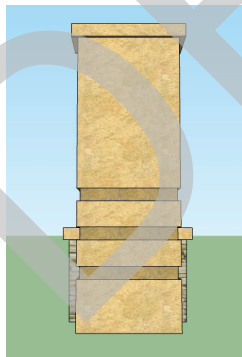
ELEVATION AT EASTBOUND BUNKER LAKE



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT WESTBOUND BUNKER LAKE

EXHIBIT B.9

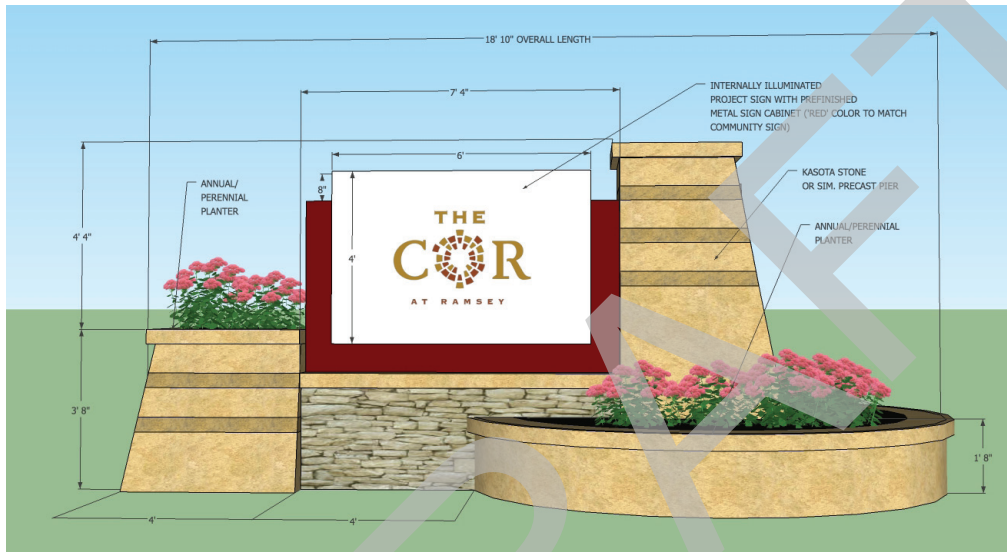
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PROJECT GATEWAY SIGN 'C'
Bunker Lake Blvd. & Center St.





ELEVATION AT SOUTH BOUND RAMSEY



ELEVATION AT WESTBOUND BUNKER LAKE



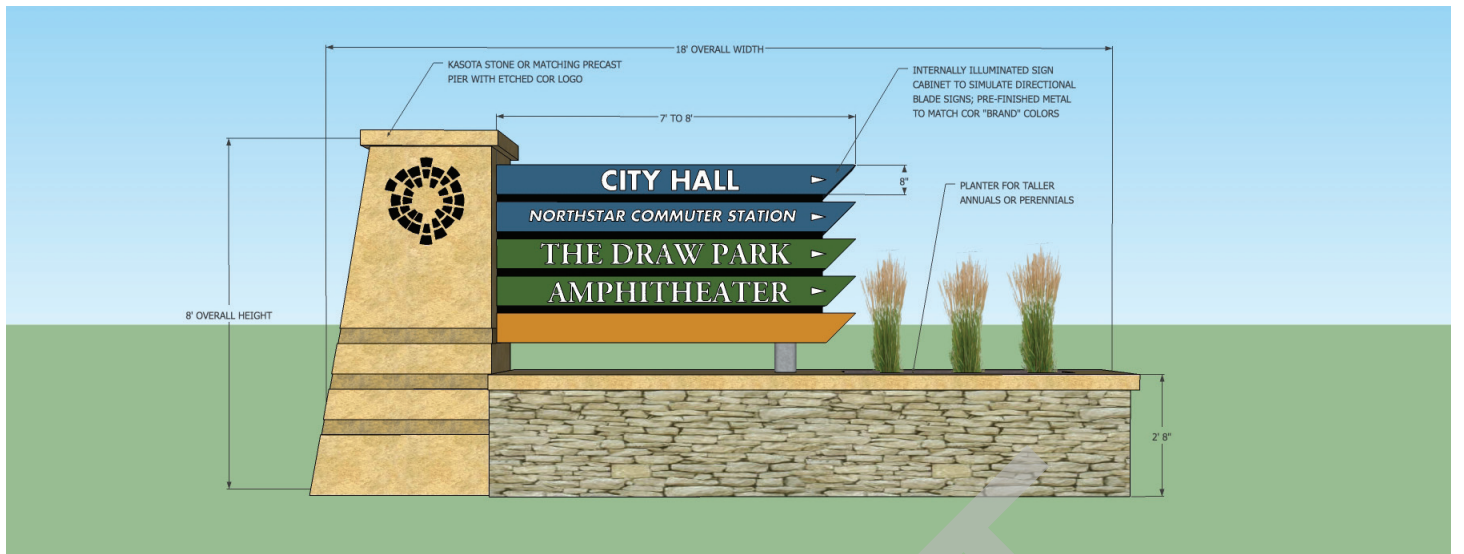
PERSPECTIVE

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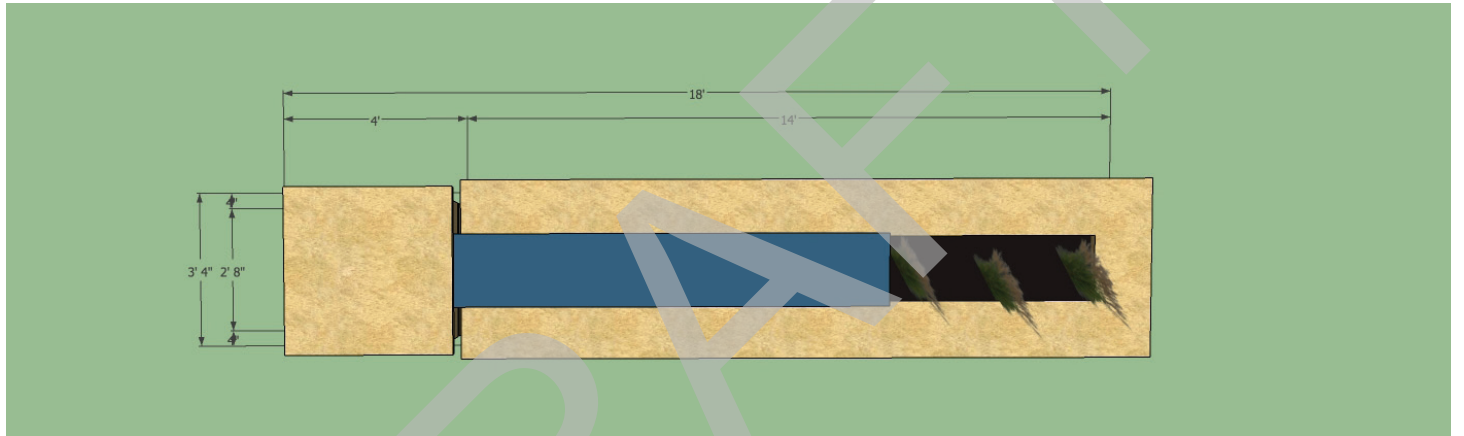
● ●
L A N D F O R M
 From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
 PROJECT GATEWAY SIGN 'D'
 Bunker Lake Blvd. & Ramsey Blvd.





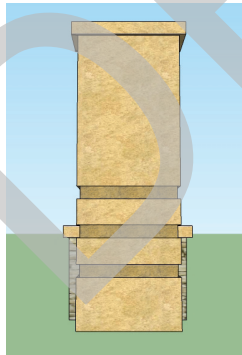
ELEVATION AT SOUTHBOUND RAMSEY



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT NORTHBOUND RAMSEY

EXHIBIT B.11

LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PROJECT GATEWAY SIGN 'E'
Sunwood Dr. & Ramsey Blvd.





EXHIBIT B.12

●
●

L A N D F O R M

 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
PUBLIC PARKING SIGNAGE
Standard Signage





EXHIBIT B.13



L A N D F O R M

 From Site to Finish

DESIGN FRAMEWORK

The COR at Ramsey

LIGHT POLE BANNER SIGNS

Standard Signage

THE

COR

 RAMSEY, MINNESOTA

ORDINANCE #12-__

**CITY OF RAMSEY
ANOKA COUNTY STATE OF MINNESOTA**

AN AMENDMENT TO CHAPTER 117 WHICH IS KNOWN AS THE ZONING AND SUBDIVISION OF LAND CHAPTER OF THE CITY CODE OF RAMSEY, MINNESOTA.

AN ORDINANCE AMENDING SECTION 117-118 “THE COR DISTRICTS” OF CHAPTER 117 OF THE CITY CODE OF RAMSEY, MINNESOTA TO AMEND SUB-DISTRICT BOUNDARIES, PARKING REGULATIONS, PARKS SECTION, AND MASTER SIGN PLAN.

The City of Ramsey ordains:

SECTION 1. AMENDMENT

Section 117-118 of Ramsey City Code shall be amended as follows:

- (2) Development standards.
- (d) *General development standards.* All development within the COR District shall meet the COR Development Framework that is incorporated into this chapter by reference, as amended by the City Council on October 23rd, 2012. These standards will be used by the city as the minimum requirements for evaluating development proposals and site plans. However, the standards are not intended to restrict creativity in design. An applicant may request modification or waiver of any standard in favor of an alternate approach that will achieve the same design objective.

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SECTION 2. SUMMARY

The following official summary of Ordinance #12-__ has been approved by the City Council of the City of Ramsey as clearly informing the public of the intent and effect of the Ordinance.

It is the intent of this Ordinance to amend the standards in Section 111-118 of Chapter 117 of the Ramsey City Code to amend minimum and maximum parking regulations for restaurant users, amend the Master Sign Plan to include standards for Community Signs, amend sign regulations for users with shared access, and amend the Parks section to note the City’s preferred location of the Central Anoka County Regional Trail. The Design Framework is incorporated by reference in Chapter 117, and has the effect of being the Zoning Code for The COR District.

SECTION 3. EFFECTIVE DATE

The effective date of this ordinance is thirty (30) days after its passage and publication, subject to City Charter Section 3.9.

Adopted by the Ramsey City Council the ___ day of _____, 2012.

Mayor

ATTEST:

City Administrator

Introduction Date: _____

Posting Dates: _____

Adoption Date: _____

Publication Date: _____

Effective Date: _____

DRAFT

6.04: Public Hearing: Consider Ordinance to Amend City Code Section 117-118 (The COR) Approving Amendment to the COR Design Framework Related to Off-Street Parking, Signs, Parks and Trails, and Sub-District Boundaries

Public Hearing

Chairperson Levine called the public hearing to order at 8:36 p.m.

Presentation

Senior Planner presented the staff report. He requested the Commission consider an Ordinance to amend City Code Section 117-118 (The COR) approving an amendment to the COR Design Framework related to off-street parking, signs, parks and trails, and sub-district boundaries. Staff reviewed the proposed revisions in detail with the Commission and recommended approval of the proposed Ordinance, which includes the revised Design Framework and Master Sign Plan.

Citizen Input

Commissioner Bauer asked if shared parking was addressed in this framework.

Development Services Manager Gladhill commented this was a fundamental guideline throughout the entire COR.

Motion by Commissioner Bauer, seconded by Commissioner Field, to close the public hearing.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, Field, Brauer, and Van Scoy. Voting No: None. Absent: Commissioner Perez and Schiller.

Chairperson Levine closed the public hearing closed at 8:50 p.m.

Commission Business

Commissioner Bauer questioned if the proposed tenant signs would be able to be shared between more than three users.

Development Services Gladhill commented the retail gateway signs were proposed to have three tenants; however, the three signs could be split in half and allow for signage for up to six businesses.

Commissioner Brauer asked if the signs should be overbuilt to assure there was enough signage for future businesses.

Development Services Manager Gladhill discussed the proposed signage in further detail with the Commission noting Staff was proposing 150 square feet for tenant signs, along with wall signs.

Commissioner Field inquired if the retail gateway signs would be split up by lots or tenants.

Development Services Manager Gladhill explained the framework would allow for community gateway signs to be 150 square feet per lot. This would be broken down to 50 square feet per user or tenant.

Commissioner Brauer commented he was in favor of the kiosks.

Commissioner Van Scoy asked if the COR would have enough parking if several intense uses move into the area.

Development Services Manager Gladhill indicated a parking study was completed which explored some of these concerns. He noted the restaurants would have more surface parking than retail. He stated the area should have sufficient parking based on the requirements within the framework.

Commissioner Field was pleased with the restaurant parking recommendations.

Commissioner Brauer suggested the signage square footage for the retail gateway signs be increased from 150 square feet to 200 square feet. He commented this would allow for greater flexibility in the future.

Development Services Manager Gladhill indicated this could be done, but the general aesthetics should be considered. He stated future tenants could also pursue a larger sign size through the CUP process.

Commissioner Field questioned how many kiosk signs would be located in the COR. He suggested the scale of these signs be reduced to lower the expense of the proposed kiosk signs.

Development Services Manager Gladhill noted there were three kiosk signs proposed for the COR. He then reviewed the proposed location of these signs. He explained the stone used would be a precast material to reduce expenses. He indicated the monument signs would be high quality, consistent throughout the development and would assist pedestrian traffic traveling through the COR.

Chairperson Levine indicated the proposed location of the kiosk signs would serve future pedestrian traffic well.

Commissioner Van Scoy inquired if the Central Anoka County Regional Trail would be rerouted to run through the COR. He asked Staff for a visual of the trail location. He then asked if the sub-district boundaries would be redone.

Development Services Manager Gladhill explained the trail was previous discussed by the Council and a revised map was included in the framework draft. He noted the Mississippi skyway would tie into the proposed ramp and would be explored further with the Metropolitan Council. He then reviewed the area of change within the sub-district boundaries with the

Commission and discussed a more detailed map of the COR. He noted the preferred alignment of the regional trail was designated on the map to assist the City in gaining future dollars to assist with the realignment.

Commissioner Van Scoy questioned where the trail would cross Highway 10.

Development Services Manager Gladhill described the location with the Commission stating this would be a key connection.

Motion by Commissioner Bauer, seconded by Commissioner Field, to recommend that City Council adopt the proposed ordinance amending City Code Section 117-118, which includes the revised Design Framework and Master Sign Plan, redefining the sub-district boundaries between 2A and 2B, along with revising the restaurant seating requirements.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, Field, Brauer, and Van Scoy. Voting No: None. Absent: Commissioner Perez and Schiller.

Meeting Date: 11/27/2012

By: Tim Gladhill, Community Development

Information

Title:

Adopt Ordinance to Amend City Code Chapter 117, Article II, Division 8 (Signs) Related to Temporary, Construction, Real Estate, and Off-Premise Signs

Background:

In reviewing sign permits approved over the past several years, Staff has identified several topics for discussion:

- Temporary Signs
- Real Estate Signs
- Construction Signs

The last major revision to the City's Sign Code was completed in 2008. This revision was completed in response to a Minnesota Supreme Court ruling regarding a local Minnesota city's ordinance and content-based regulation. This revision attempted to best match the feedback from said court ruling, removing all references to content-based regulation. In addition, dynamic display technology began to emerge, especially for billboard technology. Since then, the City has processed two (2) minor adjustments to the Sign Code. Staff has also been monitoring new, emerging technology and surrounding communities' sign regulations. Based on that analysis, and feedback from previous applicants, Staff is suggesting the following changes.

The following recommendations are based on previous reviews and feedback received from sign applicants, and is intended to provide a more flexible set of standards that better matches the community's goals and desires.

The case includes two (2) attachments related to revised text. 'Redlined Text' is a document with all language included in the amended sections. 'Ordinance' is a shortened version for publication that only includes sections that are proposed for revision. To best understand the context of each amendment, refer to 'Redlined Text'.

Notification:

The Public Hearing was properly published in the Anoka County Union, the City's official newspaper.

Observations:

The following is a summary of existing standards and proposed amendments to the City's general sign regulations. In reviewing potential amendments, the Planning Commission should focus on standards to address size, location, duration, and construction specifications. Sign regulations should not be based on the content of the sign. Please note that these sign regulations do not apply to The COR development, as that area carries its own standards due to the architectural requirements of the development.

Temporary Signs

The most frequent request Staff responds to is in regard to temporary signs. The temporary sign regulations are currently drafted in an effort to mainly address the portable, interchangeable letter signs. Largely due to aesthetics, these types of signs are limited to six (6) weeks per calendar year per parcel (or per business on a multi-tenant parcel), with a bonus four (4) weeks afforded to those parcels with no documented sign violations within the past twelve (12) months. In a cursory review of surrounding communities, the timeline established in Ramsey is average to above average. Temporary Signs are limited to fifty (50) square feet.

Staff has not proposed any changes in the draft ordinance at this time, but is seeking feedback from the Planning Commission on any desired amendments. The City could consider separate timeframes based on construction

specifications. For example, the City could consider interchangeable letter signs differently than vinyl affixed lettering or banners.

Real Estate Signs

The second most frequent request for information Staff receives is in regard to real estate signs. With the 2008 sign code update, Staff was working off recommendations that real estate signs should not be treated any differently than temporary signs. Staff suggests considering these closer to non-commercial speech signs. These signs are not advertising for a product or service itself occurring on the site, rather indicating that the space itself is available. Staff believes there is a greater public benefit in providing exemptions to real estate signs in order to protect the health, safety, and welfare associated with vacant areas. The current draft does not specify a limit on the size. The Planning Commission could consider a size limitation.

Construction Signs

City Code does currently have provisions that actually require certain construction related signs. However, the Sign Code is silent on Construction Signs. The attached draft provides an exemption to the temporary sign regulations for these required types of signage. The current draft does not specify a size limit. The Planning Commission could consider a size limitation. If that is the case, Staff would recommend either 32 or 64 square feet per face, based on the standard size of a sheet of plywood typically used for these types of signs. Alternatively, these signs could be limited to the size allowed for permanent signs in the underlying zoning district.

Dynamic Display Signs

Over the past several years, technology related to dynamic display continues to evolve and improve. The City's current standards aim to reduce driver distraction by addressing length of display, animation, brightness, and size/percentage of overall sign. Currently, dynamic display is limited to 35% of the total sign face.

As Staff has reviewed multiple requests for dynamic display, it has become evident that the percentage limitation has become somewhat of a barrier, especially on higher speed roads where a larger display may actually be more beneficial by making the sign more readable. Staff suggests considering removing the percentage limitation and focus on an overall size limitation. By focusing on limiting the size, brightness, and other distracting factors, a percentage of the overall sign face may not be necessary. There are a few examples in the near vicinity of larger dynamic displays to consider.

Also, the City has received a request for the use of dynamic display for a portable, temporary sign. The entire sign face would be dynamic. Sizes for temporary, portable dynamic displays could be limited to traditional temporary sign limitations of 32 square feet or 50 square feet.

Off-Premise Signs

Currently, all off-premise signs are considered billboards, which are currently prohibited. Staff suggests a more flexible definition of off-premise, perhaps focusing on proximity rather than limiting to just the parcel on which the activity is occurring. The attached draft creates allowances for shared signage at shared access and shared signage for unified developments.

The ordinance was introduced by the City Council on November 13th, and is now eligible for adoption.

Funding Source:

Preparation of the ordinance is being handled as part of regular Staff duties.

Staff Recommendation:

The Planning Commission met on Thursday, October 4th, 2012 and recommended approval of the ordinance amending City Code Chapter 117, Article II, Division 8 (Signs), with an amendment to the draft to include a maximum size for Real Estate Signs (included in current draft).

Committee Action:

Motion to waive the City Charter requirement to read the ordinance aloud;

-AND-

Motion to adopt the ordinance amending City Code Chapter 117, Article II, Division 8 (Signs).

Roll Call Vote:

Councilmember Wise
Councilmember Backous
Councilmember Elvig
Councilmember McGlone
Councilmember Tossey
Councilmember Strommen
Mayor Ramsey

Attachments

First Amendment

Dynamic Display

RSF Study

Sample Ordinance

ALL Revised Text

Proposed Ordinance

Form Review

Inbox	Reviewed By	Date
Tim Gladhill (Originator)	Tim Gladhill	11/19/2012 04:58 PM
Kurt Ulrich	Kurt Ulrich	11/21/2012 11:33 AM
Form Started By: Tim Gladhill		Started On: 11/18/2012

Final Approval Date: 11/21/2012



RISK MANAGEMENT INFORMATION
SIGN ORDINANCES AND THE FIRST AMENDMENT

This memorandum discusses some of the basics of designing a sign ordinance that meets the requirements of the First Amendment.

General First Amendment Principles

The First Amendment protects signs as speech, and courts will look very closely at any attempts to regulate signs.

