

**City of Ramsey**  
**Agenda**  
**Regular Planning Commission**  
**Thursday August 6, 2015**  
**7:00 pm**  
**Council Chambers, 7550 Sunwood Drive NW**

1. **Call to Order**
2. **Citizen Input**
3. **Approve Agenda**
4. **Approve Minutes**
  1. Approve the July 9, 2015 Planning Commission Meeting Minutes  
Approve the July 9, 2015 Special Planning Commission Meeting Minutes
5. **Public Hearing/Commission Business**
  1. PUBLIC HEARING: Consider Recommendation on Resolution #15-08-191 Approving Comprehensive Plan Amendment and Ordinance #15-14 Approving a Zoning Amendment for 14100 Sunfish Lake Blvd. NW
  2. Consider Request for Site Plan Amendment to Remove Patio Area/Walls at 14787, 14851, and 14875 Olivine St NW; Case of Distinctive Living Rentals, LLC
  3. Review Comparison Document of The COR Development Plan and Original Ramsey Town Center Master Plan Prepared by the Planning Commission
  4. Discuss Potential Future City Code Amendment to Construction Hours
  5. Receive Future Business Park Infrastructure Analysis Preliminary Report
6. **Commission/Staff Input**
  1. Zoning Bulletins
7. **Adjournment**

**Regular Planning Commission**

**4. 1.**

**Meeting Date:** 08/06/2015

**By:** JoAnn Shaw, Community Development

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**Information**

**Title:**

Approve the July 9, 2015 Planning Commission Meeting Minutes  
Approve the July 9, 2015 Special Planning Commission Meeting Minutes

**Purpose/Background:**

n/a

**Notification:**

**Observations/Alternatives:**

**Funding Source:**

**Recommendation:**

**Action:**

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**Attachments**

[07.09.15 Minutes](#)

[07.09.15 Special Meeting Minutes](#)

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**Form Review**

**Inbox**

Tim Gladhill

Form Started By: JoAnn Shaw

Final Approval Date: 07/31/2015

**Reviewed By**

Tim Gladhill

**Date**

07/31/2015 02:09 PM

Started On: 07/27/2015 11:44 AM

**PLANNING COMMISSION  
CITY OF RAMSEY  
ANOKA COUNTY  
STATE OF MINNESOTA**

The Ramsey Planning Commission conducted a regular meeting on Thursday, July 9, 2015, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present:                   Chairperson Gary Levine  
                                          Commissioner Randy Bauer  
                                          Commissioner Matthew Maul (arrived at (6:02 p.m.)  
                                          Commissioner Cindy Nosan  
                                          Commissioner Gary VanScoy

Members Absent:                   Commissioner Andrew Andrusko  
                                          Commissioner Ralph Brauer

Also Present:                       Community Development Director Timothy Gladhill  
                                          City Planner Chris Anderson

**1.     CALL TO ORDER**

Chairperson Levine called the regular meeting to order at 6:00 p.m.

**2.     CITIZEN INPUT**

None.

**3.     APPROVAL OF AGENDA**

Motion by Commissioner VanScoy, seconded by Commissioner Bauer, to approve the agenda as presented.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Bauer, and Nosan.  
Voting No: None. Absent: Commissioners Andrusko, Brauer and Maul.

**4.     APPROVE PLANNING COMMISSION MINUTES**

**4.01: Approve the Following Planning Commission Minutes:**

**4.01.1: Planning Commission Meeting Minutes Dated June 4, 2015**

Commissioner VanScoy requested that his and Commissioner Bauer's vote on the St. Catherine issue be changed from absent to abstain.

Motion by Commissioner VanScoy, seconded by Commissioner Bauer, to approve the following minutes as amended: Planning Commission Meeting Minutes dated June 4, 2015.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Bauer, and Nosan. Voting No: None. Absent: Commissioners Andrusko, Brauer and Maul.

## **5. PUBLIC HEARINGS/COMMISSION BUSINESS**

### **5.01: PUBLIC HEARING: Request for a Conditional Use Permit to Exceed Sign Size Restrictions at 6228 Highway 10 NW; Case of Country Side Services of Minnesota, Inc.**

#### **Public Hearing**

Chairperson Levine called the public hearing to order at 6:02 p.m.

#### **Presentation**

Community Development Director Gladhill presented the staff report stating the City has received an application from the Country Side Services of Minnesota, Inc. requesting a conditional use permit (CUP) to exceed sign size restrictions for their parcel located at 6228 Highway 10 NW. The subject property is vacant and located directly adjacent to the east of Country Side Services' current site. The applicant desires to add a twenty-eight (28) square foot dynamic display sign to an existing 105 square foot sign. Staff finds the request reasonable for the Highway 10 Corridor and recommends approval of the CUP to exceed the square footage limitations for a ground sign in a business type district.

#### **Citizen Input**

There was no Citizen Input.

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to close the public hearing.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Chairperson Levine closed the public hearing closed at 6:04 p.m.

#### **Commission Business**

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to recommend that City Council adopt Resolution #15-07-169 approving Findings of Fact #0951.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to recommend that City Council adopt Resolution #15-07-170 relating to Country Side Services of Minnesota, Inc's request for a Conditional Use Permit to exceed sign size restrictions established by City Code.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

**5.02: PUBLIC HEARING: Consider Request for a Variance to Minimum Lot Size to Facilitate a Lot Split at 16520 Germanium Street NW: Case of Paul and Mary Johnson**

**Public Hearing**

Chairperson Levine called the public hearing to order at 6:05 p.m.

**Presentation**

City Planner Anderson presented the staff report stating the City received an application from Paul and Mary Johnson for a variance to the minimum lot size in the R-1 Residential (Rural Developing) district as well as the Scenic River Overlay District. Ultimately, the applicant desires to complete a lot split of their property located at 16520 Germanium Street NW to create one (1) new buildable lot. However, prior to expending the necessary funds to prepare a Minor Plat, the applicants wanted to first see if a deviation from the minimum lot size requirement would be possible. Thus, should this request be approved, the applicant would still be subject to the standard Minor Plat application process outlined in City Code. Staff reviewed the request in further detail and recommend approval of the variance contingent upon completion of a wetland delineation that is approved by the LRRWMO and application providing evidence of a recorded drainage and utility easement for the rerouted ditch section or dedicating a drainage and utility easement that encumbers the rerouted ditch section.

**Citizen Input**

Paul Johnson, 16520 Germanium Street NW, thanked staff for the thorough report. He explained he has lived on this property and noted he would like to create a single level house.

Commissioner VanScoy asked if the homeowner would have access issues to the newly created lot.

Mr. Johnson did not believe this would be a problem.

City Planner Anderson reported that a wetland delineation would have to be completed on the new lot prior to construction. He did not anticipate that the home or driveway would impact the

wetlands. If an impact were to happen, the applicant would have to work with the LRRWMO. He provided further comment on the LRRWMO meeting schedule. He reported that the applicant's variance would last for one year and after that time, an extension could be requested.

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to close the public hearing.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Chairperson Levine closed the public hearing closed at 6:17 p.m.

### **Commission Business**

Commissioner Bauer asked how the easement would be addressed.

City Planner Anderson commented that the easement would be handled by the applicant through the minor platting process.

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to adopt Resolution #15-07-162 approving Findings of Fact #0948.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Motion by Commissioner Bauer, seconded by Commissioner VanScoy, to adopt Resolution #15-07-163 approving a Variance to minimum lot size on the property located at 16520 Germanium Street NW contingent upon:

- Completion of a wetland delineation that is approved by the LRRWMO; and
- Application providing evidence of a recorded drainage and utility easement for the rerouted ditch section or dedicating a drainage and utility easement that encumbers the rerouted ditch section

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Bauer, VanScoy, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

### **5.03: PUBLIC HEARING: Consider Request for a Variance to Driveway Setback Standards on the Property Located at 17209 Tungsten Street NW**

### **Public Hearing**

Chairperson Levine called the public hearing to order at 6:19 p.m.

### **Presentation**

City Planner Anderson presented the staff report stating the City has received an application from Frank and Dorothy Frederick for a Variance to the required setback for a driveway on the property located at 17209 Tungsten Street NW. The applicant is requesting a variance to allow a new concrete driveway to be installed up to the side lot line (extending a line event with the front wall of an attached garage) and to continue use of an existing gravel surface along the side of the attached garage for parking purposes. Staff reviewed the request in detail with the Commission and recommended approval of the variance to the driveway setbacks for a portion of the driveway but not the entire length of it. He commented that staff approved of the 41-foot length of the variance as requested by the applicant.

### **Citizen Input**

Frank Frederick, 17209 Tungsten Street NW, explained that he purchased his home last August and noted he was planning to upgrade his driveway. He discussed the turning radius required to park his motorhome alongside his garage and requested the 41 feet be approved by the Planning Commission.

Motion by Commissioner VanScoy, seconded by Commissioner Bauer, to close the public hearing.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Bauer, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Chairperson Levine closed the public hearing closed at 6:29 p.m.

### **Commission Business**

City Planner Anderson stated the drainage and utility easement required an encroachment agreement. He reported that the applicant was aware of this fact and staff would work with him to prepare the agreement, which would be reviewed by the City Council.

Motion by Commissioner VanScoy, seconded by Commissioner Bauer, to adopt Resolution #15-07-164 approving Findings of Fact #0949.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Bauer, Maul, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Motion by Commissioner VanScoy, seconded by Commissioner Maul, to adopt Resolution #15-07-165 approving a Variance to allow a portion of the driveway to be up to the side lot line on the property located at 17209 Tungsten Street NW contingent upon executing an Encroachment Agreement with the City and amended by the applicant's Site Plan dated July 9, 2015.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Maul, Bauer, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

**5.04: PUBLIC HEARING: Consider Request for Conditional Use Permit for Auto Sales in a B-2 Highway Business District; Case of Dumark's Auto Sales, LLC**

**Public Hearing**

Chairperson Levine called the public hearing to order at 6:32 p.m.

**Presentation**

Planning Intern Solomonson presented the staff report stating the City received an application from Dumark's Auto Sales, LLC requesting a conditional use permit (CUP) to allow auto sales on the property at 6650 Highway 10 NW. The applicant currently conducts auto sales at 7127 Highway 10 NW and plans to relocate to the subject property. The subject property currently houses Twin Cities Pawn in the northern section of the structure and the applicant would be leasing the southern portion, on the condition that the current tenant for the southern portion has officially vacated before the applicant's occupation. Staff reviewed the request further with the Commission and recommended approval of the request for a conditional use permit to allow auto sales in a B-2 Highway Business District.

**Citizen Input**

Commissioner Bauer asked how many parking spaces would be set aside for employees.

Planning Intern Solomonson reviewed the parking spaces on the Site Plan that were designated for employees.

Ryan Duccette, Dumark's Auto Sales, explained that he was interested in relocating his business. He discussed his niche market was in used auto sales. He indicated that he was interested in keeping his business within the City of Ramsey.

Motion by Commissioner VanScoy, seconded by Commissioner Maul, to close the public hearing.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Maul, Bauer, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Chairperson Levine closed the public hearing closed at 6:36 p.m.

**Commission Business**

Motion by Commissioner Maul, seconded by Commissioner Nosan, to recommend that City Council adopt #15-06-158 approving Findings of Fact #0947.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Maul, Nosan, Bauer, and VanScoy. Voting No: None. Absent: Commissioners Andrusko and Brauer.

Motion by Commissioner Maul, seconded by Commissioner Nosan, to recommend that City Council adopt Resolution #15-06-159 relating to Dumark's Auto Sales, LLC request for a Conditional Use Permit to allow auto sales in a B-2 Highway Business District.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners Maul, Nosan, Bauer, and VanScoy. Voting No: None. Absent: Commissioners Andrusko and Brauer.

## **6. COMMISSION / STAFF INPUT**

Staff then provided the Commission with an update on the development taking place in and around the City.

Commissioner Bauer was in favor of reviewing the City's Sign Ordinance given the number of variances that were being reviewed by the Planning Commission.

Community Development Director Gladhill understood this concern and noted staff was also interested in reviewing the Sign Ordinance.

### **6.01: Zoning Bulletins**

Zoning Bulletins were noted.

## **7. ADJOURNMENT**

Motion by Commissioner VanScoy, seconded by Commissioner Maul, to adjourn the meeting.

Motion Carried. Voting Yes: Chairperson Levine, Commissioners VanScoy, Maul, Bauer, and Nosan. Voting No: None. Absent: Commissioners Andrusko and Brauer.

The regular meeting of the Planning Commission adjourned at 6:47 p.m.

Respectfully submitted,

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Tim Gladhill  
Community Development Director

ATTEST:

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JoAnn Shaw  
Community Development Assistant

Drafted by Heidi Guenther  
*TimeSaver Off Site Secretarial, Inc.*

**PLANNING COMMISSION  
CITY OF RAMSEY  
ANOKA COUNTY  
STATE OF MINNESOTA**

The Ramsey Planning Commission conducted a special work session on Thursday, July 9, 2015, in the Lake Itasca Room at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Chairperson Gary Levine  
Commissioner Randy Bauer  
Commissioner Matthew Maul  
Commissioner Cindy Nosan  
Commissioner Gary VanScoy

Members Absent: Commissioner Andrew Andrusko  
Commissioner Ralph Brauer

Also Present: Community Development Director Tim Gladhill  
City Planner Chris Anderson  
Assistant City Administrator/Economic Development Manager Patrick Brama  
Community Development Intern Geoff Solomonson

**1. CALL TO ORDER**

Chairperson Levine called the regular meeting to order at 7:03 p.m.

**2. APPROVAL OF AGENDA**

Motion carried by Commissioner Bauer, seconded by Commissioner VanScoy, to approve the agenda as presented.

Motion Carried.

**3. MINUTES**

Community Development Director Gladhill explained the purpose of the special work session was to develop a comparison document of the COR Development Plan and the Original Ramsey Town Center Master Plan.

He explained how at the May 26, 2015 Joint Meeting with the City Council, Staff was directed to prepare a comparison document and made the decision to touch base with the commission. The COR vision is very different from the past ideas, but headed in the right direction. He asked if there were to be changes to the vision and ask “what needs to change?”

Community Development Director Gladhill described the history of the COR which originated from the 2020 Comprehensive Plan (1998-2001). In the plan was described how the area was an agricultural use and due to growth forecasts, Ramsey decided to protect its rural areas and concentrate this new growth into a more centralized area.

The question arose of what is a mixed-use development. In the 2001 general election, an advisory question for the ballot was developed, essentially a survey question for residents. The question was “Do you want a Town Center with restaurants, shopping, and other amenities along the Highway 10 Corridor?” This was around the same time as the Northstar Commuter Rail transitway planning.

Between 2009-2010, the City began a process to provide a new vision for the development to better match current market conditions at the time, the revision of the plan to the COR plan today.

Community Development Director Gladhill then asked the commissioners to choose their top 3 components of the original master plan for Ramsey Town Center.

### **ORIGINAL MASTER PLAN (PREVIOUS)**

Commissioner Nosan asked what the “West 30” referred to in the original plan.

Community Development Director Gladhill described the area to the North and West of the roundabout at Sunwood Drive guided for retail.

Commissioner VanScoy asked about redevelopment and the current zoning of the West 30.

Community Development Director Gladhill explained the vision as a more traditional suburban retail center, while still maintaining a higher degree of architectural standards.

Discussion ensued about parking standards for traditional suburban retail.

Commissioner Bauer explained how he wrote down shared parking, such as the ramp next to the Municipal Center [and apartment complex] and how it is critical not to lose the idea of shared parking.

Commissioner VanScoy explained how density requires parking.

Commissioner Nosan responded how the church [Northgate Church] is a great example of successful shared parking.

Mixed-Use was the next topic. Community Development Director Gladhill asked if it meant vertical mixed use within a single building or different types of single-use buildings within a single block.

Chairperson Levine commented it did not mean different uses within a single block.

Commissioner Nosan expressed concern about too much mixed-use in the area. She explained currently it seems to be only the residents actually living in the COR who use the area.

Commissioner Bauer described the apartment complex connected to the Municipal Parking Ramp and how the City Council originally wanted retail on the first floor of the complex. The developer did not want retail on the first floor. Commissioner Bauer then explained without a large store/attraction there is no market for mixed-use commercial in this area.

Commissioner Nosan remarked how the residents of Ramsey are always going to have cars and is what the culture of the town is used to. She explained how Ramsey will never be Uptown [Minneapolis].

Community Development Director Gladhill then clarified the consensus on the meaning of mixed-use for the original plan was vertical mixed use. Gladhill then explained that the City would continue to discuss the current definition in the next step, and perhaps continue to discuss this as a policy question moving forward.

Commissioner Maul explained how mixed-use development works very well in Minneapolis and Uptown because there is limited space, whereas there is still plenty of land in Ramsey.

Community Development Director Gladhill then went through the remaining components of architectural standards, which was split into the architecture of the building itself as well as the architectural review, density; strong blue system, of which The Draw is itself a utility and amenity; and the Master Park Plan.

Commissioner Bauer explained he had written the strong blue system and wanted to see it developed further. He then said water re-use is important and water is unique to Ramsey because of the two rivers. He said he would like to see developments with ideas like fountains using rainwater.

City Planner Anderson explained The Draw uses storm water and is recycled through the park surrounding it.

Chairperson Levine then remarked about how the West 30 idea has changed from the medium-density residential idea to now [retail].

Community Development Director Gladhill then asked the commissioners if there was any components of the Original Master Plan they would like to strike from or add to the list.

Commissioner Maul stated he would like to see the strict sign standards removed.

Chairperson Levine agreed and said he would like to see more relaxed sign standards.

Chairperson Levine spoke of how there was an educational component to the Original Master Plan in buildings being a multi-use, multi-educational facility for shared resources and how he would like to see that brought back in.

Commissioner VanScoy remarked how the residential density was a top priority in the old plan, but not in the new (COR plan). He explained how the old plan was to drive people into the center with a transit-oriented development and build a population of enough people to support retail and subsequently encourage those living in the outer areas to drive in to use the amenities as well.

Commissioner Bauer remarked how he had second thoughts on the retail area in the southwest are of the COR. He explained how the type of retail should be changed to a unique destination type, similar to Stillwater [MN] with craft and antique stores.

Chairperson Levine agreed stating Stillwater is a much better example for Ramsey to follow than Arbor Lakes [Maple Grove].

### **THE COR DEVELOPMENT PLAN (CURRENT)**

Community Development Director Gladhill then asked the commissioners to repeat the process with their top 3 components of the current COR development plan. He explained the similarities of the plans, but some changes occurred when they tried to apply it, such as the West 30 [retail vs residential], eliminating the architectural review, and reducing the floor to area ratio (FAR).

The first topic was the definition of mixed-use. Community Development Director Gladhill explained the change from vertical mixed-use to many single uses within a block.

Commissioner VanScoy agreed the definition had changed, but asked what is considered a block.

Community Development Director Gladhill explained a block is an area surrounded by public or private streets, but could use a better definition [for this plan].

Chairperson Levine made the comment how less is happening in the COR now.

Community Development Director Gladhill explained how less vertical mixed-use is happening, but there is still activity and interest. Gladhill noted that mixed-use is still a goal of the community, however the community has experienced more residential growth compared to retail.

Chairperson Levine remarked how less four (4) story structures and more one (1) story structures equals less activity, or density. He then said less activity equals less rooftops (residents).

Commissioner VanScoy remarked how mixed-use in the COR now seems to be all residential, something different which wants to be built seems like it needs a variance. He then asked how you plan to get the mixed use [in the future] which we want.

Commissioner Maul agreed adding scale should be asked in addition to Commissioner VanScoy's question. He then remarked if we want vertical, we need to plan for it now.

Community Development Director Gladhill then explained more of the components as architectural standards & relaxed sign standards and parking.

Chairperson Levine made the comment how shared parking does not always work.

Commissioner Maul agreed adding and asking again, to what scale.

Commissioner VanScoy remarked how there was no strategy to implementing the shared parking [in the original plan].

Chairperson Levine asked how we pay for parking.

Community Development Director Gladhill explained that the City has had a strategy to implement shared parking, but does require an up front investment and has required some form of public assistance in the past in Ramsey's examples, namely through grant awards. Gladhill reviewed the history of the Ramsey Municipal Ramp and the feedback the City originally received in the early stages of the development before uses such as Residence at The COR was completed.

Commissioner VanScoy remarked how there needs to be a new strategy to shared parking because parking supports density and retail.

Commissioner Bauer explained how the original plan had plans for a second parking ramp. He then explained how it should be pursued in a development which has the developer paying half and the City paying half.

Commissioner VanScoy asked how we define parking counts and how there are conflicting methods for parking counts/requirements.

Community Development Director Gladhill then went through the rest of the components on the board which were the West 30; transit; storm water; density; and streetscape. He then remarked how the streetscape portion has been redesigned different than the original plan, especially Ramsey Parkway. He explained how in the original plan the Parkway continued west with landscape. He then asked what components should be in the streetscape.

Chairperson Levine remarked how Ramsey Parkway was an attraction on the original plan.

Community Development Director Gladhill asked the commissioners if there were anything in the COR plan that they would like to strike out or add to the list.

No commissioners had anything to strike from or add to the list.

Chairperson Levine asked what the 'strong downtown feel in center' meant.

Chairperson Levine expressed concern about the West 30.

## **FUTURE POLICY QUESTIONS**

Community Development Director Gladhill asked if there were additional questions to add to the list of policy questions from city staff [handout].

Chairperson Levine asked about McDonalds, remarking on the question of whether Ramsey wants to continue to allow uses with a drive-thru and gas station. He said McDonalds certainly has a drive-thru and where they go, other restaurants [and retail] will follow.

Commissioner Bauer responded with the reconstruction of Armstrong [Boulevard], it will have the effect of getting people off Highway 10, for example weekends when gas, groceries, and meals are needed on the way [in and out of the city and metropolitan area].

Community Development Director Gladhill responded that drive-thru and gas station type business should be acceptable in portions because Ramsey needs and wants those types of businesses.

Commissioner VanScoy remarked how the plan for the places of employment (southeast quadrant of the COR) needs to be reevaluated.

Community Development Director Gladhill responded, adding with the question, what are appropriate uses within the business park.

Commissioner Bauer responded with the fiber-optic in place, it is not marketed towards retail, but should be marketed towards others as job opportunities.

Chairperson Levine asked for clarification on the second and third policy questions. [2. Are there uses we are not allowing that we do not want to allow? 3. Are there uses we are allowing that we do not want to allow?]

Commissioner Nosan asked how we are not allowing uses we do not want when we do not know what those uses are.

Chairperson Levine remarked [question 3.] was saying basically what does [the City] not want here.

Commissioner Nosan responded saying she does not want the COR to turn into a solid line of bars like Jackson Street in Anoka.

Chairperson Levine responded noting that there may be uses we do not want to allow within the development, and that the City should review uses that are currently allowed as part of this process.

Community Development Director Gladhill remarked the definition might be too broad.

Commissioner VanScoy asked to look at the sub-districts [of the COR], what is their intent, and what do they mean.

Chairperson Levine added to Commissioner VanScoy's question with what the sub-district was and what it should be.

Commissioner Nosan asked Assistant City Administrator/Economic Development Manager Brama about the possibility of Target in Ramsey. She spoke of her displeasure of driving to Elk River or Coon Rapids just to visit the store.

Assistant City Administrator/Economic Development Manager Brama explained how Target in Ramsey comes down to logistics and access. He then went to say there is interest from business based on the Armstrong [Blvd] interchange, but they are waiting for 'more rooftops' in the community. He said it comes down to the economy and how Ramsey is surrounded by communities that have a Target, for example.

Community Development Director Gladhill explained that lack access across the Mississippi River to Dayton and across the Rum River reduces the market area or drive-time for retail users.

Assistant City Administrator/Economic Development Manager Brama added when speaking to a broker, they discussed how Ramsey is an island within the area, in terms of access [the lack of current highway 10 freeway] and connections [across the Mississippi] to other communities. He

also went to say there is a possibility retail is too saturated with the amount available due to the restricted access and connections.

Community Development Director Gladhill explained how it would be helpful to see existing examples, local examples, or other retail areas. He suggested a tour with stops including Silver Lake Village in St. Anthony, Arbor Lake in Maple Grove, and Riverdale Village in Coon Rapids. These stops would give the commission and idea of what they do and do not want in the COR.

Commissioner Nosan asked Assistant City Administrator/Economic Development Manager Brama asking if retail has a hard time coming here.

Assistant City Administrator/Economic Development Manager Brama responded saying the Armstrong [Blvd] interchange could help and could also possibly be the domino that falls [propelling growth].

Commissioner VanScoy asked where the City and County stand in regards to the Dayton Bridge [Mississippi River crossing].

Community Development Director Gladhill responded saying [Ramsey's] comprehensive plan and [the City of ] Dayton's comprehensive plans both have it listed and both plans match, even to the degree of prioritizing a corridor from Highway 10 to Interstate 94.

Commissioner VanScoy asked if Hennepin County and Anoka County were doing anything in regards towards completing this matter.

Community Development Director Gladhill explained how Anoka County has purchased land in anticipation of the connection, and the connection lines up in both the City of Ramsey and the City of Dayton, but the timeline for this project is undetermined as it is not on the State [of Minnesota] funding plans or MNDOT's plans.

Community Development Director Gladhill told the commission how staff would refine the document based on the responses that evening plus other future meetings, eventually to bring back to the Planning Commission and the City Council.

Commissioner Bauer asked where the planning staff stood with the upcoming comprehensive plan.

Community Development Director Gladhill responded saying the plan is being approached in smaller sections and the information in the meeting that evening would be incorporated into the future document. He then went on to explain that as the smaller section become developed, citizen engagement opportunities and feedback would be initiated closer to fall.

#### **4. ADJOURNMENT**

Motion to adjourn the meeting.

Motion Carried.

The special work session of the Planning Commission adjourned at 8:20 p.m.

Respectfully submitted,

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Tim Gladhill  
Community Development Director

ATTEST:

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JoAnn Shaw  
Community Development Assistant

Drafted by Geoff Solomonson

DRAFT

**Regular Planning Commission**

**5. 1.**

**Meeting Date:** 08/06/2015

**By:** Tim Gladhill, Community Development

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**Information**

**Title:**

PUBLIC HEARING: Consider Recommendation on Resolution #15-08-191 Approving Comprehensive Plan Amendment and Ordinance #15-14 Approving a Zoning Amendment for 14100 Sunfish Lake Blvd. NW

**Purpose/Background:**

The purpose of this case is to consider two (2) separate, but related, actions:

1. Comprehensive Plan Amendment from Places to Shop to Places to Work
2. Zoning Amendment from B-1 General Business District to E-1 Employment District

The City has been approached by a potential buyer for the parcel, which is commonly known as the former Health Quest site. The Buyer is proposing a use allowable under the E-1 Employment District. The Buyer is considering submitting a request to the City under the City's Business Subsidy Policy, which would require the Planning Commission to adopt a resolution finding that the proposed use is consistent with the Comprehensive Plan. In order to pass said resolution, an amendment to the City's Comprehensive Plan is necessary. The City's Zoning Code/Official Zoning Map must be consistent with the Comprehensive Plan, so a parallel request for a Zoning Amendment is also included.

**Notification:**

The Public Hearing Notice was advertised in the Anoka County UnionHerald. Additionally, Staff attempted to notify all Property Owners within 350 feet of the Subject Property.

**Observations/Alternatives:**

Observations

This amendment will actually revert the Land Use Map and Zoning District back to the original, pre-2007 designations. In 2007, the City received and approved a Comprehensive Plan Amendment, Zoning Amendment, and Site Plan for a multi-tenant retail redevelopment of the former Health Quest site. However, said redevelopment never materialized and the site went into foreclosure.

A new perspective buyer has approached the City to discuss an amendment to revert back to the original Zoning District. The proposed future manufacturing and warehouse building appears to be compatible with the surrounding uses, which are predominantly employment based with the exception of Casey's General Store. The current owner, First National Bank, is supportive of this change and feels that this is in line with the current market, regardless if there had been a current buyer.

Regarding the proposed future land use, the positive would be a potentially more immediate redevelopment of a site that has sat vacant for approximately ten (10) years. The negative of the proposed amendment could be larger, albeit fewer, truck traffic movements on two (2) County Roads.

Regarding the future land use as currently shown, the positives would be a higher per square foot tax base. The negatives would be additional comparable traffic and conflict points on two (2) county roads.

For privacy and negotiation purposes, the buyer wishes to remain anonymous for the time being. It is noted that regardless of actions on the Comprehensive Plan and Zoning Map, any future user must abide by current Zoning Code and other City Code provisions regarding development standards and process. As this would likely be a new

building, the Planning Commission and City Council are likely to see the actual project when it comes forward through Site Plan Review.

#### Alternatives

Alternative 1: Approve the amendments as presented. This will allow for the redevelopment of the parcel in a manner consistent with the abutting properties.

Alternative 2: Do not approve the amendments. This will mean that the City finds that a retail use is the most compatible use for the parcel and that the Property Owner will need to continue to pursue a retail user.

#### Funding Source:

The amendment is being handled as part of normal Staff duties.

#### Recommendation:

Staff recommends approval of the amendments.

#### Action:

Motion to adopt Resolution #15-08-191 approving a Comprehensive Plan Amendment and Ordinance #15-14 approving a Zoning Amendment for 14100 Sunfish Lake Blvd. NW.

---

#### Attachments

[Site Location Map](#)

[Future Land Use Map](#)

[Resolution #15-08-191](#)

[Ordinance #15-14](#)

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#### Form Review

##### Inbox

Chris Anderson  
Tim Gladhill (Originator)  
Form Started By: Tim Gladhill  
Final Approval Date: 07/31/2015

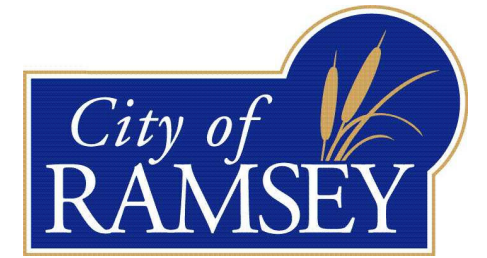
##### Reviewed By

Chris Anderson  
JoAnn Shaw

##### Date

07/31/2015 02:40 PM  
07/31/2015 02:47 PM  
Started On: 07/14/2015 09:34 AM

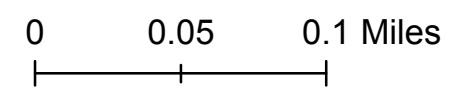




# 2030 Comprehensive Plan Future Land Use Map

### Future Land Use

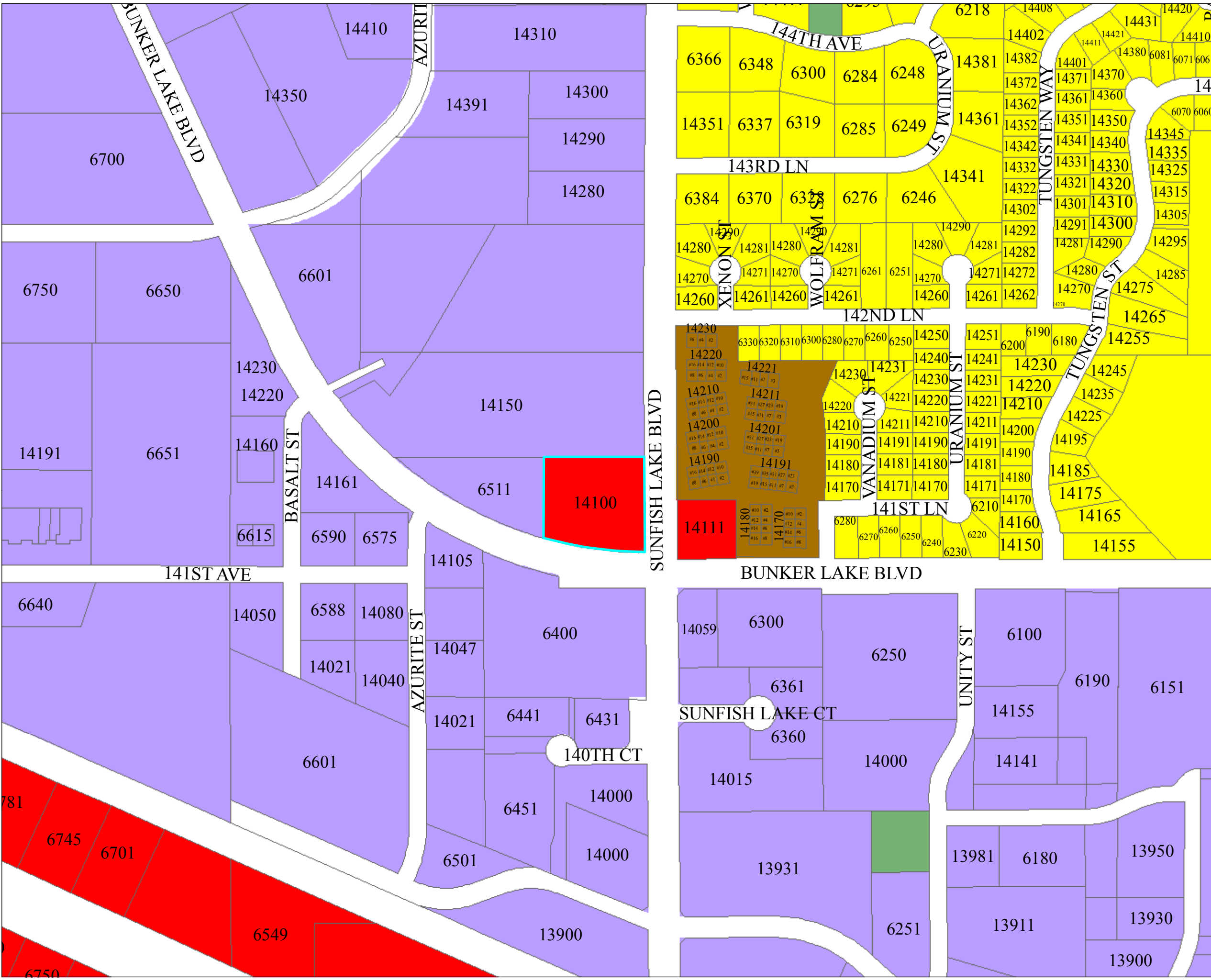
- Low Density Residential
- Medium Density Residential
- High Density Residential
- Office Park
- Commercial
- MU
- Business Park
- Public
- Rural Developing
- Rural Preserve
- Park
- MRCCA Boundary
- MUSA



This map has been compiled using information gathered from various governmental offices and other sources and is to be used for reference purposes only. It is neither a legally recorded map nor a survey and is not intended for use as one. The Geographic Information System (GIS) data used to develop this map is not warranted by the City as being error-free.

The City does not represent that the GIS data can be used for exact measurement of distance or direction or precision in the depiction of geographic features. If errors or discrepancies are found, please contact (763) 427-1410.

The City of Ramsey disclaims any responsibility for or liability for the accuracy of the information at any point of initial contact with a GIS to which the public has general access. The preceding disclaimer is provided pursuant to Minnesota Statute 466.03, Subd. 21 (2000), and the user of this map acknowledges that the City of Ramsey is immune from any and all claims brought by User, its employees or agents, or third parties which arise out of the user's access of the data.



Councilmember \_\_\_\_ introduced the following resolution and moved for its adoption:

**RESOLUTION #15-08-191**

**A RESOLUTION ADOPTING FINDINGS, APPROVING A COMPREHENSIVE PLAN AMENDMENT TO CHANGE A LAND USE DESIGNATION FROM PLACES TO SHOP TO PLACES TO WORK AND AUTHORIZING CITY STAFF TO SUBMIT AN APPLICATION TO THE METROPOLITAN COUNCIL FOR AN AMENDMENT TO THE COMPREHENSIVE PLAN**

**WHEREAS**, on August 4, 2015, the Planning Commission of the City of Ramsey held a Public Hearing on an amendment to the Comprehensive Plan to amend the future land use designation from Places to Shop to Places to Work and recommends approval of said amendment, for the following legally described property:

Lot 1, Block 1, GATEWAY NORTH INDUSTRIAL PARK PLAT 3

(the “Subject Property”)

**WHEREAS**, on October 9, 2007, the City approved a Comprehensive Plan Amendment at the request of the property owner at that time from Places to Work to Places to Shop based off a submitted site plan to redevelop the site as a convenience retail development; and

**WHEREAS**, the site plan approved in 2007 never materialized; and

**WHEREAS**, the City has been approached by a potential buyer of the Subject Property who proposes a manufacturing and warehouse facility, necessitating a Comprehensive Plan Amendment to Places to Work, which will actually revert back to the original designation and be consistent with abutting properties; and

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:**

- 1) That the Ramsey City Council hereby approves the request for a land use designation change of the Subject Property from Places to Shop to Places to Work.
- 2) That the Ramsey City Council hereby authorizes City Staff to submit the necessary documentation to Metropolitan Council for an amendment to the Comprehensive Plan to change the land use designation to Places to Work.
- 3) That this land use change shall become effective upon City Council ratification of Metropolitan Council action.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember \_\_\_\_\_, and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the 25<sup>th</sup> day of August, 2015.

---

Mayor

**ATTEST:**

---

City Clerk

**ORDINANCE #15-14**

**CITY OF RAMSEY  
ANOKA COUNTY  
STATE OF MINNESOTA**

**AN AMENDMENT TO CHAPTER 117 WHICH IS KNOWN AS THE ZONING AND SUBDIVISIONS CHAPTER OF THE CITY CODE OF RAMSEY, MINNESOTA.**

**AN ORDINANCE AMENDING SECTION 117-90 "MAP" OF CHAPTER 117 OF THE CITY CODE OF RAMSEY, MINNESOTA.**

**SECTION 1. AMENDMENT**

The following legally described properties or portions thereof, are hereby rezoned from B-1 General Business District to E-1 Employment District

Lot 1, Block 1, GATEWAY NORTH INDUSTRIAL PARK PLAT 3

(the "Subject Property")

**SECTION 2. MAP**

The City is hereby instructed to cause this amendment to be shown on the "City of Ramsey Zoning Map", which map was adopted pursuant to Section 117-90 of the Ramsey City Code.

**SECTION 3. EFFECTIVE DATE**

This ordinance becomes effective 30 days after its passage and publication, subject to City Charter Section 5.04.

PASSED by the City Council of the City of Ramsey, Minnesota the \_\_\_\_\_ day of \_\_\_\_\_, 2015.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Administrator

Introduction date:

Posting dates:

Adoption date:

Publication date:

Effective date:

**Regular Planning Commission**

5. 2.

**Meeting Date:** 08/06/2015

**By:** Geoff Solomonson, Community  
Development

---

**Information**

**Title:**

Consider Request for Site Plan Amendment to Remove Patio Area/Walls at 14787, 14851, and 14875 Olivine St NW; Case of Distinctive Living Rentals, LLC

**Purpose/Background:**

The purpose of this case is to consider a request for an amended Site Plan. The request is to eliminate an outdoor patio wall and concrete patio areas between the center units of three (3), eight-unit townhomes. Distinctive Living Rentals, LLC (the "Applicant") is currently constructing an eight (8) unit townhome building on on Lot 3, Block 1 Town Center Gardens 3rd Addition and also owns Lots 1 and 4, Block 1 Town Center Gardens 3rd Addition (together, the "Subject Property"). The Applicant is seeking this amendment of all three (3) parcels and has identified reasons for the requests as size (or lack there of for the patio areas), access, and placement of utilities.

**Notification:**

Notification is not required when considering an amendment to a site plan.

**Observations/Alternatives:**

The Applicant is seeking this amendment due to the limited size of the area, a six (6) foot by twelve (12) foot area, which will be further reduced once the A/C unit and individual gas meters in installed in this area. Gas meters were previously banked together on an end unit, but are now being placed in front of each individual unit to avoid the inefficiency of running gas lines out and around to each unit. The patio areas also have no direct ingress or egress from this space; rather, an occupant would need to walk out and around a wall to enter the patio area. The Applicant also stated that it was his opinion that that the space may also be unsuitable for using a grill, as it has a greater potential to melt the siding due to being a small space.

The Applicant is proposing to eliminate the impervious surfacing and replace it with turf grass and shrubs. Proposed landscaping will need to comply with the City's landscaping standards with regard to acceptable/preferred species and size (twenty-four [24] inches in height at time of planting for shrubs).

**Alternatives**

Option 1: Recommend City Council adopt Resolution #15-08-190 approving the request to amend the Site Plan. The size of each patio area is small and lacks direct ingress and egress. This area will be further reduced after the placement of the A/C unit and individual gas meters, leaving its functionality somewhat questionable. Thus, replacing this hardscape with landscaping may serve as a more desirable option to future occupants. Staff supports this option.

Option 2: Recommend that City Council deny the request. The requested amendment is minor in nature and does appear to significantly alter the exterior aesthetic of the building(s). The City would need to draft findings specifically identifying the reasoning to deny this request. Staff does not support this option.

**Funding Source:**

All costs associated with this request are the responsibility of the Applicant.

**Recommendation:**

Staff recommends approval of the request to amend the site plan to eliminate the patio division walls and concrete patio areas between the center units only on the Subject Property.

**Action:**

Motion to recommend that City Council adopt Resolution #15-08-190 approving the request to amend the site plan by replacing the patio areas and half wall between the center units on Lots 1, 3, and 4, Block 1 Town Center Gardens 3rd Addition.

---

**Attachments**

[Site Location Map](#)

[Site Plan](#)

[Images of Site](#)

[Landscape Plan](#)

[Resolution #15-08-190](#)

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**Form Review**

**Inbox**

Chris Anderson

Tim Gladhill

Form Started By: Geoff Solomonson

Final Approval Date: 07/31/2015

**Reviewed By**

Chris Anderson

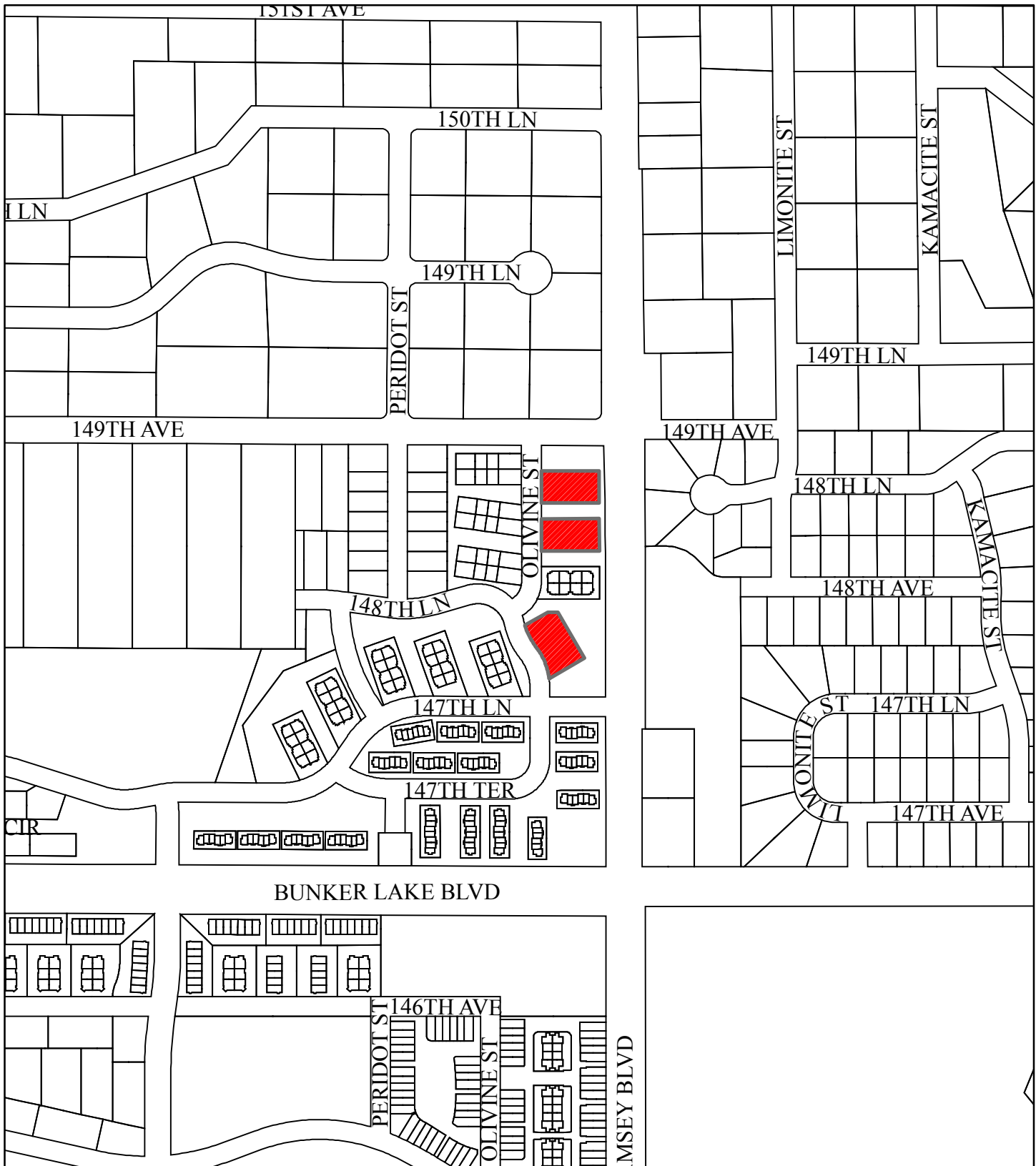
Tim Gladhill

**Date**

07/30/2015 05:12 PM

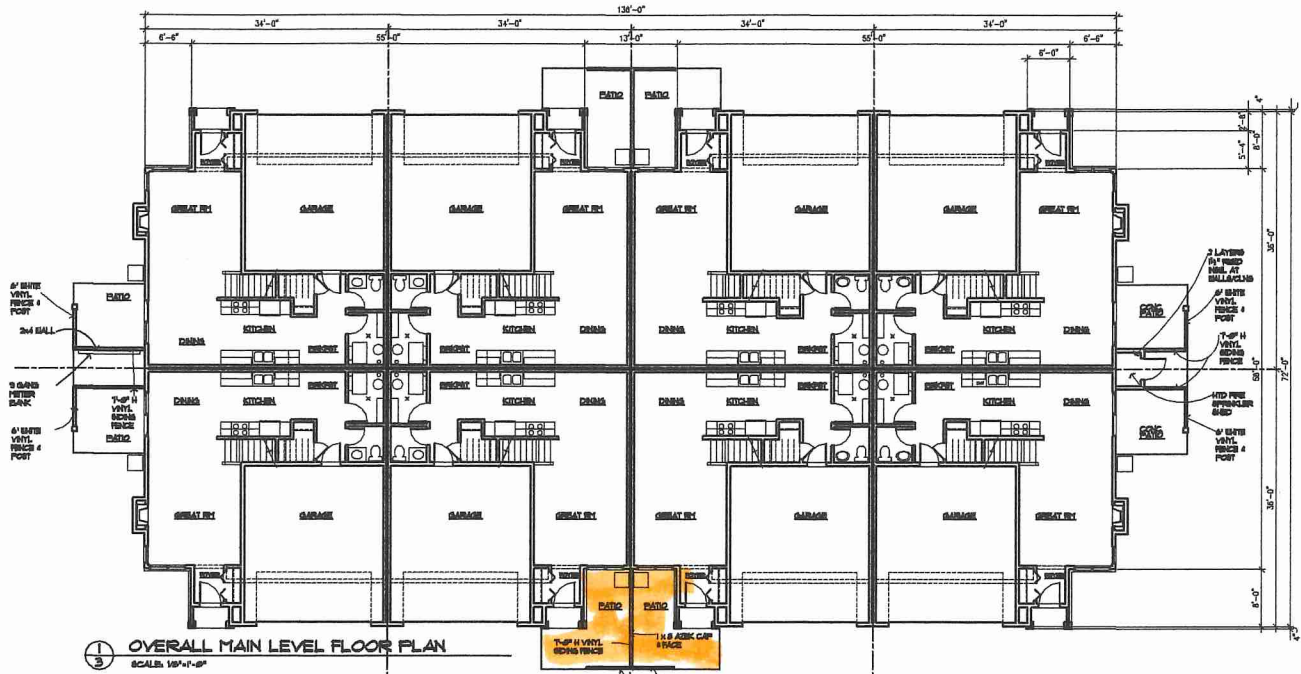
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Started On: 07/29/2015 09:45 AM

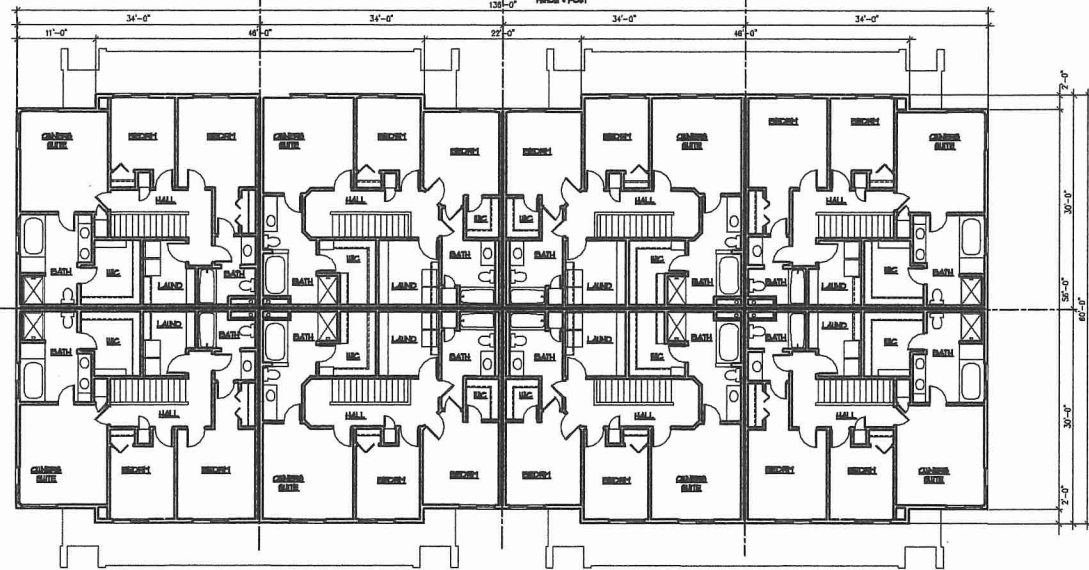


Distinctive Living Rentals, LLC





① OVERALL MAIN LEVEL FLOOR PLAN  
SCALE: 1/8"=1'-0"



② OVERALL UPPER LEVEL FLOOR PLAN  
SCALE: 1/8"=1'-0"



401 EAST LAKE STREET  
WATATA, MN 55391  
Phone: 952.473.8777  
Fax: 952.473.8222

DATE: 22 JANUARY 2015  
REVISION: NONE DATED  
BY: MVT

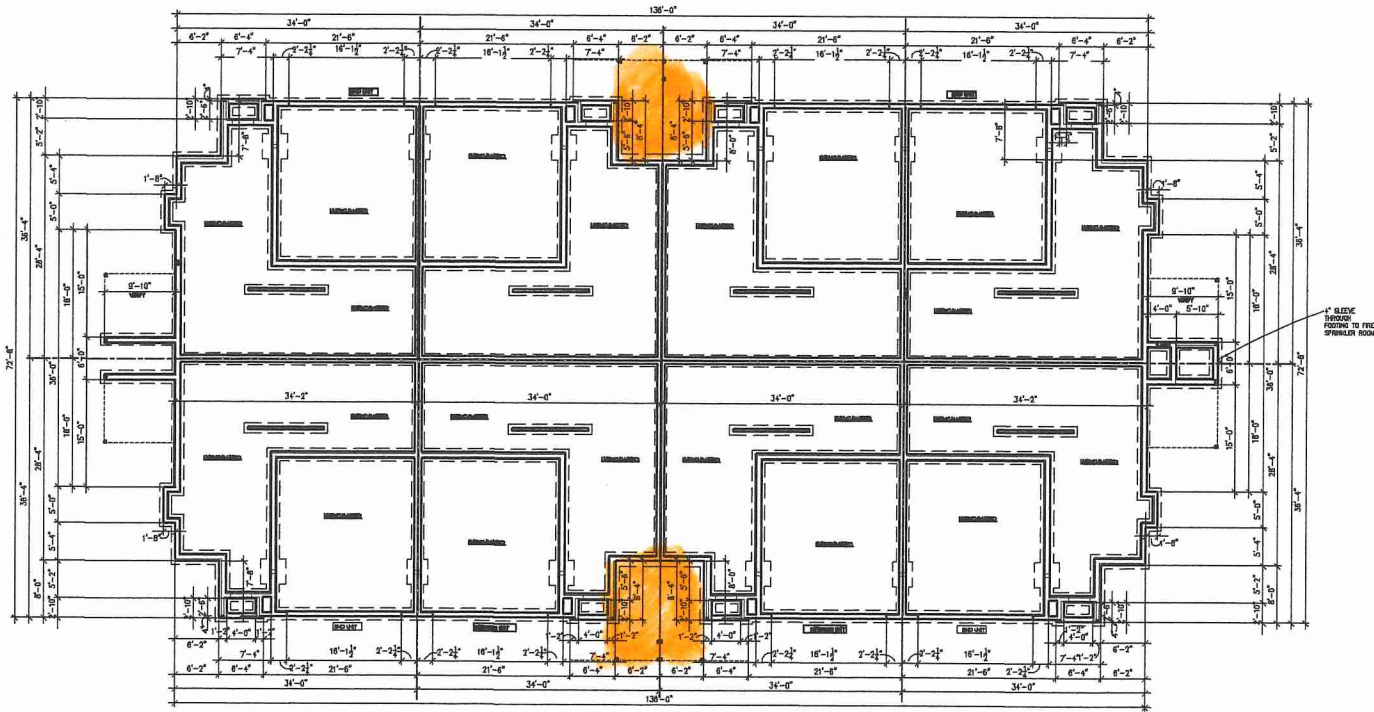
DESCRIPTION: OVERALL MAIN UPPER LEVEL FLOOR PLAN  
ADVISORY

PERMIT SET

RAMSEY TOWNHOMES



① OVERALL FRONT ELEVATION  
SCALE: 1/8"=1'-0"



② OVERALL FOUNDATION PLAN  
SCALE: 1/8"=1'-0"



401 EAST LAKE STREET  
WAYZATA, MN 55391  
Phone: 952.473.8777  
FAX: 952.473.8222

DATE DATE:  
22 JANUARY 2015  
DRAWING NAME: RAMSEY  
JOB NO: 15

DRAWING DESCRIPTION:  
OVERALL FRONT ELEVATION  
& FOUNDATION PLAN  
ADVISOR/DESIGNER

PERMIT SET

RAMSEY TOWNHOMES



Ramsey Town Center Gardens 3<sup>rd</sup> Addition Lot 2



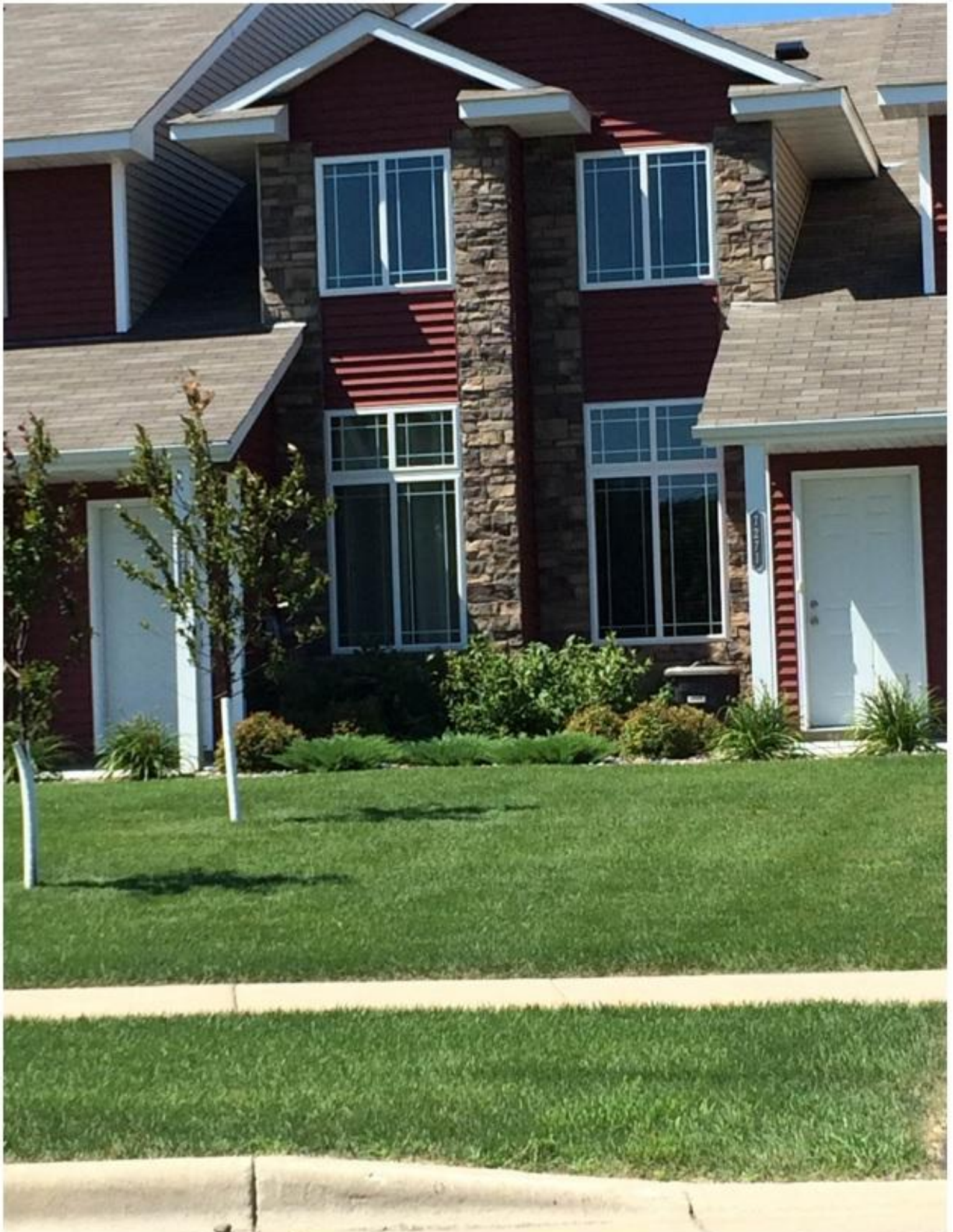
Ramsey Town Center Gardens 3<sup>rd</sup> Addition Lot 2



Ramsey Town Center Gardens 3<sup>rd</sup> Addition Lot 2



Ramsey Town Center Gardens 3<sup>rd</sup> Addition Lot 3 Construction Site

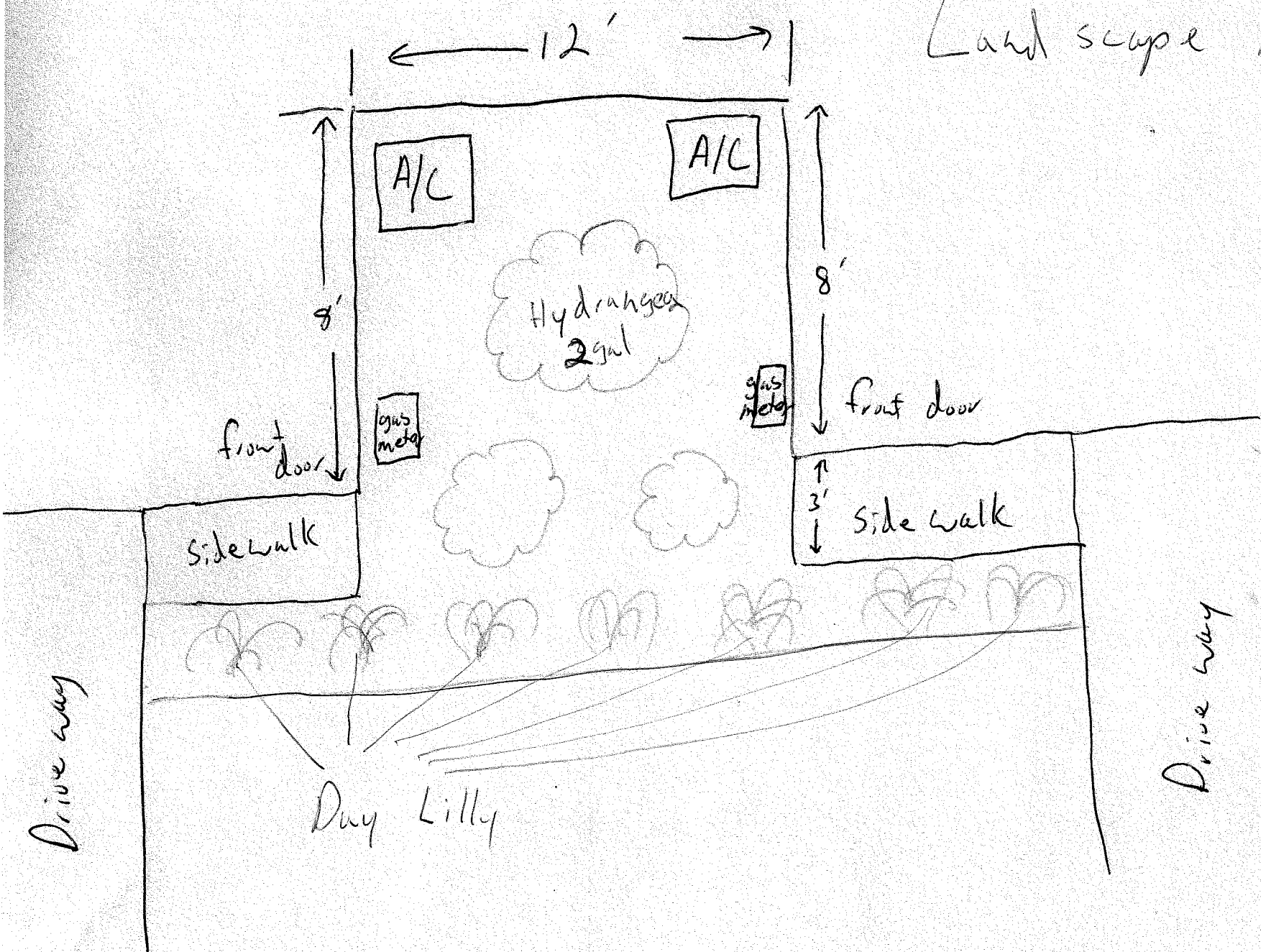


Ramsey Town Center Gardens 2<sup>nd</sup> Addition Landscape Example



Ramsey Town Center Gardens 2<sup>nd</sup> Addition Landscape Example

# Land scape Plan



← 12' →

8'

8'

front door

front door

sidewalk

↑ 3' ↓  
sidewalk

Drive way

Drive way

Day Lilly

gas meter

gas meter

A/C

A/C

Hydrangeas  
2 gal

Councilmember \_\_\_\_\_ introduced the following resolution and moved for its adoption:

**RESOLUTION #15-08-190**

**A RESOLUTION APPROVING AN AMENDED SITE PLAN FOR TOWN CENTER GARDENS 3<sup>RD</sup> ADDITION.**

**WHEREAS**, on August 9, 2005, the City Council approved the final plat and site plan for Town Center Gardens 3<sup>rd</sup> Addition; and

**WHEREAS**, on July 2, 2015, the City received an application for a site plan amendment to eliminate the patio division walls and concrete patio areas on the center units of the properties of 14787, 14851, and 14875 Olivine St. NW and be replaced with landscaping; and

**WHEREAS**, on August 6, 2015, the Planning Commission reviewed the request and recommended approval/denial of the amended site plan.

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:**

That the Ramsey City Council hereby approves an amendment to the site plan for Town Center Gardens Third Addition that eliminates the patio division walls and the concrete patio area from the center units of the properties of 14787, 14851, and 14875 Olivine St. NW and replaces them with landscaping.

The motion for the adoption of the foregoing resolution was duly seconded by Councilmember \_\_\_\_\_, and upon vote being taken thereon, the following voted in favor thereof:

and the following voted against the same:

and the following abstained:

and the following were absent:

whereupon said resolution was declared duly passed and adopted by the Ramsey City Council this the \_\_\_ day of August, 2015.

\_\_\_\_\_  
Mayor

**ATTEST:**

\_\_\_\_\_  
City Clerk

**Regular Planning Commission**

5. 3.

**Meeting Date:** 08/06/2015**By:** Tim Gladhill, Community Development

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**Information****Title:**

Review Comparison Document of The COR Development Plan and Original Ramsey Town Center Master Plan Prepared by the Planning Commission

**Purpose/Background:**

The purpose of this case is to review a comparison document between the original Ramsey Town Center Master Plan and the current The COR Development Plan as requested by the City Council and Planning Commission on May 26, 2015. Minutes from this discussion are attached to this case as background of the purpose of this exercise.

At the May 26, 2015 Joint Meeting with the City Council, Staff was directed to prepare a comparison document outlining the differences between the current COR Development Plan and the original Ramsey Town Center Master Plan. Direction was to simply prepare a document outlining the changes and check back with the City Council and its advisory boards for future direction. It is anticipated that the output of this exercise may lead to some changes to the Design Framework for The COR to either better clarify the vision, eliminate any conflicting language, simplify the document, or potential changes to the official vision.

This step is focused on the inventory of the previous plan and the current plan. The intent of this topic report is not to begin developing a new vision for The COR. The Planning Commission met on July 9 and prepared the first draft of this document. The City Council reviewed the first draft of the document at their July 28, 2015 meeting and minutes from said meeting are attached to this case. Revisions based on the City Council's discussion include a clearer emphasis the additional steps will be coming after this first step that will discuss market realities and future revisions to the vision. This first step is simply inventory the current plan elements to ensure all policy makers are on the same page, and then subsequently decide if any changes need to be made to the elements or implementation items.

**Notification:**

Notification is not required for this discussion topic.

**Observations/Alternatives:**

Attached are documents from the original Ramsey Town Center Master Plan and Development Guidelines (the original vision) as well as the COR Development Plan and Design Framework (current vision). Additionally, the Planning Commission has several policy questions that could be used in future joint meeting or surveys of the City Council, its advisory boards and commissions, or the general public.

Finally, Staff has included a generalized timeline of The COR key milestones. This is a working document that will need to be refined over time. Of key update, Staff would like to populate the initial planning stages that led to the creation of The COR, starting with the 2020 Comprehensive Plan Update (completed in 1998), City ballot advisory question, and design charette process. Staff will utilize the introduction to this case to review the overall history of The COR and its original formation.

**Funding Source:**

This case is being handled as part of normal Staff duties.

**Recommendation:**

Based on direction of the City Council, Staff recommends adoption of the attached Comparison Document for The COR. A reminder that this is a review of current elements and processes. Adopting this document does not create any policy implications.

**Action:**

Motion to recommend that the City Council adopt the attached Comparison Document for The COR.

---

**Attachments**

Comparison Document

ORIGINAL Master Plan

ORIGINAL Development Guidelines

CURRENT Development Plan (Master Plan)

CURRENT Design Framework (Development Guidelines)

Timeframe of Key Milestones

Joint CC PC Minutes dated May 26, 2015

Planning Commission Work Session Minutes dated July 9, 2015

City Council Meeting Minutes dated July 28, 2015

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**Form Review**

**Inbox**

Tim Gladhill (Originator)

Patrick Brama

Kurt Ulrich

Tim Gladhill (Originator)

Form Started By: Tim Gladhill

Final Approval Date: 07/29/2015

**Reviewed By**

Tim Gladhill

Jo Thieling

Kurt Ulrich

Tim Gladhill

**Date**

07/22/2015 09:40 AM

07/23/2015 09:10 AM

07/23/2015 09:24 AM

07/29/2015 10:08 AM

Started On: 07/22/2015 07:50 AM

**The COR Development Plan  
Clarification/Audit of Current Vision  
Outcome of Planning Commission Work Session - July 9, 2015  
Reviewed by City Council – July 28, 2015**

**Overview of this document**

1. Exercise 1: Comparison of Current and Previous Master Plan for The COR.
2. Exercise 2: What is the market reality of the Master Plan? (Future Step)
3. Exercise 3: Do we need to make any changes? If so, where do we go from here? (Future Step)

**Exercise 1: Comparison of Current and Previous Master Plan for The COR  
(Current Step)**

**Overview of Exercise 1**

1. Quick Summary/History of The COR
2. Comparison of Master Plan Versions
3. Future Policy Questions

**Section 1.1: Quick Summary of The COR**

- 2020 Comprehensive Plan
- City of Ramsey Ballot Advisory Question
- Ramsey Town Center Design Charrette

**2020 Comprehensive Plan**

The 2020 Comprehensive Plan was developed/amended between 1998 and 2001. The 2020 Plan called for a mixed use development. Part of the primary reason was to identify an opportunity to address forecasted household growth while protecting the rural character of much of the existing community. The 2020 Plan called to concentrate more of the growth forecasts in a smaller area of the community.

**City of Ramsey Ballot Advisory Question**

In 2001, the City of Ramsey included several Ballot Questions pertaining to the City Charter as well as multiple Advisory Ballot Questions. Of note to this discussion, a question asked was the following:

**“Do you want a Town Center with restaurants, shopping and other amenities along the Highway 10 Corridor?”**

In the general election in 2001, 61.20 responded yes to the above question; 38.20 voted no.

**Ramsey Town Center Design Charrette**

Between 2002-2003, with amendments to the Comprehensive Plan and results from the Advisory Ballot Question, the City embarked on an in-depth public process to assist in the planning for the Ramsey Town Center. Step No. 1 of the current exercise is to develop a summary of the output of this process (previous vision).

**2009/2010 Re-visioning Exercise**

Following the foreclosure of the Ramsey Town Center development and the purchase by the City of approximately 150 acres of the development, the City embarked on an effort to revise the vision to better match market conditions at that time. Step No. 2 of the current exercise is to develop a summary of the output of this process (current vision).

DRAFT

## Section 1.2: Comparison of Ramsey Town Center Master Plan to The COR Development Plan

Ramsey Town Center Master Plan	The COR Development Plan
<b>Land Use and Development Standards</b>	
Mixed Use – Vertically mixed use buildings in center	Mixed Use – Mix of uses within each block; single use buildings acceptable
Strong shared parking strategies	Shared parking strategies reduced, but encouraged (floor area ratio reduced)
Development regulations all but required shared, structured parking in first quarter-mile.	Vision remains such to pursue shared, structured parking structures, but stops short of requirement.
High densities to support retail and protect rural character of other areas of community	Still high density, but less than original vision
Centered around transit station	Centered around transit station
Strong ‘Blue System’ – multi-use surface water management	Blue system amended, but still in existence – see Lake Ramsey planning
1.0 Floor Area Ratio amended to 0.75	0.75 FAR amended to 0.65
2 accesses planned on Armstrong Boulevard	1 access allowed on Armstrong Boulevard due to access spacing and elevation of Armstrong Interchange
Emphasis on pedestrian orientation	Emphasis on pedestrian orientation, but diminished in West 30
Strong streetscape design standards	Strong streetscape design standards
Strong Downtown Feel in center with densities of multi-story buildings, high quality design, mix of uses (residential and commercial)	Strong Downtown Feel in center with densities of multi-story buildings, high quality design, mix of uses (residential and commercial)
<b>Building Design</b>	
Strong architectural standards for individual buildings	Strong architectural standards for individual buildings
Architectural Review Committee – review all new plans and models	No Architectural Review Committee
Strict Sign Standards	Relaxed Sign Standards, yet still stricter than citywide standards
<b>Parks, Recreation, and Open Space</b>	
Master Park Plan	Master Park Plan still intact – but with amendments
West Meandering Commons ‘mirrors’ East Meandering Commons (now The Draw)	West Meandering Commons now Lake Ramsey Commons – layout amended
Park Dedication + Additional contribution to parks	Base Park Dedication – reduced rate granted for high density residential
<b>Land Use Sub-Areas</b>	
Medium Density Residential in north west corner of development	Traditional suburban retail in north west corner of development (The ‘West 30’)
Office Park Development in south east corner	Office Park Development in south east corner

## **Section 1.3: Policy Questions for Future Discussion and/or Survey (City Council, Advisory Boards and Commissions, Residents)**

The intent is to frame a series of questions to send as a survey to advisory boards and City Council. The intent at this stage is not necessarily to answer these questions specifically, but feedback and consensus is more than welcome.

### **Land Use and Development Standards**

1. How much vertically mixed-use buildings is too much?
2. How do we plan for a mix of single-use buildings on a single-block (what happens if the last user in is locked in to a particular use, but doesn't want that use)?
3. How should the City manage parking in The COR? Should the City establish a new strategy for shared parking?
4. How should the City define parking calculations?
5. The remaining westerly leg of Ramsey Parkway – should that continue to have an enhanced design (green medians, enhanced plantings, etc. Similar to existing leg without one-way segments on larger stormwater feature in middle)?
6. Do we want to continue to allow uses with a drive-thru and gas stations?

### **Building Design Standards**

7. Are The COR sign standards still too strict?
8. Should the City re-establish some form of an Architectural Review Committee for The COR?

### **Land Use Sub-Areas**

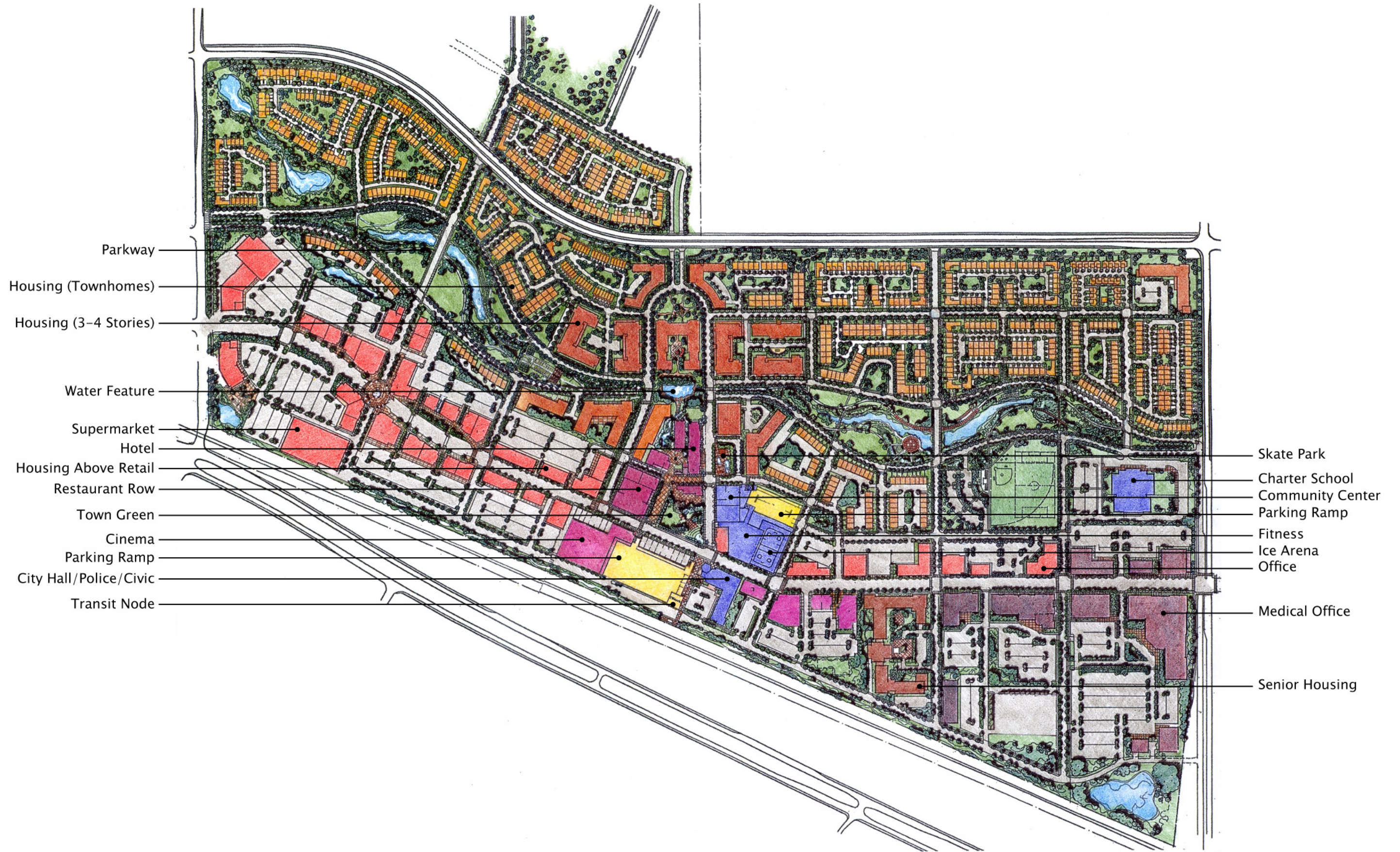
9. How flexible should Staff be in recommending boundary changes to Sub-District Boundaries?
10. Should the City explore or otherwise re-evaluate the vision of the north west quadrant of the development (south east corner of Armstrong Boulevard and Bunker Lake Boulevard)?
11. The remaining undeveloped area just east of Coborn's – should that stay retail, or is Staff able to entertain residential inquiries?
12. Should we continue to hold the area west of Center Street north of Sunwood Drive for a community type use such as a Community Center or movie theater?

## **Exercise 2: What are the market realities of the plan components? (Future Step)**

Exercise 2 will be commenced upon completion of Exercise 1 above. As of July 28, 2015, Exercise 1 has commenced but had not been completed. The intent of Exercise 2 is to take the information above and begin a dialogue/survey of stakeholders in The COR as well as the City's professional services groups for market analysis. These market experts include, but are not limited to, the City's financial, economic development, real estate brokerage, and planning firms.

## **Exercise 3: Do we need to make changes to the plan? If so, where do we go from here? (Future Step)**

The outcomes of Exercise 1 and 2 will inform the process for Exercise 3. This section will be updated as needed.



12 March 2004

Ramsey Town Center

# Development Guidelines

Final Draft 6/8/04



The Future is Ramsey.

# Ramsey Town Center Development Guidelines

## Introduction

This is the third in the series of documents created to help direct design solutions and define project character for the Town Center. With increasing levels of detail and specificity, the Master Plan, the Zoning Code and these Development Guidelines clarify and promote the desired outcomes expressed in the illustrative Concept Plan. Each of these documents serves a specific purpose as part of the Town Center reference library, but also work in concert with other city codes and criteria to provide the information needed for review, critique and approval of development projects within the Town Center district.

## Purpose

The primary purpose of Development Guidelines is to set basic parameters, describe preferences and illustrate design intent. These guidelines serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

## Application

The format and content of these guidelines are specifically tailored for use as a reference workbook and are organized into two sections. The first section outlines issues and recommendations that apply to the entire Town Center district. The second section highlights specific guidelines that apply to each sub-district within the Town Center.

## Implied Responsibility

All participants in the development of the Town Center recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

## Part One: Overall Guidelines

There are a number of guidelines that apply to the entire Town Center including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall Town Center guidelines overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Plan:

- Context – local, city, regional
- Blue System – surface water features, stormwater management
- Green Structure – parks, trails and open space
- Land Use Mix – preferred uses, horizontal/vertical mixed use
- Architecture – built form, character of development
- Streets and Blocks – access, circulation, arrangement
- Parking Strategies – quantity, location, type
- Transit – rail, bus, other options
- Utilities and Infrastructure – sewer, water, electric, gas

It is essential that proposed design solutions for development projects and other improvements within the Town Center demonstrate an understanding of the interplay between these layers.

# Overall Guidelines - Context

## Overview

Although unique to Ramsey, the Town Center is not intended to be a stand alone district within the the City; instead it will be a part of, and connect with, a variety of local, city-wide and regional systems. Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

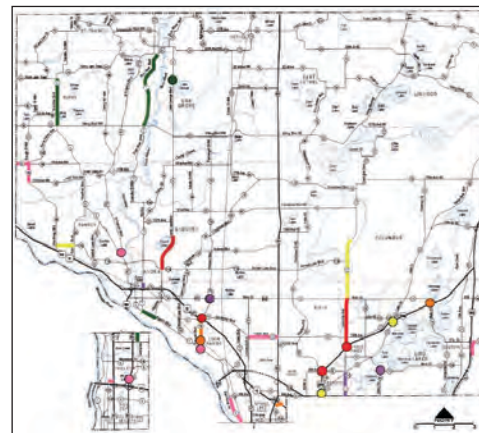
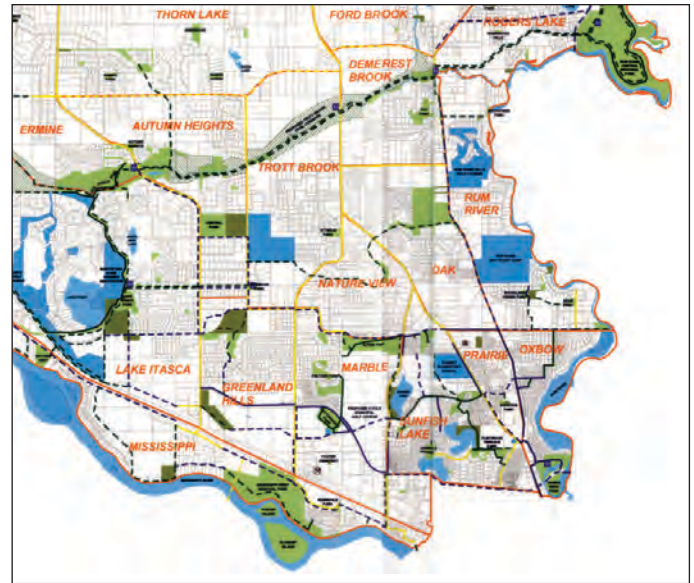
## Guideline Recommendations

To ensure that the Town Center takes full advantage of local and regional systems, development should:

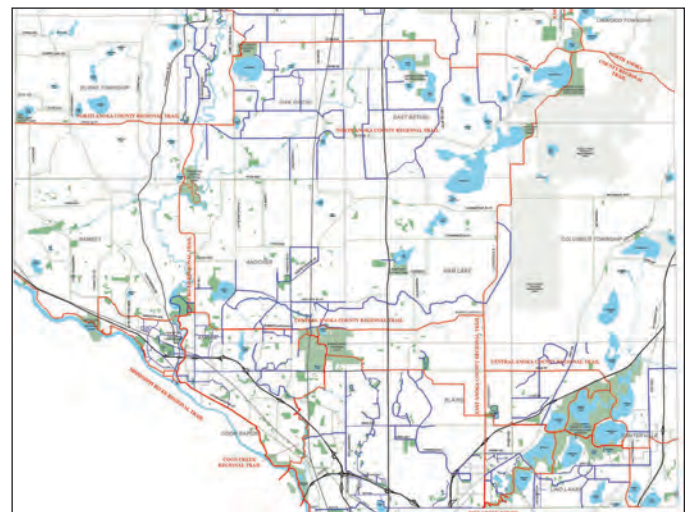
- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the future Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/ stormwater management plan

## Objectives

- Overall Town Center design (and all new private development within the Center) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into the Town Center from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a child on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all Town Center residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis



Regional Transit Map



Anoka Parks, Trails Map

# Overall Guidelines - Blue System

## Overview

Water features and drainage systems are essential components of the Town Center Master Plan. The potential mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize 'best management practices' (BMP) to pretreat run-off, reduce erosion and encourage infiltration. Water elements are not intended to be separate stand alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for the Town Center. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.



## Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- 'Green architecture', expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



## Guideline Recommendations

Blue System guidelines not only define the overall role and character of public space, but also encourage each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout the Town Center
- **Promote** high quality, creative and appealing aesthetics for all blue system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support Town Center development. While this overall strategy addresses the water quantity issue for the entire Town Center by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods (listed in the examples below) to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.



# Overall Guidelines - Green Structure

## Overview

The overall design framework of the Town Center is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within the Town Center. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout the Town Center. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

## Guideline Recommendations

Ramsey Town Center will set a new standard for the design of parks and open space in suburban Twin Cities communities. Toward that end, Green Structure guidelines encourage the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements

Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.



## Objectives

Based on the Parks, Trails and Open Space Concept Plan for the Town Center, a variety of public and private green space is proposed including:

- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities. Playgrounds, game fields and park buildings are suggested park amenities and will target a wider user demand from the surrounding community.
- At the Town Center scale, the central linear park will connect with all other parks and open space elements, providing a focal point for activity and a sense of place similar to Minnehaha Creek in Minneapolis. Gardens, public pavilions and shelters, playlots and open play fields are suggested amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to the Town Center.

# Overall Guidelines - Land Use Mix

## Overview

The Town Center Master Plan identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, these guidelines focus on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

## Guideline Recommendations

The mix of land uses in the Town Center will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, the Center closely resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of the Town Center, exemplified by the new City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.



## Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, 'big box' retailing within the Town Center; only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a 'family' or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district
- All uses should reinforce the street edge



# Overall Guidelines - Architecture

## Overview

High quality architecture is an essential component in fulfilling the objectives of the Master Plan. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

## Guideline Recommendations

Of all the layers that combine to form the Ramsey Town Center Master Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for the Town Center. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- Promote an animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience
- Promote variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Promote a variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other Town Center uses
- Encourage the use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets (corridors) and important crossroads (nodes) throughout the Town Center
- Buildings should always be located at all four corners of intersections



## Examples

Providing a creative mix of building types, scales and expressions are critical objectives to be met – photographs on this page (and throughout these guidelines) suggest some of the many possibilities.

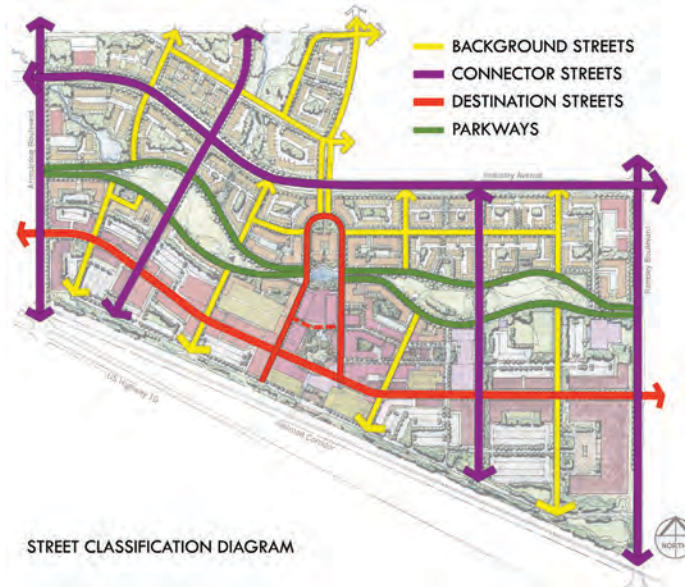


# Overall Guidelines - Streets and Blocks

## Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within, and around the Town Center. The size, type and configuration of this street hierarchy is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample



STREET CLASSIFICATION DIAGRAM



Background Street – quiet residential neighborhood, parking one side



Destination Street – active shopping district with streetscape amenities



Connector Street – equally inviting for both pedestrians and cars

space for street-side activities, gracious strolling promenades, pockets of green and other amenities is critical to the success of the Town Center. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as the primary way to move about and conduct business in the core area.

## Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in the Town Center and encourages a more dispersed and less congested traffic flow (many choices or travel paths)
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout the Town Center
- Convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street)

## Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities (Sunwood Drive is an example)
- Connector street – primary routes that link various districts within the Town Center, usually providing the most direct means of getting from point A to point B (Rhinstone Street is an example)
- Background street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout the Town Center (residential streets are an example)
- Parkway – premier ‘green streets’ adjacent to primary public park / green space (Town Center Parkway is an example)
- Service – still considered public streets, service corridors provide important access to delivery areas, parking lots/ramps and maintenance functions (Civic Center Drive is an example)

# Overall Guidelines - Parking

## Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of the Town Center is a significant challenge. The goal is to provide adequate parking, both quantity and location, while promoting new strategies that support the overall needs of the Town Center. Parking will be provided through a combination of on-street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of the Town Center. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street-level uses should be integrated into ramp design along important street frontages.

## Guideline Recommendations

Parking guidelines encourage the following:

- Locate off-street surface parking lots away from the major streets, either behind or to the side of primary buildings
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Work with the City to review existing standards to provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering



Parking ramp design (building on the right) includes awnings, arched openings and similar materials to reflect character of adjacent building.

- Provide parallel parking on both public and private streets throughout the Town Center, to add parking and create a traffic calming, pedestrian buffer
- Explore options to integrate stormwater management requirements into parking area design
- Explore various landscape approaches to soften parking area edges, provide shade, integrate native plantings, offset islands and other ideas to reduce to 'sea of parking' image

## Objectives

The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in the Town Center. In addition to providing adequate space, excellent design is a major factor as well. Guidelines for the Town Center encourage creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs on this page (and throughout these guidelines) suggest some of the many possibilities.



Surface parking lot located to the side and behind grocery store, also illustrates walkway edge improvements including ornamental fence and plantings.



Structured parking (see entrance sign mid-block) imbedded within mixed-use development that includes ground level commercial and residential above.



This photo shows both on-street parking and parking lot edge improvements that together provide a safe and inviting pedestrian walkway.

# Overall Guidelines - Transit

## Overview

The Town Center is designed to be 'transit ready' in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multi-modal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. This station is designed to meet the short term needs of the Northstar Commuter Coach service, and also the long term objectives for proposed commuter rail service in the future. Transit components include the following:

- North Star Commuter Coach service provides peak period, peak direction bus service between downtown Minneapolis and Elk River
- North Star Commuter Rail will provide train service between St. Cloud and downtown Minneapolis
- 400 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within the Town Center and into the surrounding neighborhoods
- The Town Center can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station

The Town Center also uses a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. This TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions. The Town Center will use a coordinator to manage the delivery of these services and to promote and market the TDM program.

## Guideline Recommendations

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in the Town Center, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following guidelines:

- Understand and take advantage of opportunities to use existing and proposed transit components
- Provide transit related facilities specifically linked to uses and character within each Town Center district, including signage and lighting for way-finding and bicycle amenities

## Objectives

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. Guidelines for the Town Center promote public transportation as part of the solution and suggest a variety of street improvements and other amenities to invite increased use. Photographs on this page (and throughout these guidelines) suggest some of the many possibilities.



# Overall Guidelines - Utilities

## Overview

The Master Plan, together with detailed engineering drawings for site improvements, defines a network of utilities that will serve the Town Center. While understanding the location, size and characteristics of sanitary sewer, storm sewer and water system components is important, development proposals must also acknowledge awareness of the Town Center's relationship to the city water supply – specifically, its relationship to the regional aquifer. New development projects have a responsibility to use best practices for proper connection to utilities, monitor flow of materials into storm sewer and provide recommendations for landscape and pavement maintenance in an effort to reduce, or eliminate, potential pollution sources.

The Town Center will also be served by a full complement of utility connections including cable, fiber optic, electric, telephone and so on. As always call for location of all site utilities prior to construction.

## Guideline Recommendations

An important part of the Master Plan, utility guidelines promote the idea of balancing quality service, adequate capacity and the physical needs of site development with the more qualitative issues of aesthetics, proper maintenance and environmental health. These guidelines encourage the following:

- Subscribe to, and implement best management practices as the benchmark for utility design and maintenance

- Electrical service, and other utilities as appropriate, should be buried underground or located along service corridors and other 'back of house' locations

## Objectives

Not easily photographed (mostly underground), the intent is to suggest that even site utilities have a role in the overall success of the Town Center.



# Part Two: Subdistrict Guidelines

## Overview

The Master Plan and Zoning Code identify five distinct, but overlapping sub-districts within the Town Center. Each of these sub-districts (outlined on the plan below) is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. They include:

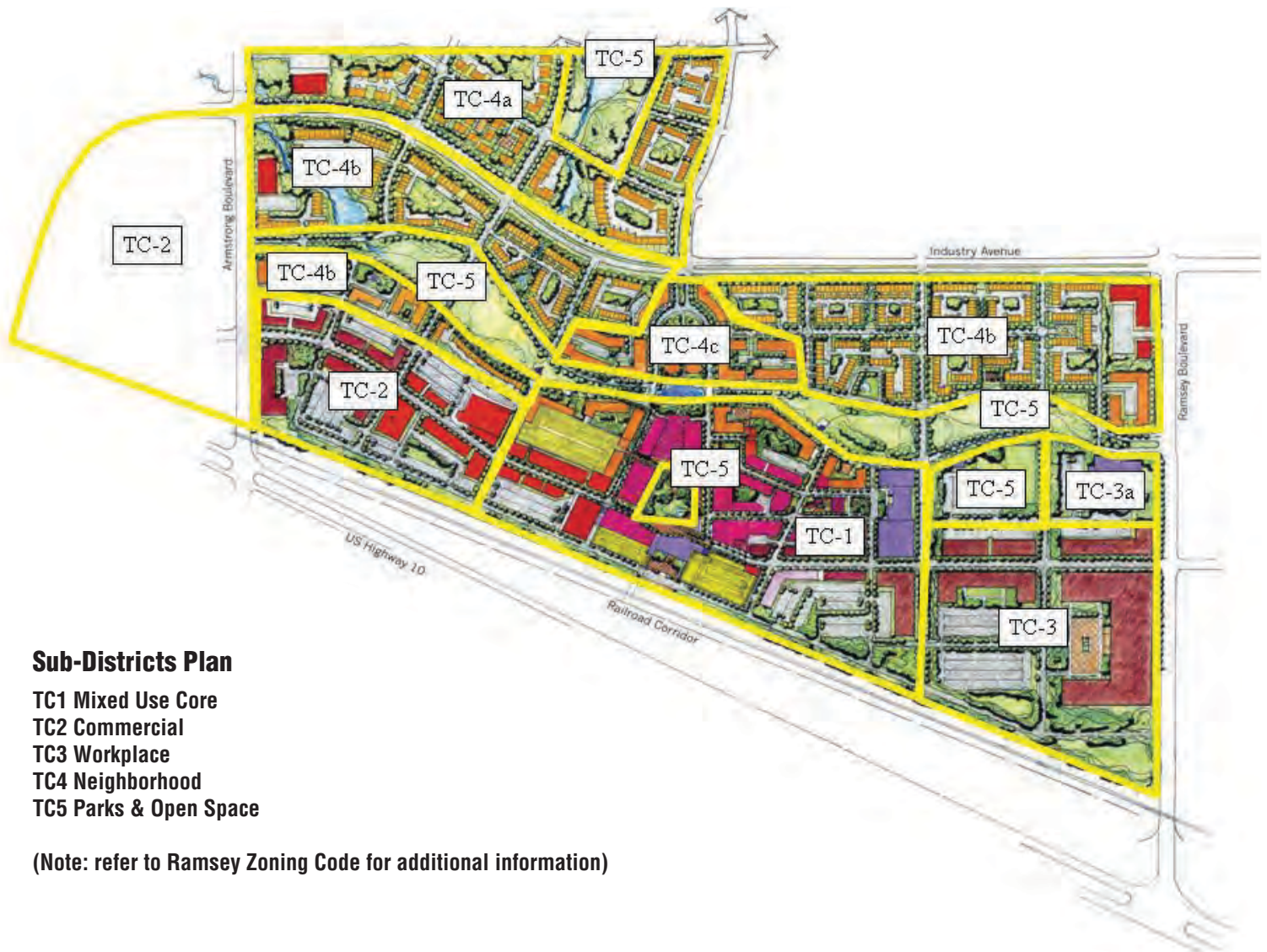
- **TC1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **TC2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses

- **TC3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **TC4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **TC5 Parks and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout the Town Center

Development plans must respond to the overall Town Center guidelines described in Part One of this document, but also must address the following detailed guidelines for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Guidelines
- Site Development Guidelines
- Architectural Guidelines

In all cases, the sub-district guidelines must be applied in harmony with the overall Town Center guidelines and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development.



## Sub-Districts Plan

- TC1 Mixed Use Core
- TC2 Commercial
- TC3 Workplace
- TC4 Neighborhood
- TC5 Parks & Open Space

(Note: refer to Ramsey Zoning Code for additional information)

## Public Realm/Streetscape Guidelines



Public realm/streetscape guidelines define the visual character and physical improvements for all public spaces within the Town Center. These guidelines also provide the general framework and examples for design of proposed quasi-public and private space located within each development parcel. Public realm/streetscape guidelines focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space

The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. Materials, colors and intensity of improvements can vary for each sub-district, but should also reflect the desired character and identity of the entire Town Center.

## Site Development Guidelines



Site development guidelines describe the relationship between building edge and public space. These guidelines imply a strong link between architecture and site - how buildings define, or are defined by the spaces around them, and the visual and physical connections between the private and public realm. Site development guidelines focus on the following elements:

- Land Use – proposed mix of uses defined in the Zoning Code
  - Building Placement – street frontage, set back and orientation all contribute to a continuous edge, while providing some flexibility to add interest with recessed or projected elements
  - Parking Strategies – district parking solutions to support a mix of uses
- Issues related to density, land use, safety and security, access, public space and views inform decisions about building placement, scale and massing. Various combinations of these pieces come together to define the unique character and identity of each sub-district within the Town Center.

## Architectural Guidelines

Architectural guidelines describe the character, quality and design direction for proposed buildings within each sub-district. Architectural guidelines focus on the following elements:

- Building design, character and expression – buildings should not imitate historic examples, but rather promote the integration of both traditional and contemporary styles

- Scale – provide emphasis and detailing to create interest and promote ‘human scale’ experience for the pedestrian
- Proportion – provide a harmonious relation of one part to another (or parts to the whole)
- Massing – include a variety of building forms, building types and building heights
- Materials – maintain regional traditions and local building techniques with a preference toward masonry and other natural materials
- Windows and doors – emphasize transparency of streetside façade
- Street presence – buildings should be placed parallel to the street, should contribute to the vitality of the street scene, should minimize blank walls along the public right of way and should consider all street frontage (including buildings at corners) as primary elevations

These architectural guidelines suggest an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



# Sub-District Guidelines - TCi Mixed Use Core

## Public Realm / Streetscape

### Overview

Public realm and streetscape improvements define the character of the Town Center and create a memorable signature for the Mixed Use Core sub-district. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year. Public realm and streetscape guidelines include the following:

### TC1 Parks, plazas and other public gathering spaces

Public spaces in the Mixed Use Core should:

- Blend both contemporary and traditional motifs with a focus on pedestrian use
- Encourage a balance of hardscape with landscape; a mix of pavements with plantings.
- Promote an overall landscape character that tends toward a more urban park or plaza feel, with structured spaces and highly maintained gardens
- Provide a variety of opportunities to capture different microclimates with many choices for seating- including movable chairs- and access to both sun and shade
- Provide a variety of levels or elevations to add interest to the ground plane and to provide informal steps for sitting
- Promote safety and security in all design solutions, including landscape lighting, open views into the site, and accessibility
- Provide seasonal interest and color; promote high quality materials and finishes
- Provide opportunities and specific sites for public art, performances and markets

### TC1 Street/sidewalk dimensions and streetscape elements

Destination streets (like Sunwood) and connector streets (like Rhinestone) must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian (see street/sidewalk diagram) including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore street light spacing, height and a mix of fixture types to support the increased intensity of uses and activities in the core, (note however, that too much light is as bad as too little)
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)

### TC1 Bicycle/pedestrian connections (on-street and off-street)

The Town Center Master Plan describes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the

Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces (specifically Civic Square and Downtown Commons). Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.

### TC1 Residential courts, pocket parks and other private green space

Along with proposed public parks and great streets, there are many opportunities to integrate private courtyards and pocket parks within this sub-district. These small-scale greens provide important 'breathing space', contribute to stormwater management and offer additional recreational amenities (tot lot, sitting areas) to serve specific building tenants or residents. Back gardens, auto-courts and other options should be considered as part of residential and commercial development plans in the core.



### Overview

Guidelines that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, site development guidelines must also describe the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within the Town Center. Site development guidelines include the following:

### TC1 Land Use

- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Promote housing density at the core, specifically targeting locations along Sunwood Drive, Town Center Drive, along park edges and above street-level uses around the Civic Square and Downtown Commons
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.

### TC1 Building Placement

- Generally, buildings should be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, for example, at Sunwood and Town Center Drives, or where streets intersect with parks, such as the streets that front on or end near the Civic Square. These locations can often be seen from



several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as the Town Center develops

- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Encourage recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage

### TC1 Parking Strategies

- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on-street, small surface lot, underground and ramped parking solutions

- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area
- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Discourage driveway access to parking facilities along Sunwood Drive in favor of side streets or Civic Center Drive

### Overview

Buildings in the Mixed Use Core will play a major role in defining the overall character of the Town Center. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be '4-sided' at all locations visible to public areas. Emphasis to be on great design along public street frontage. Architecture guidelines include the following:

### TC1 Building Design, Character and Expression

- Provide a variety of building types and styles, expressed both in large scale (overall building) and small scale (design details)
- Emphasize transparency at street level to encourage two way viewing – both window shopping and 'eyes on the street'
- Promote interesting, animated architectural features without being thematic or artificial – faux decorative elements are discouraged
- Avoid large, unarticulated façades



- Emphasize important intersections and other nodes by placing distinctive architectural elements or interesting façades at these prominent locations
- Provide a variety of front elevations for rows of buildings along any given block

### TC1 Scale, Proportion and Building Height

- Provide a variety of building heights along any given block, perhaps related to changes in use or at demising walls
- Articulate front façades with projections and recesses
- Encourage increased building height, higher density and larger scale buildings adjacent to the Civic Square and Downtown Commons



- Promote multiple stories as the norm in this sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of 20 feet to the roofline

### TC1 Street Presence

- Emphasize pedestrian experience with architectural features at street level – canopies, decorative light fixtures and material detail
- Encourage variety of color, texture and materials to complement other streetscape elements
- Conceal service entries, loading facilities and mechanical services from view along public streets
- Create a continuous building edge along street frontage, visually connecting adjacent buildings with complementary façade treatments



### TC1 Windows and Doors

- Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – recommend 50% minimum of total ground level façade, or demonstrate great design through other means
- Provide real window openings for all street facing façades above ground level – recommend 40% minimum of total façade
- Provide recessed entries wherever possible
- Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
- Provide primary access doors facing public streets



### TC1 Materials

- Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street
- Encourage durable, high quality materials - local sources if available
- Siding is not allowed (vinyl, metal, wood) except as a complimentary use
- Encourage the use of mostly brick or comparable alternative; minimum requirement of 50% of non-glazed wall area
- Metal and wood components are acceptable as a complimentary use
- Encourage overhead projections, either as a specific building component or as permanent/temporary canopies
- Select materials that relate to the building use, not artificially contrived or themed
- Avoid overly complex or arbitrary use of materials
- Include smaller scale elements, variety of materials and careful detailing to support street level activities and enhance the pedestrian experience



# Sub-District Guidelines - TC2 Commercial

## Public Realm / Streetscape

### Overview

While the Commercial Sub-district encourages a mix of large and small floor plates, accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential. Public realm and streetscape guidelines include the following:

### TC2 Parks, plazas and other public gathering spaces

Public spaces in the Mixed Use Core should:

- Blend both contemporary and traditional motifs with a focus on pedestrian connections and small group sitting areas
- Encourage a balance of hardscape with landscape; a mix of pavements with plantings.
- Provide small urban parks or plazas integrated with building entries or taking advantage of shared space between buildings
- Promote safety and security in all design solutions, including landscape lighting, views into the site, and accessibility
- Provide seasonal interest and color; promote high quality materials and finishes
- Provide opportunities and specific sites for public art

### TC2 Street/sidewalk dimensions and streetscape elements

Destination streets (like Sunwood) and connector streets (like Zeolite) must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to invite pedestrian traffic
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a unique character for this district
- Explore street light spacing, height and a variety of fixture type that would support proposed mix of uses and activities (note, however, that too much light is as bad as too little)
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)

### TC2 Bicycle/pedestrian connections (on-street and off-street)

The Town Center Master Plan describes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to one another. These connections tend to become part of the shared right of way along the major streets. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. However, providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects.

### TC2 Residential courts, pocket parks and other private green space

As previously mentioned, there are many opportunities to integrate privately owned and maintained courtyards and pocket parks within this sub-district. These small-scale greens provide important 'breathing space', contribute to stormwater management and offer additional amenities (sitting areas) to serve specific building tenants and adjacent uses. Back gardens, auto-courts and other options should also be considered as part of residential and commercial development plans in this sub-district.



### Overview

Site development guidelines for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for the Town Center. Specifically this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours. Site development guidelines include the following:

### TC2 Land Use

- Encourage mixed use (within specific blocks) throughout this district
- Promote housing in close proximity to retail uses, specifically targeting locations along the edge of Town Center Park
- Include a mix of large floor plate variety stores, service and convenience uses (grocery, drug and hardware stores), specialty shops and other destination retailers
- Emphasize Sunwood Drive as the commercial 'Main Street'

### TC2 Building Placement

- Generally, buildings should be located at the street right-of-way line (zero setback), allowing recessed space for outdoor dining and other sidewalk activities; no more than 40% of building beyond maximum setback
- Generally, buildings should be located parallel to the street they face
- Buildings should be placed at all four corners of key intersections
- Reduce the scale and mass (street side visual impacts) of large floor plate buildings by including liner stores or break up building façades into smaller sections
- Large, blank walls along any public street are unacceptable
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, for example, at Sunwood and Zeolite, or at key development entries, for example, at Sunwood and Armstrong – frequently these locations can be seen from several directions, from a distance, or sit at prominent crossroads and will likely become future landmarks as the Town Center develops
- Minimize openings between buildings where storefronts line the street, although limited driveway access (one per block along destination streets) and some provision for pedestrian connections through blocks are encouraged – large gaps in the building wall along street frontage to accommodate surface parking lots, greater building setbacks or other purposes are discouraged



- No more than 25% of street frontage can be utilized for surface parking lots

### TC2 Parking Strategies

- Provide parking spaces for typical daily use satisfying both quantity and location needs through a combination of on-street and surface lot parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the sub-district
- Minimize surface parking lots fronting on primary streets, locate surface parking lots away from the major streets, either behind or to the side of buildings



### Overview

Buildings in the Commercial Sub-district make a significant contribution to first impressions of the Town Center. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make the Town Center unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis to be on great design along public street frontage. Architecture guidelines include the following:



### TC2 Building Design, Character and Expression

- Provide a variety of building types and styles, expressed both in large scale (overall building) and small scale (design details)
- Emphasize transparency at street level to encourage two way viewing – both window shopping and eyes on the street
- Promote interesting, animated architectural features without being thematic or artificial – faux decorative elements are discouraged
- Avoid large, unarticulated façades
- Emphasize important intersections and other nodes by placing distinctive architectural elements or interesting façades at these prominent locations
- Provide a variety of front elevations for rows of buildings along any given block



### TC2 Scale, Proportion and Building Height

- Provide a variety of building heights along any given block, perhaps related to changes in use or at demising walls – commercial buildings should express 12 foot first floor height
- Articulate front façades with projections and recesses
- Encourage increased building height and larger scale buildings adjacent to the railroad corridor, Armstrong Boulevard and as you approach the core sub-district
- Promote multiple stories in this sub-district, but single story buildings are acceptable; continuous height rooflines should not dominate any street frontage and should be a minimum of 18 feet to the roofline

### TC2 Street Presence

- Emphasize pedestrian experience with architectural features at street level – canopies, decorative light fixtures and material details
- Encourage variety of color, texture and materials to complement other streetscape elements
- Conceal service entries, loading facilities and mechanical services from view along public streets as much as possible
- Create a continuous building edge for the majority of street frontage, visually connecting adjacent buildings with complimentary façade treatments – in limited areas where gaps occur, architectural treatments, windows and detailing of side façades should match front façade



### TC2 Windows and Doors

- Maximize glass openings for all ground level, street front façades - especially for retail, restaurant and other commercial uses – recommend 535% minimum of total ground level façade, or demonstrate great design through other means
- Provide real window openings for all street facing façades
- Provide recessed entries wherever possible
- Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is unacceptable; faux windows are not allowed
- Provide primary access doors facing public streets



### TC2 Materials

- Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street
- Encourage durable, high quality materials - local sources if available
- Siding is not allowed (vinyl, metal, wood) except as a complimentary use
- Encourage the use of mostly brick or comparable alternative; minimum requirement of 50% of non-glazed wall area
- Metal and wood components are acceptable as a complimentary use
- Accommodate changes in material as follows – return 8 foot minimum at side walls and avoid terminating in the same plane
- Encourage overhead projections, either as a specific building component or as permanent/temporary canopies
- Avoid overly complex or arbitrary use of materials
- Include smaller scale elements, variety of materials and careful detailing to support street level activities and enhance the pedestrian experience

### Overview

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of the Town Center is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. South Park, an active, neighborhood scale, recreational play space has been proposed for this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts. Public realm and streetscape guidelines include the following:

### TC<sub>3</sub> Parks, plazas and other public gathering spaces

Public spaces in the Mixed Use Core should:

- Blend both contemporary and traditional motifs with a focus on pedestrian use
- Encourage a balance of hardscape with landscape; a mix of pavements with plantings.
- Promote an overall landscape character that tends toward a more urban park or campus feel, with structured spaces, open lawns and layered garden plantings
- Provide a variety of opportunities to capture different microclimates with many choices for seating and access to both sun and shade
- Promote safety and security in all design solutions, including landscape lighting, open views into the site, and accessibility
- Provide seasonal interest and color; promote high quality materials and finishes
- Provide opportunities and specific sites for public art

### TC<sub>3</sub> Street/sidewalk dimensions and streetscape elements

Destination streets (like Sunwood) and connector streets (like Rhinestone) must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian (see street/sidewalk diagram) including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore street light spacing, height and type of fixtures to support the uses and activities in this sub-district (note however that too much light is as bad as too little)
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note however that simpler is better and too much clutter is a negative)



### TC<sub>3</sub> Bicycle/pedestrian connections (on-street and off-street)

The Town Center Master Plan describes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to one another. These connections tend to become part of the shared right of way along the major streets. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. There is no provision for dedicated bike lanes on any of the Workplace Sub-district streets at this time. However, providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects.

### TC<sub>3</sub> Residential courts, pocket parks and other private green space

Along with proposed public parks and great streets, there are many opportunities to integrate private courtyards and pocket parks within this sub-district. These small-scale greens provide important 'breathing space', contribute to stormwater management and offer additional recreational amenities (tot lot, sitting areas) to serve specific building tenants or residents. Back gardens, auto-courts and other options should be considered as part of residential and commercial development plans in this sub-district.



### Overview

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts. Site development guidelines include the following:

### TC3 Land Use

- Encourage horizontal mixed use (within each block) throughout this district, and explore potential for vertical mixed use (within each building) where possible
- Encourage residential development, perhaps targeting senior housing near the core sub-district and rental units above storefronts along Sunwood Drive or adjacent to the park
- Include a mix of office, medical, institutional, housing, food venues and other uses that encourage a variety of activities throughout the day.

### TC3 Building Placement

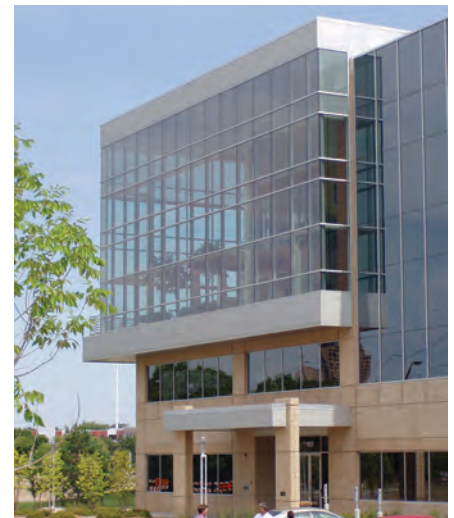
- Generally, buildings should be located at the street right-of-way line (zero setback), allowing recessed areas for building entries; no more than 60% of building beyond maximum setback
- Buildings should be placed at all four corners of key intersections
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, for example, at Sunwood and Rhinestone, or where streets intersect with parks, for example the streets that front on

South Park – frequently these locations can be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent activity node and will likely become future landmarks or helpful orientation/wayfinding icons as the Town Center develops

- Minimize openings between buildings, although limited driveway access (one per block along destination streets) and some provision for pedestrian connections through blocks are encouraged – gaps in the building wall along street frontage to accommodate large surface parking lots, greater building setbacks or other purposes are discouraged
- Encourage recessed entries and windows to create street-level interest, variety and pedestrian scale along street frontage
- No more than 25% of street frontage can be utilized for surface parking lots

### TC3 Parking Strategies

- Provide parking spaces to support typical daily use, satisfying both quantity and location needs through a mix of on-street, surface lots internal to the block, underground and ramped parking solutions
- Encourage shared parking solutions that support special events or around the clock shifts in destination, audience – take advantage of peak/off-peak hours of operation for various uses within this, and adjacent sub-districts
- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; large surface parking lots along major street frontage are not allowed
- Discourage driveway access to parking facilities along Sunwood Drive in favor of side streets or along Civic Center Drive



### Overview

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter the Town Center via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this subdistrict suggest a variety of potential building types – blending medical campus with small office, storefront and some housing demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis to be on great design along public street frontage. Architecture guidelines include the following:

### TC<sub>3</sub> Building Design, Character and Expression

- Provide a variety of building types and styles, expressed both in large scale (overall building) and small scale (design details)
- Emphasize transparency at street level to encourage two way viewing – both window shopping and eyes on the street
- Promote interesting, animated architectural features without being thematic or artificial – faux decorative elements are discouraged
- Avoid large, unbroken façades
- Emphasize important intersections and other nodes by placing distinctive architectural elements or interesting façades at these prominent locations
- Provide a variety of front elevations for rows of buildings along any given block



### TC<sub>3</sub> Scale, Proportion and Building Height

- Provide a variety of building heights along any given block, perhaps related to changes in use or at demising walls
- Articulate front façades with projections and recesses
- Encourage increased building height, higher density and larger scale buildings adjacent to the Civic Square and Downtown Commons
- Promote multiple stories in this sub-district, but occasional single story buildings are acceptable; continuous height rooflines and single story buildings should not dominate any street frontage and should be a minimum of 18 feet to the roofline

### TC<sub>3</sub> Street Presence

- Emphasize pedestrian experience with architectural features at street level – canopies, decorative light fixtures and material detail
- Encourage variety of color, texture and materials to complement other streetscape elements
- Conceal service entries, loading facilities and mechanical services from view along public streets
- Create a continuous building edge along street frontage, visually connecting adjacent buildings with complimentary façade treatments

### TC3 Windows and Doors

- Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – recommend 35% minimum of total ground level façade, or demonstrate great design through other means
- Provide real window openings for all street facing façades above ground level – recommend 40% minimum of total façade
- Provide recessed entries wherever possible
- Use clear glass for all windows and doors – mirrored, reflective or highly tinted glazing is discouraged
- Provide primary access doors facing public streets

### TC3 Materials

- Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street
- Encourage durable, high quality materials - local sources if available
- Siding is not allowed (vinyl, metal, wood) except as a complimentary use
- Encourage the use of mostly brick or comparable alternative; minimum requirement of 35% of non-glazed wall area
- Encourage overhead projections, either as a specific building component or as permanent/temporary canopies
- Avoid overly complex or arbitrary use of materials
- Include smaller scale elements, variety of materials and careful detailing to support street level activities and enhance the pedestrian experience



### Overview

Two prominent public parks supply much of the informal green space for the Town Center, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Park surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. Town Center Park cuts through the heart of the property and is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout the Town Center. Public realm and streetscape guidelines include the following:

### TC<sub>4</sub> Parks, plazas and other public gathering spaces

Public spaces in the Neighborhood Sub-district should:

- Blend both contemporary and traditional motifs with a focus on pedestrian and bicycle use, large and small group gatherings
- Emphasize informal green space, but also include a balance of hardscape with landscape; some paved areas integrated with plantings.
- Promote an informal or natural landscape appearance – this does not mean it isn't mown and maintained, but instead reflects the idea of soft landforms, groves of trees, meandering walks and other less structured elements
- Integrate stormwater management practices and features into park design as described in overall guidelines
- Take advantage of different microclimates, including sun and shade, orientation (north, south, east, west), top of slope/bottom of slope, water's edge
- Promote safety and security in all design solutions, including landscape lighting, open views into the site, and accessibility
- Provide seasonal interest and color; promote high quality materials and finishes
- Provide opportunities and specific sites for public art, special events, performance

### TC<sub>4</sub> Street/sidewalk dimensions and streetscape elements

Destination streets (like Town Center Parkway) and connector streets (like Rhinestone) should not only work for the automobile, but should also provide a safe, attractive and inviting experience for the pedestrian (see street/sidewalk diagram) including:

- Provide ample walk widths to support intensity of pedestrian use and bicycle traffic – recommend 6 foot minimum for pedestrian walks and 10 foot minimum for bicycle paths
- Provide urban streetscape solutions (sidewalks, boulevard plantings, curb and gutter) along Ramsey, Armstrong and Bunker Lake Boulevards, replacing the existing rural road sections (road shoulder, drainage ditch)
- Install street trees to frame the street and provide shade

- Provide street lighting that meets all safety standards and design criteria, creates a signature for this sub-district and compliments overall lighting design for the Town Center
- Explore street light spacing, height and type of fixtures that fit with residential use (note, however, that too much light is as bad as too little)
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)

### TC<sub>4</sub> Bicycle/pedestrian connections (on-street and off-street)

The Master Plan describes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect sub-districts to various amenities within the Town Center and to the rest of the City. Proposed park improvements include off-street pedestrian and bicycle paths including grade-separated crossings through Town Center Park at Rhinestone and Zeolite Streets. Bicycle riders may choose to share the street with vehicular traffic, but there is no provision for dedicated bike lanes on any of the Town Center streets at this time. Providing and maintaining bicycle and pedestrian connections should be a key component for all development projects in this sub-district.

### TC<sub>4</sub> Residential courts, pocket parks and other private green space

Along with proposed public parks and great streets, there are many opportunities to integrate private courtyards and pocket parks within the Neighborhood Sub-district. These small-scale greens provide important 'breathing space', contribute to stormwater management and offer additional recreational amenities (tot lot, sitting areas) to serve specific building tenants or residents. Back gardens, auto-courts and other options should be considered as part of every residential site development plan.



### Overview

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of the Town Center. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The major streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of the Town Center. Site development guidelines include the following:

### TC4 Land Use

- Provide a mix of housing types, styles and price range including a variety of unit and building types within each block
- Promote increased density closer to the Mixed Use Core Sub-district and along major streets, specifically targeting locations along Town Center Parkway and the surrounding boulevard streets

### TC4 Building Placement

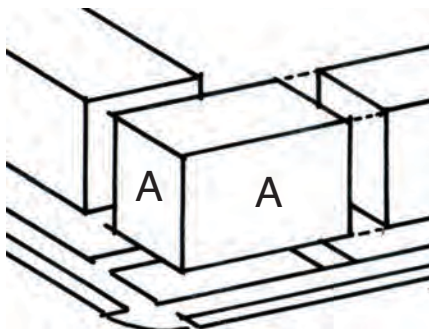
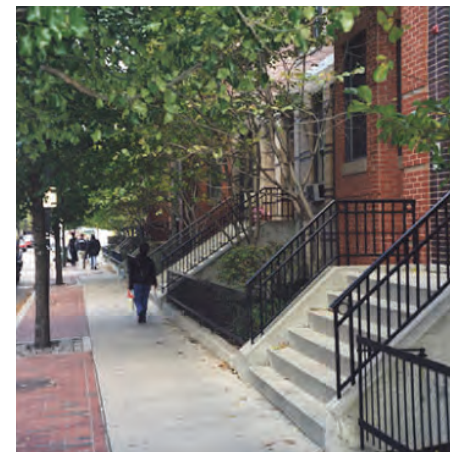
- Building setbacks are based on three factors – housing type, street width and the combination of desired public streetscape improvements and private 'garden' spaces
- Special consideration should be given to architectural elements, materials and other design features for buildings located at primary intersections, for example, at Bunker Lake Boulevard and Town Center Drive
- Special attention should be given where streets intersect with parks, for example many of the streets that front on or end near Town Center Park
- No more than 25% of building beyond maximum setback



- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps in the building wall along street frontage to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Build to corners of each block, maintaining the building wall at street intersections

### TC4 Parking Strategies

- Incorporate surface parking lots into the center of blocks, screened from public streets with landscape, fencing and other ornamental elements
- Locate majority of visitor parking on-street
- Locate garage doors away from public view; garage doors fronting on public streets are not allowed
- Avoid long stretches of garage doors in the same plane – recesses and projections, landscape features and other site elements will provide some relief to this building face



- Build to corners
- Treat both sides as fronts

### Overview

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of the Town Center property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for the Town Center. Architecture to be '4-sided' at all locations visible to public areas. Emphasis to be on great design along public street frontage. Architecture guidelines include the following:

### TC4 Building Design, Character and Expression

- Provide a variety of building types and styles, expressed both in large scale (overall building) and small scale (design details)
- Express residential character through definition and identification of individual units within larger buildings
- Vary roof styles along street fronts including some recesses and setbacks
- Promote interesting, animated architectural features without being thematic or artificial – faux decorative elements are discouraged
- Avoid large, unbroken façades, specifically at 'side elevations' facing streets
- Emphasize important intersections and other nodes by placing distinctive architectural elements or interesting façades at these prominent locations
- Provide distinctly different front elevations for each building type along any given block

### TC4 Scale, Proportion and Building Height

- Provide a variety of architectural elements of different heights along any given block that serve to break up the predominant building mass including porches, entry canopies, bay extensions and other features
- Emphasize individual building and unit entries through changes in height, scale and massing
- Minimize single story portions of any façade along street frontage
- Encourage increased building height, higher density and larger scale buildings along major streets, along park frontage and close to the core sub-district



### TC4 Street Presence

- Locate building and unit entrances on primary façade; articulate entrances to ensure easy visibility and access
- Provide public / visitor access doors on building façades that face streets having on-street parking; buildings facing streets without on-street parking should treat the street-side elevation as the primary façade including covered porches, canopies, recesses and projections
- Provide windows and other architectural features for end units to enhance building character; blank walls are not allowed
- Address both street-facing elevations for corner buildings; treat both façades equally as primary elevations
- Provide outdoor living space (garden, terrace) along all public streets





#### TC4 Windows and Doors

- Maximize window area and design diversity along street front façades
- Provide windows into below grade levels that extend more than 3 feet above grade
- Provide recessed entries wherever possible
- Use clear glass for all windows and doors – mirrored, reflective or highly tinted glazing is discouraged
- Provide primary access doors facing public streets

#### TC4 Materials

- Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street
- Encourage durable, high quality materials - local sources if available
- Encourage the use of brick, stone and a variety of high quality materials
- Discourage the use of vinyl siding; large expanses of continuous siding is discouraged
- Include smaller scale elements, variety of materials and careful detailing that appeal to pedestrian activities; material or color changes should occur at these elements



- Horizontal material changes should only occur at inside corners or should return 8' of any wall
- 35% of non-glazed street front wall are should be brick unless building design is enhanced through other means such as covered porch areas, broken roof lines, articulated façade and material or color changes.

### Overview

The Town Center Master Plan illustrates a strong armature of green, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in the Town Center. Site development guidelines include the following:

### TC5 Land Use

- Encourage connections to proposed public parks via streets, walkways and trails as part of every development plan
- Encourage the creation of additional park space, perhaps mostly private or semi-public greens, as part of every development plan
- Include park features and recreational amenities specifically related to surrounding land use, sub-district character and defined needs of adjacent buildings
- Uses, structures and events, other than those specifically related to parks and recreational programs and activities, are not allowed



### TC5 Building Placement

- Locate park structures within reasonable viewing distance from adjacent streets, or park walkways and trails, for easy access and visual surveillance
- Locate park structures near primary intersections, pedestrian crosswalks and bridge crossings to create visual markers and interesting objects within the park landscape – frequently these locations can be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent activity node and will likely become future landmarks



### TC5 Parking Strategies

- Provide on-street parking spaces adjacent to all public and private parks
- Provide options for additional/overflow parking on park property to support holiday traffic and special events including porous pavements and turf parking areas (Netlon and other soil amendments)
- Minimize surface parking lots on park property whenever possible to maximize green space
- Locate surface parking lots away from the major streets and provide adequate screening that includes plantings, fencing, earth berms or other attractive solutions

### Overview

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for the Town Center. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Architecture guidelines include the following:

### TC5 Building Design, Character and Expression

- Create structures that reflect, and perhaps blend qualities of both the natural landscape and the unique urban form of various sub-districts and specific blocks within the Town Center

- Provide structures that add character, comfort and support proposed uses within each park
- Provide structures that are unique, highly detailed and durable
- Provide structures that heighten the overall design character and quality of the Town Center, where park buildings, as a separate category, distinguish themselves as great architecture

### TC5 Scale, Proportion and Building Height

- Emphasize intimate scale to support street level activities and promote pedestrian comfort
- Emphasize scale and proportion that will complement, not compete, with surrounding character of park space

### TC5 Materials

- Provide a diverse mix of materials, applied in a variety of proportions and design details
- Encourage the use of durable, high quality materials, including brick and stone, together with woods and metals to promote the idea of permanence



### Overview

The overall structure of the Town Center is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within the Town Center. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles. Public realm and streetscape guidelines include the following:

### TC5 Parks, plazas and other public gathering spaces

Public spaces throughout the Town Center should:

- Blend both contemporary and traditional motifs with a focus on pedestrian use
- Encourage a balance of natural/informal landscapes with formal/urban plazas and squares; a mix of pavements and plantings, promenades and meandering trails
- Integrate stormwater management practices and features into every park design
- Provide a variety of opportunities to capture different microclimates with many choices for seating - including movable chairs, fixed benches, open lawn - and access to both sun and shade
- Provide a variety of levels or elevations to add interest to the ground plane and to provide informal steps, or lawn terraces for sitting
- Promote safety and security in all design solutions, including landscape lighting, open views into the site, and accessibility
- Provide seasonal interest and color; promote high quality materials and finishes
- Provide opportunities and specific sites for public art, special events, performance and markets



### TC5 Street/sidewalk dimensions and streetscape elements

Destination streets (like Sunwood), connector streets (like Rhinestone), and background streets (like many of the residential neighborhood streets) all front on public parks and all contribute to the desired connectivity throughout this new community. While each street must provide a defined level of service for the automobile, creating attractive and inviting amenities for pedestrian and bicycle use is equally important. Streets adjacent to public parks in the Town Center will include the following:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the various sub-districts
- Provide off-street bicycle trails wherever possible
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for each sub-district
- Explore street light spacing, height and type of fixtures to support uses and activities (note, however, that too much light is as bad as too little)
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)



### **TC5 Bicycle/pedestrian connections (on-street and off-street)**

The Master Plan describes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect sub-districts to various amenities within the Town Center and to the rest of the City. Proposed park improvements include off-street pedestrian and bicycle paths including grade-separated crossings through Town Center Park at Rhinestone and Zeolite Streets. Bicycle riders may choose to share the street with vehicular traffic, but there is no provision for dedicated bike lanes on any of the Town Center streets at this time. Providing and maintaining bicycle and pedestrian connections should be a key component for all development projects in this sub-district.

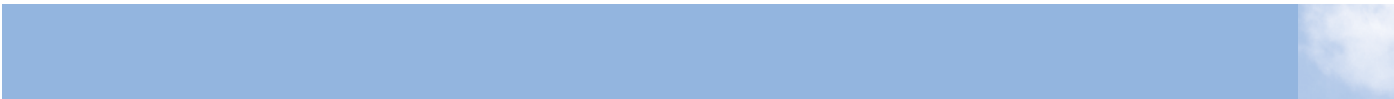
small-scale greens provide important 'breathing space', contribute to stormwater management and offer additional recreational amenities (tot lot, sitting areas) to serve specific building tenants or residents. Back gardens, auto-courts and other options should be considered as part of every residential site development plan.


### **TC5 Residential courts, pocket parks and other private green space**

Along with proposed public parks and great streets, there are many opportunities to integrate private courtyards and pocket parks within the Neighborhood Sub-district. These

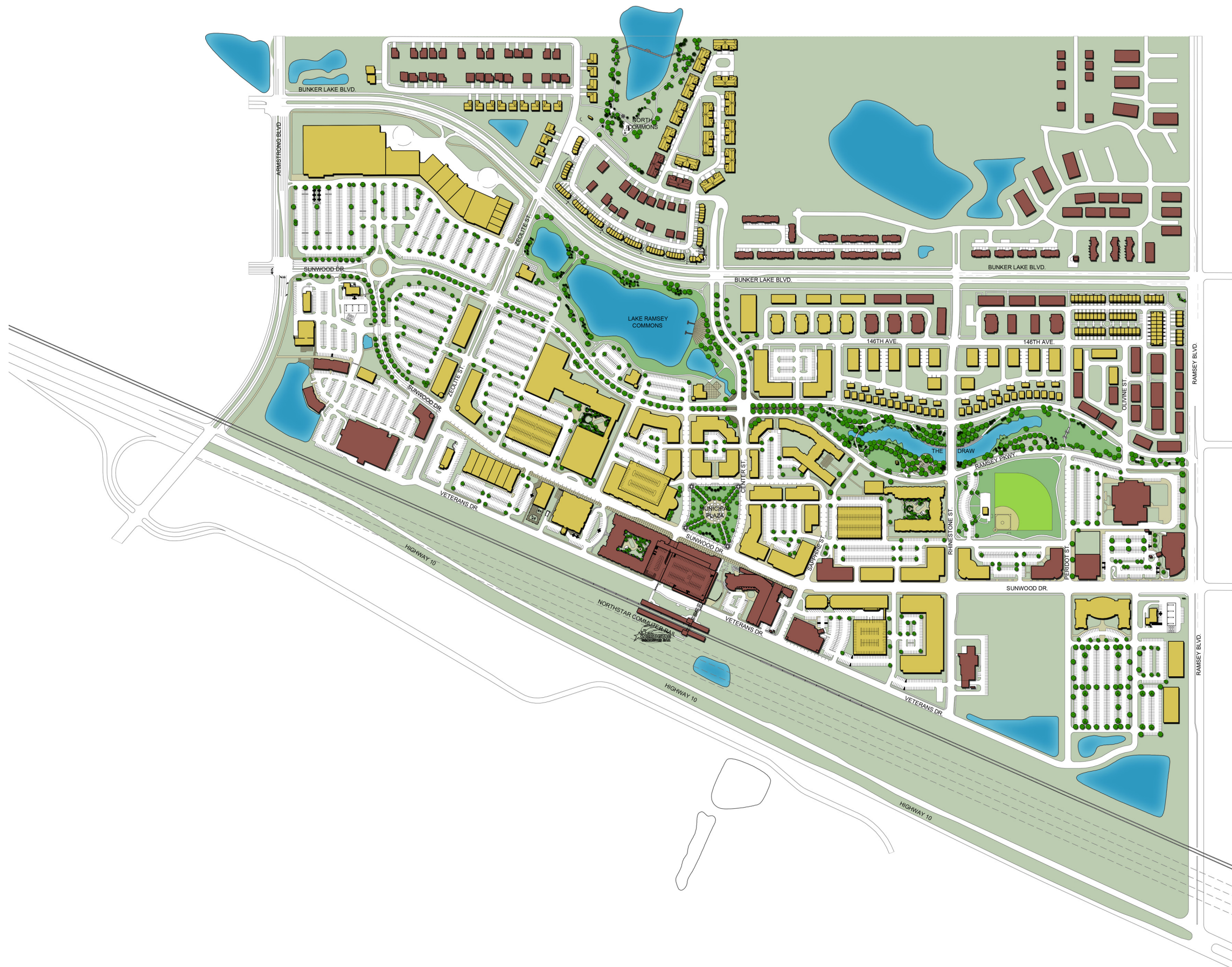


# Sub-District Guidelines - Signs & Lights



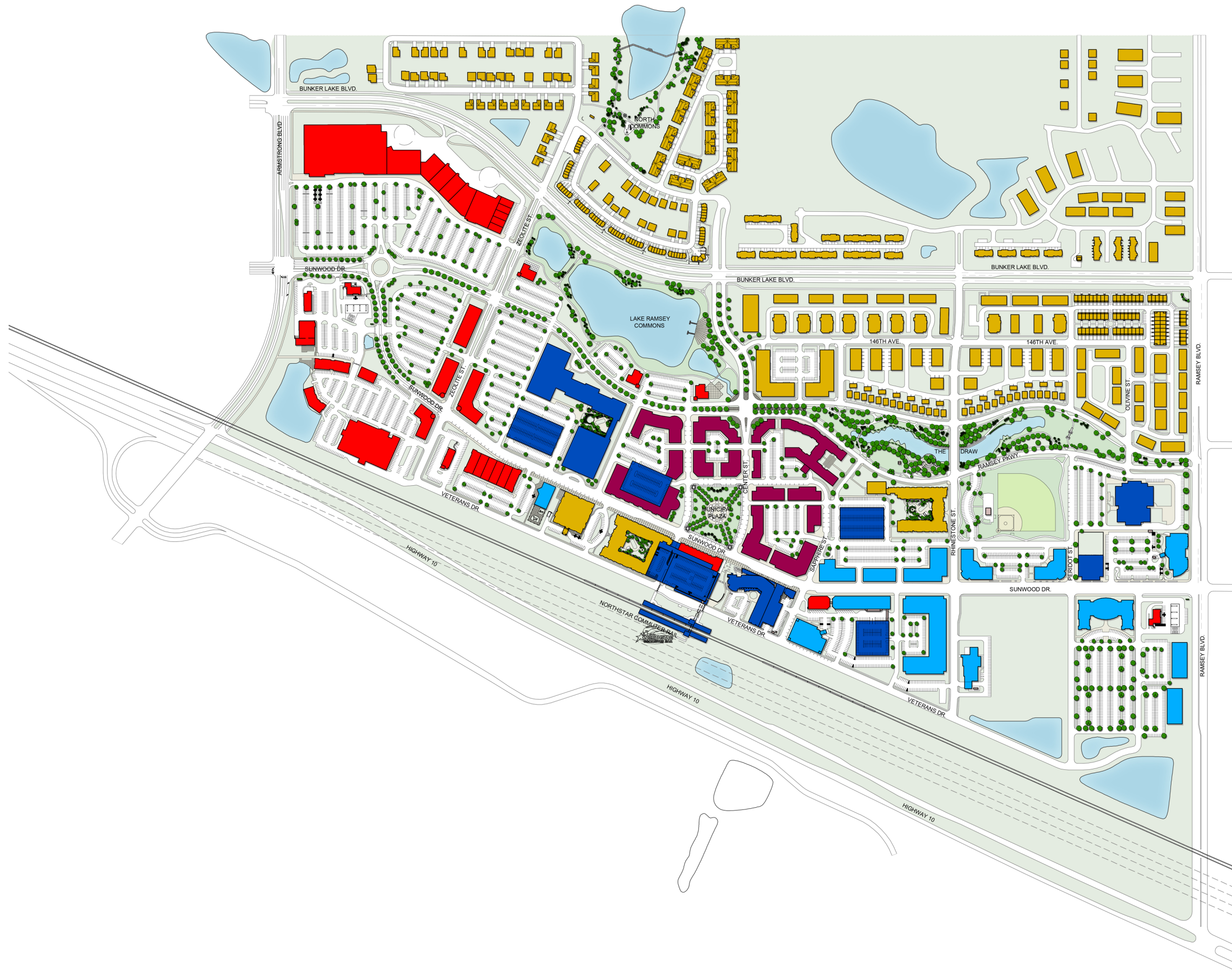


Ramsey  
Town Center,  
L.L.C.



**DEVELOPMENT STATUS:**

- Existing Development
- Proposed Development



**DEVELOPMENT LAND USE:**

- Commercial (retail, restaurant)
- Office (office, medical, hotel, day-care center)
- Public/Institutional (government, school, church, parking structure)
- Residential (condo, apartment, senior housing, townhomes, single family)
- Mixed Use (residential, commercial, office)



# Design Framework



February 28, 2012

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### Appendix A: Specifications and Details

## **Design Framework – The COR**

### **Purpose**

The primary purpose of Design Framework is to set basic parameters, describe preferences and illustrate design intent. These framework standards serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

### **Application**

The format and content of this framework are specifically tailored for use as a supplement to the Zoning Ordinance. This document is organized into two sections. The first section outlines issues and recommendations that apply to the entire COR. The second section highlights specific framework that apply to each sub-district within The COR. The City of Ramsey has adopted Development Plan 5.03 as the Master Plan for The COR. This master plan provides illustrative guidance for building locations and orientation. Any changes to this approved master plan must be reviewed and approved by the City Council as part of the development application. This Development Plan will be updated by the City as changes are approved.

### **Implied Responsibility**

All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

## Part One: Overall Framework

There are a number of framework standards that apply to the entire COR, including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall COR framework standards overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Framework:

- Context – local, city, regional
- Street Hierarchy--access, circulation, arrangement
- Streetscape—preferred design, location
- Parking -- quantity, location, type
- Building Design-- preferred uses, horizontal/vertical mixed use, built form, character of development
- Signage—design, location, quantity, type
- Stormwater Management-- surface water features, stormwater management
- Parks/Public Spaces – parks, trails and open space

It is essential that proposed design solutions for development projects and other improvements within The COR demonstrate an understanding of the interplay between these layers.

## Overall Framework - Context

### Overview

The COR is a unique area within the City; but it is a part of, and connected with, a variety of local, city-wide and regional systems (See Figure 1: Development Master Plan 5.03). Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

### Guideline Recommendations

To ensure that The COR takes full advantage of local and regional systems, development should:

- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/stormwater management plan

### Objectives

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

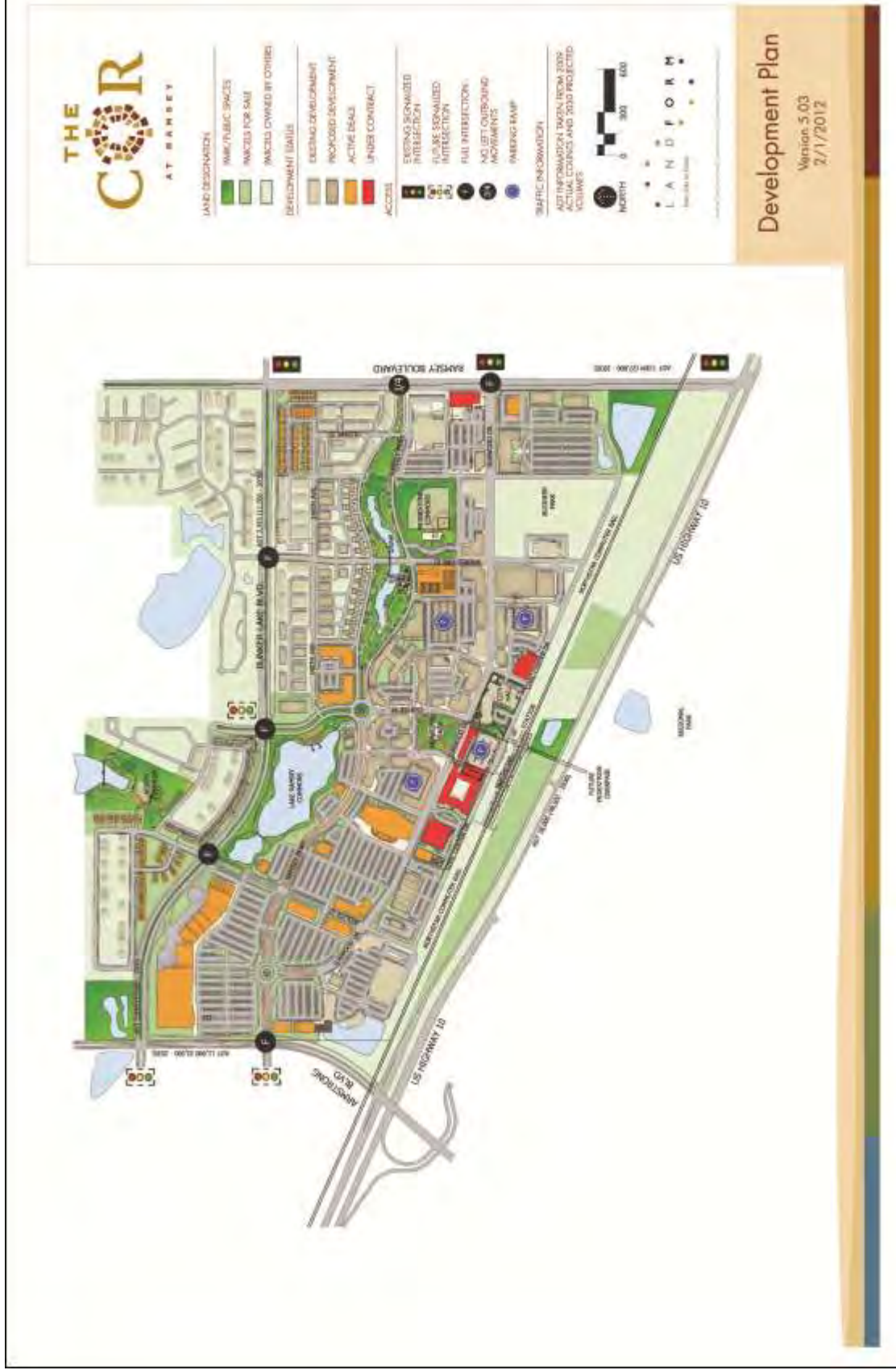


Figure 1: Development Master Plan 5.03

## Overall Framework – Street Hierarchy

### Overview

The Development Master Plan defines a specific strategy for the layout of streets and blocks within, and around The COR. The size, type and configuration of this street hierarchy (See Figure 2: Street Hierarchy) is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample space for street-side activities, strolling promenades, pockets of green and other amenities is critical to the success of The COR. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as a primary way to move about and conduct business in the core area.

### Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in The COR and encourages a more dispersed and less congested traffic flow (many choices or travel paths).
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout The COR.
- Work with Anoka County to convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches.
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street).



### Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Arterial street – these streets provide the boundary of The COR and all access into The COR will be from one of these three streets:
  - Bunker Lake Boulevard
  - Armstrong Boulevard
  - Ramsey Boulevard
- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities
  - Sunwood Drive



- Parkway – premier ‘green streets’ adjacent to primary public park / green space, with more landscaping than other streets in The COR
  - Ramsey Parkway
  - Center Street (north of the roundabout)
- Connector street – primary routes that link various districts within The COR, usually providing the most direct means of getting from point A to point B
  - Rhinestone Street
  - Zeolite Street
  - Veterans Drive
- Downtown street – the most urban of street sections where high pedestrian traffic is intended and traffic calming measures such as on street parking, bump outs and other methods will be employed
  - Center Street (south of the roundabout)
  - Sapphire Street (north of Sunwood)
  - Other streets as shown on the Street Hierarchy Map
- Local street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout The COR.



These street hierarchy classifications are intended to provide guidance regarding the streetscape, but it should be noted that within a street classification, there may be construction or design distinctions, based on the sub-district classification. For example, Sunwood Drive is defined as a destination street throughout The COR, but the design may be different between the COR1 and COR2 sub-districts. The Master Streetscape Map will provide design direction.



### *Transit Overview*

The COR is designed to be a Transit Oriented Development (TOD) in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multimodal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. The COR is the Twin Cities' first and only transit-oriented development along the new Northstar Commuter Rail Line. Ramsey Station at The COR will open for riders boarding the train in November 2012. As Northstar's most visible station, The COR will immediately bolster the ridership on the line and continue to build recognition for this regional service.



Transit components include the following:

- Ramsey Star Express service provides peak period, peak direction bus service between downtown Minneapolis and Ramsey until the Northstar Commuter Rail station stop is complete
- Northstar Commuter Rail provides train service between Big Lake and downtown Minneapolis
- 350 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within The COR and into the surrounding neighborhoods
- The COR can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station



The COR also encourages developers to participate in a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. The TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions.

- Developers shall submit a TDM plan with their site plan application for new development and work with the City to implement their plans.

### *Transit Guideline Recommendations*

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in The COR, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following framework:

- Understand and take advantage of opportunities to use existing and proposed transit components

- Provide transit related facilities specifically linked to uses and character within each COR district, including signage and lighting for way-finding and bicycle amenities
- Building future Northstar Commuter Rail riders through the Ramsey Star Express bus service

### *Transit Objectives*

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. The framework for The COR promotes public transportation as part of the solution and suggests a variety of street improvements and other amenities to invite increased use. Photographs throughout the framework suggest some of the many possibilities.

## Overall Framework - Streetscape

### Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within and around The COR. The amenities within the public right-of-way -and immediately adjacent -play an important role in the aesthetics of The COR. Great streets are essential components of an attractive and inviting public realm. Developing a streetscape that is comfortable and inviting to pedestrians is important to realizing the vision for The COR. The framework will encourage interesting and inviting places for the public.

The Streetscape framework defines the visual character and physical improvements for all public spaces within The COR. The document also provides the general framework and examples for design of proposed quasi-public and private space located within each development parcel.

Public realm/streetscape framework focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space



The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. The City has developed a series of street cross-sections to illustrate these design expectations. Typical street sections are provided in Appendix A as a reference.

Destination and Connector streets must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)



The COR includes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.



Figure 3: Master Lighting Plan

## Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, including the following:

- Bike racks interspersed throughout The COR to encourage cycling as an alternative mode of transportation.
- Benches on the public sidewalks to encourage people to linger in The COR
- Trees and planters along the streets to create an inviting space
- Wide sidewalks and both on- and off-street bicycle trails
- Kiosks and other way finding tools for visitors and residents in The COR
- Outdoor dining is encouraged on the street side of buildings within all districts.
- Public plaza space is strongly encouraged to be located adjacent to the streets



## Objectives

Creative design solutions for streetscape improvements should include:

- Streetscape improvements that provide traffic calming, particularly along Destination and Downtown streets
- Inviting streets that draw you into and through The COR
- A marriage of building design and streetscape design that encourages active spaces near the streets



## Street Trees and Plantings

Street trees and planter boxes are a critical element in the unique character of The COR and will be used to help distinguish between different street types and sub-districts within The COR. However, there may be variety within the same street when the street crosses into sub-districts.

- Street trees will be chosen to ensure that the species allow for clear pedestrian access along the streets and require minimal maintenance.
- Along Destination, Downtown, Parkway and Local streets, where regularly spaced blocks exist, one (1) tree species shall be provided per block. In order to provide variety and protect against disease, adjacent blocks shall not be allowed the same street tree. Initial development along a block shall establish the tree species for that block, subject to City review and approval.
- Along streets without regular block space or longer blocks, such as Connector and Arterial streets, the City may approve groupings of 3-6 trees of one (1) species in lieu of the regular block spacing.
- Alternatives to the above mentioned planting plan shall be subject to review and approval by the City Council.

## Destination Street Trees:

- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Bicolor Oak (*Quercus bicolor*)
- Sienna Glenn Maple (*Acer x freemanii* 'Sienna')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Ginko (*Ginkgo biloba*)
- Accolade Elm (*Ulmus x 'Accolade'*)

### Parkway Street Trees:

- Valley Forge Elm (*Ulmus americana* 'Valley Forge')
- Bitternut Hickory (*Carya cordiformis*)
- Siouxlant Poplar (*Populus deltoides* 'Siouxlant')
- Sienna Glen Maple (*Acer x freemanii* 'Sienna')
- Boulevard Linden (*Tilia americana* 'Boulevard')
- Northern Catalpa (*Catalpa speciosa*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Korean Mountain Ash (*Sorbus alnifolia*)
- Ohio Buckeye (*Aesculus glabra*)
- Ginkgo (*Ginkgo biloba*)

### Connector Street Trees:

- Bicolor Oak (*Quercus bicolor*)
- Northern Red Oak (*Quercus rubra*)
- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

### Downtown Street Trees:

- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

### Local and Arterial Street Trees:

Local and Arterial Street trees may be chosen from any species on the above lists.

### Planter Beds/Boxes/Tree Grates

- Planter beds, tree grates and planters shall be as shown on the Streetscape Master Plan. Details and specification shall be added to Appendix A as developed and approved by the City. Appendix A shall be amended as needed.
- Irrigation shall be provided to ensure survival of plant materials. The City shall provide irrigation systems along the Parkway and Destination streets. Unless otherwise approved, all other irrigation shall be the responsibility of the landowner.

### Street Furniture

Bike racks, trash enclosures and benches are encouraged on all streets, but will be required on Destination, Connector and Parkways and other streets as shown conceptually on the Streetscape Master Plan.

Bike racks shall be the standard bike rack (see Appendix A for specifications).

Trash enclosures shall be the standard trash enclosures (see Appendix A for specifications).

Benches shall be the standard bench (See Appendix A for specifications).

Benches shall be grouped in twos that face each other and provide an opportunity for an outdoor conversation space.

Alternative bike rack, trash enclosures and bench designs will be considered on a case-by-case basis. The City will review alternative designs as part of the site plan review and will evaluate these alternatives based on the following:

- Does the design maintain the character of the district?
- Does the alternative design maintain the quality of the standard feature?
- Does the design provide a relationship between the public elements of the streetscape and the building that would not otherwise be possible?



### Sidewalks and Boulevard

In the design framework, the term “boulevard” is defined as “the space between the sidewalk and the curb.” The boulevard area provides the required space for public features such as street lights, benches, fire hydrants, street signs, etc. The boulevard will have different characteristics based on the street hierarchy.

The street width and layout within different zones may affect the ability of a particular development to provide the minimum boulevard width and the minimum sidewalk width within the existing public right-of-way. In those cases, the landowner shall provide a permanent public easement over the portion of the sidewalk that is located on private property.

### *Street Lights*

Street lights are owned and installed by Connexus Energy. The streetlight design was developed in conjunction with the City of Ramsey and Connexus (See Figure 3: Master Lighting Plan). The standard street lights are designed to accommodate banner arms should banner signage be desired. The banner arms should be the manufacturer's standard break-away banner arms to ensure safety and minimize maintenance costs. Connexus provides street light maintenance for the City. Connexus bills the City of Ramsey for the cost of installation and maintenance.

The choice of street light was intended to emphasize the pedestrian scale of The COR. Light poles within the public right-of-way should be planned to accommodate banners, flowers, hanging artwork and electrical outlets.

There are three (3) different streetlight types within The COR:

- 24-foot high poles at intersections
- 14-foot high poles at mid-block
- 12-foot high poles at The Draw



The City will ensure that the appropriate streetlights are installed on all public streets as described by the Design Framework. Private developers shall provide parking lot or other project lighting that retains these design

objectives. All lighting shall comply with City Code performance standards for lighting. All lighting shall be reviewed by City staff to ensure compliance with the Design Framework and City Code standards described above.

### *Standards for Outdoor Dining.*

Outdoor dining is encouraged as an attractive addition to a pedestrian- and retail-friendly, vital COR. Such dining is encouraged subject to the standards in Section 117-118 of the Zoning Ordinance.

## Overall Framework - Parking

### Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of The COR is a significant challenge. The goal is to provide adequate parking; both quantity and location, while promoting new strategies that support the overall needs of The COR. Parking will be provided through a combination of on street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of The COR. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street level uses should be integrated into ramp design along important street frontages. The City also encourages developers to wrap the ramp with multi-story uses that will provide commercial and residential opportunities and help to define the street edge.

### Guideline Recommendations

Parking guidelines encourage the following:

- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views, soften parking area edges, provide shade, integrate native plantings, offset islands and reduce to 'sea of parking' image
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering
- Provide parallel parking on both public and private streets throughout The COR, to add parking and create a traffic calming, pedestrian buffer; Bumpouts are encouraged as a traffic calming measure on all streets within the COR.
- Explore options to integrate stormwater management requirements into parking area design
- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on street, small surface lot, underground and ramped parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area

- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Discourage driveway access to parking facilities along Arterial, Destination and Parkway streets, in favor of side streets.

### Objectives

The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in The COR. The Design Framework anticipates the construction of a total of 3 parking structures in COR1 to serve the needs of the area by providing structured parking that can be shared among users, allow developers to maximize the development on each individual lot.

As discussed later in this section, sizing and timing of the structured parking will be finalized as the area develops and parking demand grows. In addition to providing adequate space, excellent design is a major factor as well. The framework for The COR encourages creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs throughout the Design Framework suggest some of the many possibilities.



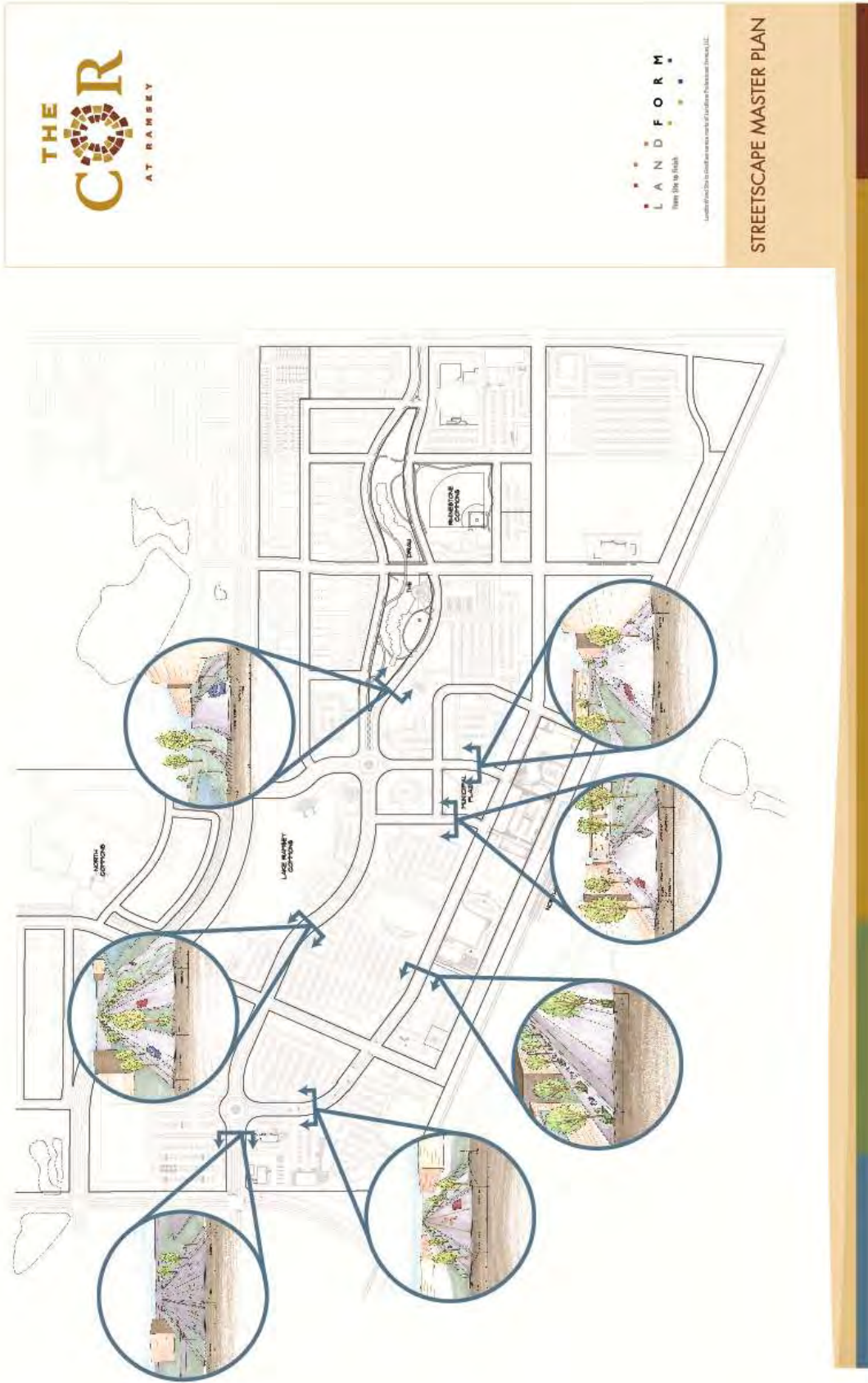
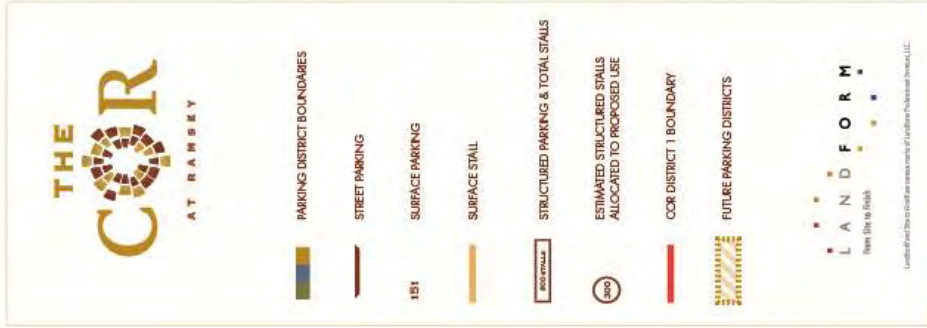


Figure 4: Streetscape Master Plan



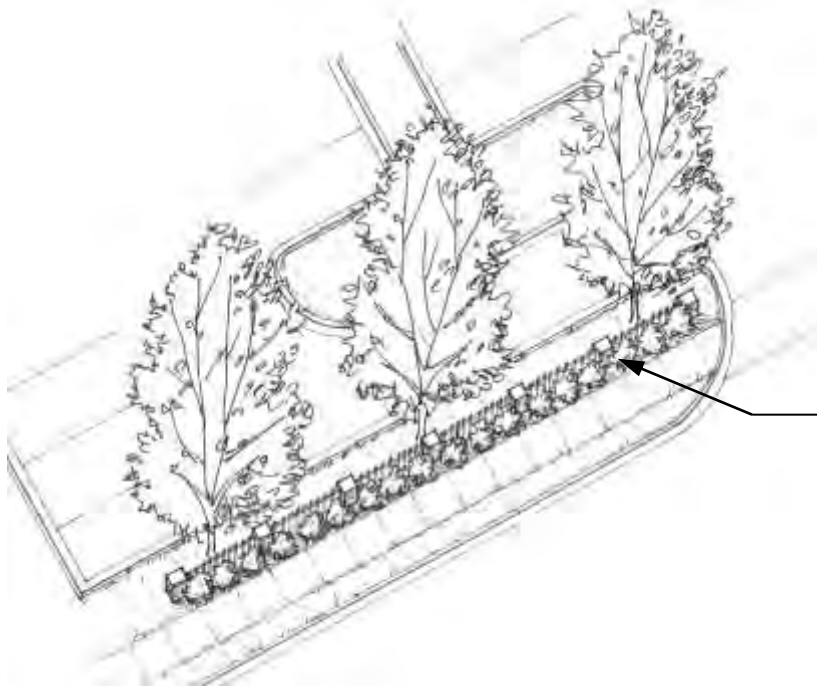
**Parking Districts**  
 VERSION 5.03  
 9/27/11

Figure 5: Parking District Map

### *Minimum Standards*

Parking in The COR is intended to be shared to the greatest extent practicable in all mixed-use areas. The parking standards are intended to provide a practical basis for providing adequate parking within the COR District through a careful analysis of uses, shared parking arrangements, use of public street parking and reduction allowances for the proximity to the transit station. A parking plan has been prepared to identify the planned number, location, sharing arrangements and public use (see Figure 4: Streetscape Master Plan and Figure 5: Parking District Map). The parking plan provides information that can be used to help address longer term parking arrangements where the possibility of use conversion or building expansion exists. Parking shall be provided under any of the following arrangements:

- A. No parking shall be located between the front of the principal structure and the street on parcels abutting Destination and Downtown streets in COR1.
- B. Not more than fifty (50%) percent of the parking shall be located in front of the principal structure and the street on parcels abutting Arterial, Parkway, Connector and Local streets.
  1. In the COR2 district, developments may be allowed more than fifty (50%) percent parking in the front yard, provided all other design standards have been met, including the use of a wall, railing, hedge or combination to create a street edge and screen parking.
- C. Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4<sup>1</sup>/<sub>2</sub>') above the level of the parking lot, at the build-to line required for the property. The amount of required screening may vary by sub-district and street frontage and is described in Part Two of this document.



Landscaped edge that combines fence with trees, low shrubs and ground cover for parking lot screening and buffering along sidewalk to screen parking

- D. No parking spaces shall be located on corner lots at the point of street intersections.
- E. Shared parking in COR1 will be accomplished through a Parking Use and Maintenance Agreement (PUMA).
- F. Off-street parking in the COR2 and COR3 sub-districts shall be secured for public use through parking easements and other appropriate conveyances. Shared parking arrangements between nearby uses are encouraged in both sub-districts.
  - 1. The City may approve joint parking for one or more businesses where the total number of parking stalls provided for joint use is less than the sum of the total required for each business should they provide them separately. The applicant must demonstrate the feasibility of the arrangement in a written report. Such a permit shall not be granted except when the following conditions are found to exist:
    - i. Proximity. The building or use for which application is being made to utilize the off-street parking facilities provided by another building or use shall be located within three hundred (300') feet of such parking facilities, excluding public rights-of-way.
    - ii. Conflict in Hours. The applicant shall demonstrate in documented fashion that there is no substantial conflict in the principal operating hours of the two (2) buildings or uses for which joint use of off-street parking facilities is proposed.
    - iii. Written Consent and Agreement. A legally binding instrument, executed by the parties concerned, for joint use of off-street parking facilities, duly approved as

to title of grantors or lessors, and in a form and manner of execution approved by the City Attorney, shall be filed with the City Clerk and recorded with the Anoka County Recorder or Registrar of Titles, and a certified copy of the recorded document shall be filed with the City within sixty (60) days after approval of the joint parking use by the City or the interim use permit shall be considered null and void.

- G. On-street parking adjacent to buildings shall not be used for the purposes of calculating parking requirements.
- H. Within the COR-4 sub-district, parking on individual parcels serving individual uses may be provided if designated and approved as part of the master plan.
- I. Within all sub-districts, a minimum driveway length of twenty-five (25') feet is required to limit conflicts between vehicles parked in the driveway and street or sidewalk users.
- J. In order to ensure the pedestrian orientation of the COR Districts, minimum/maximum on-site parking standards are set based on the following table:

**Table 1**

Use	Minimum number of parking stalls	Maximum number of parking stalls*
Retail	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Restaurants	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Offices	2 per 1,000 sq. ft.	3 per 1,000 sq. ft.
Medical offices, clinics	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Health clubs	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Theaters, places of assembly	1 per 5 seats	1 per 3 seats
Residential		
Attached or detached	1 per unit	2 per unit
Multifamily units	1 per unit	2 per unit

\*If a parking structure is provided on site, maximum parking stalls do not apply.

- K. Non-Specified Uses. For uses not specifically listed above, off-street parking requirements shall be computed by the Zoning Administrator on the same basis as required for the most similar listed uses. In such cases, the Zoning Administrator shall also consult off-street parking reference materials including, but not limited to, manuals prepared by the American Planning Association and Institute of Transportation Engineers.
- L. Handicap Parking. The size, number, and location of stalls reserved for handicapped parking shall be provided and identified as required by applicable regulations. These spaces are included in the computation for the minimum parking space requirement.
- M. Change in Land Use. When the site intensity or use of a building and/or property is increased with consequential effect upon the parking requirements as prescribed in this Section, the

parking requirements as prescribed herein shall be used to provide for such increase in the site intensity and/or use.

- N. Use of required parking spaces. Required parking spaces must be available for the use of residents, customers or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.
- O. Transit Service Reduction. The minimum number of parking stalls required on site may be reduced by 10% for any parcel located within ¼ mile of a transit stop. To qualify for this reduction, the transit stop must provide regular service on all days of the week and direct pedestrian access must be provided between the building and the transit stop.
- P. Bike Racks. In addition to the bike racks provided as part of the streetscape, individual businesses are encouraged to provide bike racks for customers. Bike rack locations will be reviewed and approved by the City as part of the site plan review.
- Q. Increase to Maximum Required Parking. If additional parking is sought to exceed these maximum requirements, a conditional use permit can be sought to increase maximums up to twenty-five (25%) percent.
  - 1. In the COR-2 sub-district, an increase of twenty-five (25%) percent in the maximum number of parking stalls is permitted without the issuance of a conditional use permit if approved as part of the site plan.
- R. In the COR1 sub-district, a PUMA (Parking Use and Maintenance Agreement) shall be applied to all development as follows:
  - 1. It is the City's intent to maximize development on individual lots. In order to help landowners accomplish this goal, the City will develop public parking ramps, which will be available for public parking within The COR. In order to minimize the amount of the site required for parking, the landowner may choose to enter into the PUMA for utilization of the public parking ramps to meet all or a portion of their minimum parking requirements.
  - 2. Each development shall be subject to a PUMA that specifies that each property in the COR1 shall be financially responsible for its proportionate share of a shared parking facility. The proportionate share shall be determined on the basis of the property's off-street parking needs, as determined by the parking requirements of Item J above.

3. The City shall establish fees by ordinance. The developer shall be responsible for payment in an amount equal to the value of the required parking on a per-stall price basis for ramp parking through the PUMA. The City will determine the appropriate contribution.
4. Funds collected by the City shall be deposited in a special fund used only to acquire and/or develop off-street parking facilities for the COR. Financial responsibility shall cover the construction and continuing maintenance of the parking facility.
5. The parking facility may be constructed and maintained by the City or by a private management entity acceptable to the City.

### *Private Streets*

Private streets may be allowed within residential subdivisions, subject to the standards in Section 117-112 (e)3.b. of the Zoning Ordinance.

### *Loading Areas*

- A. Loading areas and docks shall be located in the rear of the building.
- B. Loading areas shall not be located in the front yard and shall be fully screened from public streets, public parks and residential areas through landscaping and building design measures.



## Overall Framework – Building Design

### Overview

The COR Development Plan 5.03 identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, this Design Framework focuses on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

### Guideline Recommendations

The mix of land uses in The COR will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, The COR resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of The COR, anchored by City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.

### Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street, particularly in the center of the district.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, ‘big box’ retailing and auto-oriented uses within the COR2 sub-district only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a ‘family’ or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district.
- All uses should reinforce the street edge.



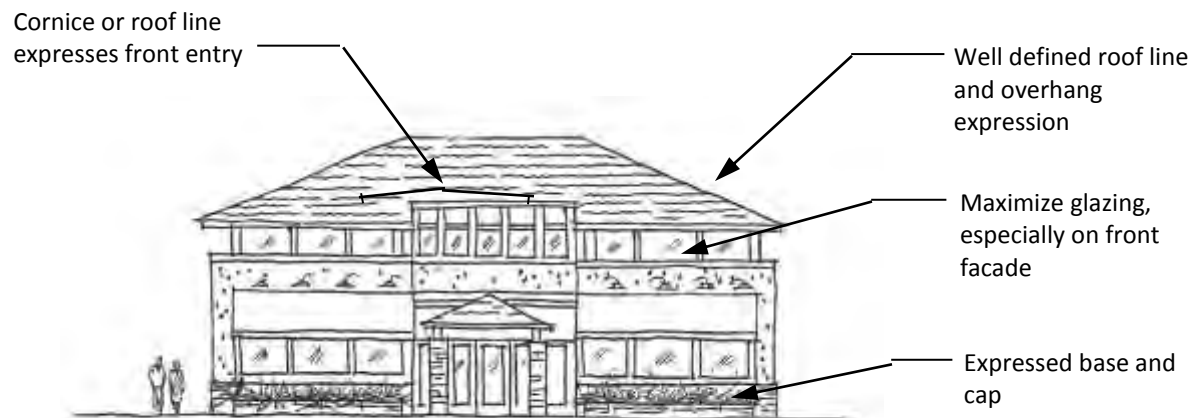
## Architectural Overview

High quality architecture is an essential component in fulfilling the objectives of the Design Framework. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

## Architecture Recommendations

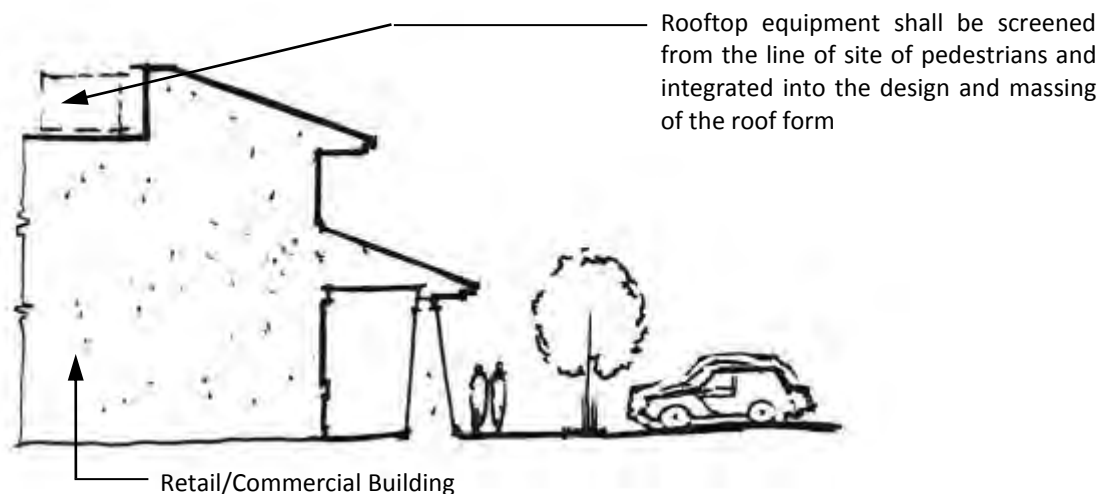
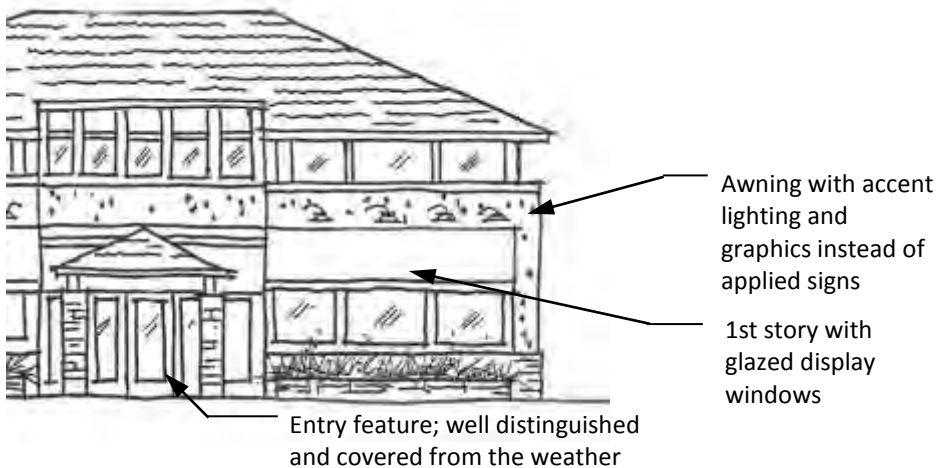
Of all the layers that combine to form The COR Development Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for The COR. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- An animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience

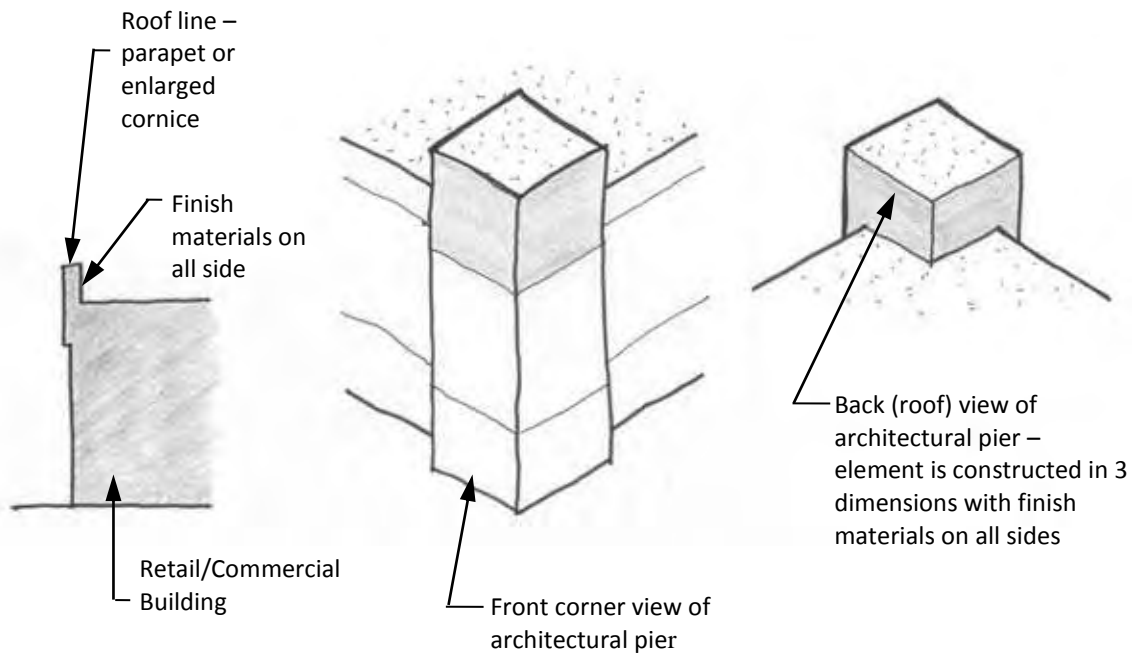


- Variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other COR uses
- Use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets and important crossroads throughout The COR
- Buildings should always be located at all four corners of intersections.
  - In the COR2 sub-district, if the City finds that there are unique circumstances where buildings cannot be located on the corner, the City may approve the use of fences, walls or other design elements to create that "street edge"

- Buildings will be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, or where streets intersect with parks, such as the streets that front on or end near the Municipal Plaza. These locations can often be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as The COR develops
- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.
- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Use recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage



- Equipment, mechanical systems, transformers, etc. must be screened from view in a way that is integral to the architecture of the building. Screening should be accomplished with materials similar to or compatible with materials used on the main structure; metal fencing or mechanical vent screens alone are not sufficient.
- Roof lines and cornice details shall be completed in a 3-dimensional manner so that the back of roof features or similar unfinished areas are not visible.



## Overall Framework - Signage

### Overview

Signage within The COR should provide a system for clear wayfinding for all modes of transportation and should ensure successful business operation while maintaining the design aesthetic of this unique project. These sign standards are intended to allow flexibility and creativity while maintaining the design concepts of The COR. Due to the distinctive nature of The COR, the sign standards from the City Code are not applicable and signage shall be allowed as prescribed by the Design Framework. A Master Sign Plan identifies the location of key sign locations (see Figure 6: Signage Plan).

### Guideline Recommendations

The COR development manager shall prepare a Master Sign Plan for the public elements of the project, which are in compliance with the design framework and will be adopted by reference. The Master Sign Plan will specifically address public signs including:

- The three (3) off-site community signs
- Banner signs on light poles
- Signage for public parks
- Signage for public parking ramps
- Signage for public buildings
- Community kiosks



### Objectives

Signage should be used as a tool to help identify businesses and neighborhoods with The COR and should have elements that are focused on the pedestrian nature of The COR as well as the signage needs for businesses.

- Signage and lighting which is integrated into the design of the building is encouraged.

### Definitions

*Awning Sign* means a sign incorporated into or attached to an awning.

*Community sign* means an off-site sign identifying the development name and key tenants. Community signs may also include public information. Reader boards are allowed to be incorporated into community signs within The COR.

*Temporary Sign* means a sign which is erected or displayed for a limited period of time and not affixed to a Minnesota State Building Code approved structure.

*Project sign* means a freestanding area identification sign which identifies a single-family or multifamily residential subdivision, a commercial development or an industrial park or office park and which is located on the same parcel as the development it identifies.

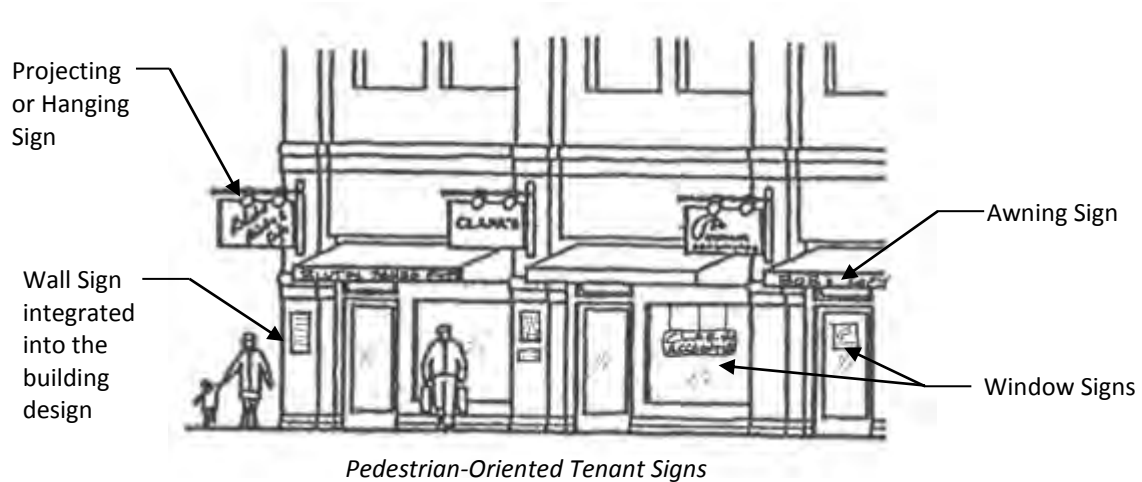
*Projecting sign* means any sign, all or any part of which extends beyond the surface of the building or wall by more than 16 inches.

*Public Realm/Streetscape sign* means any sign erected and maintained by public officials or public agencies, or approved and authorized for use by state or local governmental authorities.

*Sandwich Board Signs* means a self-supporting, freestanding temporary sign with only two (2) sides that are situated adjacent to a business with the intent to attract pedestrian traffic to businesses. Sandwich board signs are not meant to be read by vehicular traffic.

*Wall sign* means any sign which is affixed to a wall of any building. This definition includes individual letter signs and signs on mansards.

*Window sign* means a sign installed inside a window, or any sign placed within a building for the purpose of being visible from the public right-of-way. This does not include merchandise on display.



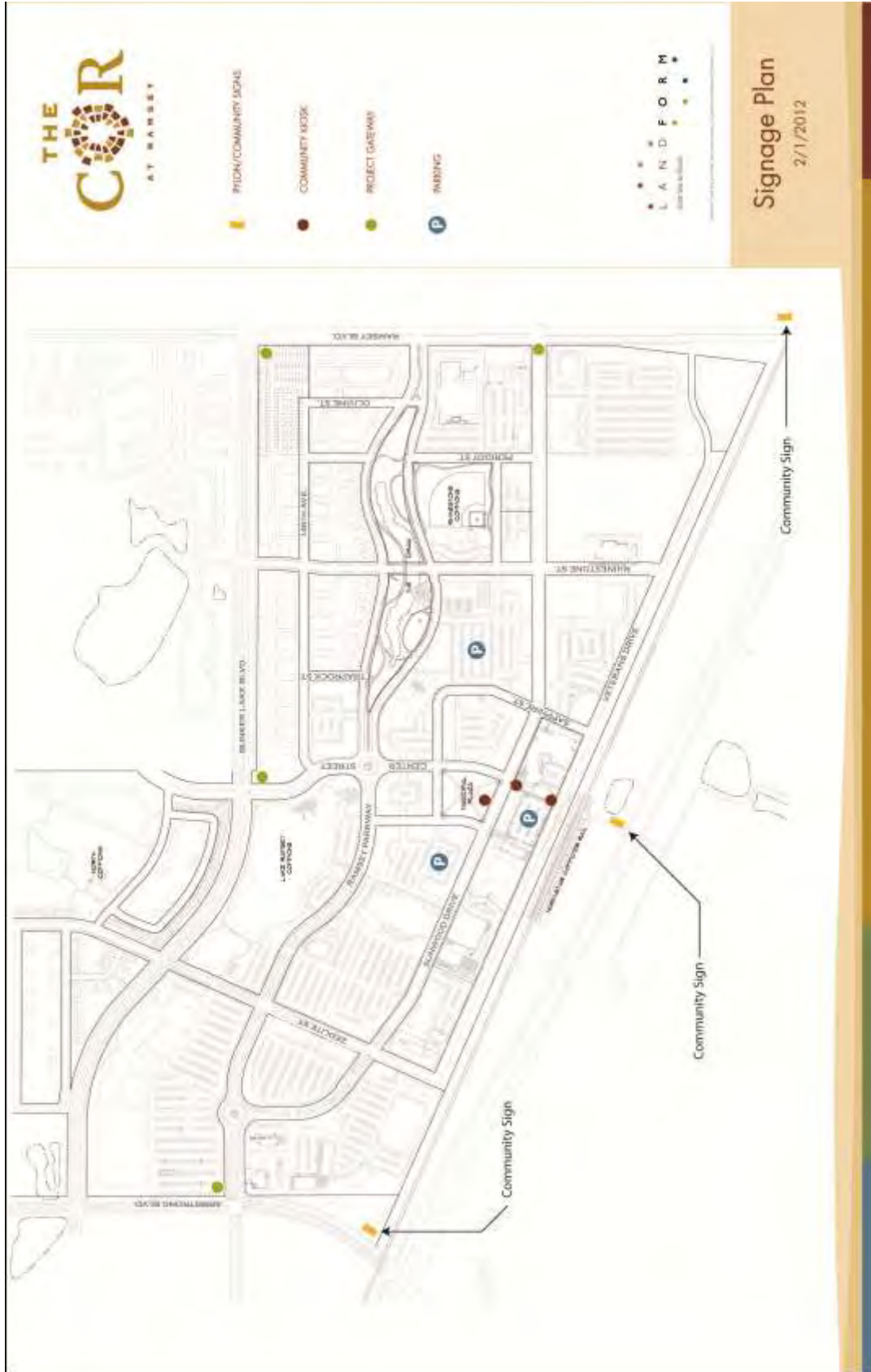


Figure 6: Signage Plan

## Standards

### Public Realm/Streetscape Signs

The pedestrian oriented nature of The COR requires unique signage that will build on the design concepts for The COR. These types of signs include banner signs on the streetlights, kiosks, street signs, wayfinding signs, signage for public parking ramps, etc. As the design concept for The COR has been refined, so has the signage plan. The COR development manager shall develop standards for these public realm signs as part of the Master Sign Plan. The Master Sign Plan shall be incorporated by reference and may be updated from time to time. Such signage shall be allowed on public spaces as needed to provide information and wayfinding. No advertising shall be allowed on Public Realm/Streetscape Signs except for community events.

### Community signs

Community signs include are planned to include three (3) signs on Highway 10, as shown on the Signage Plan (see Figure 6: Signage Plan). The signs include the existing Community Sign near the future transit plaza, the sign at the northwest corner of Highway 10 and Ramsey Boulevard and the planned sign at the northwest corner of Highway 10 and Armstrong Boulevard.

These off-site signs are allowed as shown on the signage plan and shall have consistent materials and colors. The Master Sign Plan shall detail the materials, colors and standards for these three (3) community signs. The Master Sign Plan shall include standards for which tenants shall be allowed on the community signs.



*Desirable signs are designed to be architecturally compatible with buildings within the development and other signage within the development*

### Project Signs

Project signs are permitted within all COR sub-districts.

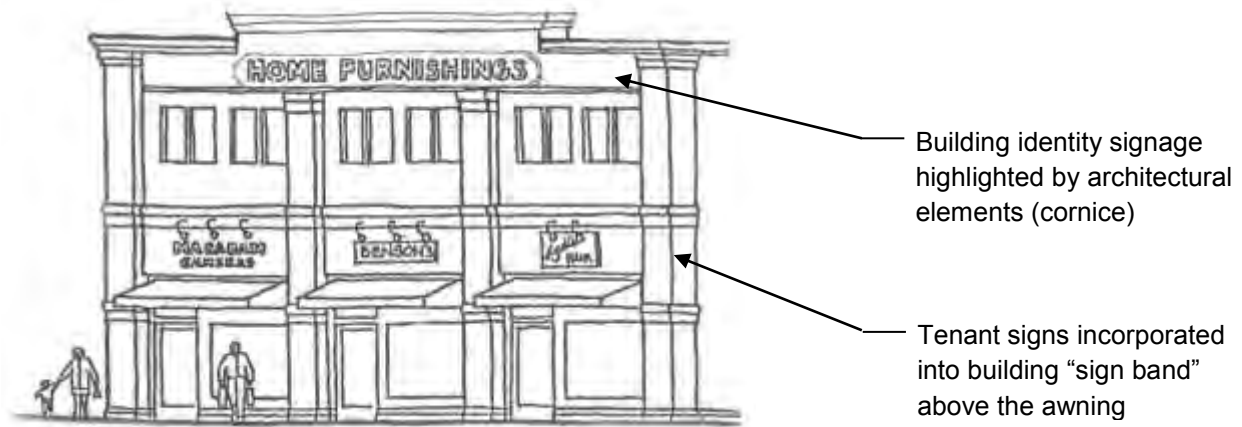
One (1) freestanding sign is allowed per building, provided that the following standards are met:

- A. The building in which the advertising business is located, shall be set back a minimum of six (6) feet from a public street right-of-way.
- B. The area of each face of the signboard shall not exceed six (6) square feet and the signboard shall not have more than two (2) readable faces
- C. The height of the top of the signboard, or of any posts, brackets, or other supporting elements shall not exceed six (6') feet from the ground.

- D. The signboard shall be constructed of wood, acrylic, aluminum or metal and shall be architecturally compatible with the style, composition, materials, colors and details of the building.
- E. No part of the sign shall encroach on the right-of-way and its location shall not interfere with pedestrian or vehicular circulation.
- F. Limited to one (1) sign per building and shall not be in addition to wall-mounted, applied letter or projecting signs.
- G. The readable faces of the sign shall be perpendicular to the adjacent street.

In the COR2 District, the following signage shall be allowed:

- A. Ground sign. There shall not be more than one (1) ground sign for each parcel. The gross surface area of a ground sign shall not exceed one hundred (100) square feet for each exposed face nor exceed an aggregate gross surface area of two hundred (200) square feet.
- B. Menu board. One (1) on-site menu board per drive-up or walk-up lane of a drive-in restaurant up to a maximum of thirty-two (32) square feet each and a maximum height of three (3) feet. Menu boards are allowed a message on one (1) side only and cannot contain an advertising message.
- C. Directional signs.
  - 1. Directional or instructional signs are permitted in accordance with Section 117-463(I).
  - 2. Parking lot directional signs designating parking area entrances and exits are limited to one (1) sign for each entrance and/or exit and shall not exceed four (4) square feet for each exposed face. Parking lot directional signs shall not project higher than five feet (5') in height, as measured from the established grade of the parking area to which such signs are accessory.
  - 3. Parking lot instructional signs designating the conditions of use or identification parking areas shall not exceed eight (8) square feet and shall not project higher than ten (10) feet in height for wall signs and seven (7) feet in height for ground signs, as measured from the established grade of the parking area to which such signs are accessory.



### Wall Signs

Wall signs are permitted within all COR sub-districts.

Wall signs shall be permitted on one (1) wall, except that lots with frontage on more than one (1) street may have signage on one (1) wall per street frontage.

Within the COR1, COR3, COR4 and COR5 districts, the following standards apply:

- A. Wall-mounted or painted signs, provided the following standards are met:
  1. The sign shall be affixed to the front facade of the building, and shall project outward from the wall to which it is attached no more than six inches (6").
  2. Single Tenant Buildings. Each building shall be allowed one (1) wall sign per street frontage. Signage must be located on the street elevation. The area of the sign shall not exceed five (5%) percent of the ground floor building facade area or fifty (50) square feet, whichever is less.
  3. Multi-Tenant Buildings.
    - a. Each building shall be allowed wall signage on the street frontage elevation. For buildings with multiple street frontages, the allowable signage is per street frontage. Signage must be located on the street elevation.
    - b. Each building is allowed wall signage that shall not exceed 5% of the wall area of the building façade adjacent to the street or 50 square feet, whichever is less.
    - c. Tenant signage must be located on the tenant lease space.
    - d. At least fifty (50%) percent of the allowed signage must be allocated to ground floor tenants and located on the ground floor.
    - e. Signage is limited to a maximum of one (1) sign per business/tenant, except that tenants with frontage on multiple streets may be permitted to have signage on each street frontage.
    - f. Multi-tenant buildings must have wall signs of similar design. Sign permits for tenants in multi-tenant buildings shall only be permitted by the City after the

building owner has submitted a comprehensive sign plan approved by the Zoning Administrator. The comprehensive sign plan for the building shall include similar design standards including sign material, color, style, spacing and size.

4. Signs for buildings facing Highway 10. Single or multi-tenant buildings that have frontage on Highway 10 or are separated from Highway 10 only by other public right-of-way, shall be allowed to have signage on that street elevation that is up to five (5%) percent of the wall area of that building façade with no maximum square footage.
- B. Wall-mounted building directory signs identifying the occupants of a commercial building, including upper story business uses, provided the following standards are met:
1. The sign is located next to the entrance.
  2. The sign shall project outward from the wall to which it is attached no more than six inches (6").
  3. The sign shall not extend above the parapet, eave, or building facade.
  4. The area of the signboard shall not exceed three (3) square feet, with each tenant limited to one (1) square foot.
  5. The height of the lettering, numbers, or graphics shall not exceed four inches (4").
  6. One (1) such sign is allowed per public building entrance and is allowed in addition to other permitted wall signage.
- C. Applied letters may substitute for wall-mounted signs, if constructed of painted wood, painted cast metal, bronze, brass, acrylic or black anodized aluminum. The height of applied letters shall not exceed 12 inches.
- D. Logos are considered signs and shall be included in the maximum allowable sign area.
- E. Window or door signs, provided that the following standards are met:
1. The sign shall not exceed ten percent of the window or door area or four (4) square feet, whichever is less.
  2. The sign shall be silk screened, hand painted, applied letters/graphics, neon tubing or other sign technologies that meet these standards.
  3. Limited to one (1) sign per business, applied on either the window or the door, but not on both.
  4. The sign shall not have an opaque backing of any type although smoked glass is allowed.

5. Window signage (includes graphics) shall be permitted on the same building walls that have, or are allowed to have, wall signage pursuant to Item A above. Window signage shall be deducted from the allowable wall signage area.

Within the COR2 district, where more conventional suburban development is expected, rather than the neo-traditional development planned for the other COR sub-districts, the following standards apply:

- A. Wall, canopy or marquee sign. Total sign area may not exceed 15 percent of the front building facade. At least 50 percent of the signage area must be placed on the measured wall with remaining signage area, if desired, distributed on any other wall. Sign height shall not exceed the top of the parapet wall or, if no parapet wall, sign height shall not exceed the height of the eaves. The gross surface area of a wall, canopy or marquee sign may be increased by ten percent if such wall sign:
  1. Consists only of individual, outlined alphabetic, numeric and/or symbolic characters without background except that provided by the building surface to which the sign is to be affixed;
  2. Illumination, if any, is achieved through shielded illumination, shielded silhouette lighting, or shielded spot lighting but not any lighting where the light source is visible or exposed on the face or sides of the characters; and
  3. A wall, canopy or marquee sign may be located on the outermost wall of any principle building but shall not project more than 16 inches from the wall to which the sign is to be affixed. A wall sign shall not project higher than the parapet line of the wall to which the sign is to be affixed.
- B. Window signs are restricted to 30 percent of the area of the window in which the sign is to be displayed.

In all COR sub-districts, restaurants and cafes shall be permitted one (1) wall-mounted display featuring the actual menu as used at the dining table, to be contained within a shallow wood or metal case and clearly visible through a glass front. The display case shall be attached to the building wall, next to the main entrance, at a height of approximately five feet (5'), shall not exceed a total area of two square feet, and may be lighted. This signage is allowed in addition to other permitted wall signage.

### Projecting Signs

Projecting signs are permitted within all COR sub-districts.

Projecting signs, including graphics or icon signs, mounted perpendicular to the building wall, are encouraged in all sub-districts, provided the following standards are met:

- A. The sign area shall not exceed six (6) square feet.
- B. The distance from the ground to the lower edge of the signboard shall be ten feet or greater.



- C. The height of the top edge of the signboard shall not exceed the height of the wall from which the sign projects, if attached to a single story building, or the height of the sill or bottom of any second story window, if attached to a multistory building.
- D. The distance from the building wall to the signboard shall not exceed six inches (6").
- E. The width of the signboard shall not exceed three (3) feet.
- F. Limited to one (1) sign per business. Projecting sign area shall be deducted from the allowable wall signage area.
- G. Granted to ground floor commercial uses only.

### Awning Signs

Awning signs are allowed in all COR sub-districts. Awning signs are allowed for ground floor uses only, provided that the following standards are met:

- A. If acting as the main business sign, it shall not exceed 24 square feet in area, and the height of the lettering, numbers, or graphics shall not exceed 12 inches.
- B. If acting as an auxiliary business sign, it shall be located on the valance only, shall not exceed four (4) square feet in area, and the height of the lettering, numbers, or graphics shall not exceed four inches (4").
- C. Limited to two such signs per business.
- D. If acting as the main business sign, it shall not be in addition to a wall-mounted or applied letter sign.

### Sandwich Board Signs

Sandwich board signs are permitted within all COR sub-districts.

- A. One (1) sandwich board sign per business is permitted in any business, commercial and mixed use district and shall be located within five feet (5') of the main building entrance to the business it advertises.
- B. Sandwich board signs shall be displayed only during open business hours and must be removed daily.
- C. Sandwich board signs shall be no more than a total of two feet (2') in width and three feet (3') in height



- D. Sandwich board signs must leave a minimum of five feet (5') of clearance for pedestrian access if placed on a public or private sidewalk. Sandwich board signs may not hinder the ability of persons to access vehicles parked at the curb and/or access to a building.
- E. Acceptable materials for sandwich board signs shall include the following: metal, wood synthetic materials such as a chalk board and whiteboard. Sandwich board signs shall not be illuminated, nor shall they contain moving parts, or have balloons, streamers, stringers, pennants or similar adornments attached to them. Sandwich board signs shall be maintained in a good appearance at all times.
- F. No sandwich board sign shall be secured, tethered or installed on traffic devices, utility equipment, street furniture, street lights, or any other public fixture.
- G. Sandwich board signs are temporary signs and shall not be counted towards the total sign area of the site for permanent signage.

### Temporary Signs

Temporary signs are allowed per Section 117-465 (Temporary signs) of the Zoning Ordinance, except as noted below:

- A. Freestanding temporary signs shall only be allowed in the COR1 District.
- B. In all other COR districts, temporary signs shall be limited to temporary sandwich board signs or temporary wall signage.

### Off-Site Signs

Off-site signs are prohibited except for wayfinding and community signage specifically allowed by the Design Framework.

**Table 2**

Sign Standards					
Development Standard	COR1	COR2	COR3	COR4	COR5
<b>Project Signs</b>					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign (maximum)	6 sq. ft./6 feet high	100 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high
Menu Board**	N/A	1 per drive-through/36 sq. ft. max.	N/A	N/A	N/A
Directional Sign	N/A	Per 117-463(l)	N/A	N/A	N/A
<b>Wall Sign</b>					
Number of signs allowed	1*	1*	1*	1*	1*
<b>Size of Sign</b>					
Single Tenant Building	50 sq. ft. or 5% of the ground floor façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less
Multi-Tenant Buildings	50 sq. ft. or 5% of the façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less
Building Facing Highway 10	5% of the façade area	15% of façade area	5% of the façade area	5% of the façade area	5% of the façade area
<b>Window Sign</b>					
Number of signs allowed	1	1	1	1	1
Size of Sign	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	30% of area of window. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.
<b>Directory Sign</b>					
Number of signs allowed	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance
Size of Sign	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum
<b>Projecting Sign</b>					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide
<b>Awning Sign</b>					
Number of signs allowed	2 per business	2 per business	2 per business	2 per business	2 per business
Size of Sign	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.

\*One (1) sign is allowed per street frontage (either wall or project)

\*\*Restaurants and cafes in all districts are allowed one (1) wall mounted menu board not to exceed 2 sq. ft. in addition to other permitted wall signage.

**Additional standards**

- A. Businesses with service entrances may identify these with one (1) wall-mounted or applied letter sign not exceeding two square feet.

- B. One (1) directional sign, facing a rear parking lot. This sign may be any type of permitted sign other than a freestanding sign, but shall be limited to four (4) square feet in area.

### Creative Sign Standards

- A. *Purpose.* This section establishes standards and procedures for the design, review, and approval of creative signs. The purposes of this creative sign program are to:
  - 1. Encourage signs of unique design, and that exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
  - 2. Provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- B. *Applicability.* An applicant may request approval of a sign permit for a creative sign to authorize onsite signs that employ standards that differ from the other provisions of this chapter but comply with the provisions of this section.
- C. *Application Requirements.* A sign permit application for a creative sign shall include all information and materials required by the City.
- D. *Procedure.* A sign permit application for a creative sign shall be subject to review and approval by the City as part of the Site Plan review process. When the creative sign is proposed after site plan review is complete, a creative sign may be approved by the Zoning Administrator when the proposed sign is fifty square feet or less, and shall be approved by the City Council when the sign is larger than fifty square feet.
- E. *Design Criteria.* In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
  - 1. Design Quality. The sign shall:
    - a. Constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area;
    - b. Be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
    - c. Provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
  - 2. Contextual Criteria. The sign shall contain at least one (1) of the following elements:
    - a. Classic historic design style;
    - b. Creative image reflecting current or historic character of the City;
    - c. Inventive representation of the use, name, or logo of the structure or business.

3. Architectural Criteria. The sign shall:
  - a. Utilize or enhance the architectural elements of the building; and
  - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
4. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

#### Prohibited signs

The following signs are prohibited:

- A. Box signs or cabinet signs, whether on a wall, projecting or on canopies are prohibited except for logo signs permitted as part of an overall sign plan.
- B. Signs employing mercury vapor, low pressure and high pressure sodium and metal halide lighting; plastic panel rear-lighted signs.
- C. Signs on roofs, dormers, and balconies.
- D. Billboards.
- E. Signs painted or mounted upon the exterior side or rear walls on any principle or accessory building or structure, except as otherwise permitted hereunder.
- F. Free standing pylon signs over six (6) feet in height, except community signs.
- G. Back-lit awnings.
- H. Interchangeable letter boards or panels.
- I. Flashing signs.
- J. Off-premises signs, except community signs.

## Overall Framework – Stormwater Management

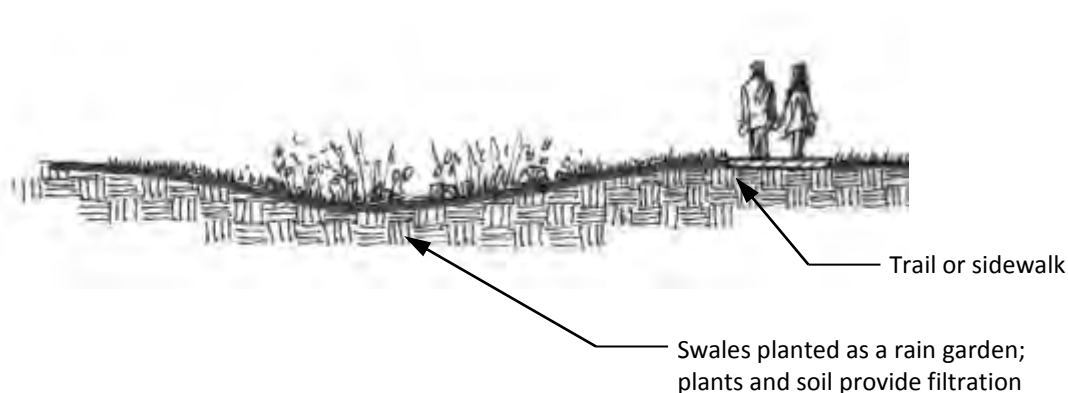
### Overview

Water features and drainage systems are essential components of The COR Master Plan. The mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize ‘best management practices’ (BMP) to pretreat run-off, reduce erosion and encourage infiltration, with the understanding that regional treatment will be provided. Water elements are not intended to be separate stand-alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for The COR. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.

### Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- ‘Green architecture’, expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



### Guideline Recommendations

This framework not only defines the overall role and character of The COR, but also encourages each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout The COR
- **Promote** high quality, creative and appealing aesthetics for all system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support COR development. While this overall strategy addresses the water quantity issue for the entire COR by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.

## Overall Framework – Parks/Public Spaces

### Overview

The overall design framework of The COR is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout The COR. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

### Guideline Recommendations

The Design Framework encourages the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements



Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.

### Objectives

Based on the Development Master Plan for The COR, a variety of public and private green space is proposed including:

- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities.
- Playgrounds, game fields and park buildings are park amenities that will target a wider user demand from the surrounding community.
- The Municipal Plaza will connect with all other parks and open space elements, providing a focal point for activity and a sense of place. Gardens, public pavilions and shelters, play lots and open play fields are amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to The COR.



## Part Two: Sub-district Framework

### Overview

The Master Plan and Zoning Ordinance identify five (5) distinct sub-districts within The COR. Each of these sub-districts is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. The sub-districts are shown the attached Sub-district Map (Figure 7: Sub-district Map). They include:

- **COR1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **COR2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses
- **COR3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **COR4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **COR5 Park and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout The COR

Development plans must respond to the overall COR Design Framework described in Part One of this document, but also must address the following detailed framework standards for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Framework
- Site Design Framework
- Architectural Framework

In all cases, the sub-district framework must be applied in harmony with the overall COR framework and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development. However, within each district, development standards shall be based, in part, on the street hierarchy established in The COR.

### Public Realm/Streetscape

- A. The developer shall be responsible for construction of all streetscape improvements along adjacent streets. These improvements include all hardscape improvements, such as trails, sidewalks, benches, bike racks, etc., as well as street trees and other greenscape.
- B. The landowner shall be responsible for maintenance of the streetscape improvements along adjacent streets. Such maintenance may be managed as part of a Special Service District (SSD) or similar.

### Plazas

- A. All properties with principal buildings in excess of 30,000 square feet shall be required to have a public plaza space.
- B. The public plaza space shall include benches, bike racks trash receptacles, lighting fixtures and other amenities to create a welcoming space for tenants of the building and members of the public.

C. These plaza spaces should be visible from the public street.

### Architecture

These architectural framework standards provide an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



**Auto Versus Human Design:** Providing rear alleys or side loaded garages enables the home to present itself to the street, which promotes sociability and walkability.

### Minimum Standards

#### Definitions

**Build to line:** The line at which construction of a building facade is to occur on a lot. The build to line provides a maximum setback for all building facades from the right-of-way or inside edge of the sidewalk, whichever is greater. The build to line applies to all facades abutting a street right of way. Build to line standards are based on the sub-district the parcel is located and the street in which the building is located upon.

**Setback:** The minimum required distance between a parking lot or the vertical wall of a building and a lot line. Setbacks are typically more flexible than build to lines and allow a building to be set further back from the lot line and street right-of-way.

#### Requirements:

- A. Additional development standards shall be defined within each sub-district.
- B. Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street.

C. Buildings should be articulated to break up the mass of the building façade on all elevations. Large blank exterior walls shall be prohibited. Windows are encouraged to be used to break up the mass of the building. Rooflines and building elevations should be articulated to break up the mass of buildings. Buildings must be broken at a minimum of every fifty (50) feet. This variation can be accomplished through one (1) or more of the following:

1. Wall face variations (minimum depth of two (2) feet)
2. Pilasters or columns (minimum depth of two (2) feet)
3. Upper wall break in color and/or materials (minimum of 2/3 up the height of the wall)
4. Wainscot (minimum of four (4) feet high)
5. Canopies (minimum four (4) foot width)
6. Corner and demising wall or building separations



More Desirable

D. Each development application must list building materials, roofing materials and building colors on the plans. Building articulation, mechanical screening by architectural elements and design elements on all building elevations must be identified on the plans. The plans shall be subject to City review and approval to ensure that the design intent of the Framework has been met.



Less Desirable

E. Building materials shall be high-quality durable materials, such as glass, stone, brick, windows, canvas awnings, etc.

1. Use local sources if available
2. Siding is not allowed (vinyl, metal, wood) except as a complimentary use
3. Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
4. Metal and wood components are acceptable as a complimentary, not primary, building material
5. COR1 sub-district buildings shall be primarily brick or a comparable alternative and shall comprise a minimum of fifty (50%) percent of the non-glazed wall area

F. Residential development should provide variety within the subdivision to avoid monotony. Developers will include front porches and windows that face all streets. Corner units shall have more than one (1) window per side of building to help create a sense of place in the community and promote safety on the street.

G. Garage forward or “snout houses” are highly discouraged on all residential development as they dominate the view from the street and sidewalk.

H. All sides of buildings shall have an equal finish in terms of materials and general design.

I. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.

- J. Building massing shall be oriented parallel to the street frontage of the lot.
- K. Trash and recycling areas shall be designed internal to the building.
- L. Provide recessed entries wherever possible.
- M. Buildings shall have a base and top to the architecture.
- N. Multi-story buildings are preferred in the COR1 sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of twenty (20) feet to the roofline.
- O. Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – fifty (50%) minimum of total ground level façade, or demonstrate great design through other means.
- P. Provide real window openings for all street facing façades above ground level in all districts—a minimum of forty (40%) percent of total façade.



Residential uses in COR4 and COR5 must also comply with the following:

- A. *Location of main entrance*
  1. A building must include a front porch or covered balcony/patio at all main entrances that face a street. The porch or stoop shall adjoin the main entrance and the main entrance shall be accessible from the porch.
  2. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.
- B. *Porches:* Porches used to satisfy the design criteria shall comply with the following:
  1. Porches shall be covered by a solid roof. The roof shall not be located more than 12 feet above the floor of the porch. If the roof of a required porch is developed as a deck or balcony, it may be flat.
  2. The porch shall have minimum dimensions of 6 feet by 6 feet. (6' x 6'). For single-family detached dwelling units, the covered area provided by the porch must be at least



forty-eight (48) square feet and a minimum of eight (8) feet wide. If the main entrance is for more than one (1) dwelling unit, the covered area provided by the porch must be at least sixty-three (63) square feet and a minimum of nine (9) feet wide.



C. *Covered balconies/patio*: The covered area must be at least forty-eight (48) square feet, a minimum of eight (8) feet wide, and no more than fifteen (15) feet above grade. The covered area must be accessible from the interior living space of the house.

D. *Openings between porch floor and ground*: Openings of more than one (1) foot between the porch floor and the ground must be covered with a solid material or lattice.

E. *Roofs*

1. *Slope*: Principal structures must have a roof that is sloped, with a pitch that is no flatter than six (6) units of horizontal run to twelve (12) units of horizontal rise.
2. *Architectural features*: The roof of a principal structure shall include the following architectural details:
  - a. At least one (1) dormer facing the street. If only one (1) dormer is included, it shall be at least five (5) feet wide and shall be centered horizontally between each end of the front elevation. If more than one (1) dormer is provided, a dormer at least four (4) feet wide must be provided on each side of the front elevation;  
or
  - b. A gabled end, or a gabled end of a roof projection, facing the street.
3. *Roof eaves*: Roof eaves must project from the building wall at least twelve (12) inches, measured horizontally, on at least the front and side elevations.





Figure 7: Sub-district Map

## **Sub-District Framework - COR1 Mixed Use Core**

### **Public Realm / Streetscape**

Public realm and streetscape improvements define the character of The COR and create a memorable signature for the Mixed Use Core sub-district. This is the most urban of all sub-districts in The COR and will be developed with a very urban pattern. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year.

### **Site Development**

Framework standards that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, the Design Framework also describes the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within The COR.

### **Architecture**

Buildings in the Mixed Use Core will play a major role in defining the overall character of The COR. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis will be on great design along public street frontage.

**Table 3**

<b>COR1 Development Standards</b>						
	<b>Arterial Street</b>	<b>Destination Street</b>	<b>Parkway</b>	<b>Connector Street</b>	<b>Downtown Street</b>	<b>Local Street</b>
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre <sup>(1)</sup>	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.75	.75	.75	.75	.75	.75
<b>Build to Line</b>						
Front yard <sup>(4)</sup>	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

## **Sub-District Framework - COR2 Commercial**

### **Public Realm / Streetscape**

While the Commercial Sub-district encourages a mix of large and small floor plates accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential.

### **Site Development**

Site Design Framework for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for The COR. Specifically, this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. However, the market realities suggest that the types of auto-oriented users in this district will have the potential for larger parking fields and drive-throughs that will make a true urban model challenging. The City will hold all users to this design vision while modifying the COR2 standards slightly to accommodate these types of more typical suburban land uses. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours.

### **Architecture**

Buildings in the Commercial Sub-district make a significant contribution to first impressions of The COR. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make The COR unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 4

COR2 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Minimum lot depth	100 feet	100 feet	100 feet	100 feet	100 feet	100 feet
Allowable residential density in dwelling units per acre <sup>(1)</sup>	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Build to Line						
Front yard <sup>(4)</sup>	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (40% of front facade w/in max.)	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (40% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Boulevard Sod	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	6 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

- A. At least forty (40%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply only to one (1) street frontage.
  1. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
  2. The City may approve a variation from the required build to line if the applicant provides a street edge consisting of fencing, decorative wall and/or landscaping with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
- B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the

level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the forty (40%) percent required on all street frontages.

## Sub-District Framework - COR3 Workplace

### Public Realm/Streetscape

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of The COR is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. The Draw, an active, neighborhood scale, recreational play space has been included in this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts.

### Site Development

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts.

### Architecture

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter The COR via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this sub-district suggest a variety of potential building types – blending medical campus with small office, storefront and some housing -- demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 5

COR3 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	40 feet	40 feet	40 feet	40 feet	40 feet	40 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre <sup>(1)</sup>	none	none	none	none	none	none
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard <sup>(4)</sup>	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

## **Sub-District Framework - COR4 Neighborhood**

### **Public Realm/Streetscape**

Two prominent public parks supply much of the informal green space for The COR, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Commons surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. The sub-district is adjacent to The Draw, which is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout The COR.

### **Site Development**

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of The COR. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The arterial streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of The COR.

### **Architecture**

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of The COR property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for The COR. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 6

COR4 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre <sup>(1)</sup>	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard <sup>(4)</sup>	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

## **Sub-District Framework - COR5 Parks & Open Space**

### **Public Realm/Streetscape**

The overall structure of The COR is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles.

### **Site Development**

The COR Master Plan illustrates a strong framework of green space, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in The COR.

### **Architecture**

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for The COR. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Additionally, limited retail is planned along the perimeter of the park.

**Table 7**

COR5 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	0.5	N/A	0.5	0.5	0.5	0.5
Minimum lot width	N/A	N/A	N/A	N/A	N/A	N/A
Minimum lot depth	N/A	N/A	N/A	N/A	N/A	N/A
Allowable residential density in dwelling units per acre <sup>(1)</sup>	N/A	N/A	N/A	N/A	N/A	N/A
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	N/A	N/A	N/A	N/A	N/A	N/A
Build to Line						
Front yard <sup>(4)</sup>	30 feet as measured from building front to right-of-way <sup>(2)(3)</sup> (60% of front facade w/in max.)	N/A	10 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way <sup>(2)</sup> (60% of front facade w/in max.)
Side yard	no req.	N/A	no req.	no req.	no req.	no req.
Rear yard	no req.	N/A	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet	N/A	25 feet	25 feet	25 feet	25 feet
Building height (min. - max.)	1-2 stories.	N/A	1-2 stories.	1-2 stories.	1-2 stories.	1-2 stories.
Planter Style	N/A	N/A	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	N/A	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	N/A	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	N/A	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Where commercial buildings are planned, at least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

## Implementation

### Introduction

This framework provides distinct strategies for guiding The COR's form and appearance. This framework's vision for the COR will take several years to implement. Successful implementation will require consistent application of the Design Framework by City staff. Zoning enforcement and design framework standards will not succeed without the cooperation and commitment of landowners, development interests, and the rest of the community.

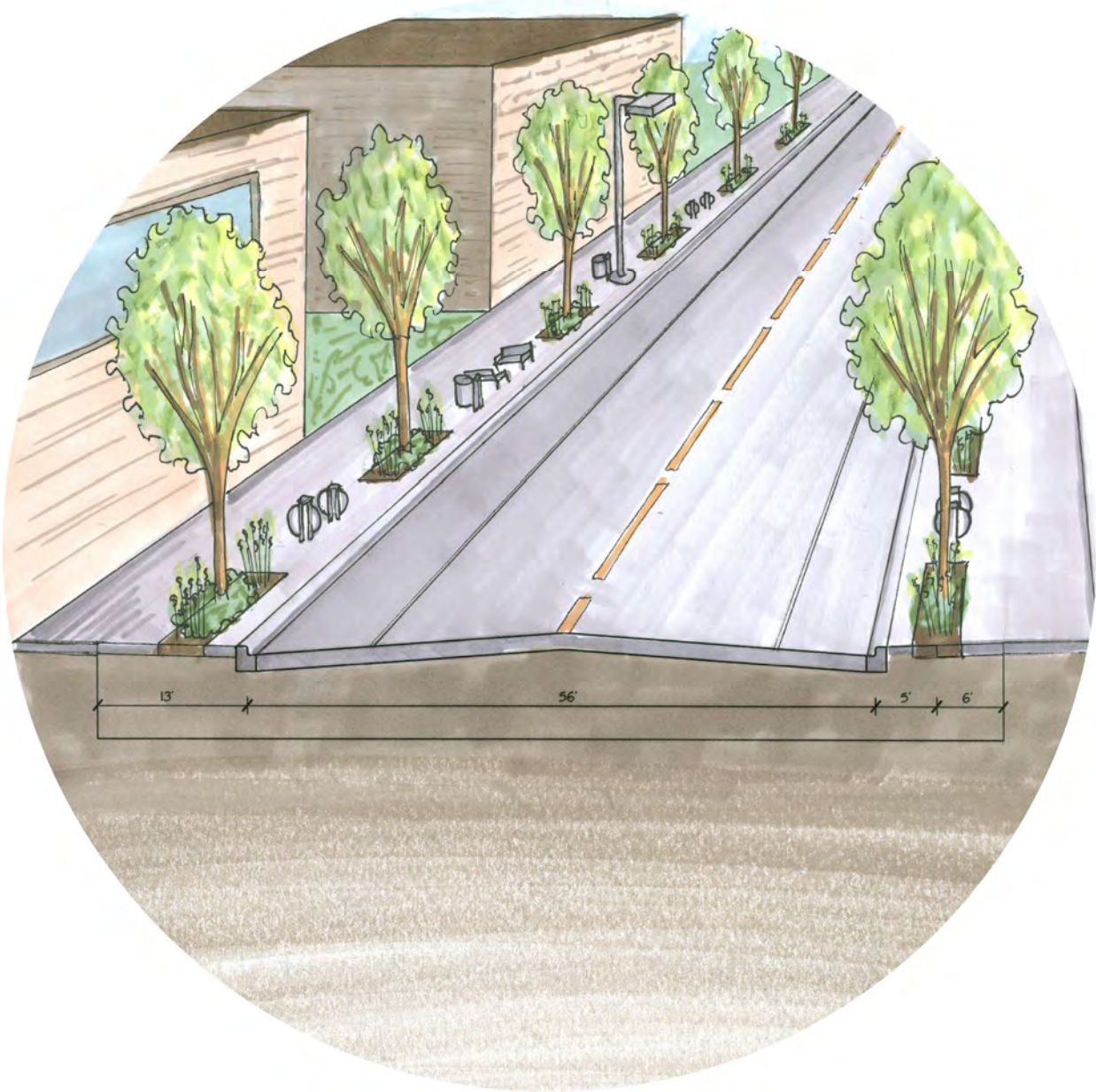
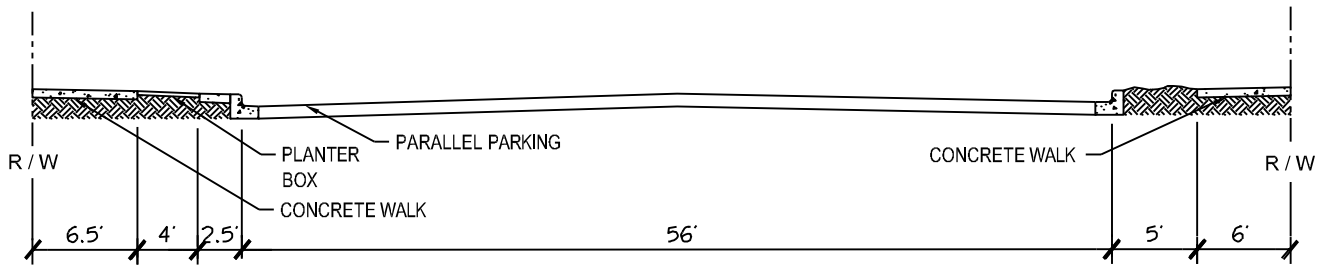
### Design Review

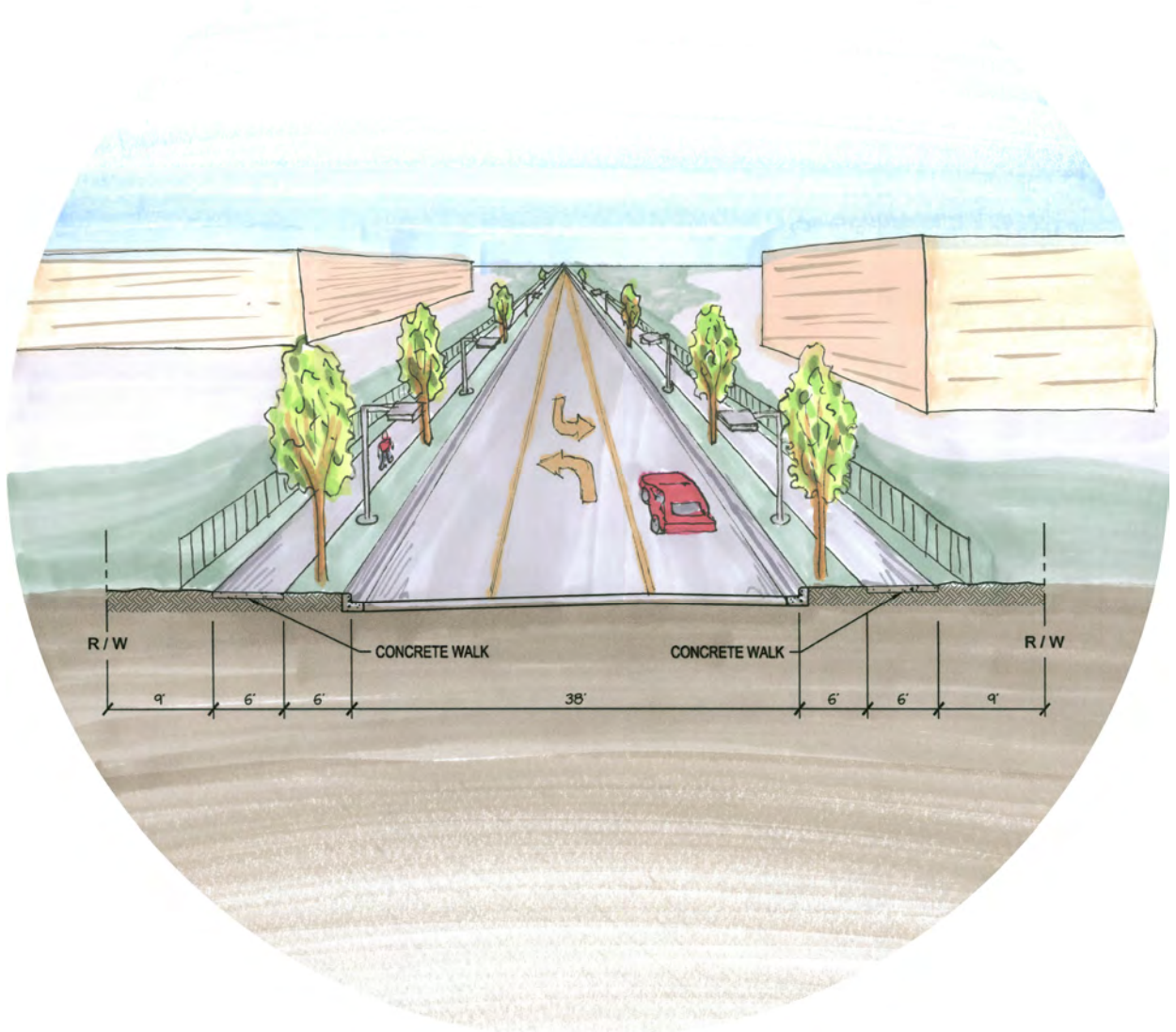
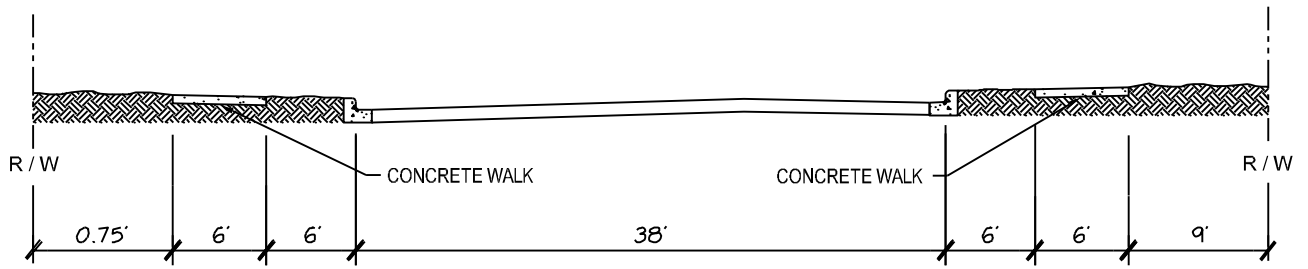
The Design Framework will be administered by staff as part of the development review process. Staff will incorporate design review to ensure compliance with the Design Framework as a standard step in the development review process.

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# APPENDIX A: SPECIFICATIONS AND DETAILS

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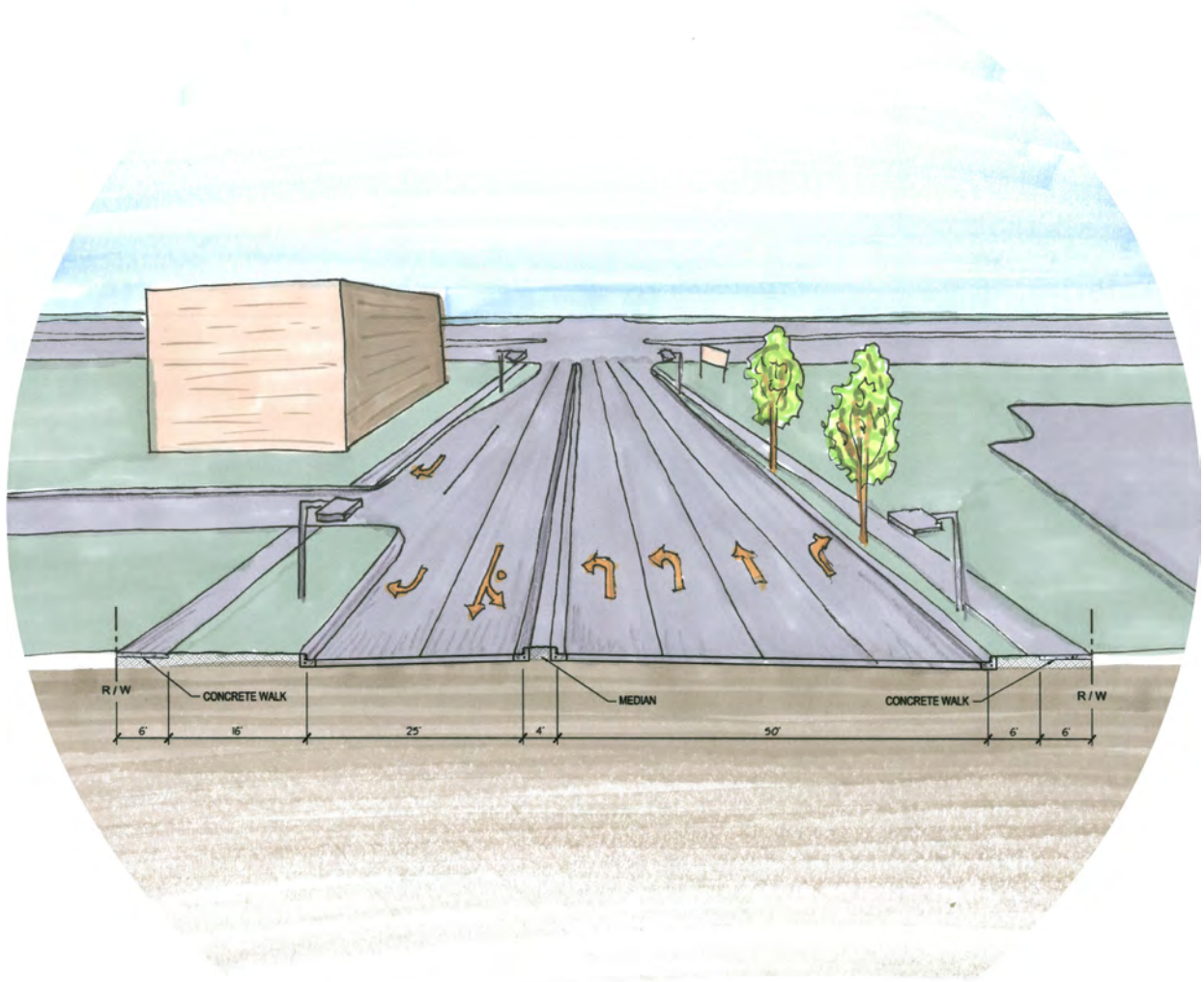
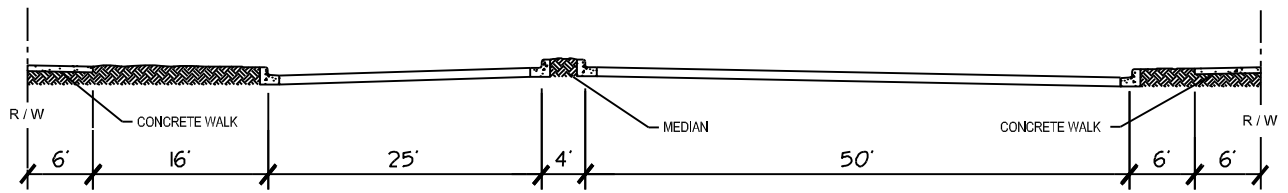




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**L A N D F O R M**
  
 From Site to Finish

**DESIGN FRAMEWORK**  
 The COR at Ramsey  
 PERSPECTIVE SECTION #2  
 Sunwood Dr. West of Zeolite St.

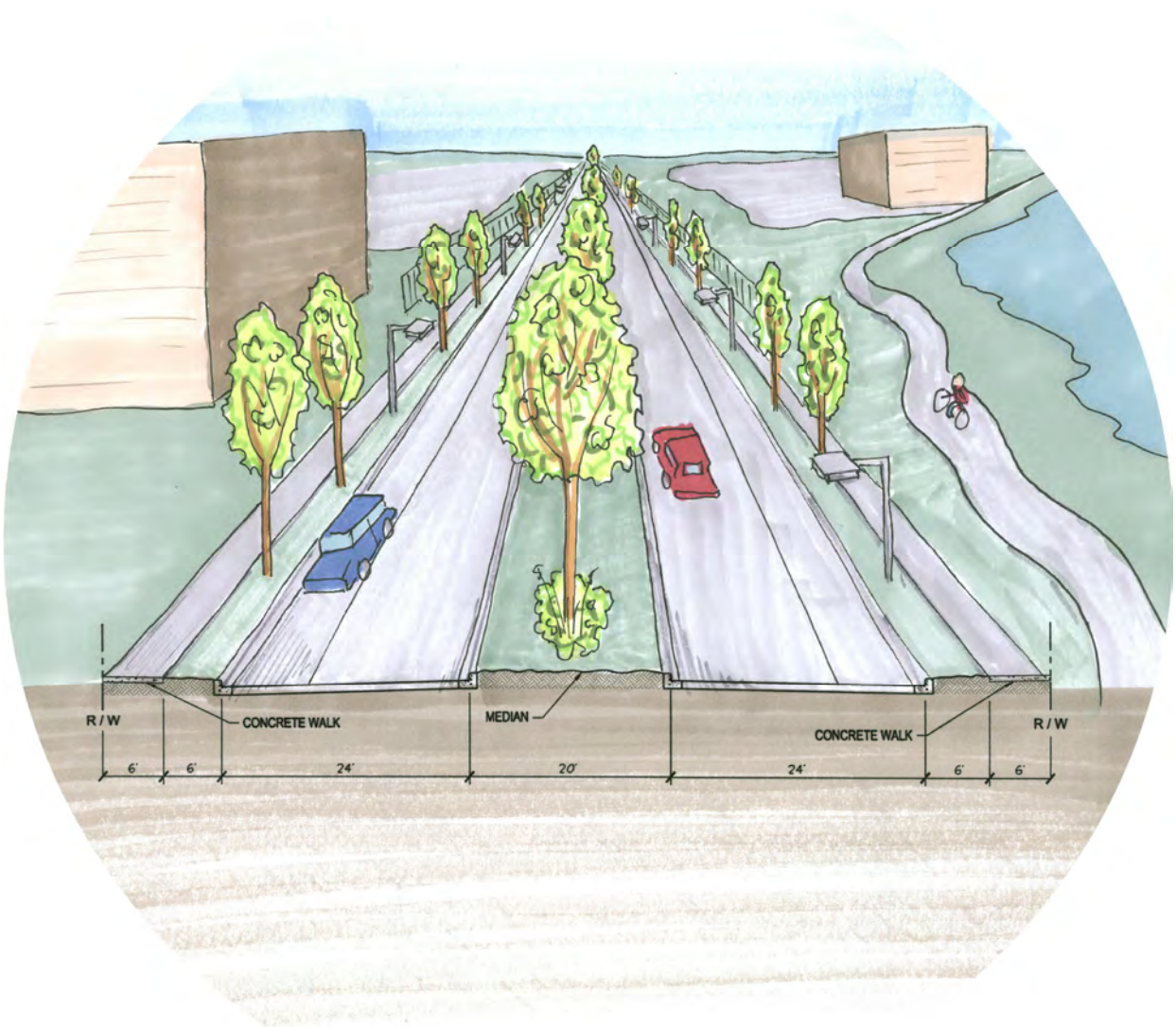
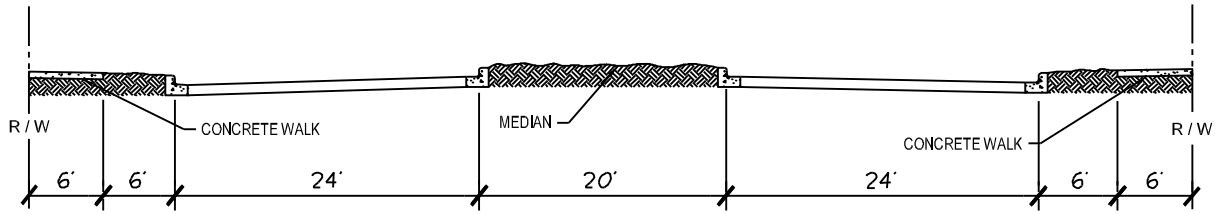




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**L A N D F O R M**
  
 From Site to Finish

**DESIGN FRAMEWORK**  
**The COR at Ramsey**  
 PERSPECTIVE SECTION #3  
 Ramsey Pkwy. West of Roundabout

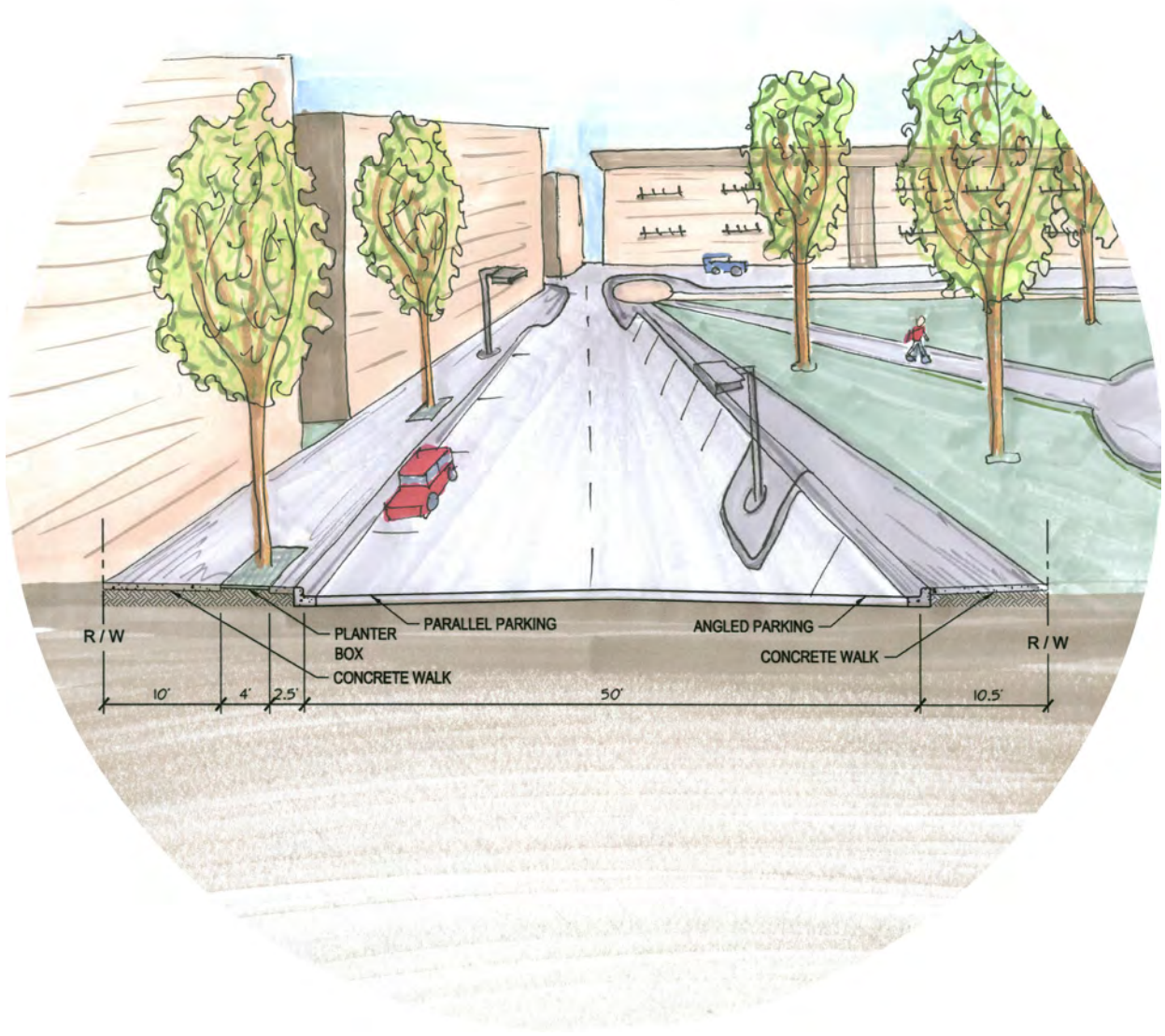
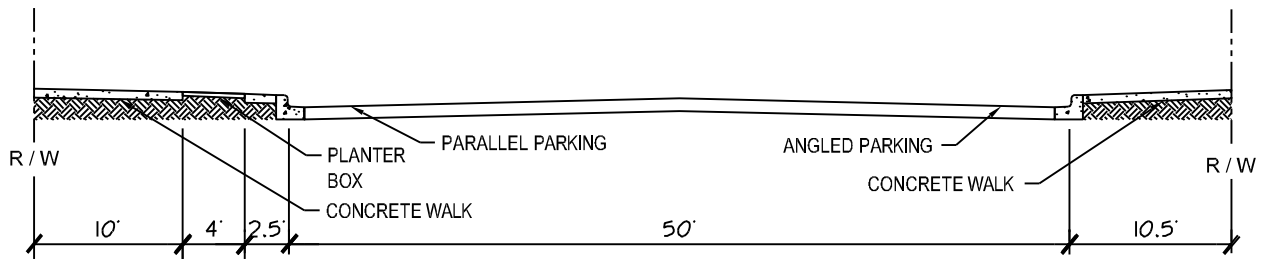


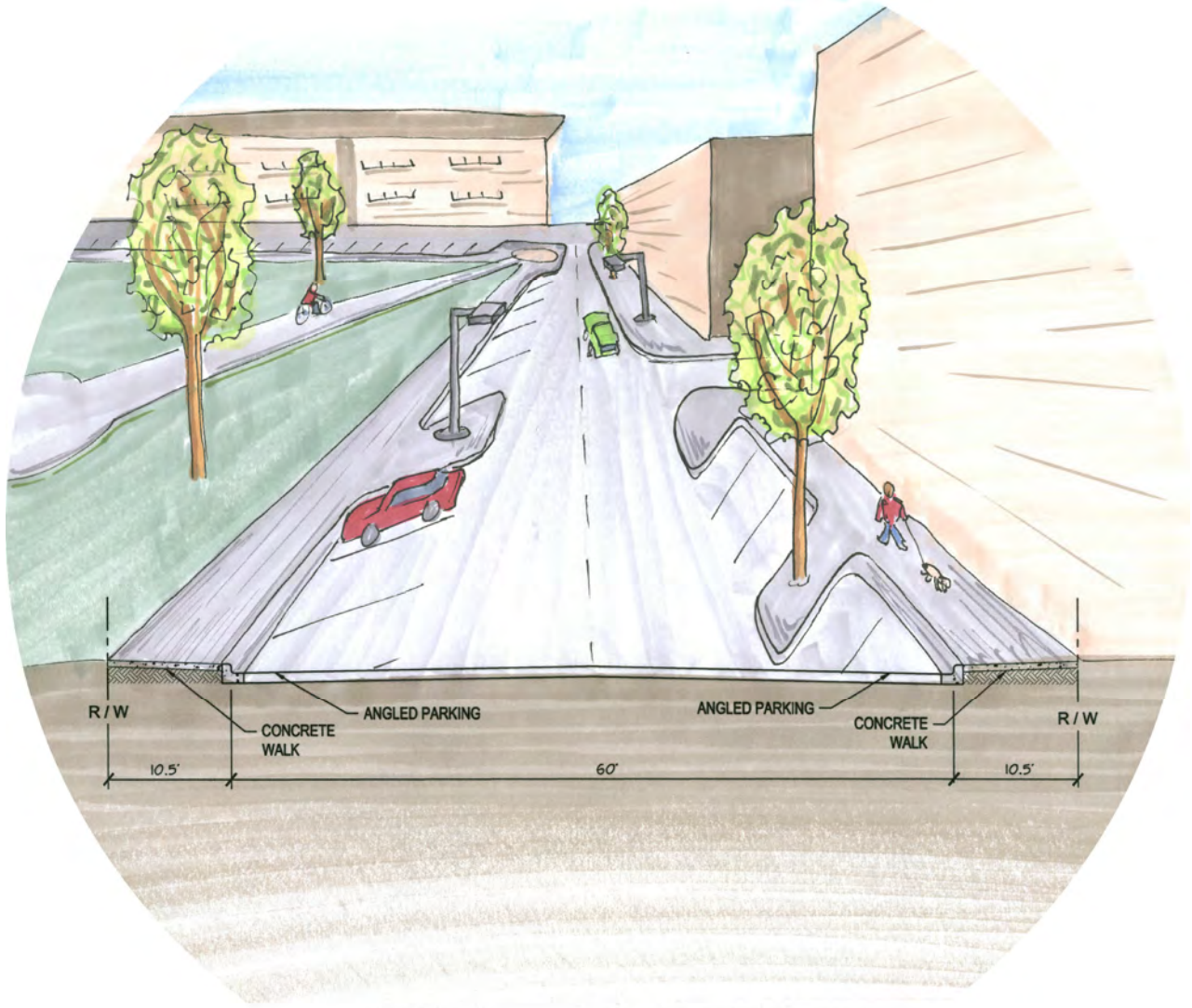
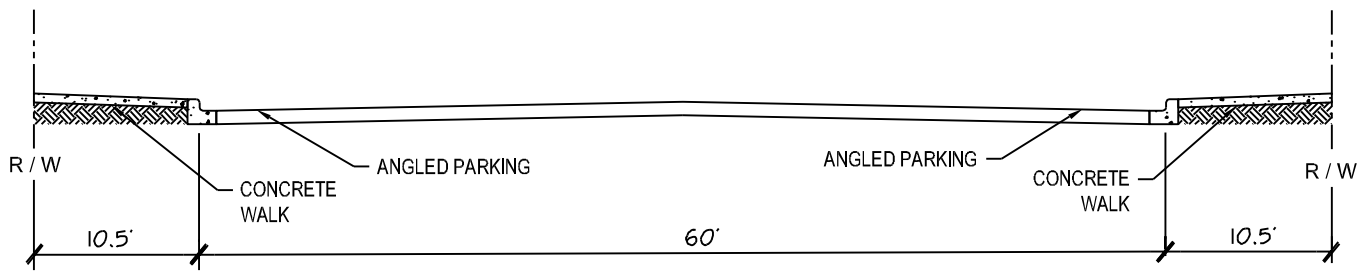


**LANDFORM**
  
 From Site to Finish

**DESIGN FRAMEWORK**  
 The COR at Ramsey  
 PERSPECTIVE SECTION #4  
 Ramsey Pkwy. East of Zeolite St.

**THE**  
**COR**  
 RAMSEY, MINNESOTA

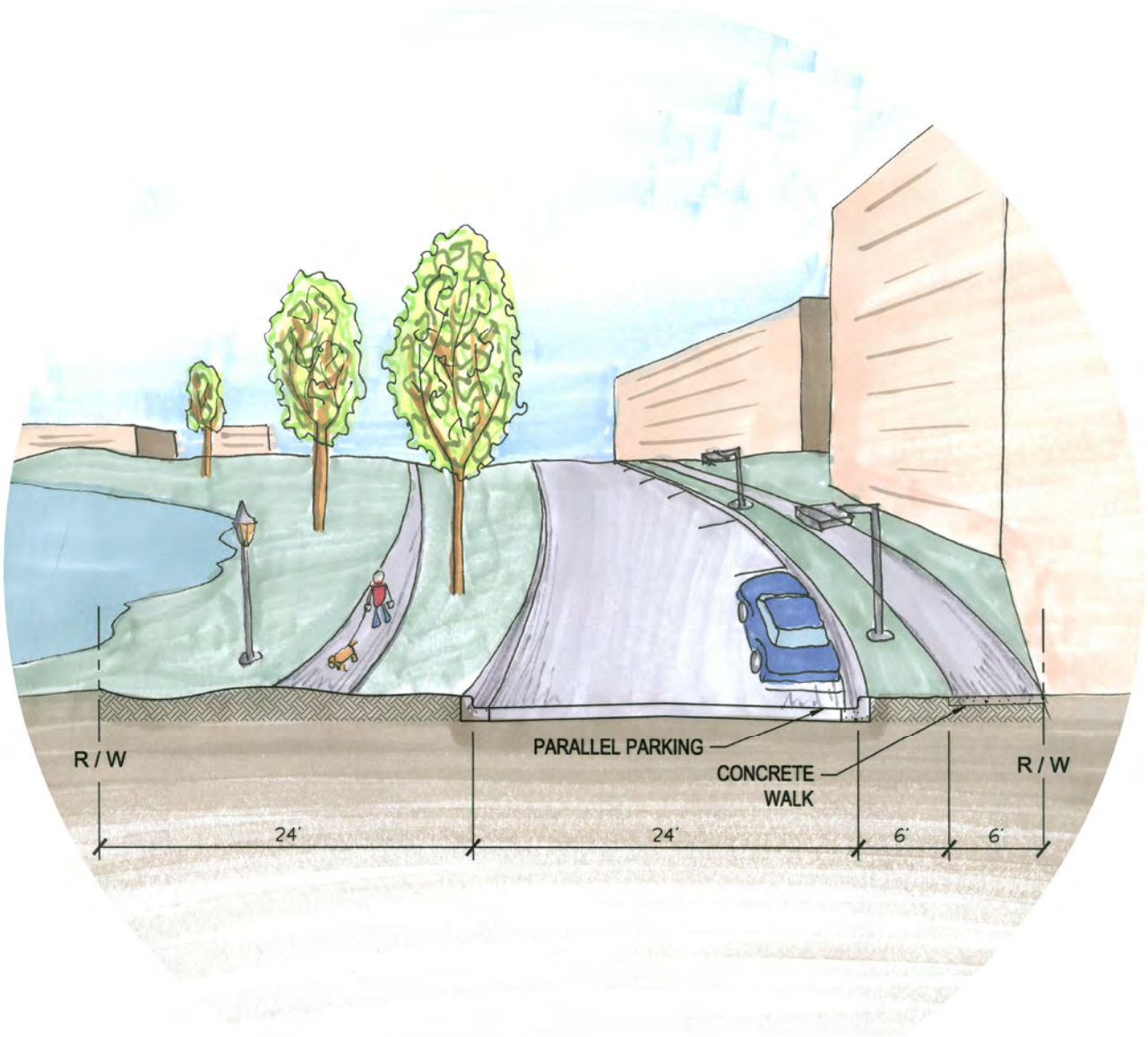
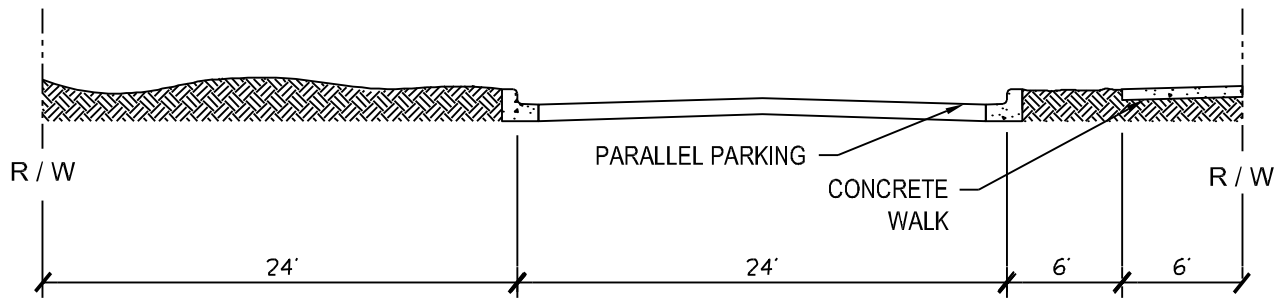




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**L A N D F O R M**  
 From Site to Finish

**DESIGN FRAMEWORK**  
**The COR at Ramsey**  
 PERSPECTIVE SECTION #6  
 Center St. East of Municipal Plaza





● ●  
**L A N D F O R M**  
 From Site to Finish

**DESIGN FRAMEWORK**  
 The COR at Ramsey  
 PERSPECTIVE SECTION #7  
 Ramsey Pkwy. South of The Draw



# THE COR

AT RAMSEY

-  PYLON/COMMUNITY SIGNS
-  COMMUNITY KIOSK
-  PROJECT GATEWAY
-  PARKING

\*See 'Appendix B' for sign details



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## Signage Plan

09/06/2012





EXHIBIT B.1

	<p><b>DESIGN FRAMEWORK</b>  <b>The COR at Ramsey</b></p> <hr/> <p>PYLON/COMMUNITY SIGN 'A'          Armstrong Blvd. &amp; Highway 10</p>	
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Eastbound



Westbound

EXHIBIT B.2



DESIGN FRAMEWORK  
The COR at Ramsey

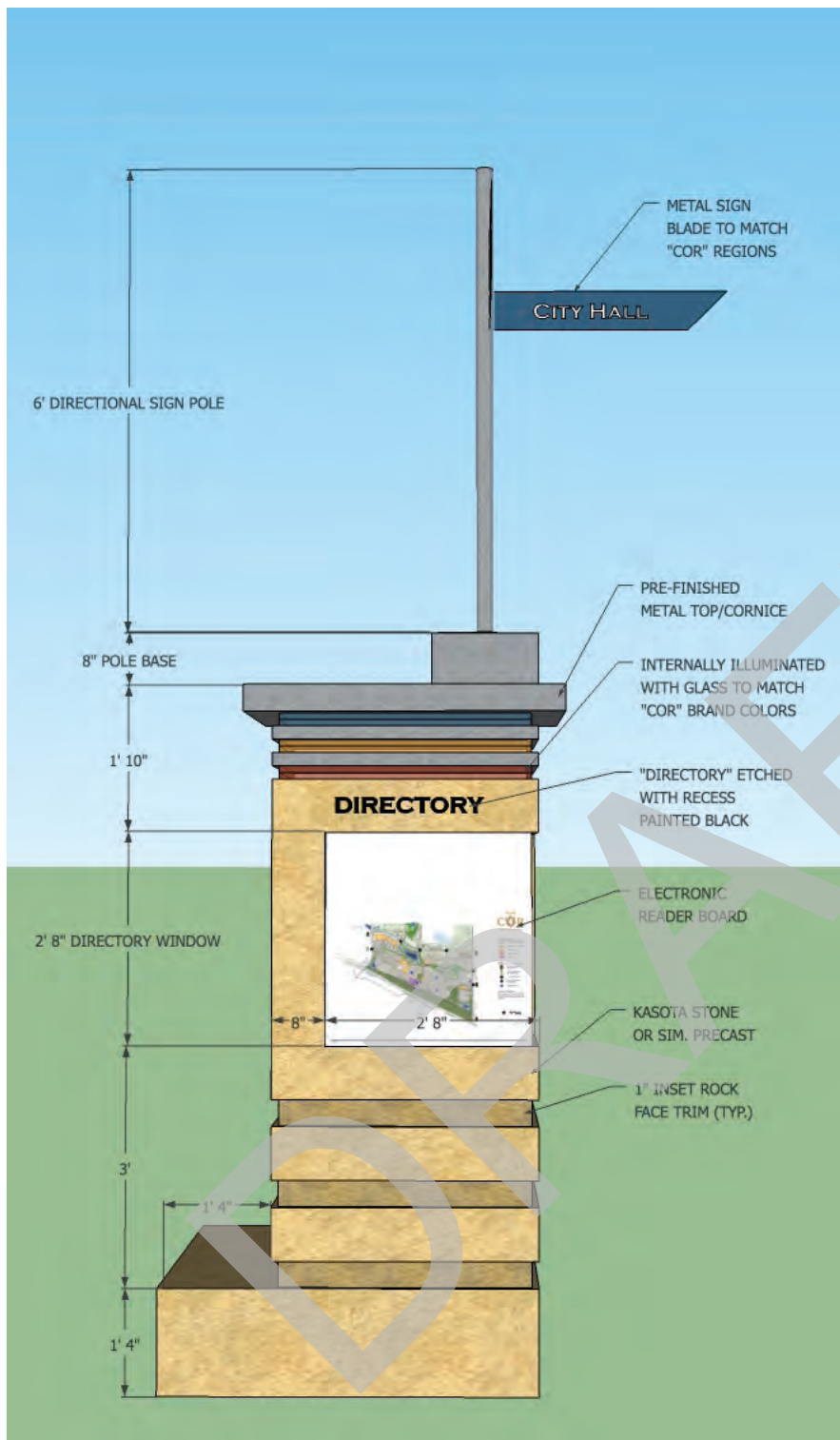
PYLON/COMMUNITY SIGN 'B'  
Commuter Rail & Highway 10





EXHIBIT B.3

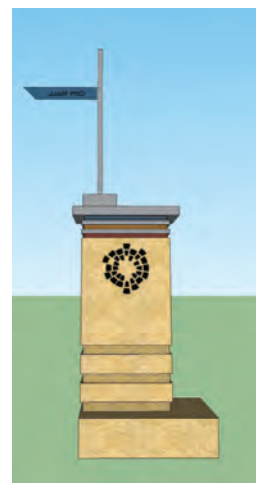
	<p><b>DESIGN FRAMEWORK</b>  <b>The COR at Ramsey</b></p> <hr/> <p>PYLON/COMMUNITY SIGN 'C'          Ramsey Blvd. &amp; Highway 10</p>	
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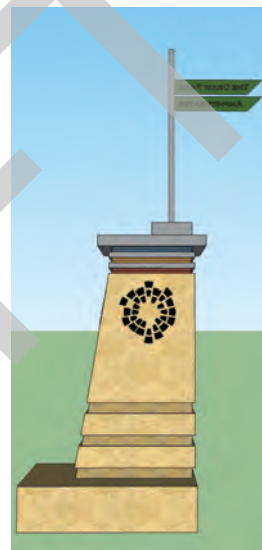
DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



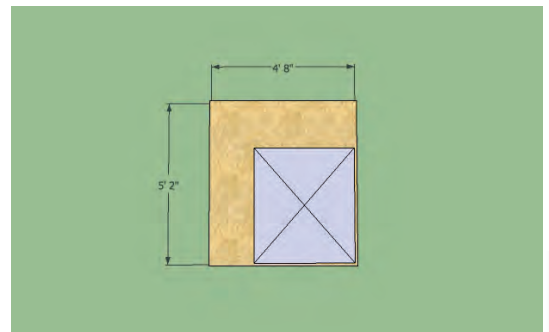
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

EXHIBIT B.4



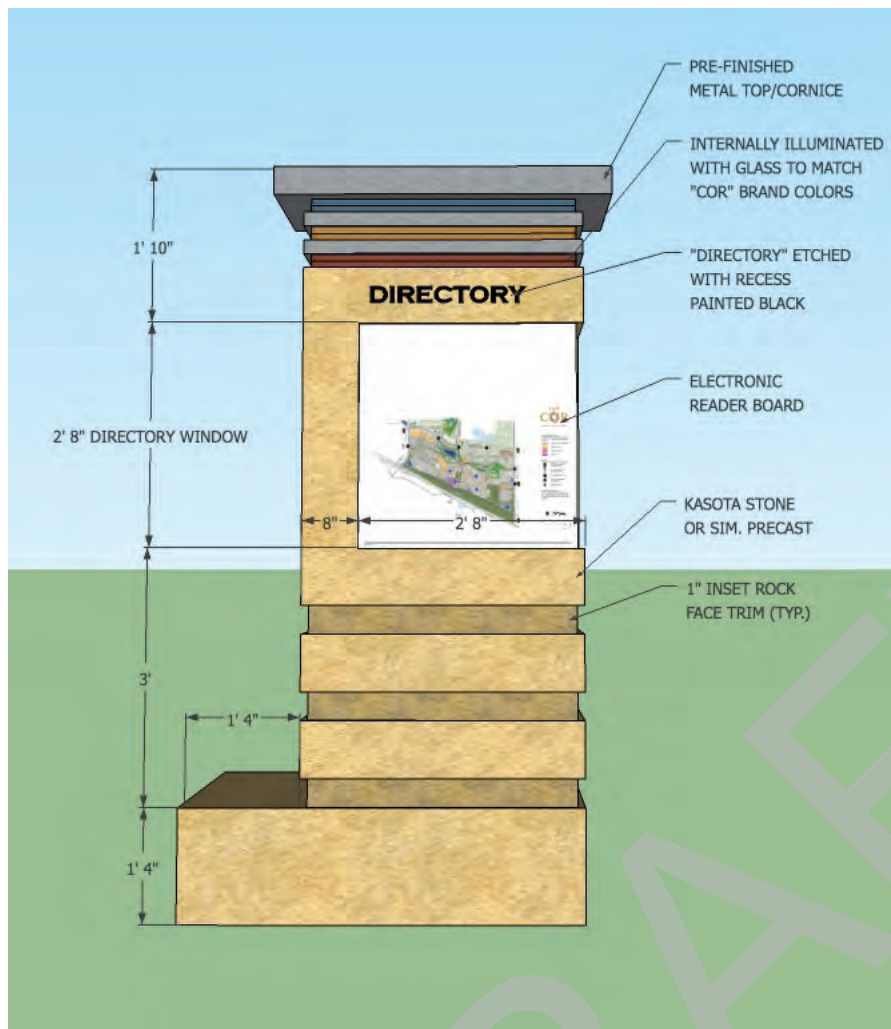
DESIGN FRAMEWORK

The COR at Ramsey

COMMUNITY KIOSK 'A'

Commuter Rail & Veterans Dr.