There are a few rules for regulating signs:

1. Do not regulate based on content.
2. Do not favor commercial speech (advertising) over noncommercial speech.
3. Restrictions on signs must accomplish a substantial government interest and be no broader than necessary. The main substantial governmental interests recognized by courts are traffic safety and aesthetics.

With this background in mind, there are several steps cities can take when drafting ordinances.

Provisions All Sign Ordinances Should Have

Every sign ordinance should probably contain the following provisions:

Statement of Purpose

Tells why the ordinance was drafted and how it should be applied. Should state clearly that it is not intended to have content-based restrictions and should not be applied that way. Provides a quick clear statement of government purposes and how the ordinance fulfills those purposes rather than needing to review your legislative record if challenged.

Sample Ordinance

View a sample sign ordinance from the city of Hopkins in the Land Use area of the League website at www.lmc.org.

Substitution Clause

Provides that for every sign that is allowed, any non-commercial message could be legally substituted. Ensures that non-commercial speech is never discriminated against based on content because it will always allow a noncommercial message on any sign. Many ordinances inadvertently define signs in terms of advertising and may incidentally seem to allow only commercial messages. A substitution clause may correct these mistakes by providing a catch-all allowance of noncommercial messages notwithstanding other provisions.

This material is provided as general information and is not a substitute for legal advice.
Consult your attorney for advice concerning specific situations.

Severability Clause

Provides that if any provision of the ordinance is found to be invalid, the remainder of the ordinance stands on its own and is still valid. May prevent a flaw in part of the ordinance from invalidating all of it.

Acknowledgement of election season pre-emption

Minnesota Statute 211B.045 requires municipalities to allow noncommercial signs of any size during election season, from August 1 until ten days after the election.

Do not misinterpret this language. It gives extra protection to noncommercial speech during election season. It does not limit political signs to only that time.

Content Neutral regulations based on time, place, and manner

Regulations should be objectively based on time, place, and manner, not content. Examples include regulations based on size, brightness, zoning district, spacing, and movement.

Provisions All Sign Ordinances Should Avoid

Unfettered discretion

Avoid discretionary approval by the city. Having discretion creates the potential for favoring some messages or messengers over others, whether or not that discretion is actually abused. Permit requirements should be transparent and objective.

Exemptions or favoritism

Avoid exempting certain groups or messages, such as church signs or official flags, from permit requirements. This could be content-based discrimination.

Exemptions also may “water down” the substantial government interest. For example, if an ordinance prohibits temporary signs but allows a long list of exemptions, it suggests the city is not really concerned about temporary signs.

This is different from providing exemptions based on valid time, place, or manner restrictions, such as exempting all signs under a certain size from permitting requirements.

Inadvertently treating non-commercial speech differently by defining “sign” as “advertising”

Beware of over-defining terms. This occasional problem is the combination of a few steps:

1. Signs are defined as advertising devices.
2. The ordinance allows signs as defined.
3. All other signs are prohibited.

This arguably prohibits noncommercial speech, which is unconstitutional.

Common Questions and Issues

Off-premise advertising (Billboards)

Off-premise advertising consists of commercial signs that do not advertise for a business on the same premises as the sign. It is legal to forbid off-premise advertising, so long as the prohibition does not extend to noncommercial messages.

Flags

Be cautious of regulations that might favor some types of flags, particularly the United States Flag, over other flags. This is a good place for the substitution clause; if one type of noncommercial flag would be acceptable, any noncommercial flag should be allowed.

Yard Signs

Some courts have held that yard signs are constitutionally protected and cannot be prohibited. Be especially cautious about provisions that favor some messages over others, such as exemptions for real estate or construction project signs.

Electronic Signs

Electronic signs present new challenges, as the technology is capable of new levels of brightness, movement, flashing, and potential distraction. Most sign ordinances do not adequately address these issues. The League has commissioned a study on the traffic safety implications of the technology. Cities may wish to consider moratoriums while the study is conducted and then drafting ordinances that apply the information to each community. A moratorium may prevent electronic signs from becoming grandfathered.

More Information

Learn more about dynamic signage in:

- *Regulating Dynamic Signage*

It's available at www.lmc.org.

Summary

Keep in mind these basic rules of thumb:

1. Do not regulate content.
2. Do not favor commercial speech over noncommercial speech.
3. Provide and follow clear procedures.
4. Explain your rationale and purpose.
5. Avoid exceptions.

There are exceptions to these rules, but they should be approached cautiously and with legal advice.

For assistance or sample ordinances, contact one of the LMCIT land use attorneys: Paul Merwin at 651-281-1278; or Jed Burkett at 651-281-1247.



RISK MANAGEMENT INFORMATION
REGULATING DYNAMIC SIGNAGE

Executive summary

Cities have authority and responsibility to regulate dynamic signs as appropriate for each community. There is no single correct approach to regulation. Because the regulation of signs involves the First Amendment, courts hold sign regulations to a higher standard than most land use regulations. Cities still have considerable discretion to regulate, as long as they do so reasonably and without regard to sign content.

Introduction

In the fall of 2006, a number of Minnesota cities were surprised by the appearance of large electronic billboards akin to giant television screens. These signs are the next generation of sign displays with the ability to feature changing images and movement—known collectively as dynamic signs. Attempts to regulate them resulted in litigation in at least one community- Minnetonka. In developing a regulatory response, Minnetonka partnered with the League of Minnesota Cities to commission a study, conducted by SRF Engineering, on the impact of such dynamic signs on traffic safety. This memorandum discusses the legal framework of regulating dynamic signage in light of the recent litigation and study.

More Information

Find the results of the SRF Consulting Group's research on dynamic signage in:

- *"Dynamic" Signage: Research Related to Driver Distraction and Ordinance Recommendations*

It's available in the Land Use area of the League website at www.lmc.org.

Regulatory framework

While the federal and state government can enact and have enacted laws regulating signs, those regulations only provide minimum standards. Courts have explicitly recognized that cities have the ability to regulate signs, including dynamic signs, more restrictively.

There is no uniform system of regulation that cities must follow. Each community is different and has different needs that local ordinances may reflect. Such regulations must meet the same basic legal tests for all sign regulation.

Most city land use decisions get a very deferential standard of review known as *rational basis* review. Under this level of review, city decision will be upheld if they have any rational basis.

Because sign regulations implicate free speech rights which are protected by the First Amendment, they are subjected to higher levels of scrutiny. The highest level of scrutiny, called *strict scrutiny*,

This material is provided as general information and is not a substitute for legal advice.
Consult your attorney for advice concerning specific situations.

applies when government tries to regulate based on the content of speech. The only content-based sign regulation that courts have upheld is treating off-premise signs (billboards) differently than on-premise signs that advertise the business on the same property.

One distinction that may seem like it is content based, but our federal court of appeals has said is not, is a ban on dynamic signs with an exception for time and temperature displays. The court held

that because of their unique nature, allowing only time and temp displays is not a prohibited content-based regulation. It is important not to overstate this, however. Regulations that go further and carve out a broader exception for “public information” are likely to be struck down as impermissibly content-based.

Sign regulations that are not content based are subject to *intermediate scrutiny*, which tests whether the regulation is substantially related to a significant government interest. This roughly translates to “regulate for a good reason.” Cities should take care that the *scope* of the regulation is not excessive when viewed in light of all of the regulatory objectives, and that they do not create exceptions to the regulations that cannot be justified by reference to one or more of the city’s articulated objectives

Big-picture regulatory tools

The available research on traffic impacts supports significant content-neutral limits or even bans on dynamic signs for safety reasons. The studies confirm that billboards can tend to distract drivers, dynamic features contribute to the distraction, and even short distractions can increase the risk of accidents. This is not surprising as promotional materials put out by sign companies themselves boast the signs’ ability to hold viewer attention as a benefit of dynamic signs.

Safety is only one concern. Cities may also regulate signs based on values, preferences, and aesthetics. Not every sign is appropriate in every community or every neighborhood. Not every community wishes to become Las Vegas or even downtown Minneapolis.

Cities can take a number of different macro-level approaches to regulation. Some examples include:

1. Complete or near-complete bans that do not allow dynamic signs at all.
2. Allow dynamic signs with restrictions such as minimum display time, allowing only a percentage of a sign to change, or text size limitations.
3. Allow different things in different zoning districts, such as allowing brighter dynamic signs in a downtown business district than in residential neighborhoods.
4. Offering incentive programs to billboard companies to allow dynamic signs in exchange for removal of non-conforming static signs.
5. Encourage dynamic displays. Some communities like the clean, new look of dynamic signs and encourage them to remove old blighted and poorly maintained signs.

More Information

Learn more about sign regulations and free speech in:

- *Sign Ordinances and the First Amendment*

It’s available at www.lmc.org.

Regulating sign aspects

A content-neutral regulation that regulates dynamic signage will be subject to intermediate scrutiny, so a community must show a regulation is substantially related to a significant government interest. In plain language, you must articulate what problem a regulation is intended to address and how the regulation addresses it.

There are at least six aspects of dynamic signs that regulations may address:

1. **Duration of messages/ speed of changeover.** Studies have described the Zeigarnik effect, a psychological need to see a task through to its end. In the case of dynamic signs, a driver's desire to read an entire message before it changes or to complete a scrolling message has been shown to negatively impact drivers' tendencies to maintain a constant speed or remain in a lane. To address these issues, many cities have imposed minimum message durations that might vary depending on community preference and traffic conditions.
2. **Motion, animation, and video.** Motion can range from simple visual effects to full realistic video. Motion can extend the period of time a driver will keep watching a sign, increasing distractedness. Cities may prohibit motion or limit it either to specific areas or to specific characteristics such as a motion time frame calibrated to traffic speed.
3. **Brightness.** Brightness can be a safety factor, particularly at night, as sudden brightness can be distracting or diminish night vision. A number of communities limit brightness based on time of day and by color displayed. This can be difficult to quantify and measure.
4. **Sign placement and spacing.** The number of signs and their location can be a big factor in driver awareness. A large number of signs can increase distractedness. Poorly placed signs may block views or cause distraction in unsafe areas. Cities may impose site standards and spacing requirements. These may present regulatory challenges as spacing may be dependent on the actions of neighboring property owners.
6. **Size of signs.** Size can have impacts in several ways. Too big, and it obstructs views and distracts. Too small, and it takes longer to read and encourages sign users to sequence messages. Cities may limit dynamic signs or the percentage of a sign that can be dynamic.
7. **Text size and legibility.** Signs that are difficult to read invite increased driver focus. Regulations can, for example, require minimum sizes based on road speed.

The specifics of how to regulate each of these aspects is up to each community. Because review of regulations must face intermediate scrutiny, cities have to take some extra steps when drafting and adopting ordinances.

For each aspect regulated, cities should consider adopting findings or local studies that articulate the reason and any support for the regulation. The SRF study and other materials can provide a scientific basis for a number of regulatory steps. In addition, cities may choose more stringent regulation in order to take a conservative approach to protecting safety.

Moving forward

It is recommended that cities think about dynamic signs as early as possible. Regardless of your city's approach, it is better to make a rational choice rather than by having dynamic signs arrive before you have thought about the issue. Once the signs are up, Minnesota's nonconforming use law arguably grants them "grandfathered" status, with a narrow exception for safety.

If your city would like more information about regulating dynamic signs, Paul Merwin, LMCIT Senior Land Use Attorney, can provide assistance and refer you to more information and resources. Contact Paul at (651) 281-1278 or pmerwin@lmc.org.

Disclaimer: This memorandum is intended as general information only and should not be read as legal advice or as creating an attorney-client relationship. This memo addresses general concerns and has not been reviewed in the context of a specific client or situation. This memo was drafted as a loss control document and is intended to avoid conflicts rather than form an opinion as to the legality or defensibility of any action.

Paul Merwin 07/07

Sample Ordinance

View a sample sign ordinance from the city of Hopkins in the Land Use area of the League website at www.lmc.org.

**“DYNAMIC” SIGNAGE:
RESEARCH RELATED TO DRIVER DISTRACTION
AND
ORDINANCE RECOMMENDATIONS**

Submitted by
SRF Consulting Group, Inc.

Prepared for
City of Minnetonka
June 7, 2007

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1.0 INTRODUCTION

This study was precipitated by concerns raised by the City of Minnetonka, Minnesota in regard to the installation of two LED (“light emitting diode”) billboards along Interstate 394 and Interstate 494. The LED function was applied to two existing “static” image billboards located adjacent to the interstate. Following installation of the LED function, the City turned off the power to the signs through a stop work order based on current city ordinance prohibiting flashing signs, which is broadly defined, as well as permitting requirements for the retrofitting of the signs to the upgraded technology. The billboard owner sued the City, and the court response to this legal action as of the writing of this study has been to allow limited use of the LED billboards. A moratorium on further signage of this type was established by the City to facilitate the study of issues related to driver distraction and safety and appropriate regulatory measures for LED and other types of changeable signage.

This study was undertaken on behalf of the City of Minnetonka to examine these issues. While the concerns were precipitated by LED billboards in particular, this report examines more broadly “dynamic” display signage which is defined as any characteristics of a sign that appear to have movement or that appear to change, caused by any method other than physically removing and replacing the sign or its components, whether the apparent movement or change is in the display, the sign structure itself, or any other component of the sign. This includes a display that incorporates a technology or method allowing the sign face to change the image without having to physically or mechanically replace the sign face or its components. This also includes any rotating, revolving, moving, flashing, blinking, or animated display and any display that incorporates rotating panels, LED lights manipulated through digital input, “digital ink” or any other method or technology that allows the sign face to present a series of images or displays. These capabilities may be provided by a variety of technologies which are discussed later in this report.

As the study progressed, additional communities within the Twin Cities Metropolitan Area, as well as the League of Minnesota Cities, expressed interest in these issues. However, it is not the intention of this report to provide a comprehensive study of all issues raised by dynamic signage, or other types of billboards, but rather to focus narrowly on the issues of concern to the City of Minnetonka.

2.0 PURPOSE OF STUDY AND METHODOLOGY

Driving a motor vehicle is a complex task that requires the ability to divide one’s attention. Simultaneously maintaining a steady and legal speed, changing lanes, navigating traffic and intersections, reading and interpreting street signs, drivers are often challenged by conditions that can change in the blink of an eye. Internal and external physical conditions can affect how safely the driving task is accomplished. Drug or alcohol intoxication, fatigue and/or distractions in the driving environment all can play a role in motor vehicle crashes. However, these conditions are rarely the sole reason for a crash. Rather, these conditions serve to exacerbate an already-complex driving environment and subsequent mistakes in judgment can lead to crashes.

Increasingly complex traffic and roadway environments require greater attention to and focus on the driving task.

The purpose of this study is to understand what existing transportation research tells us about the effects of dynamic signs on motorists. This study also explores regulatory measures enacted in other jurisdictions to address concerns related to driver distraction. Due to time and scope constraints, this report is not comprehensive, but rather addresses the most frequently cited and easily accessible information available. The report concludes with a discussion of regulatory options for the City of Minnetonka to consider in their formulation of policies to address dynamic signage.

Information collected for this report draws from a variety of sources including interviews with subject matter experts, government and academic research, and policies developed to regulate various types of signage.

Several city and county sign ordinances were used as references for policy and regulatory research. In some cases, ordinances were brought to our attention by planners and others following the sign ordinance issue. In others, Internet searches were conducted using words and references that apply specifically to dynamic signs.

Several sign manufacturers and sign companies provided an industry perspective through a workshop with the SRF Consulting Group and the City of Minnetonka staff on February 27, 2007. This meeting yielded information about sign characteristics that can be addressed through policy and regulatory measures. Daktronics, a company that manufactures and markets LED signs, was also helpful in this regard, providing informational materials about characteristics of signs that can be regulated and examples of city sign ordinances with which they are familiar.

3.0 SELECTED RESEARCH FINDINGS

This following section presents a summary of expert opinions and selected driver distraction research conducted by government and academic researchers examining roadside signage and its effects on the driving task. Studies are organized around critical questions with serious research ramifications.

- *Is there reason to believe that billboards are a source of distraction?*
- *Is there reason to believe that “dynamic” billboards are an additional source of distraction?*
- *How much distraction is a problem?*
- *How does “brightness” affect driver safety concerns?*
- *How should billboards and other signage be regulated from a driver safety perspective?*

3.1 Expert Opinions

A combination of researchers and public policy experts were interviewed for this study. Individuals were identified while conducting background research into driver distraction and were interviewed because of their credibility in the field.

Kathleen Harder, a researcher at the University of Minnesota, has conducted driver distraction research for a variety of applications, including research for Mn/DOT. She is an expert in the field of human factors and psychology. She indicated that electronic billboards pose a driver distraction threat because of their ability to display high resolution color images, their ability to change images, and their placement in relationship to the roadway, particularly in areas where the road curves, exits and entrances are present, merges, lane drops, weaving areas, key locations of official signs, and/or areas where roadways divide.

Greg Davis, a researcher with the FHWA Office of Safety Research and Development, in Washington, DC was involved in the 2001 FHWA study on electronic billboards. He was interviewed to gain a deeper understanding of this critical study and to learn of recent research in this area. Davis stated that while no research has established a direct cause and effect relationship between electronic outdoor advertising signs and crash rates, the lack of such a research finding does not preclude a causal relationship between electronic billboards and crashes. He advocated for a new study that can control all variables and determine if a cause and effect relationship exists.

Scott Robinson, an outdoor advertising regulator for Mn/DOT, wrote the 2003 technical memorandum that addresses allowable changes for outdoor advertising devices. Mr. Robinson indicated that the memo was originally written in 1998 to establish a permitted rate of change for tri-vision signs and that the application to electronic billboards was not considered. The minimum change rate of 4.9 seconds for 70 mph roadways and 6.2 seconds for 55 mph roadways was based on the travel time between static signs spaced at the minimum allowed distance apart. Mr. Robinson also indicated that the memo is not a Mn/DOT policy, statute or rule, but rather it was written to provide internal guidance.

Jerry Wachtel, an Engineering Psychologist and highway safety expert in private practice, was the lead author for the FHWA's original (1980) study on electronic billboards. He has continued his active involvement in this field, and advises Government agencies as well as the outdoor advertising industry on sign ordinances, sign operations, and the implications of the latest research on road safety. Mr. Wachtel believes that it is neither feasible from the perspective of research design and methodology, nor necessary from a regulatory perspective, to demonstrate a causal relationship between digital billboards and road safety. Rather, he believes that we have a strong understanding, based on many years of research, of driver information processing capabilities and limitations, and of the contributions to, and consequences of, driver distraction, on crash risk; and that this understanding is sufficient to support development of guidelines and ordinances for the design, placement, and operation of digital billboards so as to lessen their potentially adverse impact on road safety and traffic operations.

Wachtel also offered comments on drafts of this report. In later conversations related to his review, Wachtel stated his belief that even though visual fixations on roadway signs decrease as route familiarity increases, a strength of the new digital billboards is that they can present messages *that are always new*. Thus, the conclusion from the 1980 FHWA study is another argument against these billboards; namely, drivers spend more time looking at the unfamiliar signs than at familiar ones, suggesting digital billboards are more dangerous than traditional fixed billboards. Wachtel also suggested his preference for a goal to have any given driver experience only one, or a maximum of two, messages from an individual roadside sign.

3.2 Billboards: a Source of Driver Distraction? ¹

The purpose of a sign is to attract the attention of passersby so that a message is conveyed. To the degree signs attract the attention of vehicle drivers, they may distract them from the activity of driving. While this report primarily examines the impact of *dynamic* roadside advertising, the role traditional *static* advertising plays in driver distraction is discussed below.

The relationship between roadside advertising and crash rates has been the subject of several studies. The majority of this research was conducted in the 1950s, 60s and 70s. While some of the earliest studies have been subsequently criticized for flawed methodologies and improper statistical techniques, some findings emerge when the totality of the studies are examined. One of these findings is that the correlation between crash rates and roadside advertising is strongest in complex driving environments. For example, higher crash rates were found at intersections (generally considered a complex environment) that have advertising than those intersections that do not have advertising. A few of the studies that are important in this field are summarized below.

Minnesota Department of Transportation Field Study (1951) and Michigan State Highway Department Field Study (1952) ²

These two studies from the early 1950s used similar methods but came to significantly different conclusions. Recognized as the more scientifically rigorous study, the Minnesota study found that increases in the number of advertising signs per mile are correlated with increases in motor vehicle crash rates. It also found that intersections with at least four advertising signs experienced three times more crashes than intersections with no advertising signs. Conversely, the less rigorous Michigan study found the presence of advertising signs had no effect on the number of crashes.

Iowa State College, Do Road Signs Affect Accidents? (Lauer & McMonagle, 1955)³

A laboratory test was created to determine the effect of advertising signs on driver behavior. The results of this study found removing all advertising signs from the driver's field of vision did not improve driver performance. When signs were included, driver performance was slightly better. Note that laboratory methods used in this study are considered to be dated by today's standards.

Faustman (California Route 40) Field Study (1961)⁴ and Federal Highway Administration, Reanalysis of Faustman Field Study (1973)⁵

Two studies that appear to have stood the test of time are Faustman’s original analysis of California Route 40 and its re-examination by FHWA more than a decade later. The original analysis tried to improve upon previous research by limiting variables, such as roadway geometric design and roadway access controls. The FHWA reanalysis focused on disaggregating the data and converting actual crashes to expected crash rates on specific roadway sections. Each of the sections was given a value based on the number of billboards on the section. A linear regression was performed to determine the expected crash rates. An analysis of variance of the regression coefficients found that the number of billboards on a section was statistically significant. The reanalysis found a strong correlation between the number of billboards and crash rates as shown in Table 1.

Table 1. FHWA Reanalysis of Faustman’s Findings.

No. of Billboards	Expected No. of Accidents in a 5-year Period	Cumulative Increase in Accident Rate
0	5.92	
1	6.65	12.3
2	7.38	24.2
3	8.11	37.0
4	8.84	49.3
5	9.57	61.7

Federal Highway Administration

Safety and Environmental Design Considerations in the Use of Commercial Electronic Variable-Message Signage (Wachtel & Netherton, 1980)⁶

This extensive review provides a comprehensive discussion of roadside advertising research as of 1980. The study authors noted “attempts to quantify the impact of roadside advertising on traffic safety have not yielded conclusive results.” The authors found that courts typically rule on the side of disallowing billboards because of the “readily understood logic that a driver cannot be expected to give full attention to his driving tasks when he is reading a billboard.” Because the distraction evidence is not conclusive, these decisions were generally not based on empirical evidence.

The research review noted that accident reports often cite “driver distraction” as a default category used by uncertain law enforcement officers who must identify the cause of a crash. As a result, the authors believe crashes due to driver distraction are not always properly identified. In addition, law enforcement officers often fail to indicate the precise crash locations on crash reports, making it difficult to establish relationships between crashes and roadside features.

Accident Research Unit, School of Psychology, University of Nottingham
Attraction and distraction of attention with roadside advertisements (Crundall et al., 2005)⁷

This research used eye movement tracking to measure the difference between street-level advertisements and raised advertisements in terms of how they held drivers' attention at times when attention should have been devoted to driving tasks. The study found that street-level advertising signs are more distracting than raised signs.

3.3 “Dynamic” Billboards: an Additional Source of Distraction?

Signage owners or leasers want to incorporate dynamic features into their signage for a number of reasons: to enhance the sign's ability to attract attention, to facilitate display of larger amounts of information within the same sign area, to conveniently change message content, and to enhance profitability. As mentioned earlier, this report uses the term “dynamic” signs to refer to non-static signs capable of displaying multiple messages. Several studies documented the ability of a sign to accomplish the first of these goals.

University of Toronto
Observed Driver Glance Behavior at Roadside Advertising Signs (Beijer & Smiley, 2004)⁸

Research done at the University of Toronto compared driver behavior subject to passive (static) and active (dynamic) signs. The study found that about twice as many glances were made toward the active signs than passive signs. A disproportionately larger number of long glances (greater than 0.75 seconds) taken were toward the active signs. The duration of 0.75 seconds is important because it is close to the minimum perception-reaction time required for a driver to react to a slowing vehicle. For vehicles with close following distances, or under unusually complex driving conditions, a perception delay of this length could increase the chance of a crash. The following findings were reported in this study:

- 88% of the subjects made long glances (greater than 0.75 seconds).
- 22% of all glances made at all signs were long glances (greater than 0.75 seconds).
- 20% of all the subjects made long glances of over two seconds.
- As compared to static and scrolling text signs, video and tri-vision signs attracted more long glances.
- Video and scrolling text signs received the longest average maximum glance duration.
- All three of the moving sign types (video, scrolling text and tri-vision) attracted more than twice as many glances as static signs.

University of Toronto

Impact of Video Advertising on Driver Fixation Patterns (Smiley et al., 2001)⁹

Another study completed at the University of Toronto used similar eye fixation information in urban locations to show that drivers made roughly the same number of glances at traffic signals and street signs with and without full-motion video billboards present. This may be interpreted to mean that while electronic billboards may be distracting, they do not appear to distract drivers from noticing traffic signs. This study also found that video signs entering the driver's line of sight directly in front of the vehicle (e.g., when the sign is situated at a curve) are very distracting.

City of Seattle Report (Wachtel, 2001)¹⁰

The City of Seattle commissioned a report in 2001 to examine the relationship between electronic signs with moving/flashing images and driver distraction. The report found that electronic signs with moving images contribute to driver distraction for longer intervals than electronic signs with no movement. Following are major points made in the report:

- New video display technologies produce images of higher quality than previously available technologies. These signs have improved color, image quality and brightness.
- New video display technologies use LEDs with higher viewing angles. Drivers can read the sign from very close distances when they are at a large angle from the face of the sign.
- Signs with a visual story or message that carries for two or more frames are particularly distracting because drivers tend to focus on the message until it is completed rather than the driving task at hand.
- Research has shown that drivers expend about 80 percent of their attention on driving related tasks, leaving 20% of their attention for non-essential tasks.
- The Seattle consultant suggests a "10 second rule" as the maximum display time for a video message.

The expanded content of a dynamic sign also contributes to extended distraction from the driving task. The Seattle Report examined how this may be due in part to the *Zeigarnik effect* which describes the psychological need to follow a task to its conclusion. People's attention is limited by the ability to only focus on a small number of tasks at a time, and by the tendency to choose to complete one task before beginning another. In a driving environment, drivers' attention might be drawn to the sign rather than the task of driving because they are waiting to see a change in the message. This loss of attention could lead to unsafe driving behaviors, such as prolonged glances away from the roadway, slowing, or even lane departure.

While the Zeigarnik effect may be present in a wide variety of driving situations, possible scenarios that could affect drivers include:

- A scrolling message requires the viewer to concentrate as the message is revealed. Based on the size and resolution of the sign, and the length of the message, this could range from less than one second to many seconds.
- A sequence of images or messages that tell a story, during which the driver's attention may be captured for the entire duration that the sign is visible. Instead of merely glancing at the sign and then returning concentration to the driving task, more attention may be given to the message.
- Anticipation of a new image appearing, even if the expected new image is not related to the first image. In this case, the driver may be distracted while waiting for the change.

Federal Highway Administration

Safety and Environmental Design Considerations in the Use of Commercial Electronic Variable-Message Signage (Wachtel & Netherton, 1980)¹¹

This research provides information on the use of on-premise Commercial Electronic Variable-Message Signs (CEVMS) that display public service information (i.e., time and temperature) and advertising messages along the Interstate highway system. The research found the following major considerations:

- **Highway Safety Considerations**

The link between changing messages that attract drivers' attention and crashes has been an issue of concern since the earliest forms of electronic signage became available. This study thoroughly reviewed the literature seeking information regarding a potential link between CEVMS and crashes:

“Although a trend in recent findings has begun to point to a demonstrable relationship between CEVMS and accidents, the available evidence remains statistically insufficient to scientifically support this relationship.”

The study also noted that studies have not documented information about “such occurrences as ‘near misses’ or traffic impedances that are widely recognized as relevant to safety, and which may or may not be attributable to the presence of roadside advertising.”

- **Human Factors Considerations**

Human factors relate to all the elements that explain driver behavior, such as eye glances and driver responses to a variety of driving-related stimuli. The study makes the point that simple driving-related tasks consume relatively little information processing capacity. However, when other conditions, such as congestion, complicated roadway geometries, or weather are also considered, the marginal extra

amount of attention required to read roadside advertisements could lead to driving errors that could cause crashes.

“The enormous flexibility of display possessed by CEVMS makes it possible to use them in ways that can attract drivers' attention at greater distances, hold their attention longer, and deliver a wider variety of information and image stimuli than is possible by the use of conventional advertising signs.”

Texas Transportation Institute for FHWA, Impacts of Using Dynamic Features to Display Messages on Changeable Message Signs (Dudek et al., 2005) ¹²

This study examined the comprehension times for three different scenarios for DOT-operated changeable message signs. The scenarios evaluated were:

- Flashing an entire one-phase message
- Flashing one line of a one-phase message while two other lines of the message remain constant
- Alternating text on one line of a three-line CMS while keeping the other two lines of text constant on the second phase of the message

The findings of this study were:

- Flashing messages did not produce faster reading times.
- Flashing messages may have an adverse effect on message comprehension for unfamiliar drivers.
- Average reading times for flashing line messages and two-phase messages were significantly longer than for alternating messages.
- Message comprehension was negatively affected by flashing line messages.

While this research did not evaluate advertising-related signs, it does demonstrate that flashing signs require more of the driver's time and attention to comprehend the message. In the case of electronic billboards, this suggests that billboards that flash may require more time and attention to read than static ones.

3.3.1 OTHER INFORMATION

NHTSA Driver Distraction Internet Forum (2000) ¹³

The National Highway Traffic Safety Administration held an internet forum to gather research and public comment related to driver distraction with an emphasis on the use of cell phones, navigation systems, wireless Internet and other in-vehicle devices. During this forum, participants were invited to take a poll to determine the most prominent driver

distraction issues. Electronic billboards were identified as one of six noted sources of distraction.

Parliament of Victoria, Australia, Report of the Road Safety Committee on the Inquiry into Driver Distraction (2006)¹⁴

This report identified road signs and advertising as one of the largest sources of driver distraction. At least three billboards near Melbourne, Australia display moving images.

“The Committee considers these screens to be at the high end of potential visual distraction and accordingly, present a risk to drivers.”

The study also included a quote from the Manager of the Road User Behaviour group at VicRoads (the State's road and traffic authority) from a December 2005 hearing:

What we do know is when there is movement involved, such as flicker or movement in the visual periphery, that this is more likely to capture a driver's attention. We actually are hard-wired as human beings to movement, so particularly moving screens and information that scrolls at intersections and in highly complex driving situations – these are risky, and in particular researchers have been most concerned about those sort of advertising materials.

This opinion would suggest that electronic signs can present a distraction to drivers.

3.4 How Much Distraction Is a Problem?

A number of studies were identified that discussed concerns with driver distraction generally. It should be noted that some of the studies cited use specific crash data that is ten or more years old. Direct comparison of distraction sources to influences of today may not be completely valid due to increased technological sophistication of distracting influences. These could include in-vehicle technology (e.g., navigation systems, MP3 players, DVD players, CD players, computer systems, etc.) as well as other potentially distracting influences (e.g., cell phones, text messaging, dynamic signage, other roadway elements, etc.) that were not commonplace when the data for these studies was collected:

Australian Road Research Board
Investigations of Distraction by Irrelevant Information (Johnston & Cole, 1976)¹⁵

This research used five experiments to test whether drivers could maintain efficient performance in their driving tasks while being subjected to content that was information rich, but irrelevant to driving. The findings were that a small, but statistically significant amount of performance degradation was observed when the participant was under a critical load of stimuli.

National Highway Traffic Safety Administration/ Virginia Tech Transportation Institute

Impact of Driver Inattention on Near-Crash/Crash Risk: An Analysis Using the 100-Car Naturalistic Driving Study Data (Klauer et al., 2006)¹⁶

This study analyzed the data from a driving database developed by the National Highway Traffic Safety Administration. This database contained exhaustive data recorded by instrumented vehicles that measured glance position, impairment, drowsiness, risk taking and many other parameters potentially involved in crash causation. Vehicles were instrumented so that an observer did not need to be in the vehicle to collect data. Automated data collection reduced the problem of an observer influencing driver behavior. The study found that glances of two seconds or greater doubled the risk of crashes or near-crashes. The study also found that 22 percent of crashes are accompanied by “secondary-task” distraction whether inside or outside the vehicle.

National Highway Traffic Safety Administration/ Virginia Tech Transportation Institute

Driver Inattention is a Major Factor in Serious Traffic Crashes (2001)¹⁷

The National Highway Traffic Safety Administration commissioned a study to examine the causes of crashes. The study gathered information from four areas throughout the country and used data from the National Automotive Sampling System (NASS) from April 1996-April 1997 for analysis. The geographic areas were selected because they had good crash investigation practices and high interview completion rates. The results of this study are summarized in Table 2.

Table 2. Crash Causation Summary

Causal Category	Percentage of Drivers Contributing to Causation
Driver Inattention	22.7
Vehicle Speed	18.7
Alcohol Impairment	18.2
Perceptual Errors	15.1
Decision Errors	10.1
Incapacitation	6.4
Other	8.8

Association for the Advancement of Automotive Medicine

The Role of Driver Inattention in Crashes; New Statistics from the 1995 Crashworthiness Data System (Wang, 1996)¹⁸

This report analyzed the NHTSA 1995 Crash Worthiness Data System (CDS). It found that the greatest source of driver distraction (3.2 percent) was due to a specified person, object or event outside the vehicle. The full results of the study are presented in Table 3.

Table 3. Percentage of CDS Crashes Involving Inattention-Distraction Related Crash Causes

Data Element	% of Drivers	% of Crashes
Attentive or not distracted	46.6 %	28.4 %
Looked but did not see	5.6 %	9.7 %
Distracted by other occupant [specified]	0.9 %	1.6 %
Distracted by moving object in vehicle [specified]	0.3 %	0.5 %
Distracted while dialing, talking, or listening to cellular phone [location and type of phone specified]	0.1 %@	0.1 %@
Distracted while adjusting climate controls	0.2 %@	0.3 %@
Distracted while adjusting radio, cassette, CD [specified]	1.2 %	2.1 %
Distracted while using other device/object in vehicle [specified]	0.1 %	0.2 %
Sleepy or fell asleep	1.5 %	2.6 %
Distracted by outside person, object, or event [specified]	2.0 %	3.2 %
Eating or drinking	0.1 %	0.2 %
Smoking-related	0.1 %	0.2 %
Distracted/inattentive, details unknown	1.5 %	2.6 %
Other distraction [specified]	1.3 %	2.2 %
Unknown/No Driver	38.5 %	46.0 %

Weighted driver N = 4,627,000 (7,943, unweighted); weighted crash N = 2,619,000 (4,536);
 In order for a crash to be classified "attentive," all involved drivers had to be classified "attentive."
 @ - estimate based on 5-9 cases.

University of North Carolina Highway Safety Research Center
The Role of Driver Distraction in Traffic Crashes (Stutts et al., 2001)¹⁹

A study prepared by the University of North Carolina Highway Safety Research Center for the AAA Foundation for Traffic Safety examined the sources of driver distraction in traffic crashes. The data came from the CDS from 1995-1999. Of the thirteen specific sources of distraction tracked by the study, the greatest source of distraction was an outside person, object or event. While the study does not break down the sources of outside distraction, it does show that distractions outside the vehicle are the largest factor in distraction-related crashes. The results of this study are presented in Table 4.

Table 4. Specific Sources of Distraction Among Drivers in Distraction-Related Crashes

Specific Distraction	Percentage of Drivers
Outside person, object or event	29.4
Adjusting radio, cassette, CD	11.4
Other occupant in vehicle	10.9
Moving object in vehicle	4.3
Other device/object brought into vehicle	2.9
Adjusting vehicle/climate controls	2.8
Eating or drinking	1.7
Using/dialing cell phone	1.5
Smoking related	0.9
Other distraction	25.6
Unknown distraction	8.6
Total	100.0

Three studies were found which attempted to measure driver behavior specifically in response to dynamic signage. Two of these studies demonstrated a potential relationship between dynamic signage and crash rates:

Minnesota Department of Transportation, The Effectiveness and Safety of Traffic and Non-Traffic Related Messages Presented on Changeable Message Signs (CMS) (Harder, 2004) ²⁰

This study used a driving simulator to measure the effect of Department of Transportation changeable message signs on traffic flow. The two messages evaluated were a “crash ahead” warning and an AMBER Alert (child abduction information). The research found that just over half of the participants used the “crash ahead” message and 60 percent could recall the AMBER Alert with scores of Good or Better. Over one fifth of the participants slowed down by at least 2 mph upon seeing the AMBER Alert, demonstrating that messages relevant to drivers are associated with changes in at least some drivers’ travel speed .

Decision of the Outdoor Advertising Board in the Matter of John Donnelly & Sons, Permittee, Telespot of New England, Inc., Intervenor, and Department of Public Works, Intervenor, with Respect to Permit Numbered 19260 as Amended (1976) ²¹

This proceeding documents the Commonwealth of Massachusetts Outdoor Advertising Board’s ruling regarding one of the first changeable signs. This sign was located near an arterial road in Boston and used magnetic discs to portray a message that changed every 30 seconds. The original sign permit was rejected based on four criteria, one of which was safety. Upon appeal, the Massachusetts Department of Public Works allowed the permit based on the fact that the sign would give the public a benefit. However, they ultimately determined that the sign was a safety hazard based on crash rates before and after the sign was installed. Tables 5 and 6 show the change in crash rates.

Table 5. Telespot Sign Crash Rates - Expressway Southbound

	Average per year (1/1/1970-12/31/1972)	Average per year (1/1/1973-3/31/1975)	Average Percent Change
Crashes where the sign was viewable (north of sign)	29.0	20.0	-31.0
Crashes where the sign was not viewable (south of sign)	39.0	15.6	-60.0

Table 6. Telespot Sign Crash Rates - Expressway Northbound

	Average per year (1/1/1970- 12/31/1972)	Average per year (1/1/1973- 3/31/1975)	Average Percent Change
Crashes where the sign was viewable (south of sign)	46.3	42.7	-7.8
Crashes where the sign was not viewable (north of sign)	8.0	1.8	-77.5

This analysis shows that while crash rates decreased on comparable sections in the years after the sign was installed, the sections where the sign was visible experienced smaller crash rate decreases. Due to these arguments, the Board ruled that the operation of the sign must be terminated.

**Wisconsin Department of Transportation
Milwaukee County Stadium Variable Message Sign Study – Impacts of an
Advertising Variable Message Sign on Freeway Traffic (1994)**²²

A study prepared by the Wisconsin Department of Transportation (WisDOT) examined crash rates before and after an advertising variable message sign was installed in 1984 on the Milwaukee County Stadium, home of the Milwaukee Brewers professional baseball team. Crash statistics were analyzed for the three years before and the one and three years after the sign was installed. As they are often associated with driver distraction, side-swipe and rear-end crashes, as well as total crashes, were examined for both the eastbound and westbound directions. The sign was much more visible to eastbound traffic due to the stadium’s proximity to the roadway and the amount of visual obstructions for westbound traffic.

The analysis found an increase in crash rates for all crash types in the eastbound direction after the sign was installed. Most pronounced was an 80 percent increase in side-swipe crashes after the first year of installation. Results in the westbound direction were mixed, with a 29 percent decrease in crashes the first year the sign was in place and a 35 percent increase in the three years the sign was in place. Although no control roadway sections were studied, an interview with the study author revealed that the introduction of a sign on a high volume curving roadway may have introduced enough distraction to an already demanding driving environment to explain the higher crash rate in the eastbound direction. The study author also stated that the study was not able to establish a causal relationship between the sign and the crash rates.²³

**Federal Highway Administration
Research Review of Potential Safety Effects of Electronic Billboards on Driver
Attention and Distraction (2001)**²⁴

The Federal Highway Administration published a comprehensive report in 2001 that consisted of a literature search, literature review and a description of research needs for

the topic of electronic billboards (EBBs). While the study did not conduct any new research, it does provide an excellent summary of the role electronic billboards play in traffic safety and includes good descriptions of the terminology related to electronic billboards. Selected findings from that synthesis are provided below:

“In most instances, researchers were not able to verify that an EBB was a major factor in causing a crash. Only one study since the 1980 review and one lawsuit were identified.”

“Studies were identified that verified that: an increase in distraction, a decrease in conspicuity, or a decrease in legibility may cause an increase in the crash rate.”

“Commercial EBBs are designed to ‘catch the eye’ of drivers. Their presence may distract drivers from concentrating on the driving task and visual surrounds.”

“There is indication that individual differences in age and driving experience may be important considerations in driver distraction, and are relevant to understanding driver responses to the external environment. Furthermore, research regarding driver familiarity of their route demonstrated that visual fixations on roadway signs decreases as route familiarity increases. This research may show that there is a difference between commuter and visiting drivers.”