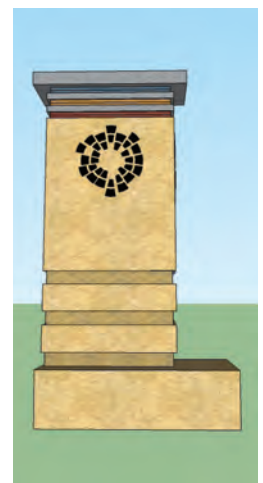




DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



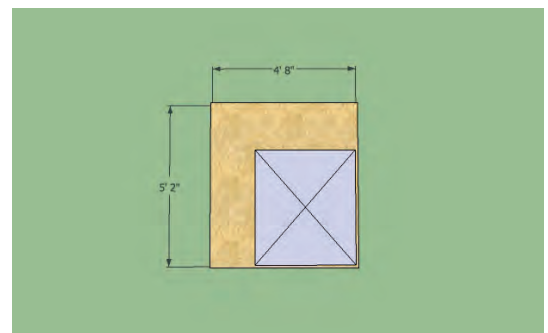
REAR VIEW



SIDE VIEW



PERSPECTIVE



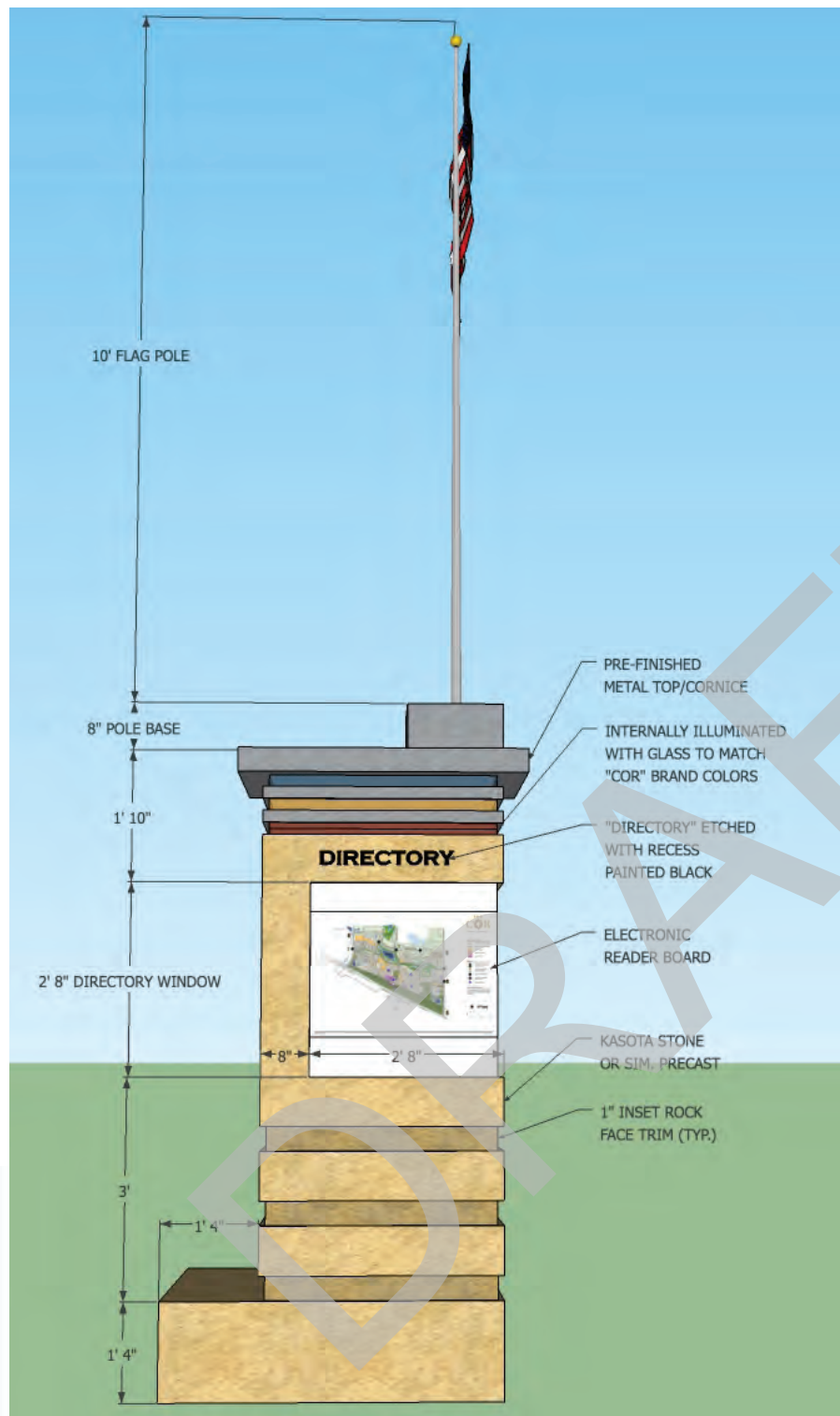
PLAN VIEW

EXHIBIT B.5

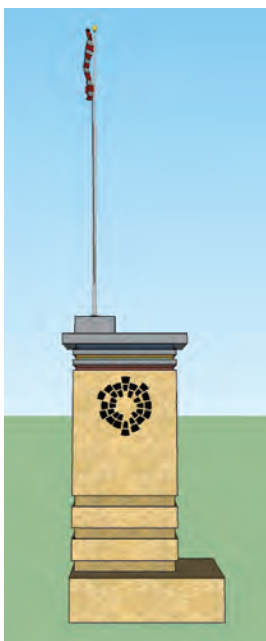


DESIGN FRAMEWORK  
 The COR at Ramsey  
 COMMUNITY KIOSK 'B'  
 Sunwood Dr. & Center St.

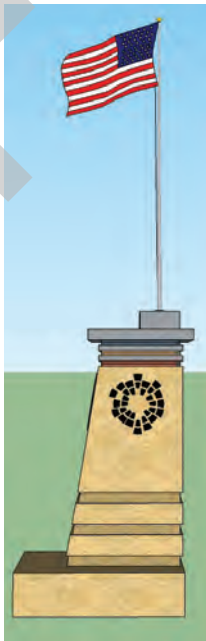




EVENTS PANEL VIEW



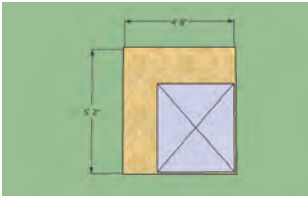
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

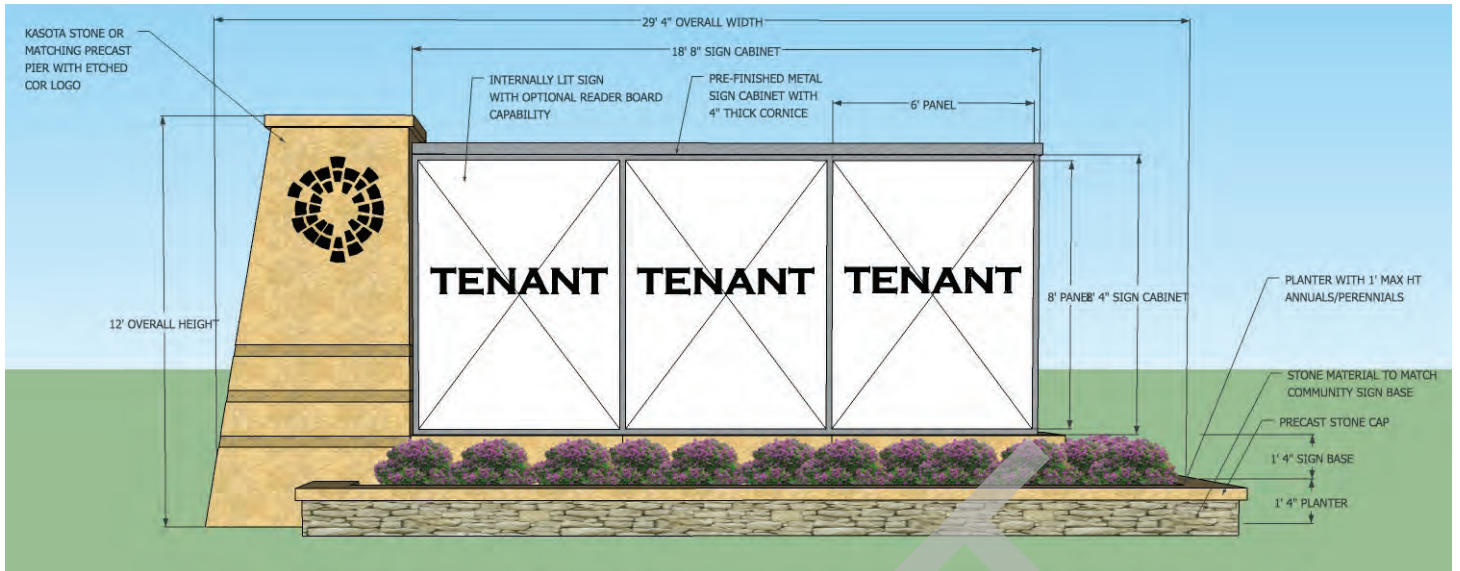
DIRECTORY PANEL VIEW

EXHIBIT B.6

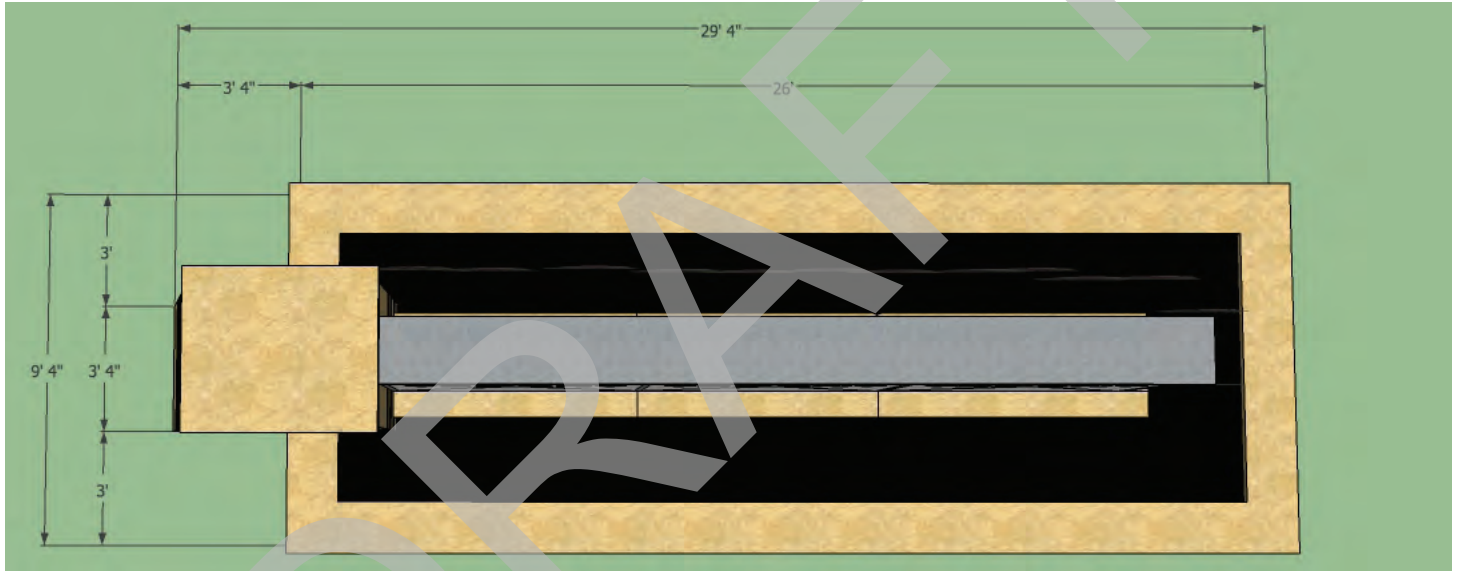
**LANDFORM**  
From Site to Finish

**DESIGN FRAMEWORK**  
The COR at Ramsey  
COMMUNITY KIOSK 'C'  
Sunwood Dr. at Municipal Plaza

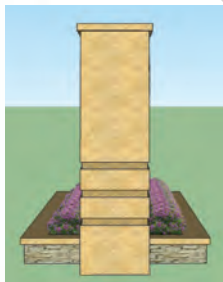
**THE COR**  
RAMSEY, MINNESOTA



ELEVATION AT SUNWOOD



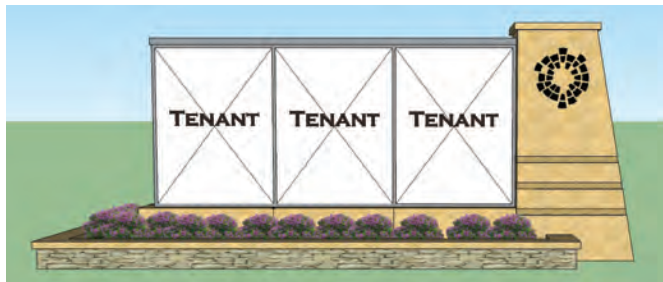
PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION

EXHIBIT B.7



**DESIGN FRAMEWORK**  
**The COR at Ramsey**  
 PROJECT GATEWAY SIGN 'A'  
 Sunwood Dr. & Armstrong Blvd.



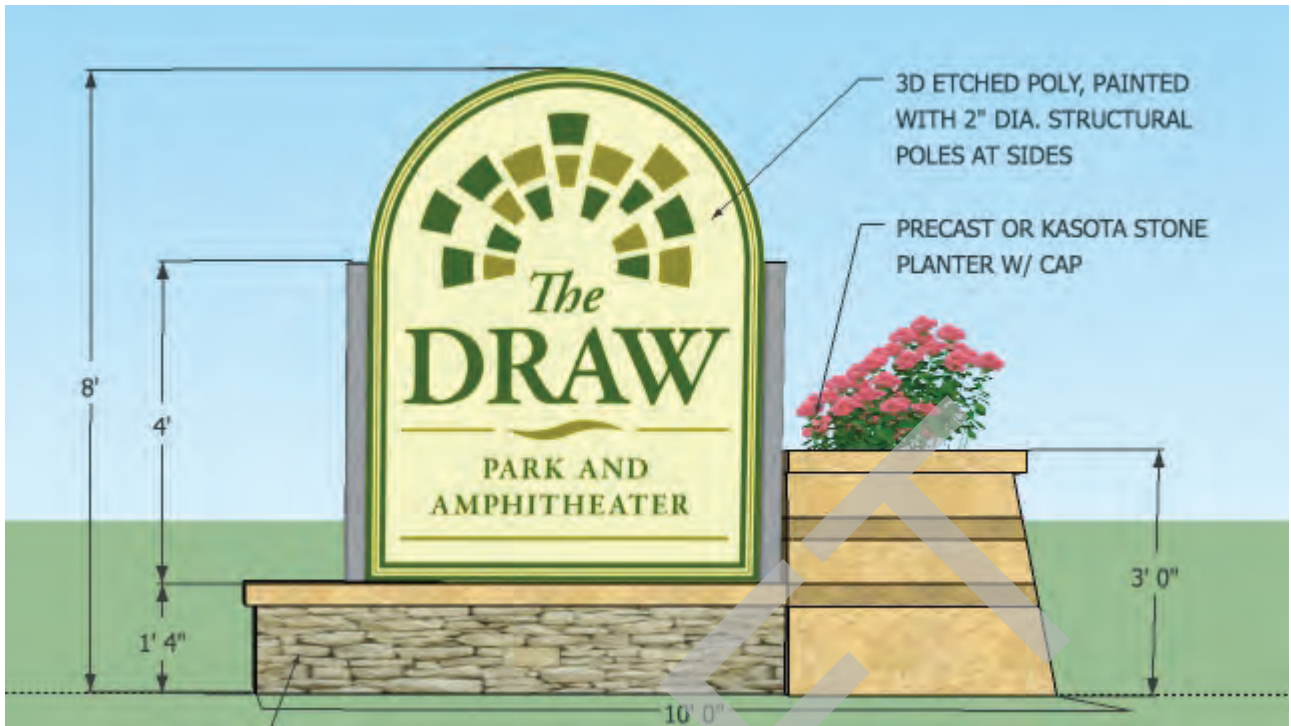
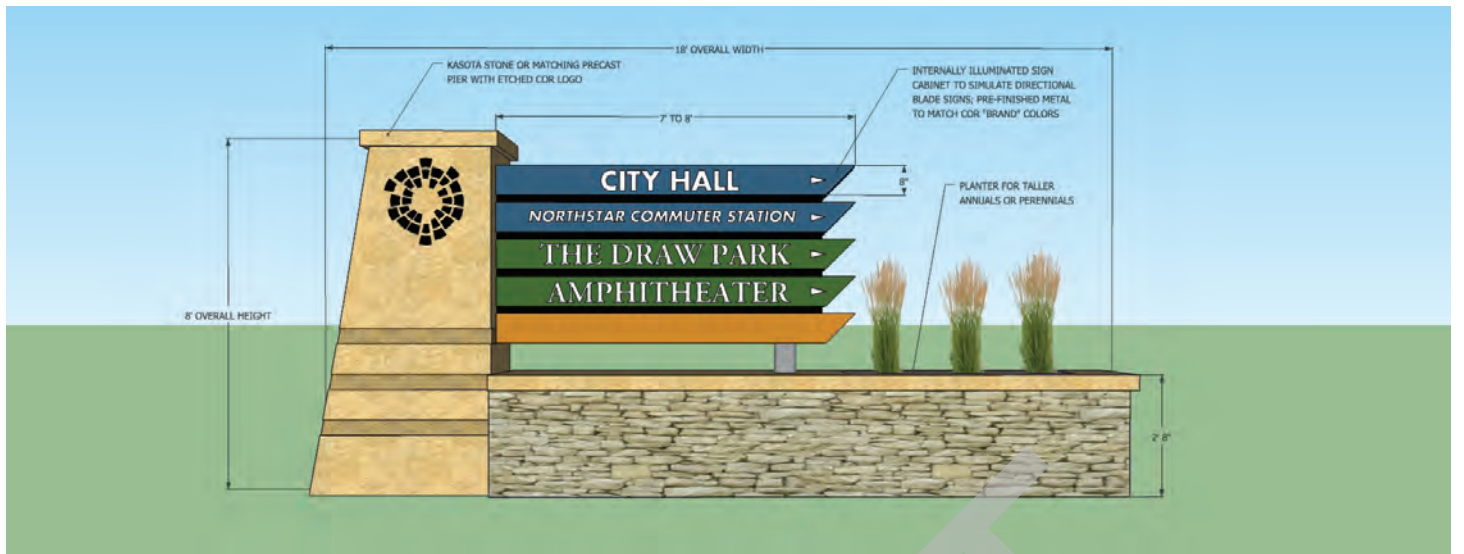
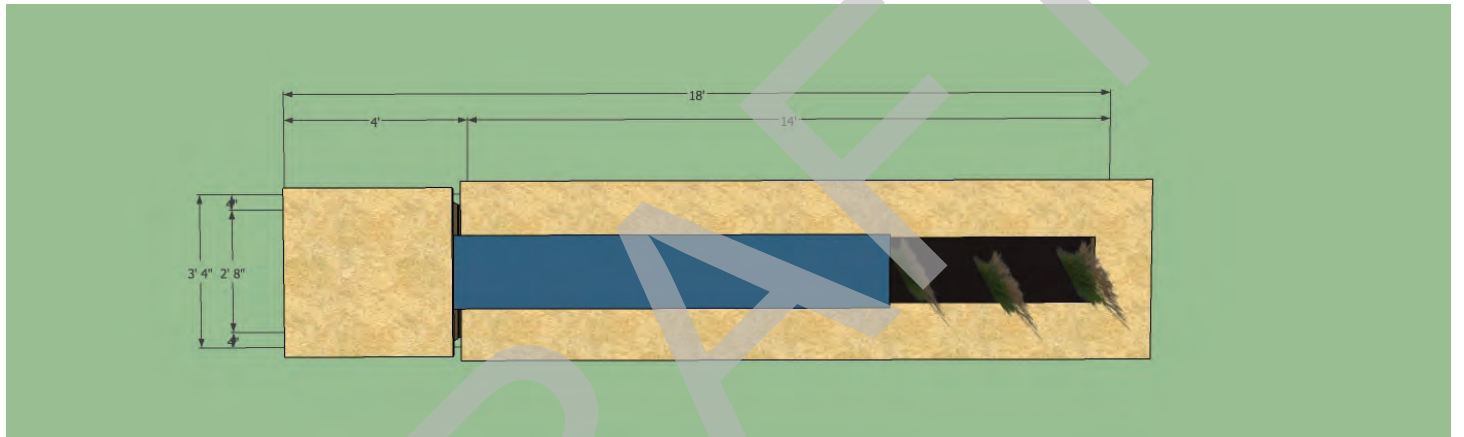


EXHIBIT B.8



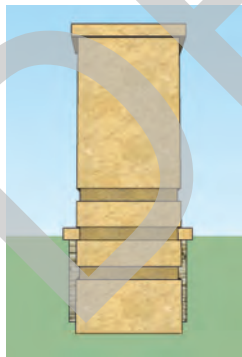
ELEVATION AT EASTBOUND BUNKER LAKE



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT WESTBOUND BUNKER LAKE

EXHIBIT B.9

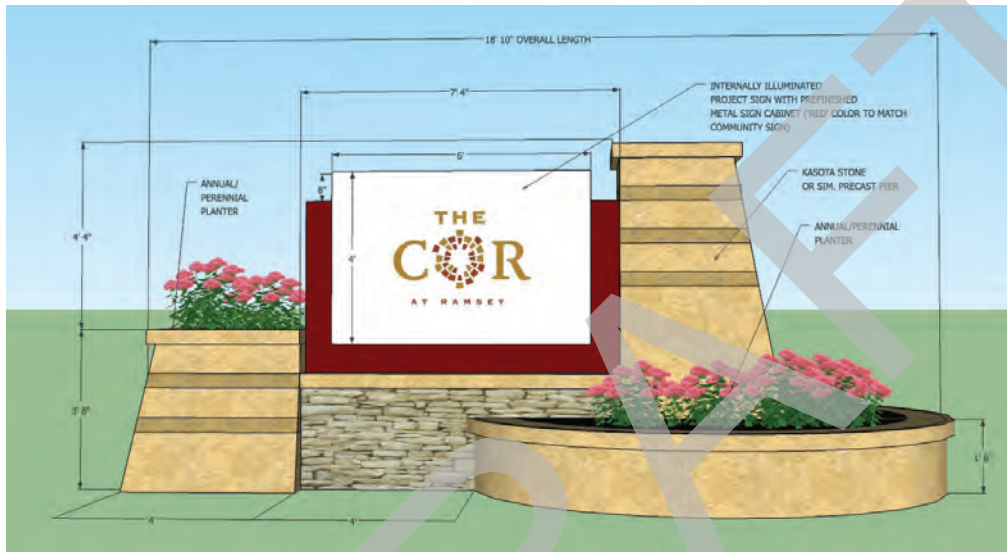


**DESIGN FRAMEWORK**  
**The COR at Ramsey**  
 PROJECT GATEWAY SIGN 'C'  
 Bunker Lake Blvd. & Center St.





ELEVATION AT SOUTH BOUND RAMSEY



ELEVATION AT WESTBOUND BUNKER LAKE



PERSPECTIVE

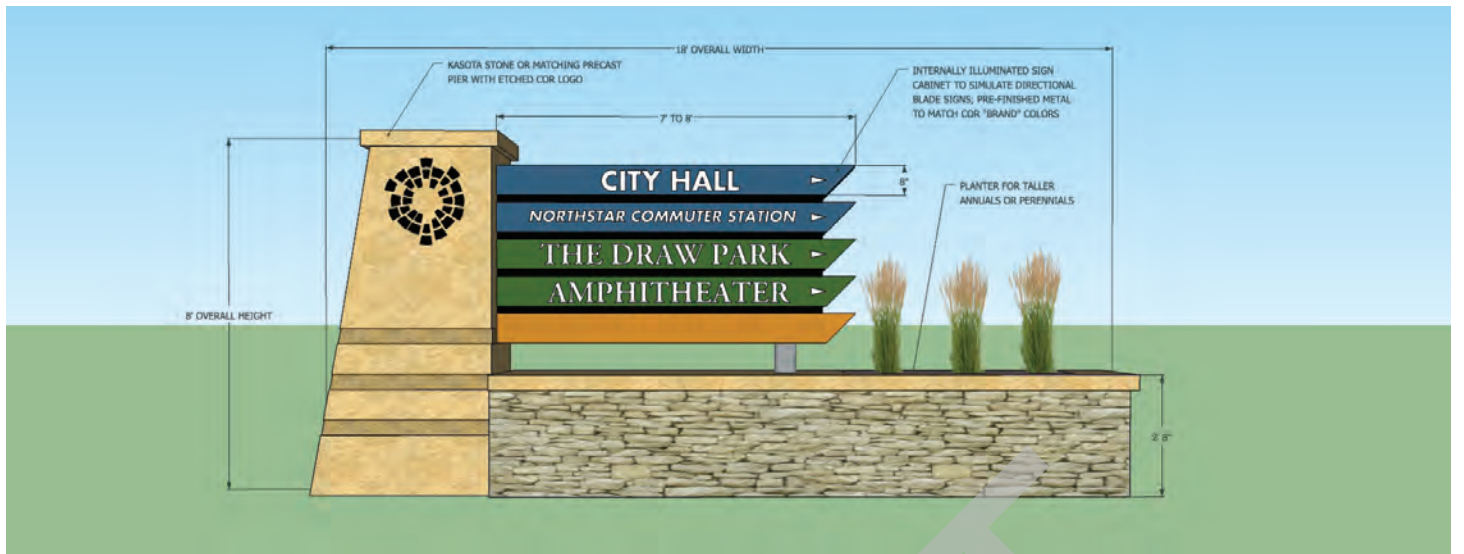
EXHIBIT B.10



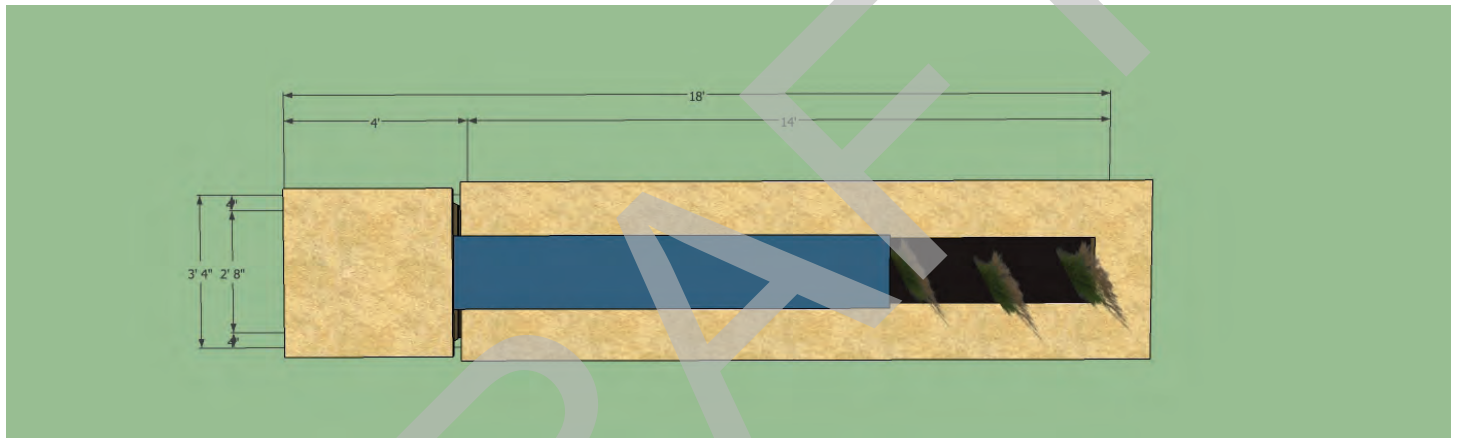
**DESIGN FRAMEWORK**  
**The COR at Ramsey**

PROJECT GATEWAY SIGN 'D'  
 Bunker Lake Blvd. & Ramsey Blvd.





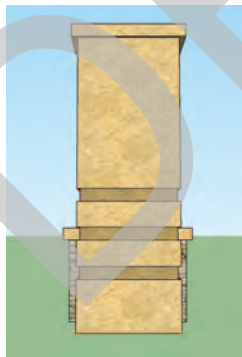
ELEVATION AT SOUTHBOUND RAMSEY



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT NORTHBOUND RAMSEY

EXHIBIT B.11



**DESIGN FRAMEWORK**  
**The COR at Ramsey**  
 PROJECT GATEWAY SIGN 'E'  
 Sunwood Dr. & Ramsey Blvd.





EXHIBIT B.12

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**L A N D F O R M**
  
 From Site to Finish

**DESIGN FRAMEWORK**  
 The COR at Ramsey  
 PUBLIC PARKING SIGNAGE  
 Standard Signage

**THE**  
**COR**  
 RAMSEY, MINNESOTA



EXHIBIT B.13

**LANDFORM**
  
 From Site to Finish

**DESIGN FRAMEWORK**
  
**The COR at Ramsey**
  
 LIGHT POLE BANNER SIGNS
   
 Standard Signage





PERMANENT TENANT SIGN 1.



RAMP MARQUEE SIGN 2.



INFILL BANNERS 3.



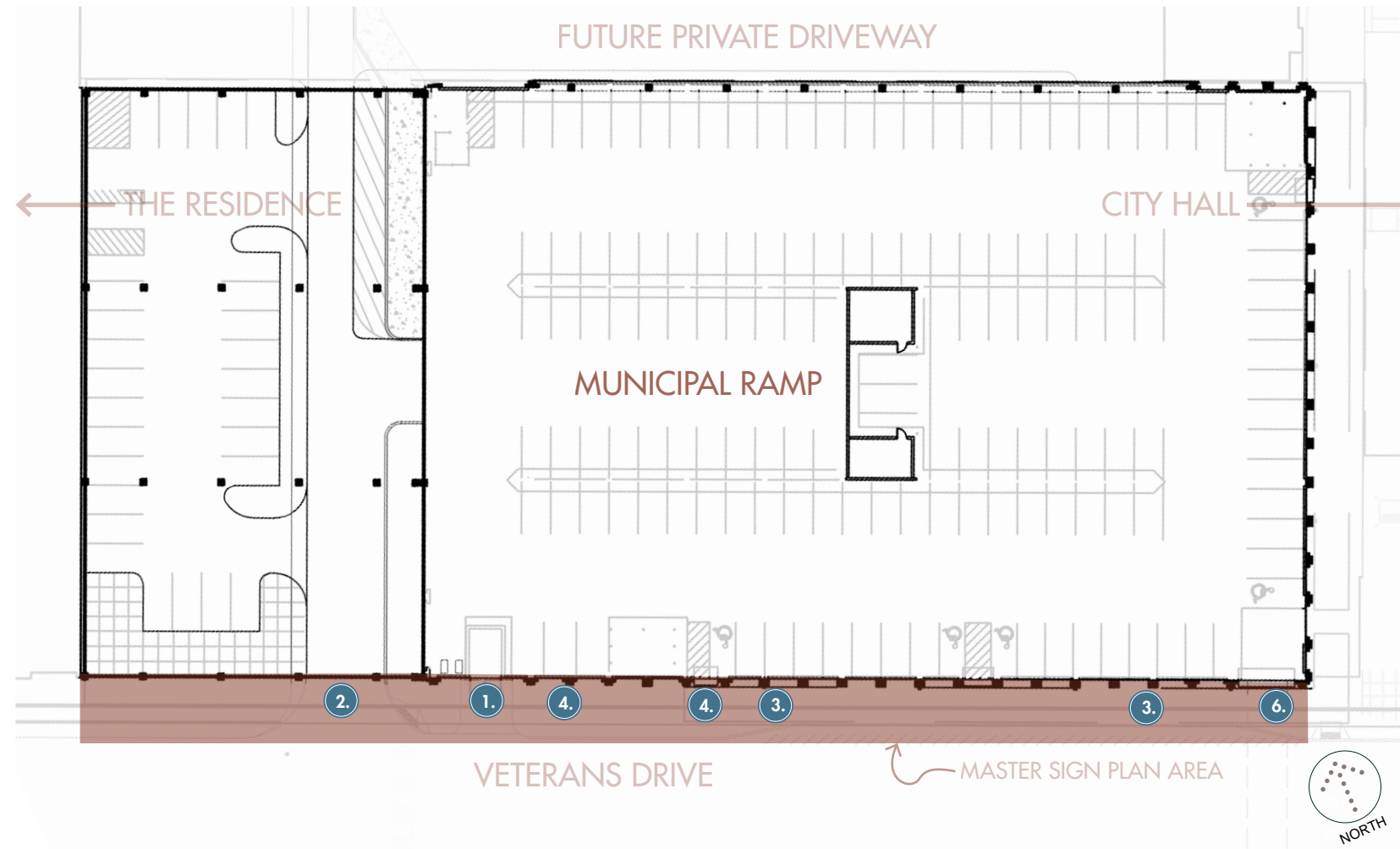
COLUMN BANNERS 4.



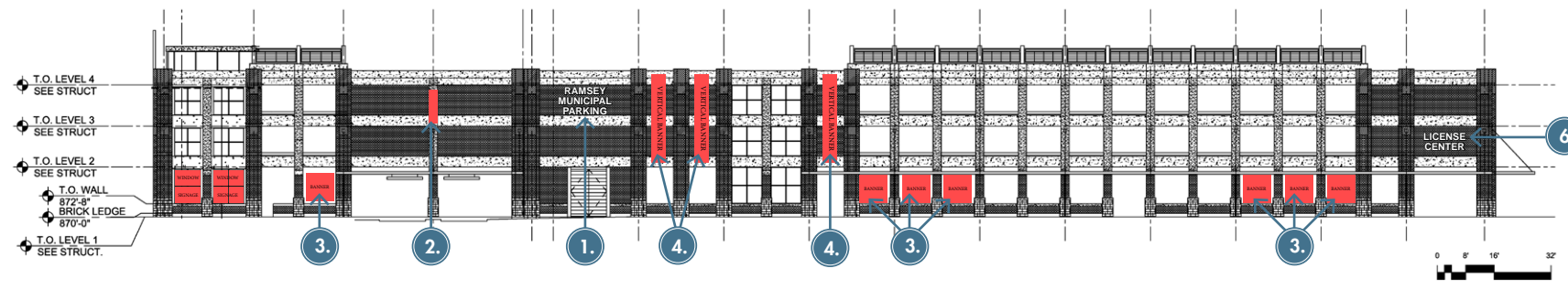
WINDOW FILM SIGN 5.



PERMANENT TENANT SIGN 6.



PLAN - N.T.S.



SOUTH ELEVATION - N.T.S.



**Sign Descriptions:**

- 1. 'RAMSEY MUNICIPAL PARKING' brushed aluminum letters, flood illuminated.  
Typical Sign Area = ±66 S.F.  
**Qty. 1 Total South Elevation = 66 S.F.**
- 2. Two-sided Internally illuminated marquee sign cabinet. Typical Sign Area = ±60 S.F.  
**Qty. 1 Total South Elevation = 60 S.F.**
- 3. Typical banner 6'x 6' printed perforated canvas with finished seams; occurs at open bays.  
Typical Sign Area = 36 S.F.  
**Qty. 6 Total South Elevation = 216 S.F.**
- 4. Typical banner 4'x20' printed canvas x 3 sides.  
Typical Sign Area = 240 S.F.  
**Qty. 3 Total South Elevation = 720 S.F.**
- 5. Window film signs. Sizes vary.  
Typical Sign Area = 64 S.F.  
**Qty. 2 Total South Elevation = 128 S.F.**
- 6. "LICENSE CENTER" brushed aluminum letters  
Typical Sign Area = 60 S.F.  
**Qty. 1 Total South Elevation = 60 S.F.**

**Ramp Elevation & Sign Area Calculations:**

South Elevation:

Total Elevation Area = ±15,589 S.F.  
 Total Allowable Sign Area (15% Elevation Area) = ±2,338 S.F.  
 Total Proposed Sign Area Used = 1250 S.F.



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The COR Timeline of Significant Events—Updated June 12<sup>th</sup> 2012



**CITY COUNCIL WORK SESSION  
CITY OF RAMSEY  
ANOKA COUNTY  
STATE OF MINNESOTA**

The Ramsey City Council conducted a City Council Work Session on Tuesday, May 26, 2015, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Mayor Sarah Strommen  
Councilmember Jill Johns  
Councilmember Mark Kuzma  
Councilmember John LeTourneau  
Councilmember Chris Riley  
Councilmember Melody Shryock (arrived at 6:05 p.m.)  
Councilmember Kristine Williams

Members Absent: None

**Planning Commission**

Members Present: Chairperson Gary Levine  
Commissioner Andrews Andrusko  
Commissioner Randy Bauer (arrived at 6:07 p.m.)  
Commissioner Ralph Brauer  
Commissioner Cindy Nosan  
Commissioner Gary VanScoy

**Planning Commission**

Members Absent: Commissioner Matthew Maul

Also Present: City Administrator Kurtis Ulrich  
Parks and Assistant Public Works Superintendent Mark Riverblood  
Community Development Director Timothy Gladhill  
City Planner Chris Anderson  
Asst. City Administrator/Economic Development Manager Patrick Brama  
City Attorney Joe Langel

**1. CALL TO ORDER**

Mayor Strommen called the City Council Work Session to order at 5:45 p.m.

**2. TOPICS FOR DISCUSSION**

**2.01: Review Development Plan and Design Framework for The COR**

Community Development Director Gladhill reviewed the staff report and key discussion topics that he wanted the group to consider.

Commissioner VanScoy stated that he was a part of the group when the plan was originally developed and since that time, as the City has gone through process of approving projects, there is more varying from the original concept. He stated that it makes him uncomfortable for the City to consistently approve variances from the Ordinances, as that inhibits the ability to accomplish the original goal. He questioned if the original concept is still valid or whether he is holding onto old values that simply do not work. He stated that he measures applications against the Ordinances and whether or not the project fits within the desired intent of the City. He believed that the City should review the basics and if the vision has changed, make the necessary amendments. He stated that parking is still an issue, noting the desire for shared parking but the lack of a plan to reach that end result.

Mayor Strommen stated that multiple plans can still bring the City to the same vision. She questioned whether the Commissioner believes the vision is the same but the plan has changed or whether he believes the vision and plan have changed.

Commissioner VanScoy stated that he believed both the plan and vision have changed, noting that he does not oppose the change but simply believes that the vision and plan should be amended to fit the City's current path.

Commissioner Bauer stated that the idea had been that The COR should never look like Riverdale. He stated that there is a sense that perhaps the development is moving in more of a Riverdale direction. He questioned the Council's idea for the future development of The COR.

Commissioner Levine stated that things have changed just as life changes and that currently the City is not "playing by the rules". He stated that the City should review what is being done and what needs to be done and amend the rules to better fit the desired outcome as that has changed over time, rather than continuing to issue variances.

Mayor Strommen agreed that the Master Plan has changed several times throughout this course but believed that many elements of the vision remain the same. She stated that there may be degrees of implementation to reach the vision, noting that some elements of the original vision may still exist but in a different manner than originally thought.

Community Development Director Gladhill stated that one of the main issues is parking strategies, specifically surface parking, and believed that some of the language in the zoning code could be further clarified. He noted that mixed use is another large component and confirmed that there may also be better language to address that component.

Mayor Strommen agreed that clarification of the zoning code could be made but stated that perhaps there should also be a higher-level discussion regarding the elements of the original vision as some of those items may have changed over time.

Community Development Director Gladhill questioned whether that discussion should occur at the Planning Commission level, a joint Commission and Council meeting or whether public input should also be gathered.

Commissioner Levine stated that he liked the idea of a joint meeting to check back in and make sure the discussion the Commission is having is moving in the right direction but did not believe that a large public meeting would be necessary.

Commissioner Bauer stated that he wanted to make sure that the process is transparent as well because multiple residents have expressed that they felt the original discussions for The COR had been hushed and were not transparent.

Councilmember Shryock arrived.

Mayor Strommen stated that through this process there should be a review of the original vision to determine if that is the same or where changes need to be made to the elements or implementation items. She stated that perhaps those results are then shared with the public.

Councilmember Riley stated that another step of the process would be to determine the impediments to the desired vision.

Commissioner Randy Bauer arrived.

City Administrator Ulrich stated that there is a good amount of community interest in The COR and agreed that could be a good public involvement process.

Mayor Strommen agreed but believed that the review should be completed first in order for everyone to be on the same page.

Councilmember LeTourneau agreed that this would be a good exercise. He stated that there has been a good effort for the City to get out of the development business but believed that there would be a good benefit for the City to take a step back and review the best methods to reach the desired end result.

Mayor Strommen agreed that the City has been focused on being a landowner and getting out of the development business which has distracted the City from their overall role in the planning process.

Councilmember Kuzma stated that his platform had been for the City to get out of the development business and believed that the City has done a lot in the past ten years. He stated that there has been a large amount of development in The COR in the past ten years along with beginning the Armstrong interchange project. He stated that the City was dealing with these issues while bringing on a new economic development staff member and working with reality rather than what the Metropolitan Council had projected. He also believed that the City has been much more transparent than it had been in the past.

Councilmember Williams stated that the Economic Development Authority (EDA) has recently been brought into discussions, which she thought was a great idea and also provides another level of transparency.

Commissioner Levine stated that this process has been underway for about 15 years and acknowledged that things change. He stated that the City is trying to be transparent and believed an open forum would be a great part of this process where the changes and vision can be discussed. He also believed that it is important for the Council and Commission to meet jointly in order to facilitate discussion of this nature.

Commissioner Bauer stated that from a systemic perspective the interest in The COR has grown because this Council has been more open and residents feel that they are being listened to and are more a part of the discussion. He stated that they also need to remember that some people had expressed the opinion that the original plans were not attainable and therefore could have never been reached.

Councilmember Johns stated that this would be a great opportunity to refresh on what is possible, noting the timing of the Armstrong interchange project actually moving forward.

The consensus of the Council and Commission was to conduct an inventory on the elements of the original plan compared to the current status and the vision for The COR today, which will be reviewed with the Planning Commission, other relative Advisory Boards, and City Council.

Councilmember Riley stated that the market must also be considered, noting that it may not be economically feasible for development to fund a multi-million dollar parking ramp. He stated in that case the City would need to fund the element or amend the development plans.

Mayor Strommen agreed that the plan should be reviewed to determine if some of the original elements are feasible.

Community Development Director Gladhill stated that along with this process he would like to see the plan made more user friendly as it can be very confusing to follow in the current format.

Commissioner Andrusko questioned what the Commission would do in the time being.

Community Development Director Gladhill stated that the Commission would continue to enforce the regulations as they exist today.

Commissioner VanScoy stated he is having difficulty supporting the land uses proposed by applicants as there is conflict with the existing regulations. He stated that the Commission will still look at the land use and Ordinances to determine if they fit and make a recommendation to the City Council based on that, noting that the Council may have different priorities than the Commission.

Mayor Strommen appreciated the Commission bringing this discussion forward as she believed it would be a benefit to have this refresher in order to better match the implementation items with the intent. She stated that policy decisions will need to be made as the projects come. She stated that the opinion and recommendation of the Planning Commission matters, whether or not the recommendation is followed. She agreed that the City Council does have different priorities

regarding the policies and acknowledged the difficult position the Commission is placed in during this review period.

Commissioner Bauer stated that there are companies that gather consumer information that may be useful to the City during this process.

## **2.02: Discuss Approach for Comprehensive Plan Update**

Community Development Director Gladhill reviewed the staff report and the role of the different Advisory Boards and Commissions during this process.

Councilmember Kuzma stated that he has attended a number of the focus meetings hosted by Community Development Director Gladhill and believes he does a great job of facilitating and gathering public opinion.

Mayor Strommen encouraged everyone to think carefully about the questions, such as those in the work planning document, to ensure that the questions are able to be answered by all residents. She provided the example of using questions that everyone can answer rather than higher level planning questions that could be intimidating.

## **2.03: Discussion on Potential Improvements to Other Planning Commission Topics**

Community Development Director Gladhill presented the staff report.

Commissioner Bauer stated that perhaps it would be a good idea to do a review of all the planning Ordinances as there are so many variances being issued, in order to better fit the intent of the City within the Code.

Mayor Strommen suggested that staff use a prioritization level, beginning with those issues that are constantly receiving variances and delaying review on Ordinances that may not need to be amended.

Commissioner VanScoy stated that at one time there was a small architectural review board that considered projects proposed for The COR and believed that perhaps there should be a discussion on whether that group be re-established.

Community Development Director Gladhill stated that board was removed during the revisioning process as the City was very involved, to the point of hiring an architect to sit on that board. He stated that staff agrees that there could be benefit to re-establishing some format of that group to review applications within The COR.

Councilmember LeTourneau stated that it may be beneficial to define a process where the community could be divided up into sections, noting that while some development regulations fit within certain areas of the City, those regulations do not fit well with other areas of the City.

Mayor Strommen questioned if that process would be outside of the Comprehensive Planning process.

Councilmember LeTourneau stated that he was unsure as currently the City makes choices on a community level over the entire City and perhaps decisions would be more appropriately made over smaller development areas.

Commissioner Andrusko stated that perhaps the idea of neighborhood boards could be used in order to gain input from different areas of the City during the Comprehensive Plan process.

Community Development Director Gladhill stated that the City has been doing that to some degree and agreed that would be a great idea to continue that aspect, noting that additional ideas could be built into the Work Plan.

City Administrator Ulrich stated that idea reminds him of the Northfork Home Owners Association meeting that Mayor Strommen and Councilmember LeTourneau attended. He explained that residents often feel more comfortable sharing ideas about their geographic level when the group comes to them on “their turf”.

Mayor Strommen agreed that could be done by Ward, visiting one Ward each quarter.

Community Development Director Gladhill questioned if the Commission and Council feel that enough information is being brought forward through staff reports and with contingent conditions, which sometimes allow an application to move a bit faster.

Commissioner Levine commended staff, noting that staff is doing an excellent job of supplying the necessary information to facilitate a review while still allowing applications to move forward.

Commissioner Bauer agreed that the City has a great staff team in place.

Councilmember Riley stated that he would not like to see conditions made contingent upon agreements that have not yet been discussed.

Mayor Strommen agreed that this process and the input from staff has been a vast improvement from where the City has been in the past and appreciated the upfront planning of staff. She believed that staff was doing an excellent job of using their judgement to determine when there is sufficient information for an application to move forward or whether additional information is necessary before review can occur. She thanked the Commission for their time and input during the discussion tonight.

### **3. TOPICS FOR FUTURE DISCUSSION**

Noted.

### **4. MAYOR / COUNCIL / STAFF INPUT**

None.

**5. ADJOURNMENT**

The Work Session of the City Council was adjourned at 6:55 p.m.

Respectfully submitted,

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Kurtis G. Ulrich  
City Administrator

ATTEST:

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Jo Ann M. Thieling  
City Clerk

Drafted by Amanda Staple  
*TimeSaver Off Site Secretarial, Inc.*

**PLANNING COMMISSION  
CITY OF RAMSEY  
ANOKA COUNTY  
STATE OF MINNESOTA**

The Ramsey Planning Commission conducted a special work session on Thursday, July 9, 2015, in the Lake Itasca Room at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present:     Chairperson Gary Levine  
Commissioner Randy Bauer  
Commissioner Matthew Maul  
Commissioner Cindy Nosan  
Commissioner Gary VanScoy

Members Absent:     Commissioner Andrew Andrusko  
Commissioner Ralph Brauer

Also Present:     Community Development Director Tim Gladhill  
City Planner Chris Anderson  
Assistant City Administrator/Economic Development Manager Patrick Brama  
Community Development Intern Geoff Solomonson

**1.     CALL TO ORDER**

Chairperson Levine called the regular meeting to order at 7:03 p.m.

**2.     APPROVAL OF AGENDA**

Motion carried by Commissioner Bauer, seconded by Commissioner VanScoy, to approve the agenda as presented.

Motion Carried.

**3.     MINUTES**

Community Development Director Gladhill explained the purpose of the special work session was to develop a comparison document of the COR Development Plan and the Original Ramsey Town Center Master Plan.

He explained how at the May 26, 2015 Joint Meeting with the City Council, Staff was directed to prepare a comparison document and made the decision to touch base with the commission. The COR vision is very different from the past ideas, but headed in the right direction. He asked if there were to be changes to the vision and ask “what needs to change?”

Community Development Director Gladhill described the history of the COR which originated from the 2020 Comprehensive Plan (1998-2001). In the plan was described how the area was an agricultural use and due to growth forecasts, Ramsey decided to protect its rural areas and concentrate this new growth into a more centralized area.

The question arose of what is a mixed-use development. In the 2001 general election, an advisory question for the ballot was developed, essentially a survey question for residents. The question was “Do you want a Town Center with restaurants, shopping, and other amenities along the Highway 10 Corridor?” This was around the same time as the Northstar Commuter Rail transitway planning.

Between 2009-2010, the City began a process to provide a new vision for the development to better match current market conditions at the time, the revision of the plan to the COR plan today.

Community Development Director Gladhill then asked the commissioners to choose their top 3 components of the original master plan for Ramsey Town Center.

### **ORIGINAL MASTER PLAN (PREVIOUS)**

Commissioner Nosan asked what the “West 30” referred to in the original plan.

Community Development Director Gladhill described the area to the North and West of the roundabout at Sunwood Drive guided for retail.

Commissioner VanScoy asked about redevelopment and the current zoning of the West 30.

Community Development Director Gladhill explained the vision as a more traditional suburban retail center, while still maintaining a higher degree of architectural standards.

Discussion ensued about parking standards for traditional suburban retail.

Commissioner Bauer explained how he wrote down shared parking, such as the ramp next to the Municipal Center [and apartment complex] and how it is critical not to lose the idea of shared parking.

Commissioner VanScoy explained how density requires parking.

Commissioner Nosan responded how the church [Northgate Church] is a great example of successful shared parking.

Mixed-Use was the next topic. Community Development Director Gladhill asked if it meant vertical mixed use within a single building or different types of single-use buildings within a single block.

Chairperson Levine commented it did not mean different uses within a single block.

Commissioner Nosan expressed concern about too much mixed-use in the area. She explained currently it seems to be only the residents actually living in the COR who use the area.

Commissioner Bauer described the apartment complex connected to the Municipal Parking Ramp and how the City Council originally wanted retail on the first floor of the complex. The developer did not want retail on the first floor. Commissioner Bauer then explained without a large store/attraction there is no market for mixed-use commercial in this area.

Commissioner Nosan remarked how the residents of Ramsey are always going to have cars and is what the culture of the town is used to. She explained how Ramsey will never be Uptown [Minneapolis].

Community Development Director Gladhill then clarified the consensus on the meaning of mixed-use for the original plan was vertical mixed use. Gladhill then explained that the City would continue to discuss the current definition in the next step, and perhaps continue to discuss this as a policy question moving forward.

Commissioner Maul explained how mixed-use development works very well in Minneapolis and Uptown because there is limited space, whereas there is still plenty of land in Ramsey.

Community Development Director Gladhill then went through the remaining components of architectural standards, which was split into the architecture of the building itself as well as the architectural review, density; strong blue system, of which The Draw is itself a utility and amenity; and the Master Park Plan.

Commissioner Bauer explained he had written the strong blue system and wanted to see it developed further. He then said water re-use is important and water is unique to Ramsey because of the two rivers. He said he would like to see developments with ideas like fountains using rainwater.

City Planner Anderson explained The Draw uses storm water and is recycled through the park surrounding it.

Chairperson Levine then remarked about how the West 30 idea has changed from the medium-density residential idea to now [retail].

Community Development Director Gladhill then asked the commissioners if there was any components of the Original Master Plan they would like to strike from or add to the list.

Commissioner Maul stated he would like to see the strict sign standards removed.

Chairperson Levine agreed and said he would like to see more relaxed sign standards.

Chairperson Levine spoke of how there was an educational component to the Original Master Plan in buildings being a multi-use, multi-educational facility for shared resources and how he would like to see that brought back in.

Commissioner VanScoy remarked how the residential density was a top priority in the old plan, but not in the new (COR plan). He explained how the old plan was to drive people into the center with a transit-oriented development and build a population of enough people to support retail and subsequently encourage those living in the outer areas to drive in to use the amenities as well.

Commissioner Bauer remarked how he had second thoughts on the retail area in the southwest are of the COR. He explained how the type of retail should be changed to a unique destination type, similar to Stillwater [MN] with craft and antique stores.

Chairperson Levine agreed stating Stillwater is a much better example for Ramsey to follow than Arbor Lakes [Maple Grove].

### **THE COR DEVELOPMENT PLAN (CURRENT)**

Community Development Director Gladhill then asked the commissioners to repeat the process with their top 3 components of the current COR development plan. He explained the similarities of the plans, but some changes occurred when they tried to apply it, such as the West 30 [retail vs residential], eliminating the architectural review, and reducing the floor to area ratio (FAR).

The first topic was the definition of mixed-use. Community Development Director Gladhill explained the change from vertical mixed-use to many single uses within a block.

Commissioner VanScoy agreed the definition had changed, but asked what is considered a block.

Community Development Director Gladhill explained a block is an area surrounded by public or private streets, but could use a better definition [for this plan].

Chairperson Levine made the comment how less is happening in the COR now.

Community Development Director Gladhill explained how less vertical mixed-use is happening, but there is still activity and interest. Gladhill noted that mixed-use is still a goal of the community, however the community has experienced more residential growth compared to retail.

Chairperson Levine remarked how less four (4) story structures and more one (1) story structures equals less activity, or density. He then said less activity equals less rooftops (residents).

Commissioner VanScoy remarked how mixed-use in the COR now seems to be all residential, something different which wants to be built seems like it needs a variance. He then asked how you plan to get the mixed use [in the future] which we want.

Commissioner Maul agreed adding scale should be asked in addition to Commissioner VanScoy's question. He then remarked if we want vertical, we need to plan for it now.

Community Development Director Gladhill then explained more of the components as architectural standards & relaxed sign standards and parking.

Chairperson Levine made the comment how shared parking does not always work.

Commissioner Maul agreed adding and asking again, to what scale.

Commissioner VanScoy remarked how there was no strategy to implementing the shared parking [in the original plan].

Chairperson Levine asked how we pay for parking.

Community Development Director Gladhill explained that the City has had a strategy to implement shared parking, but does require an up front investment and has required some form of public assistance in the past in Ramsey's examples, namely through grant awards. Gladhill reviewed the history of the Ramsey Municipal Ramp and the feedback the City originally received in the early stages of the development before uses such as Residence at The COR was completed.

Commissioner VanScoy remarked how there needs to be a new strategy to shared parking because parking supports density and retail.

Commissioner Bauer explained how the original plan had plans for a second parking ramp. He then explained how it should be pursued in a development which has the developer paying half and the City paying half.

Commissioner VanScoy asked how we define parking counts and how there are conflicting methods for parking counts/requirements.

Community Development Director Gladhill then went through the rest of the components on the board which were the West 30; transit; storm water; density; and streetscape. He then remarked how the streetscape portion has been redesigned different than the original plan, especially Ramsey Parkway. He explained how in the original plan the Parkway continued west with landscape. He then asked what components should be in the streetscape.

Chairperson Levine remarked how Ramsey Parkway was an attraction on the original plan.

Community Development Director Gladhill asked the commissioners if there were anything in the COR plan that they would like to strike out or add to the list.

No commissioners had anything to strike from or add to the list.

Chairperson Levine asked what the 'strong downtown feel in center' meant.

Chairperson Levine expressed concern about the West 30.

## **FUTURE POLICY QUESTIONS**

Community Development Director Gladhill asked if there were additional questions to add to the list of policy questions from city staff [handout].

Chairperson Levine asked about McDonalds, remarking on the question of whether Ramsey wants to continue to allow uses with a drive-thru and gas station. He said McDonalds certainly has a drive-thru and where they go, other restaurants [and retail] will follow.

Commissioner Bauer responded with the reconstruction of Armstrong [Boulevard], it will have the effect of getting people off Highway 10, for example weekends when gas, groceries, and meals are needed on the way [in and out of the city and metropolitan area].

Community Development Director Gladhill responded that drive-thru and gas station type business should be acceptable in portions because Ramsey needs and wants those types of businesses.

Commissioner VanScoy remarked how the plan for the places of employment (southeast quadrant of the COR) needs to be reevaluated.

Community Development Director Gladhill responded, adding with the question, what are appropriate uses within the business park.

Commissioner Bauer responded with the fiber-optic in place, it is not marketed towards retail, but should be marketed towards others as job opportunities.

Chairperson Levine asked for clarification on the second and third policy questions. [2. Are there uses we are not allowing that we do not want to allow? 3. Are there uses we are allowing that we do not want to allow?]

Commissioner Nosan asked how we are not allowing uses we do not want when we do not know what those uses are.

Chairperson Levine remarked [question 3.] was saying basically what does [the City] not want here.

Commissioner Nosan responded saying she does not want the COR to turn into a solid line of bars like Jackson Street in Anoka.

Chairperson Levine responded noting that there may be uses we do not want to allow within the development, and that the City should review uses that are currently allowed as part of this process.

Community Development Director Gladhill remarked the definition might be too broad.

Commissioner VanScoy asked to look at the sub-districts [of the COR], what is their intent, and what do they mean.

Chairperson Levine added to Commissioner VanScoy's question with what the sub-district was and what it should be.

Commissioner Nosan asked Assistant City Administrator/Economic Development Manager Brama about the possibility of Target in Ramsey. She spoke of her displeasure of driving to Elk River or Coon Rapids just to visit the store.

Assistant City Administrator/Economic Development Manager Brama explained how Target in Ramsey comes down to logistics and access. He then went to say there is interest from business based on the Armstrong [Blvd] interchange, but they are waiting for 'more rooftops' in the community. He said it comes down to the economy and how Ramsey is surrounded by communities that have a Target, for example.

Community Development Director Gladhill explained that lack access across the Mississippi River to Dayton and across the Rum River reduces the market area or drive-time for retail users.

Assistant City Administrator/Economic Development Manager Brama added when speaking to a broker, they discussed how Ramsey is an island within the area, in terms of access [the lack of current highway 10 freeway] and connections [across the Mississippi] to other communities. He

also went to say there is a possibility retail is too saturated with the amount available due to the restricted access and connections.

Community Development Director Gladhill explained how it would be helpful to see existing examples, local examples, or other retail areas. He suggested a tour with stops including Silver Lake Village in St. Anthony, Arbor Lake in Maple Grove, and Riverdale Village in Coon Rapids. These stops would give the commission and idea of what they do and do not want in the COR.

Commissioner Nosan asked Assistant City Administrator/Economic Development Manager Brama asking if retail has a hard time coming here.

Assistant City Administrator/Economic Development Manager Brama responded saying the Armstrong [Blvd] interchange could help and could also possibly be the domino that falls [propelling growth].

Commissioner VanScoy asked where the City and County stand in regards to the Dayton Bridge [Mississippi River crossing].

Community Development Director Gladhill responded saying [Ramsey's] comprehensive plan and [the City of ] Dayton's comprehensive plans both have it listed and both plans match, even to the degree of prioritizing a corridor from Highway 10 to Interstate 94.

Commissioner VanScoy asked if Hennepin County and Anoka County were doing anything in regards towards completing this matter.

Community Development Director Gladhill explained how Anoka County has purchased land in anticipation of the connection, and the connection lines up in both the City of Ramsey and the City of Dayton, but the timeline for this project is undetermined as it is not on the State [of Minnesota] funding plans or MNDOT's plans.

Community Development Director Gladhill told the commission how staff would refine the document based on the responses that evening plus other future meetings, eventually to bring back to the Planning Commission and the City Council.

Commissioner Bauer asked where the planning staff stood with the upcoming comprehensive plan.

Community Development Director Gladhill responded saying the plan is being approached in smaller sections and the information in the meeting that evening would be incorporated into the future document. He then went on to explain that as the smaller section become developed, citizen engagement opportunities and feedback would be initiated closer to fall.

#### **4. ADJOURNMENT**

Motion to adjourn the meeting.

Motion Carried.

The special work session of the Planning Commission adjourned at 8:20 p.m.

Respectfully submitted,

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Tim Gladhill

Community Development Director

ATTEST:

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JoAnn Shaw

Community Development Assistant

Drafted by Geoff Solomonson

DRAFT

#### **7.04: Review Comparison Document of The COR Development Plan and Original Ramsey Town Center Master Plan Prepared by the Planning Commission**

Community Development Director Gladhill reviewed the staff report noting that the purpose for tonight is to ensure that the format, general content and layout match that desired through the Council and expressed at the last joint meeting. He advised that the Planning Commission will be reviewing this at depth during their August meeting and noted that the other Boards and Commissions would also be reviewing the information to provide input.

Councilmember Riley stated that he is less interested in where the City has been and more interested in the direction the City would like to go.

Councilmember Kuzma commended the Planning Commission and the amount of work they have done and continue to do on this matter.

Councilmember Williams also appreciated seeing where the City has been and being able to see that scope. She appreciated the efforts of the Planning Commission as they are being asked to review requests under standards of the original town center plans rather than the current plans for The COR.

Councilmember Shryock also agreed that it was helpful to see the path the City has taken. She also agreed with Councilmember Riley and stated that the City should focus on the desire for the future and the current market. She noted that although there are certain resident desires for The COR, the market may not drive that type of development in those locations. She referenced the sign regulations and believed that the City is in a good position in that regard and did not know that additional amendment would be necessary in that area.

Community Development Director Gladhill explained that they would not be reducing the sign design but instead amending the process for approval.

Councilmember Williams stated that she would like to see input from those within The COR as well as Coborn's, specific to the parcel adjacent to their property.

Acting Mayor LeTourneau stated that the original plan was flexible in order to be responsive to the conditions in the market, which he believed would continue to be important. He stated that perhaps the previous Councils were not open with amendments to the plan and thought it was great to see the public involvement in this open process.

Councilmember Shryock agreed that the communication has been an important focus throughout this process, making this process open and communicating updates to the residents. She welcomed any additional input from residents or businesses.

Community Development Director Gladhill confirmed the direction that he heard tonight which will include additions to the document such as highlighting the next steps and highlighting continuing work with property owners within The COR.

**Regular Planning Commission**

5. 4.

**Meeting Date:** 08/06/2015

**By:** Geoff Solomonson, Community Development

**Information**

**Title:**

Discuss Potential Future City Code Amendment to Construction Hours

**Purpose/Background:**

The City of Ramsey has the code as listed below for construction hours within the city. The reason for this possible amendment to the Code is due to the number of concerns that have been raised to City Council regarding the time limits and to address some of the public perception of the listed hours. The question was most recently raised during the review of a Major Subdivision. Residents have had concerns with the hours listed as the maximum allowance, not necessarily being the standard hours of construction that most contractors choose to use. The contractor for the development of Harvest Estates, for example, negotiated construction hours to stop at 8:00 p.m. instead of the maximum stopping time of 10:00 p.m.

The goal of this amendment is to look at the possibly of introducing an earlier stopping time for construction with exemptions built in for public improvement projects, private homeowner construction, and extenuating circumstances which would involve City Administrator approval or City Council approval. It is noted, however, that it is rare that the complaints the City actually receive are that of evening work hours. Almost exclusively, the complaints are in regards to early start hours in the morning.

Also included in this document for examples are the hours of construction and stipulations of neighboring communities as a basis for discussion. It is worth noting that the majority of the surrounding communities have similar hours in effect as the City of Ramsey with some starting at 6:00 a.m.

**The code as it currently exists:**

**Sec. 105-1. - Restrictions on hours for construction.**

(a) *Work hours.* It shall be unlawful to engage in or conduct any activity in the construction of any building or structure, or the laying of any pavement, including but not limited to the making of any excavation, clearing of surface land and loading or unloading material, equipment or supplies in any residential district of the city, except between the hours of 7:00 a.m. and 10:00 p.m. Monday through Saturday. However, such activity shall be lawful from 6:00 a.m. to 10:00 p.m. with prior permission from the city administrator and his designee due to extenuating circumstances such as excessively warm temperature, the need to avoid traffic during normal work hours, etc. Any Sunday activity is also limited to the hours of 7:00 a.m. to 10:00 p.m. and to that which is being performed by the owner/occupant of the parcel on which the activity is occurring.

(b) *Permit.* Application for a permit shall be made in writing to the administrator and shall state the name of the applicant and their business address, the location of the proposed work, and the reason for seeking a permit to do such work on Sunday as well as the estimated time of the proposed operations. No such permit shall be issued excepting where the public welfare will be harmed by failure to perform the work at the time indicated.

(c) *Provision not to prevent necessary work.* Nothing in this section shall be construed to prevent any work necessary to prevent injury to persons or property at any time.

(Code 1978, § 7.81; Ord. No. 95-17)

**Possible code edit amendment:**

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surface land and loading or unloading material, equipment or supplies in any residential district of the city, except between the hours of 7:00 a.m. and 8:00 p.m. Monday through Saturday unless work is done by a homeowner, in which residential homeowner construction is limited from 7:00 a.m. to 10:00 p.m. However, such activity shall be lawful from 6:00 a.m. to 10:00 p.m. with prior permission from the city administrator and his designee due to extenuating circumstances such as excessively warm temperature, the need to avoid traffic during normal work hours, etc. Any Sunday activity is also limited to the hours of 7:00 a.m. to 10:00 p.m. and to that which is being performed by the owner/occupant of the parcel on which the activity is occurring. Public improvement projects, public works construction activities and other similar emergency construction activity shall be exempt from the construction hours listed.

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(Code 1978, § 7.81; Ord. No. 95-17)

**Notification:**

**Observations/Alternatives:**

**Funding Source:**

**Recommendation:**

While an amendment to City Code may be warranted, Staff poses a goal to balance the tranquility of the surrounding neighborhoods with the flexibility to be able to negotiate later hours where feasible. This topic report is for discussion only. There is no policy change being proposed at this time.

**Action:**

No action is being requested. Staff is seeking initial feedback to aide in development an actual ordinance for future consideration.

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**Attachments**

Construction Hours Document

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**Form Review**

<b>Inbox</b>	<b>Reviewed By</b>	<b>Date</b>
Tim Gladhill	Tim Gladhill	07/31/2015 02:05 PM
Form Started By: Geoff Solomonson		Started On: 07/31/2015 01:43 PM
Final Approval Date: 07/31/2015		

# City of Ramsey

## Code – Construction Hours

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- (b) *Permit.* Application for a permit shall be made in writing to the administrator and shall state the name of the applicant and their business address, the location of the proposed work, and the reason for seeking a permit to do such work on Sunday as well as the estimated time of the proposed operations. No such permit shall be issued excepting where the public welfare will be harmed by failure to perform the work at the time indicated.
- (c) *Provision not to prevent necessary work.* Nothing in this section shall be construed to prevent any work necessary to prevent injury to persons or property at any time.

(Code 1978, § 7.81; Ord. No. 95-17)

Other neighboring community's construction hours as it appears in their respective city code.

City	Hours	Notes:	Code
Ramsey	7:00am-10:00pm, Monday-Saturday	Construction may begin at 6am with permission from city administrator	Part 2, Chapter 105, Article 1, §105-1-A
Andover	7:00am-10:00pm		Title 5, Chapter 6, §5-6-3
Anoka	7:00am-10:00pm	Between 10pm and 7am, equipment, events, or activities must be authorized, sponsored, permitted or approved by city council or City Manager with conditions to the license, permit or contract.	Chapter 38, Article 1, §38-1-C-1-D
Apple Valley	6:00am-10:00pm	Snow removal and street-sweeping are exempt. Emergency work to preserve the public health, safety, or welfare, or in the performance of work necessary to restore a public service or eliminate a public hazard is exempt with reasonable actions to minimize the amount of noise.	Title XIII, Chapter 130, §130.56 & §130.57
Blaine	7:00am-10:00pm	City or state authorized, sponsored, or licensed work to preserve public health, safety, or welfare or to restore public service or eliminate a public hazard shall be exempt.	Chapter 50, Article IV, Division 2, §50- 203-5
Bloomington	7:00am-10:00pm, Monday-Friday; 9:00am-9:00pm, Saturday		Part 2, Chapter 10, §10.29.07-C
Burnsville	7:00am-10:00pm		Title 7, Chapter 1, §7-1-2 D-1
Champlin	7:00am-9:00pm	In the case of urgent necessity to the public health and safety and with a permit from the city administrator for a period not to exceed three days without renewal.	Part 1, Chapter 38, Article 3, §38-70-B- 8
Coon Rapids	7:00am-10:00pm, Monday-Saturday	City or state authorized, sponsored, or licensed work to preserve public health, safety, or welfare or to restore public service or eliminate a public hazard shall be exempt.	Title 8, Chapter 8, §8-403-10
Cottage Grove	7:00am-7:00pm, Monday-Friday; 9:00am-7:00pm, Saturday	Residential property owners or tenants 7:00am-10:00pm, M-F; 8:00am-9:00pm, weekends and legal holidays. Public projects performed by governmental agency or their agents are exempt. Repairs to utility structures that are damaged, in disrepair, or out of service and pose a danger to life, health, or significant loss of property are also exempt. Construction activities related to the following are exempt: refueling machinery and equipment, labor work not requiring power tools or equipment, construction equipment repair, surveying, construction staking, delivery of equipment or materials, and work preapproved by the city.	Title 4, Chapter 9, §4-9-8 & §4-9-3
Dayton	7:00am-7:00pm, Monday-Friday; 9:00am-4:00pm, Sat., Sun., Holiday	Excavation work may only commence between 7:00am and 7:00pm	Chapter 800, §806.03-4
Elk River	6:00am-9:00pm, Monday-Saturday	Single-family occupied residence owners and an authorized building inspector-based emergency for the protection of health and safety are exempt.	Chapter 46, Article 2, §46-37-7
Fridley	7:00am-9:00pm, Monday-Friday; 9:00am-9:00pm, Saturday	Alternate hour's work permit, Sunday, and legal holiday work permit may be issued by the city upon application.	Chapter 206, §206.09-5
Maple Grove	7:00am-9:00pm, Monday-Friday; 8:00am-9:00pm, Sat.-Sun.	8:00am-9:00pm on public holidays	Chapter 20, Article 3, Division 2, §20-85
Minnetonka	7:00am-10:00pm		Chapter 8, §850.015-1

Osseo	7:00am-10:00pm		Title XV, Chapter 150: §150.19-E
Otsego	7:00am-10:00pm, Monday-Saturday; 8:00am-10:00pm, Sunday	The Building Official may, in cases of emergency, grant permission to repair at any time when they find that such repair work will not affect the health and safety of the persons in the vicinity.	Chapter 4, Section 1, §4-1-4-A
Rogers		May limit as deemed necessary.	Part 2, Chapter 125, Article V, Division 2, Subdivision 1, §125-333-B
Plymouth	7:00am-9:00pm, Monday-Friday; 8:00am-9:00pm, Sat.-Sun.	8:00am-9:00pm on public holidays	Chapter 20, §2025.05, Subd. 4
Spring Lake Park	7:00am-10:00pm	Between 10pm and 7am, noise shall not exceed 50 decibels in any octave band of frequency above 300 cycles per second.	Title XV, Chapter 156, §156.033-B-1-A
St. Michael	7:00am-Sunset	Except for emergency construction activities such as street and other public works construction activities, utility repairs, flood prevention, snow removal, and other similar emergency construction activity	Title IX, Chapter 91, §91.18-AE

**Regular Planning Commission**

**5. 5.**

**Meeting Date:** 08/06/2015

**Submitted For:** Patrick Brama, Administrative Services

**By:** Patrick Brama, Administrative Services

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**Information**

**Title:**

Receive Future Business Park Infrastructure Analysis Preliminary Report

**Purpose/Background:**

**PURPOSE:**

The purpose of this case is to review a *preliminary* report from Bolton & Menk on required infrastructure improvements to the City's future business park; and to take comments/ suggestions.

The EDA reviewed this document in July; and will be reviewing a revised version on August 13. Staff is not looking for a specific motion or direction from the Planning Commission at this point. The intent is to simply convey they information to the Planning Commission early in the process in order to frame future policy discussions. That being said, Staff welcomes thoughts on the land use implications of the proposed future improvements shown in the study. Adopting the study in no means obligates the City to any improvements or investments.

For background on this work, please see attached RFQ.

**Staff Note:**

It is Staff's perspective, the "Project Financing" portion of this report is the last major outstanding item (page 11). Bolton & Menk would like to receive comments/ amendments to this preliminary report before the "Project Financing" section is completed. Staff intends to direct Bolton & Menk to develop multiple financing options. Staff will also connect with other City's and private developers to identify alternative strategies for assigning costs/ project financing. This report will be in front of the City Council on August 25.

**Notification:**

NA

**Observations/Alternatives:**

NA

**Funding Source:**

The study is being funded from the Tax Increment Finance (TIF) District No. 1

**Recommendation:**

NA

**Action:**

Staff is not looking for a specific motion or direction from the Planning Commission. Staff is looking for general comments/ questions/ suggested amendments to the report as it relates to land use implications of the study. The EDA and City Council will be the primary reviewers of the financing sections of the study.

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## Attachments

Ramsey Business Park 07022015

Request For Qualifications (RFQ) Future Business Park Bunker and Puma Improvements 11 19 2014

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### Form Review

**Inbox**

Tim Gladhill

Kurt Ulrich

Form Started By: Patrick Brama

Final Approval Date: 07/31/2015

**Reviewed By**

Tim Gladhill

JoAnn Shaw

**Date**

07/31/2015 02:09 PM

07/31/2015 02:22 PM

Started On: 07/20/2015 09:36 AM



# **Preliminary Report**

**for**

## **Future Business Park**

### **City of Ramsey**



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## **APPENDICES**

- APPENDIX A – FIGURES**
- APPENDIX B – TRAFFIC STUDY**
- APPENDIX C – STORM WATER ANALYSIS**

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## INTRODUCTION

The Ramsey City Council and Ramsey Economic Development Authority (EDA) have identified the need to prepare for the development of a new business park within the City. The City is targeting a large area of privately owned green-field space located on the north side of Trunk Highway 10, west of Armstrong Boulevard, for a future Ramsey Business Park. Currently, the City is in the process of rezoning this area to meet future land use goals. Figure 1 in Appendix A depicts the general area of the business park and study area.

The future business park is one component (about 92 acres) of a larger green-field area ready for development (about 350 acres). This larger green-field area includes areas for single-family residential development (about 118 acres), medium-density residential (about 31 acres) and room for a future private school campus (about 90 acres). In addition to green-field development, the City expects a portion of existing adjoining light-industrial space to be redeveloped into a traditional retail/commercial area (about 42 acres). See Figure 2 in Appendix A for future land use goals.

The City's future business park, and larger developable green-field area, will be served by the future Trunk Highway 10/Armstrong Boulevard (CSAH 83) interchange. Construction of this interchange began in the spring of 2015 and is expected for completion in the spring of 2017. The City expects the construction of this major interchange to increase the demand for development of this green-field area; and the future Ramsey Business Park.

The primary roadway system serving this developable area is Bunker Lake Boulevard and Puma Street. These roadways are both Municipal State-Aid (MSA) streets. This analysis includes studying traffic impacts resulting from the future business park (along with other users), examining the feasibility of constructing supporting infrastructure, and developing preliminary design layout plans and specifications.

## STUDY CONTENT

The primary purpose of this analysis is to determine minimum required infrastructure needs and costs associated with developing the green-field area. Roadways and intersections included in the study are as follows:

- Bunker Lake Boulevard from Armstrong Boulevard to Puma Street,
- Puma Street from Bunker Lake Boulevard to Alpine Drive,
- The Bunker Lake Boulevard/ Armstrong Boulevard intersection,
- The Bunker Lake Boulevard/ Puma Street intersection,
- The Puma Street/ Alpine Drive intersection, and

- 
- The Armstrong Boulevard/ Alpine Drive intersection.

Figure 3 in Appendix A depicts the roadways and intersections included in this analysis.

Our analysis consists of four separate components that, when tied together, provide an overview of the corridor needs:

- **Traffic Impact Study** – Defines the needs of the roadways and intersections in the study area,
- **Regional Storm Water Analysis** – Defines conceptual storm water ponding needs,
- **Preliminary Design Layout** – Provides a graphical depiction of the roadways and intersections, and
- **Final Report** – Describes required infrastructure improvements, layouts and costs.

## INITIAL INFORMATION

Prior to the study, the City of Ramsey provided the following information and guidance for the study area.

- The City is not master-planning this entire developable area. It is unknown where internal driveways, roadways and curb-cuts will be needed. Therefore, the design of Bunker Lake Boulevard and Puma Street should not include any new curb-cut locations.
- Stubbing of sewer and water utilities along Bunker Lake Boulevard and Puma Street should be completed at regular intervals based on future land use needs.
- Cost estimations and design should include: roadway, trails/sidewalks, storm water management, street lighting, trunk water service and trunk sewer service.
- The intersection of Bunker Lake Boulevard and Armstrong Boulevard was constructed in 2011. Verification that this intersection was constructed to accommodate traffic demands must be completed.
- The City has adopted Comprehensive Sanitary Sewer and Water Plans. These plans should be reviewed and consulted as forecasting and design work is completed.
- Regional storm water considerations and solutions should be included in the study.

## GENERAL DISCUSSION

While the primary focus of the analysis is related to providing infrastructure to serve development, additional considerations can influence decision-making. The following items were considered throughout the course of the analysis.

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## Right-of-Way Requirements

While it is anticipated that most of the improvements will be development driven, and Right of Way will be secured through the platting process, understanding and documenting the potential needs will allow the City to plan in advance for acquisitions. The City can then provide that documentation to developers as they begin to consider options within the study area.

## Phased Improvements

While there is a general understanding of the improvements required to ultimately serve the area, sequencing of the improvements will allow for planning and fiscal responsibility. Our understanding of the most logical phasing sequence includes:

- **Phase 1** – Complete construction of Bunker Lake Boulevard from Armstrong Boulevard to the westerly school property driveway. Sanitary sewer and water have already been extended to this point.
- **Phase 2** – Construction of Bunker Lake Boulevard from the Phase 1 limits to Puma Street. Construction of Puma Street from Bunker Lake Boulevard to the north. The northerly limits along Puma Street will be the approximate location of a lift station required to serve areas further to the north and west.
- **Phase 3** – Completion of improvements along Puma Street.
- **Future Phases** – We verified the improvements considered can service areas beyond Phase 3. For instance, the gravity sanitary sewer can serve areas west of Puma Street. No further work was completed beyond Phase 3 other these verifications.

Costs for each phase were developed to allow for the City to plan for the sequenced implementation of the improvements. The anticipated phasing is depicted on Figure 4 in Appendix A.

## Jurisdictional Authority/Approvals/Permits

As the project moves from the planning stages to design and construction, permits will be required from various agencies. Understanding and planning for requirements associated with obtaining permits and approvals at this time will be critical to the ultimate success of the process. The following agencies will be permitting entities for considered improvements:

- Minnesota Department of Transportation State Aid: Bunker Lake Boulevard and Puma Street are State Aid routes,
- Minnesota Pollution Control Agency: NPDES Storm Water Permit,
- Minnesota Pollution Control Agency: Sanitary Sewer Extension Permit,
- Minnesota Department of Health (MDH): Watermain Extension and Dewatering,

- 
- Anoka County: Work in Right of Way, and
  - Lower Rum River Watershed Management Organization: Storm Water.

### **Recent Improvements**

Improvements to the area were completed in 2011. The street and utility improvements completed at that time included:

- Extension of sanitary sewer along the west side of Armstrong Boulevard from just north of Sunwood Drive to Bunker Lake Boulevard,
- Extension of watermain from the east side of Armstrong Boulevard to the west side of Bunker Lake Boulevard,
- Extension of sanitary sewer and watermain in newly platted Bunker Lake Boulevard right-of-way,
- Development of a storm sewer system to service the street and right-of-way requirements,
- Extension of Bunker Lake Boulevard roadway,
- Paving of Puma Street, and
- Extension of a bituminous trail along Puma Street from Bunker Lake Boulevard to Alpine Drive.

All of the improvements completed at that time were consistent with the City's Comprehensive Plans.

## **UNDERSTANDING PREVIOUS IMPROVEMENTS**

Significant consideration and planning was completed prior to completion of the improvements constructed in 2011. Understanding the thoughts behind previous improvements can help avoid omissions when considering future improvements.

### **Street and Trail Improvements**

#### Bunker Lake Boulevard

The previous improvements provided a two-lane roadway from Armstrong Boulevard to Puma Street. The roadway was designed to allow for future widening and expansion as development and other potential transportation improvements surrounding the area increased traffic on this roadway. It was anticipated that ultimate expansion of Bunker Lake Boulevard to two lanes in each direction with center turn lane would be required for future development along Bunker Lake Boulevard.

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The Comprehensive Plan indicates that projected traffic on this roadway could be 11,000 ADT if the street is part of a future Mississippi River crossing. Without being part of a River crossing, the Comprehensive Plan provides 20-year projections of 5,000 ADT.

The roadway was located within the right-of-way to provide future flexibility in roadway expansion with minimal modifications to what will be constructed.

#### Puma Street

Improvements to Puma Street were evaluated because of the need to extend Bunker Lake Boulevard to Puma Street, a distance of about one-half mile.

Puma Street received only minimal improvements, being widened from 18 feet width to 24 feet and paved with 2 inches of bituminous over 6 inches of aggregate base. More significant upgrades to the roadway were anticipated, but not completed at that time.

The anticipated future requirements included a 32 foot wide street with curb and gutter with full base and pavement improvement.

#### Bituminous Trails

A 10-foot wide bituminous trail was constructed along the north side of the Bunker Lake Boulevard from Armstrong Boulevard to Puma Street and on the west side of Puma Street from Bunker to Alpine Drive.

#### Intersections

Bunker Lake Boulevard and Armstrong Boulevard was reconstructed to include turn lanes and signalization.

### **Sanitary Sewer Improvements**

Sanitary sewer was extended north along the west side of Armstrong Boulevard from 146th Avenue/Sunwood Drive NW to Bunker Lake Boulevard. To meet the Comprehensive Sanitary Sewer Plan and serve additional areas beyond the study area, a 21-inch sewer main was constructed. It was intended to extend the 21-inch trunk sewer further to the north to service Fire Station No. 1 and the proposed future location of the water treatment plant.

In accordance with the Comprehensive Plan, an 18-inch sanitary sewer was extended westward along Bunker Lake Boulevard. This line will provide service to the study area, as well as future development that may occur on the south side of Bunker Lake Boulevard, and could be further extended in accordance with the Comprehensive Plan.

### **Water Distribution and Supply Improvements**

Watermain was extended across Armstrong Boulevard from the main on the east side of Armstrong Boulevard. From there, the watermain was further extended westward in Bunker Lake Boulevard.

It was planned to extend this trunk watermain west to Puma Street and north to Alpine Drive via

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Puma Street to provide future looping and provide water to unserved areas and future developments. Loops within future development would be utilized to promote reliability and functional flexibility as well as promoting fresh water moving throughout the system.

It was previously determined that the elevated storage for the City of Ramsey is adequate to provide fire flow to the study area. The distribution system was also found to be adequate to move the water from the City's three towers to the study area.

***Additional Consideration:***

*The right-of-way of Bunker Lake Boulevard could be utilized for a future route of a 30-inch raw watermain from a future intake on the Mississippi River to the proposed site of the water treatment plant, just south of Fire Station No. 1. Because the exact location of the intake and raw watermain alignment had not yet been determined, no provisions were made during the previous improvements.*

**Storm Water Management**

Drainage for the study area is essentially sheet flow to the center of the site to existing wetlands, and then easterly through the wetlands and through a culvert under Armstrong Boulevard. Runoff continues to flow into the actively developing COR area.

A storm sewer system was installed in Bunker Lake Boulevard to manage the storm water runoff within the right-of-way. We anticipate that this included considerations for the future widening and extension of Bunker Lake Boulevard as well.

Extensive hydraulic modeling was refined and detail added to represent preliminary storm water management conditions for future development scenarios.

## **ADDITIONAL ANALYSIS**

As a portion of this analysis, we completed traffic and storm water studies and reviewed City Comprehensive Plans for sanitary sewer and water main needs for the study area. The traffic study is included as Appendix B and the storm water study is included as Appendix C to this report. The following is a summary of results for the various analyses and reviews.

### **Traffic Analysis**

The traffic analysis was completed to determine required lane geometry for Bunker Lake Boulevard and Puma Street, along with turn lane requirements at four intersections in the study area.

The Future Business Park development is proposed north of Trunk Highway (T.H. 10) and west of Armstrong Boulevard. Armstrong Boulevard is a critical north-south corridor for the City of Ramsey, Anoka County, and the region carrying traffic from T.H. 10 to surrounding areas. The Business Park development includes residential, business park, commercial, and institutional land uses. These land uses result in an increase of 18,500 to 23,300 trips per day into and out of the area at full build.

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The traffic increase from both the background growth and the development results in a need for capacity improvements at individual roadways and intersections in the study area. The following concise summary of improvements should be completed based on the mitigation necessary to achieve acceptable operations. For the 2040 Full-Build scenario, operations can be improved, but will still be considered unacceptable at many of the intersections. This is due to the large amount of traffic entering and exiting on Bunker Lake Boulevard and Armstrong Boulevard. Short term improvements are intended to mitigate current safety or operations problems, mid-term improvements are needed to accommodate both development and background traffic growth, and long-term improvements are needed to handle the overall development to year 2040.

Exact timing for improvements will be based upon the actual development timing and background traffic growth. Short Term improvements can be considered as the minimum requirements based on projected traffic growth. Mid Term and Long Term improvements are considerations and may ultimately be required to serve the area.

#### Short Term Improvements

- Bunker Lake Boulevard (west of Armstrong Boulevard): Expand to a four lane section for development.
  - The eastbound approach should include two 300 foot left turn lanes, two through lanes, and one right turn lane.
  - A full median should be provided to the west end of the commercial area.
  - A full access should be at least 845 feet from Armstrong Boulevard and a right in/right out access should be at least 470 feet from Armstrong Boulevard.
- Bunker Lake Boulevard (west of commercial section): Expand to a three lane section for development (two through lanes and one center left turn lane).
  - Right turn lanes (locations and lengths) will be determined based on development type.
- Puma Street: Expand to a three lane section for development (two through lanes and one center left turn lane).
  - Right turn lanes (locations and lengths) will be determined based on development type.
- Bunker Lake Boulevard & Puma Street: An all-way stop, two-way stop, or roundabout will operate adequately at this intersection for the 2040 Full-Build conditions. The roundabout option may offer better operations than the other two options at 2040 Full-Build.

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### Mid-Term Improvements

- Armstrong Boulevard & Alpine Drive: Add northbound and southbound left turn lanes. Modify eastbound and westbound lanes to include a thru/left and a right turn lane.
- Alpine Drive & Puma Street: Add a westbound left turn lane and eastbound right turn lane.
- Armstrong Boulevard & Bunker Lake Boulevard: Re-stripe southbound lanes to include a dual southbound left turn lane. A southbound double left turn lane will help reduce queues entering the COR development. Improvements were done in 2011 to this intersection and a future southbound left turn lane was designed, but not striped.

### Long-Term Improvements

- Armstrong Boulevard & Bunker Lake Boulevard: Modify the southerly eastbound through lane to a through-right lane. Another option would be to keep the two through lanes and modify the right turn lane into a free right with an add lane that runs south to T.H. 10.

### Alternative Improvements

At several locations along the corridor, opportunities exist for implementation of alternative alignments. These are depicted as options to consider based on development concepts. Alternative concepts are depicted at the intersection of Bunker Lake Boulevard and Puma Street (Figure 10) and the intersection of Puma Street and Alpine Drive (Figure 12). The alternatives are provided for future discussion and will not significantly impact project costs.

Figures 5 through 12 in Appendix A depict future roadway and intersection improvements in the study area.

### **Sanitary Sewer and Water Main**

An 18-inch sanitary sewer main was extended west along Bunker Lake Boulevard as a portion of the 2011 improvements. Based on information contained in the Comprehensive Plan, The 18-inch line will be extended along Bunker Lake Boulevard and north along Puma Street. North of the Puma Street/Bunker Lake Boulevard intersection, grade becomes an issue and a sanitary sewer lift station is required to serve areas further north and west. North of the lift station, the Comprehensive plan indicates a 12-inch gravity main will be adequate to convey sanitary sewer flows from the area.

A 16-inch water main was extended west along Bunker Lake Boulevard as a portion of the 2011 improvements. The Comprehensive plan indicated that either a 12-inch or a 16-inch water main would be required along Bunker Lake Boulevard and Puma Street, depending on the final selected location of a future water treatment plant to be constructed in this area. For our analysis, we assumed a 16-inch water main would be constructed.

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Figures 13 through 15 in Appendix A depict sanitary sewer and water main improvements.

### **Storm Water Management**

Storm water management concepts were developed to maintain existing drainage patterns and preserve the conveyance and flood storage capacity of the primary wetland corridor that bisects the area. This will restrict development along the wetland corridor and retain the pre-development flood capacity, thereby maintaining existing flow rates into the COR.

The study area can be segmented into three drainage districts, generally delineated by future land use. Figure 16 in Appendix A displays the drainage area breakdown and a general regional pond layout. The drainage area consists of a multi-use site (Area 1), residential area (Area 2), and commercial and industrial sites (Area 3). The watersheds were modeled under future land use conditions to generally size retention ponds to meet existing flow rates. The ponds were also located with respect to potential storm sewer depths, reductions in wetland impacts, maximization of developable area and potential aesthetic function.

The ponds depicted on Figure 16 in Appendix A depict areas that are best suited for regional rate control basins only. Additional design parameters and regional storm water management planning should be further refined as the areas begin to develop.

Cost estimates were not developed for the regional pond construction or internal site storm sewer conveyance. It is anticipated that costs associated with pond construction will be completed by the developer as a portion of the site grading. Also, it is assumed that internal site drainage will be accommodated by dry swales and ditch systems to reduce storm sewer costs.

### **Street Lighting**

Street lighting was included in the project costs to the level provided along Riverdale Drive, east of Armstrong Boulevard. This includes both street level lighting and pedestrian lighting along the corridors.

### **Trails/Sidewalks**

Existing trails were previously constructed along the north side of Bunker Lake Boulevard and the west side of Puma Street. Project costs were prepared which include trails along the south side of Bunker Lake Boulevard and the east side of Puma Street.

### **Phase Transitions**

There is the potential for portions of the roadways to be constructed to wider sections than currently exist. If this occurs, the new roadways will need to taper to meet the existing roadway widths. The tapered sections will then be removed as the next phase of improvements is completed. Costs were included in each phase for these tapering sections.

### **Right Turn Lane Additions**

The roadways depicted in the exhibits depict through lanes and left turn lanes. Right turn lanes will be required at each access off of Bunker Lake Boulevard and Puma Street. Individual access locations were not considered in this study, and so the locations and lengths of right turn lanes were not considered at

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this time. The actual access locations will be dependent upon the type of land use and internal site characteristics. We included allowances for the right turn lanes in each phase for budgeting purposes.

### **Landscaping**

The base project includes a very utilitarian approach to the area. Bituminous trails and lighting were included, but other features, such as trees, shrubs, decorative features and monuments are not included in the estimated project costs.

### **Right of Way and Easements**

Locations of right of way needs are depicted on the exhibits based on roadway and trail needs. The exact areas will need to be determined during the platting process. We assumed that the rights of way would be dedicated as a portion of the development process and no costs are included in the project costs for acquisitions.

## **COST CONSIDERATIONS**

Costs were developed based upon phased implementation. The phasing is as depicted on Figure 4 in Appendix A.

<b><u>Improvement Type</u></b>	<b><u>Phase 1</u></b>	<b><u>Phase 2</u></b>	<b><u>Phase 3</u></b>
Roadway	\$ 1,153,000	\$ 920,000	\$ 1,160,000
Trails/Sidewalks	\$ 172,000	\$ 126,000	\$ 151,000
Storm Water Management	\$ 0	\$ 0	\$ 64,000
Street Lighting	\$ 154,000	\$ 112,000	\$ 136,000
Trunk Water	\$ 7,000	\$ 272,000	\$ 283,000
Trunk Sanitary Sewer	\$ 0	\$ 185,000	\$ 620,000
Phase Transition	\$ 120,000	\$ 120,000	\$ 80,000
Right Turn Lane Additions	<u>\$ 200,000</u>	<u>\$ 200,000</u>	<u>\$ 200,000</u>
Improvements Costs/Phase	\$ 1,806,000	\$ 1,935,000	\$ 2,694,000

The above costs are considered project costs and include 30% contingencies and project development costs.

The Phase Transition amounts are costs associated with tapering pavements sections to match in-place sections where required.

Right turn lanes are not depicted on the graphics, but will be required. The actual right turn lane locations and lengths will be determined by the entrance locations, land use and associated traffic impacts. Costs included for right turn lanes in the estimate are considered average amounts based on anticipated accesses for the area.

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Street lighting costs are based upon recent installations within the City of Ramsey.

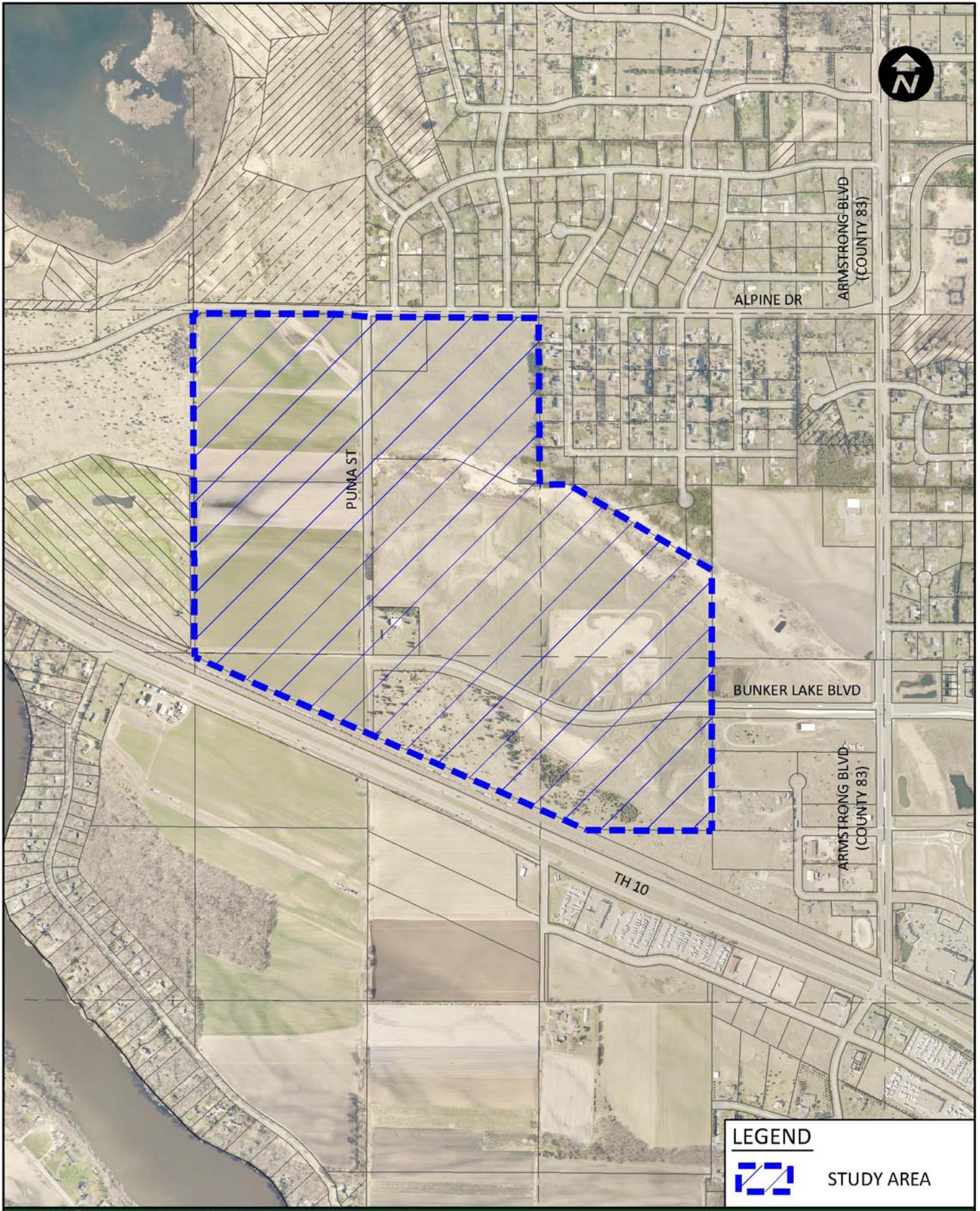
For storm water management, costs were included for manholes, catch basins, and pipe within the street sections. No costs for ponding were included for Phases 1, 2 and 4. For Phase 3, ponding costs were included for excavation related activities. We assumed a ponding area would be acquired through the platting process, and ponding within a development site would be expanded to include volume for roadway drainage.

The Phase 2 limits are based upon the area that can be served by a gravity sanitary sewer system. The Phase 3 sanitary sewer costs include a lift station and forcemain.


## **PROJECT FINANCING**

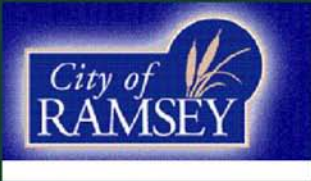


## APPENDIX A - FIGURES



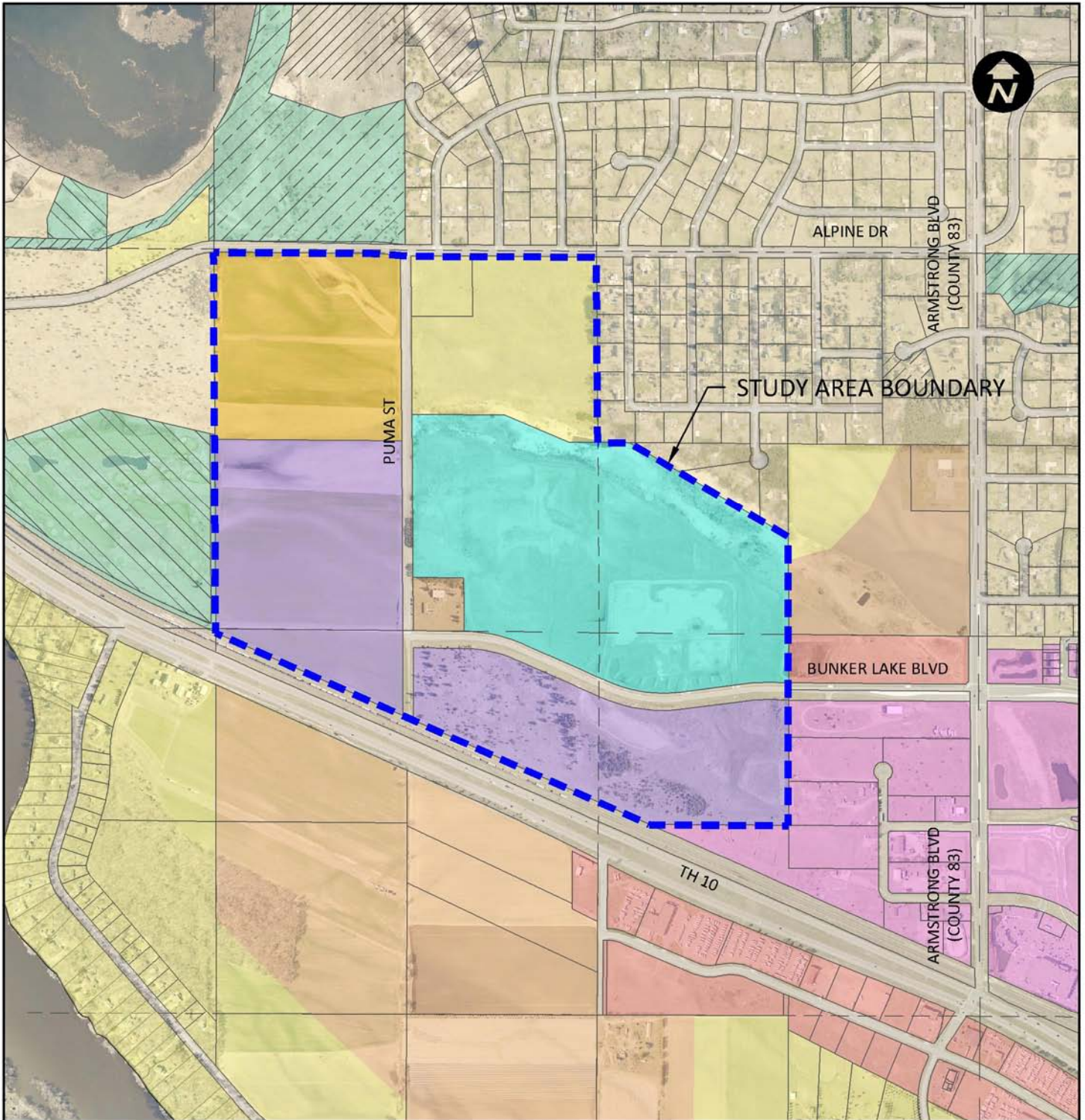
**LEGEND**

 STUDY AREA





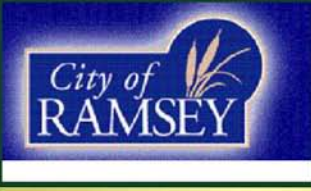
# FUTURE BUSINESS PARK CITY OF RAMSEY, MINNESOTA

FIGURE 1 - STUDY AREA  
Date, 2015



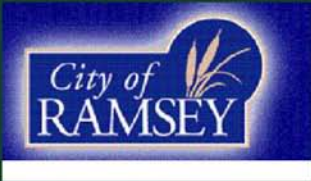
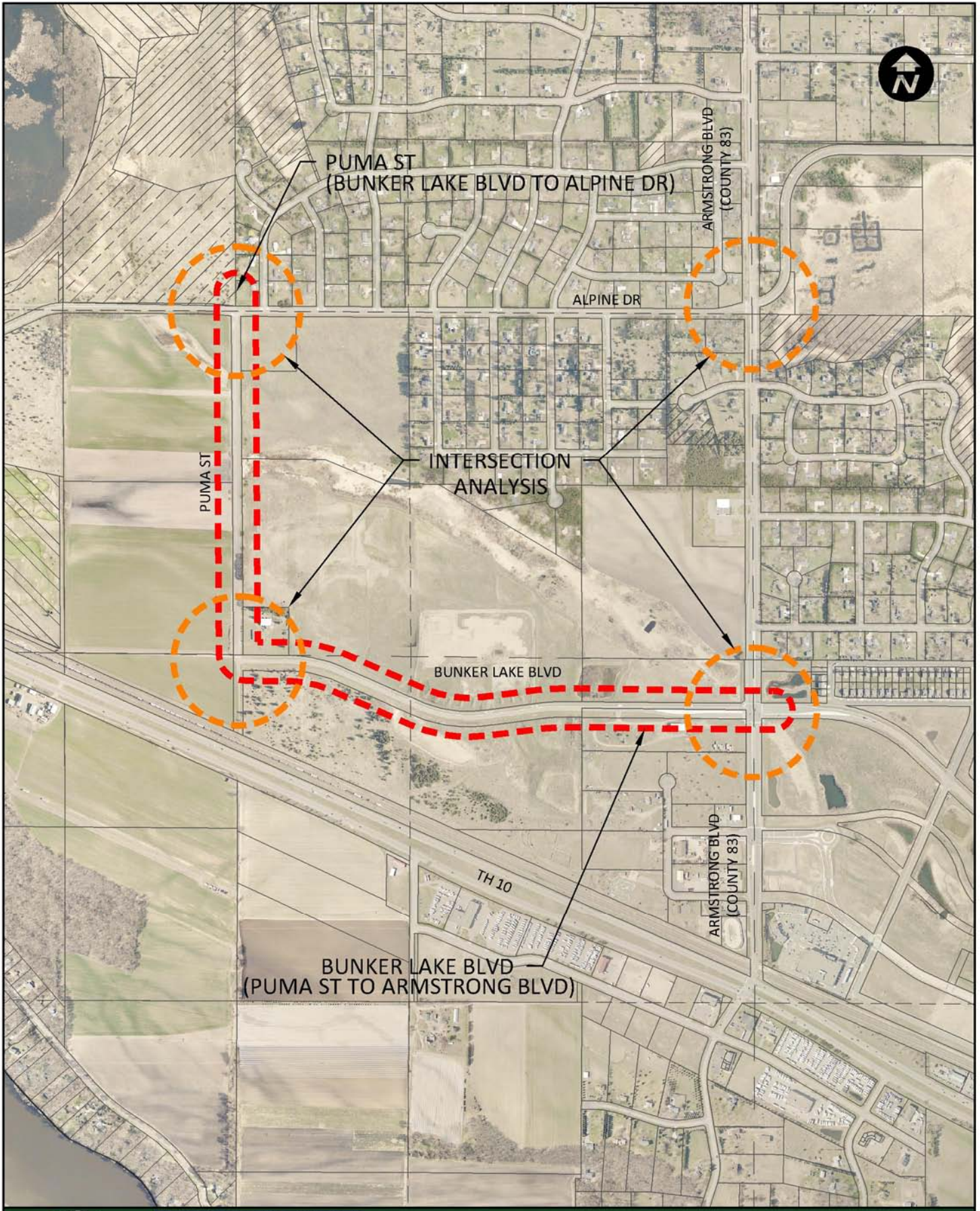
**LEGEND**

 LOW DENSITY RESIDENTIAL	 OFFICE PARK	 BUSINESS PARK	 RURAL PRESERVE
 MEDIUM DENSITY RESIDENTIAL	 COMMERCIAL	 PUBLIC	 PARK
 HIGH DENSITY RESIDENTIAL	 MU	 RURAL DEVELOPING	



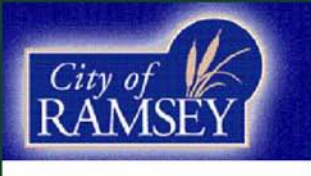
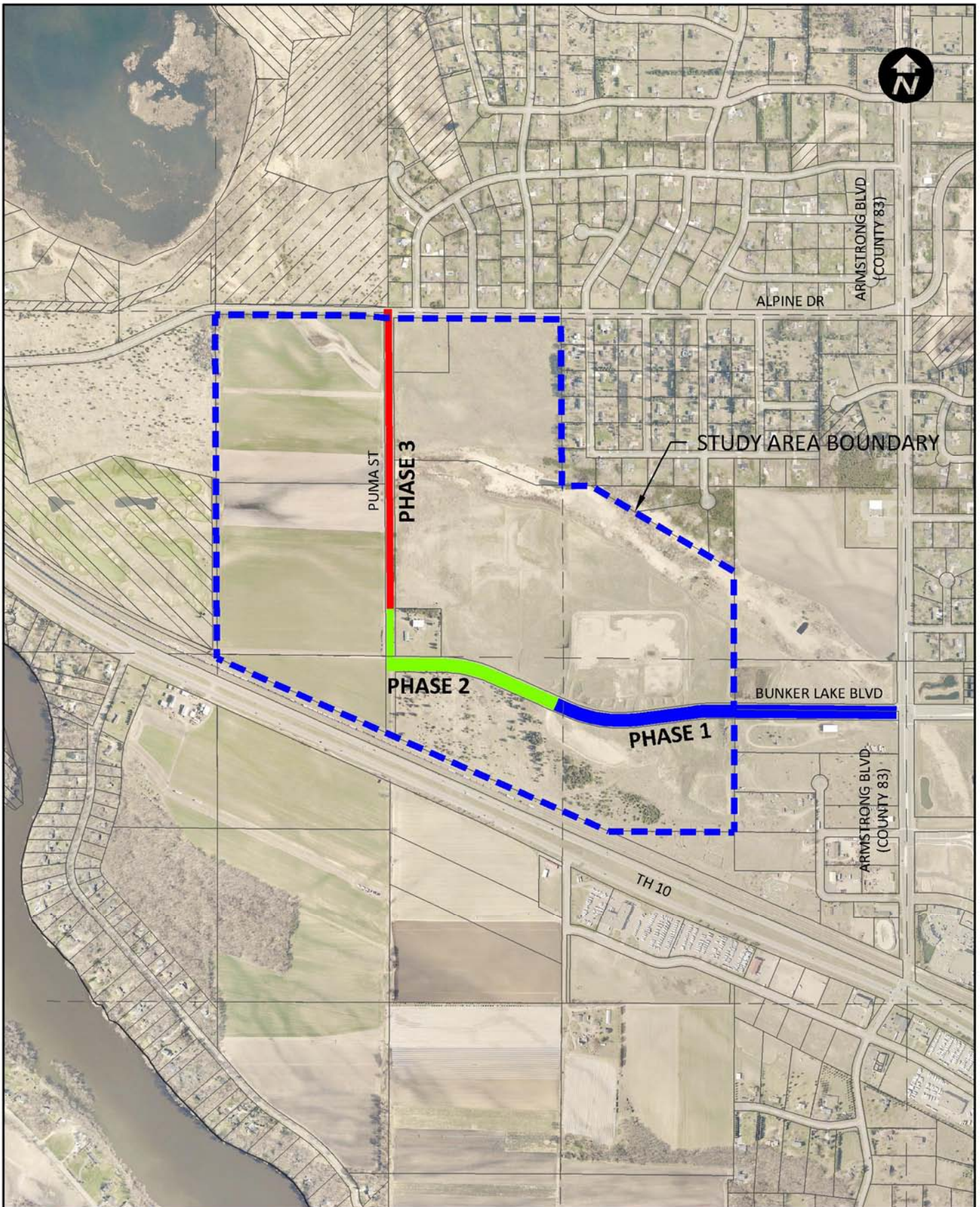
**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 2 - LAND USE  
Date, 2015



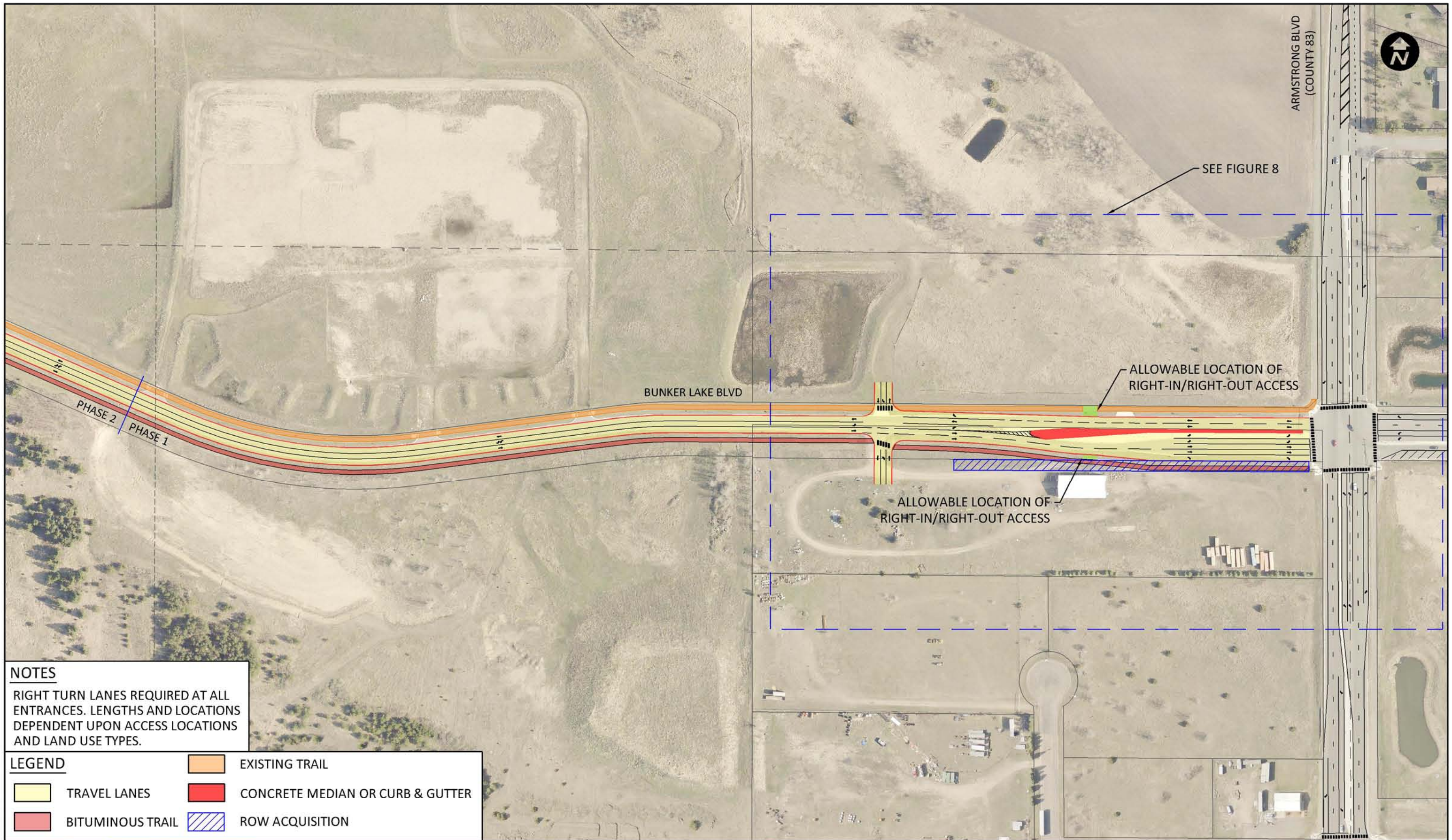
# FUTURE BUSINESS PARK CITY OF RAMSEY, MINNESOTA

FIGURE 3 - INFRASTRUCTURE ANALYSIS  
Date, 2015



# FUTURE BUSINESS PARK CITY OF RAMSEY, MINNESOTA






FIGURE 4 - PHASING  
Date, 2015



**NOTES**

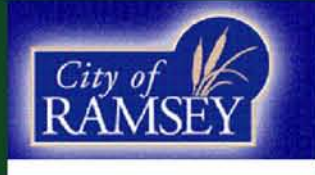
RIGHT TURN LANES REQUIRED AT ALL ENTRANCES. LENGTHS AND LOCATIONS DEPENDENT UPON ACCESS LOCATIONS AND LAND USE TYPES.

**LEGEND**

	EXISTING TRAIL
	TRAVEL LANES
	CONCRETE MEDIAN OR CURB & GUTTER
	BITUMINOUS TRAIL
	ROW ACQUISITION

**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 5 - GEOMETRICS  
Date, 2015





PUMA ST

PHASE 3  
PHASE 2

SEE FIGURES 9 & 10

BUNKER LAKE BLVD

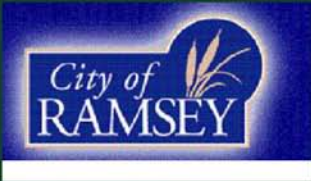
PHASE 2  
PHASE 1

**NOTES**

RIGHT TURN LANES REQUIRED AT ALL ENTRANCES. LENGTHS AND LOCATIONS DEPENDENT UPON ACCESS LOCATIONS AND LAND USE TYPES.

**LEGEND**

- TRAVEL LANES
- EXISTING TRAIL
- CONCRETE MEDIAN OR CURB & GUTTER
- BITUMINOUS TRAIL
- ROW ACQUISITION



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 6 - GEOMETRICS  
Date, 2015



ALPINE DR

SEE FIGURES 11 & 12

PUMA ST

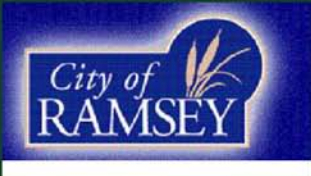
PHASE 3

**NOTES**

RIGHT TURN LANES REQUIRED AT ALL ENTRANCES. LENGTHS AND LOCATIONS DEPENDENT UPON ACCESS LOCATIONS AND LAND USE TYPES.

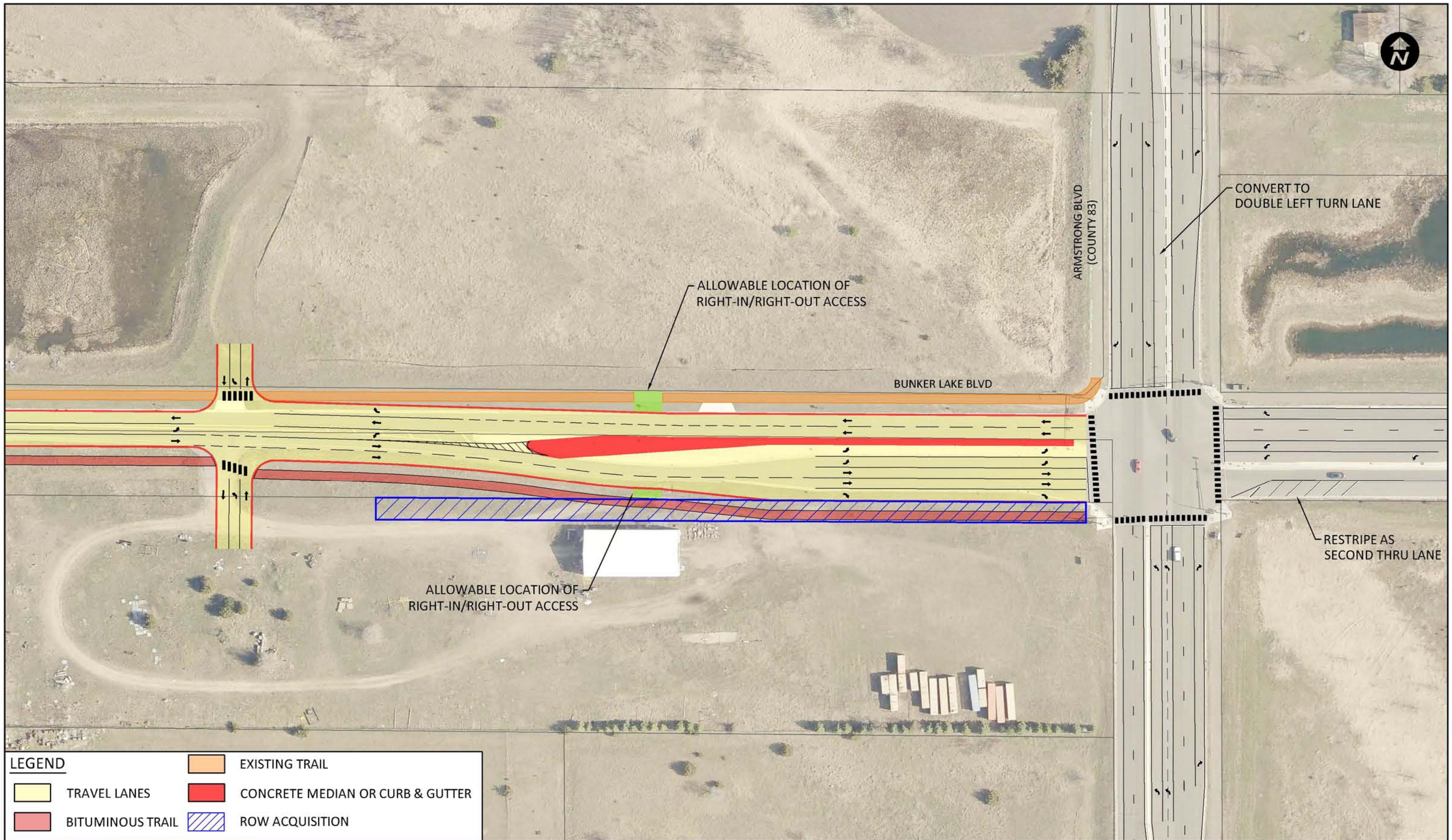
**LEGEND**

- TRAVEL LANES
- CONCRETE MEDIAN OR CURB & GUTTER
- EXISTING TRAIL
- ROW ACQUISITION
- BITUMINOUS TRAIL



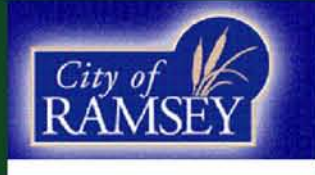
**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

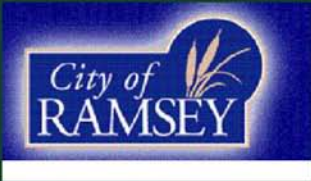
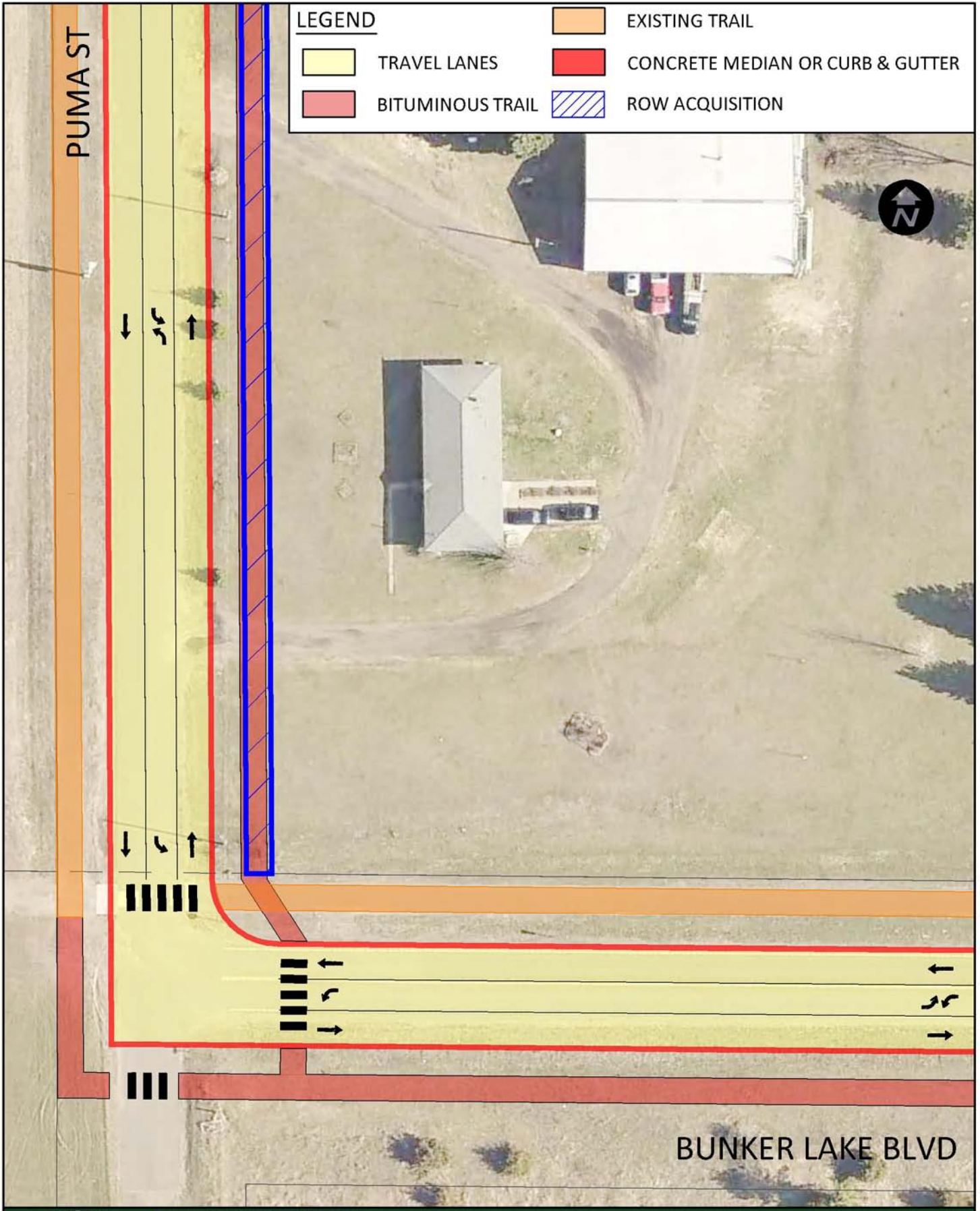
FIGURE 7 - GEOMETRICS  
Date, 2015



FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA

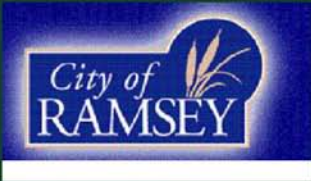
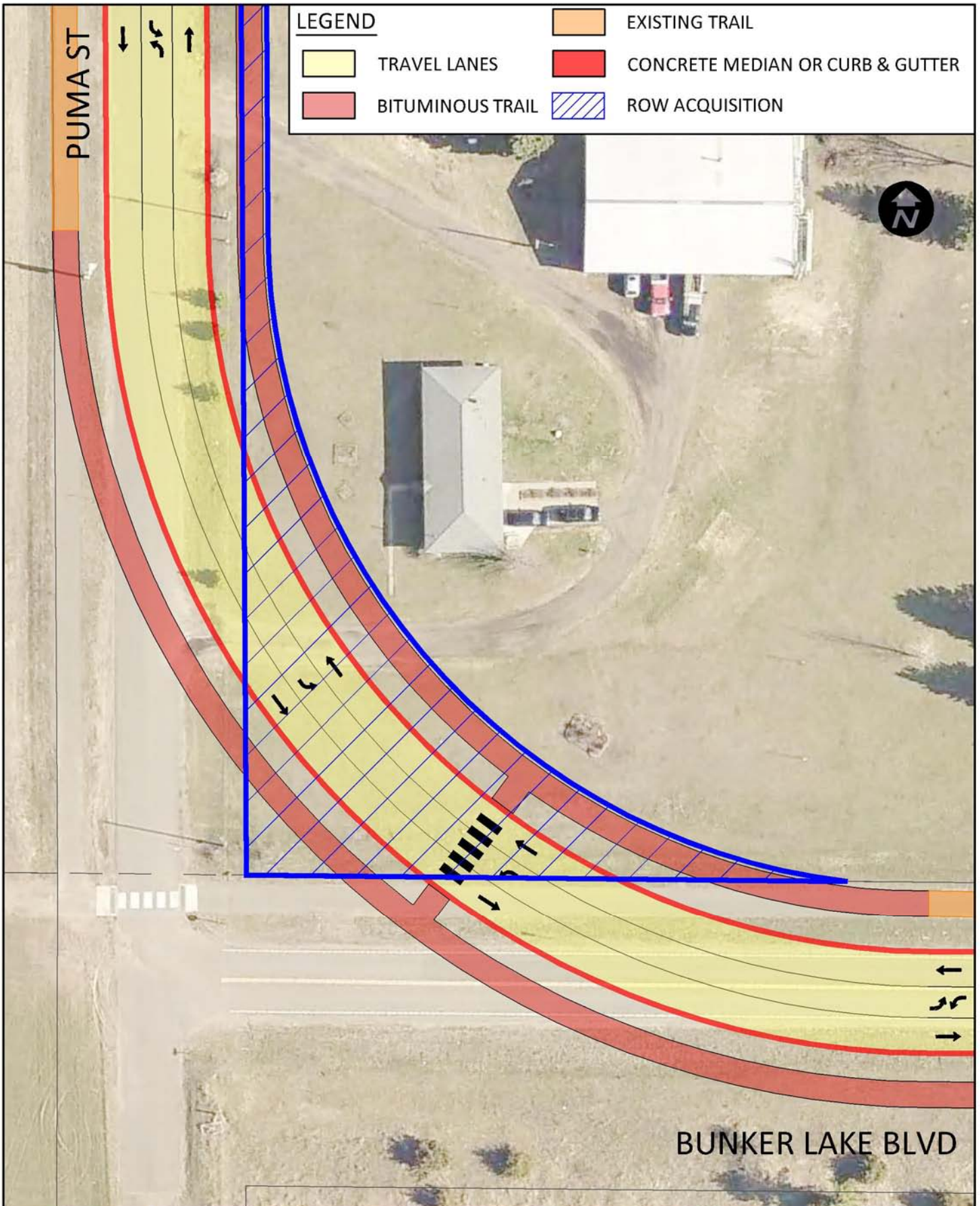
FIGURE 8 - BUNKER LAKE BOULEVARD AT ARMSTRONG BOULEVARD  
Date, 2015





**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

**FIGURE 9 - BUNKER LAKE BOULEVARD & PUMA STREET**  
Date, 2015



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**  
 FIGURE 10 - BUNKER LAKE BOULEVARD & PUMA STREET  
 ALTERNATIVE CONCEPT

Date, 2015

**LEGEND**



TRAVEL LANES



BITUMINOUS TRAIL



EXISTING TRAIL



CONCRETE MEDIAN OR CURB & GUTTER

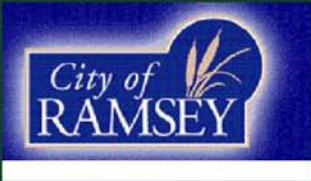
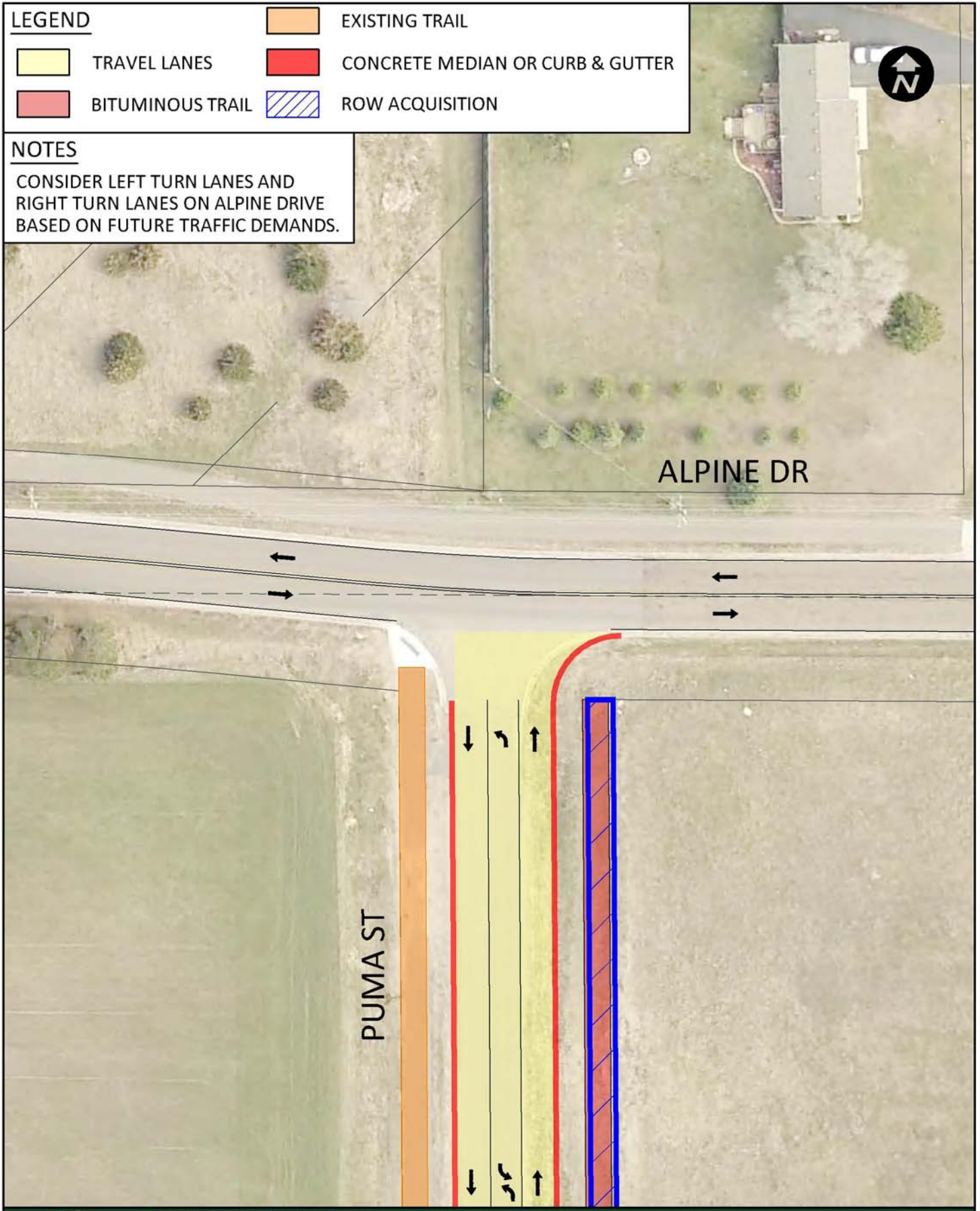


ROW ACQUISITION



**NOTES**

CONSIDER LEFT TURN LANES AND RIGHT TURN LANES ON ALPINE DRIVE BASED ON FUTURE TRAFFIC DEMANDS.



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

**FIGURE 11 - PUMA STREET & ALPINE DRIVE**

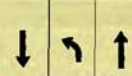
Date, 2015

ALPINE DR



PUMA ST

POTENTIAL ROW VACATION



**NOTES**

CONSIDER LEFT TURN LANES AND RIGHT TURN LANES ON ALPINE DRIVE BASED ON FUTURE TRAFFIC DEMANDS.

**LEGEND**



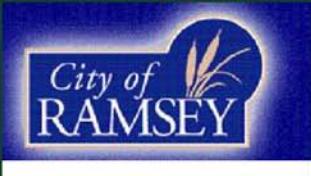
TRAVEL LANES



CONCRETE MEDIAN OR CURB & GUTTER

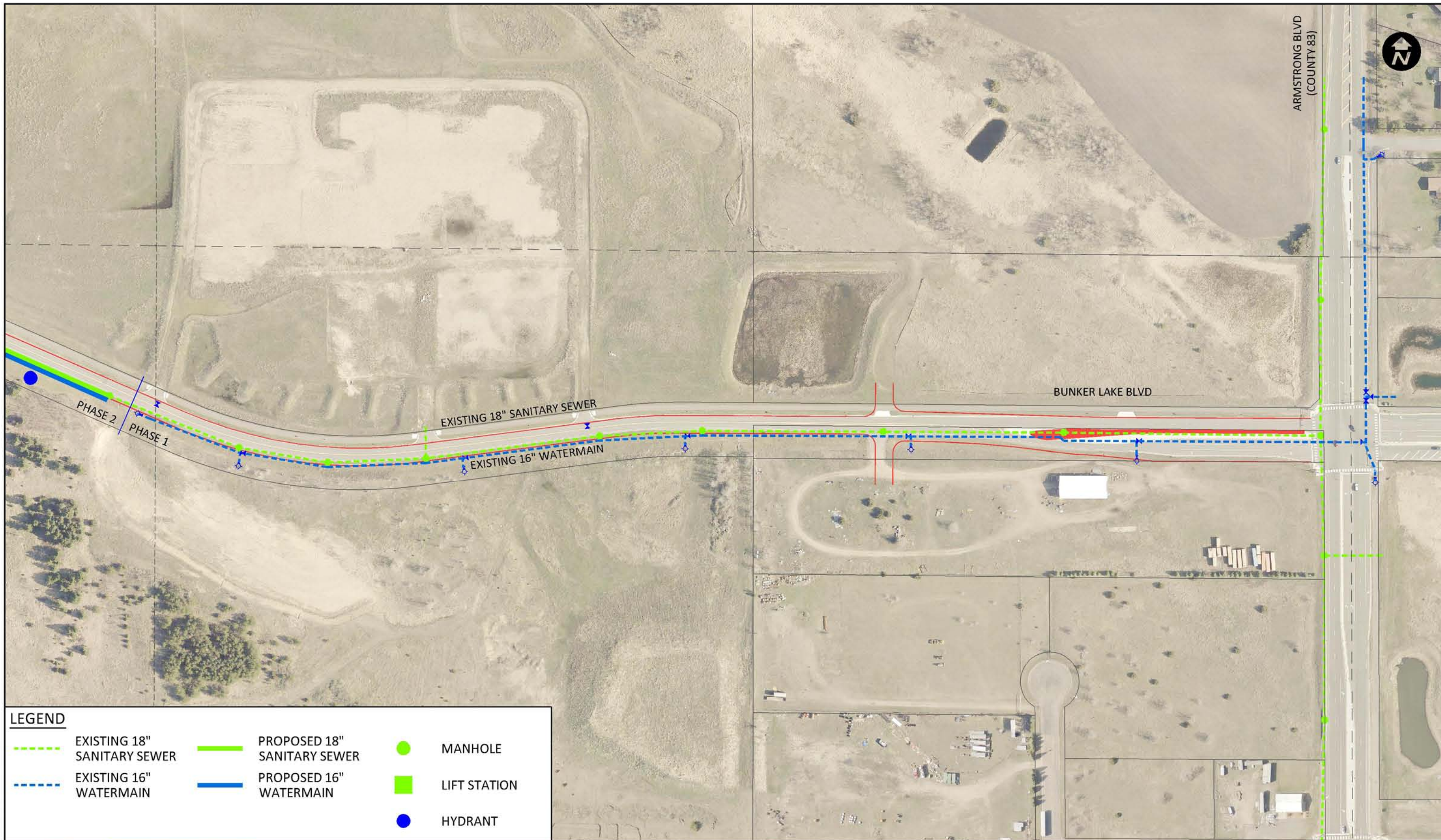


BITUMINOUS TRAIL

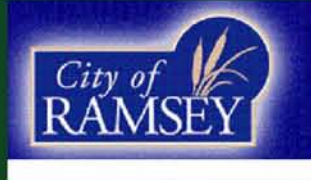


**FUTURE BUSINESS PARK**  
**CITY OF RAMSEY, MINNESOTA**  
FIGURE 12 - PUMA STREET & ALPINE DRIVE  
ALTERNATIVE CONCEPT

Date, 2015



LEGEND					
	EXISTING 18" SANITARY SEWER		PROPOSED 18" SANITARY SEWER		MANHOLE
	EXISTING 16" WATERMAIN		PROPOSED 16" WATERMAIN		LIFT STATION
					HYDRANT



FUTURE BUSINESS PARK  
 CITY OF RAMSEY, MINNESOTA  
 FIGURE 13 - SANITARY SEWER AND WATER  
 Date, 2015



PUMA ST

PROPOSED SANITARY SEWER LIFT STATION

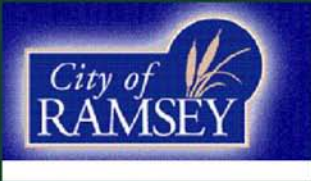
PHASE 3  
PHASE 2

BUNKER LAKE BLVD

PHASE 2  
PHASE 1

**LEGEND**

- |                                                                                     |                             |                                                                                     |                             |                                                                                      |              |
|-------------------------------------------------------------------------------------|-----------------------------|-------------------------------------------------------------------------------------|-----------------------------|--------------------------------------------------------------------------------------|--------------|
|  | EXISTING 18" SANITARY SEWER |  | PROPOSED 18" SANITARY SEWER |  | MANHOLE      |
|  | EXISTING 16" WATERMAIN      |  | PROPOSED 16" WATERMAIN      |  | LIFT STATION |
|                                                                                     |                             |                                                                                     |                             |  | HYDRANT      |



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 14 - SANITARY SEWER AND WATER  
Date, 2015



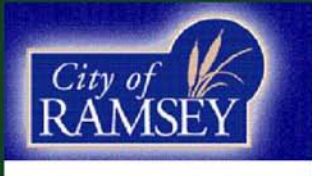
ALPINE DR

PUMA ST

PHASE 3

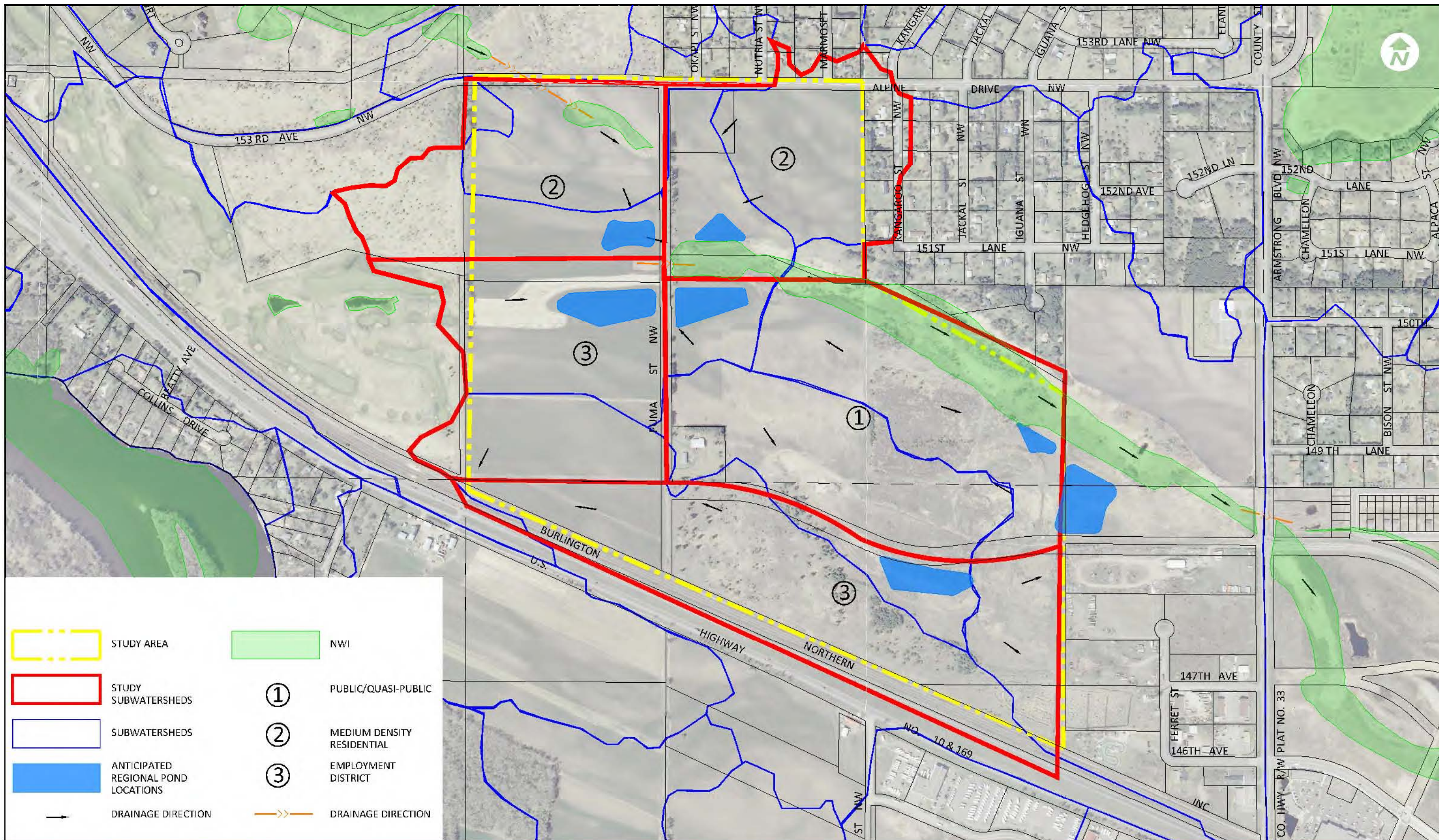
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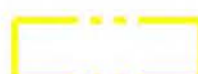
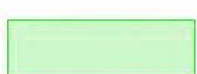








- |                                                                                     |                             |                                                                                     |                             |                                                                                      |              |
|-------------------------------------------------------------------------------------|-----------------------------|-------------------------------------------------------------------------------------|-----------------------------|--------------------------------------------------------------------------------------|--------------|
|  | EXISTING 18" SANITARY SEWER |  | PROPOSED 18" SANITARY SEWER |  | MANHOLE      |
|  | EXISTING 16" WATERMAIN      |  | PROPOSED 16" WATERMAIN      |  | LIFT STATION |
|                                                                                     |                             |                                                                                     |                             |  | HYDRANT      |

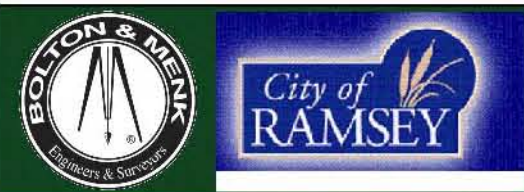


**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 15 - SANITARY SEWER AND WATER  
Date, 2015



- |                                                                                     |                                     |                                                                                     |                            |
|-------------------------------------------------------------------------------------|-------------------------------------|-------------------------------------------------------------------------------------|----------------------------|
|  | STUDY AREA                          |  | NWI                        |
|  | STUDY SUBWATERSHEDS                 |  | PUBLIC/QUASI-PUBLIC        |
|  | SUBWATERSHEDS                       |  | MEDIUM DENSITY RESIDENTIAL |
|  | ANTICIPATED REGIONAL POND LOCATIONS |  | EMPLOYMENT DISTRICT        |
|  | DRAINAGE DIRECTION                  |  | DRAINAGE DIRECTION         |



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**

FIGURE 16 - Regional Stormwater Considerations  
June, 2015



## APPENDIX B - TRAFFIC STUDY

---

Traffic Impact Study for

# Future Business Park

City of Ramsey, MN

June 24, 2015

Project Number R16.109828

---

**DRAFT**

**Submitted by:**

Bolton & Menk, Inc.  
12224 Nicollet Avenue  
Burnsville, MN 55337  
P: 952-890-0509  
F: 952-890-8065



**CERTIFICATION**

**DRAFT**

Traffic Impact Study

for

Future Business Park

Ramsey, MN

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision, and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

By: \_\_\_\_\_

Bryan T. Nemeth, P.E., PTOE

License No. 43354

Date: 6/25/2015



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## EXECUTIVE SUMMARY

The Future Business Park development is proposed north of T.H. 10 and west of Armstrong Boulevard. Armstrong Boulevard is a critical north-south corridor for the City of Ramsey, Anoka County, and the region carrying traffic from Trunk Highway (T.H.) 10 to surrounding areas. The Business Park development includes residential, business park, commercial, and institutional land uses. These land uses result in an increase of approximately 18,500 to 23,300 trips per day into and out of the area at full build.

The traffic increase from both the background growth and the development results in a need for capacity improvements at individual intersections in the study area. The following concise summary of improvements should be completed based on the mitigation necessary to achieve acceptable operations. For the 2040 Full-Build scenario, operations can be improved, but are still considered unacceptable at many of the intersections. This is due to the large amount of traffic entering and exiting on Bunker Lake Boulevard and Armstrong Boulevard. Short term improvements are intended to mitigate current safety or operations problems, mid-term improvements are needed to accommodate both development and background traffic growth, and long-term improvements are needed to handle the overall development out to 2040. Exact timing for improvements should be based upon the actual development timing and background traffic growth.

### A. Short Term

- Bunker Lake Boulevard (west of Armstrong Blvd): Expand to a four lane section for development.
  - The eastbound approach should include two 300 ft left turn lanes, two through lanes, and one right turn lane.
  - A full median should be provided to the west end of the commercial area.
  - A full access should be at least 845 ft from Armstrong Boulevard and a right in/right out access should be at least 470 ft from Armstrong Boulevard.
- Bunker Lake Boulevard (west of commercial section): Expand to a three lane section for development (two through lanes and one center left turn lane).
  - Right turn lanes (locations and lengths) are to be dictated by development type.
- Puma Street: Expand to a three lane section for development (two through lanes and one center left turn lane).
  - Right turn lanes (locations and lengths) are to be dictated by development type.
- Bunker Lake Boulevard & Puma Street: An all-way stop, two-way stop, or roundabout will operate adequately at this intersection for the 2040 Full-Build conditions. The roundabout option may offer better operations than the other two options at 2040 Full-Build.

### B. Mid-Term

- Armstrong Boulevard & Alpine Drive: Add northbound and southbound left turn lanes. Modify eastbound and westbound lanes to include a thru/left and a right turn lane.
- Alpine Drive & Puma Street: Add a westbound left turn lane and eastbound right turn lane.
- Armstrong Boulevard & Bunker Lake Boulevard: Re-stripe southbound lanes to include

---

a dual southbound left turn lane. A southbound double left turn lane will help reduce queues entering the COR development. Improvements were done in 2011 to this intersection and a future southbound left turn lane was designed, but not striped.

### C. Long-Term

- Armstrong Boulevard & Bunker Lake Boulevard: Modify the southerly eastbound through lane to a through-right lane. Another option would be to keep the two through lanes and modify the right turn lane into a free right with an add lane that runs south to T.H. 10.

## I. INTRODUCTION

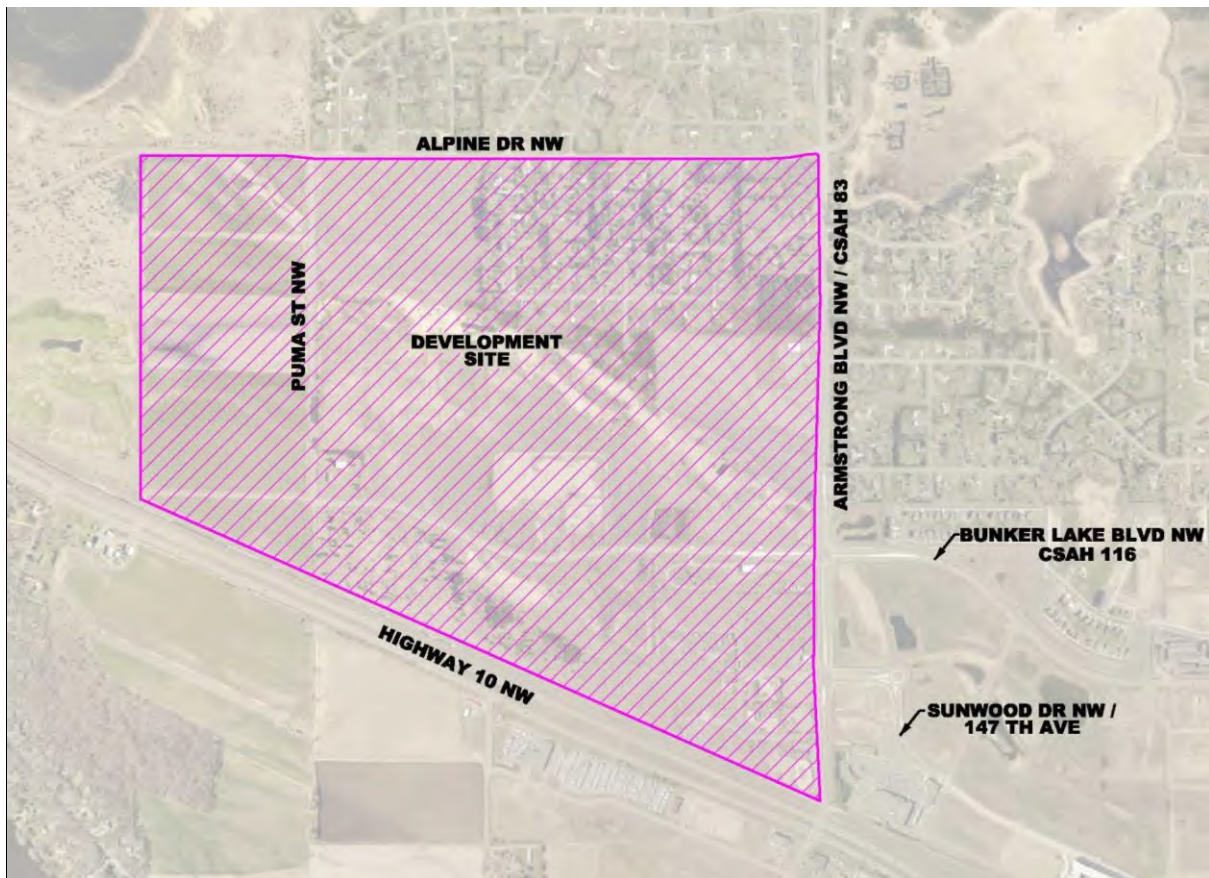
The proposed Future Business Park development is located on the southwest side of the City of Ramsey, north of T.H. 10 and just west of Armstrong Boulevard (CSAH 83). The future development is anticipated to impact the traffic on the public roadway system and surrounding area. Consequently, the traffic control and roadway geometry in the area may need to be modified to accommodate the increased traffic to maintain safety and adequate operations. This study analyzes the anticipated impacts of the Future Business Park development on the surrounding area.

## II. BACKGROUND

### A. Proposed Development

The proposed Future Business Park development is located just west of Armstrong Boulevard, between T.H. 10 and Alpine Drive. The development will have two main access points: the north entrance at Alpine Drive and Puma Street, and the east entrance at Bunker Lake Boulevard and Armstrong Boulevard. **Figure 1** below outlines the proposed development site. The development is anticipated to include residential, commercial, business park, and institutional land uses.

**Figure 1: Proposed Future Business Park Development Location**



---

### III. MEASURES OF EFFECTIVENESS

The traffic operations analysis for the intersections consider the following measures to determine the adequacy of the intersection design to meet acceptable operations: intersection delay/Level of Service (LOS) and volume-to-capacity ratios. An explanation of each of these measures is provided below:

#### A. Level of Service and Delay

The operational analysis results are described as a Level of Service (LOS) ranging from A to F. These letters serve to describe a range of operating conditions for different types of facilities. Levels of Service are calculated based on the 2010 Highway Capacity Manual, which defines the level of service, based on control delay. Control delay is the delay experienced by vehicles slowing down as they are approaching the intersection, the wait time at the intersection, and the time for the vehicle to speed up through the intersection and enter into the traffic stream. The average intersection control delay is a volume weighted average of delay experienced by all motorists entering the intersection on all intersection approaches. The control delay is modeled within the analysis software, Trafficware Synchro/SimTraffic. Level of Service D is commonly taken as an acceptable design year LOS. The level of service and its associated intersection delay for a signalized and unsignalized intersection is presented below. The delay threshold for unsignalized intersections is lower for each LOS compared to signalized intersections, which accounts for the fact that people expect a higher level of service when at a stop-controlled intersection.

**Table 1: Level of Service Criteria**

	<b>Signalized Intersection</b>	<b>Unsignalized Intersection</b>
<b>LOS</b>	<b>Control Delay per Vehicle (sec.)</b>	<b>Control Delay per Vehicle (sec.)</b>
A	≤ 10	≤ 10
B	>10 and ≤ 20	>10 and ≤ 15
C	>20 and ≤ 35	>15 and ≤ 25
D	>35 and ≤ 55	>25 and ≤ 35
E	>55 and ≤ 80	>35 and ≤ 50
F	>80	>50

---

## IV. EXISTING CONDITIONS

There are a total of four signalized intersections and three unsignalized intersections that will be evaluated in this study. The interchange on T.H. 10 & Armstrong Boulevard is currently being built, so the analysis will include a scenario with and without the new overpass. Based on MnDOT's Traffic Data Mapping Application, Armstrong Boulevard carries a range of 4,500 vehicles per day near the north end of the study area and 6,200 vehicles per day near the south end of the study area. Armstrong Boulevard will be a 4 lane divided roadway at the south end of the study area converting to a 2 lane undivided roadway at the north end of the study area. The posted speed limit is 55 mph through the study area along Armstrong Boulevard.