Based on these findings, the FHWA recommended additional research to further demonstrate how roadway characteristics, sign characteristics and legibility, driver characteristics and other potential driver distractions affect traffic safety. FHWA was contacted to see if any new information was available. Greg Davis, a Research Psychologist with the FHWA Office of Safety R&D, indicated that the FHWA has not performed additional studies on the topic since the report was published. He stated that there is “no direct correlation between electronic outdoor advertising signs and crash rates”. He referred to a before/after study of electronic signs installed along a freeway in Las Vegas that found no change in crash rates. He went on to say that the lack of a research finding that links signs with crash rates does not mean that a causal relationship does not exist. He indicated that he has been contacted by several law enforcement agencies regarding the link between driver distraction and dynamic message signs/electronic billboards. He indicated that this is a timely and pertinent topic for many states due to the increasing popularity and capabilities of electronic outdoor advertising devices, and he expects further research to be forthcoming. He advocates for a new study that can control for all variables and determine if a cause and effect relationship exists.²⁵

3.5 How Does “Brightness” Affect Driver Safety Concerns?

The brightness of any sign, static or dynamic, raises concerns with discomfort or disability glare to the driver that may arise when viewing any lighted object. *Disability Glare* occurs when a

driver is exposed to a light source so bright that it temporarily blinds the driver, impairing their ability to perform driving tasks. This temporary blindness is brief, but can be dangerous. *Discomfort Glare* occurs when a light source is bright enough to distract or encourage the driver to look away from the light, but is not blinding. Discomfort glare is of particular concern in cases where a bright sign is located in the same line of sight as a traffic sign, signal or another vehicle.

While concerns about glare are not unique to dynamic signs, newer sign technologies, which often include dynamic components, have the technical capability to emit more light and/or respond to ambient light conditions, raising additional concerns about sign brightness in areas where signs compete with regulatory traffic signs or signals.

3.6 Billboards and Other Signage Regulation: a Minnesota Perspective

Roadside signage is governed by policies and laws at the federal, state and local levels. Minnesota Statute, Chapter 173 seeks to “reasonably and effectively regulate and control the erection or maintenance of advertising devices on land adjacent to such highways.” The statute requires adherence to federal statutes with respect to interstate and primary systems of highways.

Minnesota Statute Ch. 173.16 Subd. 3. regulates lighting of signs. Signs which are “illuminated by any flashing light or lights, except those giving public service information” (time, date, temperature, weather or news) are prohibited. This section also states:

(b) Advertising devices shall not be erected or maintained which are not effectively shielded so as to prevent beams or rays of light from being directed at any portion of the traveled way of an interstate or primary highway, of such intensity or brilliance as to cause glare or impair the vision of the operator of any motor vehicle; or which otherwise interfere with any driver’s operation of a motor vehicle are prohibited.

and

(c) Outdoor advertising devices shall not be erected or maintained which shall be so illuminated that they interfere with the effectiveness of or obscure any official traffic sign, device or signal.

3.7 Billboard and Other Signage Regulation: Other Perspectives

During the course of this study, several articles were found which summarize regulation of dynamic signage in other states:

Wisconsin Department of Transportation
Electronic Billboards and Highway Safety (2003) ²⁶

The Wisconsin Department of Transportation also published a literature review report to further explain the current state of EBB research. Although much of the information is

mentioned in other sections of this report, the Wisconsin review did summarize Wisconsin's regulations for electronic billboards.

- No message may be displayed for less than one-half second;
- No message may be repeated at intervals of less than two seconds;
- No segmented message may last longer than 10 seconds;
- No traveling message may travel at a rate slower than 16 light columns per second or faster than 32 columns per second (light column defined as pixel column);
- No variable message sign lamp may be illuminated to a degree of brightness that is greater than necessary for adequate visibility.

National Alliance of Highway Beautification Agencies (1999) ²⁷

Although this survey is eight years old, it generated the following information related to electronic billboards:

- Nine states had specific regulations governing signs,
- Nine states had regulations on tri-vision signs that were either being drafted or in pending legislation,
- Fifteen states had regulations regarding moving parts and/or lights,
- Nine state had no regulations on tri-vision signs, and
- Six states and Washington, DC, prohibited tri-vision signs.

An investigation into state outdoor advertising regulations was also conducted.

- Thirty-six states had prohibitions on signs with red, flashing, intermittent, or moving lights,
- Twenty-nine states prohibited signs that were so illuminated as to obscure or interfere with traffic control devices, and
- Twenty-nine states prohibited signs located on interstate or primary highway outside of the zoning authority of incorporated cities within 500 ft of an interchange or intersection at grade or safety roadside area.

Parliament of Victoria, Australia, Report of the Road Safety Committee on the Inquiry into Driver Distraction (2006) ²⁸

This report, cited earlier for its driver distraction opinions, identifies road signs and advertising as one of the largest sources of driver distraction. VicRoads, the state's road and traffic authority, has implemented the following regulations.

Figure 1. VicRoads' Ten Point Road Safety Checklist

An advertisement, or any structure, device or hoarding for the exhibition of an advertisement, is considered to be a road safety hazard if it:

1. obstructs a driver's line of sight at an intersection, curve or point of egress from an adjacent property; or
2. obstructs a driver's view of a traffic control device, or is likely to create a confusing or dominating background which might reduce the clarity or effectiveness of a traffic control device; or
3. could dazzle or distract drivers due to its size, design or colouring, or it being illuminated, reflective, animated or flashing; or
4. is at a location where particular concentration is required (eg. high pedestrian volume intersection); or
5. is likely to be mistaken for a traffic control device, for example, because it contains red, green or yellow lighting, or has red circles, octagons, crosses or triangles, or arrows; or
6. requires close study from a moving or stationary vehicle in a location where the vehicle would be unprotected from passing traffic; or
7. invites drivers to turn where there is fast moving traffic or the sign is so close to the turning point that there is no time to signal and turn safely; or
8. is within 100 metres of a rural railway crossing; or
9. has insufficient clearance from vehicles on the carriageway;
or
10. could mislead drivers or be mistaken as an instruction to drivers.

VicRoads also gives operational requirements for electronic advertising message signs. Signage must:

- not display animated or moving images, or flashing or intermittent lights;
- remain unchanged for a minimum of 30 seconds;
- not be visible from a freeway; and
- satisfy the ten-point checklist.

4.0 SUGGESTED REGULATORY APPROACH

Local governments regulate electronic outdoor advertising devices in widely varying degrees. Some cities completely prohibit the use of all electronic signs (sometimes specifying LED signs), while others have no regulations specific to electronic signs. Between those two extremes, there are many levels and types of control that can be applied.

The primary concerns to keep in mind when considering sign regulations are 1) First Amendment rights, which can be affected by regulations that affect the content of a sign's message, and therefore should be avoided, and 2) changing technology, which can quickly make a sign ordinance no longer applicable if the ordinance has been specifically written to address a certain type of sign technology. Performance based measures may therefore be preferable as they remain viable even as sign technology advances.

4.1 Definitions

Signage discussions often include a number of different words or phrases used to describe the technical characteristics of signage devices or their components (such as LEDs). For the purpose of zoning, some additional terms are also used to describe sign characteristics. Any regulatory efforts should take care to precisely define terminology. One possible resource in this effort is "Street Graphics and the Law," published by the American Planning Association (APA) Planning Advisory Service²⁹.

4.2 Types of Regulatory Measures

4.2.1 Complete or Partial Prohibition of Electronic Signs

Some cities have completely prohibited the use of electronic outdoor advertising devices. For example, the City of Maple Valley, WA prohibits all types of electronic outdoor advertising devices including animated signs, electronic changeable message signs, flashing signs or displays, moving signs, scrolling displays, and traveling displays. This applies to both on-premise and off-premise signs.

Other cities are very selective about where electronic signs are allowed, allowing them only in certain zoning districts. There are very few "standard" approaches. For the most part, each local

government tailors their regulations to their own situation. One approach adopted by cities is to prohibit electronic outdoor advertising devices in residential zoning districts, and for a certain distance away from residential zoning districts, similar to the zoning limitations placed on illuminated signs. Some ordinances require that electronic signs be situated such that the sign face is not visible from nearby residences.

4.2.2 Size Limitations on Electronic Signs

Another way of regulating electronic signs is to limit their size. Again, there is no set standard for this. One ordinance reviewed for the purpose of this study limits the electronic portion of a sign to no more than 50 percent of the sign face with the overall size determined by whatever the sign ordinance allows for a particular zoning district. Other examples of electronic sign size limitations include five square feet, 1,000 square inches, 20 square feet, and so forth. In other ordinances, there is no differentiation made between the size of electronic signs and other signs.

According to input from representatives of the sign industry, the smaller the size of the electronic sign, the more desirable it is for businesses to use frequent message changes, or sequenced messages, where more than one screen of text is used to convey an entire message.

4.2.3 Rate-of-Change Limitations on Electronic Signs

Many communities that allow electronic signs also regulate the rate at which the messages on the signs can be changed. Research on sign codes has shown this to range from as little as four seconds to as long as 24 hours.

The Interstate 394 sign between Ridgedale Drive and Plymouth Road is visible for approximately 45 seconds at free flow traffic speeds. Depending on text size, the message may not be readable by drivers during this entire duration, but the message changes can attract attention from long distances. Depending on how often the message changes occur and the speed of traffic, drivers on this segment could see a varying number of discrete messages. Table 7 provides the number of message changes a driver would see at different change durations and traffic speeds.

Table 7. Number of New Messages Seen at Various Driver Speeds and Time Intervals Between Messages

Speed (mph)	Time sign is clearly visible* (seconds)	Number of Messages Seen					
		Message Display Time (seconds)					
		6	8	10	60	1800 (30 minutes)	3600 (1 hour)
30	60	11	9	7	2	1	1
45	40	8	6	5	2	1	1
55	33	7	5	4	2	1	1

*Assuming the sign is clearly visible from one-half mile away.

Prohibiting displays from changing quickly can minimize potential driver distraction, but it would significantly limit the message owner’s ability to convey information that does not fit on one screen of the sign. Using two or more successive screens to convey a message is referred to as sequencing. Based on the studies summarized in part 3 of this Report, including the glance duration studies performed by Klaur for the FHWA in 2006 and by Beijer & Smiley in 2004, and Wachtel’s analysis for Seattle of the Zeigarnik effect, a message delivery system such as sequencing that requires or induces a driver to watch the sign for several seconds increases the likelihood of driver distraction. Based on information from the sign industry, for sequencing to be effective in a marketing sense, a brief rate-of-change (1-2 seconds) is generally used before transitioning into the next screen.

Some codes specify how an image changes, while other codes prohibit the use of transitions. The change from one image to another can be accomplished by various techniques: no transition – simply a change from one screen to another, or fading or dissolving one image into the next. Flashing, spinning, revolving, or other more distracting transition methods can be prohibited, allowing businesses to use sequencing in an effective manner without making the signs overly distracting. Another way of regulating distracting transitions is to require a very short time of a dark or empty screen between images.

4.2.4 Motion, Animation, or Video Limitations on Electronic Signs

Motion on a sign can consist of everything from special text effects (spinning, revolving, shaking, flashing, etc.) to simple graphics, such as balloons or bubbles rising across the screen, to more realistic moving images that have the appearance of a television screen. According to sign industry representatives, video imagery on a sign is referred to as “animation” if the sign is limited to the capability of 10 frames per second. Fewer frames per second make the moving image look more like animation. Imagery produced by signs that have the capability of processing up to 30 frames per second is accurately referred to as “video” imaging.

Many communities that allow dynamic signs do not allow the application of any type of motion, animation, or video on the signs. However, Seattle was obliged to allow video imagery on their signs after earlier signage code regulating certain types of signs was not strictly enforced. In addition to requiring a dark period between successive messages to overcome the Zeigarnik effect, Seattle also limits the duration of the video message to a minimum of two seconds and a

maximum of 10 seconds. This time frame was established based upon careful calculations of the streets from which these signs could be seen, speed limits and traffic volumes in addition to the community's concern over the extent to which moving images could distract drivers. However, Seattle also limits the size of their electronic signs to a maximum of 1,000 square inches, with no single dimension greater than three feet, thus minimizing the effect of video images.

4.2.5 Sign Placement and Spacing

Regulating the number of dynamic sign potentially visible to a driver at any one time as well as the position of the sign in relationship to the roadway may reduce distraction to drivers. Spacing requirements should consider the speed, width and horizontal and vertical alignment of the roadway.

Some communities have established minimum distances between electronic signs. Establishing an adequate distance between these types of devices seems particularly important if a fairly fast rate of change is allowed for the purpose of facilitating sequenced messages or if animation and video imaging is allowed. Closely spaced signs attempting to convey sequenced messages may simply create visual overload and an over-stimulated driving environment. Research conducted to date has not yielded information about optimal electronic sign spacing. Seattle adopted a 35-foot spacing requirement for their electronic signs based upon multiple levels of analysis of the downtown city environment in which these signs are present.

Due to the varying characteristics of individual roadways in this regard, overlay districts allowing dynamic signage with conditions specific to that area could be considered. Overlay districts could also take into account other locational factors such as offset from the roadway and conspicuity. Determining appropriate offsets from the roadway must consider roadway clear zone requirements as well as spacing of frontage roads and access points, while also considering the signage too far outside the driver's line of sight may be a further distraction. Conspicuity, a sign's ability to stand out from its surroundings, should also be considered.

4.2.6 Text Size

Legibility is another important property of signage. The preferred approach used within highway signing is that drivers can read text that is 1 inch high from 30 feet away. Larger text is needed for signs to be legible at greater distances. Large, legible text allows the driver to read the billboard from varying distances and focus on the driving task. Conversely, with small text, the driver is more likely to focus on the sign for a longer period of time and possibly be more adversely distracted. However, the size or type of text or the amount of text due is rarely regulated.

4.2.7 Brightness Limitations on Electronic Signs

One of the main concerns about the use of electronic signs, regardless of whether they consist of changeable text, animation, or video, is the brightness of the image. The brightness of an object can be characterized in two ways. *Illuminance* is the total brightness of all the light at a point of measurement. Illuminance often describes ambient light and can be measured with a standard light meter such as is used in photography. *Luminance* is the measure of the light emanating from an object with respect to its size and is the term is used to quantify electronic sign brightness. The unit of measurement for luminance is nits, which is the total amount of light emitted from a sign divided by the surface area of the sign (candelas per square meter).

Many, but not all, LED-type signage can be time-programmed to respond to day and nighttime light levels. Higher-end signage types are equipped with photo cells to respond to ambient light conditions. Despite these controls, LED signs have been observed that are considered to be excessively bright. Sign industry representatives indicate that excessive brightness can be the result of 1) sign malfunction or improper wiring, 2) lack of photo cell and/or dimming mechanism, or 3) operator error or lack of understanding that brightness is not necessarily an advantage, especially if it makes a sign unreadable or unpleasant to look at. They also maintain that the intent of the electronic sign industry is to establish a brightness level that is similar to a traditional internally or externally lit sign. Recent observations of sign technicians calibrating the Interstate 394 LED billboard noted that the brightness controls are not calibrated to specific nit levels, but rather vary in proportion to a set maximum level, like a volume control dial on a typical car radio.

To control the extent to which electronic signs are a distraction or the extent to which they are readable, many local governments have adopted regulations that limit nit levels. At this time, ordinances that use nit level limitations typically differentiate between day time and night time nit levels. A common daytime nit limitation ranges from 5,000 to 7,000 nits. A common nighttime limitation is 500 nits, although in areas that are extremely dark at night, with very little in the way of ambient light levels, less than 500 nits may be appropriate. Other communities have taken this farther, such as Lincoln, Nebraska, whose sign code incorporates a graph of varying ambient light levels ranging from night time to a bright sunny day and all conditions between those two extremes, and has correlating nit limitations for the various ambient light levels.

Enforcement of these types of regulations is challenging as luminance of electronic signs is very difficult to measure in the field. Typically, sign luminance is measured and calibrated in a controlled factory setting using a spectral photometer to measure the light output. This calibration setting is then used in conjunction with a photo cell to control the brightness of the sign. The higher the ambient light levels, the brighter the sign. There are different nit thresholds for various colors. White is most often used to set dimming levels because at a constant nit level, white has the most intensity as perceived by the human eye.

Lincoln uses a light meter to conduct testing on electronic signs and found a wide range of luminance levels. One small electronic sign had luminance levels of 13,000 nits. The process that Lincoln uses to check luminance levels is to hold a luminance meter close to the face of the sign so that it captures only the light emitted from the sign. They have not had any requests to

measure the brightness of LED billboards, so the viability of using this approach on billboards has not been explored.

In Seattle, sign luminance was found too difficult to measure, so signs are visually inspected when complaints from the public are received. Sign owners are then contacted and asked to adjust sign luminance accordingly.

Both Mesa, Arizona and Lincoln, Nebraska have included a requirement for written certification from the sign manufacturer that the light intensity has been preset not to exceed the illumination levels established by their code, and the preset intensity level is protected from end user manipulation by password protected software or other method approved by the appropriate city official. This language appears to offer the advantage of ensuring that electronic signs, at a minimum, cannot exceed a certain established level of brightness.

At a minimum, it is important for communities to require all electronic signs to be equipped with a dimmer control. A requirement for both a dimmer control and a photo cell, which constantly keeps track of ambient light conditions and adjusts sign brightness accordingly, is optimal.

Over time, the LEDs used in electronic signs have a tendency to lose some of their intensity, and an owner may choose to have the sign adjusted and calibrated, which involves adjusting the level of electrical current in a manner that affects the brightness of the sign. This occurs over the course of two or three years. Having maximum nit levels established would ensure that the sign company has upper limits to work with as far as adjusting the sign is concerned.

4.3 Public Review

Most communities establish rules within their sign code and do not create opportunities for electronic signs to be approved through conditional use permits or special use permits. Some communities with special overlay districts, or areas that are oriented toward entertainment and night life, have established a review process for electronic signs, or for various functions of electronic signs such as animation and video.

Other communities take the opposite approach, where they allow electronic signs with no controls whatsoever, except in certain special areas, such as a historic overlay district, or a historic downtown district, where the signs are prohibited. Each community needs to tailor their application of electronic signs to meet their needs.

As of the writing of this report, no ordinances have been discovered that have a special review committee just for the purpose of electronic signs. Typically, sign regulations established in the zoning ordinance would be reviewed in accordance with existing review and approval processes. As with other development features, dynamic signage should be either prohibited, permitted, or conditional depending upon the zoning district and/or the specific features of the sign as established within the city's regulations (i.e. size, specific location with respect to the adjacent roadway, zoning district, proximity of sensitive uses). The recommended review process for permitted dynamic signs should be the same as procedures already in place for administrative

review. For dynamic signs requiring a Conditional Use Permit (CUP), the standard process for public notification and a public hearing before the planning commission should apply.

5.0 CONCLUSIONS AND RECOMMENDATIONS

Driver distraction plays a significant role in traffic safety. Driver distraction is a factor in one in four crashes, and of those crashes involving driver distraction, one in four involves distractions outside the vehicle. The extent to which dynamic signage contributes to traffic safety has been examined in this study. Following are some of the major findings from a review of available research.

- Drivers that are subjected to information-rich content that is irrelevant to the driving task (such as digital advertising) may be temporarily distracted enough to cause a degradation in their driving performance. This degradation could lead to a crash.
- The unlimited variety of changing content allows dynamic signage to attract drivers' attention at greater distances and hold their attention longer than traditional static billboards.
- Several studies have found a correlation between crashes and the complexity of the driving environment. For example, crash rates are higher at intersections because the difficulty of the driving task is increased by the roadway's complexity. Complex driving environments place a high demand on drivers' attention. Introducing a source of distraction in an already demanding driving environment is more likely to result in crashes. This is illustrated by the 1994 Wisconsin DOT study that examined crash rates before and after installation of an electronic sign on a high-volume curving roadway. Introduction of this sign was identified as a likely factor of the 80 percent increase in side-swipe crashes that was experienced.
- Many studies have noted a correlation between outdoor advertising signs and crash rates, but have not established a *causal* relationship between the signs and crash rates. Driving is a complex task influenced by multiple factors. It is not necessary to establish a direct causal relationship between outdoor advertising signs and crash rates to show that they can make the driving task less safe. While the research shows that driver distraction is a key factor in many motor vehicle crashes, this often includes many interacting factors that distract drivers. The specific driver distraction danger that advertising signs contribute is difficult to quantify. A study that could control for multiple variables (human factors, vehicle, enforcement and the roadway environment) would be needed to provide a definitive statement on the level of driver distraction that signs produce. Such a study would likely find that not all advertising signs cause distraction that would lead to crashes, but some signs in some situations are more likely to contribute to crashes than others.

Overall, the literature review conducted for the purpose of this study identifies a relationship between driver distraction and electronic outdoor advertising devices. As indicated, driver distraction is a significant factor in crashes. The purpose of dynamic signage is to attract the attention of people in vehicles, so a natural conclusion from that knowledge is that drivers may be distracted by them. Professional traffic engineering judgment concludes that driver distraction generally contributes to a reduction in safe driving characteristics.

For this reason, state departments of transportation have carefully studied the design and location of dynamic signs within the highway right-of-way. Their goal is to convey a message to the traveling public in a manner that is as straight-forward and readable as possible without being a visual “attraction”. The goal of the outdoor advertising sign is to be a visual attraction outside the right-of-way, possibly making it a source of driver distraction. Nevertheless, the actual change in crash rates influenced by the presence of any specific device has not been quantified in a manner that fully isolates the impacts of an electronic sign. Recent studies conducted by FHWA and others have cited the need for further research.