### A. Data Collection

#### 1. Traffic Counts

Traffic data was collected in April 2015. This includes 13 hour traffic volumes at the following intersections.

- Armstrong Boulevard & 147<sup>th</sup> Ave - Signalized
- Armstrong Boulevard & Bunker Lake Boulevard – Signalized
- Armstrong Boulevard & Alpine Drive - Unsignalized
- Alpine Drive & Puma Street – Unsignalized

Traffic data at T.H. 10 & Armstrong Boulevard was pulled from a previous study conducted in 2013.

Figures 2.1 and 2.2 show the existing intersection traffic counts.

Figure 2.1: Existing Traffic Volumes

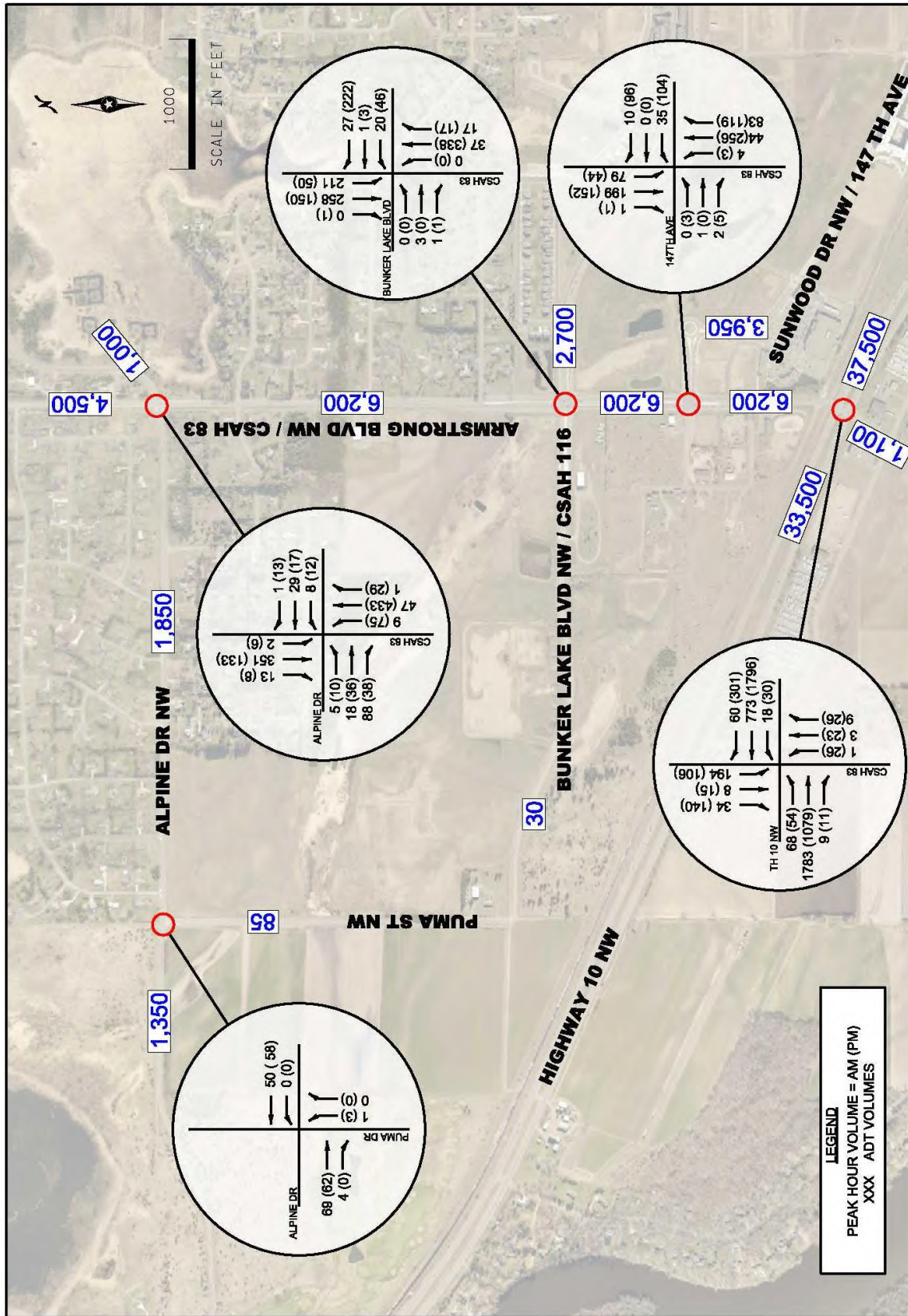
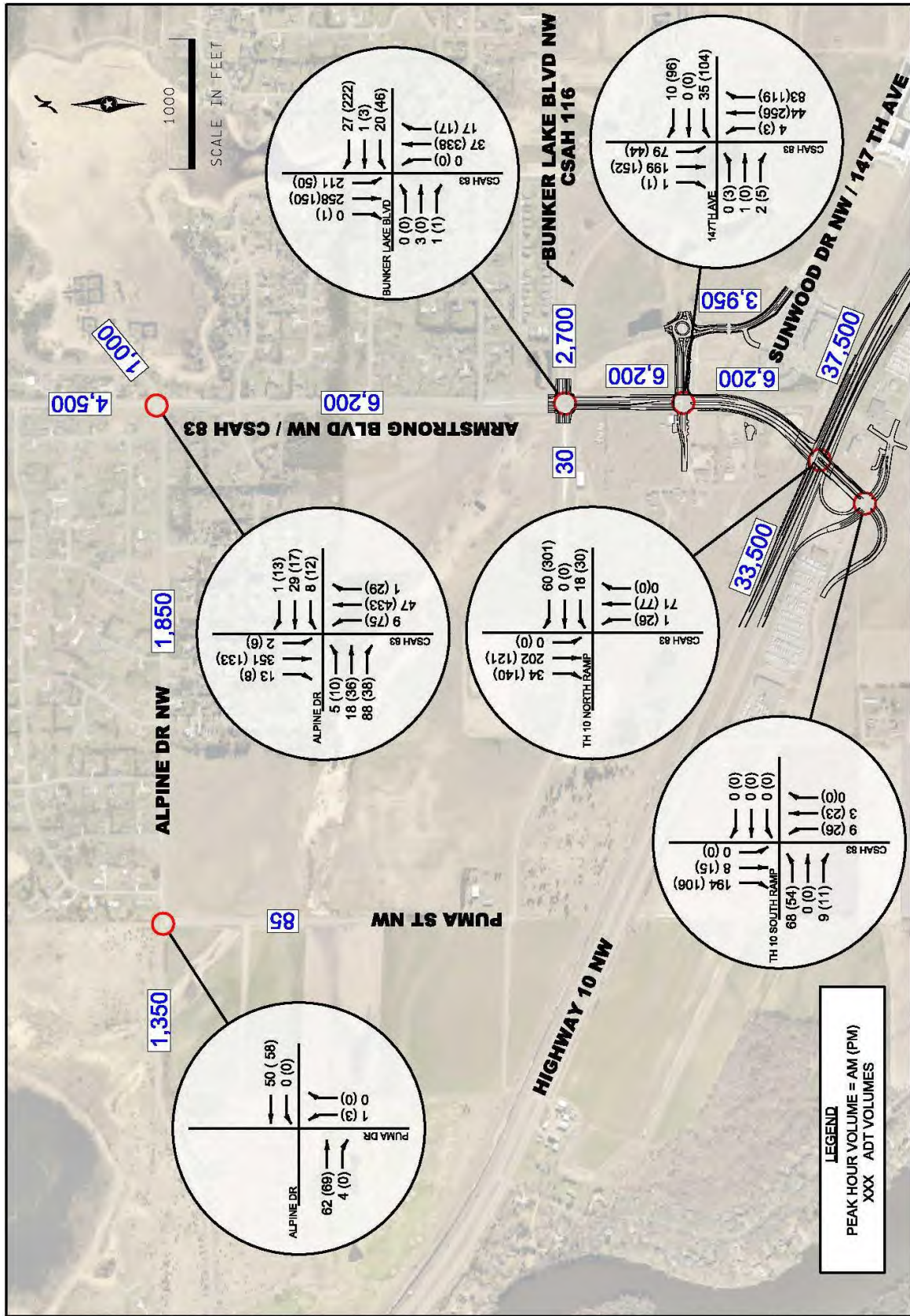


Figure 2.2: Existing Traffic Volumes (with Overpass)



## B. Operations

The existing operations analysis was completed for the five intersections within the study area and is shown in **Table 2**. The new interchange between T.H. 10 and Armstrong Boulevard was analyzed at the two exit ramps and is shown in **Table 3**.

**Table 2: 2015 Existing Traffic Operations Analysis**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>Existing 2015</b>									
TH 10/169 & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	46	D	107	F	EBL	EBT	644	1048
	PM	40	D	88	F	WBL	WBT	496	524
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	9	A	35	C	NBL	SBL	53	101
	PM	9	A	33	C	EBL	NBT	39	73
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd)) <i>Signal</i>	AM	10	A	33	C	EBT	SBL	83	143
	PM	9	A	21	C	WBL	NBT	52	87
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	2	A	9	A	EBT	EBR	32	52
	PM	4	A	13	B	WBL	EBL/T	27	54
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	1	A	4	A	NBL	NBL/R	2	14
	PM	1	A	5	A	NBL	NBL/R	3	18

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

**Table 3: 2015 Existing Traffic Operations Analysis (with Overpass)**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>Existing 2015 (With Overpass)</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	5	A	14	B	NBL	EBL	33	59
	PM	5	A	12	B	EBL	EBL	23	53
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	9	A	20	B	WBL	SBT	50	96
	PM	8	A	20	B	WBL	SBT	41	79
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	7	A	27	C	NBL	SBL	43	90
	PM	9	A	31	C	EBL	SBT	30	65
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd)) <i>Signal</i>	AM	11	B	31	C	WBL	SBL	86	153
	PM	9	A	20	B	SBL	NBT	59	105
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	2	A	9	A	WBL	EBR	34	57
	PM	4	A	12	B	EBL	EBL/T	30	59
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	1	A	6	A	NBL	NBL/R	1	8
	PM	1	A	4	A	NBL	NBL/R	3	17

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

Overall, all intersections within the study area operate within acceptable service levels with a LOS of C or better. The T.H 10 & Armstrong Boulevard interchange will alleviate the delays that were caused at the previous signalized intersection. Currently, there is no need for operational mitigation.

---

## C. Safety

### 1. Safety/Crash Analysis

Beyond the operational analysis, the crash history of the studied intersections was completed using the crash data pulled from Minnesota Crash Mapping Analysis Tool (MnCMAT). The Armstrong Boulevard corridor from 147<sup>th</sup> Ave to Alpine has had a total of 25 crashes over the 5 year period (2010-2014) with the majority being property damage only crashes. The individual intersection crash reports are shown in **Appendix C**.

Most of the intersections had very few crashes over the last 5 years. The T.H. 10 & Armstrong Boulevard intersection was not analyzed for crashes because it is currently being modified into an interchange. The following is a summary of crashes per intersection:

#### *Armstrong Boulevard at 147<sup>th</sup> Ave*

- 3 crashes (2 rear end, 1 right angle)

#### *Armstrong Boulevard at Bunker Lake Boulevard*

- 1 crash (rear end)

#### *Armstrong Boulevard at Alpine Drive*

- 14 crashes (8 right angle, 2 rear end, 2 left turn, 1 sideswipe, 1 other)
- The majority of the right angle crashes are caused by westbound vehicles failing to yield to right of way. The existing skew of the east leg may contribute to these right angle crashes by preventing adequate sight distances.
- There has also been an incapacitating injury at this intersection in 2013 (southbound rear end) and a non-incapacitating injury at this intersection in 2014 (northbound left turn into traffic)

#### *Alpine Drive at Puma Street*

- 0 crashes

---

## 2. Mitigation Recommendations

Mitigation due to safety concerns includes the following:

*Bunker Lake Boulevard (west of commercial section)*

- Three lane section is recommended (two through lanes & a center two-way-left-turn-lane) once the development is constructed. (short-term)
- Right turn lanes (locations and lengths) are to be dictated by development type. (short-term)

*Armstrong Boulevard & Alpine Drive*

- Add northbound and southbound left turn lanes. (mid-term)

*Alpine Drive & Puma Street*

- Add westbound left turn lane. (mid-term)
- Add eastbound right turn lane. (mid-term)

*Puma Street*

- Three lane section is recommended (two through lanes & a center two-way-left-turn-lane) once the development is constructed. (short-term)
- Right turn lanes (locations and lengths) are to be dictated by development type. (short-term)

## V. NO-BUILD CONDITIONS

Historic daily traffic volumes from 2000 to 2013 were used to formulate a 20 year background growth rate of 1.8% for Armstrong Boulevard. Therefore, background traffic was projected to grow by 43% along Armstrong Boulevard. No-Build conditions includes the background growth along with the traffic generated from the COR development. It is assumed that the COR development, east of Armstrong Boulevard, is fully built out for this scenario. The Future Business Park development, the focus of this study, is assumed to generate no traffic for this no-build scenario.

Parts of the COR development have already been built out, so only the portions that haven't been built out were analyzed for potential trips. The total new COR development is projected to generate 37,500 daily trips (6,150 peak hour trips). It was assumed that 1/3 of the traffic generated by the COR development will enter/exit on Bunker Lake Boulevard & Armstrong Boulevard. The COR development trips are included in **Appendix B**.

Assumptions made for all future scenarios include that the signals are uncoordinated, which is what they operate as currently. Signal timings are optimized for each scenario. Left turn movements at signals are all protected except for the westbound left turn at the T.H.10 North Ramp.

### A. 2040 No-Build Operations

Future traffic volumes for 2040 were forecasted for the study area without any additional development. Historic growth rates were used to calculate the 20 year growth rate of 1%. This growth rate was applied to Armstrong Boulevard to account for background traffic along the roadway. No-Build 2040 traffic volumes are shown in **Figures 3**.

**Table 4: 2040 No-Build Future Traffic Operations Analysis**

Traffic Control Scenario	Peak Hour	Intersection Delay* - LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>Design Year 2040 No-Build</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	8	A	19	B	NBL	EBL/T	68	106
	PM	8	A	17	B	NBL	EBL/T	68	107
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	10	A	22	C	WBL	WBT	117	198
	PM	12	B	25	C	NBL	SBT	125	217
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	8	A	27	C	NBL	WBT	72	164
	PM	12	B	33	C	NBL	SBT	136	248
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd)) <i>Signal</i>	AM	15	B	36	D	EBT	NBR	92	165
	PM	18	B	31	C	WBL	WBL	178	251
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	4	A	15	B	WBL	EBL/T	30	75
	PM	7	A	38	E	EBL	NBL/T	56	130
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	1	A	4	A	NBL	NBL/R	1	10
	PM	1	A	5	A	NBL	NBL/R	3	17

*No Build Scenario assumes the east (COR) development is built out, but the west development has not been built out*

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

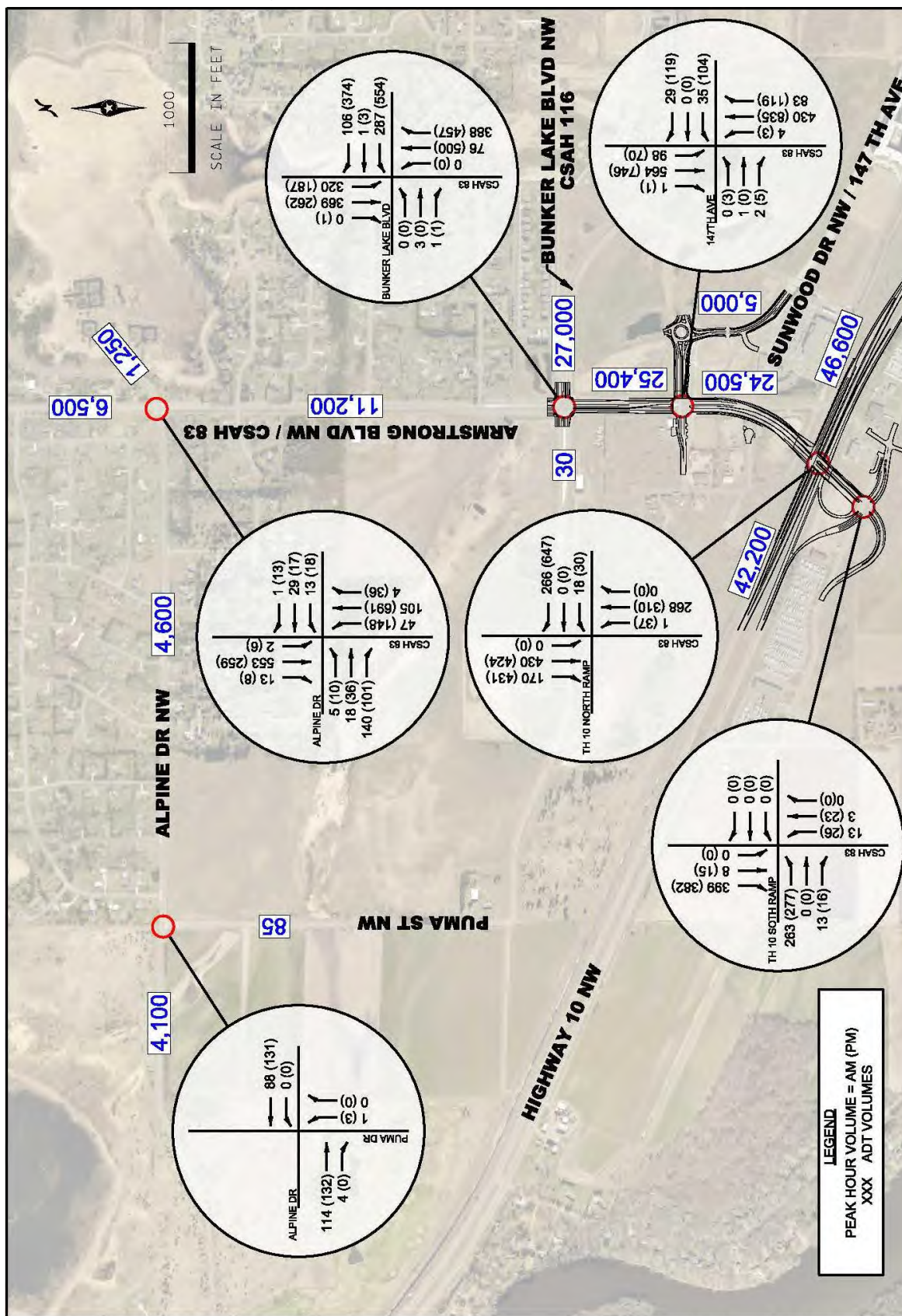
\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

---

Based on the expected growth in the area, the corridor is anticipated to experience acceptable operations at most of the intersections. All intersections operate at an overall level of service of B or higher. Level of Service D is commonly taken as an acceptable design year LOS. The only intersection experiencing significant delay at an individual movement is the eastbound left turn at Armstrong Boulevard & Alpine Drive. This movement experiences 38 seconds of delay (LOS E) during the PM peak hour.

Figure 3: 2040 No-Build Traffic Volumes



---

## B. No-Build Mitigation

There are few mitigation recommendations due to the adequate level of service at most intersections. The following suggestions are anticipated to provide adequate service for the 2040 No-Build conditions.

### 1. Geometric Improvements

It is recommended that the following changes be made with regard to the intersection geometry in the study area to provide acceptable operations in 2040:

#### *Armstrong Boulevard & Alpine Drive*

- Add eastbound and westbound thru/left and right turn lanes (mid-term)

#### *Armstrong Boulevard & Bunker Lake Boulevard*

- Re-stripe southbound lanes to include a dual southbound left turn lane. A southbound double left turn lane will help reduce queues entering the COR development. Improvements were done in 2011 to this intersection and a future southbound left turn lane was designed, but not striped. (mid-term)

The proposed 2040 no-build mitigation should provide a LOS of D or better for all intersection movements. A more thorough investigation should be conducted for a possible re-alignment of the east leg of Armstrong Boulevard & Alpine Drive.

---

## VI. BUILD CONDITIONS

### A. Traffic Forecasts

The full build traffic forecast was calculated by combining the anticipated growth (2040 No-build volumes) with the expected amount of trips the proposed development will generate. Trip forecasts were generated using the information within the Institute of Transportation Engineers (ITE) Trip Generation Manual. ITE provides codes that correlate a land use with the anticipated traffic based on previous studies.

**Figure 4** shows different trip generation zones within the development along with the land use in each zone. These zones are based on where traffic will enter/exit the development. Zone 1 will be evaluated with four alternatives in the north quadrant; a high school, K-12 schools, Business Park, and low density residential. The business park option is expected to generate the most traffic in Zone 1 (2,000 peak hour trips, 7,050 daily trips), while the low density residential option is expected generate the least amount of traffic in Zone 1 (1,150 peak hour trips, 4,900 daily trips). The total development (Zones 1-5) is projected to generate anywhere from 18,500 to 23,300 new daily trips. More detailed information regarding trip generations for each scenario is included in **Appendix B**.

The following access assumptions were made for each zone:

Access off of Bunker Lake Boulevard:

- Zone 1 & 3

Access off of Armstrong Boulevard:

- Zone 2

Access off of Puma Street

- Zone 4

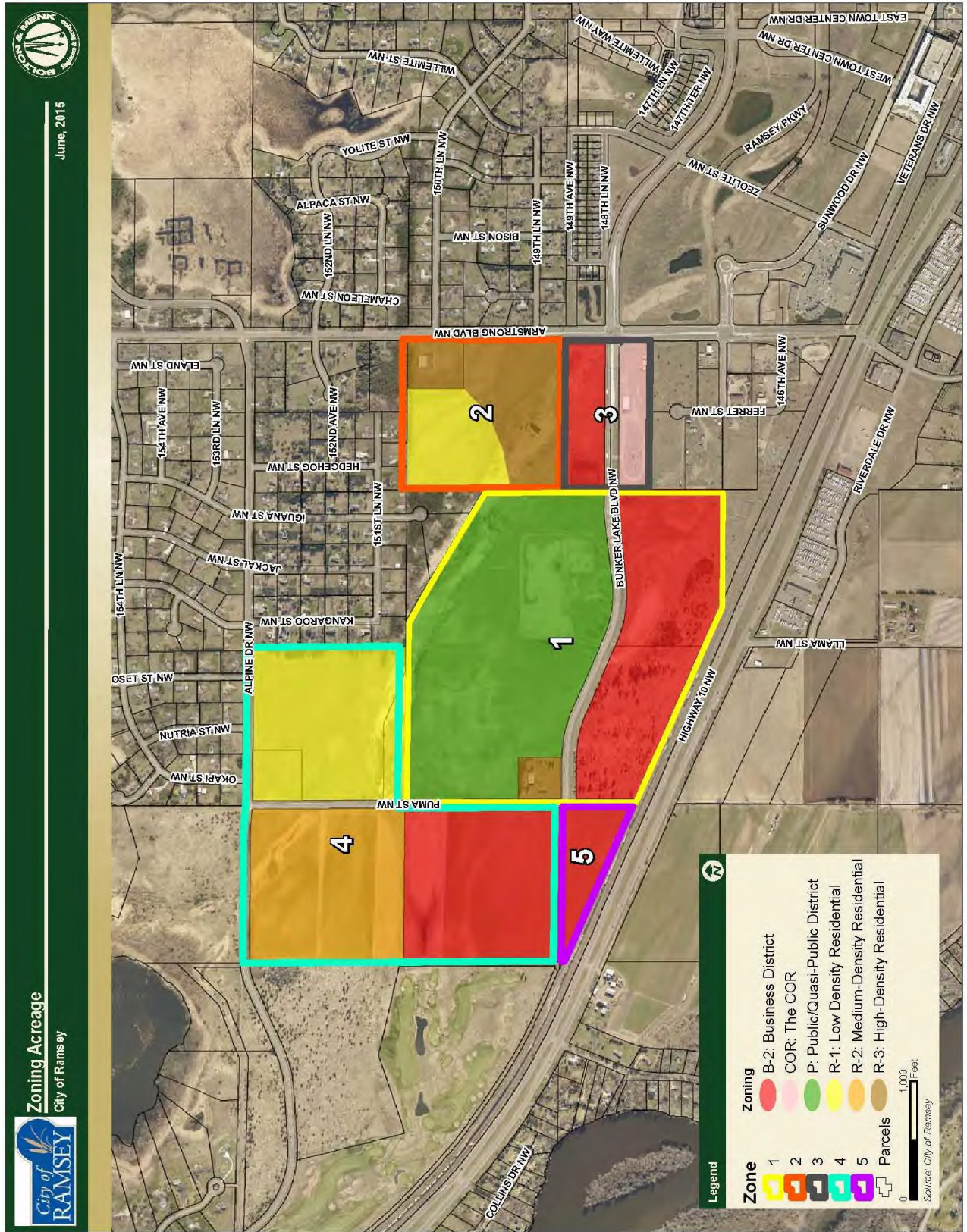
Accesses off of Bunker Lake Boulevard and Puma Street

- Zone 5

Trips to and from the development area are generally directed to the south to T.H. 10. The trips were distributed to the roadway using existing trip distribution as a basis. The generated trips from the proposed development were added to the 2040 No-Build forecasted volumes to develop the 2040 Build conditions. The trip distribution to and from the development is shown in **Figure 5**. The 2040 Build traffic volumes can be seen in **Figure 6.1** through **6.4**.

The different alternatives correspond to the land use on the north side of Zone 1. Alternative 1A assumes a high school is built, Alternative 1B assumes K-12 schools are built, Alternative 2 assumes a business park is built, and Alternative 3 assumes low density residential homes are built.

Figure 4: Trip Generation Zones



June, 2015

Zoning Acreage  
City of Ramsey



Figure 5: Trip Distribution Map

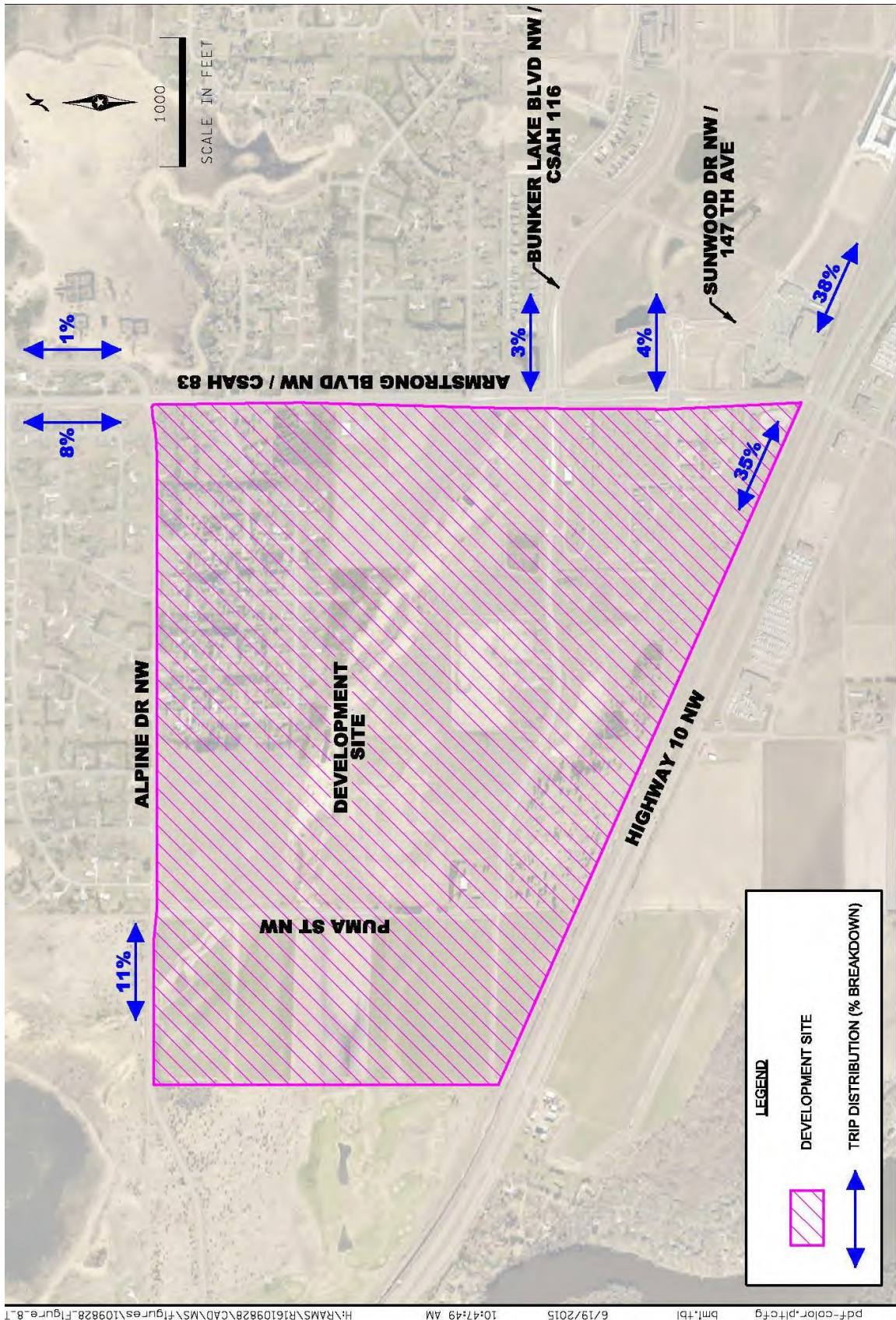


Figure 6.1: 2040 Build Traffic Volumes (Alternative 1A: High School)

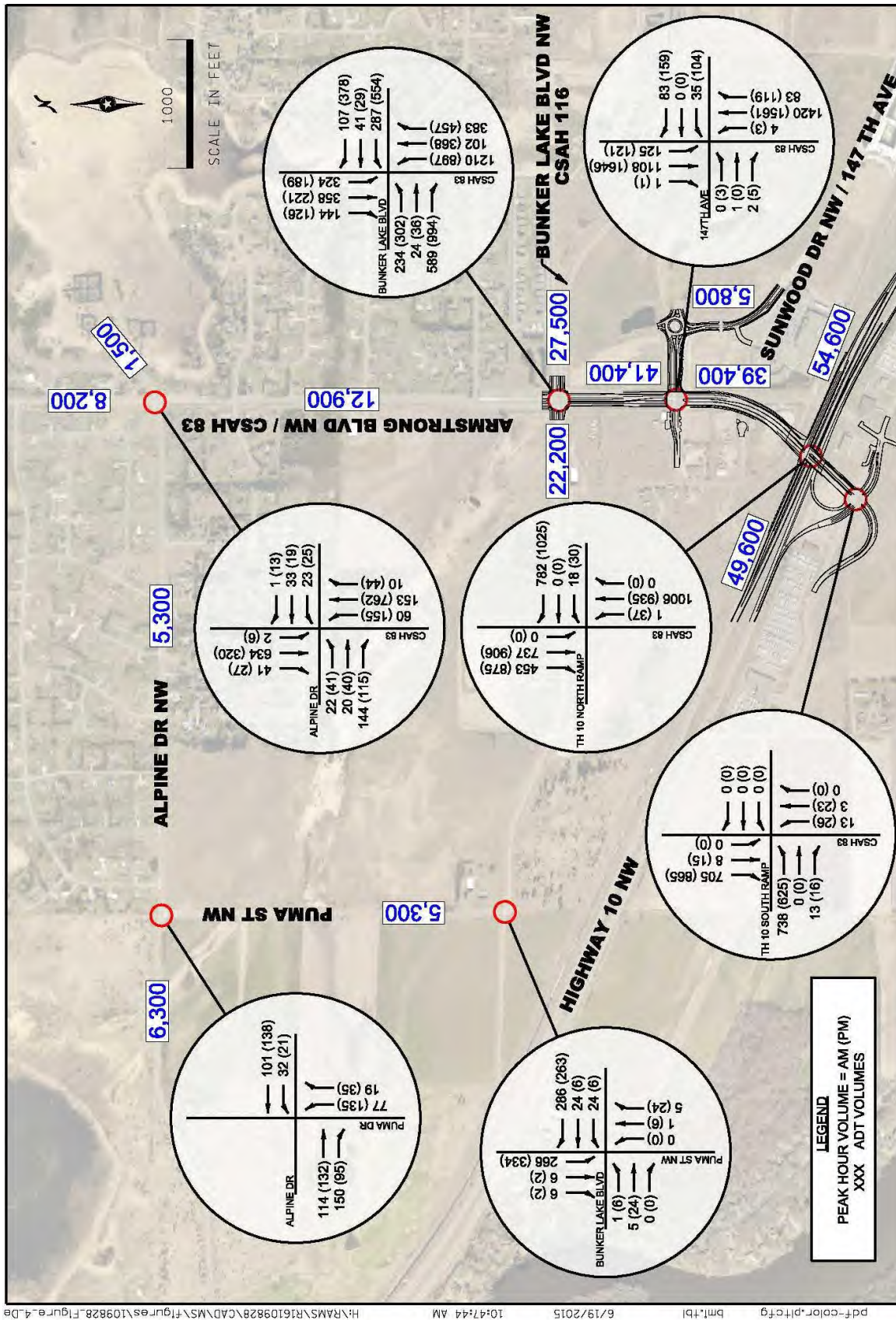


Figure 6.2: 2040 Build Traffic Volumes (Alternative 1B: K-12 Schools)

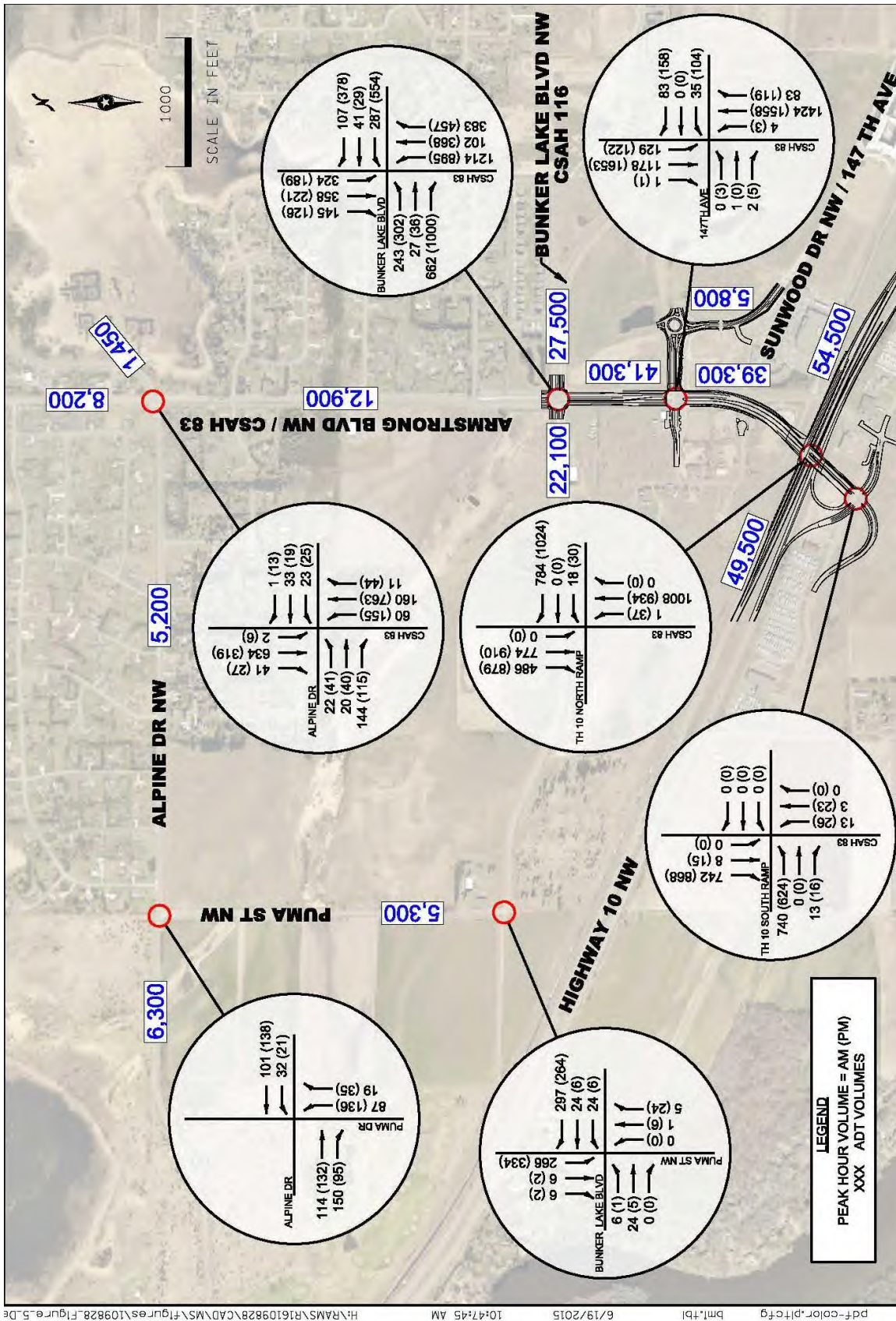


Figure 6.3: 2040 Build Traffic Volumes (Alternative 2: Business Park)

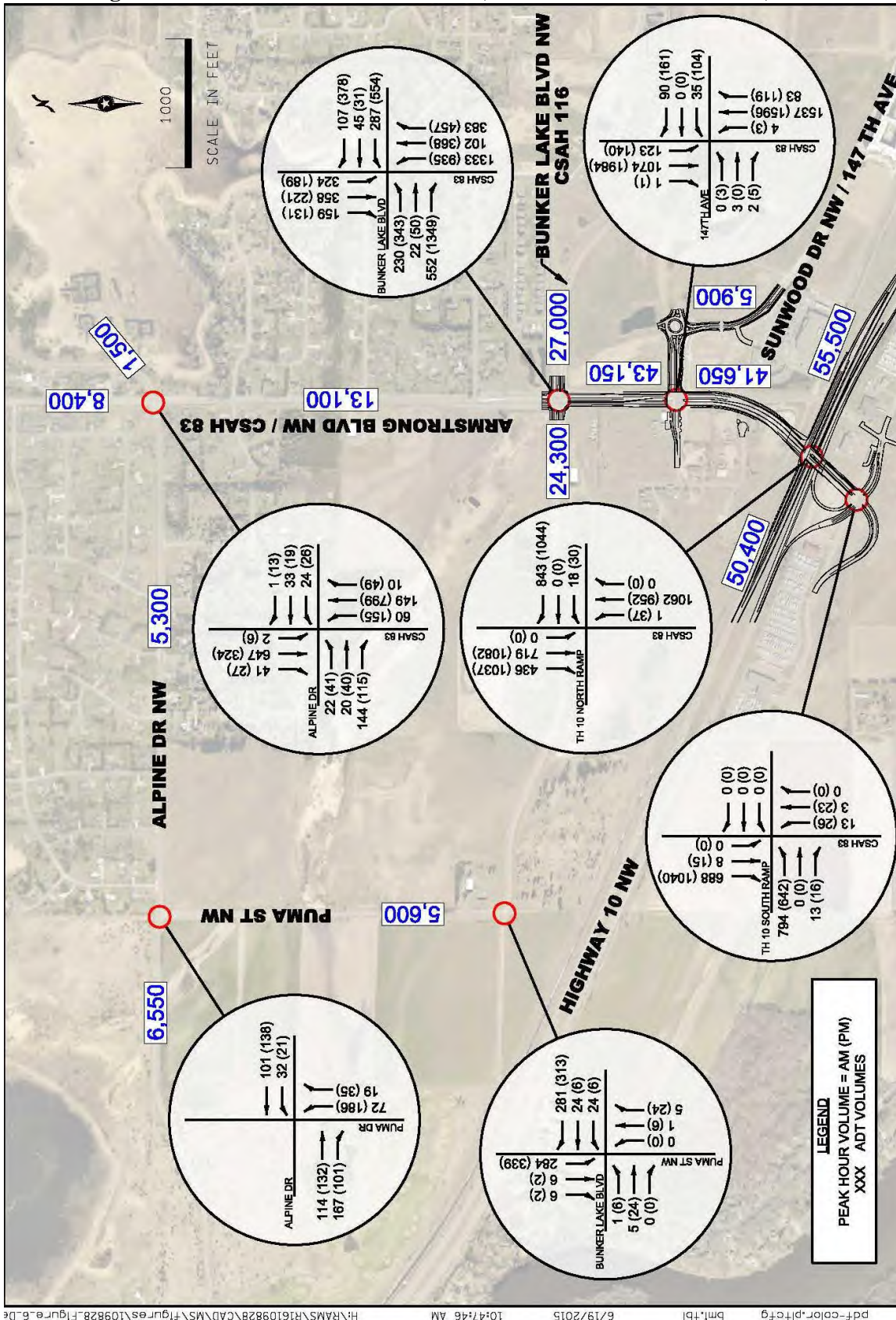
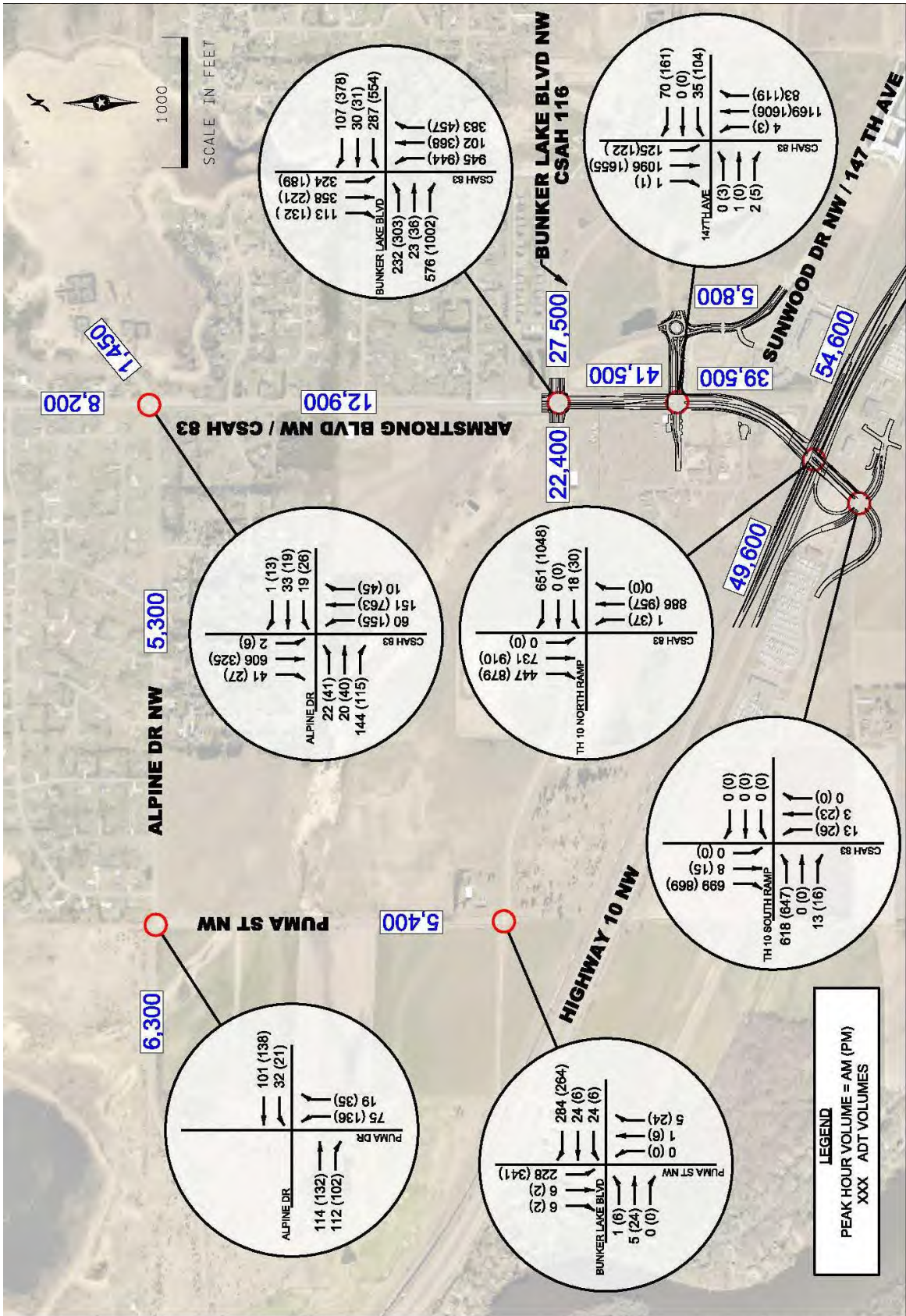


Figure 6.4: 2040 Build Traffic Volumes (Alternative 3: Low Density Residential)



## B. 2040 Build Operations

### 1. Operations Analysis

The traffic operations results for the 2040 Build is included in Tables 8-11.

**Table 8: 2040 Build Operations Analysis (Alternative 1A: High School)**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft)****	
<b>Design Year 2040 Alternative 1A</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	47	D	86	F	EBL	EBT	149	734
	PM	18	B	29	C	NBL	EBL	130	241
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	131	F	326	F	WBR	WBT/R	1106	2034
	PM	112	F	280	F	WBR	WBT/R	1104	2061
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	67	E	175	F	NBL	NBT	894	1276
	PM	60	E	114	F	NBT	NBT	873	1262
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd) <i>Signal (Added EB through-right, left, and SB left turn lanes)</i>	AM	66	E	162	F	WBL	NBT	793	990
	PM	74	E	182	F	NBL	NBT	821	1014
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	4	A	25	C	WBR	EBL/T	36	88
	PM	9	A	54	F	EBL	EBL/T	76	171
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	2	A	7	A	NBL	NBL/T	36	58
	PM	3	A	8	A	NBL	NBL/R	47	74
Puma Street NW & Bunker Lake Blvd <i>Option 1: AWSC</i>	AM	4	A	9	A	SBT	SBL/T/R	56	84
	PM	4	A	9	A	WBL	SBLR/E	59	86
Puma Street NW & Bunker Lake Blvd <i>Option 2: 3 Legged intersection (with curve)</i>	AM	1	A	11	B	EBL	NBT/L	14	49
	PM	1	A	8	A	EBL	EBL/T	23	43
Puma Street NW & Bunker Lake Blvd <i>Option 3: Roundabout</i>	AM	3	A	4	A	SBT	SBL/T/R	18	57
	PM	3	A	4	A	SBT	WBL/T/R	25	64

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

**Table 9: 2040 Build Operations Analysis (Alternative 1B: K-12 Schools)**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft)****	
<b>Design Year 2040 Alternative 1B</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	32	C	56	E	EBL	EBL	225	446
	PM	20	B	34	C	EBL	EBL	140	277
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	137	F	322	F	WBR	WBT/R	1064	1975
	PM	104	F	243	F	WBR	WBT/R	1037	2033
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	90	F	166	F	NBL	NBT	920	1217
	PM	86	F	237	F	NBL	NBT	822	1330
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd) <i>Signal (Added EB through-right, left, and SB left turn lanes)</i>	AM	85	F	322	F	WBL	NBT	802	994
	PM	100	F	296	F	WBL	NBT	797	1074
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	4	A	19	C	EBT	EBL/T	36	80
	PM	7	A	29	D	WBL	EBL/T	52	106
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	2	A	8	A	NBL	NBL/T	38	60
	PM	3	A	9	A	NBL	NBL/T	49	79
Puma Street NW & Bunker Lake Blvd <i>Option 1: AWSC</i>	AM	4	A	9	A	SBT	SBL/T/R	54	77
	PM	4	A	10	A	SBT	SBL/T/R	59	87
Puma Street NW & Bunker Lake Blvd <i>Option 2: 3 Legged intersection (with curve)</i>	AM	1	A	8	A	EBL	NBL/R	13	43
	PM	1	A	8	A	EBL	EBL/R	24	46
Puma Street NW & Bunker Lake Blvd <i>Option 3: Roundabout</i>	AM	3	A	3	A	SBL	SBL/T/R	18	53
	PM	3	A	5	A	NBT	SBL/T/R	19	58

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

**Table 10: 2040 Traffic Operations Analysis (Alternative 2: Business Park)**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>Design Year 2040 Alternative 2</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	81	F	147	F	EBL	EBT	434	1574
	PM	48	D	95	F	EBL	EBT	174	815
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	118	F	312	F	WBR	WBT/R	1134	2008
	PM	115	F	347	F	WBR	WBT/R	1212	2070
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	62	E	189	F	NBL	NBT	948	1183
	PM	86	F	140	F	EBL	NBT	893	1263
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd) <i>Signal (Added EB through-right, left, and SB left turn lanes)</i>	AM	73	E	228	F	WBL	NBT	798	996
	PM	100	F	293	F	WBL	NBT	841	959
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	4	A	19	C	WBL	EBL/T	35	82
	PM	6	A	27	D	EBT	NBL/T	47	123
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	2	A	7	A	NBL	NBL/R	33	52
	PM	3	A	9	A	NBL	NBL/T	51	79
Puma Street NW & Bunker Lake Blvd <i>Option 1: AWSC</i>	AM	4	A	10	A	SBT	SBL/T/R	60	91
	PM	5	A	10	A	WBL	SBL/T/R	59	90
Puma Street NW & Bunker Lake Blvd <i>Option 2: 3 Legged intersection (with curve)</i>	AM	1	A	11	B	EBL	NBL/T	14	45
	PM	1	A	8	A	EBL	EBL/R	23	47
Puma Street NW & Bunker Lake Blvd <i>Option 3: Roundabout</i>	AM	3	A	4	A	SBT	SBL/T/R	20	57
	PM	3	A	4	A	EBT	SBL/T/R	20	64

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

**Table 11: 2040 Build Operations Analysis (Alternative 3: LD Residential)**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>Design Year 2040 Alternative 3</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	13	B	24	C	NBL	EBL/R	101	176
	PM	28	C	52	D	EBL	EBL/R	190	417
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	AM	85	F	225	F	WBR	WBT/R	694	1628
	PM	109	F	285	F	WBR	WBT/R	1143	2056
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal</i>	AM	62	E	272	F	NBL	NBT	824	1289
	PM	87	F	163	F	NBL	NBT	887	1282
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd) <i>Signal (Added EB through-right, left, and SB left turn lanes)</i>	AM	72	E	189	F	NBL	NBT	812	1025
	PM	98	F	307	F	WBL	NBT	817	1026
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	AM	4	A	18	C	EBL	NBL/T	32	86
	PM	7	A	33	D	EBL	EBL/T	59	131
Alpine Drive NW & Puma Street NW <i>TWSC</i>	AM	2	A	7	A	NBL	NBL/R	32	55
	PM	3	A	8	A	NBL	NBL/T	45	72
Puma Street NW & Bunker Lake Blvd <i>Option 1: AWSC</i>	AM	4	A	7	A	WBL	SBL/T/R	52	80
	PM	4	A	9	A	SBT	SBL/T/R	62	95
Puma Street NW & Bunker Lake Blvd <i>Option 2: 3 Legged intersection (with curve)</i>	AM	1	A	8	A	EBL	NBL/T	12	45
	PM	1	A	8	A	EBL	EBL/R	23	41
Puma Street NW & Bunker Lake Blvd <i>Option 3: Roundabout</i>	AM	3	A	4	A	EBT	SBL/T/R	16	51
	PM	3	A	4	A	NBT	SBL/T/R	26	71

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

Based on the expected growth in the area, and the generated trips from the proposed development, the study area is anticipated to have unacceptable operations at multiple intersections. Three of the signalized intersections have an overall LOS of E or worse in all of the alternative scenarios. The other signalized intersection, T.H. 10 South Ramp, is projected to have an acceptable overall LOS in Alternative 1A, 1B, and 3. The alternative 2 scenario shows the T.H. 10 South Ramp intersection having a LOS F during the AM peak hour. All signalized intersections have at least one movement with a LOS of D or worse in all of the scenarios. The unsignalized intersections are projected to operate adequately with the exception of Armstrong Boulevard & Alpine Drive. The eastbound and westbound approaches are experiencing excessive delays at this intersection for all alternatives. Alternative 2 will be used to determine what mitigation is necessary in the study area because it generated the most traffic out of the four alternatives. Overall, the operations can be improved, but are still considered unacceptable at many of the intersections. This is due to the large amount of traffic entering and exiting on Bunker Lake Boulevard and Armstrong Boulevard.

## 2. Additional Analysis

Additional operations analysis was completed to determine what intersection control would be adequate for the first development entrance west of Armstrong Boulevard on Bunker Lake Boulevard. Because of the heavy traffic along Bunker Lake Boulevard, it was assumed that stop control would not be suitable for this intersection. A roundabout and signal were modeled for the 2040 PM Build Alternative 2 scenario and the results are shown below.

**Table 12: Development Intersection Operations Analysis**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**	Limiting Movement ***	Max Approach Queue			
						Direction	Average Queue (ft)	Max Queue (ft) ****	
<b>First Development Entrance on Bunker Lake Road (West of Armstrong Boulevard)</b>									
Signal <i>Lanes: (EB/WB: L, T, T, R. NB/SB: L/T,R)</i>	PM	30	C	58	E	WBL	SBT	152	310
Roundabout <i>Lanes: (EB/WB: L/T, T/R. NB/SB: L/T/R)</i>	PM	27	D	46	E	EB	EBT/R	150	300

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

The signal or roundabout options both operate with an adequate overall level of service if the lanes shown are constructed. There will be at least one movement that has a LOS E or worse in both of the options.

Analysis was also completed to model the intersections if a river crossing was constructed along Armstrong Boulevard. It was assumed that the largest change in traffic due to the addition of the river crossing would be on T.H. 10. Therefore, the T.H. 10 ramp intersections were evaluated for operations. Intersections north of T.H. 10 were assumed to operate similarly to the scenarios without the river crossing.

**Table 13: Operations Analysis with River Crossing**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**		Limiting Movement ***	Max Approach Queue		
							Direction	Average Queue (ft)	Max Queue (ft) ****
<b>2040 Build Alt 2 (With River Crossing)</b>									
T.H. 10 South Ramp <i>Signal</i>	PM	151	F	475	F	EBL	EBT	1621	2762
T.H. 10 North Ramp <i>Signal</i>	PM	98	F	205	F	SBT	WBT/R	667	1550

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

The delays at these intersections get worse with the river crossing because additional vehicles are using these ramps to access the river crossing from T.H. 10. A more thorough investigation should be conducted in the future to determine countermeasures if a river crossing is to be constructed.

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## C. Proposed Mitigation

### 1. Mitigation

The proposed No-Build mitigations identified under the No-Build Conditions should be considered as well as the following to improve operations for the 2040 Build condition. Although these changes will improve operations, many of the intersections will still be considered unacceptable. This is due to the large amount of traffic entering and exiting on Bunker Lake Boulevard and Armstrong Boulevard.

#### a) Traffic Control

It is recommended that the following changes be made with regard to the traffic control in the study area:

##### *Bunker Lake Boulevard & Puma Street*

- All-way stop control, a 3 legged intersection with two-way stop control, and a roundabout were evaluated at this intersection. Each option operated with adequate levels of service. (short-term)

#### b) Geometric Improvements

It is recommended that the following changes be made with regard to the intersection geometry of the study area:

##### *Armstrong Boulevard & Bunker Lake Boulevard*

- Add a second eastbound left turn lane (short-term)
- Convert the southerly eastbound through lane into a through-right lane (long-term)
- Another option would be an eastbound free-right with an add lane that can be extended to the T.H. 10 North Ramp (long-term)

It is recommended that free-rights only be constructed when actually needed due the impact they have on pedestrian movements. Another option may be to signalize the right turn movement and provide an overlap phase to provide more green time to the right turn.

## D. Additional Operations Analysis (50% Development Completion)

A 2030 analysis was also completed to determine when the study area would start experiencing significant delays. This analysis assumes 50% of the Future Business Park development is complete and 50% of the COR development is complete.

**Table 14: 2030 (50% Development) Operations Analysis**

Traffic Control Scenario	Peak Hour	Intersection Delay*- LOS		Maximum Delay-LOS**		Limiting Movement ***	Max Approach Queue		
							Direction	Average Queue (ft)	Max Queue (ft) ****
<b>Design Year 2030 Build Alternative Alternative 2 (50% Development)</b>									
TH 10/169 South Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	PM	9	A	17	B	NBL	EBL	64	100
TH 10/169 North Ramp & CSAH 83 (Armstrong Blvd) <i>Signal</i>	PM	12	B	22	C	NBL	SBT	178	330
CSAH 83 (Armstrong Blvd & 147th Avenue) <i>Signal (Added EB through, right, left, and SB left turn lanes)</i>	PM	11	B	37	D	EBL	NBT	130	203
CSAH 83 (Armstrong Blvd & CSAH 116 (Bunker Lake Blvd) <i>Signal</i>	PM	37	D	72	E	WBL	EBT	146	561
CSAH 83 (Armstrong Blvd) & Alpine Drive NW <i>TWSC</i>	PM	5	A	22	C	EBL	NBL/T	40	126
Alpine Drive NW & Puma Street NW <i>TWSC</i>	PM	3	A	6	A	NBR	NBL/T	41	66

*Assumes the east (COR) development and west development are 50% built out*

\*Delay in seconds per vehicle

\*\*Maximum delay and LOS on any approach and/or movement

\*\*\*Limiting Movement is the highest delay movement.

\*\*\*\*Max Queue refers to the 95% Queue (Passenger car stored length = 25 ft, Heavy vehicle stored length = 45 ft)

At 50% development, the intersections are anticipated to operate with adequate delays. Therefore, it is projected that the intersections will start to experience excessive delays anywhere from 50% to 100% completion of the development.



**APPENDIX A-**  
**TH 10 & ARMSTRONG BOULEVARD**  
**OVERPASS LAYOUT**



ANDRA  
83  
COUNTY

ACCESS CLOSED

ACCESS CLOSED

ROADWAY REMOVED










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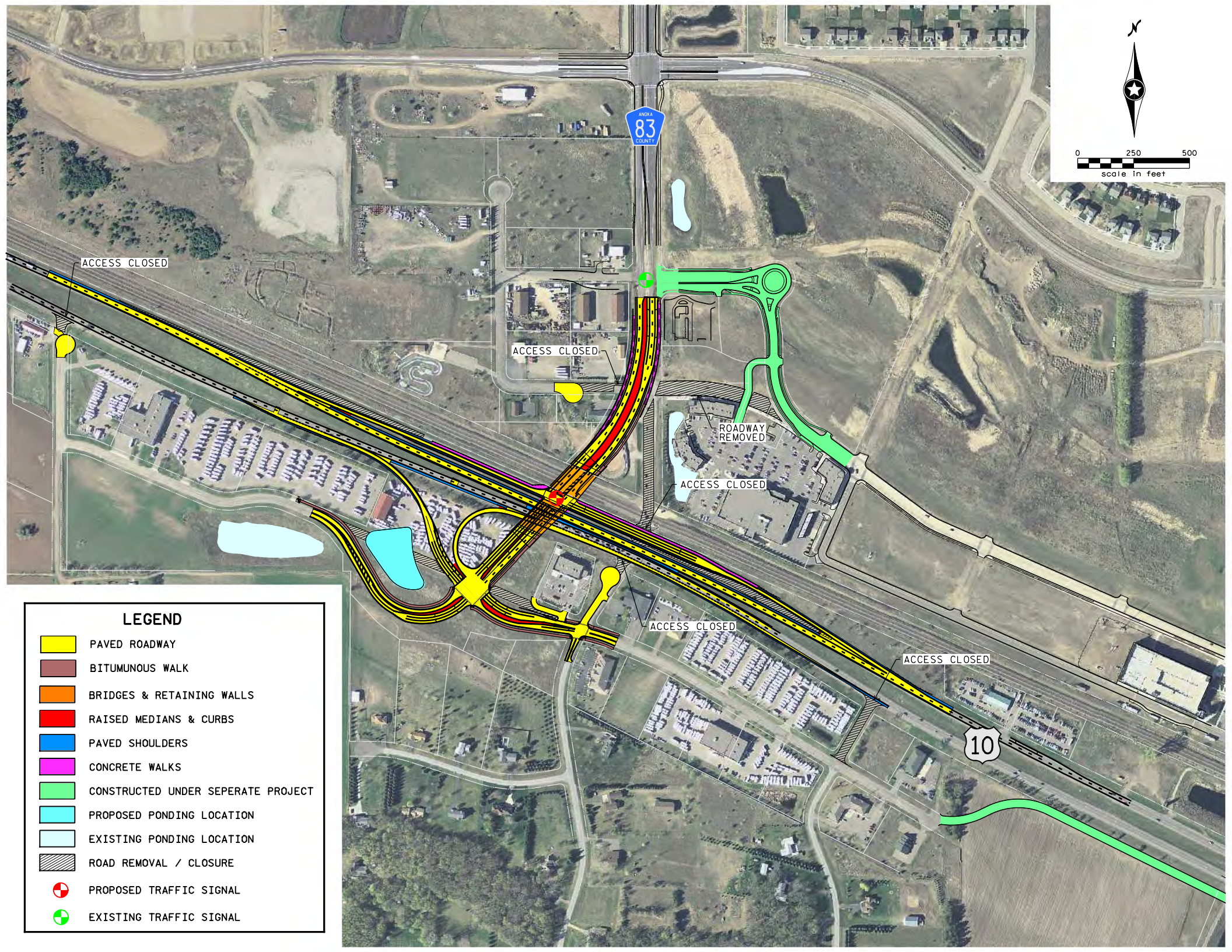
ACCESS CLOSED

ACCESS CLOSED

10

### LEGEND

-  PAVED ROADWAY
-  BITUMINOUS WALK
-  BRIDGES & RETAINING WALLS
-  RAISED MEDIANS & CURBS
-  PAVED SHOULDERS
-  CONCRETE WALKS
-  CONSTRUCTED UNDER SEPERATE PROJECT
-  PROPOSED PONDING LOCATION
-  EXISTING PONDING LOCATION
-  ROAD REMOVAL / CLOSURE
-  PROPOSED TRAFFIC SIGNAL
-  EXISTING TRAFFIC SIGNAL





## APPENDIX B- TRAFFIC VOLUMES



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 1 - Alpine Dr & Puma St NW, 4-8-15, 6am-7pm

Site Code : 1

Start Date : 4/8/2015

Page No : 1

Alpine Dr and Puma St NW  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Southbound						Alpine Dr Westbound						Puma St NW Northbound						Alpine Dr Eastbound						Int. Total						
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total							
06:00 AM	0	0	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	10
06:15 AM	0	0	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	18
06:30 AM	0	0	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	0	0	13	1	0	14	0	0	0	0	0	0	21
06:45 AM	0	0	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	21
Total	0	0	0	0	0	0	0	0	38	0	0	38	0	0	0	0	0	0	0	0	31	1	0	32	0	0	0	0	0	0	70
07:00 AM	0	0	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	19
07:15 AM	0	0	0	0	0	0	0	0	15	0	0	15	0	0	0	0	0	0	0	0	14	1	0	15	0	0	0	0	0	0	30
07:30 AM	0	0	0	0	0	0	0	0	19	0	0	19	0	1	0	0	0	0	1	0	19	2	0	21	0	0	0	0	0	0	41
07:45 AM	0	0	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	0	0	17	1	0	18	0	0	0	0	0	0	27
Total	0	0	0	0	0	0	0	0	50	0	0	50	0	1	0	0	0	0	1	0	62	4	0	66	0	0	0	0	0	0	117
08:00 AM	0	0	0	0	0	0	0	0	14	0	0	14	0	1	0	0	0	0	1	0	11	2	0	13	0	0	0	0	0	0	28
08:15 AM	0	0	0	0	0	0	0	0	11	0	0	11	0	1	0	0	0	0	1	0	15	0	0	15	0	0	0	0	0	0	27
08:30 AM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	7	1	0	8	0	0	0	0	0	0	19
08:45 AM	0	0	0	0	0	0	0	1	8	0	0	9	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	20
Total	0	0	0	0	0	0	0	1	44	0	0	45	0	2	0	0	0	0	2	0	44	3	0	47	0	0	0	0	0	0	94
09:00 AM	0	0	0	0	0	0	0	1	10	0	0	11	0	0	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	16
09:15 AM	0	0	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	16
09:30 AM	0	0	0	0	0	0	0	0	7	0	0	7	0	1	0	0	0	0	1	0	5	0	0	5	0	0	0	0	0	0	13
09:45 AM	0	0	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	10
Total	0	0	0	0	0	0	0	1	31	0	0	32	0	1	0	0	0	0	1	0	22	0	0	22	0	0	0	0	0	0	55
10:00 AM	0	0	0	0	0	0	0	0	8	0	0	8	0	1	0	0	0	0	1	0	7	0	0	7	0	0	0	0	0	0	16
10:15 AM	0	0	0	0	0	0	0	0	4	0	0	4	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	5
10:30 AM	0	0	0	0	0	0	0	0	6	0	0	6	0	1	0	3	0	0	4	0	5	1	0	6	0	0	0	0	0	0	16
10:45 AM	0	0	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	17
Total	0	0	0	0	0	0	0	0	21	0	0	21	0	3	0	3	0	0	6	0	26	1	0	27	0	0	0	0	0	0	54
11:00 AM	0	0	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	11
11:15 AM	0	0	0	0	0	0	0	0	10	0	0	10	0	1	0	0	0	0	1	0	7	1	0	8	0	0	0	0	0	0	19
11:30 AM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	17
11:45 AM	0	0	0	0	0	0	0	0	8	0	0	8	0	1	0	0	0	0	1	0	6	0	0	6	0	0	0	0	0	0	15
Total	0	0	0	0	0	0	0	0	35	0	0	35	0	2	0	0	0	0	2	0	24	1	0	25	0	0	0	0	0	0	62
12:00 PM	0	0	0	0	0	0	0	0	8	0	0	8	0	1	0	0	0	0	1	0	21	1	0	22	0	0	0	0	0	0	31
12:15 PM	0	0	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	11
12:30 PM	0	0	0	0	0	0	0	0	6	0	0	6	0	2	0	0	0	0	2	0	8	0	1	9	0	0	0	0	0	0	17
12:45 PM	0	0	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	15
Total	0	0	0	0	0	0	0	0	21	0	0	21	0	3	0	0	0	0	3	0	48	1	1	50	0	0	0	0	0	0	74
01:00 PM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	22
01:15 PM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	7	1	0	8	0	0	0	0	0	0	19
01:30 PM	0	0	0	0	0	0	0	1	9	0	0	10	0	0	0	1	0	0	1	0	4	1	0	5	0	0	0	0	0	0	16
01:45 PM	0	0	0	0	0	0	0	0	11	0	0	11	0	2	0	0	0	0	2	0	7	0	0	7	0	0	0	0	0	0	20
Total	0	0	0	0	0	0	0	1	42	0	0	43	0	2	0	1	0	0	3	0	29	2	0	31	0	0	0	0	0	0	77



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 1 - Alpine Dr & Puma St NW, 4-8-15, 6am-7pm

Site Code : 1

Start Date : 4/8/2015

Page No : 2

Alpine Dr and Puma St NW  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Southbound						Alpine Dr Westbound						Puma St NW Northbound						Alpine Dr Eastbound						Int. Total	
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total		
02:00 PM	0	0	0	0	0	0	0	0	14	0	1	15	0	0	0	0	0	0	0	0	0	7	0	0	7	22
02:15 PM	0	0	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	0	0	11	0	0	11	16	
02:30 PM	0	0	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	0	0	10	0	1	11	20	
02:45 PM	0	0	0	0	0	0	0	0	16	0	0	16	0	3	0	0	0	3	0	0	7	0	0	7	26	
Total	0	0	0	0	0	0	0	0	44	0	1	45	0	3	0	0	0	3	0	0	35	0	1	36	84	
03:00 PM	0	0	0	0	0	0	0	1	12	0	0	13	0	0	0	0	0	0	0	0	13	0	0	13	26	
03:15 PM	0	0	0	0	0	0	0	1	16	0	0	17	0	2	0	1	0	3	0	0	9	0	0	9	29	
03:30 PM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	12	0	0	12	23	
03:45 PM	0	0	0	0	0	0	0	0	8	0	0	8	0	0	0	1	0	1	0	0	21	0	2	23	32	
Total	0	0	0	0	0	0	0	2	47	0	0	49	0	2	0	2	0	4	0	0	55	0	2	57	110	
04:00 PM	0	0	0	0	0	0	0	0	16	0	2	18	0	0	0	0	0	0	0	0	18	1	0	19	37	
04:15 PM	0	0	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	1	0	17	0	0	18	32	
04:30 PM	0	0	0	0	0	0	0	0	22	0	0	22	0	0	0	0	0	0	0	0	19	0	0	19	41	
04:45 PM	0	0	0	0	0	0	0	0	8	0	0	8	0	2	0	0	0	2	0	0	20	0	1	21	31	
Total	0	0	0	0	0	0	0	0	60	0	2	62	0	2	0	0	0	2	1	0	74	1	1	77	141	
05:00 PM	0	0	0	0	0	0	0	0	14	0	1	15	0	1	0	0	0	1	0	0	13	0	0	13	29	
05:15 PM	0	0	0	0	0	0	0	0	14	0	1	15	0	2	0	0	0	2	0	0	28	0	0	28	45	
05:30 PM	0	0	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	0	0	24	0	0	24	38	
05:45 PM	0	0	0	0	0	0	0	0	15	0	0	15	0	1	0	0	0	1	0	0	13	0	0	13	29	
Total	0	0	0	0	0	0	0	0	57	0	2	59	0	4	0	0	0	4	0	0	78	0	0	78	141	
06:00 PM	0	0	0	0	0	0	0	0	10	0	0	10	0	2	0	0	0	2	0	0	10	0	0	10	22	
06:15 PM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	1	4	5	0	0	12	0	2	14	30	
06:30 PM	0	0	0	0	0	0	0	0	11	0	1	12	0	0	0	0	1	1	0	0	7	1	0	8	21	
06:45 PM	0	0	0	0	0	0	0	0	10	0	1	11	0	0	0	0	4	4	0	0	6	0	3	9	24	
Total	0	0	0	0	0	0	0	0	42	0	2	44	0	2	0	1	9	12	0	0	35	1	5	41	97	
Grand Total	0	0	0	0	0	0	0	5	532	0	7	544	0	27	0	7	9	43	1	0	563	15	10	589	1176	
Apprch %	0	0	0	0	0	0	0	0.9	97.8	0	1.3		0	62.8	0	16.3	20.9		0.2	0	95.6	2.5	1.7			
Total %	0	0	0	0	0	0	0	0.4	45.2	0	0.6	46.3	0	2.3	0	0.6	0.8	3.7	0.1	0	47.9	1.3	0.9	50.1		
Cars +	0	0	0	0	0	0	0	4	506	0	6	516	0	26	0	7	8	41	1	0	532	15	10	558	1115	
% Cars +	0	0	0	0	0	0	0	80	95.1	0	85.7	94.9	0	96.3	0	100	88.9	95.3	100	0	94.5	100	100	94.7	94.8	
Trucks	0	0	0	0	0	0	0	1	26	0	1	28	0	1	0	0	1	2	0	0	31	0	0	31	61	
% Trucks	0	0	0	0	0	0	0	20	4.9	0	14.3	5.1	0	3.7	0	0	11.1	4.7	0	0	5.5	0	0	5.3	5.2	



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

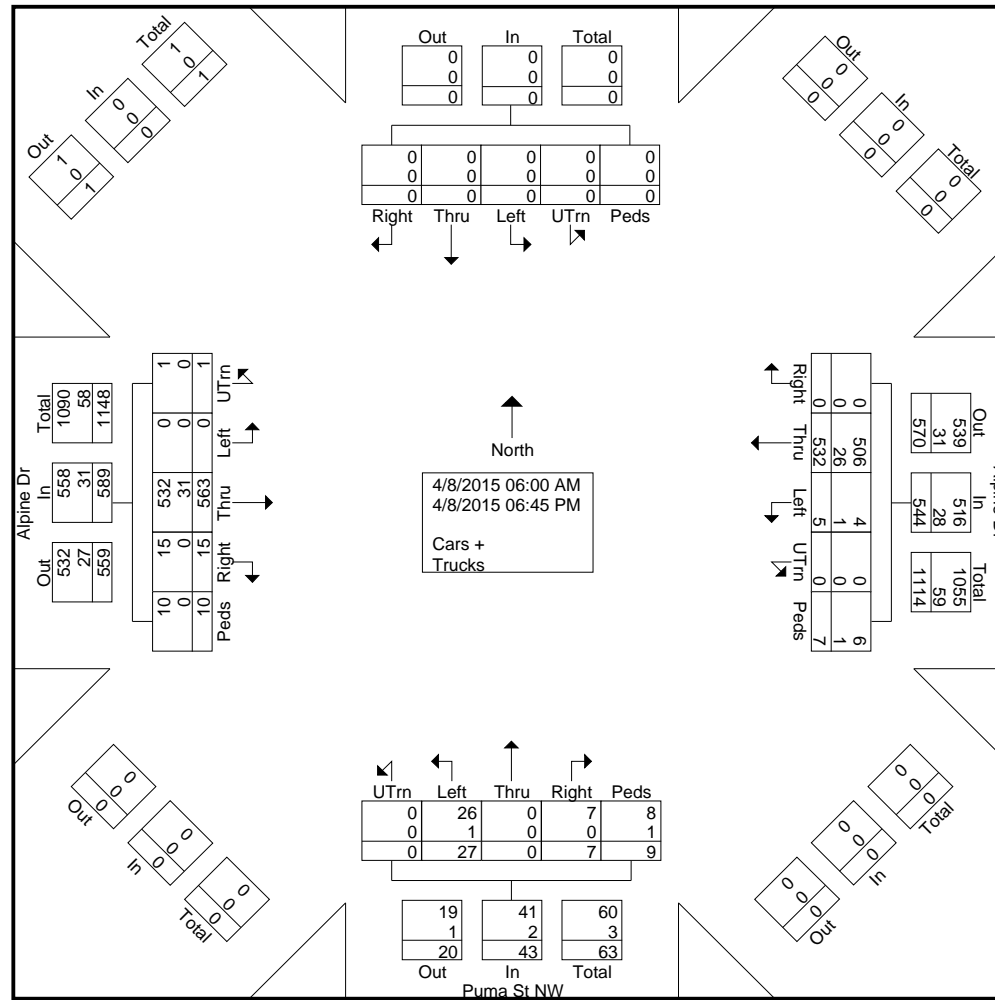
File Name : 1 - Alpine Dr & Puma St NW, 4-8-15, 6am-7pm

Site Code : 1

Start Date : 4/8/2015

Page No : 3

Alpine Dr and Puma St NW  
Ramsey, MN





# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 1 - Alpine Dr & Puma St NW, 4-8-15, 6am-7pm

Site Code : 1

Start Date : 4/8/2015

Page No : 4

## Alpine Dr and Puma St NW Ramsey, MN

Start Time	Southbound						Alpine Dr Westbound					Puma St NW Northbound					Alpine Dr Eastbound					Int. Total			
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru		Right	Peds	App. Total
Peak Hour Analysis From 06:00 AM to 09:45 AM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 07:15 AM																									
07:15 AM	0	0	0	0	0	0	0	0	15	0	0	15	0	0	0	0	0	0	0	0	14	1	0	15	30
07:30 AM	0	0	0	0	0	0	0	0	19	0	0	19	0	1	0	0	0	1	0	0	19	2	0	21	41
07:45 AM	0	0	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	0	0	17	1	0	18	27
08:00 AM	0	0	0	0	0	0	0	0	14	0	0	14	0	1	0	0	0	1	0	0	11	2	0	13	28
Total Volume	0	0	0	0	0	0	0	0	57	0	0	57	0	2	0	0	0	2	0	0	61	6	0	67	126
% App. Total	0	0	0	0	0	0	0	0	100	0	0	100	0	100	0	0	0	0	0	0	91	9	0	100	
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.750	.000	.000	.750	.000	.500	.000	.000	.000	.500	.000	.000	.803	.750	.000	.798	.768
Peak Hour Analysis From 10:00 AM to 01:45 PM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 11:15 AM																									
11:15 AM	0	0	0	0	0	0	0	0	10	0	0	10	0	1	0	0	0	1	0	0	7	1	0	8	19
11:30 AM	0	0	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	6	0	0	6	17
11:45 AM	0	0	0	0	0	0	0	0	8	0	0	8	0	1	0	0	0	1	0	0	6	0	0	6	15
12:00 PM	0	0	0	0	0	0	0	0	8	0	0	8	0	1	0	0	0	1	0	0	21	1	0	22	31
Total Volume	0	0	0	0	0	0	0	0	37	0	0	37	0	3	0	0	0	3	0	0	40	2	0	42	82
% App. Total	0	0	0	0	0	0	0	0	100	0	0	100	0	100	0	0	0	0	0	0	95.2	4.8	0	100	
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.841	.000	.000	.841	.000	.750	.000	.000	.000	.750	.000	.000	.476	.500	.000	.477	.661
Peak Hour Analysis From 02:00 PM to 06:45 PM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 04:30 PM																									
04:30 PM	0	0	0	0	0	0	0	0	22	0	0	22	0	0	0	0	0	0	0	0	19	0	0	19	41
04:45 PM	0	0	0	0	0	0	0	0	8	0	0	8	0	2	0	0	0	2	0	0	20	0	1	21	31
05:00 PM	0	0	0	0	0	0	0	0	14	0	1	15	0	1	0	0	0	1	0	0	13	0	0	13	29
05:15 PM	0	0	0	0	0	0	0	0	14	0	1	15	0	2	0	0	0	2	0	0	28	0	0	28	45
Total Volume	0	0	0	0	0	0	0	0	58	0	2	60	0	5	0	0	0	5	0	0	80	0	1	81	146
% App. Total	0	0	0	0	0	0	0	0	96.7	0	3.3	100	0	100	0	0	0	0	0	0	98.8	0	1.2	100	
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.659	.000	.500	.682	.000	.625	.000	.000	.000	.625	.000	.000	.714	.000	.250	.723	.811



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 2 - Armstrong Blvd NW & Alpine Dr, 4-8-15, 6am-7pm

Site Code : 2

Start Date : 4/8/2015

Page No : 1

Armstrong Blvd NW and Alpine Dr  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						Alpine Dr Westbound						Armstrong Blvd NW Northbound						Alpine Dr Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
06:00 AM	0	0	58	0	0	58	0	2	3	0	0	5	0	0	6	0	0	6	0	0	1	10	0	11	80
06:15 AM	0	3	73	2	0	78	0	3	4	0	0	7	0	4	12	0	0	16	0	0	1	7	0	8	109
06:30 AM	0	2	82	0	0	84	0	1	4	0	0	5	0	1	8	0	0	9	0	4	2	15	0	21	119
06:45 AM	0	3	87	0	0	90	0	0	8	0	0	8	0	1	6	0	0	7	0	1	3	21	0	25	130
Total	0	8	300	2	0	310	0	6	19	0	0	25	0	6	32	0	0	38	0	5	7	53	0	65	438
07:00 AM	0	2	89	3	0	94	0	0	4	0	0	4	0	4	9	0	0	13	0	1	4	21	0	26	137
07:15 AM	0	0	90	2	0	92	0	2	9	0	0	11	0	1	10	0	0	11	0	0	4	23	0	27	141
07:30 AM	0	0	107	6	0	113	0	1	13	1	0	15	0	2	11	1	0	14	0	3	4	27	0	34	176
07:45 AM	0	0	65	2	0	67	0	5	3	0	0	8	0	2	17	0	0	19	0	1	6	17	0	24	118
Total	0	2	351	13	0	366	0	8	29	1	0	38	0	9	47	1	0	57	0	5	18	88	0	111	572
08:00 AM	0	0	63	1	0	64	0	7	8	1	0	16	0	9	14	0	0	23	0	4	1	12	0	17	120
08:15 AM	0	1	43	1	0	45	0	2	5	0	0	7	0	6	9	2	0	17	0	2	5	19	0	26	95
08:30 AM	0	1	56	2	0	59	0	4	5	2	1	12	0	3	15	1	0	19	0	1	2	11	0	14	104
08:45 AM	0	1	36	4	0	41	0	0	2	0	0	2	0	6	14	1	0	21	0	2	4	9	0	15	79
Total	0	3	198	8	0	209	0	13	20	3	1	37	0	24	52	4	0	80	0	9	12	51	0	72	398
09:00 AM	0	2	53	2	0	57	0	0	3	1	0	4	0	5	12	3	0	20	0	0	2	7	0	9	90
09:15 AM	0	0	43	1	0	44	0	1	4	1	0	6	0	5	24	1	0	30	0	0	3	7	0	10	90
09:30 AM	0	1	28	0	0	29	0	0	3	1	0	4	0	4	17	0	0	21	0	0	3	11	0	14	68
09:45 AM	0	0	23	0	0	23	0	1	3	1	0	5	0	7	25	2	0	34	0	0	0	7	0	7	69
Total	0	3	147	3	0	153	0	2	13	4	0	19	0	21	78	6	0	105	0	0	8	32	0	40	317
10:00 AM	0	1	28	1	0	30	0	1	3	0	0	4	0	3	26	4	0	33	0	1	1	7	0	9	76
10:15 AM	0	0	25	1	0	26	0	1	1	2	0	4	0	6	19	3	0	28	0	0	2	3	1	6	64
10:30 AM	0	3	25	1	0	29	0	3	2	1	0	6	0	3	25	1	0	29	0	0	2	3	0	5	69
10:45 AM	0	2	25	1	0	28	0	4	1	2	0	7	0	2	23	0	0	25	0	2	5	9	0	16	76
Total	0	6	103	4	0	113	0	9	7	5	0	21	0	14	93	8	0	115	0	3	10	22	1	36	285
11:00 AM	0	0	33	1	0	34	0	2	3	0	0	5	0	7	26	1	0	34	0	1	0	7	0	8	81
11:15 AM	0	0	33	1	0	34	0	0	3	2	0	5	0	13	55	3	0	71	0	1	2	5	0	8	118
11:30 AM	0	2	28	0	0	30	0	2	1	0	0	3	0	9	29	1	1	40	0	0	4	7	1	12	85
11:45 AM	0	2	26	0	0	28	0	2	1	0	0	3	0	10	28	2	0	40	0	2	1	6	0	9	80
Total	0	4	120	2	0	126	0	6	8	2	0	16	0	39	138	7	1	185	0	4	7	25	1	37	364
12:00 PM	0	2	33	0	0	35	0	3	6	2	0	11	0	2	25	2	0	29	0	2	3	13	0	18	93
12:15 PM	0	1	20	0	0	21	1	0	5	0	0	6	0	4	29	1	0	34	0	1	2	9	0	12	73
12:30 PM	0	1	27	0	0	28	0	1	1	1	0	3	0	3	21	4	0	28	0	1	3	7	0	11	70
12:45 PM	0	1	30	0	0	31	0	1	2	2	1	6	0	12	33	0	1	46	0	2	4	10	0	16	99
Total	0	5	110	0	0	115	1	5	14	5	1	26	0	21	108	7	1	137	0	6	12	39	0	57	335
01:00 PM	0	0	30	2	0	32	0	1	4	0	0	5	0	8	26	2	0	36	0	0	2	10	0	12	85
01:15 PM	0	0	21	0	0	21	0	2	5	0	0	7	0	10	25	3	0	38	0	1	0	9	1	11	77
01:30 PM	0	1	31	0	0	32	0	3	1	0	0	4	0	8	31	1	0	40	0	0	0	5	0	5	81
01:45 PM	0	0	26	3	0	29	0	1	1	2	0	4	0	8	35	1	0	44	0	1	5	7	0	13	90
Total	0	1	108	5	0	114	0	7	11	2	0	20	0	34	117	7	0	158	0	2	7	31	1	41	333