In the interest of promoting public safety, this report recommends that electronic signs be viewed as a form of driver distraction and a public safety issue. Therefore, the ordinance recommendations identified here should be considered. These recommendations should be reviewed in the future as additional research becomes available.

With respect to regulatory measures for electronic outdoor advertising signs, it is important that local governments take a thorough approach to updating their ordinances to address this issue. For example, an ordinance that addresses sign motion, but does not address brightness and intensity levels may leave the door open for further controversy. This report seeks to identify all of the aspects of electronic outdoor advertising devices that are subject to regulation. It does not specifically state what those regulations should be (e.g. the size of electronic signs), since these are all things that policy makers and staff must take into careful consideration. Further, as driver distraction and resulting influences on safety do not, in a practical sense, distinguish between on-premise and off-premise signage, this distinction is not highlighted in the recommendations below.

Regulatory Measures recommended for consideration

To properly address the issue of dynamic signage, it is recommended that the sign code address the following:

1. Identify specific areas where dynamic signs are prohibited. This would typically be done by specifying certain zoning districts where they are not allowed under any circumstances. If dynamic signs are to be allowed in specific areas, this could be done by zoning district (only higher level commercial districts are recommended for consideration) or by zoning overlay related to specific purposes (e.g. entertainment or sports facility district) or to specific roadway types.
2. Determine the acceptable level of operational modes in conjunction with such zoning districts or overlays. The various levels include:
 - a. Static display only, with no transitions between messages,
 - b. Static display with fade or dissolve transitions, or transitions that do not have the effect of moving text or images,
 - c. Static display with scrolling, traveling, spinning, zooming in, or similar special effects that have the appearance of movement, animation, or changing in size, or get revealed sequentially rather than all at once (e.g. letters dropping into place, etc.), and

d. Full animation and video.

3. If one of the forms of static display is identified as the preferred operational mode, a minimum display time should be established. This display time should correspond to the operation roadway speed (rather than posted speed limit), allowing at most one image transition during the time that the sign is visible to a driver traveling at the operational speed.

If a shorter minimum display time is considered, the effects of message sequencing should be considered. Wait intervals of more than 1-2 seconds between sequenced messages have the potential to become more of a distraction as viewers wait impatiently for the next screen, in an effort to view the complete message.

4. If the community wishes to accommodate animation or video in some or all locations where dynamic are permitted, a minimum and maximum duration of a video image should be established. The purpose for establishing a time limit is to ensure that the message is conveyed in a short, concise time frame that does not cause slowing of traffic to allow drivers to see the entire message. Given the creativity of advertising, these video images may be seen as a form of entertainment, and people typically like to see an entertaining message through to the end.

Differentiate between zoning districts where dynamic signs are permitted by right, and zoning districts, overlay districts, or special districts where they should only be allowed through the approval of a Conditional Use Permit. A CUP would involve public notification and review and approval by the Planning Commission. Other options would include a design review board or other dispute resolution process.

5. Consider the establishment of minimum distance requirements between electronic outdoor advertising devices in relation to the zoning district or roadway context in which the signs are allowed.
6. Consider size limitations on dynamic signs for zoning districts where they are allowed. This may vary from one district to another.
7. Consider if dynamic signs are allowed independently, or if they must be incorporated into the body of another sign, and therefore become a limited percentage of the overall sign face.
8. Establish a requirement for that all dynamic signs that emit light be equipped with mechanisms that allow brightness to be set at specific nit levels and respond accurately to changing light conditions. The City must establish the authority to disable or turn the device off if it malfunctions in a manner that creates excessive glare or intensity that causes visual interference or blind spots, and require that the device remain inoperable until such time that the owner demonstrates to the appropriate city official that the device is in satisfactory working condition. If such technology is not available, consideration should be given to banning dynamic signs that emit light until such time as the technology allows brightness levels to be precisely controlled.

9. Consider maximum brightness levels that correlate to ambient (day or night condition, lighting of surrounding context) light levels. A maximum daytime and separate nighttime nit/footcandle level should be established. Consider wording that requires the sign to automatically adjust its nit level based on ambient light conditions.
10. Consider a requirement for a written certification from the sign manufacturer that the individual sign's maximum light intensity has been preset not to exceed the maximum daytime illumination levels established by the code, and that the maximum intensity level is protected from end user manipulation by password protected software or other method approved by the appropriate city official.
11. Require sign owners to provide an accurate field method of ensuring that maximum light levels are not exceeded. If such a method cannot technically be provided, consider banning dynamic signs that emit light until such time as the technology is available.

APPENDICES

Appendix A

Current Sign Technologies

Appendix A – Current Sign Technologies

Roadside signage has long been used to alert and direct travelers to retail businesses, lodging, attractions and other destinations. Until the 20th century much of this image was “static” in nature, presenting a single image that could only be altered by repainting or otherwise removing an image and replacing it with another. With the advent of motorized travel, signage became more “dynamic” or active in its efforts to attract the traveler’s attention as they moved at ever increasing speeds. Initially, motion was created by flashing bulbs or alternating sets of neon tubes.

Today’s technologies allow for an increasingly sophisticated display of images that can be manipulated by a few strokes of a keyboard. Simpler forms of signs capable of displaying multiple images include “tri-vision” signs which present a series of images through mechanical rotation of multi-sided vertical strips. The rotation occurs at regular intervals presenting a series of static images. Other forms are electronically produced, allowing for a wide range of colors, messages and images depending on the level of technology, and typically produced by light emitted by the sign face. Basic levels of technology present letters or numbers in a single color of light, such as “time and temperature” signs or gas pricing signs. Many of these signs can present longer images in a scrolling fashion, or can provide simple animations.

Recent advances have introduced a variety of technologies to the outdoor advertising arena. The largest impact has been made with LED signs which offer an inexpensive yet powerful approach that combines full motion, brilliant colors and a readable display. Other technologies are in development, including “digital ink” signs that offer a changeable medium on a surface that looks like a normal vinyl billboard. These signs manipulate ink on the surface, allowing for a dynamic presentation of images without being internally illuminated.

The various sign technologies are referenced by a wide array of terms: “changeable message signs,” “electronic billboards,” “animated signs.” In general, this report focuses on the broad range of signage types which are capable of displaying multiple images through electronic manipulation, which we will refer to as “dynamic” signing. Reference to specific signage types is made when necessary to discussion of specific issues (e.g. the brightness of LED signage).

Appendix B

Outdoor Advertising Sign Brightness Definitions

Appendix B – Outdoor Advertising Sign Brightness Definitions

This appendix defines various technical terms that are used to describe the operational aspects of electronic billboards.

Billboard Illuminance

Billboard illumination is typically discussed using two terms: illuminance and luminance. Because this section includes some technical jargon, a glossary that further defines terms used in outdoor advertising is provided in Appendix C.

Illuminance: The amount of light that is incident to the surface of an object. This is the method for describing ambient light levels or the amount of light that is projected onto a front-lit sign. This parameter is typically measured in lux (footcandles x meters). For the purposes of dimming, illuminance is discussed to describe the ambient light that hits the photocell.

Luminance: The amount of light that emanates from an internally illuminated sign. This parameter is measured in nits. The nit levels necessary for the sign to be legible vary with the ambient light conditions. On a sunny day, the nit levels must be very high, while at night, the levels must be very low to prevent the image from distorting and to prevent glare.

Billboard Luminance (Brightness)

Luminance is measured in nits (candelas/square meter) and describes how bright the image is. In essence, it is the amount of light that is radiated from the sign divided by the amount of surface area of the sign. No matter how big the sign is, the luminance of the sign is consistent. For example, the brightness of computer monitors is also measured in nits.

The European standard “EN 12966” specifies that at certain ambient light levels, the sign should output a given number of nits. There are different tables for each color due to the properties of how the human eye interprets each color. The color that is most often used to set dimming levels is white.

The FHWA has developed recommended practices for dynamic message signs installed within the roadway right-of-way. The standard is NEMA’s TS-4 “Hardware Standards for Dynamic Message Signs (DMS) With NTCIP Requirements.” Note that these standards were prepared for message signs deployed within the roadway right-of-way and should not be taken as recommended luminance levels for advertising signs. Table A-1 provides a simplified version of the NEMA TS-4 standard for the color white.

Table A-1 - Luminance Standards

Ambient Light (lux)	Approximate Light	Minimum Luminance (nits)	Maximum Luminance (nits)
40,000	Sunlight	12,400	62,000

10,000	Cloudy	12,400	-
4,000	Overcast	2,200	11,000
400	Sunrise/Sunset	600	3,000
40	Candlelight	250	1,250
less than 4	Moonlight	75	375

Source: NEMA TS-4 (2005)

Billboard Resolution

Billboards require far less resolution than print advertisements. For example, Clear Channel’s LED “Digital Outdoor Network” LED bulletin-size (14’ x 48’) billboards require dimensions of only 208 pixels high by 720 pixels wide. If this image were to be printed at 300 dots per inch (dpi), a typical print resolution, the entire image would be less than 1.7 square inches. Therefore, it is ideal to keep the message on these signs simple and clear because they do not currently allow resolutions similar to printed images.

Dimming

To maintain readability, the brightness of a sign must be adjusted to match ambient light conditions. If this is not done, the image will appear too bright and can even degrade the image quality through a phenomenon called “blooming.” If the image blooms, the brightest areas of the image bleed over into darker parts and the image clarity is degraded.

Dimming is typically controlled by a photocell, which measures the ambient light conditions and varies the light output of the sign based on preconfigured settings. As ambient light conditions darken, the photocell senses the decrease and lowers the light output of the sign. Some sign manufacturers do not incorporate photocells in their electronic signs.

Electronic billboard dimming can also be controlled by scheduled dimming according to time of day or manual dimming. On-premise signs may use any of these methods, but most, if not all, off-premise standard size electronic billboards are auto dimmed by photocell. Some signs include user-defined dimming curve capability allowing total control over sign brightness and adjustability to accommodate local brightness ordinances.

Appendix C

Electronic Outdoor Advertising Device Visual Performance Definitions

Appendix C – Electronic Outdoor Advertising Device Visual Performance Definitions

Conspicuity

Conspicuity is the property that related to the contrast between a sign and its background and its ability to stand out from its surroundings. This is a subjective property that depends on many factors of both the environment and the viewer.

Contrast

Contrast is the property that defines the relationship between the brightness of the brightest color possible to the darkest color possible on a sign. In times when ambient conditions are very bright, such as a sunny day, the darkest color may still be very bright due to the sun's reflection off the sign. In these cases, the lighter colored areas of the billboard's image must be much brighter than the contrasting dark areas.

Legibility

The ability of the driver to read a sign is related to its legibility. Large, legible text allows the driver to read the billboard from varying distances and focus on the driving task. Conversely, with small text the driver is more likely to focus on the sign for a longer period of time and possibly wait until the sign is very close.

State departments of transportation use NEMA's TS-4 document for this criterion. This document specifies many characteristics related to legibility including character height, resolution and color.

Glare

Disability Glare

The first form of glare is disability glare. This occurs when a driver is exposed to a light source so bright that it temporarily blinds the driver, impairing their ability to perform driving tasks. This temporary blindness is brief, but can be dangerous.

Discomfort Glare

Discomfort glare is when a light source is bright enough to distract or encourage the driver to look away from the light, but is not blinding. Discomfort glare is of particular concern in cases where a bright sign is located in the same line of sight as a traffic sign, signal or another vehicle.

Frequency of Change

The frequency of change is determined by the interval of time between sign image changes. The rate of change can usually be adjusted by the owner and operator of the sign. Frequency

of change is highly variable, with some on-premise signs changing faster than once per second. While no standard is generally accepted, local government agencies have used ordinances to limit the frequency to anywhere from 5 seconds to 24 hours.

Interactive signs

Interactive signs change their message based on the person viewing it. For example, the carmaker MINI has installed variable message signs that display a customized message to car owners who have special key dongles containing a radio frequency identification (RFID) chips when the dongle is in close proximity to the sign.

Another example is a microphone system that identifies the radio stations passing drivers are listening to and displays a specific message for that station.

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MEMORANDUM

TO: Tom Grundhoefer
League of Minnesota Cities

FROM: Karen Sprattler, Senior Associate 
SRF Consulting Group, Inc.

DATE: June 21, 2007

SUBJECT: DYNAMIC" SIGNAGE: RESEARCH RELATED TO DRIVER DISTRACTION AND
ORDINANCE RECOMMENDATIONS REPORT

This study was originally commissioned in response to litigation brought by Clear Channel Communications, Inc. in response to actions taken by the City of Minnetonka, Minnesota in regard to the installation of two LED ("light emitting diode") billboards along Interstate 394 and Interstate 494. This study was undertaken to examine issues surrounding the Minnetonka billboards. While the concerns were precipitated by LED billboards in particular, this report examines more broadly "dynamic" display signage. However, this report is not the intended to be a comprehensive study of all issues raised by dynamic signage or other types of billboards,

As the study progressed, additional communities and the League of Minnesota Cities expressed interest in these issues. While it is true that the study was prepared for the City of Minnetonka, it is acknowledged that the many of the findings and conclusions, and the broader discussion of many of the issues of concern may be useful to other communities involved in similar situations.

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Annotated Sample Sign Ordinance- City of Hopkins

Comment: The following regulations reflect the policy choices of the City of Hopkins. Different communities may make different policy decisions.

SECTION I – PURPOSE AND DEFINITIONS

570.01. Findings, purpose and effect.

a. Findings. The city council hereby finds as follows:

1. Exterior signs have a substantial impact on the character and quality of the environment.
2. Signs provide an important medium through which individuals may convey a variety of messages.
3. Signs can create traffic hazards, aesthetic concerns and detriments to property values, thereby threatening the public health, safety and welfare.
4. The city's zoning regulations have, since as early as 1966, included the regulation of signs in an effort to provide adequate means of expression and to promote the economic viability of the business community, while protecting the city and its citizens from a proliferation of signs of a type, size, location and character that would adversely impact upon the aesthetics of the community and threaten the health, safety and welfare of the community. The regulation of the physical characteristics of signs within the city has had a positive impact on traffic safety and the appearance of the community.

b. Purpose and intent. It is not the purpose or intent of this sign ordinance to regulate the message displayed on any sign; nor is it the purpose or intent of this article to regulate any building design or any display not defined as a sign, or any sign which cannot be viewed from outside a building. The purpose and intent of this article is to:

1. Regulate the number, location, size, type, illumination and other physical characteristics of signs within the city in order to promote the public health, safety and welfare.
2. Maintain, enhance and improve the aesthetic environment of the city by preventing visual clutter that is harmful to the appearance of the community.
3. Improve the visual appearance of the city while providing for effective means of communication, consistent with constitutional guarantees and the city's goals of public safety and aesthetics.

4. Provide for fair and consistent enforcement of the sign regulations set for herein under the zoning authority of the city.

c. Effect. A sign may be erected, mounted, displayed or maintained in the city if it is in conformance with the provisions of these regulations. The effect of this sign ordinance, as more specifically set forth herein, is to:

1. Allow a wide variety of sign types in commercial zones, and a more limited variety of signs in other zones, subject to the standards set forth in this sign ordinance.

2. Allow certain small, unobtrusive signs incidental to the principal use of a site in all zones when in compliance with the requirements of this sign ordinance.

3. Prohibit signs whose location, size, type, illumination or other physical characteristics negatively affect the environment and where the communication can be accomplished by means having a lesser impact on the environment and the public health, safety and welfare.

4. Provide for the enforcement of the provisions of this sign ordinance.

570.03 Severability

If any section, subsection, sentence, clause, or phrase of this Sign Ordinance is for any reason held to be invalid, such decision shall not affect the validity of the remaining portions of this Sign Ordinance. The City Council hereby declares that it would have adopted the Sign Ordinance in each section, subsection, sentence, or phrase thereof, irrespective of the fact that any one or more sections, subsections, sentences, clauses, or phrases be declared invalid.

570.05 Definitions.

The following words and terms, when used in this Sign Ordinance, shall have the following meanings, unless the context clearly indicates otherwise:

Abandoned sign - any sign and/or its supporting sign structure which remains without a message or whose display surface remains blank for a period of one (1) year or more, or any sign which pertains to a time, event or purpose which no longer applies, shall be deemed to have been abandoned. Signs applicable to a business temporarily suspended because of a change in ownership or management of such business shall not be deemed abandoned unless the property remains vacant for a period of one (1) year or more. Any sign remaining after demolition of a principal structure shall be deemed to be abandoned. Signs which are present because of being legally established nonconforming signs or signs which have required a conditional use permit or a variance shall also be subject to the definition of abandoned sign.

Awning - a roof-like cover, often of fabric, plastic, metal or glass designed and intended for protection from the weather or as a decorative embellishment, and which projects from a wall or

roof of a structure primarily over a window, walk, or the like. Any part of an awning which also projects over a door shall be counted as an awning.

Awning sign - a building sign or graphic printed on or in some fashion attached directly to the awning material.

Balloon sign - a sign consisting of a bag made of lightweight material supported by helium, hot, or pressurized air which is greater than twenty-four (24) inches in diameter.

Building - any structure used or intended for supporting or sheltering any use or occupancy.

Building sign - any sign attached or supported by any structure used or intended for supporting or sheltering any use or occupancy.

Cabinet sign - any wall sign that is not of channel or individually mounted letter construction.

Canopy - a roof-like cover, often of fabric, plastic, metal, or glass on a support, which provides shelter over a doorway.

Canopy sign - any sign that is part of or attached to a canopy, made of fabric, plastic, or structural protective cover over a door or entrance. A canopy sign is not a marquee and is different from service area canopy signs.

Changeable copy sign - a sign or portion thereof with characters, letters, or illustrations that can be changed or rearranged without altering the face or the surface of the sign. Changeable copy signs do not include signs upon which characters, letters or illustrations change or rearrange only once in a 24-hour period.

Commercial Speech – speech advertising a business, profession, commodity, service or entertainment.

Elevation - the view of the side, front, or rear of a given structure(s).

Elevation area - the area of all walls that face any lot line.

Erect - activity of constructing, building, raising, assembling, placing, affixing, attaching, creating, painting, drawing or any other way of bringing into being or establishing.

Flag - any fabric or similar lightweight material attached at one end of the material, usually to a staff or pole, so as to allow movement of the material by atmospheric changes and which contains distinctive colors, patterns, symbols, emblems, insignia, or other symbolic devices.

Flashing sign - a directly or indirectly illuminated sign which exhibits changing light or color effect by any means, so as to provide intermittent illumination which includes the illusion of intermittent flashing light by means of animation. Also any mode of lighting which resembles zooming, twinkling, or sparkling.

Freestanding sign - any sign which has supporting framework that is placed on, or anchored in, the ground and which is independent from any building or other structure.

Frontage - the line of contact of a property with the public right-of-way.

Grade - grade shall be construed to be the final ground elevation after construction. Earth mounding criteria for landscaping and screening is not part of the final grade for sign height computation.

Ground sign - any freestanding sign with its sign face mounted on the ground or mounted on a base at least as wide as the sign and which has a total height not exceeding eight (8) feet.

Height of sign - the height of the sign shall be computed as the vertical distance measured from the base of the sign at grade to the top of the highest attached component of the sign.

Hotel, motel, motor hotel - any building or combination of buildings contained six or more rooms used for sleeping purposes by guest on a transient basis.

Illuminated sign - any sign which contains an element designed to emanate artificial light internally or externally.

Interior sign - a sign which is located within the interior of any building, or within an enclosed lobby or court of any building, and a sign for and located within the inner or outer body, court or entrance of any theater.

Issuing Authority - the City of Hopkins Department of Economic Development and Planning

Legally established nonconforming sign - any sign and its support structure lawfully erected prior to the effective date of this ordinance which fails to conform to the requirements of this ordinance. A sign which was erected in accordance with a variance granted prior to the adoption of this ordinance and which does not comply with this ordinance shall be deemed to be a legal nonconforming sign. A sign which was unlawfully erected shall be deemed to be an illegal sign.

Marquee - any permanent roof-like structure projecting beyond a theater building or extending along and projecting beyond the wall of that building, generally designed and constructed to provide protection from the weather.

Marquee sign - any building sign painted, mounted, constructed or attached in any manner, on a marquee.

Monument sign - any freestanding sign with its sign face mounted on the ground or mounted on a base at least as wide as the sign and which has a height exceeding eight (8) feet.

Multiple tenant site - any site which has more than one (1) tenant, and each tenant has a separate ground level exterior public entrance.

Non-commercial speech – dissemination of messages not classified as Commercial Speech which include, but are not limited to, messages concerning political, religious, social, ideological, public service and informational topics.

Off-premise sign – a commercial speech sign which directs the attention of the public to a business, activity conducted, or product sold or offered at a location not on the same premises where such business sign is located. For purposes of this sign ordinance, easements and other appurtenances shall be considered to be outside such platted parcel of land and any sign located or proposed to be located in an easement or other appurtenance shall be considered an off-premise sign.

On-premise messages – identify or advertise an establishment, person, activity, goods, products or services located on the premises where the sign is installed.

Parapet (wall) - that portion of building wall that rises above the roof level.

Pole sign - see Pylon Sign.

Portable sign - any sign which is manifestly designed to be transported, including by trailer or on its own wheels, even though the wheels of such sign may be removed and the remaining chassis or support is converted to another sign or attached temporarily or permanently to the ground since this characteristic is based on the design of such a sign.

Porte cochere - a roofed structure or roof-like cover, extending from the entrance of a building and which provides shelter over a doorway.

Principal building - the building in which the principal primary use of the lot is conducted. Lots with multiple principal uses may have multiple principal buildings, but storage buildings, garages, and other clearly accessory uses shall not be considered principal buildings.

Projecting sign - any sign which is affixed to a building or wall in such a manner that its leading edge extends more than two (2) feet beyond the surface or such building or wall face.

Property owner - legal owner of property as officially recorded by Hennepin County.

Public notices - official notices posted by public officers, employees or their agents in the performance of their duties, or as directed by such officers, employees or agents.

Public street right-of-way - the planned right-of-way for a public street.

Pylon sign - any freestanding sign which has its supportive structure(s) anchored in the ground and which has a sign face elevated above ground level by pole(s) or beam(s) and with the area below the sign face open.

Residential district - any district zoned for residential uses.

Roof - the exterior surface and its supporting structure on the top of a building or structure. The structural make-up of which conforms to the roof structures, roof construction and roof covering sections of the Uniform Building Code.

Roof line - the upper-most edge of the roof or in the case of an extended facade or parapet, the upper-most height of said facade.

Roof sign - any sign erected and constructed wholly on and above the roof of a building, supported by the roof structure, and extending vertically above the highest portion of the roof.

Roof sign, integral - any building sign erected or constructed as an integral or essentially integral part of a normal roof structure of any design, so that no part of the sign extends vertically above the highest portion of the roof and so that no part of the sign is separated from the rest of the roof by a space of more than six (6) inches.

Rotating sign - a sign or portion of a sign which turns about on an axis.

Setback, front - the minimum horizontal distance permitted between the public right-of-way and a structure on the premises. In instances in which a property fronts on more than one (1) street, front setbacks are required on all street frontages.

Setback, rear - the minimum horizontal distance permitted between the property line opposite the principal street frontage and a structure on the premises.

Setback, side - the minimum horizontal distance permitted between the side lot line and a structure on the premises.

Shimmering signs - a sign which reflects an oscillating sometimes distorted visual image.

Sign - any letter, word or symbol, poster, picture, statuary, reading matter or representation in the nature of advertisement, announcement, message or visual communication, whether painted, posted, printed, affixed or constructed, including all associated brackets, braces, supports, wires and structures, which is displayed for informational or communicative purposes.

Sign face - the surface of the sign upon, against, or through which the message of the sign is exhibited.

Sign structure - any structure including the supports, uprights, bracing and framework which supports or is capable of supporting any sign.

Site - a plot or parcel of land, or combination of contiguous lots or parcels of land, which are intended, designated, and/or approved to function as an integrated unit.

Stringer - a line of string, rope, cording, or an equivalent to which is attached a number of pennants.

Suspended sign - any building sign that is suspended from the underside of a horizontal plane surface and is connected to this surface.

Total site signage - the maximum permitted combined area of all freestanding and wall identification signs allowed on a specific property.

Visible - capable of being seen by a person of normal visual acuity (whether legible or not) without visual aid.

Wall - any structure which defines the exterior boundaries or courts of a building or structure and which has a slope of sixty (60) degrees or greater with the horizontal plane.

Wall sign - any building sign attached parallel to, but within two (2) feet of a wall, painted on the wall surface of, or erected and confined within the limits of an outside wall of any building or structure, which is supported by such wall or building, and which displays only one (1) sign surface.

Window sign - any building sign, pictures, symbol, or combination thereof, designed to communicate information about an activity, business, commodity, event, sale, or service, that is placed inside a window or upon the window panes or glass and is visible from the exterior of the window.

SECTION II – ADMINISTRATION AND ENFORCEMENT

570.07. **Permit required.**

No sign shall be erected, altered, reconstructed, maintained or moved in the city without first securing a permit from the city. The content of the sign shall not be reviewed or considered in determining whether to approve or deny a sign permit. Application for a permit shall be in writing addressed to the issuing authority and shall contain the following information:

- a. names and addresses of the owners of the display structure and property;
- b. the address at which any signs are to be erected;
- c. the lot, block and addition at which the signs are to be erected and the street on which they are to front;
- d. a complete set of plans showing the necessary elevations, distances, size and details to fully and clearly represent the construction and place of the signs;
- e. the cost of the sign;
- f. type of sign (i.e. wall sign, monument sign, etc.);
- g. certification by applicant indicating the application complies with all requirements of the sign ordinance; and
- h. if the proposed sign is along state trunk highway or interstate highway, the application shall be accompanied by proof that the applicant has obtained a permit from the state for the sign.

The issuing authority shall approve or deny the sign permit in an expedited manner no more than 30 days from the receipt of the complete application, including applicable fee. All permits not approved or denied within 30 days shall be deemed approved. If the permit is denied, the issuing authority shall prepare a written notice of within 10 days its decision, describing the applicant's appeal rights under Section 525.15, and send it by certified mail, return receipt requested, to the applicant.

Comment:

The timeline for a permit decision is not required to be 30 days. However, the provisions of Minn. Stat. 15.99 would likely apply and require a decision to be made within 60 days, or 120 days with an extension.

570.09 Exemptions.

The following signs shall not require a permit. These exemptions, however, shall not be construed as relieving the owner of the sign from the responsibility of its erection and maintenance, and its compliance with the provisions of this ordinance or any other law or ordinance regulating the same.

- a. The changing of the display surface on a painted or printed sign only. This exemption, however, shall apply only to poster replacement and/or on-site changes involving sign painting elsewhere than directly on a building.
- b. Signs six (6) square feet or less in size.

570.11. Fees.

Sign permit fees are set by Chapter X.

570.13. Repairs.

Any sign located in the city which may now be or hereafter become out of order, rotten or unsafe, and every sign which shall hereafter be erected, altered, resurfaced, reconstructed or moved contrary to the provisions of this section, shall be removed or otherwise properly secured in accordance with the terms of this section by the owners thereof or by the owners of the grounds on which said sign shall stand, upon receipt of proper notice so to do, given by the issuing authority. No rotten or other unsafe sign shall be repaired or rebuilt except in accordance with the provisions of this section and upon a permit issued by the issuing authority.

570.15. Removal.

In the event of the failure of the owner or person, company or corporation having control of any sign, or the owner of the ground on which the sign is located, to remove or repair said sign within 60 days after the use is terminated, a notice shall be given pursuant to subsection 430.09 and the sign may be removed by the city at the expense of the owner or manager of the sign, or the owner of the ground upon which the sign stands.

570.17. Violations.

Violation of this section is a misdemeanor. Each day that the violation continues is a separate offense.

SECTION III – GENERAL PROVISIONS

570.18 **Size**

No sign shall exceed 250 square feet in area.

Comment: This size limitation reflects the policy choice of the City of Hopkins. Different communities may impose different size restrictions.

570.19. **Regulations.**

Subd. 1. General. Except as hereinafter provided, no signs shall be erected or maintained at any angle to a building or structure which sign extends or projects over the sidewalk, street or highway. No sign which is erected or maintained flat against any building or structure shall extend or project more than fifteen inches over the sidewalk, street or highway.

Subd. 2. Exceptions. The provisions of this subsection do not prohibit:

- a. the erection and maintenance of signs, either illuminated or unilluminated, which are on the sides of a marquee which is firmly attached to and a part of a theatre or other building, providing such signs are an integral part of the marquee and do not project above or below the marquee; or
- b. the erection and maintenance of signs, not illuminated, which are attached to the marquee and which do not project more than 16 inches above the marquee.

570.21. **Below marquee.**

No sign, either illuminated or non-illuminated, may project below a marquee.

570.23. **Electrical signs.**

Electrical signs must be installed in accordance with the current electrical code and a separate permit from the building official must be obtained prior to placement.

570.25. **Unauthorized signs.**

The following signs are unauthorized signs:

- a. Any sign, signal, marking or device which purports to be or is an imitation of or resembles any official traffic control device or railroad sign or signal, or emergency vehicle sign, or which attempts to direct the movement of traffic or which hides from

view or interferes with the effectiveness of any official traffic-control device or any railroad sign or signal.

- b. All off-premise signs.
- c. Signs painted, attached or in any other manner affixed to trees, rocks, or similar natural surfaces, or attached to public utility poles, bridges, towers, or similar public structures.
- d. Portable signs.
- e. Changeable copy signs.

570.27. Setbacks.

Subd. 1. Yards. Signs shall conform to building yard regulations for the zoning district in which the signs are located except as otherwise specified in this section.

	B-1	B-2	B-3	B-3 Abutting County Road	Residential
Front Yard	10'	1'	1'	10'	5'
Side Yard	5'	0'	0'	5'	5'
Rear Yard	5'	5'	10'	10'	5'
				I-1	I-2
Front Yard				10'	10'
Side Yard				10'	10'
Rear Yard				10'	10'
Rear Yard - abutting R District				20'	20'

570.29. Area.

The area within the frame shall be used to calculate the square footage except that the width of a frame exceeding 12 inches shall constitute sign face, and if such letters or graphics be mounted directly on a wall or fascia or in such way as to be without a frame the dimensions for calculating the square footage shall be the area extending six inches beyond the periphery formed around such letters or graphics in a plane figure bounded by straight lines connecting the outermost points thereof. Each surface utilized to display a message or to attract attention shall be measured as a separate sign and shall be calculated in the overall square footage. Symbols, flags, pictures, wording, figures or other forms of graphics painted on or attached to windows, walls, awnings, free-standing structures, suspended by balloons, or kites or on persons, animals, or vehicles are considered a sign and are included in calculating the overall square footage.

570.31. Canopies, marquees and fixed awnings.

Canopies, marquees and fixed awnings are an integral part of the structure to which they are attached. They are allowed in the Business and Industrial Districts if they meet following requirements and the applicable square footage requirements.

- a. an awning, canopy or marquee may not project into the public right-of-way nearer than 30 inches to the street curb or curb line;
- b. awnings, canopies or marquees may have no part of the structure other than supports nearer the ground surface than seven feet;
- c. the architectural style of the awning, canopy or marquee may be consistent with the building being served;
- d. awnings, canopy or marquees projecting into the required yards may not be enclosed except with a transparent material permitting through vision; and
- e. awnings, canopies or marquees built over the public right-of-way must be included in a liability insurance policy holding the city free of all responsibility.

570.33. Illumination

External illumination for signs shall be so constructed and maintained that the source of light is not visible from the public right-of-way or residential property.

570.35. Height.

The top of a sign, including its superstructure, if any, shall be no higher than the roof of the building to which such sign may be attached or 35 feet above ground level, whichever height is less; except that the height of any changeable sign which is attached to or an integral part of a functional structure, such as a water tower, smoke stack, radio or TV transmitting tower, beacon or similar structure shall be no higher than such structure. Signs, including any superstructure standing or erected free of any building or other structure, shall not exceed an overall height of 35 feet from ground level and shall be located on land in an area which is landscaped or if such land is part of an approved parking area, it shall be surfaced or paved as required in the zoning code.

570.36 Retroactive affect.

This sign ordinance shall apply to all sign applications applied for and/or pending prior to its enactment.

570.365 Non-commercial speech.

Notwithstanding any other provisions of this sign ordinance, all signs of any size containing Non-Commercial Speech may be posted from August 1 in any general election year until ten (10) days following the general election and thirteen (13) weeks prior to any special election until ten (10) days following the special election.

Comment: Note that this provision does not mean that the stated times are the only times that noncommercial signs or political signs may be posted.

SECTION IV – SPECIFIC REGULATIONS BY ZONING DISTRICT

570.37. **Permitted signs by district.**

Subd. 1 **Residential Districts**

a. Within residential zoning districts, signs are permitted as follows:

<u>District</u>	<u>Maximum sign area of single sign</u>	<u>Total area of all signs</u>
R-1, R-2	8 square feet per surface	16 square feet
R-3, R-4, R-5, R-6	12 square feet per surface	24 square feet

b. The following types of signs are not permitted in residential zoning districts:

1. Awning signs;
2. Balloon signs;
3. Canopy signs;
4. Flashing signs;
5. Marquee signs;
6. Pole signs;
7. Pylon signs; and
8. Shimmering signs.

Subd. 2 **Business Districts**

a. Within business zoning districts, signs are permitted as follows:

<u>District</u>	<u>Maximum sign area of single sign</u>	<u>Total area of all signs</u>
B-1, B-2	60 square feet	2 square feet per front foot of building abutting a public right-of-way 50 feet or more in width.
B-3, B-4	80 square feet	3 square feet per front foot of lot (narrowest footage on a corner lot) abutting public right-of-way 50 feet or more in width.

Subd. 3 **Industrial Districts**

a. Within industrial zoning districts, signs are permitted as follows:

<u>District</u> I-1, I-2	<u>Maximum sign area of single sign</u> 250 square feet	<u>Total area of all signs</u> 4 square feet per front foot of Lot plus 1 square foot per foot of side yard abutting a public right-of-way of 50 feet or more. Least width of frontage shall be considered front yard.
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570.39. Permitted signs: business park district.

Subd. 1. Wall Signs. Each tenant other than those in multi-tenant buildings may have one flat wall sign, not extending more than 18 inches from the face of the building, except that such sign age may extend from the face of the roof over a covered walk. Such wall signs shall not exceed 15% of the area of the wall to which the sign is attached, to a maximum of 96 square feet.

Subd. 2. Monument signs. Uses other than those in multi-tenant buildings may have a monument sign that shall not exceed 80 square feet per surface area, and 15 feet in height, and is setback a minimum 20 feet from the property lines.

Subd. 3. Multi-tenant signs. Each tenant in a multi-tenant building may have a flat wall sign, not extending more than 18 inches from the face of the building. The aggregate area of such signs shall not exceed 5% of the area of the wall to which they are attached.

Subd. 4. Multi-tenant monument signs. One monument sign shall be permitted for each multi-tenant building provided the surface area of the sign does not exceed 100 square feet per side, 15 feet in height, and is setback in no case less than 20 feet from the property lines. The area may be increased to a maximum of 150 square feet per side for developments of over 20 acres.

Subd. 5. Canopies and Awnings. The design of canopies shall be in keeping with the overall building design in terms of location, size, and color. No canopies with visible wall hangers shall be permitted. Signage on canopies may be substituted for allowed building signage and shall be limited to 25% of the canopy area. Internally illuminated canopies must be compatible with the overall color scheme of the building.

SECTION V – NON-CONFORMING USES

570.41. Non-conforming signs: compliance.

It is recognized that signs exist within the zoning districts which were lawful before this sign ordinance was enacted, which would be prohibited, regulated or restricted under the terms of this chapter or future amendments. It is the intent of this sign ordinance that nonconforming signs shall not be enlarged upon, expanded or extended, nor be used as grounds for adding other signs or uses prohibited elsewhere in the same district. It is further the intent of this sign ordinance to permit legal nonconforming signs existing on the effective date of this sign ordinance, or amendments thereto, to continue as legal nonconforming signs provided such signs are safe, are

maintained so as not to be unsightly, and have not been abandoned or removed subject to the following provisions:

- a. No sign shall be enlarged or altered in a way which increases its nonconformity.
- b. Should such sign or sign structure be destroyed by any means to an extent greater than fifty (50) percent of its replacement cost and no building permit has been applied for within 180 days of when the property was damaged, it shall not be reconstructed except in conformity with the provisions of this ordinance.
- c. Should such sign or sign structure be moved for any reason for any distance whatsoever, it shall thereafter conform to the regulations for the zoning district in which it is located after it is moved.
- d. No existing sign devoted to a use not permitted by the zoning code in the zoning district in which it is located shall be enlarged, extended or moved except in changing the sign to a sign permitted in the zoning district in which is it located.
- e. When a structure loses its nonconforming status all signs devoted to the structure shall be removed and all signs painted directly on the structure shall be repainted in a neutral color or a color which will harmonize with the structure.

SECTION VI – Noncommercial Speech

570.43 Substitution Clause

The owner of any sign which is otherwise allowed by this sign ordinance may substitute non-commercial copy in lieu of any other commercial or non-commercial copy. This substitution of copy may be made without any additional approval or permitting. The purpose of this provision is to prevent any inadvertent favoring of commercial speech over non-commercial speech, or favoring of any particular non-commercial message over any other non-commercial message. This provision prevails over any more specific provision to the contrary.

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[Sec. 117-457. - Purpose.](#)
[Sec. 117-458. - Substitution clause.](#)
[Sec. 117-459. - Variations.](#)
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[Sec. 117-461. - Violations.](#)
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[Sec. 117-464. - Prohibited signs.](#)
[Sec. 117-465. - Temporary signs.](#)
[Secs. 117-466—117-483. - Reserved.](#)

Sec. 117-457. - Purpose.

The purpose of this division is to protect and promote the general welfare, health, safety and order within the city through the establishment of a comprehensive and impartial series of standards, regulations and procedures governing the erection, use and/or display of devices, signs or symbols serving as a visual communicative media to persons situated within or upon public rights-of-way or properties. The provisions of this division are intended to encourage creativity, a reasonable degree of freedom of choice, an opportunity for effective communication and a sense of concern for the visual amenities on the part of those designing and displaying communicative media for the types regulated by this division, while at the same time assuring that the public health and welfare is not endangered.

(Code 1978, § 9.12.01; Ord. No. 94-02, 2-28-1994; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

Sec. 117-458. - Substitution clause.

The owner of any sign that is otherwise allowed by this division may substitute noncommercial speech in lieu of any other commercial speech or noncommercial speech. The owner of any sign that is otherwise allowed by this division may substitute commercial speech in lieu of any other commercial speech or noncommercial speech. This substitution of copy may be made without any additional approval or permitting. The purpose of this division is to prevent any inadvertent favoring of commercial speech over noncommercial speech, favoring of noncommercial speech over commercial speech, favoring any particular commercial speech over any other commercial speech, or favoring of any particular noncommercial speech over any other noncommercial speech. This division prevails over any more specific provision to the contrary.

Sec. 117-459. - Variations.

Request for variations from the provisions of this division shall be processed by the applicant applying to the zoning administrator for a conditional use permit, which conditional use permit procedure shall be as prescribed in section 117-50.

(Code 1978, § 9.12.24; Ord. No. 94-02, 2-28-1994; Ord. No. 03-31, 9-15-2003; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

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Sec. 117-460. - Conflict.

If any portion of this division is found to be in conflict with any other provision of any zoning, building, fire, safety or health ordinance of the city, the provision that establishes the higher standard shall prevail.

(Code 1978, § 9.12.25; Ord. No. 94-02, 2-28-1994; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

Sec. 117-461. - Violations.

(a) When, in the opinion of the zoning administrator, a violation of this Code exists, the zoning administrator shall issue a written order to either the owner of the sign, or the owner of the property, or tenant leasing the property, on which the sign is placed. The order shall specify those sections of this Code involved, describe the violation and shall direct that the violation be corrected within five days from the date of the order, unless otherwise specified by the zoning administrator. If the violation is not corrected, the violation will be penalized through the administrative citation or removal, whichever is appropriate.

(b) If the zoning administrator or building official finds that a sign is abandoned or is structurally, or electrically defective, or in any way endangers the public, the zoning administrator or building official shall issue a written order to the owner of the sign and occupant of the premises stating the nature of the violation and requiring repair or removal of the sign within 60 days of the date of the order.

(Code 1978, § 9.12.21; Ord. No. 94-02, 2-28-1994; Ord. No. 03-31, 9-15-2003; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

Sec. 117-462. - Removal of signs by the zoning administrator.

(a) The zoning administrator may cause the removal of any illegal sign, any sign remaining after a business permanently closes, or any sign not properly maintained in cases of emergency, or after failure to timely comply with written orders for removal or repair. After removal or demolition of the sign, a notice shall be mailed to the sign owner and owner of the property where the sign was located stating the nature of the work and the date on which it was performed and demanding payment of the costs as certified by the zoning administrator together with an additional 50 percent for inspection and incidental costs.

(b) If the amount specified in the notice is not paid within 30 days after mailing of the notice, it shall become a lien against the parcel where the sign was located and shall be certified as an assessment against the property together with ten percent interest for collection in the same manner as the real estate taxes.