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 2 - Armstrong Blvd NW & Alpine Dr, 4-8-15, 6am-7pm

Site Code : 2

Start Date : 4/8/2015

Page No : 2

Armstrong Blvd NW and Alpine Dr  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						Alpine Dr Westbound						Armstrong Blvd NW Northbound						Alpine Dr Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
02:00 PM	0	0	28	0	0	28	0	4	5	1	0	10	0	8	41	5	1	55	0	1	0	12	1	14	107
02:15 PM	0	1	31	1	0	33	0	2	3	1	0	6	0	8	52	2	0	62	0	0	2	11	0	13	114
02:30 PM	0	3	31	0	0	34	0	1	0	1	0	2	0	16	52	1	0	69	0	0	4	8	0	12	117
02:45 PM	0	2	25	2	0	29	0	1	4	3	0	8	0	14	68	1	0	83	0	2	3	7	0	12	132
Total	0	6	115	3	0	124	0	8	12	6	0	26	0	46	213	9	1	269	0	3	9	38	1	51	470
03:00 PM	0	3	23	1	0	27	0	1	4	2	0	7	0	20	71	2	0	93	0	0	6	8	0	14	141
03:15 PM	0	2	29	2	0	33	0	1	3	0	0	4	0	12	66	4	0	82	0	5	2	8	3	18	137
03:30 PM	0	3	34	2	0	39	0	1	6	3	0	10	0	13	79	2	0	94	0	1	6	8	0	15	158
03:45 PM	0	0	37	0	0	37	0	3	2	1	0	6	0	9	78	7	1	95	0	3	11	8	1	23	161
Total	0	8	123	5	0	136	0	6	15	6	0	27	0	54	294	15	1	364	0	9	25	32	4	70	597
04:00 PM	0	0	37	2	0	39	0	1	6	1	0	8	0	22	85	2	0	109	0	1	8	13	2	24	180
04:15 PM	0	1	35	0	0	36	0	2	5	6	0	13	0	16	96	11	1	124	0	3	10	6	1	20	193
04:30 PM	0	2	37	2	0	41	0	3	5	4	0	12	0	28	126	11	0	165	0	1	7	15	1	24	242
04:45 PM	0	3	32	6	0	41	0	3	3	3	0	9	0	9	102	4	0	115	0	4	11	12	1	28	193
Total	0	6	141	10	0	157	0	9	19	14	0	42	0	75	409	28	1	513	0	9	36	46	5	96	808
05:00 PM	0	0	29	0	0	29	0	4	4	0	1	9	0	22	109	3	1	135	0	2	8	5	0	15	188
05:15 PM	0	0	27	2	0	29	0	1	7	1	0	9	0	19	81	5	0	105	0	4	12	11	0	27	170
05:30 PM	0	1	38	4	0	43	0	2	5	1	0	8	0	24	84	2	1	111	0	3	10	15	1	29	191
05:45 PM	0	0	39	0	0	39	0	0	1	2	0	3	0	17	76	6	0	99	0	1	7	4	0	12	153
Total	0	1	133	6	0	140	0	7	17	4	1	29	0	82	350	16	2	450	0	10	37	35	1	83	702
06:00 PM	0	2	23	1	0	26	0	1	2	1	0	4	0	19	76	3	0	98	0	2	5	7	1	15	143
06:15 PM	0	1	35	2	0	38	0	1	5	2	0	8	0	16	54	6	0	76	0	1	5	8	1	15	137
06:30 PM	0	3	37	3	0	43	0	9	5	1	0	15	0	10	36	4	0	50	0	1	2	9	0	12	120
06:45 PM	0	2	39	2	0	43	0	6	8	0	0	14	0	11	56	1	0	68	0	0	4	6	0	10	135
Total	0	8	134	8	0	150	0	17	20	4	0	41	0	56	222	14	0	292	0	4	16	30	2	52	535
Grand Total	0	61	2083	69	0	2213	1	103	204	56	3	367	0	481	2153	122	7	2763	0	69	204	522	16	811	6154
Apprch %	0	2.8	94.1	3.1	0		0.3	28.1	55.6	15.3	0.8		0	17.4	77.9	4.4	0.3		0	8.5	25.2	64.4	2		
Total %	0	1	33.8	1.1	0	36	0	1.7	3.3	0.9	0	6	0	7.8	35	2	0.1	44.9	0	1.1	3.3	8.5	0.3	13.2	
Cars +	0	56	2039	67	0	2162	1	101	196	52	2	352	0	459	2089	118	3	2669	0	64	196	506	9	775	5958
% Cars +	0	91.8	97.9	97.1	0	97.7	100	98.1	96.1	92.9	66.7	95.9	0	95.4	97	96.7	42.9	96.6	0	92.8	96.1	96.9	56.2	95.6	96.8
Trucks	0	5	44	2	0	51	0	2	8	4	1	15	0	22	64	4	4	94	0	5	8	16	7	36	196
% Trucks	0	8.2	2.1	2.9	0	2.3	0	1.9	3.9	7.1	33.3	4.1	0	4.6	3	3.3	57.1	3.4	0	7.2	3.9	3.1	43.8	4.4	3.2



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PO Box 16296  
St. Louis Park, MN 55416

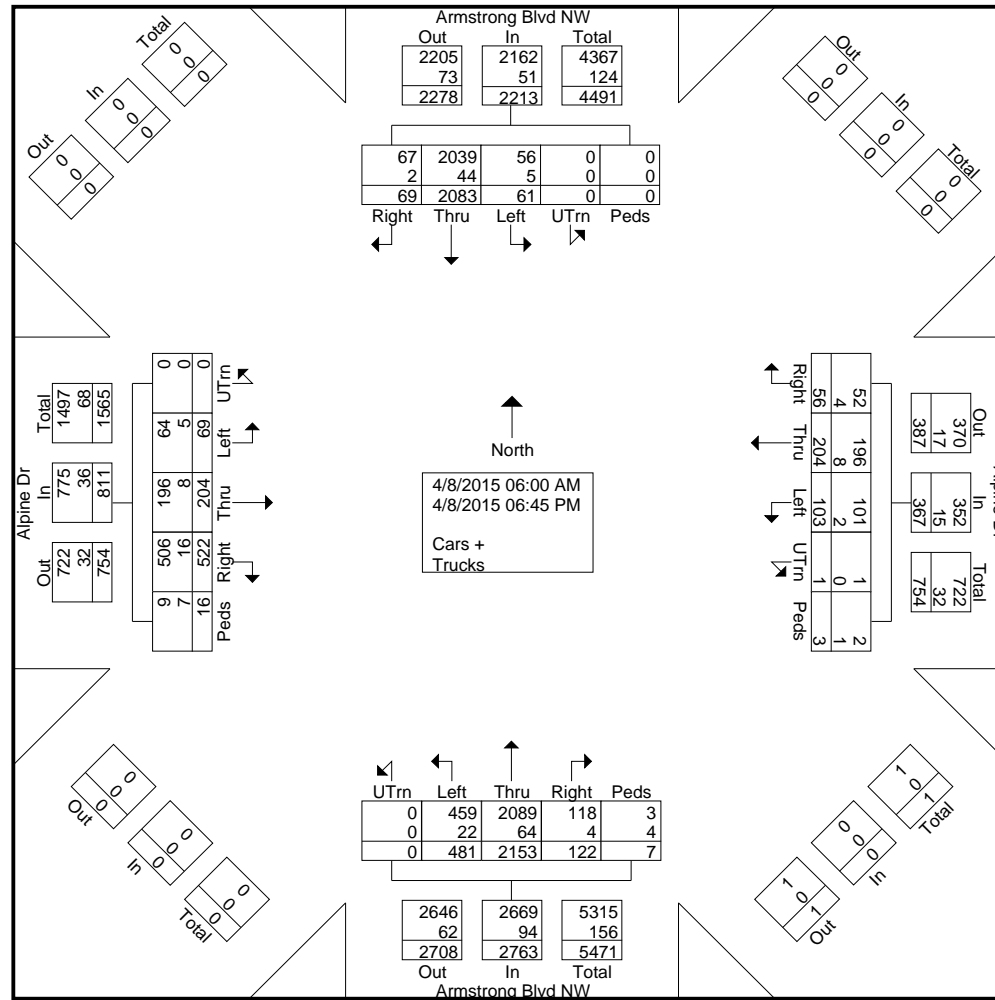
File Name : 2 - Armstrong Blvd NW & Alpine Dr, 4-8-15, 6am-7pm

Site Code : 2

Start Date : 4/8/2015

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Armstrong Blvd NW and Alpine Dr  
Ramsey, MN





# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 2 - Armstrong Blvd NW & Alpine Dr, 4-8-15, 6am-7pm

Site Code : 2

Start Date : 4/8/2015

Page No : 4

## Armstrong Blvd NW and Alpine Dr Ramsey, MN

Start Time	Armstrong Blvd NW Southbound						Alpine Dr Westbound					Armstrong Blvd NW Northbound						Alpine Dr Eastbound					Int. Total		
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right		Peds	App. Total
Peak Hour Analysis From 06:00 AM to 09:45 AM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 06:45 AM																									
06:45 AM	0	3	87	0	0	90	0	0	8	0	0	8	0	1	6	0	0	7	0	1	3	21	0	25	130
07:00 AM	0	2	89	3	0	94	0	0	4	0	0	4	0	4	9	0	0	13	0	1	4	21	0	26	137
07:15 AM	0	0	90	2	0	92	0	2	9	0	0	11	0	1	10	0	0	11	0	0	4	23	0	27	141
07:30 AM	0	0	107	6	0	113	0	1	13	1	0	15	0	2	11	1	0	14	0	3	4	27	0	34	176
Total Volume	0	5	373	11	0	389	0	3	34	1	0	38	0	8	36	1	0	45	0	5	15	92	0	112	584
% App. Total	0	1.3	95.9	2.8	0		0	7.9	89.5	2.6	0		0	17.8	80	2.2	0		0	4.5	13.4	82.1	0		
PHF	.000	.417	.871	.458	.000	.861	.000	.375	.654	.250	.000	.633	.000	.500	.818	.250	.000	.804	.000	.417	.938	.852	.000	.824	.830
Peak Hour Analysis From 10:00 AM to 01:45 PM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 11:15 AM																									
11:15 AM	0	0	33	1	0	34	0	0	3	2	0	5	0	13	55	3	0	71	0	1	2	5	0	8	118
11:30 AM	0	2	28	0	0	30	0	2	1	0	0	3	0	9	29	1	1	40	0	0	4	7	1	12	85
11:45 AM	0	2	26	0	0	28	0	2	1	0	0	3	0	10	28	2	0	40	0	2	1	6	0	9	80
12:00 PM	0	2	33	0	0	35	0	3	6	2	0	11	0	2	25	2	0	29	0	2	3	13	0	18	93
Total Volume	0	6	120	1	0	127	0	7	11	4	0	22	0	34	137	8	1	180	0	5	10	31	1	47	376
% App. Total	0	4.7	94.5	0.8	0		0	31.8	50	18.2	0		0	18.9	76.1	4.4	0.6		0	10.6	21.3	66	2.1		
PHF	.000	.750	.909	.250	.000	.907	.000	.583	.458	.500	.000	.500	.000	.654	.623	.667	.250	.634	.000	.625	.625	.596	.250	.653	.797
Peak Hour Analysis From 02:00 PM to 06:45 PM - Peak 1 of 1																									
Peak Hour for Entire Intersection Begins at 04:15 PM																									
04:15 PM	0	1	35	0	0	36	0	2	5	6	0	13	0	16	96	11	1	124	0	3	10	6	1	20	193
04:30 PM	0	2	37	2	0	41	0	3	5	4	0	12	0	28	126	11	0	165	0	1	7	15	1	24	242
04:45 PM	0	3	32	6	0	41	0	3	3	3	0	9	0	9	102	4	0	115	0	4	11	12	1	28	193
05:00 PM	0	0	29	0	0	29	0	4	4	0	1	9	0	22	109	3	1	135	0	2	8	5	0	15	188
Total Volume	0	6	133	8	0	147	0	12	17	13	1	43	0	75	433	29	2	539	0	10	36	38	3	87	816
% App. Total	0	4.1	90.5	5.4	0		0	27.9	39.5	30.2	2.3		0	13.9	80.3	5.4	0.4		0	11.5	41.4	43.7	3.4		
PHF	.000	.500	.899	.333	.000	.896	.000	.750	.850	.542	.250	.827	.000	.670	.859	.659	.500	.817	.000	.625	.818	.633	.750	.777	.843



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 3 - Armstrong Blvd NW & Bunker Lake Blvd, 4-8-15, 6am-7pm

Site Code : 3

Start Date : 4/8/2015

Page No : 1

Armstrong Blvd NW and Bunker Lake Blvd  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						Bunker Lake Blvd Westbound						Armstrong Blvd NW Northbound						Bunker Lake Blvd Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
06:00 AM	0	12	59	0	0	71	0	1	0	5	0	6	0	0	4	2	0	6	0	0	0	0	0	0	83
06:15 AM	0	24	61	0	0	85	0	0	0	3	0	3	0	0	12	1	0	13	0	0	0	0	0	0	101
06:30 AM	0	37	66	0	2	105	0	3	0	5	2	10	0	0	4	4	0	8	0	0	0	1	0	1	124
06:45 AM	0	50	63	0	0	113	0	1	0	4	0	5	0	0	4	4	0	8	0	0	0	0	0	0	126
Total	0	123	249	0	2	374	0	5	0	17	2	24	0	0	24	11	0	35	0	0	0	1	0	1	434
07:00 AM	0	42	66	0	0	108	0	3	0	7	0	10	0	0	7	1	0	8	0	0	0	0	0	0	126
07:15 AM	0	50	66	0	0	116	0	4	0	7	0	11	0	0	5	2	0	7	0	0	1	0	0	1	135
07:30 AM	0	71	74	0	0	145	0	7	1	6	0	14	0	0	10	7	0	17	0	0	1	1	0	2	178
07:45 AM	0	48	52	0	0	100	0	6	0	7	0	13	0	0	15	7	0	22	0	0	1	0	0	1	136
Total	0	211	258	0	0	469	0	20	1	27	0	48	0	0	37	17	0	54	0	0	3	1	0	4	575
08:00 AM	0	22	60	0	0	82	0	1	1	13	0	15	0	0	10	3	0	13	0	0	1	0	0	1	111
08:15 AM	0	21	49	0	0	70	0	3	1	11	0	15	0	0	16	6	0	22	0	0	1	0	0	1	108
08:30 AM	0	32	55	0	0	87	0	1	0	5	0	6	0	0	16	0	0	16	0	0	0	1	0	1	110
08:45 AM	0	7	44	0	0	51	0	6	0	14	0	20	1	0	11	1	0	13	0	0	1	0	0	1	85
Total	0	82	208	0	0	290	0	11	2	43	0	56	1	0	53	10	0	64	0	0	3	1	0	4	414
09:00 AM	0	17	44	0	0	61	0	4	0	5	0	9	0	0	17	1	0	18	0	0	1	0	0	1	89
09:15 AM	0	5	43	0	0	48	0	2	0	15	0	17	0	0	18	1	0	19	0	0	0	0	0	0	84
09:30 AM	0	11	38	0	0	49	0	5	1	7	0	13	0	0	19	2	0	21	0	0	0	0	0	0	83
09:45 AM	0	10	31	0	0	41	0	3	0	8	0	11	0	0	26	1	0	27	0	0	0	0	0	0	79
Total	0	43	156	0	0	199	0	14	1	35	0	50	0	0	80	5	0	85	0	0	1	0	0	1	335
10:00 AM	0	7	36	0	0	43	0	2	1	12	0	15	0	0	19	1	0	20	0	0	0	0	0	0	78
10:15 AM	0	8	24	0	0	32	1	3	1	5	1	11	0	0	23	2	0	25	0	0	0	0	0	0	68
10:30 AM	0	5	32	2	0	39	0	4	0	11	0	15	0	2	17	3	0	22	0	0	0	1	0	1	77
10:45 AM	0	10	30	0	0	40	0	6	0	11	0	17	0	0	14	0	0	14	0	0	0	0	0	0	71
Total	0	30	122	2	0	154	1	15	2	39	1	58	0	2	73	6	0	81	0	0	0	1	0	1	294
11:00 AM	0	11	38	0	1	50	1	4	0	11	1	17	0	0	27	2	1	30	0	0	0	0	1	1	98
11:15 AM	0	13	29	0	0	42	0	5	1	12	1	19	0	0	28	2	0	30	0	0	0	1	0	1	92
11:30 AM	0	6	34	0	0	40	0	7	0	15	0	22	0	1	32	2	0	35	0	0	0	0	0	0	97
11:45 AM	0	3	37	0	0	40	0	3	0	11	1	15	0	0	36	3	0	39	0	0	0	0	0	0	94
Total	0	33	138	0	1	172	1	19	1	49	3	73	0	1	123	9	1	134	0	0	0	1	1	2	381
12:00 PM	1	10	30	0	0	41	0	1	0	6	0	7	0	1	25	6	0	32	0	0	0	2	0	2	82
12:15 PM	0	8	28	0	0	36	0	4	0	16	0	20	1	0	22	2	0	25	0	0	0	0	0	0	81
12:30 PM	0	9	33	0	0	42	0	4	0	9	0	13	0	1	23	2	0	26	0	0	0	0	0	0	81
12:45 PM	0	12	29	0	1	42	0	5	0	18	1	24	0	0	30	7	1	38	0	0	0	0	0	0	104
Total	1	39	120	0	1	161	0	14	0	49	1	64	1	2	100	17	1	121	0	0	0	2	0	2	348
01:00 PM	0	10	27	0	0	37	0	4	0	11	0	15	0	0	27	5	0	32	0	0	0	0	0	0	84
01:15 PM	0	11	28	0	1	40	0	4	0	12	2	18	0	3	30	1	1	35	0	0	0	2	1	3	96
01:30 PM	0	10	26	0	0	36	0	3	1	9	0	13	0	0	31	2	0	33	0	0	2	0	0	2	84
01:45 PM	0	4	31	0	0	35	0	2	1	14	1	18	1	0	32	2	1	36	0	0	0	0	0	0	89
Total	0	35	112	0	1	148	0	13	2	46	3	64	1	3	120	10	2	136	0	0	2	2	1	5	353



# Traffic Data Inc

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File Name : 3 - Armstrong Blvd NW & Bunker Lake Blvd, 4-8-15, 6am-7pm

Site Code : 3

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Page No : 2

Armstrong Blvd NW and Bunker Lake Blvd  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						Bunker Lake Blvd Westbound						Armstrong Blvd NW Northbound						Bunker Lake Blvd Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
02:00 PM	0	10	30	0	0	40	0	5	0	18	0	23	0	0	40	3	0	43	0	0	0	0	0	0	106
02:15 PM	0	11	31	0	0	42	0	6	0	13	0	19	0	0	45	10	0	55	0	0	0	1	0	1	117
02:30 PM	0	8	39	0	0	47	0	4	0	28	0	32	0	1	41	5	0	47	0	1	0	0	0	1	127
02:45 PM	0	13	19	0	0	32	0	4	3	26	0	33	0	0	58	5	0	63	0	0	0	0	0	0	128
Total	0	42	119	0	0	161	0	19	3	85	0	107	0	1	184	23	0	208	0	1	0	1	0	2	478
03:00 PM	0	13	23	0	0	36	0	7	0	49	0	56	0	0	50	7	0	57	0	0	0	0	0	0	149
03:15 PM	0	9	33	0	0	42	0	5	1	38	0	44	0	0	49	3	0	52	0	0	0	0	0	0	138
03:30 PM	0	13	34	0	3	50	0	16	1	46	0	63	0	0	53	5	0	58	0	0	0	0	2	2	173
03:45 PM	0	15	36	0	0	51	0	6	0	37	0	43	2	0	63	5	0	70	0	0	0	0	0	0	164
Total	0	50	126	0	3	179	0	34	2	170	0	206	2	0	215	20	0	237	0	0	0	0	2	2	624
04:00 PM	0	17	27	0	0	44	0	5	0	45	1	51	0	0	65	4	0	69	0	0	1	0	0	1	165
04:15 PM	0	12	37	0	0	49	0	14	0	50	0	64	0	0	80	2	1	83	0	0	0	0	1	1	197
04:30 PM	0	15	43	0	1	59	0	17	0	77	2	96	1	0	90	6	0	97	0	0	0	1	1	2	254
04:45 PM	0	14	38	0	1	53	0	8	2	39	1	50	0	0	82	5	2	89	0	0	0	0	2	2	194
Total	0	58	145	0	2	205	0	44	2	211	4	261	1	0	317	17	3	338	0	0	1	1	4	6	810
05:00 PM	0	9	31	1	0	41	0	7	1	56	0	64	0	0	86	4	0	90	0	0	0	0	0	0	195
05:15 PM	0	8	32	0	0	40	0	9	2	27	2	40	0	0	87	7	0	94	0	0	0	0	0	0	174
05:30 PM	0	14	43	0	1	58	0	4	0	42	0	46	0	0	81	3	1	85	0	0	0	0	1	1	190
05:45 PM	1	14	33	0	0	48	0	1	2	20	0	23	0	1	77	5	1	84	0	1	0	0	0	1	156
Total	1	45	139	1	1	187	0	21	5	145	2	173	0	1	331	19	2	353	0	1	0	0	1	2	715
06:00 PM	0	10	28	0	2	40	0	5	1	33	2	41	0	0	68	3	1	72	0	0	1	0	1	2	155
06:15 PM	0	13	33	1	1	48	0	5	0	24	2	31	0	0	56	3	6	65	0	0	0	0	5	5	149
06:30 PM	0	9	36	0	0	45	0	4	0	17	0	21	0	0	37	4	0	41	0	0	0	0	0	0	107
06:45 PM	0	9	38	0	0	47	0	1	0	17	0	18	0	0	58	2	0	60	0	0	1	0	0	1	126
Total	0	41	135	1	3	180	0	15	1	91	4	111	0	0	219	12	7	238	0	0	2	0	6	8	537
Grand Total	2	832	2027	4	14	2879	2	244	22	1007	20	1295	6	10	1876	176	16	2084	0	2	12	11	15	40	6298
Apprch %	0.1	28.9	70.4	0.1	0.5		0.2	18.8	1.7	77.8	1.5		0.3	0.5	90	8.4	0.8		0	5	30	27.5	37.5		
Total %	0	13.2	32.2	0.1	0.2	45.7	0	3.9	0.3	16	0.3	20.6	0.1	0.2	29.8	2.8	0.3	33.1	0	0	0.2	0.2	0.2	0.6	
Cars +	2	807	1984	4	13	2810	2	231	21	972	15	1241	6	10	1838	168	13	2035	0	2	12	11	9	34	6120
% Cars +	100	97	97.9	100	92.9	97.6	100	94.7	95.5	96.5	75	95.8	100	100	98	95.5	81.2	97.6	0	100	100	100	60	85	97.2
Trucks	0	25	43	0	1	69	0	13	1	35	5	54	0	0	38	8	3	49	0	0	0	0	6	6	178
% Trucks	0	3	2.1	0	7.1	2.4	0	5.3	4.5	3.5	25	4.2	0	0	2	4.5	18.8	2.4	0	0	0	0	40	15	2.8



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

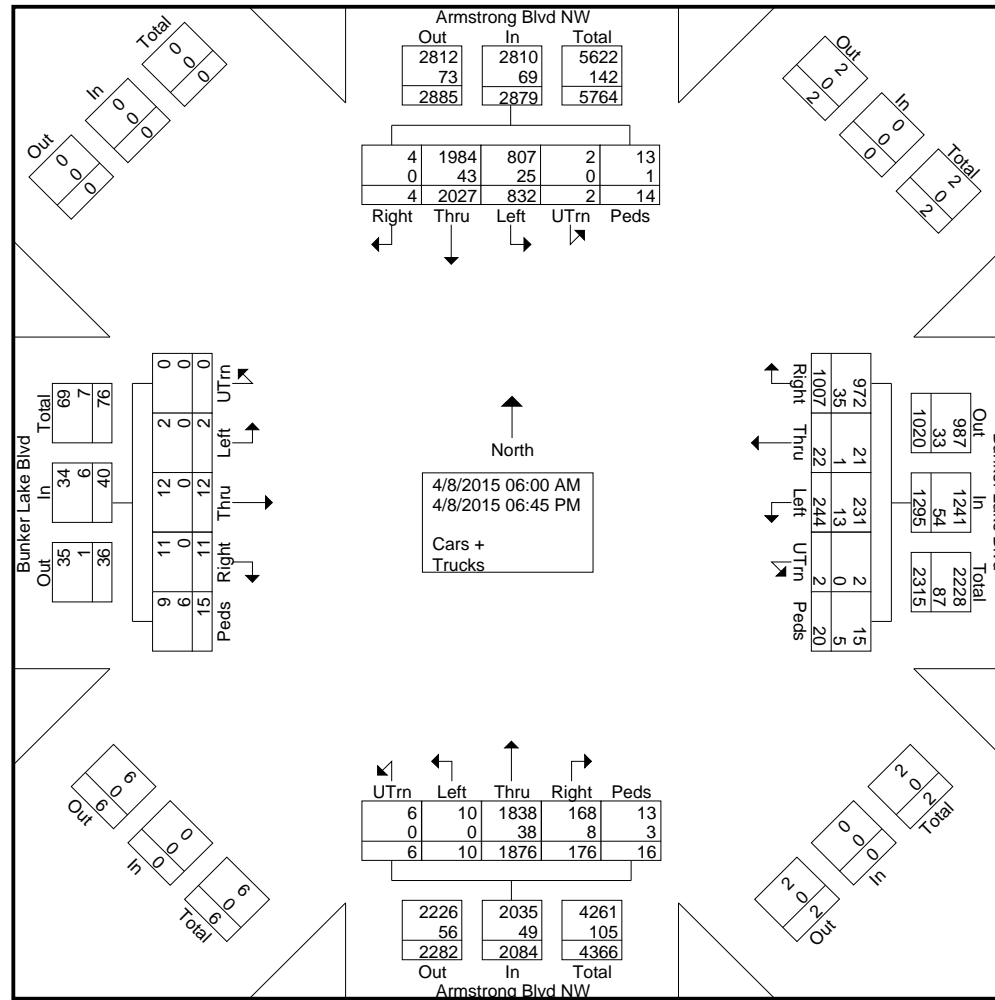
File Name : 3 - Armstrong Blvd NW & Bunker Lake Blvd, 4-8-15, 6am-7pm

Site Code : 3

Start Date : 4/8/2015

Page No : 3

Armstrong Blvd NW and Bunker Lake Blvd  
Ramsey, MN





# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 3 - Armstrong Blvd NW & Bunker Lake Blvd, 4-8-15, 6am-7pm

Site Code : 3

Start Date : 4/8/2015

Page No : 4

## Armstrong Blvd NW and Bunker Lake Blvd Ramsey, MN

Start Time	Armstrong Blvd NW Southbound						Bunker Lake Blvd Westbound					Armstrong Blvd NW Northbound						Bunker Lake Blvd Eastbound					Int. Total			
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right		Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 09:45 AM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 07:00 AM																										
07:00 AM	0	42	66	0	0	108	0	3	0	7	0	10	0	0	7	1	0	8	0	0	0	0	0	0	0	126
07:15 AM	0	50	66	0	0	116	0	4	0	7	0	11	0	0	5	2	0	7	0	0	1	0	0	0	1	135
07:30 AM	0	71	74	0	0	145	0	7	1	6	0	14	0	0	10	7	0	17	0	0	1	1	0	0	2	178
07:45 AM	0	48	52	0	0	100	0	6	0	7	0	13	0	0	15	7	0	22	0	0	1	0	0	0	1	136
Total Volume	0	211	258	0	0	469	0	20	1	27	0	48	0	0	37	17	0	54	0	0	3	1	0	0	4	575
% App. Total	0	45	55	0	0		0	41.7	2.1	56.2	0		0	0	68.5	31.5	0		0	0	75	25	0			
PHF	.000	.743	.872	.000	.000	.809	.000	.714	.250	.964	.000	.857	.000	.000	.617	.607	.000	.614	.000	.000	.750	.250	.000	.500	.808	
Peak Hour Analysis From 10:00 AM to 01:45 PM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 11:00 AM																										
11:00 AM	0	11	38	0	1	50	1	4	0	11	1	17	0	0	27	2	1	30	0	0	0	0	1	1	1	98
11:15 AM	0	13	29	0	0	42	0	5	1	12	1	19	0	0	28	2	0	30	0	0	0	1	0	0	1	92
11:30 AM	0	6	34	0	0	40	0	7	0	15	0	22	0	1	32	2	0	35	0	0	0	0	0	0	0	97
11:45 AM	0	3	37	0	0	40	0	3	0	11	1	15	0	0	36	3	0	39	0	0	0	0	0	0	0	94
Total Volume	0	33	138	0	1	172	1	19	1	49	3	73	0	1	123	9	1	134	0	0	0	1	1	1	2	381
% App. Total	0	19.2	80.2	0	0.6		1.4	26	1.4	67.1	4.1		0	0.7	91.8	6.7	0.7		0	0	0	50	50			
PHF	.000	.635	.908	.000	.250	.860	.250	.679	.250	.817	.750	.830	.000	.250	.854	.750	.250	.859	.000	.000	.000	.250	.250	.500	.972	
Peak Hour Analysis From 02:00 PM to 06:45 PM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 04:15 PM																										
04:15 PM	0	12	37	0	0	49	0	14	0	50	0	64	0	0	80	2	1	83	0	0	0	0	1	1	1	197
04:30 PM	0	15	43	0	1	59	0	17	0	77	2	96	1	0	90	6	0	97	0	0	0	1	1	1	2	254
04:45 PM	0	14	38	0	1	53	0	8	2	39	1	50	0	0	82	5	2	89	0	0	0	0	2	2	2	194
05:00 PM	0	9	31	1	0	41	0	7	1	56	0	64	0	0	86	4	0	90	0	0	0	0	0	0	0	195
Total Volume	0	50	149	1	2	202	0	46	3	222	3	274	1	0	338	17	3	359	0	0	0	1	4	5	840	
% App. Total	0	24.8	73.8	0.5	1		0	16.8	1.1	81	1.1		0.3	0	94.2	4.7	0.8		0	0	0	20	80			
PHF	.000	.833	.866	.250	.500	.856	.000	.676	.375	.721	.375	.714	.250	.000	.939	.708	.375	.925	.000	.000	.000	.250	.500	.625	.827	



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 4 - Armstrong Blvd NW & 147th Ave NW, 4-8-15, 6am-7pm

Site Code : 4

Start Date : 4/8/2015

Page No : 1

Armstrong Blvd NW and 147th Ave NW  
Ramsey, MN

## Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						147th Ave NW Westbound						Armstrong Blvd NW Northbound						147th Ave NW Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
06:00 AM	0	10	53	0	0	63	0	3	0	1	0	4	0	0	7	7	0	14	0	0	0	0	0	0	81
06:15 AM	0	10	51	0	0	61	0	6	0	1	1	8	0	0	12	9	0	21	0	0	0	0	0	0	90
06:30 AM	0	8	61	1	0	70	0	8	1	0	0	9	0	1	9	13	0	23	0	0	1	0	0	1	103
06:45 AM	0	13	50	0	0	63	0	8	1	1	0	10	0	3	7	14	0	24	0	0	0	0	0	0	97
Total	0	41	215	1	0	257	0	25	2	3	1	31	0	4	35	43	0	82	0	0	1	0	0	1	371
07:00 AM	0	13	57	0	0	70	0	4	0	2	0	6	0	2	6	8	0	16	0	0	0	0	0	0	92
07:15 AM	0	29	43	0	0	72	0	6	0	1	0	7	0	0	6	26	0	32	0	0	0	0	0	0	111
07:30 AM	0	20	60	0	0	80	0	18	0	4	0	22	0	1	12	27	0	40	0	0	1	2	0	3	145
07:45 AM	1	17	39	1	0	58	0	7	0	3	0	10	0	1	18	22	0	41	0	0	0	0	0	0	109
Total	1	79	199	1	0	280	0	35	0	10	0	45	0	4	42	83	0	129	0	0	1	2	0	3	457
08:00 AM	0	13	45	4	0	62	0	7	1	4	0	12	0	0	9	17	0	26	0	0	1	0	0	1	101
08:15 AM	0	9	37	1	0	47	0	7	1	5	0	13	0	1	20	16	0	37	0	0	0	2	0	2	99
08:30 AM	0	14	42	0	0	56	0	8	0	5	0	13	0	0	8	12	0	20	0	1	0	1	0	2	91
08:45 AM	0	13	38	0	0	51	0	7	1	6	0	14	0	2	8	20	0	30	0	0	1	0	0	1	96
Total	0	49	162	5	0	216	0	29	3	20	0	52	0	3	45	65	0	113	0	1	2	3	0	6	387
09:00 AM	0	14	34	1	0	49	0	5	0	4	0	9	0	2	12	6	0	20	0	1	0	2	0	3	81
09:15 AM	0	10	36	0	0	46	0	4	0	7	0	11	0	0	13	17	0	30	0	0	0	1	0	1	88
09:30 AM	0	6	35	1	0	42	0	15	0	9	0	24	0	0	11	19	0	30	0	0	0	4	0	4	100
09:45 AM	0	7	26	0	0	33	0	10	0	7	0	17	0	1	22	12	0	35	0	0	0	0	0	0	85
Total	0	37	131	2	0	170	0	34	0	27	0	61	0	3	58	54	0	115	0	1	0	7	0	8	354
10:00 AM	0	13	25	1	0	39	0	6	0	5	0	11	0	0	14	19	0	33	0	0	0	0	0	0	83
10:15 AM	0	3	24	0	0	27	0	12	2	7	0	21	0	0	19	17	0	36	0	0	2	1	0	3	87
10:30 AM	0	12	24	0	0	36	0	13	1	6	0	20	0	0	17	12	0	29	0	0	0	0	0	0	85
10:45 AM	0	9	26	0	0	35	0	15	1	0	0	16	0	0	14	17	0	31	0	0	0	1	0	1	83
Total	0	37	99	1	0	137	0	46	4	18	0	68	0	0	64	65	0	129	0	0	2	2	0	4	338
11:00 AM	0	16	27	0	0	43	0	10	2	9	1	22	0	0	20	22	0	42	0	0	0	0	0	0	107
11:15 AM	0	10	26	0	0	36	0	10	1	10	1	22	0	2	21	22	0	45	0	0	3	0	0	3	106
11:30 AM	1	14	24	1	0	40	0	10	1	12	0	23	0	2	19	20	0	41	0	0	1	2	0	3	107
11:45 AM	0	14	21	1	0	36	0	17	0	9	0	26	0	1	29	27	0	57	0	1	0	1	0	2	121
Total	1	54	98	2	0	155	0	47	4	40	2	93	0	5	89	91	0	185	0	1	4	3	0	8	441
12:00 PM	0	12	23	1	0	36	0	15	0	8	0	23	0	1	22	27	0	50	0	1	0	2	0	3	112
12:15 PM	0	6	26	0	0	32	0	16	0	8	0	24	0	1	20	24	0	45	0	0	1	1	0	2	103
12:30 PM	0	14	24	0	0	38	0	17	1	8	0	26	0	0	16	21	0	37	0	0	0	0	0	0	101
12:45 PM	0	10	23	0	0	33	0	14	1	10	0	25	0	0	28	23	0	51	0	0	0	2	0	2	111
Total	0	42	96	1	0	139	0	62	2	34	0	98	0	2	86	95	0	183	0	1	1	5	0	7	427
01:00 PM	0	7	24	1	0	32	1	12	0	7	0	20	0	0	25	16	0	41	0	0	0	0	0	0	93
01:15 PM	0	14	18	1	0	33	0	8	0	9	0	17	0	2	23	21	0	46	0	1	0	1	0	2	98
01:30 PM	0	7	23	0	0	30	0	14	0	12	0	26	0	0	21	25	0	46	0	0	0	1	0	1	103
01:45 PM	0	12	22	0	0	34	0	22	0	16	1	39	0	0	18	35	0	53	0	0	2	1	0	3	129
Total	0	40	87	2	0	129	1	56	0	44	1	102	0	2	87	97	0	186	0	1	2	3	0	6	423



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 4 - Armstrong Blvd NW & 147th Ave NW, 4-8-15, 6am-7pm

Site Code : 4

Start Date : 4/8/2015

Page No : 2

Armstrong Blvd NW and 147th Ave NW  
Ramsey, MN

### Groups Printed- Cars + - Trucks

Start Time	Armstrong Blvd NW Southbound						147th Ave NW Westbound						Armstrong Blvd NW Northbound						147th Ave NW Eastbound						Int. Total
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	
02:00 PM	0	8	26	0	0	34	0	17	1	13	0	31	0	2	29	26	0	57	0	0	0	2	0	2	124
02:15 PM	0	13	25	1	0	39	0	9	1	15	0	25	0	0	44	26	0	70	0	0	0	0	0	0	134
02:30 PM	0	12	31	0	0	43	0	13	0	7	0	20	0	1	37	28	0	66	0	0	0	0	0	0	129
02:45 PM	0	8	13	1	0	22	0	10	0	18	0	28	0	0	45	27	0	72	0	1	0	0	0	1	123
Total	0	41	95	2	0	138	0	49	2	53	0	104	0	3	155	107	0	265	0	1	0	2	0	3	510
03:00 PM	0	4	26	0	0	30	0	20	0	16	0	36	0	0	42	37	0	79	0	1	0	0	0	1	146
03:15 PM	0	12	25	0	0	37	0	19	0	12	0	31	0	1	37	28	0	66	0	1	0	1	0	2	136
03:30 PM	0	20	31	1	2	54	0	13	1	12	0	26	0	1	50	29	0	80	0	0	1	0	0	1	161
03:45 PM	0	15	27	0	0	42	0	20	0	20	0	40	0	0	50	34	0	84	0	0	0	1	0	1	167
Total	0	51	109	1	2	163	0	72	1	60	0	133	0	2	179	128	0	309	0	2	1	2	0	5	610
04:00 PM	0	11	24	0	0	35	0	12	0	22	0	34	0	2	49	28	0	79	0	0	0	1	0	1	149
04:15 PM	0	13	36	0	0	49	0	29	0	28	0	57	0	0	55	31	0	86	0	0	0	1	1	2	194
04:30 PM	0	11	49	1	1	62	0	26	0	30	1	57	0	1	70	25	0	96	0	3	0	0	0	3	218
04:45 PM	0	9	40	0	0	49	0	24	0	19	2	45	0	0	61	36	0	97	0	0	0	0	0	0	191
Total	0	44	149	1	1	195	0	91	0	99	3	193	0	3	235	120	0	358	0	3	0	2	1	6	752
05:00 PM	0	11	27	0	0	38	0	25	0	19	0	44	0	2	68	27	0	97	0	0	0	4	0	4	183
05:15 PM	0	11	31	0	2	44	0	15	0	28	0	43	0	2	71	41	0	114	0	1	0	2	0	3	204
05:30 PM	1	17	27	0	0	45	0	21	0	22	0	43	0	0	59	28	0	87	0	0	0	0	0	0	175
05:45 PM	0	10	26	0	0	36	0	16	0	26	1	43	0	0	51	38	0	89	0	1	0	3	0	4	172
Total	1	49	111	0	2	163	0	77	0	95	1	173	0	4	249	134	0	387	0	2	0	9	0	11	734
06:00 PM	0	9	22	0	0	31	0	20	0	18	1	39	0	0	59	35	0	94	0	0	0	0	0	0	164
06:15 PM	0	14	27	0	0	41	0	20	0	21	1	42	0	0	38	27	0	65	0	0	0	0	5	5	153
06:30 PM	0	10	30	0	0	40	0	14	0	10	1	25	0	0	28	23	0	51	0	0	0	0	0	0	116
06:45 PM	0	13	26	0	0	39	0	10	0	19	0	29	0	0	46	19	0	65	0	0	0	0	0	0	133
Total	0	46	105	0	0	151	0	64	0	68	3	135	0	0	171	104	0	275	0	0	0	0	5	5	566
Grand Total	3	610	1656	19	5	2293	1	687	18	571	11	1288	0	35	1495	1186	0	2716	0	13	14	40	6	73	6370
Apprch %	0.1	26.6	72.2	0.8	0.2		0.1	53.3	1.4	44.3	0.9		0	1.3	55	43.7	0		0	17.8	19.2	54.8	8.2		
Total %	0	9.6	26	0.3	0.1	36	0	10.8	0.3	9	0.2	20.2	0	0.5	23.5	18.6	0	42.6	0	0.2	0.2	0.6	0.1	1.1	
Cars +	3	605	1619	15	2	2244	1	672	17	568	8	1266	0	31	1449	1167	0	2647	0	11	14	36	1	62	6219
% Cars +	100	99.2	97.8	78.9	40	97.9	100	97.8	94.4	99.5	72.7	98.3	0	88.6	96.9	98.4	0	97.5	0	84.6	100	90	16.7	84.9	97.6
Trucks	0	5	37	4	3	49	0	15	1	3	3	22	0	4	46	19	0	69	0	2	0	4	5	11	151
% Trucks	0	0.8	2.2	21.1	60	2.1	0	2.2	5.6	0.5	27.3	1.7	0	11.4	3.1	1.6	0	2.5	0	15.4	0	10	83.3	15.1	2.4



# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

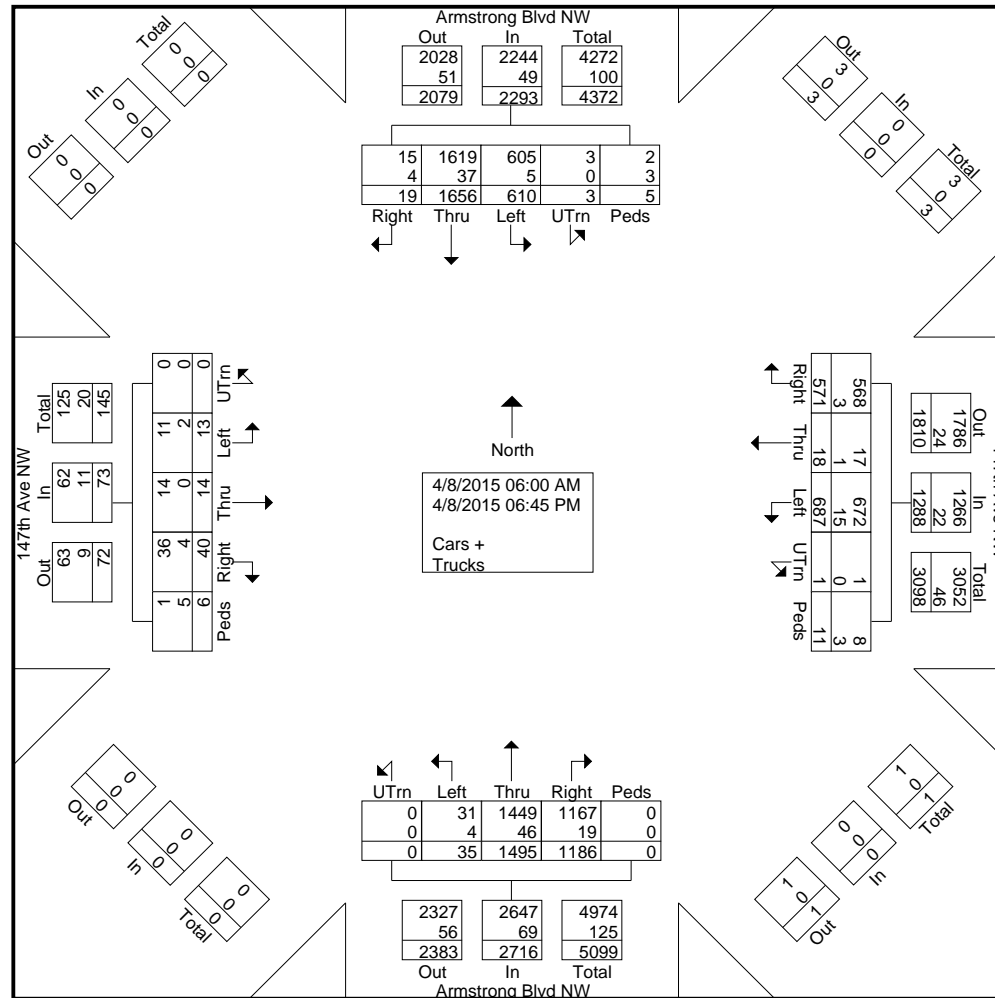
File Name : 4 - Armstrong Blvd NW & 147th Ave NW, 4-8-15, 6am-7pm

Site Code : 4

Start Date : 4/8/2015

Page No : 3

Armstrong Blvd NW and 147th Ave NW  
Ramsey, MN





# Traffic Data Inc

PO Box 16296  
St. Louis Park, MN 55416

File Name : 4 - Armstrong Blvd NW & 147th Ave NW, 4-8-15, 6am-7pm

Site Code : 4

Start Date : 4/8/2015

Page No : 4

Armstrong Blvd NW and 147th Ave NW  
Ramsey, MN

Start Time	Armstrong Blvd NW Southbound						147th Ave NW Westbound					Armstrong Blvd NW Northbound					147th Ave NW Eastbound					Int. Total				
	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru	Right	Peds	App. Total	UTrn	Left	Thru		Right	Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 09:45 AM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 07:15 AM																										
07:15 AM	0	29	43	0	0	72	0	6	0	1	0	7	0	0	6	26	0	32	0	0	0	0	0	0	0	111
07:30 AM	0	20	60	0	0	80	0	18	0	4	0	22	0	1	12	27	0	40	0	0	1	2	0	0	3	145
07:45 AM	1	17	39	1	0	58	0	7	0	3	0	10	0	1	18	22	0	41	0	0	0	0	0	0	109	
08:00 AM	0	13	45	4	0	62	0	7	1	4	0	12	0	0	9	17	0	26	0	0	1	0	0	0	1	101
Total Volume	1	79	187	5	0	272	0	38	1	12	0	51	0	2	45	92	0	139	0	0	2	2	0	0	4	466
% App. Total	0.4	29	68.8	1.8	0		0	74.5	2	23.5	0		0	1.4	32.4	66.2	0		0	0	50	50	0			
PHF	.250	.681	.779	.313	.000	.850	.000	.528	.250	.750	.000	.580	.000	.500	.625	.852	.000	.848	.000	.000	.500	.250	.000	.333	.803	
Peak Hour Analysis From 10:00 AM to 01:45 PM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 11:15 AM																										
11:15 AM	0	10	26	0	0	36	0	10	1	10	1	22	0	2	21	22	0	45	0	0	3	0	0	0	3	106
11:30 AM	1	14	24	1	0	40	0	10	1	12	0	23	0	2	19	20	0	41	0	0	1	2	0	0	3	107
11:45 AM	0	14	21	1	0	36	0	17	0	9	0	26	0	1	29	27	0	57	0	1	0	1	0	0	2	121
12:00 PM	0	12	23	1	0	36	0	15	0	8	0	23	0	1	22	27	0	50	0	1	0	2	0	0	3	112
Total Volume	1	50	94	3	0	148	0	52	2	39	1	94	0	6	91	96	0	193	0	2	4	5	0	0	11	446
% App. Total	0.7	33.8	63.5	2	0		0	55.3	2.1	41.5	1.1		0	3.1	47.2	49.7	0		0	18.2	36.4	45.5	0			
PHF	.250	.893	.904	.750	.000	.925	.000	.765	.500	.813	.250	.904	.000	.750	.784	.889	.000	.846	.000	.500	.333	.625	.000	.917	.921	
Peak Hour Analysis From 02:00 PM to 06:45 PM - Peak 1 of 1																										
Peak Hour for Entire Intersection Begins at 04:30 PM																										
04:30 PM	0	11	49	1	1	62	0	26	0	30	1	57	0	1	70	25	0	96	0	3	0	0	0	0	3	218
04:45 PM	0	9	40	0	0	49	0	24	0	19	2	45	0	0	61	36	0	97	0	0	0	0	0	0	0	191
05:00 PM	0	11	27	0	0	38	0	25	0	19	0	44	0	2	68	27	0	97	0	0	0	4	0	0	4	183
05:15 PM	0	11	31	0	2	44	0	15	0	28	0	43	0	2	71	41	0	114	0	1	0	2	0	0	3	204
Total Volume	0	42	147	1	3	193	0	90	0	96	3	189	0	5	270	129	0	404	0	4	0	6	0	0	10	796
% App. Total	0	21.8	76.2	0.5	1.6		0	47.6	0	50.8	1.6		0	1.2	66.8	31.9	0		0	40	0	60	0			
PHF	.000	.955	.750	.250	.375	.778	.000	.865	.000	.800	.375	.829	.000	.625	.951	.787	.000	.886	.000	.333	.000	.375	.000	.625	.913	



## APPENDIX C- CRASH REPORTS



# Crash Detail Report

Alpine St & Armstrong (2010-2015)

Report Version 1.0 March 2010

<b>Crash ID:</b> 102370213	<b>Date:</b> 08/25/2010	<b>Time:</b> 1806	<b>Sys:</b> 04-CSAH
<b>County:</b> ANOKA	<b>City:</b> RAMSEY		<b>Route:</b> 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE <b>Road Type:</b> 2 LANES UNDIV 2_WAY <b>Road Char:</b> STRAIGHT AND LEVEL <b>Crash Type:</b> COLL W/MV IN TRANSPORT <b>Surf Cond:</b> DRY <b>Light Cond:</b> DAYLIGHT <b>Weather 1:</b> CLEAR <b>Weather 2:</b> CLEAR	<b>First Event:</b> ON ROADWAY <b>To Junction:</b> INTERSECTION-RELATED <b>Traffic Device:</b> STOP SIGN OTHER <b>Speed Limit:</b> 55 <b>Diagram:</b> OTHER <b>Officer:</b> <b>Reliability:</b> CONFIDENT <b># of Vehicles:</b> 2.00
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<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 1</th></tr> <tr><td>Trav Dir: W</td></tr> <tr><td>Veh Act: STRAIGHT AHEAD</td></tr> <tr><td>Veh Type: PASSENGER CAR</td></tr> <tr><td>Age: 44</td></tr> <tr><td>Gender: M</td></tr> <tr><td>Cond: NORMAL</td></tr> <tr><td>Cont Fact: FAIL TO YIELD ROW</td></tr> <tr><td>Cont Fact: DISREGARD TRAFFIC DEVICE</td></tr> </table>	Unit 1	Trav Dir: W	Veh Act: STRAIGHT AHEAD	Veh Type: PASSENGER CAR	Age: 44	Gender: M	Cond: NORMAL	Cont Fact: FAIL TO YIELD ROW	Cont Fact: DISREGARD TRAFFIC DEVICE	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 2</th></tr> <tr><td>Trav Dir: N</td></tr> <tr><td>Veh Act: STRAIGHT AHEAD</td></tr> <tr><td>Veh Type: VAN OR MINIVAN</td></tr> <tr><td>Age: 27</td></tr> <tr><td>Gender: M</td></tr> <tr><td>Cond: NORMAL</td></tr> <tr><td>Cont Fact: NO IMPROPER DRIVING</td></tr> <tr><td>Cont Fact: UNKNOWN</td></tr> </table>	Unit 2	Trav Dir: N	Veh Act: STRAIGHT AHEAD	Veh Type: VAN OR MINIVAN	Age: 27	Gender: M	Cond: NORMAL	Cont Fact: NO IMPROPER DRIVING	Cont Fact: UNKNOWN	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 3</th></tr> <tr><td> </td></tr> </table>	Unit 3	
Unit 1																						
Trav Dir: W																						
Veh Act: STRAIGHT AHEAD																						
Veh Type: PASSENGER CAR																						
Age: 44																						
Gender: M																						
Cond: NORMAL																						
Cont Fact: FAIL TO YIELD ROW																						
Cont Fact: DISREGARD TRAFFIC DEVICE																						
Unit 2																						
Trav Dir: N																						
Veh Act: STRAIGHT AHEAD																						
Veh Type: VAN OR MINIVAN																						
Age: 27																						
Gender: M																						
Cond: NORMAL																						
Cont Fact: NO IMPROPER DRIVING																						
Cont Fact: UNKNOWN																						
Unit 3																						

<b>Crash ID:</b> 102370225	<b>Date:</b> 08/25/2010	<b>Time:</b> 1942	<b>Sys:</b> 04-CSAH
<b>County:</b> ANOKA	<b>City:</b> RAMSEY		<b>Route:</b> 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE <b>Road Type:</b> 2 LANES UNDIV 2_WAY <b>Road Char:</b> STRAIGHT AND LEVEL <b>Crash Type:</b> COLL W/MV IN TRANSPORT <b>Surf Cond:</b> DRY <b>Light Cond:</b> DAYLIGHT <b>Weather 1:</b> CLEAR <b>Weather 2:</b> NOT SPECIFIED	<b>First Event:</b> ON ROADWAY <b>To Junction:</b> 4-LEGGED INTERSECTION <b>Traffic Device:</b> STOP SIGN OTHER <b>Speed Limit:</b> 55 <b>Diagram:</b> REAR END <b>Officer:</b> <b>Reliability:</b> CONFIDENT <b># of Vehicles:</b> 2.00
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<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 1</th></tr> <tr><td>Trav Dir: N</td></tr> <tr><td>Veh Act: STRAIGHT AHEAD</td></tr> <tr><td>Veh Type: PASSENGER CAR</td></tr> <tr><td>Age: 17</td></tr> <tr><td>Gender: F</td></tr> <tr><td>Cond: NORMAL</td></tr> <tr><td>Cont Fact: IMPROPER PASSING</td></tr> <tr><td>Cont Fact: NOT SPECIFIED</td></tr> </table>	Unit 1	Trav Dir: N	Veh Act: STRAIGHT AHEAD	Veh Type: PASSENGER CAR	Age: 17	Gender: F	Cond: NORMAL	Cont Fact: IMPROPER PASSING	Cont Fact: NOT SPECIFIED	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 2</th></tr> <tr><td>Trav Dir: N</td></tr> <tr><td>Veh Act: LEFT TURN</td></tr> <tr><td>Veh Type: PASSENGER CAR</td></tr> <tr><td>Age: 22</td></tr> <tr><td>Gender: M</td></tr> <tr><td>Cond: NORMAL</td></tr> <tr><td>Cont Fact: OTHER</td></tr> <tr><td>Cont Fact: NOT SPECIFIED</td></tr> </table>	Unit 2	Trav Dir: N	Veh Act: LEFT TURN	Veh Type: PASSENGER CAR	Age: 22	Gender: M	Cond: NORMAL	Cont Fact: OTHER	Cont Fact: NOT SPECIFIED	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><th style="background-color: #cccccc;">Unit 3</th></tr> <tr><td> </td></tr> </table>	Unit 3	
Unit 1																						
Trav Dir: N																						
Veh Act: STRAIGHT AHEAD																						
Veh Type: PASSENGER CAR																						
Age: 17																						
Gender: F																						
Cond: NORMAL																						
Cont Fact: IMPROPER PASSING																						
Cont Fact: NOT SPECIFIED																						
Unit 2																						
Trav Dir: N																						
Veh Act: LEFT TURN																						
Veh Type: PASSENGER CAR																						
Age: 22																						
Gender: M																						
Cond: NORMAL																						
Cont Fact: OTHER																						
Cont Fact: NOT SPECIFIED																						
Unit 3																						

**Crash ID:** 102800070      **Date:** 10/01/2010      **Time:** 1402  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	W	S	
<b>Veh Act:</b>	START TRAFFIC	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	PASSENGER CAR	
<b>Age:</b>	50	37	
<b>Gender:</b>	M	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 102940180      **Date:** 10/21/2010      **Time:** 1624  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> POSSIBLE INJURY	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> NOT APPLICABLE
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	W	S	
<b>Veh Act:</b>	STRAIGHT AHEAD	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	PASSENGER CAR	
<b>Age:</b>	27	23	
<b>Gender:</b>	M	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 110860009      **Date:** 03/26/2011      **Time:** 2233  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 05-MSAS  
**Route:** 31480112      002+00.177

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> NON-JUNCTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> NOT APPLICABLE
<b>Crash Type:</b> COLL UNDERRIDE SIDE	<b>Speed Limit:</b> 45
<b>Surf Cond:</b> ICE/PACKED SNOW	<b>Diagram:</b> UNKNOWN
<b>Light Cond:</b> DARK - UNKNOWN LIGHTING	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> LESS CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 1.00

	<b>Unit 1</b>	<b>Unit 2</b>	<b>Unit 3</b>
<b>Trav Dir:</b>	EAST		
<b>Veh Act:</b>	STRAIGHT AHEAD		
<b>Veh Type:</b>	PICKUP TRUCK		
<b>Age:</b>	20		
<b>Gender:</b>	M		
<b>Cond:</b>	NORMAL		
<b>Cont Fact</b>	WEATHER		
<b>Cont Fact</b>	NOT SPECIFIED		

**Crash ID:** 123020017      **Date:** 10/27/2012      **Time:** 2256  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DARK - STREET LIGHTS ON	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	<b>Unit 1</b>	<b>Unit 2</b>	<b>Unit 3</b>
<b>Trav Dir:</b>	S	W	
<b>Veh Act:</b>	STRAIGHT AHEAD	PED. FAIL TO YIELD R/W TO T	
<b>Veh Type:</b>	SPORT UTILITY VEHICLE	PASSENGER CAR	
<b>Age:</b>	54	17	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	DISTRACTION	
<b>Cont Fact</b>	NOT SPECIFIED	FAIL TO YIELD ROW	

**Crash ID:** 130860106      **Date:** 03/27/2013      **Time:** 1404  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> INCAPACITATING INJURY	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> NOT APPLICABLE
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> REAR END
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	S	S	
<b>Veh Act:</b>	LEFT TURN	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	PASSENGER CAR	
<b>Age:</b>	20	21	
<b>Gender:</b>	M	M	
<b>Cond:</b>	UNKNOWN	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	FAIL TO YIELD ROW	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 131930115      **Date:** 07/12/2013      **Time:** 0906  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> POSSIBLE INJURY	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> INTERSECTION-RELATED
<b>Road Char:</b> STRAIGHT AND GRADE	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	W	S	
<b>Veh Act:</b>	STRAIGHT AHEAD	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	SPORT UTILITY VEHICLE	
<b>Age:</b>	16	39	
<b>Gender:</b>	F	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 132210084      **Date:** 08/09/2013      **Time:** 1316  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	N	W	
<b>Veh Act:</b>	00	PED. FAIL TO YIELD R/W TO T	
<b>Veh Type:</b>	VAN OR MINIVAN	SPORT UNTILITY VEHICLE	
<b>Age:</b>	47	70	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	FAIL TO YIELD ROW	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 140370338      **Date:** 02/05/2014      **Time:** 1818  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> ICE/PACKED SNOW	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DARK - NO STREET LIGHTS	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	EAST	N	
<b>Veh Act:</b>	PED. FAIL TO YIELD R/W TO	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	VAN OR MINIVAN	
<b>Age:</b>	49	50	
<b>Gender:</b>	F	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 141050144      **Date:** 04/15/2014      **Time:** 1639  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	N	W	
<b>Veh Act:</b>	STRAIGHT AHEAD	STRAIGHT AHEAD	
<b>Veh Type:</b>	SPORT UTILITY VEHICLE	PASSENGER CAR	
<b>Age:</b>	32	32	
<b>Gender:</b>	F	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	FAIL TO YIELD ROW	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 141440014      **Date:** 05/22/2014      **Time:** 1806  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> NON-INCAPACITATING INJURY	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND GRADE	<b>Traffic Device:</b> NOT APPLICABLE
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> LEFT TURN INTO TRAFFIC
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	N	S	
<b>Veh Act:</b>	LEFT TURN	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	PASSENGER CAR	
<b>Age:</b>	40	33	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	VISION OBSCURED - SUN OR H	NOT SPECIFIED	

**Crash ID:** 141980169      **Date:** 07/17/2014      **Time:** 1709  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> POSSIBLE INJURY	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> INTERSECTION-RELATED
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> SIDESWIPE OPPOSING
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	W	N	
<b>Veh Act:</b>	PED. FAIL TO YIELD R/W TO	STRAIGHT AHEAD	
<b>Veh Type:</b>	SPORT UNTILITY VEHICLE	TRUCK W/ SEMI TRAILER	
<b>Age:</b>	37	60	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

**Crash ID:** 143000098      **Date:** 10/27/2014      **Time:** 1150  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      001+00.001

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND GRADE	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLOUDY	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> CLOUDY	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	S	N	
<b>Veh Act:</b>	STRAIGHT AHEAD	LEFT TURN	
<b>Veh Type:</b>	PASSENGER CAR	TRUCK WITH 1 TRAILER	
<b>Age:</b>	25	30	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	FAIL TO YIELD ROW	
<b>Cont Fact</b>	NO IMPROPER DRIVING	DISTRACTION	

**Selection Filter:**

WORK AREA: COUNTY\_CODE('02') - FILTER: CRASH\_YEAR('2010','2011','2012','2013','2014','2015') - SPATIAL FILTER APPLIED

**Analyst:**

Jeremy Melquist

**Notes:**



# Crash Detail Report

Armstrong Blvd & 147th Ave (2010-2015)

Report Version 1.0 March 2010

<b>Crash ID:</b> 101570144	<b>Date:</b> 06/06/2010	<b>Time:</b> 1439	<b>Sys:</b> 04-CSAH
<b>County:</b> ANOKA	<b>City:</b> RAMSEY		<b>Route:</b> 02000083      000+00.137

<b>Severity:</b> PROPERTY DAMAGE <b>Road Type:</b> 2 LANES UNDIV 2_WAY <b>Road Char:</b> STRAIGHT AND LEVEL <b>Crash Type:</b> COLL W/MV IN TRANSPORT <b>Surf Cond:</b> WET <b>Light Cond:</b> DAYLIGHT <b>Weather 1:</b> RAIN <b>Weather 2:</b> CLOUDY	<b>First Event:</b> ON ROADWAY <b>To Junction:</b> NON-JUNCTION <b>Traffic Device:</b> NOT APPLICABLE <b>Speed Limit:</b> 55 <b>Diagram:</b> REAR END <b>Officer:</b> <b>Reliability:</b> CONFIDENT <b># of Vehicles:</b> 2.00
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	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	S	S	
<b>Veh Act:</b>	STRAIGHT AHEAD	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	PASSENGER CAR	
<b>Age:</b>	24	53	
<b>Gender:</b>	F	M	
<b>Cond:</b>	NORMAL	UNDER THE INFLUENCE	
<b>Cont Fact</b>	NO IMPROPER DRIVING	CHEMICAL IMPAIRMENT	
<b>Cont Fact</b>	NOT SPECIFIED	NOT SPECIFIED	

<b>Crash ID:</b> 102180091	<b>Date:</b> 08/06/2010	<b>Time:</b> 0904	<b>Sys:</b> 04-CSAH
<b>County:</b> ANOKA	<b>City:</b> RAMSEY		<b>Route:</b> 02000083      000+00.147

<b>Severity:</b> POSSIBLE INJURY <b>Road Type:</b> 2 LANES UNDIV 2_WAY <b>Road Char:</b> STRAIGHT AND LEVEL <b>Crash Type:</b> COLL W/MV IN TRANSPORT <b>Surf Cond:</b> DRY <b>Light Cond:</b> DAYLIGHT <b>Weather 1:</b> CLEAR <b>Weather 2:</b> NOT SPECIFIED	<b>First Event:</b> ON ROADWAY <b>To Junction:</b> 4-LEGGED INTERSECTION <b>Traffic Device:</b> NOT APPLICABLE <b>Speed Limit:</b> 55 <b>Diagram:</b> REAR END <b>Officer:</b> <b>Reliability:</b> CONFIDENT <b># of Vehicles:</b> 2.00
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	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	S	S	
<b>Veh Act:</b>	STOPPED TRAFFIC	STRAIGHT AHEAD	
<b>Veh Type:</b>	MOTORCYCLE	PASSENGER CAR	
<b>Age:</b>	30	22	
<b>Gender:</b>	F	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	NO IMPROPER DRIVING	DISTRACTION	
<b>Cont Fact</b>	NOT SPECIFIED	FOLLOWING TOO CLOSELY	

**Crash ID:** 102250199      **Date:** 08/13/2010      **Time:** 1827  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 04-CSAH  
**Route:** 02000083      000+00.147

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 4_6 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> STOP SIGN OTHER
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> DRY	<b>Diagram:</b> RIGHT ANGLE
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLOUDY	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> CLOUDY	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	S	W	
<b>Veh Act:</b>	OVERTAKING/PASSING	STRAIGHT AHEAD	
<b>Veh Type:</b>	PASSENGER CAR	SPORT UNTILITY VEHICLE	
<b>Age:</b>	16	21	
<b>Gender:</b>	F	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	IMPROPER LANE	NO IMPROPER DRIVING	
<b>Cont Fact</b>	INEXPERIENCE	NO IMPROPER DRIVING	

**Crash ID:** 150060294      **Date:** 01/06/2015      **Time:** 1120  
**County:** ANOKA      **City:** RAMSEY

**Sys:** 05-MSAS  
**Route:** 31480104      000+00.900

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> OTHER
<b>Road Type:</b> OTHER	<b>To Junction:</b> TRF CIRCLE OR ROUNDABOUT
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> NOT APPLICABLE
<b>Crash Type:</b> COLL W/LIGHT POLE	<b>Speed Limit:</b> 30
<b>Surf Cond:</b> ICE/PACKED SNOW	<b>Diagram:</b> HEAD ON
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> CLEAR	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> CLEAR	<b># of Vehicles:</b> 1.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	EAST		
<b>Veh Act:</b>	RIGHT TURN		
<b>Veh Type:</b>	PASSENGER CAR		
<b>Age:</b>	45		
<b>Gender:</b>	F		
<b>Cond:</b>	NORMAL		
<b>Cont Fact</b>	WEATHER		
<b>Cont Fact</b>	UNKNOWN		

**Selection Filter:**

WORK AREA: COUNTY\_CODE('02') - FILTER: CRASH\_YEAR('2010','2011','2012','2013','2014','2015') - SPATIAL FILTER APPLIED

**Analyst:**

Jeremy Melquist

**Notes:**



# Crash Detail Report

Armstrong Blvd & Bunker Lake Blvd (2010-2015)

Report Version 1.0 March 2010

<b>Crash ID:</b> 140950029	<b>Date:</b> 04/04/2014	<b>Time:</b> 0832	<b>Sys:</b> 04-CSAH
<b>County:</b> ANOKA	<b>City:</b> RAMSEY	<b>Route:</b> 02000116	000+00.017

<b>Severity:</b> PROPERTY DAMAGE	<b>First Event:</b> ON ROADWAY
<b>Road Type:</b> 2 LANES UNDIV 2_WAY	<b>To Junction:</b> 4-LEGGED INTERSECTION
<b>Road Char:</b> STRAIGHT AND LEVEL	<b>Traffic Device:</b> TRAFFIC SIGNALS
<b>Crash Type:</b> COLL W/MV IN TRANSPORT	<b>Speed Limit:</b> 55
<b>Surf Cond:</b> ICE/PACKED SNOW	<b>Diagram:</b> REAR END
<b>Light Cond:</b> DAYLIGHT	<b>Officer:</b>
<b>Weather 1:</b> SNOW	<b>Reliability:</b> CONFIDENT
<b>Weather 2:</b> NOT SPECIFIED	<b># of Vehicles:</b> 2.00

	Unit 1	Unit 2	Unit 3
<b>Trav Dir:</b>	W	W	
<b>Veh Act:</b>	RIGHT TURN	RIGHT TURN	
<b>Veh Type:</b>	PASSENGER CAR	BUS (16+ SEATS)	
<b>Age:</b>	20	58	
<b>Gender:</b>	M	F	
<b>Cond:</b>	NORMAL	NORMAL	
<b>Cont Fact</b>	FAIL TO YIELD ROW	NO IMPROPER DRIVING	
<b>Cont Fact</b>	ILLEGAL SPEED	NOT SPECIFIED	

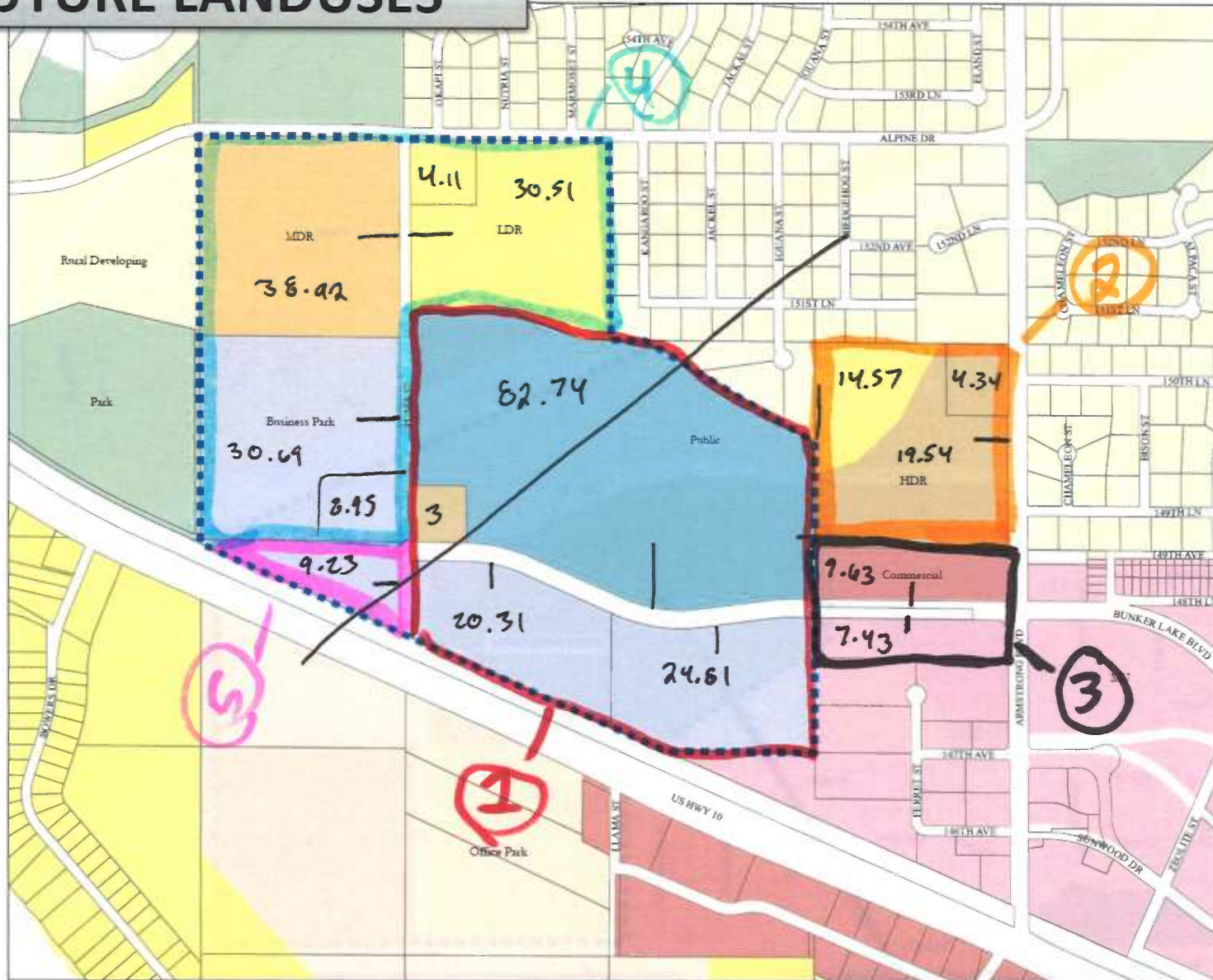
**Selection Filter:**  
 WORK AREA: COUNTY\_CODE('02') - FILTER: CRASH\_YEAR('2010','2011','2012','2013','2014','2015') - SPATIAL FILTER APPLIED

**Analyst:** Jeremy Melquist  
**Notes:**



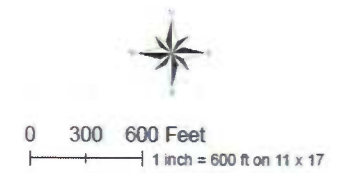
**APPENDIX D-**  
**TRIP GENERATION FOR FUTURE**  
**BUSINESS PARK DEVELOPMENT**

# FUTURE LANDUSES



## 2030 Comprehensive Plan Future Land Use Map PENDING

- Study Area
- Future Land Use**
- Low Density Residential
- Medium Density Residential
- High Density Residential
- Office Park
- Commercial
- MU
- Business Park
- Public
- Rural Developing
- Rural Preserve
- Park



This map has been prepared using information generated from various governmental offices and other sources and is to be used for reference purposes only. It is neither a legally recorded map nor a survey and is not intended for use as one. The Geographic Information System (GIS) data used to develop this map is not accompanied by the City as being erroneous.

The City does not represent that this GIS data can be used for exact measurement of distance or direction or precision in the location of geographic features. If errors or discrepancies are found, please contact (763) 437-0465.