(c) The owner of the parcel upon which the sign is located shall be presumed to be the owner of all signs thereon unless facts to the contrary are brought to the attention of the zoning administrator.

(d) In case of emergency, the zoning administrator or building official may cause the immediate removal of a dangerous or defective sign without notice. Signs removed in this manner must present a hazard to the public safety as defined in the state building code.

(Code 1978, § 9.12.22; Ord. No. 94-02, 2-28-1994; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

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Sec. 117-463. - General restrictions.

- (a) *Address sign.* One address sign shall be required per main building in all districts.
- (b) *Bench sign.* Bench signs shall be permitted only at bus stops.
- (c) *Ground sign.* A ground sign shall not project higher than 25 feet as measured from base of sign or grade of the nearest adjacent roadway, whichever is lower. Any ground sign within 50 feet of any intersection of street right-of-way lines and/or driveway entrances shall have a minimum vertical clearance of 12 feet above the centerline of the pavement unless it can be shown that it can comply with subsections (e)(2)a and b of this section.
- (d) *Canopies or marquees.* Canopies and marquees shall be considered to be an integral part of the structure to which they are an accessory. Signs may be attached to a canopy or marquee, but such structures shall not be considered as part of the wall area and thus shall not warrant additional sign area.
- (e) *Location.*
- (1) No sign other than governmental signs shall be erected or temporarily placed within any street or public right-of-way or upon any public easement.
 - (2) A permit for a sign to be located within 50 feet of any street or highway regulatory or warning sign, or any traffic sign or signal, or of any crossroad or crosswalk, will not be issued unless:
 - a. The sign will not interfere with the ability of drivers and pedestrians to see any street or highway sign, or any traffic sign or signal, or any crossroad or crosswalk; and
 - b. The sign will not distract drivers nor offer any confusion to any street or highway, sign, or any traffic sign or signal.
 - (3) A sign shall not be located so as to extend over any lot line or within 15 feet of any point of vehicular access from a parcel to a public roadway.
- (f) *Dynamic display and illumination.*
- (1) Based on findings conducted by scientific studies, the city finds that dynamic displays should be allowed on signs with appropriate regulation in order to minimize their proliferation and their potential threats to public safety.
 - (2) Regulations. Dynamic displays on signs are subject to the following conditions:
 - a. Size. No dynamic display shall exceed 35 percent of the actual copy and graphic area of a sign. The remainder of the sign must not have the capability to have dynamic displays, even if not used. Only one dynamic display area is permitted on a sign face. On-premise signs may include dynamic displays. Dynamic display signs shall not exceed the size allowed by this Chapter. Dynamic displays are not in addition to the size allowed for static signs.
 - b. Frequency of display change. A dynamic display may not change more often than once every three seconds, and no part of the display may include flashing or scrolling text, and no part of the display may flash or scroll at any time. The images display must be static, and the transition from one display to another must be instantaneous without special effects. The dynamic display shall not be allowed to project full-motion video. Subtle transition animations

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shall be allowed.

c. **Brightness.** No sign may be brighter than is necessary for clear and adequate visibility, or that it interferes with the effectiveness of a traffic sign or signal, or that it distracts a driver from motor vehicle operation.

d. **Troubleshooting.** Dynamic displays must be designed and equipped to freeze the device in one position if a malfunction occurs. The display must also be equipped with a means to immediately discontinue the display if it malfunctions, and the sign owner must immediately stop the dynamic display when notified by the city that it is not complying with the standards of this section.

(g) ***Institutional and recreational identification sign.*** One wall and/or ground sign shall be permitted to identify the civic, philanthropic, educational, public or religious organization or recreational use occupying the parcel. There shall not be more than one ground sign for each parcel. The gross surface area of a ground sign shall not exceed 100 square feet for each exposed face nor exceed an aggregate gross surface area of 200 square feet. Such facilities having more than one point of entrance or street frontage may erect secondary identification signs not to exceed 50 square feet for each exposed face at the additional entrance points or street frontages.

(h) ***Institutional attraction boards.*** There shall not be more than one institutional attraction board for each principal building and it may be either wall or ground type or constructed as a part of the institutional identification sign. The gross surface area of an attraction board shall not exceed 50 square feet for each exposed face nor exceed an aggregate gross surface area of 100 square feet. An attraction board shall not be located so as to extend over any lot line or within 15 feet of any point of vehicular access from any parcel to a public roadway. Attraction boards shall not exceed 15 feet in height as measured from the base of the sign or building to which the sign is to be affixed or the grade of the nearest adjacent roadway, whichever is higher.

(i) ***Pennants.*** The use of pennants is permitted in any commercial district provided the pennants are securely anchored and maintained in good repair, including replacement or removal when weather damage is evident. The zoning administrator, or his designee, shall have the authority to identify and notify property owners and/or occupants of the parcel on which the pennants have been determined to be in violation of this Code. Failure to remove or replace the pennants within ten days of notification shall be sufficient cause for the zoning administrator or his designee to remove said pennants. The property owner shall be billed for all costs incurred by the city in administering this subdivision.

(j) ***Banners.*** The use of banners is permitted in any commercial district as permanent wall signage provided the banner is securely anchored and maintained in good repair, including replacement or removal when weather damage is evident. The square footage dedicated to banners is limited to 50 percent of the total square footage allowance for wall signs. Facilities choosing to utilize wall banners in addition to other permanent wall signage shall be required to obtain a separate sign permit for the maximum square footage allowed for banners and payment of the permanent sign fee.

(k) ***Flags.*** The display of flags shall be permitted in all districts. However, the total square footage area of any flags used as advertising copy or as attention getting devices for commercial purposes shall be considered as permanent signage and counted towards the total allowable sign area permitted by this section for the parcel on which the flag is displayed. Flag height is restricted to 25 feet.

(l) ***Directional signs.*** Directional or instructional signs are restricted to on-site direction and instruction,

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with the exception of governmental signs, temporary real estate signs and public event signs, and shall not exceed four square feet in size. Such signs shall only provide direction or instruction to guide persons to facilities intended to serve the public.

(m) *Maintenance.*

(1) The surface and structure of all signs must be kept refinished as necessary to prevent the sign surface from becoming unkempt in appearance. The zoning administrator shall use the following guidelines to determine if the sign is unkempt: evidence of rust, peeling paint, structural damage, message damage, and/or weathering.

(2) When any sign for which a permit is required is removed, the zoning administrator shall be notified and the entire sign and its components shall be removed.

(3) The permit owner shall be responsible for all of the requirements of this section, including the liability for expense of removal and maintenance incurred by the city.

(n) *Sign content.* No sign shall contain obscene images or statements in violation of Minn. Stats. § 617.241.

(Code 1978, § 9.12.03; Ord. No. 94-02, 2-28-1994; Ord. No. 03-31, 9-15-2003; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

Sec. 117-464. - Prohibited signs.

(a) *"A" frame or sandwich board signs.* "A" frame or sandwich board signs are prohibited.

(b) *Advertising device signs.* Advertising device signs are prohibited except as provided for in section 117-465

(c) *Whirling devices, searchlights, streamers, balloons and other gas-filled figures.* Whirling devices, searchlights, streamers, balloons, and other gas filled figures, are prohibited except as a temporary sign as provided for in section 117-465

(d) *Flashing or scrolling signs, devices or lights.* Flashing signs, devices, or lights are not permitted in any district except as provided for in section 117-463(f).

(e) *Portable signs.* Portable signs are prohibited except as temporary signs as provided for in section 117-465

(f) *Projecting signs.* No projecting sign shall be permitted in any district.

(g) *Roof signs.* Roof signs are prohibited in any zoning district.

(h) *Signs on parked vehicles.* Signs painted or mounted on or attached to vehicles, trailers or equipment where the apparent primary purpose of the vehicle or equipment is to display said sign are prohibited. However, this is not in any way intended to prohibit signs placed on or affixed to vehicles and trailers, such as lettering on motor vehicles, where the sign is incidental to the primary use of the vehicle, trailer and/or any other type of mobile equipment.

(i) *Signs on trees and utility poles.* Signs which are attached or otherwise affixed to trees or other vegetation or utility poles are prohibited.

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(j) *Signs painted on walls.* Signs painted on an exterior wall, fascia, parapet or a chimney of a building or on a fence are prohibited.

(k) *Signs which imitate traffic control devices.* Signs which imitate, interfere with, obstruct the view of, or can be confused with any authorized traffic control sign, signal, or other device are prohibited.

(l) *Billboards.* No billboards shall be permitted in any zoning district of the city, except as otherwise provided in this Division and except that off-premise signs may be located on adjacent parcels or at shared entrances. Shared entrances shall be encumbered by a recordable easement, filed with the Anoka County Recorder. If a sign is placed on an adjacent parcel, it requires approval from the parcel owner and counts against the allowable signage for the parcel it is located.

(Code 1978, § 9.12.04; Ord. No. 94-02, 2-28-1994; Ord. No. 03-31, 9-15-2003; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008)

Sec. 117-465. - Temporary signs.

(a) Temporary signs are defined as signs which are erected or displayed for a limited period of time and not affixed to a state building code approved structure. A sign is considered temporary if it is not intended to remain on the property permanently and has not received a permanent sign permit and/or is not intended to be constructed per the state building code.

(1) *Duration.* Temporary signs shall be permitted for up to six weeks per year per parcel, or per business on multitenant business parcels. The six-week limit may be extended up to four weeks if there have been no sign violations in the past year by the entity who erects the sign.

(2) *Location.* Temporary signs must be located fully on private property, with the permission of the property owner. The city will remove any signs in the public right-of-way, or erected without permission from the property owner.

(3) *Number.* All properties are limited to two temporary signs per parcel. In locations where there are multiple businesses on one parcel (a multitenant facility), three signs are permitted.

(4) *Size.* Temporary signs shall be limited to 50 square feet in size.

(5) *Dynamic display.* Temporary signs may consist of dynamic display, provided all standards of Section 117-463 Subd. (f) are complied with.

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(6) *Permits and fees.* All temporary signs shall be required to obtain a temporary sign permit. The temporary sign permit application shall specify the exact times and dates the temporary sign is to be erected, the size of the sign, the location of the sign, contact information, and other information deemed necessary by the zoning administrator to determine that the temporary sign is erected within applicable code. The Temporary Sign Permit shall be clearly displayed on the temporary sign. The fee for the temporary sign permit shall be established by ordinance as part of the rates and charges. The fee shall only be collected for the first permit per parcel or per business in a multi-tenant facility. The provisions of this division apply to all temporary signs, and appropriate penalties for violations will be assessed, as outlined in section 117-461

A. *Exemptions from temporary sign Permit.* Signs less than 15 square feet in size and all temporary signs in residential districts (R-1 Residential, R-2 Residential, R-3 Residential) are exempt from temporary sign permitting requirements.

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(b) Balloons, gas filled figures, streamers, whirling devices and revolving searchlights or any such attention-getting device that is not specifically a sign may be permitted up to four weeks per year, for one week at a time.

(Code 1978, § 9.12.05; Ord. No. 94-02, 2-28-1994; Ord. No. 08-07, § 2, 2-12-2008; Ord. No. 08-25, § 2, 8-12-2008; Ord. No. 10-11, § 2, 7-27-2010)

Secs. 117-466 – Other Temporary Signs

(a) *Real Estate Signs.* One temporary real estate sign constructed of durable materials located on the premises is permitted for sale or lease of building or vacant lot for each street frontage. Real Estate Signs are limited to the following maximum sizes:

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(1) *Residential Districts.* Real Estate Signs may not exceed 32 square feet in residentially zoned districts.

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(2) *Commercial and Employment Districts.* Real Estate Signs may not exceed 64 square feet in commercially or employment zoned districts. Real Estate Signs erected on walls of buildings may not exceed 15% of the front façade of said structure.

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(b) *Construction Signs.* One temporary construction sign constructed of durable materials located on the premises is permitted on each street frontage of a development under construction.

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Secs. 117-467 – Unified Development Signs

(a) *General provisions.* A unified development is a development that consists of multiple parcels of similar zoning district and bound by major roadways consisting of arterial or collector designation or higher. Signs for multi-tenant commercial and employment developments may be erected to include off-premise copy under the following conditions:

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(1) The sign must identify the development at the top of the sign and may include provisions for individual users within the development.

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(2) The sign must be located within 500 feet of the development and may not be separated from the development by an arterial road.

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(43) The sign must not exceed 250 square feet per face (500 square feet aggregate) and 30 feet in height.

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(54) The sign may include dynamic display not to exceed 100 square feet per face (200 square feet aggregate).

(65) The sign will not be included in the total signage permitted for the property in which it is located.

(76) The general location of area identification signs for commercial and employment districts must be approved by the Planning Commission as part of a Master Sign Plan approved as part of site plan approval.

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117-468—117-483. - Reserved.

ORDINANCE #12-__

CITY OF RAMSEY
ANOKA COUNTY STATE OF MINNESOTA

AN AMENDMENT TO CHAPTER 117 WHICH IS KNOWN AS THE ZONING AND SUBDIVISION OF LAND CHAPTER OF THE CITY CODE OF RAMSEY, MINNESOTA.

AN ORDINANCE AMENDING ARTICLE II, DIVISION 8 (“SIGNS”) OF CHAPTER 117 OF THE CITY CODE OF RAMSEY, MINNESOTA.

The City of Ramsey ordains:

SECTION 1. AMENDMENT

Article II, Division 8 of Ramsey City Code shall be amended as follows:

Sec. 117-463. - General restrictions.

(f) *Dynamic display and illumination.*

(1) Based on findings conducted by scientific studies, the city finds that dynamic displays should be allowed on signs with appropriate regulation in order to minimize their proliferation and their potential threats to public safety.

(2) Regulations. Dynamic displays on signs are subject to the following conditions:

a. Size. ~~No dynamic display shall exceed 35 percent of the actual copy and graphic area of a sign. The remainder of the sign must not have the capability to have dynamic displays, even if not used. Only one dynamic display area is permitted on a sign face. On-premise signs may include dynamic displays. Dynamic display signs shall not exceed the size allowed by this Chapter. Dynamic displays are not in addition to the size allowed for static signs.~~

b. Frequency of display change. A dynamic display may not change more often than once every three seconds, ~~and no part of the display may include flashing or scrolling text, and no part of the display may flash or scroll at any time.~~ The images display must be static, and the transition from one display to another must be instantaneous without special effects. The dynamic display shall not be allowed to project full-motion video. Subtle transition animations shall be allowed.

c. Brightness. No sign may be brighter than is necessary for clear and adequate visibility, or that it interferes with the effectiveness of a traffic sign or signal, or that it distracts a driver from motor vehicle operation.

d. Troubleshooting. Dynamic displays must be designed and equipped to freeze the device in one position if a malfunction occurs. The display must also be equipped with a means to immediately discontinue the display if it malfunctions, and the sign owner must immediately stop the dynamic display when notified by the city that it is not complying with the standards of this section.

Sec. 117-464. - Prohibited signs.

(1) Billboards. No billboards shall be permitted in any zoning district of the city, except as otherwise provided in this Division and except that off-premise signs may be located on adjacent parcels or at shared entrances. Shared entrances shall be encumbered by a recordable easement, filed with the Anoka County Recorder.

Sec. 117-465. - Temporary signs.

(5) Dynamic display. Temporary signs may consist of dynamic display, provided all standards of Section 117-463 Subd. (f) are complied with.

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Secs. 117-466 – Other Temporary Signs

(a) Real Estate Signs. One temporary real estate sign constructed of durable materials located on the premises is permitted for sale or lease of building or vacant lot for each street frontage.

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(1) Residential Districts. Real Estate Signs may not exceed 32 square feet in residentially zoned districts.

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(2) Commercial and Employment Districts. Real Estate Signs may not exceed 64 square feet in commercially or employment zoned districts. Real Estate Signs erected on walls of buildings may not exceed 15% of the front façade of said structure.

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(b) Construction Signs. One temporary construction sign constructed of durable materials located on the premises is permitted on each street frontage of a development under construction.

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Secs. 117-467 – Unified Development Signs

(a) General provisions. A unified development is a development that consists of multiple parcels of similar zoning district and bound by major roadways consisting of arterial or collector designation or higher. Signs for multi-tenant commercial and employment developments may be erected to include off-premise copy under the following conditions:

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(1) The sign must identify the development at the top of the sign and may include provisions for individual users within the development.

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(2) The sign must be located within 500 feet of the development and may not be separated from the development by an arterial road.

(3) The sign must not exceed 250 square feet per face (500 square feet aggregate) and 30 feet in height.

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(4) The sign may include dynamic display not to exceed 100 square feet per face (200 square feet aggregate).

(5) The sign will not be included in the total signage permitted for the property in which it is located.

(6) The general location of area identification signs for commercial and employment districts must be approved by the Planning Commission as part of a Master Sign Plan approved as part of site plan approval.

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117-468—117-483. - Reserved.

SECTION 2. SUMMARY

The following official summary of Ordinance #12-__ has been approved by the City Council of the City of Ramsey as clearly informing the public of the intent and effect of the Ordinance.

It is the intent of this Ordinance to amend the standards in Article II, Division 8 of Chapter 117 of the Ramsey City Code to amend sign standards in the City related to size and animation of dynamic display signs, establishment of separate provisions for real estate signs and construction signs, and allow for provisions for minor off-premise signs, not to include billboards.

SECTION 3. EFFECTIVE DATE

The effective date of this ordinance is thirty (30) days after its passage and publication, subject to City Charter Section 3.9.

Adopted by the Ramsey City Council the 23rd day of October 2012.

Mayor

ATTEST:

City Clerk

Introduction Date:
Posting Dates:
Adoption Date:
Publication Date:
Effective Date:

Meeting Date: 11/27/2012

By: Tim Gladhill, Community Development

Information

Title:

Adopt Ordinance Vacating Portions of Drainage and Utility Easement at 6012 146th Ave NW; Case of Jesse and Dana VerBeek

Background:

In early October, Staff was requested to review a floor plan for a new single-family home within the VILLAGE OF SUNFISH LAKE subdivision. Staff completed a non-site specific review as it relates to bulk standards and architectural review required by the Planned Unit Development (PUD) approval for the development.

Following the initial review, the Applicant proceeded forward with choosing a lot and beginning the necessary surveying and engineering. During the surveying activities, it was discovered that the proposed lot contained larger than expected drainage and utility easements.

Notification:

In accordance with State Statutes and City Code, Staff attempted to notify all Property Owners within 350 feet of the Subject Property of the Public Hearing that was held on November 13th. The Public Hearing was properly published in the Anoka County Union.

Observations:

The ten (10) foot drainage and utility easement was the result of City Code requirements for a standard ten (10) foot drainage and utility easements, typically centered on common lot lines. The Property is technically the last on the block and does not share a common property line with adjacent parcels within the development. The Property is separated from the previously existing, adjacent subdivision of CHESTNUT RIDGE SECOND ADDITION. As part of approval of VILLAGE OF SUNFISH LAKE, density transitioning in the form of a 'buffer' outlot was created. This Buffer Outlot is completely encumbered by drainage and utility easement.

Given that the City has approximately thirty (30) feet of drainage and utility easement on the Buffer Outlot and utilities and drainage are not negatively impacted by vacation, Staff is supportive of the request. The request has been reviewed by the City Engineer.

The ordinance was introduced on November 13th, and is now eligible for adoption. An Easement Encroachment Agreement was also approved on November 13th, allowing the Applicant to begin construction on a new single-family home.

Recommendation:

Staff recommends that the City Council adopt the ordinance vacating a portion of drainage and utility easement at 6012 146th Ave NW.

Funding Source:

All costs associated with processing the request are the responsibility of the Applicant.

Council Action:

Motion to waive the City Charter requirement to read the ordinance aloud;

-AND-

Motion to adopt the ordinance vacating a portion of drainage and utility easement at 6012 146th Ave NW.

Roll Call Vote:

Councilmember Strommen

Councilmember Tossey

Councilmember McGlone

Councilmember Elvig

Councilmember Backous

Councilmember Wise

Mayor Ramsey

Attachments

Site Location Map

Easement Vacation Exhibit

Certificate of Surety for New Home

Proposed Architectural Renderings

Proposed Ordinance

Form Review

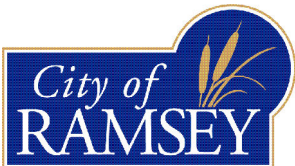
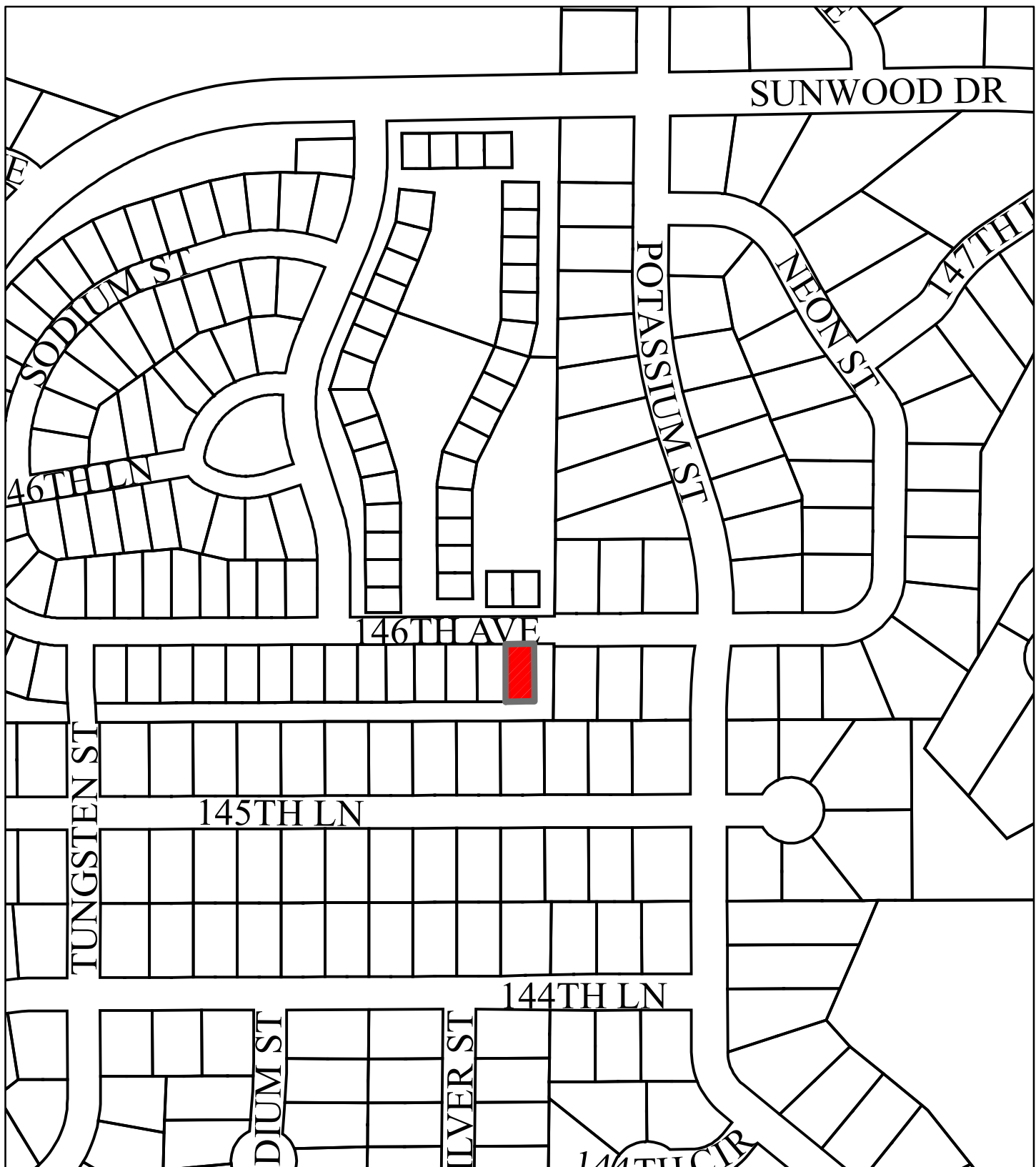
Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
11/21/2012 11:47 AM
Started On: 11/18/2012

Form Started By: Tim Gladhill

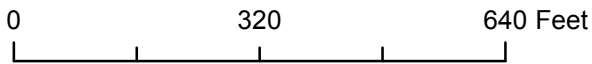
Final Approval Date: 11/21/2012



6012 146th Avenue NW

Legend

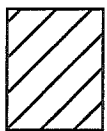
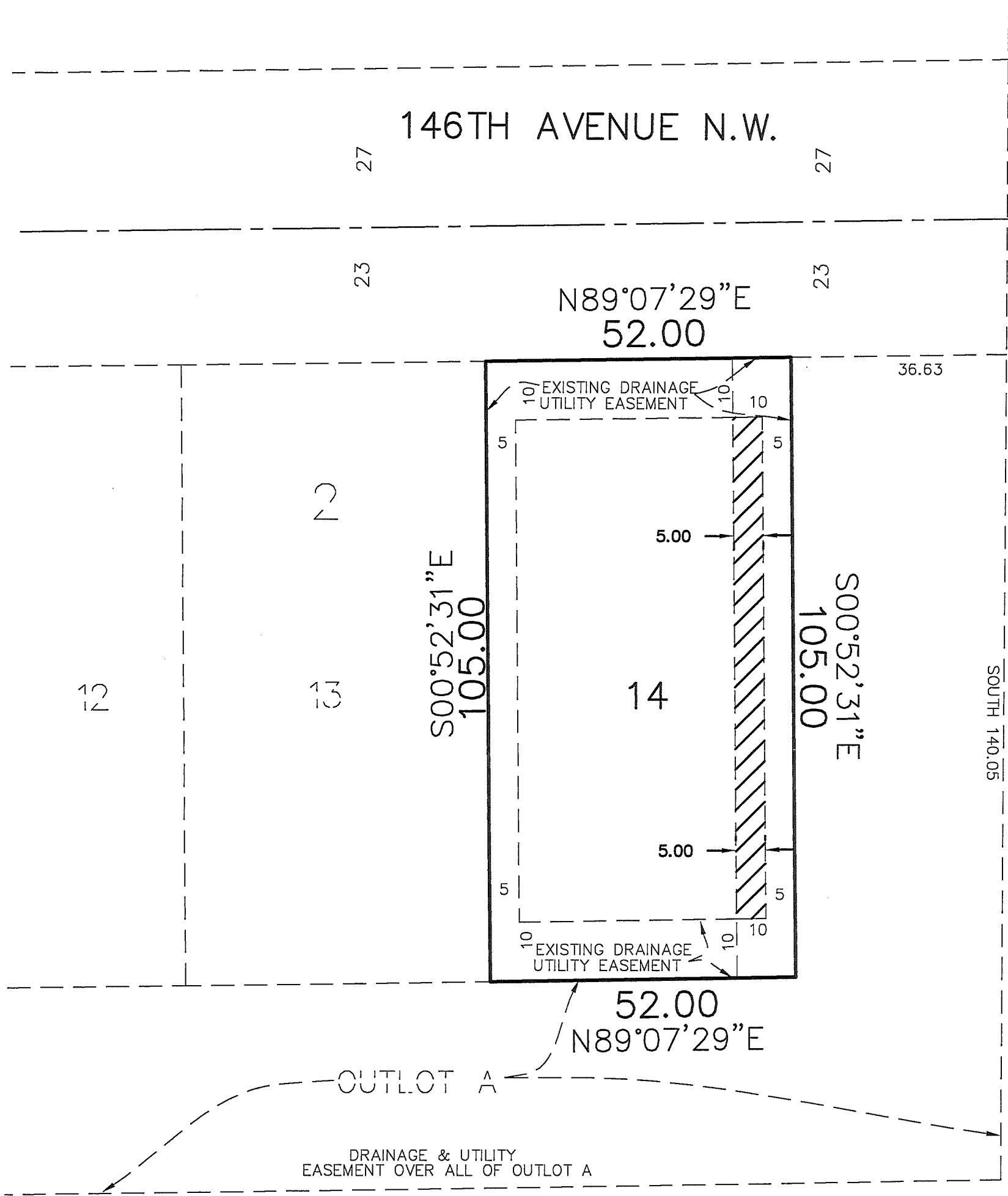
- Site
- Parcels



EASEMENT VACATION EXHIBIT

~ for ~

BAAS CONSTRUCTION, INC.



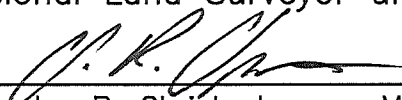
DENOTES DRAINAGE & UTILITY EASEMENT TO BE VACATED

1 INCH = 20 FEET

DESCRIPTION OF DRAINAGE & UTILITY EASEMENT TO BE VACATED

The west 5.00 feet of the east 10.00 feet of Lot 14, Block 2, VILLAGE OF SUNFISH LAKE 2ND ADDITION, Anoka County, Minnesota, lying northerly of the south 10.00 feet and southerly of the north 10.00 feet thereof.

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Land Surveyor under the laws of the State of Minnesota.


Charles R. Christopherson, MN License No. 18420

10/23/2012
Date

 **Hakanson
Anderson**

Job No.: 3769.01

Oct 24, 2012 - 5:35pm brian
K:\cad_surv\Land Desktop 2008\3769.01\dwg\376901-VACATION

Certificate of Survey *for*

BAAS CONSTRUCTION, INC.

NOTES:

1. Contractor to verify all building dimensions and elevations.
2. In providing this survey no attempt has been made to obtain or show data concerning existence, size, depth, condition, capacity or location of any utility existing on the site, whether private, municipal or public owned.
3. The professional surveyor has made no investigation or independent search for easements of record, encumbrance, restrictive covenants, ownership title evidence, or any other facts that an accurate and current title search may disclose.
4. Square footage of lot is 5,460 sq.ft. or 0.125 acres
5. This lot is in Section 26 Township 32 Range 25
6. Builder/Owner place retaining walls as needed.
7. House pad needs to be cleared - YES NO

PROPOSED ELEVATIONS

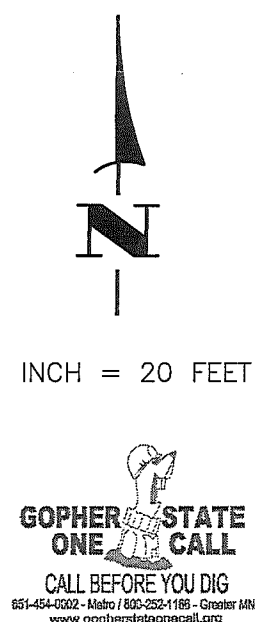
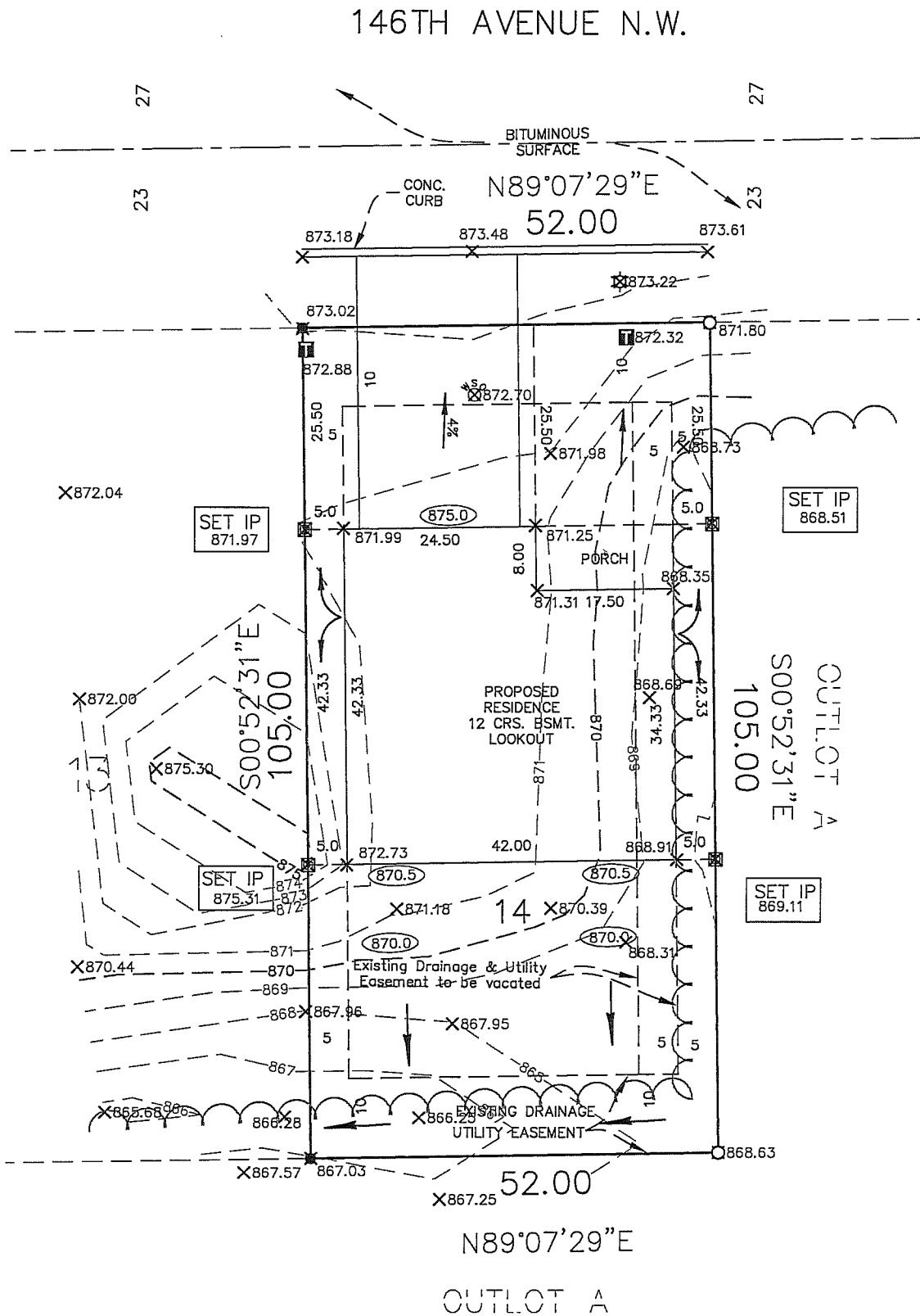
Lowest Floor Elevation: 868.0

Top of Block Elevation: 876.0

*Garage Slab Elevation: 875.0

Lookout Elevation: 871.0 min as per grading plan

*Drop Garage Slab One Course:



LEGEND

- = Iron monument found
- = Iron monument set and marked with license No. 18420.
- x 800.0 = Denotes existing elevation
- 800.0 = Denotes proposed elevation from grade or development plan
- 5 — = Denotes drainage & utility easement
- = Denotes drainage arrow
- ⊠ = Denotes offset iron
- Bearings shown are assumed
- ~ 800.0 ~ = Denotes proposed contour from development or grading plan
- 800.0 = Denotes as-built elevation
- SF — = Denotes proposed silt fence

Lot 14 , Block 2

VILLAGE OF SUNFISH LAKE

2ND ADDITION

ANOKA COUNTY, MINNESOTA
Subject to easements of record, if any.

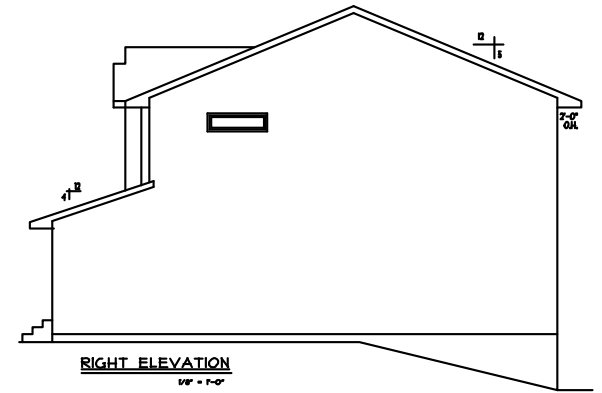
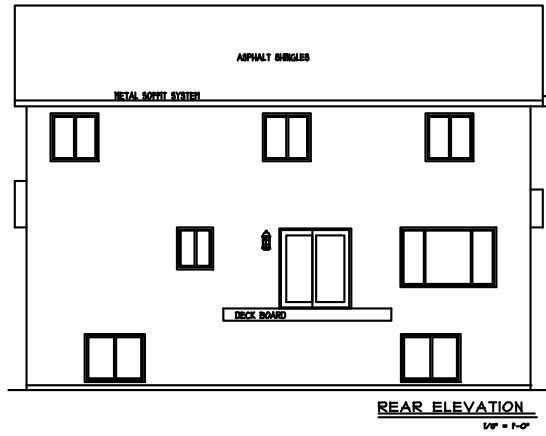
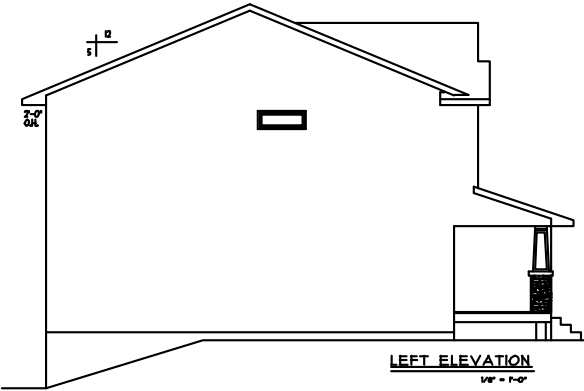
I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Land Surveyor under the laws of the State of Minnesota.

Charles R. Christopherson
Charles R. Christopherson, MN License No. 18420

11/02/12
Date



Job No.: 3769.01
Nov 02, 2012 - 2:25pm brian
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ORDINANCE NO. 12-__

**CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

AN ORDINANCE VACATING A PORTION OF AN EASEMENT FOR DRAINAGE AND UTILITY PURPOSES IN THE CITY OF RAMSEY, ANOKA COUNTY, MINNESOTA

The City of Ramsey Ordains:

SECTION 1. AUTHORITY

This ordinance is adopted pursuant to and under the authority of the Home Rule Charter of the City of Ramsey, Section 12.06 and Minnesota Statutes 412.851

SECTION 2. VACATION

The following described drainage and utility easement is hereby vacated, to-wit:

The west 5.00 feet of the east 10.00 feet of Lot 14, Block 2, VILLAGE OF SUNFISH LAKE 2ND ADDITION, Anoka County, Minnesota, lying northerly of the south 10.00 feet and southerly of the north 10.00 feet thereof.

SECTION 3. EFFECTIVE DATE

This Ordinance becomes effective thirty (30) days after its publication subject to the City Charter Section 5.07.

PASSED by the City Council of the City of Ramsey this the ___ day of _____, ____.

Mayor

ATTEST:

City Administrator

Introduction Date:
Posting Dates:
Adoption Date:
Publication Date:
Effective Date:

Meeting Date: 11/27/2012

Submitted For: Kurt Ulrich

By: Bill Goodrich, Administrative Services

Information

Title:

Consider entering into a purchase agreement for the property located at 8019 -146th Avenue NW, Minnesota owned by USCO Corp. -**PORTIONS OF THIS DISCUSSION MAY BE CLOSED TO THE PUBLIC.**

Background:

Earlier this year the Council authorized an appraisal of the USCO site and subsequently authorized the acquisition of this property for the Sunwood Drive/Realignment Project. The owner is requesting an increased sale price over the appraised value.

Observations:

Staff has negotiated with USCO Corp and would like to review the status of these negotiations and discussions with our appraiser with the Council. Pursuant to Minn. Stat. §13D.05 Subd. 3(c), the City Council may close portions of the meetings to consider/develop offers for the purchase of real property. Staff recommends that portions of the meeting be closed to discuss the offer to sell details and the status of the pending negotiations. Staff would like council direction on responding to the owner's request. When a final agreement is reached between the parties, the actual purchase price will need to be approved at an open meeting of the Council.

Funding Source:

Previously approved Sunwood Drive Alignment Funding Package.

Council Action:

Motion to close the meeting to consider/develop a final offer to purchase the real property located at 8019 - 146th Avenue NW, the closed meeting is authorized pursuant to Minn. Stat. §13D.053(c).

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date

11/21/2012 11:45 AM

Form Started By: Bill Goodrich

Started On: 11/20/2012 03:17 PM

Final Approval Date: 11/21/2012

CC Regular Session

7. 6.

Meeting Date: 11/27/2012

Submitted For: Kurt Ulrich

By: Bill Goodrich, Administrative Services

Information

Title:

Attorney-Client Privileged Discussion Regarding the pending Isanti County District Court lawsuit titled "County of Isanti, Plaintiff, vs. Keith Kiefer, Defendant, vs. City of Ramsey and City Administrator, Kurtis Ulrich, in his official capacity and individually, Third Party Defendants."

Background:

The City Council will meet with its legal council to discuss the pending case. - This is an Attorney-Client Privileged Discussion and this portion of the meeting will be Closed to the Public as Allowed by Minnesota Statutes, Section 13D.015, Subd. 3(b) in order to discuss litigation strategy regarding this pending lawsuit.

Recommendation:

Based on Discussion

Funding Source:

Not Applicable

Council Action:

Based on Discussion

Form Review

Inbox
Kurt Ulrich

Reviewed By
Kurt Ulrich

Date
11/21/2012 11:45 AM
Started On: 11/20/2012 03:34 PM

Form Started By: Bill Goodrich

Final Approval Date: 11/21/2012