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**Ramsey EDA Traffic Assumptions**  
Zone 1 Alternative A: High School

**Ramsey EDA Traffic Assumptions**  
Zone 1 Alternative 1A: High School

**High School**

	Based on Square Feet	180 K ft <sup>2</sup>			ITE Code		530		Internal-to-Internal Reduction	
	Average Rate	86.42 Acres			entering	exiting	entering	exiting		
		#	% enter	% exit						
AM	3.06	551	71	29	391	160	0%	392	160	0%
Afternoon	2.12	382	31	69	118	264	0%	119	264	0%
PM	0.97	175	54	46	95	81	0%	95	81	0%
Weekday	12.89	2321	50	50	1161	1161	0%	1161	1161	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	392	160
0	0	119	264
0	0	95	81
0	0	1161	1161

**Business Park**

	Based on Acres	45.12 Acres			ITE Code		130		Internal-to-Internal Reduction	
	Average Rate	#	% enter	% exit	entering	exiting	entering	exiting		
AM	8.55	386	83	17	320	66	10%	289	60	0%
Afternoon	4.42	200	50	50	100	100	10%	90	90	0%
PM	8.84	399	21	79	84	315	10%	76	284	0%
Weekday	63.11	2848	50	50	1424	1424	15%	1211	1211	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	289	60
0	0	90	90
0	0	76	284
0	0	1211	1211

AM	937	712	225
Afternoon	582	218	364
PM	574	178	396
Weekday	5169	2585	2585

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	681	220
Afternoon	0	0	209	354
PM	0	0	171	365
Weekday	0	0	2372	2372

**Ramsey EDA Traffic Assumptions**  
Zone 1 Alternative 1B: K-12 School

86.42 Acres

**Elementary School**

50 K ft<sup>2</sup>

ITE Code

520

Internal-to-Internal Reduction

Based on Square Feet

	Average Rate	#	% enter	% exit	entering	exiting		entering	exiting	
AM	5.2	260	56	44	146	114	10%	132	103	0%
Afternoon	3.11	156	44	56	69	87	10%	62	79	0%
PM	1.21	61	45	55	27	34	10%	25	31	0%
Weekday	15.43	772	50	50	386	386	10%	348	348	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	132	103
0	0	62	79
0	0	25	31
0	0	348	348

**Middle School**

50 K ft<sup>2</sup>

ITE Code

522

Internal-to-Internal Reduction

Based on Square Feet

	Average Rate	#	% enter	% exit	entering	exiting		entering	exiting	
AM	4.35	218	55	45	120	98	10%	108	89	0%
Afternoon	2.52	126	45	55	57	69	10%	52	63	0%
PM	1.19	60	52	48	31	29	10%	29	26	0%
Weekday	13.78	689	50	50	345	345	10%	311	311	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	108	89
0	0	52	63
0	0	29	26
0	0	311	311

**High School**

80 K ft<sup>2</sup>

ITE Code

530

Internal-to-Internal Reduction

Based on Square Feet

	Average Rate	#	% enter	% exit	entering	exiting		entering	exiting	
AM	3.06	245	71	29	174	71	10%	157	64	0%
Afternoon	2.12	170	31	69	53	117	10%	48	106	0%
PM	0.97	78	54	46	42	36	10%	38	33	0%
Weekday	12.89	1032	50	50	516	516	10%	465	465	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	157	64
0	0	48	106
0	0	38	33
0	0	465	465

**Business Park**

45.12 Acres

ITE Code

130

Internal-to-Internal Reduction

Based on Acres

	Average Rate	#	% enter	% exit	entering	exiting		entering	exiting	
AM	8.55	386	83	17	320	66	10%	289	60	0%
Afternoon	4.42	200	50	50	100	100	10%	90	90	0%
PM	8.84	399	21	79	84	315	10%	76	284	0%
Weekday	63.11	2848	50	50	1424	1424	15%	1211	1211	0%

Pass-by		New Trips	
entering	exiting	entering	exiting
0	0	289	60
0	0	90	90
0	0	76	284
0	0	1211	1211

AM	1109	760	349
Afternoon	652	278	374
PM	598	185	413
Weekday	5341	2671	2671

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	686	316
Afternoon	0	0	252	338
PM	0	0	168	374
Weekday	0	0	2335	2335

**Ramsey EDA Traffic Assumptions**  
 Zone 1 Alternative 2: Business Park

Business Park	Based on Acres	131.54 Acres		ITE Code		130		Internal-to-Internal Reduction			Pass-by		New Trips	
		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	8.55	1125	83	17	934	191	10%	841	173	0%	0	0	841	173
Afternoon	4.42	582	50	50	291	291	10%	262	262	0%	0	0	262	262
PM	8.84	1163	21	79	244	919	10%	220	827	0%	0	0	220	827
Weekday	63.11	8302	50	50	4151	4151	15%	3529	3529	0%	0	0	3529	3529

AM	1125	934	191
Afternoon	582	291	291
PM	1163	244	919
Weekday	8302	4151	4151

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	841	173
Afternoon	0	0	262	262
PM	0	0	220	827
Weekday	0	0	3529	3529

**Ramsey EDA Traffic Assumptions**

Zone 1 Alternative 3: Low Density Residential

<b>Single Family Detached Housing</b>		260 units	ITE Code		210	86.42 Acres		3 Units per Acre		New Trips	
Based on Dwelling Units		rate	#	% enter	% exit	entering	exiting	entering	exiting	entering	exiting
AM	0.75	192	25	75	48	144			48	144	
Afternoon	0.51	0	31	69	0	0			0	0	
PM	1.01	249	63	37	157	92			157	92	
Weekday	9.57	2505	50	50	1253	1253			1253	1253	

<b>Business Park</b>		45.12 Acres	ITE Code		130	Internal-to-Internal Reduction		Pass-by		New Trips				
Based on Acres		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	entering	exiting			
AM	8.55	386	83	17	320	66	10%	289	60	0%	0	0	289	60
Afternoon	4.42	200	50	50	100	100	10%	90	90	0%	0	0	90	90
PM	8.84	399	21	79	84	315	10%	76	284	0%	0	0	76	284
Weekday	63.11	2848	50	50	1424	1424	15%	1211	1211	0%	0	0	1211	1211

AM	578	368	210
Afternoon	200	100	100
PM	648	241	407
Weekday	5353	2677	2677

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	337	204
Afternoon	0	0	90	90
PM	0	0	233	376
Weekday	0	0	2464	2464

**Ramsey EDA Traffic Assumptions**

Zone 2 off of Armstrong

**Single Family Detached Housing**

44 units

ITE Code 210

14.57 Acres

3 Units per Acre

Based on Dwelling Units

	rate	#	% enter	% exit	entering	exiting
AM	0.75	41	25	75	10	31
Afternoon	0.51	23	31	69	7	16
PM	1.01	51	63	37	32	19
Weekday	9.57	489	50	50	245	245

New Trips	
entering	exiting
10	31
7	16
32	19
245	245

**Low Rise Apartment**

235 units

ITE Code 221

19.54 Acres

7 to 15 Units per Acre

Assume 12 Units per Acre

Based on Dwelling Units

	Average Rate	#	% enter	% exit	entering	exiting
AM	0.46	111	21	79	23	88
Afternoon	0.29	69	31	69	21	48
PM	0.58	144	65	35	94	50
Weekday	6.59	1591	50	50	796	796

New Trips	
entering	exiting
23	88
21	48
94	50
796	796

AM	152	34	118
Afternoon	92	29	63
PM	195	126	69
Weekday	2080	1040	1040

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	34	118
Afternoon	0	0	29	63
PM	0	0	126	69
Weekday	0	0	1040	1040

**Ramsey EDA Traffic Assumptions**  
 Zone 3: Off of Bunker Lake Blvd

Retail/Commercial	Based on Square Feet Average Rate	170.9 K ft <sup>2</sup>		17.06 Acres		ITE Code	X	Internal-to-Internal Reduction			Pass-by		New Trips	
		#	% enter	% exit	entering			exiting	entering	exiting	%	entering	exiting	entering
AM	7.41	1267	53	47	672	595	20%	538	477	46%	248	220	290	257
Afternoon	5.46	934	50	50	467	467	20%	374	374	40%	150	150	224	224
PM	10.91	1865	51	49	951	914	20%	761	732	39%	297	286	464	446
Weekday	124.59	21296	50	50	10648	10648	30%	7454	7454	39%	2908	2908	4546	4546

AM	1267	672	595
Afternoon	934	467	467
PM	1865	951	914
Weekday	21296	10648	10648

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	248	220	290	257
Afternoon	150	150	224	224
PM	297	286	464	446
Weekday	2908	2908	4546	4546

**Ramsey EDA Traffic Assumptions**

Zone 4: Off of Puma St

<b>Business Park</b>		39.64 Acres			ITE Code 130		Internal-to-Internal Reduction				Pass-by		New Trips		
Based on Acres		#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting	entering	exiting
Average Rate	entering											exiting			
AM	8.55	339	83	17	281	58	10%	254	52	0%	0	0	254	52	
Afternoon	4.42	176	50	50	88	88	10%	80	80	0%	0	0	80	80	
PM	8.84	351	21	79	74	277	10%	67	250	0%	0	0	67	250	
Weekday	63.11	2502	50	50	1251	1251	15%	1064	1064	0%	0	0	1064	1064	

<b>Single Family Detached Housing</b>		104 units			ITE Code 210		34.62 Acres 3 Units per Acre				New Trips		
Based on Dwelling Units		#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
rate	entering											exiting	
AM	0.75	83	25	75	21	62						21	62
Afternoon	0.51	53	31	69	16	37						16	37
PM	1.01	109	63	37	69	40						69	40
Weekday	9.57	1078	50	50	539	539						539	539

<b>Residential Townhouse</b>		234 units			ITE Code 230		38.92 Acres 3 to 7 Units per Acre Assume 6 Units per Acre				New Trips		
Based on Dwelling Units		#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
Average Rate	entering											exiting	
AM	0.44	102	17	83	17	85						17	85
Afternoon	0.26	61	31	69	19	42						19	42
PM	0.52	121	67	33	81	40						81	40
Weekday	5.81	1348	50	50	674	674						674	674

		Pass-by		New Trips	
		entering	exiting	entering	exiting
AM	524	0	0	292	199
Afternoon	290	0	0	115	159
PM	581	0	0	217	330
Weekday	4928	0	0	2277	2277

**Ramsey EDA Traffic Assumptions**

Zone 5: Off of Ruma EB & NB (Split 50/50)

Business Park	Based on Acres	9.23 Acres			ITE Code 130		Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	8.55	79	83	17	66	13	10%	60	13	0%	0	0	60	13
Afternoon	4.42	41	50	50	21	21	10%	19	19	0%	0	0	19	19
PM	8.84	82	21	79	17	65	10%	16	59	0%	0	0	16	59
Weekday	63.11	583	50	50	292	292	15%	248	248	0%	0	0	248	248

AM	79	66	13
Afternoon	41	21	21
PM	82	17	65
Weekday	583	292	292

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	60	13
Afternoon	0	0	19	19
PM	0	0	16	59
Weekday	0	0	248	248

**Ramsey EDA Traffic Assumptions**

Alternative 1: High School

High School	Based on Square Feet	180 K ft <sup>2</sup>			ITE Code	530	Internal-to-Internal Reduction				Pass-by		New Trips	
		86.42 Acres					entering	exiting	entering	exiting	entering	exiting	entering	exiting
		Average Rate	#	% enter										
AM	3.06	551	71	29	391	160	0%	392	160	0%	0	0	392	160
Afternoon	2.12	382	31	69	118	264	0%	119	264	0%	0	0	119	264
PM	0.97	175	54	46	95	81	0%	95	81	0%	0	0	95	81
Weekday	12.89	2321	50	50	1161	1161	0%	1161	1161	0%	0	0	1161	1161

Business Park	Based on Acres	93.99 Acres			ITE Code	130	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	
														AM
Afternoon	4.42	416	50	50	208	208	10%	188	188	0%	0	0	188	188
PM	8.84	831	21	79	175	656	10%	158	591	0%	0	0	158	591
Weekday	63.11	5932	50	50	2966	2966	15%	2522	2522	0%	0	0	2522	2522

Retail/Commercial	Based on Square Feet	170.9 K ft <sup>2</sup>			ITE Code	X	Internal-to-Internal Reduction				Pass-by		New Trips	
		17.06 Acres					entering	exiting	entering	exiting	entering	exiting		
		Average Rate	#	% enter									% exit	
AM	7.41	1267	53	47	672	595	20%	538	477	46%	248	220	290	257
Afternoon	5.46	934	50	50	467	467	20%	374	374	40%	150	150	224	224
PM	10.91	1865	51	49	951	914	20%	761	732	39%	297	286	464	446
Weekday	124.59	21296	50	50	10648	10648	30%	7454	7454	39%	2908	2908	4546	4546

Single Family Detached Housing	Based on Dwelling Units	104 units			ITE Code	210	34.62 Acres				New Trips	
		3 Units per Acre					entering	exiting	entering	exiting		
		rate	#	% enter							% exit	
AM	0.75	83	25	75	21	62					21	62
Afternoon	0.51	53	31	69	16	37					16	37
PM	1.01	109	63	37	69	40					69	40
Weekday	9.57	1078	50	50	539	539					539	539

Single Family Detached Housing	Based on Dwelling Units	44 units			ITE Code	210	14.57 Acres				New Trips	
		3 Units per Acre					entering	exiting	entering	exiting		
		rate	#	% enter							% exit	
AM	0.75	41	25	75	10	31					10	31
Afternoon	0.51	53	31	69	16	37					16	37
PM	1.01	51	63	37	32	19					32	19
Weekday	9.57	489	50	50	245	245					245	245

Residential Townhouse	Based on Dwelling Units	234 units			ITE Code	230	38.92 Acres				New Trips	
		3 to 7 Units per Acre					entering	exiting	entering	exiting		
		Assume 6 Units per Acre										
AM	0.44	102	17	83	17	85					17	85
Afternoon	0.26	61	31	69	19	42					19	42
PM	0.52	121	67	33	81	40					81	40
Weekday	5.81	1348	50	50	674	674					674	674

Low Rise Apartment	Based on Dwelling Units	235 units			ITE Code	221	19.54 Acres				New Trips	
		7 to 15 Units per Acre					entering	exiting	entering	exiting		
		Assume 12 Units per Acre										
AM	0.46	111	21	79	23	88					23	88
Afternoon	0.29	69	31	69	21	48					21	48
PM	0.58	144	65	35	94	50					94	50
Weekday	6.59	1591	50	50	796	796					796	796

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	248	220	1355	806
Afternoon	150	150	604	839
PM	297	286	992	1268
Weekday	2908	2908	10482	10482

West of Commercial Area

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	1031	431
Afternoon	0	0	342	531
PM	0	0	403	752
Weekday	0	0	4896	4896

**Ramsey EDA Traffic Assumptions**

Alternative 1B: K-12 School

86.42 Acres

<b>Elementary School</b>		50 K ft <sup>2</sup>	ITE Code	520	Internal-to-Internal Reduction					Pass-by		New Trips		
Based on Square Feet		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	5.2	260	56	44	146	114	10%	132	103	0%	0	0	132	103
Afternoon	3.11	156	44	56	69	87	10%	62	79	0%	0	0	62	79
PM	1.21	61	45	55	27	34	10%	25	31	0%	0	0	25	31
Weekday	15.43	772	50	50	386	386	10%	348	348	0%	0	0	348	348

<b>Middle School</b>		50 K ft <sup>2</sup>	ITE Code	522	Internal-to-Internal Reduction					Pass-by		New Trips		
Based on Square Feet		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	4.35	218	55	45	120	98	10%	108	89	0%	0	0	108	89
Afternoon	2.52	126	45	55	57	69	10%	52	63	0%	0	0	52	63
PM	1.19	60	52	48	31	29	10%	29	26	0%	0	0	29	26
Weekday	13.78	689	50	50	345	345	10%	311	311	0%	0	0	311	311

<b>High School</b>		80 K ft <sup>2</sup>	ITE Code	530	Internal-to-Internal Reduction					Pass-by		New Trips		
Based on Square Feet		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	3.06	245	71	29	174	71	10%	157	64	0%	0	0	157	64
Afternoon	2.12	170	31	69	53	117	10%	48	106	0%	0	0	48	106
PM	0.97	78	54	46	42	36	10%	38	33	0%	0	0	38	33
Weekday	12.89	1032	50	50	516	516	10%	465	465	0%	0	0	465	465

<b>Business Park</b>		93.99 Acres	ITE Code	130	Internal-to-Internal Reduction					Pass-by		New Trips		
Based on Acres		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	8.55	428	83	17	355	73	10%	320	66	0%	0	0	320	66
Afternoon	4.42	221	50	50	111	111	10%	100	100	0%	0	0	100	100
PM	8.84	442	21	79	93	349	10%	84	315	0%	0	0	84	315
Weekday	63.11	3156	50	50	1578	1578	15%	1342	1342	0%	0	0	1342	1342

<b>Retail/Commercial</b>		170.9 K ft <sup>2</sup>	17.06 Acres	ITE Code	X	Internal-to-Internal Reduction					Pass-by		New Trips	
Based on Square Feet		Average Rate	#	% enter	% exit	entering	exiting	entering	exiting	%	entering	exiting	entering	exiting
AM	7.41	1267	53	47	672	595	20%	538	477	46%	248	220	290	257
Afternoon	5.46	934	50	50	467	467	20%	374	374	40%	150	150	224	224
PM	10.91	1865	51	49	951	914	20%	761	732	39%	297	286	464	446
Weekday	124.59	21296	50	50	10648	10648	30%	7454	7454	39%	2908	2908	4546	4546

<b>Single Family Detached Housing</b>		104 units	ITE Code	210	34.62 Acres					New Trips	
Based on Dwelling Units		rate	#	% enter	% exit	entering	exiting	3 Units per Acre		entering	exiting
AM	0.75	83	25	75	21	62			21	62	
Afternoon	0.51	48	31	69	15	33			15	33	
PM	1.01	109	63	37	69	40			69	40	
Weekday	9.57	1078	50	50	539	539			539	539	

<b>Single Family Detached Housing</b>		44 units	ITE Code	210	14.57 Acres					New Trips	
Based on Dwelling Units		rate	#	% enter	% exit	entering	exiting	3 Units per Acre		entering	exiting
AM	0.75	41	25	75	10	31			10	31	
Afternoon	0.51	48	31	69	15	33			15	33	
PM	1.01	51	63	37	32	19			32	19	
Weekday	9.57	489	50	50	245	245			245	245	

<b>Residential Townhouse</b>		234 units	ITE Code	230	38.92 Acres					New Trips	
Based on Dwelling Units		Average Rate	#	% enter	% exit	entering	exiting	3 to 7 Units per Acre		entering	exiting
Assume 6 Units per Acre											
AM	0.44	102	17	83	17	85			17	85	
Afternoon	0.26	61	31	69	19	42			19	42	
PM	0.52	121	67	33	81	40			81	40	
Weekday	5.81	1348	50	50	674	674			674	674	

<b>Low Rise Apartment</b>		235 units	ITE Code	221	19.54 Acres					New Trips	
Based on Dwelling Units		Average Rate	#	% enter	% exit	entering	exiting	7 to 15 Units per Acre		entering	exiting
Assume 12 Units per Acre											
AM	0.46	111	21	79	23	88			23	88	
Afternoon	0.29	69	31	69	21	48			21	48	
PM	0.58	144	65	35	94	50			94	50	
Weekday	6.59	1591	50	50	796	796			796	796	

		Pass-by		New Trips	
		entering	exiting	entering	exiting
AM	2714	248	220	1079	844
Afternoon	1785	150	150	556	728
PM	2880	297	286	915	1001
Weekday	30962	2908	2908	9265	9265

West of Commercial Area

		Pass-by		New Trips	
		entering	exiting	entering	exiting
AM		0	0	755	469
Afternoon		0	0	296	423
PM		0	0	326	485
Weekday		0	0	3679	3679

**Ramsey EDA Traffic Assumptions**  
Alternative 2: Business Park

High School	Based on Square Feet	0 K ft <sup>2</sup>			ITE Code	530	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	entering
AM	3.06	0	71	29	0	0	0%	0	0	0%	0	0	0	0
Afternoon	2.12	0	31	69	0	0	0%	0	0	0%	0	0	0	0
PM	0.97	0	54	46	0	0	0%	0	0	0%	0	0	0	0
Weekday	12.89	0	50	50	0	0	0%	0	0	0%	0	0	0	0

Business Park	Based on Acres	180.41 Acres			ITE Code	130	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	entering
AM	8.55	1543	83	17	1281	262	10%	1153	237	0%	0	0	1153	237
Afternoon	4.42	798	50	50	399	399	10%	360	360	0%	0	0	360	360
PM	8.84	1595	21	79	335	1260	10%	302	1135	0%	0	0	302	1135
Weekday	63.11	11386	50	50	5693	5693	15%	4840	4840	0%	0	0	4840	4840

Retail/Commercial	Based on Square Feet	170.9 K ft <sup>2</sup>			ITE Code	X	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	entering
AM	7.41	1267	53	47	672	595	20%	538	477	46%	248	220	290	257
Afternoon	5.46	934	50	50	467	467	20%	374	374	40%	150	150	224	224
PM	10.91	1865	51	49	951	914	20%	761	732	39%	297	286	464	446
Weekday	124.59	21296	50	50	10648	10648	30%	7454	7454	39%	2908	2908	4546	4546

Single Family Detached Housing	Based on Dwelling Units	104 units			ITE Code	210	34.62 Acres				New Trips		
		rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting
AM	0.75	83	25	75	21	62						21	62
Afternoon	0.51	53	31	69	16	37						16	37
PM	1.01	109	63	37	69	40						69	40
Weekday	9.57	1078	50	50	539	539						539	539

Single Family Detached Housing	Based on Dwelling Units	44 units			ITE Code	210	14.57 Acres				New Trips		
		rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting
AM	0.75	41	25	75	10	31						10	31
Afternoon	0.51	53	31	69	16	37						16	37
PM	1.01	51	63	37	32	19						32	19
Weekday	9.57	489	50	50	245	245						245	245

Residential Townhouse	Based on Dwelling Units	234 units			ITE Code	230	38.92 Acres				New Trips		
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting
AM	0.44	102	17	83	17	85						17	85
Afternoon	0.26	61	31	69	19	42						19	42
PM	0.52	121	67	33	81	40						81	40
Weekday	5.81	1348	50	50	674	674						674	674

Low Rise Apartment	Based on Dwelling Units	235 units			ITE Code	221	19.54 Acres				New Trips		
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting
AM	0.46	111	21	79	23	88						23	88
Afternoon	0.29	69	31	69	21	48						21	48
PM	0.58	144	65	35	94	50						94	50
Weekday	6.59	1591	50	50	796	796						796	796

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	248	220	1515	759
Afternoon	150	150	657	747
PM	297	286	1041	1731
Weekday	2908	2908	11639	11639

West of Commercial Area

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	1191	384
Afternoon	0	0	395	439
PM	0	0	452	1215
Weekday	0	0	6053	6053

**Ramsey EDA Traffic Assumptions**  
Alternative 3: Low Density Residential

High School	Based on Square Feet	0 K ft <sup>2</sup>			ITE Code	530	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	
AM	3.06	0	71	29	0	0	0%	0	0	0%	0	0	0	0
Afternoon	2.12	0	31	69	0	0	0%	0	0	0%	0	0	0	0
PM	0.97	0	54	46	0	0	0%	0	0	0%	0	0	0	0
Weekday	12.89	0	50	50	0	0	0%	0	0	0%	0	0	0	0

Business Park	Based on Acres	93.99 Acres			ITE Code	130	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	
AM	8.55	804	83	17	667	137	10%	601	124	0%	0	0	601	124
Afternoon	4.42	416	50	50	208	208	10%	188	188	0%	0	0	188	188
PM	8.84	831	21	79	175	656	10%	158	591	0%	0	0	158	591
Weekday	63.11	5932	50	50	2966	2966	15%	2522	2522	0%	0	0	2522	2522

Retail/Commercial	Based on Square Feet	170.9 K ft <sup>2</sup>			ITE Code	X	Internal-to-Internal Reduction				Pass-by		New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	entering	exiting	
AM	7.41	1267	53	47	672	595	20%	538	477	46%	248	220	290	257
Afternoon	5.46	934	50	50	467	467	20%	374	374	40%	150	150	224	224
PM	10.91	1865	51	49	951	914	20%	761	732	39%	297	286	464	446
Weekday	124.59	21296	50	50	10648	10648	30%	7454	7454	39%	2908	2908	4546	4546

Single Family Detached Housing	Based on Dwelling Units	364 units			ITE Code	210	121.04 Acres				New Trips	
		rate	#	% enter			% exit	entering	exiting	entering	exiting	
AM	0.75	265	25	75	66	199					66	199
Afternoon	0.51	184	31	69	57	127					57	127
PM	1.01	337	63	37	212	125					212	125
Weekday	9.57	3414	50	50	1707	1707					1707	1707

Single Family Detached Housing	Based on Dwelling Units	44 units			ITE Code	210	14.57 Acres				New Trips	
		rate	#	% enter			% exit	entering	exiting	entering	exiting	
AM	0.75	41	25	75	10	31					10	31
Afternoon	0.51	184	31	69	57	127					57	127
PM	1.01	51	63	37	32	19					32	19
Weekday	9.57	489	50	50	245	245					245	245

Residential Townhouse	Based on Dwelling Units	234 units			ITE Code	230	38.92 Acres				New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	
AM	0.44	102	17	83	17	85					17	85
Afternoon	0.26	61	31	69	19	42					19	42
PM	0.52	121	67	33	81	40					81	40
Weekday	5.81	1348	50	50	674	674					674	674

Low Rise Apartment	Based on Dwelling Units	235 units			ITE Code	221	19.54 Acres				New Trips	
		Average Rate	#	% enter			% exit	entering	exiting	entering	exiting	
AM	0.46	111	21	79	23	88					23	88
Afternoon	0.29	69	31	69	21	48					21	48
PM	0.58	144	65	35	94	50					94	50
Weekday	6.59	1591	50	50	796	796					796	796

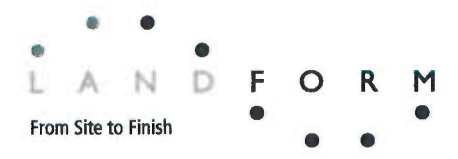
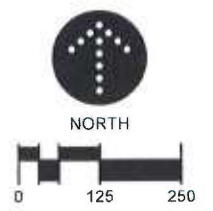
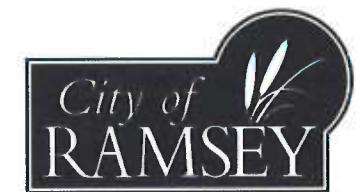
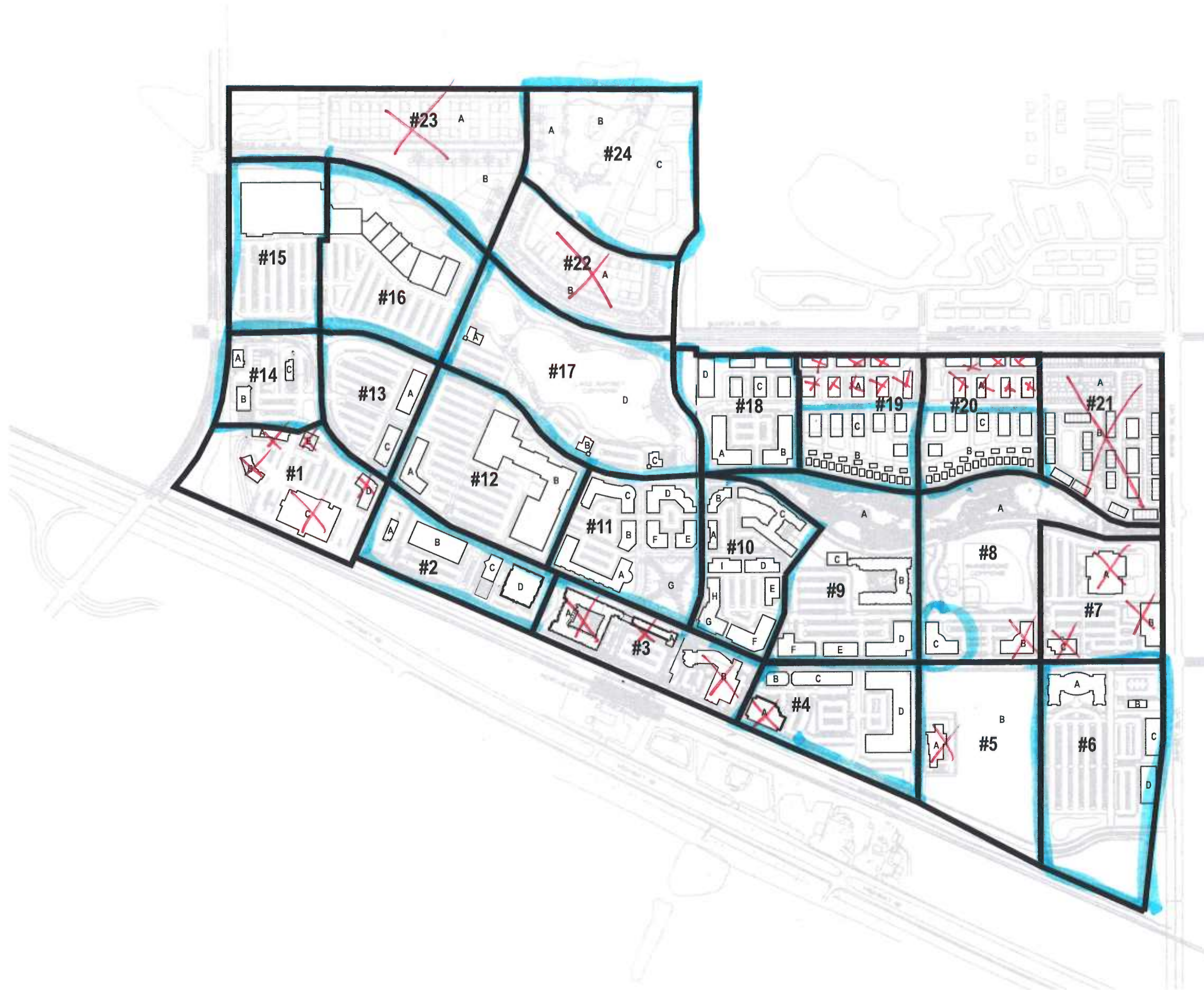
	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	248	220	1008	783
Afternoon	150	150	566	756
PM	297	286	1041	1271
Weekday	2908	2908	10489	10489

West of Commercial Area

	Pass-by		New Trips	
	entering	exiting	entering	exiting
AM	0	0	685	407
Afternoon	0	0	264	357
PM	0	0	451	756
Weekday	0	0	4903	4903



**APPENDIX E-**  
**TRIP GENERATION FOR COR**  
**DEVELOPMENT**



**EXHIBIT 5  
TAZ MAP**

05.21.2012  
Landform™ and Site to Finish™ are service marks of Landform Professional Services, LLC.

**Table 1 - Land Uses and Traffic Generation**

Zone/Block	Code	Land Use	Dwelling Units	Sq. ft.	Park Area <sup>1</sup> (sq. ft.)	ITE Code <sup>2</sup>	AM Peak			PM Peak			Daily Total
							Total	In	Out	Total	In	Out	
1a	3	Retail		11,882		820	10	6	4	35	17	18	408
1b	3	Retail		9,022		820	7	4	3	27	13	14	310
1c	3	Supermarket		62,396		850	179	109	70	524	267	257	5103
1d	3	Retail		13,283		820	11	6	4	40	19	20	456
1e	3	Retail		7,300		820	6	4	2	22	11	11	251
2a	3	Retail		5,248		820	4	3	2	16	8	8	180
2b	3	Retail		39,000		820	31	19	12	116	57	59	1340
2c	2	Daycare Center		10,320		565	101	54	48	103	48	55	654
2d	1	Senior Housing - Assisted Living	84			254	9	6	3	15	7	8	179
3a	1	Luxury Apartments / Townhomes	230			220	94	19	75	114	74	40	1224
3a	3	Retail		67,085		820	54	33	21	200	98	102	2305
3b	2	Government Office Building		49,107		730	231	194	37	48	15	33	2708
3c	0	Park & Ride (Northstar) <sup>3</sup>											
4a	2	Clinic		50,092		720	92	73	19	139	37	101	1448
4b	3	Sit Down Restaurant		9,037		931	6	3	3	54	36	18	650
4c	3	Hotel <sup>4</sup>		24,900		310	14	9	6	15	8	7	209
4d	2	Office Park		110,000		770	136	120	16	131	22	109	969
5a	2	Mortuary <sup>5</sup>		17,598		710	3	2	1	17	6	11	139
5b	2	Business Park		93,871		770	107	90	17	97	22	75	958
6a	2	Office Park		44,827		770	56	49	7	53	9	44	395
6b	3	Retail		13,070		820	10	6	4	39	19	20	449
6c	2	General Office		17,987		710	22	20	3	21	4	18	158
6d	2	General Office		17,987		710	22	20	3	21	4	18	158
7a	2	Charter School <sup>6</sup>		50,511		534	468	258	211	264	129	135	1405
7b	2	Medical Office		33,374		720	61	49	13	92	25	67	965
7c	3	Retail		24,780		820	20	12	8	74	36	38	851
8a	0	City Park w/ Lake			430,000								
8b	2	General Office		43,584		710	54	48	6	52	9	43	384
8c	2	General Office		43,584		710	54	48	6	52	9	43	384
9a	0	City Park w/ Lake			171,445								
9b	1	Apartments	95			220	39	8	31	47	31	16	505
9c	1	Apartments	200			220	82	16	65	99	64	35	1064
9d	2	General Office		59,696		710	74	65	9	71	12	59	526
9e	2	General Office		42,765		710	53	47	6	51	9	42	377
9f	2	General Office		59,208		710	73	65	9	71	12	59	522
10a	2	General Office		8,400		710	10	9	1	10	2	8	74
10a	1	Apartments	20			220	8	2	7	10	6	3	106
10b	2	General Office		8,400		710	10	9	1	10	2	8	74
10b	3	Specialty Retail		4,200		820	3	2	1	13	6	6	144
10b	1	Apartments	30			220	12	2	10	15	10	5	160
10c	1	Apartments - Senior	90			220	9	3	6	12	7	5	251
10c	1	Apartments	90			220	37	7	29	45	29	16	479
10d	2	General Office		11,500		710	14	13	2	14	2	11	101
10d	3	Specialty Retail		11,500		820	9	6	4	34	17	18	395
10e	2	General Office		8,500		710	11	9	1	10	2	8	75
10e	3	Specialty Retail		4,250		820	3	2	1	13	6	6	146
10f	2	General Office		11,900		710	15	13	2	14	2	12	105
10f	3	Specialty Retail		8,500		820	7	4	3	25	12	13	292
10f	1	Apartments	14			220	6	1	5	7	5	2	74
10g	3	Specialty Retail		7,600		820	6	4	2	23	11	12	261
10g	2	General Office		7,600		710	9	8	1	9	2	8	67
10g	1	Apartments	36			220	15	3	12	18	12	6	192
10h	3	Specialty Retail		6,300		820	5	3	2	19	9	10	216
10i	3	Specialty Retail		6,100		820	5	3	2	18	9	9	210
10i	2	General Office		6,100		710	8	7	1	7	1	6	54
10i	1	Apartments	26			220	11	2	8	13	8	5	138
11a	3	Specialty Retail		17,000		820	14	8	5	51	25	26	584
11a	1	Apartments	118			220	48	10	39	59	38	20	628
11b	2	General Office		11,000		710	14	12	2	13	2	11	97
11b	1	Apartments	26			220	11	2	8	13	8	5	138
11c	2	General Office		20,700		710	26	23	3	25	4	20	182
11c	1	Apartments	50			220	20	4	16	25	16	9	266
11d	2	General Office		10,700		710	13	12	2	13	2	11	94
11d	3	Specialty Retail		10,700		820	9	5	3	32	16	16	368
11d	1	Apartments	26			220	11	2	8	13	8	5	138
11e	2	General Office		5,900		710	7	6	1	7	1	6	52
11e	3	Specialty Retail		11,800		820	9	6	4	35	17	18	405
11e	1	Apartments	44			220	18	4	14	22	14	8	234
11f	3	Specialty Retail		11,800		820	9	6	4	35	17	18	405
11f	2	General Office		5,900		710	7	6	1	7	1	6	52
11f	1	Apartments	44			220	18	4	14	22	14	8	234
11g	0	City Park			82,804								
12a	3	Sit Down Restaurant		12,000		931	8	4	4	72	48	24	864
12b	3	Community Center		160,000		310	207	126	81	186	69	117	2929
13a	3	Retail		19,200		820	15	9	6	57	28	29	660
13b	3	Retail		16,664		820	13	8	5	50	24	25	572
14a	3	Gas Station w/Convenience Store <sup>7</sup>		5,000		945	317	162	155	388	194	194	1563
14b	3	Retail		10,628		820	9	5	3	32	16	16	365

**Table 1 - Land Uses and Traffic Generation**

Zone/Block	Code	Land Use	Dwelling Units	Sq. ft.	Park Area <sup>1</sup> (sq. ft.)	ITE Code <sup>2</sup>	AM Peak			PM Peak			Daily
							Total	In	Out	Total	In	Out	Total
14c	3	Fast Food Restaurant w/Drive-Through		4,800		934	190	97	93	129	67	62	1905
15	3	Shopping Center		135,986		820	109	66	42	406	199	207	4671
16	3	Retail		94,960		820	76	46	30	283	139	145	3262
17a	3	Sit Down Restaurant		6,000		931	4	2	2	36	24	12	432
17b	3	Sit Down Restaurant		5,470		931	4	2	2	33	22	11	394
17c	3	Sit Down Restaurant		5,470		931	4	2	2	33	22	11	394
17d	0	City Park w/ Lake			480,000								
18a	1	Condos	80			230	28	5	23	33	22	11	372
18b	1	Condos	69			230	24	4	20	29	19	9	321
18c	1	Condos	48			230	17	3	14	20	13	7	223
18d	1	Townhomes	32			230	11	2	9	13	9	4	149
19a	1	Townhomes	52			230	18	3	15	22	14	7	242
19b	1	Single Family - Detached	14			210	8	2	6	11	7	4	107
19c	1	Townhomes	31			230	11	2	9	13	9	4	144
20a	1	Townhomes	42			230	15	3	12	17	12	6	195
20b	1	Single Family - Detached	14			210	8	2	6	11	7	4	107
20c	1	Townhomes	28			230	10	2	8	12	8	4	130
21a	1	Townhomes	77			230	27	5	22	32	21	11	358
21b	1	Townhomes	90			230	32	5	26	37	25	12	418
22a	1	Single Family - Detached	23			210	14	3	10	19	12	7	176
22b	1	Townhomes	72			230	25	4	21	30	20	10	335
23a	1	Single Family - Detached	44			210	26	7	20	36	22	13	337
23b	1	Single Family - Detached	19			210	11	3	9	15	10	6	145
24a	1	Single Family - Detached	7			210	4	1	3	6	4	2	54
24b	0	City Park w/ Lake & Amphitheater			320,000								
24c	1	Single Family - Detached	17			210	10	3	8	14	9	5	130
<b>Total</b>			1,982	1,724,042	1,484,249		3,889	2,272	1,618	5,502	2,578	2,924	57,079
<b>Residential Total (Code 1)</b>			1,982	0			748	153	595	916	595	322	9,953
<b>Office Total (Code 2)</b>			0	861,111			1,755	1,326	429	1,422	395	1,027	13,177
<b>Retail Total (Code 3)</b>			0	862,931			1,386	792	594	3,164	1,588	1,576	33,949
			1,982	1,724,042			3,889	2,272	1,618	5,502	2,578	2,924	57,079

**Notes:**

<sup>1</sup> Due to the minimal amount of traffic generated by parks, they were not considered traffic generators in the original study. Likewise, parks are not considered traffic generators in this forecast.

<sup>2</sup> The trip generation was based on the methods and average rates published in the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 8th Edition*.

<sup>3</sup> The traffic generated by the park and ride was included in the analysis of the original study, however the unspecified volumes were added directly to the intersection traffic assignment instead of being listed with the other trip generation numbers. Accordingly, the traffic volumes generated by the park and ride facility are not considered with the rest of the generated traffic in this forecast.

<sup>4</sup> The number of dwelling units (DU) for the hotel was obtained by proportioning the proposed hotel to the hotel in the original study via their respective footprints. The hotel was modeled as having 32 rooms.

<sup>5</sup> Mortuary is assumed to operate similarly to a cemetery with 3 employees (ITE Land Use Code 566).

<sup>6</sup> Currently, there is no data for daily traffic volumes generated by 534 - Private School (K-8). For public elementary, junior high, and high schools, the ratio of the total daily traffic to the A.M. peak hour traffic is approximately 3.0. The total daily traffic generated by the charter school was calculated by multiplying the A.M. peak hour traffic by a factor of 3.0.

<sup>7</sup> Due to the lack of data for total weekday trips generated by 945 - Gas Station w/Convenience Store using square footage, the total weekday trips were calculated using the number of fueling positions. Based on the typical size of gas stations currently being constructed, it was assumed that new gas station will have 12 fueling positions.

The forecasts reflect a 20% internal capture rate consistent with the original AUAR's traffic impact study.



**APPENDIX F-**  
**OPERATIONS ANALYSIS (AVAILABLE**  
**UPON REQUEST)**



- Civil & Municipal Engineering
- Water & Wastewater Treatment
- Transportation Planning & Engineering Services
  - Aviation Services
- Water Resources Engineering
  - Landscape Architecture
- Environmental Review Services
- Cultural Resource Management
  - Land Surveying
- Geographic Information System Services
  - Project Funding & Financing



## APPENDIX C- STORM WATER ANALYSIS



# BOLTON & MENK, INC.

## Consulting Engineers & Surveyors

2035 County Road D East • Suite B • Maplewood, MN 55109-5314

Phone (651) 704-9970 • Fax (651) 704-9971

www.bolton-menk.com

June 25, 2015

Patrick Brama  
Economic Development Manager  
City of Ramsey  
7550 Sunwood Drive NW  
Ramsey, Minnesota 550303

RE: Future Business Park  
City of Ramsey  
Project No.: R16.109828

Dear Mr. Brama,

### **Project Background**

The city of Ramsey is considering the area west of Armstrong Boulevard, East of Plum Street, north of T.H. 10 and south of Alpine Drive, for future development. Storm water retention ponds will ultimately be required to control the runoff from the study area into the COR in accordance with the City's Surface Water Management Plan requirements. The following report summarizes the existing conditions and proposed pond layout options.

The site is situated in the Lower Rum River WMO (LRRWMO). Therefore, hydraulics, water quality, rate control, and volume reduction will be considered based on WMO and City requires. For the purposes of this study, preliminary regional stormwater management facilities were design based on assumed land uses and specific design requirements defined by LRRWMO.

### **Hydrologic and Hydraulic Modeling**

For this study, Bolton & Menk analyzed the existing and proposed conditions using Storm and Sanitary Analysis (SSA) developed by Autodesk, Inc. which utilizes NRCS TR-20 hydrology methodologies to hydrodynamically route stormwater through the drainage system. For this study, the 2-year, 10-year, and 100-year rainfall return intervals were analyzed using Atlas 14 rainfall depths and the US Soils Conservation Services (SCS) Type II 24-hour standard rainfall distribution. The corresponding rainfall depths are 2.86", 4.26", and 7.11", respectively. Modeling for future development was tied into existing regional modeling provided by the city.

### **Existing Conditions**

Drainage from the Lake Itasca outfall is directed toward the study area upstream of Alpine Dr. Overland flow passes through a series of culverts under Alpine Dr., Puma St. NW, and Armstrong Blvd. Flow at the Armstrong crossing is particularly critical as discharge enters the COR area. New development and stormwater management has been designed and constructed around the total contributing flow rate from

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the areas upstream of Armstrong Blvd.

It is our understanding that a storm sewer system installed in Bunker Lake Boulevard to manage the stormwater runoff within the right of way.

The City's regional hydrology and hydraulics model for the WMISS watershed was utilized to develop the existing peak flow rates into the COR area. Table 1 summarizes the peak inflow for a variety of rainfall return intervals.

*Table 1: Summary of Peak Flow Under Armstrong Blvd.*

Rainfall Event	Rainfall Depth	Peak Flow Through Armstrong Blvd.
	(in)	(cfs)
2-Year	2.86	10.1
10-Year	4.26	19.1
100-Year	7.1	22.1

### **Proposed Conditions**

The study area can was delineated three main areas based on land use as defined in Figure 1. These areas are further described below.

Potential regional pond locations were identified (Figure 1). Initially, ponds are assumed to be wet basins with dead storage sized equal to the runoff volume from a 2.5" rainfall event per LRRWMO. Therefore, volume reduction and water quality would be accommodated on site by the developer. These parameters may be changed as final land uses are established and as stormwater management goals are confirmed. For example:

- The basins could be converted to dry ponds and accommodate volume control.
- Ground water elevations should be confirmed to ensure that 3' separation from the water table can be achieved.
- The basins may be used as regional rate control, water quality, and volume reduction thereby eliminating the need for additional stormwater management on site.

### **Area 1: Public/Quasi-Public Area**

This area is defined as Public/Quasi-Public. Currently, the site has been proposed to house the Legacy Christian Academy campus. However, development may change. Therefore we have determined the total inflow and approximate sizing requirements per Lower Rum River WMO (LRRWMO) for each assumed rate of impervious area construction including the school site, single family residential, and employment district. Table 2 summarizes required dead storage and volume reduction for each land use option based on LRRWMO requirements.

Suggested areas for ponding include the northwest corner of the site, and the existing pond on the



southeast portion of the site. If development plans change, these ponds can easily be relocated. Also, sizing requirements may change if the area becomes multi-use.

*Table 2: Summary of Pond Volume Requirements for Multiple Uses in the Public/Quasi-Public Area.*

Future Land Use	Assumed Impervious Area	Dead Storage	1" Volume
	(ac)	(ft <sup>3</sup> )	(ft <sup>3</sup> )
School	40.5	500339	146979
Residential	34.2	434558	124110
Employment District	76.5	632759	277586

**Area 2: Residential Area**

North of the 150<sup>th</sup> Ln NW and on both east and west sides of Puma Street, the land use is proposed as single family residential. East of Puma, the proposed site is approximately 49 acres of which about 38% will be impervious. Pond volume requirements for dead storage and volume reduction are, 88,200 cubic feet and 67,590 cubic feet respectively. Proposed ponding for this site is in the south west corner of the proposed development.

West of Puma, the proposed site is approximately 47 acres of which about 38% will be impervious. Pond volume requirements for dead and live storage are, 84,600 cubic feet and 64,832 cubic feet respectively.

The proposed area for ponding is in the southeast corner of the site. The proposed ponds would overflow into the existing wetland, maintaining the existing flow paths through the downstream corridor.

**Employment District Area (Area 3)**

South of 150<sup>th</sup> Ln NW and west of Puma Street, the city is proposing to use this land for future industrial/commercial development. This area totals 51 acres, of which 85% will be new impervious, resulting in pond volumes of 91,800 cubic feet for dead storage and 157,361 cubic feet for volume reduction.

South of Bunker Lake Boulevard, the city plans to use the 79 acres for further commercial/industrial development, 85% of which will be new impervious area. This will require 142,400 cubic feet and 243,210 cubic feet of dead storage and volume reduction, respectively. Drainage from this area will ultimately connect into the storm sewer system along Bunker Lake and discharge into the pond in the southeast corner of the school property.

**Recommendations**



The ponds as laid out in this report indicate areas that are best suited for regional rate control basins only. It is recommended that additional design parameters and regional stormwater management planning be further refined as the areas begin to develop. These may include the following.

- Create regional facilities that accommodate rate control, water quality, and volume reduction and assess property owners to alleviate the City's upfront investment.
- Develop drainage and utility easements for the required pond foot print for ultimate build-out and require the developer to increase the size of the basin based on a percentage of developed area and/or impervious surface.
- Establish maximum rates of impervious surface construction per land use. Anything above those rates would have to accommodate rate control, water quality, and volume reduction on site.
- Trunk storm sewer lines should also be preliminarily sized to serve future development, routed to regional pond locations, and dedicated to drainage and utility easement.

Cost estimates have not been developed for the regional pond construction or internal site storm sewer conveyance. It is anticipated that costs associated with pond construction will be determined by the developer as site grading. Also, it is assumed that internal drainage will be accommodated by dry swales and ditch systems to reduce storm sewer costs.

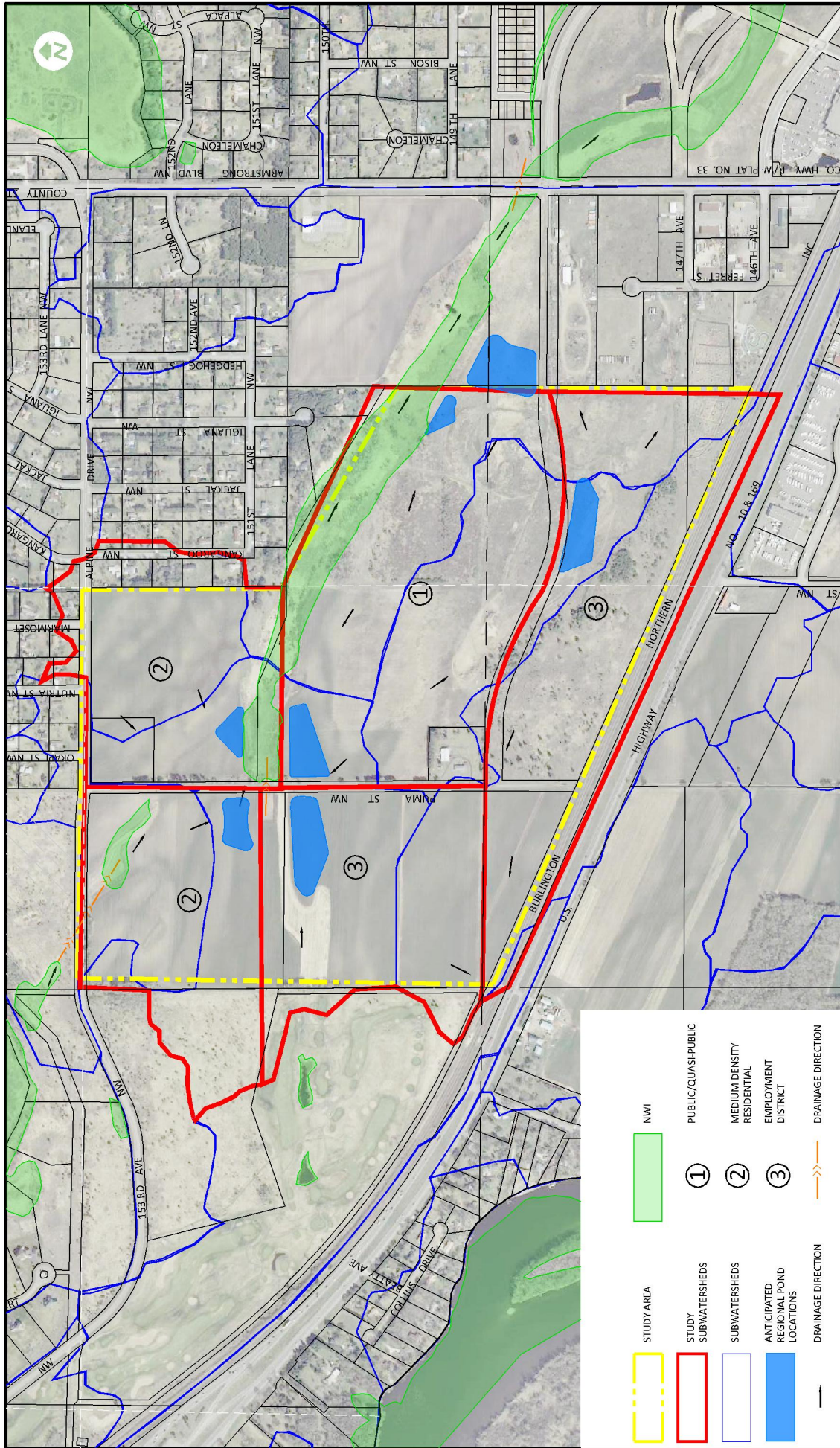
We appreciate the opportunity to present this preliminary report of findings. If you have any questions, please contact me at 651-704-9970 or [timol@bolton-menk.com](mailto:timol@bolton-menk.com) at your convenience.

Sincerely,

BOLTON & MENK, INC.

A handwritten signature in black ink that reads "Timothy J. Olson". The signature is written in a cursive, flowing style.

Timothy J. Olson, PE, CFM  
Water Resources Project Manager



**FUTURE BUSINESS PARK  
CITY OF RAMSEY, MINNESOTA**  
 FIGURE 16 - Regional Stormwater Considerations  
 June 2015



# REQUEST FOR QUOTES

Traffic Impact Study, Feasibility Report and Preliminary Design Layout:  
Improving Bunker Lake Boulevard and Puma Street in the City of Ramsey, Minnesota

## **SECTION 1: OVERVIEW**

The City of Ramsey is seeking quotes for the completion of a traffic impact study, feasibility report, and preliminary design layout of infrastructure improvements associated with the City's future business park. Improvements would be limited to (1) Bunker Lake Boulevard, between Armstrong Boulevard and Puma Street; and, (2) Puma Street, between Bunker Lake Boulevard and Alpine Drive; see Appendix for details.

It is the intent of this Request for Quotes (RFQ) to have the successful proposer enter into a professional services contract with the City of Ramsey.

## **SECTION 2: BACKGROUND**

### **City of Ramsey**

Located in the northwest Twin Cities Metro, the City of Ramsey is the fastest growing City in Anoka County. U.S. Highway 10, Minnesota Highway 169, Minnesota Trunk Highway 47 and the Northstar Commuter Rail connect the City of Ramsey to the Twin Cities and Greater Minnesota. Ramsey has an estimated population of 23,835, a median household income of \$76,560, is home to a significant manufacturing industry and is bordered by the Rum and Mississippi Rivers.

### **Future Business Park & Green-Field Development**

The Ramsey City Council and Ramsey Economic Development Authority (EDA) have identified the need to prepare for the development of a new business park within the City. The City is targeting a large area of privately owned green-field space located on the north side of U.S. Highway 10, west of Armstrong Boulevard, for a future Ramsey business park. Currently, the City is in the process of rezoning this area to meet future landuse goals.

The future business park is one component (about 92 acres) of a larger green-field area ready for development (about 350 acres). This larger green-field area includes areas for single-family residential development (about 118 acres), medium-density residential (about 31 acres) and room for a future private school campus (about 90 acres). In addition to green-field development, the City expects a portion of existing adjoining light-industrial space to be redeveloped into a traditional retail/commercial area (about 42 acres). See Appendix for a snapshot of future landuse goals.

The primary roadway system serving this developable area is Bunker Lake Boulevard and Puma Street. These roadways are both Municipal State-Aid (MSA) streets. The purpose of this RFQ is to study traffic impacts resulting from the future business park (along with other users), examine the feasibility of constructing supporting infrastructure, and developing preliminary design layout plans and specifications.

The City's future business park, and larger developable green-field area, will be served by the future U.S. Highway 10/Armstrong Boulevard (CSAH 83) interchange. Construction of this interchange will begin spring of 2015 and is expected for completion in the spring of 2017. The City expects the construction of this major interchange to increase the demand for development of this green-field area; and the future Ramsey business park.

**Request for Qualifications**

The primary purpose of this RFQ is to understand minimum required infrastructure specifications and infrastructure costs associated with developing this green-field area. This RFQ is specifically limited to Bunker Lake Boulevard, Puma Street and four intersections: (1) Bunker Lake Boulevard/ Armstrong Boulevard, (2) Bunker Lake Boulevard/ Puma Street, (3) Puma Street/ Alpine Drive (4) Armstrong Boulevard/ Alpine Drive. See Appendix A for details.

The City is seeking three deliverables:

1. Traffic Impact Study
2. Feasibility Report
3. Preliminary Design Layout

Please see Scope of Services section for additional details. Cost estimations and design should include design and construction costs for site grading, roadway, sidewalks, stormwater management, trails, landscaping, street lighting, trunk water service and trunk sewer service improvements.

### **SECTION 3: SUPPLEMENTAL INFORMATION**

1. The City is not master planning this entire developable area. It is unknown where internal driveways, roadways and curb-cuts will be needed. Therefore, the design of Bunker Lake Boulevard and Puma Street should not include any new curb-cuts.
2. Stubbing of sewer and water utilities along Bunker Lake Boulevard and Puma Street should be completed at regular intervals; based on future landuse needs per the recommendation of the firm awarded this work. This recommendation should be made with a balance of cost implications and market demand in mind (or experience with other business parks).
3. Cost estimations and design should include: roadway, sidewalks, stormwater management, trails, street lighting, trunk water service and trunk sewer service.
4. The intersection at Armstrong Boulevard was constructed in 2011. The City would like to confirm this intersection was constructed to accommodate traffic demands outlined in the Appendix landuse maps. This confirmation may be a simple narrative.
5. As-built of existing infrastructure is available upon request (e.g. Bunker Lake Boulevard was recently, partially, improved in anticipation of a new private school development).
6. The City has adopted Comprehensive Sanitary Sewer and Water Plans. These plans should be reviewed and consulted as forecasting and design work is completed.

## **SECTION 4: QUALIFICATIONS**

The following information is required and must accompany your response:

1. COVER LETTER/BACKGROUND INFORMATION: indicate your interest in providing professional services. Provide general background information on your firm. What is your experience with designing infrastructure for business parks? Do you have experience working with the City of Ramsey or nearby/similar communities?
2. FEE SCHEDULE: please describe required compensation for the proposed work. Based on the three deliverables outlined in the background section above, please breakdown required compensation into separate line items (phases).
3. CONFLICT OF INTEREST: In order to avoid a conflict of interest, or the appearance of a conflict of interest, your firm should not engage in any outside activities that are inconsistent, incompatible, or appear to conflict with your ability to exercise independent/objective judgment in the best interest of the City. Please outline all conflicts of interest that may exist for your firm in relation to providing professional services for the City.
4. GOOD STANDING: Your firm must be in compliance with Federal, State, County and Local units of government; which specifically includes good tax payment status and good corporate registration status. Please indicate your firm's tax and corporate registration status.
5. RECOMMENDATIONS: It is the City's goal to plan for a cost-effective and market-relevant business park. The City is interested in your professional recommendations regarding the proposed scope of work and project. Additionally, the City is looking for a recommendation related for the need/effectiveness of planning for a potential regional stormwater pond(s) to support the new business park locations identified in the appendix (add-on to scope of services).

## **SECTION 5: SCOPE OF SERVICES**

- A. TRAFFIC IMPACT STUDY: Based on future landuses indicated in the Appendix, the City would like a traffic impact study conducted to determine traffic forecasts on Bunker Lake Boulevard and Puma Street. The purpose of this information is to determine minimum roadway design specifications for future improvements to Bunker Lake Boulevard and Puma Street.
  
- B. FEASIBILITY REPORT: Based on a completed traffic impact study, the City would like a feasibility report completed with a preliminary cost estimate for any required improvements to Bunker Lake Boulevard and Puma Street. The purpose of this report is to establish the feasibility and estimated design and construction costs for completing the required site grading, roadway, sidewalks, stormwater management, trails, landscaping, street lighting, trunk water service and trunk sewer service improvements to support the future business park; and provide a foundation for budgeting discussions among City officials. This report will be based off of the design developed within the preliminary design layout (see step below). Easement and/or right-of-way impacts/needs must also be addressed in the report.

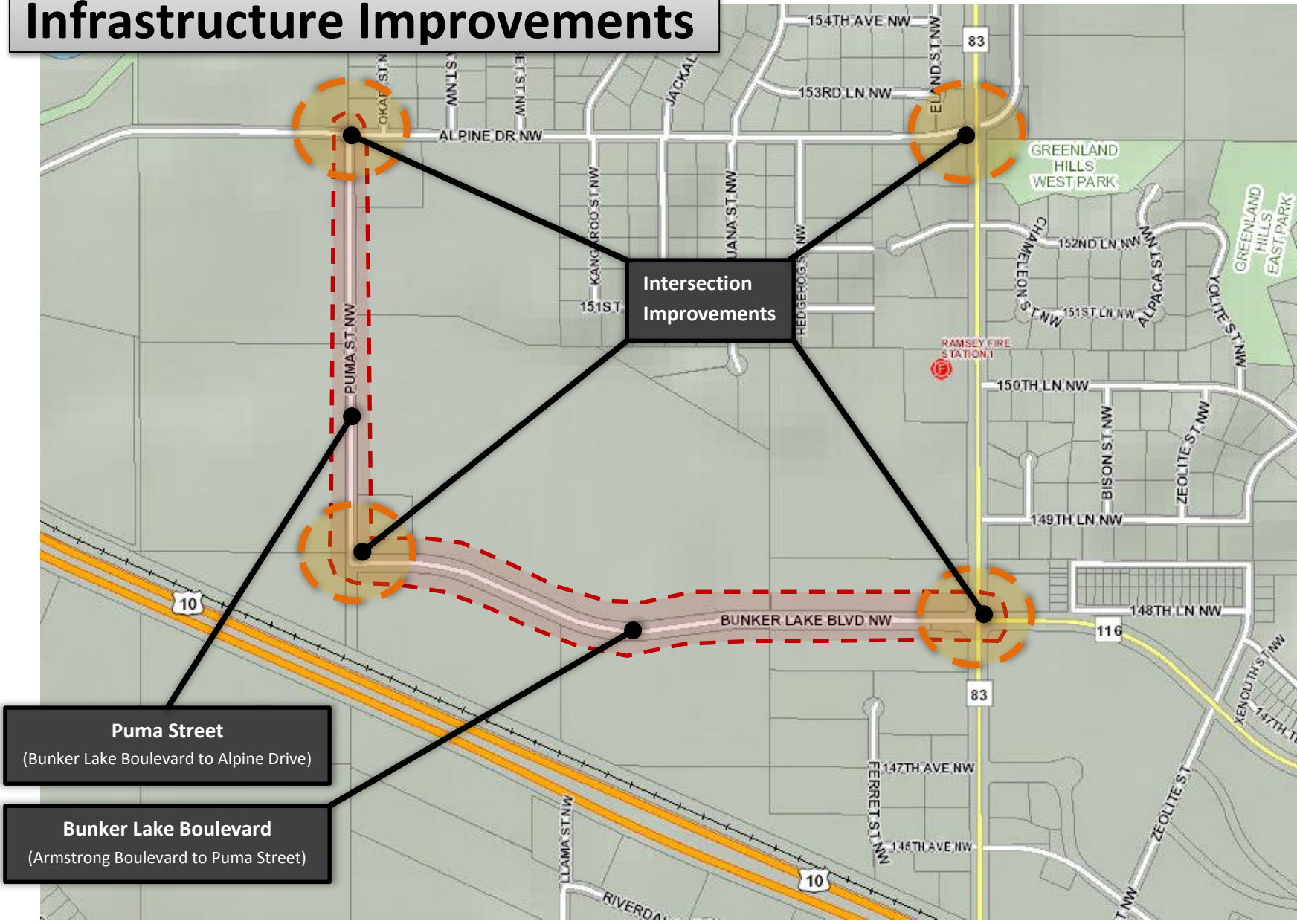
NOTE: It is likely, the proposed improvements will be constructed in stages, depending on market demand. Therefore, the feasibility report identified above shall also include costs for individual phases. The City anticipates three development phases. However, this decision will be made with input from the selected engineering firm.

- C. PRELIMINARY DESIGN LAYOUT: Develop preliminary roadway and intersection design layout based on Municipal State-Aid minimum standards and City of Ramsey development standards. This layout should include preliminary profiles and typical sections for Bunker Lake Boulevard and Puma Street, and show all proposed site grading, roadway, sidewalks, stormwater management, trails, landscaping, street lighting, and trunk water and sewer service improvements, as well as any proposed easement or right-of-way impacts/needs.
  
- D. Work with (and advise) City on strategies for infrastructure design.
  
- E. Work with (and advise) City on strategies for assessing/paying for required infrastructure.
  
- F. Provide regular activity updates, verbally, to the City's development team, a minimum two times per month
  
- G. Completion dates:
  - a. Traffic Impact Study February 28, 2015
  - b. Feasibility Report February 28, 2015
  - c. Layout/Plans & Specifications for Preliminary Design March 31, 2015

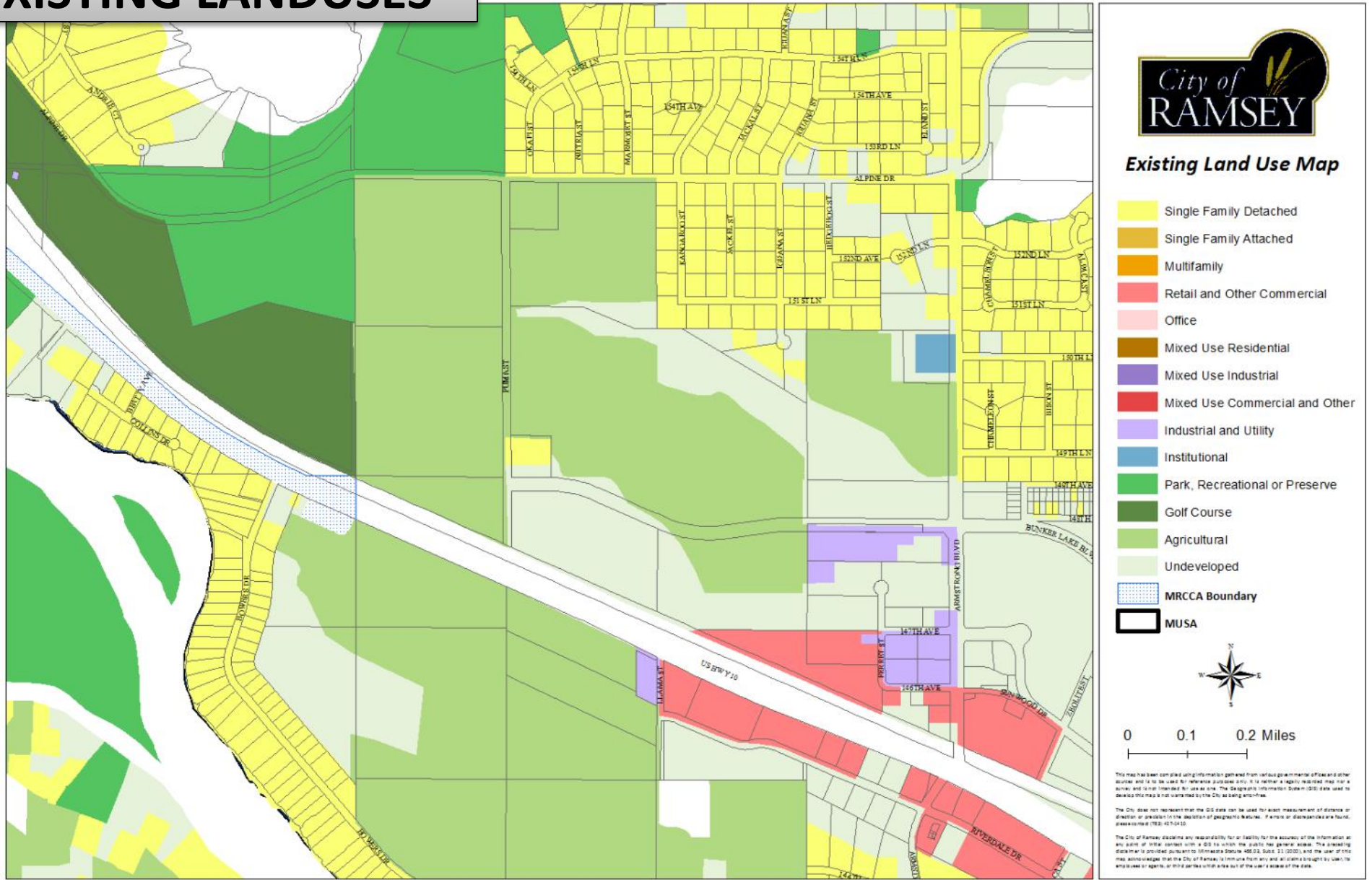
## **SECTION 6: APPLICATION PROCESS**

1. Responses must be submitted in a sealed envelope marked "Future Business Park RFQ" to the City Clerk's office, 7550 Sunwood Drive NW, Ramsey, Minnesota 55303 before 3:00 p.m. on **January 05, 2015**, at which time they will be publically opened and read.
2. Responses will be forwarded to a selection committee. At the discretion of the City, if needed, a list of finalists may be selected for interviews. Interviews would take place on one of the following dates: **January 7th or 8th, 2015** (depending on schedules).
3. A recommendation will be crafted by the selection committee and presented to the Ramsey EDA for direction on **January 15, 2015**.
4. All proposals, plans, and other documents submitted shall become the property of the City. Responses to this RFQ are considered public information and are subject to discovery under the Freedom of Information Act.
5. To be considered, firms must submit a complete response to the RFQ in the form requested. Firms not responding to items requested in the RFQ or indicating exceptions to such items may have their submittals rejected. Issuance of this RFQ and receipt of proposals does not commit the City of Ramsey to award a contract. The City of Ramsey reserves the right to postpone receipt date, accepting or rejecting any or all proposals received in response to this RFQ, or to negotiate with any of the proposers submitting an RFQ, or to cancel all or part of this RFQ. The City reserves the right to award work in phases; rather than one single project.
6. Questions/Contact:
  - Patrick Brama, Economic Development Manager, 763-433-9868, [pbrama@cityoframsey.com](mailto:pbrama@cityoframsey.com)
  - Bruce Westby, City Engineer, 763-433-9825, [bwestby@cityoframsey.com](mailto:bwestby@cityoframsey.com)
  - Kurt Ulrich, City Administrator, (763) 433-9845, [kulrich@cityoframsey.com](mailto:kulrich@cityoframsey.com)

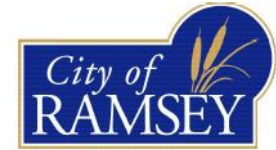
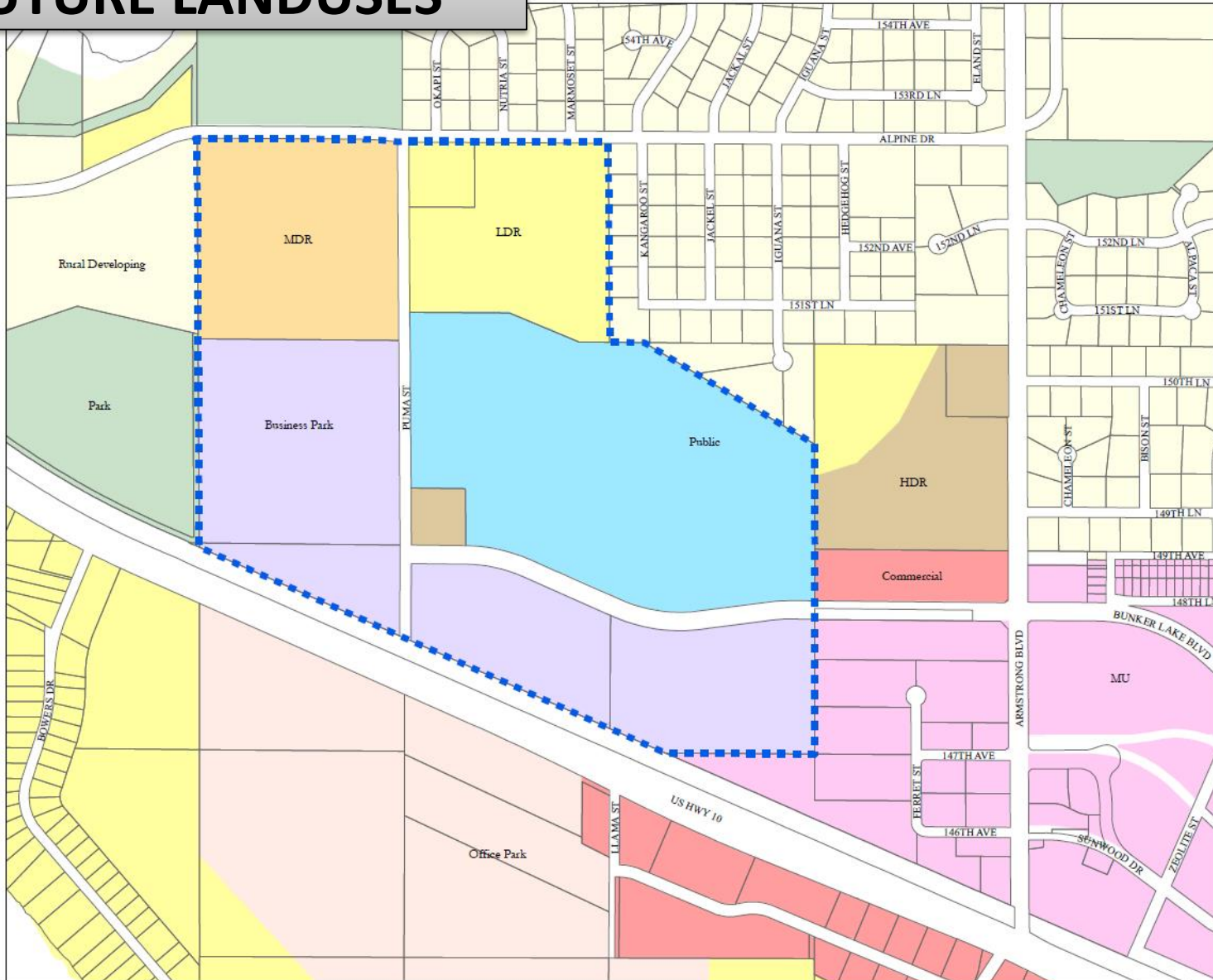
# Infrastructure Improvements



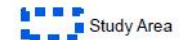
# EXISTING LANDUSES



# FUTURE LANDUSES



## 2030 Comprehensive Plan Future Land Use Map PENDING



Study Area

### Future Land Use

- Low Density Residential
- Medium Density Residential
- High Density Residential
- Office Park
- Commercial
- MU
- Business Park
- Public
- Rural Developing
- Rural Preserve
- Park



0 300 600 Feet  
1 inch = 600 ft on 11 x 17

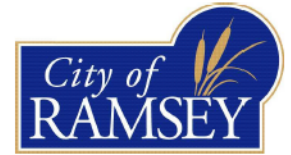
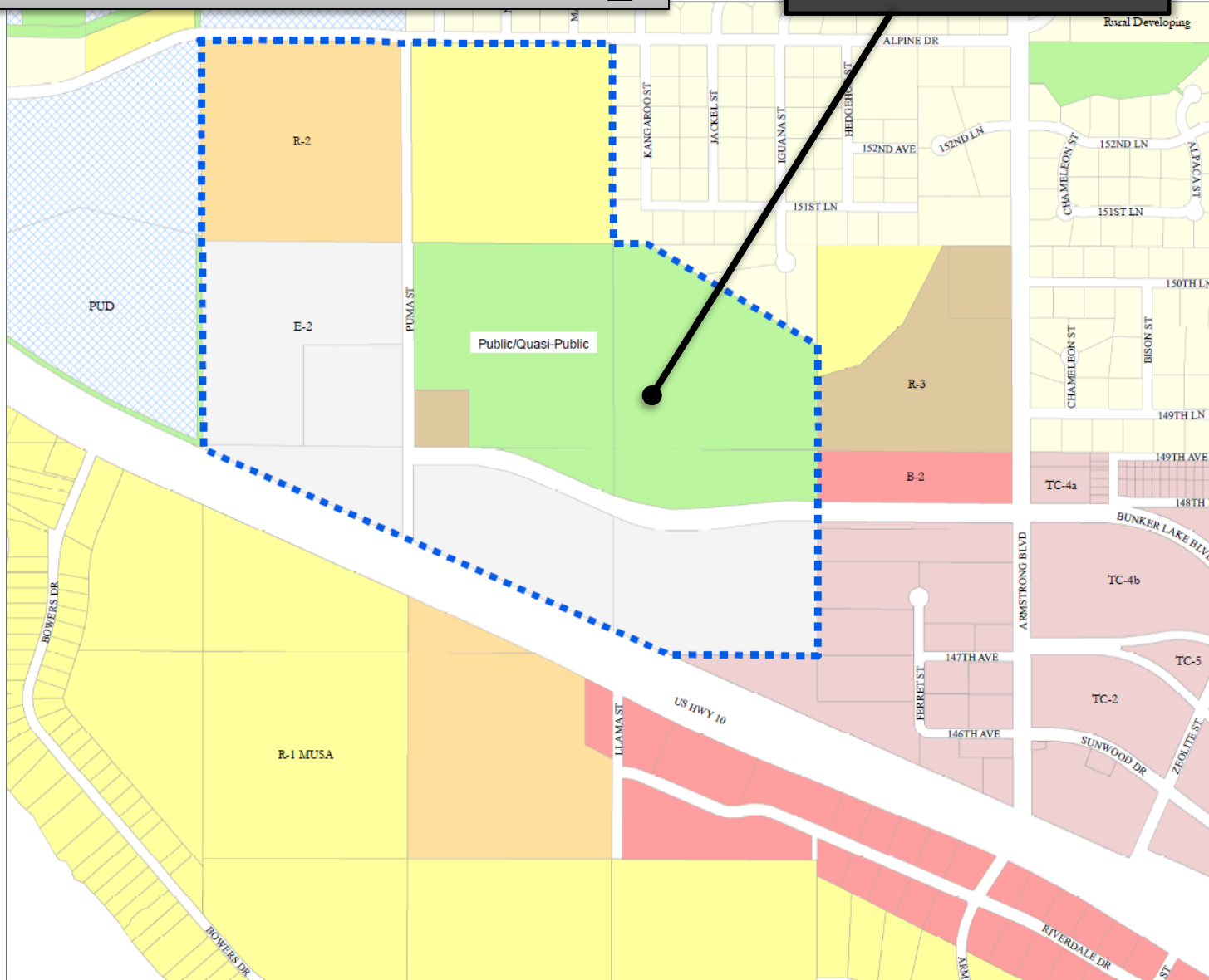
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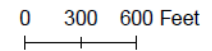
# FUTURE LANDUSES: Zoning

Proposed Private School Campus  
(currently green space)



## Zoning Map PROPOSED/PENDING

- Study Area
- Zoning District**
- R-1: Rural Developing (outside MUSA)
- R-1: MUSA
- R-2: Medium-Density Residential
- R-3: High-Density Residential
- B-1: Business District
- B-2: Business District
- H-1: Business District
- E-1: Employment District
- E-2: Employment District
- MU-PUD: Mixed-Use, Planned Unit Development
- PUD: Planned Unit Development
- COR: The COR
- P: Public/Quasi-Public District



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**Regular Planning Commission**

6. 1.

**Meeting Date:** 08/06/2015

**By:** JoAnn Shaw, Community Development

---

**Information**

**Title:**

Zoning Bulletins

**Purpose/Background:**

Enclosed are zoning periodicals for your review.

**Notification:**

**Observations/Alternatives:**

**Funding Source:**

**Recommendation:**

**Action:**

---

**Attachments**

Zoning Bulletins

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**Form Review**

**Inbox**

Tim Gladhill

Form Started By: JoAnn Shaw

Final Approval Date: 07/31/2015

**Reviewed By**

Tim Gladhill

**Date**

07/31/2015 02:09 PM

Started On: 07/27/2015 11:46 AM

# Zoning Bulletin

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Telecommunications—County denies conditional use permit application for cellular tower	5
Nonconforming Use—Restaurant obtains permit for live entertainment	7
Enforcement of Regulations—Property owner appeals notice of town ordinance violation	10
Zoning News from Around the Nation	11



## Nonconforming Use—City says nonconforming use ordinance applies to individual mobile home park lots

Mobile home park lot owner contends ordinance applies only to park use as a whole

Citation: *Cleveland MHC, LLC v. City of Richland*, 2015 WL 2250376 (Miss. 2015)

MISSISSIPPI (05/14/15)—This case addressed the issue of whether a

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mobile-home park as a whole, or the individual lots in a mobile-home park, are the nonconforming use resulting from a park's location in a zone that prohibits residential use.

**The Background/Facts:** Since the 1950s, Cleveland Mobile Home Community ("CMHC") had been operating in the city of Richland (the "City") in Rankin County (the "County"). CMHC was a mobile-home park that included spaces for 138 mobile homes and seventeen campers or recreational vehicles, which were rented to tenants. In 1975, the City zoned the land on which CMHC operated as "I-1, Light Industrial Zoning." In that zone, residential uses were prohibited. Accordingly, as of 1975, CMHC became a nonconforming use.

Regarding nonconforming uses, the City ordinances provided that nonconforming lots, uses, or structures were allowed "to continue until they are removed" but the "survival" of the nonconformity is not encouraged. Further, City ordinances provided that nonconformities "shall not be enlarged upon, expanded[,] or extended, [nor] be used as grounds for adding other [s]tructures or uses prohibited elsewhere in the same district."

For years, while CMHC was a nonconforming use, mobile homes were removed from the property and replaced. Cleveland MHC, LLC (the "Park Owner") purchased the mobile-home park in 2008. In April 2011, apparently due to deterioration of the property, the City informed the Park Owner that it would begin enforcing the zoning ordinance and, when an existing mobile home was removed, it could not be replaced.

The Park Owner appealed to the City's Board of Aldermen (the "Board"). The Board upheld the City's decision and voted unanimously to adopt the following resolution: "That in the event a mobile home or similar vehicle is removed from its then present location in the Cleveland Mobile Home Park, another mobile home or similar vehicle shall not be placed on the vacated site."

The Park Owner filed an appeal in the County circuit court; the circuit court held that the Board's action was not in error.

The Park Owner again appealed. The Court of Appeals held that the City's interpretation of the ordinance was arbitrary and capricious and that it deprived the Park Owner of its "constitutional right to enjoy [its] property." More particularly, the Court of Appeals held that the City's nonconforming use ordinance applied to the "mobile-home park as a whole," not to individual lots within the park. Thus, held the Court of Appeals, as long as the Park Owner operated as a mobile-home park and did not expand, its operation was a permitted use.

The City appealed.

**DECISION: Judgment of Court of Appeals affirmed.**

As a matter of first impression (i.e., the first time the court ruled on the issue), the Supreme Court of Mississippi held that the mobile-home park as a whole, rather than individual lots within the park, were the nonconforming use resulting from the park's location in a zone that prohibited residential uses.

In so holding, the court explained that since the individual lots in the mobile-home park were rented to tenants and not owned individually, the

nonconforming use belonged to the Park Owner and the nonconformity was the Park Owner's use of the land as a mobile-park home. While the individual structures in the park were nonconformities in themselves, they made up parts of the whole, said the court.

Having found that the nature of the nonconforming use was the Park Owner's use of the property as mobile-home park, the court concluded that the City's interpretation of the nonconforming use ordinance to apply on a lot-by-lot basis was arbitrary, capricious, and illegal in that: (1) it deprived the Park Owner of its constitutional right to enjoy its property, as the resolution effectively would destroy the mobile-home park; and (2) the City allowed mobile homes to be moved on and off the property for decades without interpreting or enforcing the nonconforming use ordinance on a lot-by-lot basis, and only did so now seemingly without reason and with an apparent "disregard for the surrounding facts and settled controlling principles."

See also: *Stagecoach Trails MHC, L.L.C. v. City of Benson*, 232 Ariz. 562, 307 P.3d 989 (Ct. App. Div. 2 2013).

See also: *Eddins v. City of Lewiston*, 150 Idaho 30, 244 P.3d 174 (2010).

See also: *Sunset Estate Properties, L.L.C. v. Village of Lodi*, 2013-Ohio-4973, 2013 WL 6021470 (Ohio Ct. App. 9th Dist. Medina County 2013), appeal allowed, 138 Ohio St. 3d 1432, 2014-Ohio-889, 4 N.E.3d 1050 (2014) and judgment aff'd, 142 Ohio St. 3d 351, 2015-Ohio-790, 30 N.E.3d 934 (2015).

Compare: *City of Foley v. McLeod*, 709 So. 2d 471 (Ala. 1998) (holding that zoning ordinance, which prohibited enlarging or expanding nonconforming use of land and prevented repair or restoration of nonconformities that were damaged beyond 50% of their replacement value, applied to replacement of existing mobile homes at nonconforming mobile home park).

---

*Case Note:*

*In its holding, the court made clear the distinction between nonconforming structures—which cannot, under Mississippi law, be replaced or rebuilt with another nonconforming structure—and nonconforming use of property, such as that here—which has not been destroyed or changed.*

---

## Telecommunications—County denies conditional use permit application for cellular tower

Permit applicant argues denial failed to comply with Telecommunications Act procedural requirements and was not supported by substantial evidence

Citation: *Smith Communications, LLC v. Washington County, Ark.*, 2015 WL 2193027 (8th Cir. 2015)

*The Eighth Circuit has jurisdiction over Arkansas, Iowa, Minnesota, Missouri, Nebraska, North Dakota, South Dakota.*

EIGHTH CIRCUIT (ARKANSAS) (02/15/11)—This case addressed the issues of whether: (1) a county provided a telecommunications conditional use permit applicant with adequate notice of the reasons for the county's denial of the permit; and (2) the county's denial of the permit application was supported by substantial evidence.

**The Background/Facts:** Smith Communications, LLC ("Smith") sought to construct a 300-foot-tall cellular tower in Washington County, Arkansas (the "County"). The property on which the proposed tower was to be located was zoned "Agriculture/Single-Family Residential." Smith applied to the County for a conditional use permit to build the cellular tower.

Section 11-200(a) of the County's zoning code governs applications for conditional use permits ("CUPs"). Among other things, § 11-200(a) provides a CUP may be granted if the proposed use: ". . . (4) . . . is compatible with the surrounding area; (5) . . . will not be detrimental to or endanger the public health, safety, moral, comfort or general welfare; [and] (6) . . . will not be injurious to the use and enjoyment of other property in the surrounding area for the purposes already permitted, nor substantially diminish or impair property values in the surrounding area."

The County Planning Board approved Smith's CUP application.

Local residents appealed that approval to the County's Quorum Court. They argued, among other things, that the proposed cellular tower would detrimentally affect their property values and would not be compatible with the surrounding area. The Quorum Court held two meetings on the appeal, and ultimately voted to reject Smith's CUP application, finding it failed to meet the requirements of zoning code § 11-200(a)(4), (5), and (6).

Four days after the Quorum Court reached its decision, the County sent Smith an e-mail containing a letter of denial for the CUP application. Approximately one hour later, the County sent Smith another e-mail stating that "[t]he minutes and video of the first and last Quorum Court meetings will act as the County's written reason for denial." At that time, minutes from the first meeting had already been available to Smith for a week; minutes from the

second meeting, however, were not available to Smith until several weeks later.

Smith filed suit against the County. Smith asserted that the County failed to provide a legally adequate explanation of its reasons for the denial and that the denial was not based on substantial evidence in violation of the Telecommunications Act of 1996 (the "Act"). (47 U.S.C. § 332(c)(7)(B)(iii) and (v).)

The district court held that the County could not rely on the meeting minutes to constitute a legally adequate explanation for the denial under the Act. The court, therefore, remanded the matter to the Quorum Court and required the County to explain the reasons for its denial in a writing separate from the minutes and written record. Accordingly, on April 18, 2014, the County filed an additional explanation, again reciting the requirements of § 11-200(a)(4), (5), and (6) and stating that the denial was based on the tower's proximity to surrounding properties, its detrimental impact on neighboring residents' "exceptional view," and its "incompatib[ility] with the surrounding area." The district court ultimately found that new statement complied with the Act. The district court also concluded that substantial evidence supported the County's denial of Smith's CUP application.

Smith appealed. On appeal, Smith argued that: (1) the district court failed to provide expedited review of Smith's appeal of the CUP denial, as required by the Act (47 U.S.C.A. § 332(c)(7)(B)(v)), because the district court "should have simply 'order[ed] the issuance of a permit' upon concluding that [the County] had failed to provide an adequate written explanation for its denial"; and (2) the County violated the Act by denying the CUP application without substantial evidence supporting its decision.

**DECISION: Judgment of district court affirmed.**

The United States Court of Appeals, Eighth Circuit, first held that the County had provided a legally adequate explanation for its CUP denial when it referred Smith to the meeting minutes for an explanation of the denial. Under the law, said the court, "a locality may rely on detailed meeting minutes" to provide its written reasons for denial, "so long as the locality's reasons are stated clearly enough to enable judicial review." Here, the court found that the meeting minutes were sufficiently clear to "enable judicial review." The meeting minutes from the first meeting alone contained 30 pages of detailed notes from the participants' presentations and discussions about the tower.

Still, the court did recognize that, by failing to provide Smith with the meeting minutes from the second meeting until 24 days after notifying Smith of its denial, the County violated the Act's requirement that written reasons be made available at "essentially the same time as [the locality] communicates its denial." However, the court found that failure was, at most, a harmless error, and did not require the district court to grant Smith immediate relief by ordering the issuance of a CUP since the meeting minutes from the first meeting were already available at the time of the written denial and informed Smith of the reasons for the denial. Thus, the court concluded that Smith received adequate notice of the reasons for the Quorum Court's denial of its CUP application, in compliance with the Act.

The court also held that substantial evidence supported the Quorum Court's

denial of Smith's application. The court explained that the Act's "'substantial evidence' requirement is 'directed at whether the local zoning authority's decision is consistent with the applicable local zoning requirements.'" Here, the court found that the Quorum Court had analyzed the 300-foot tower's placement and proximity to nearby residences and reviewed evidence (including pictures and simulations) of the specific areas in question, and that it had similarly evaluated arguments and evidence about, among other things, the tower's detrimental impact on neighboring residents' unique views and property values. "Keeping in mind that the substantial evidence standard is 'essentially deferential,' [the court concluded] that the [Quorum Court] had before it substantial evidence on the record as a whole that the tower's scale, its proximity to residences, and the surrounding environment made approval inappropriate in view of the considerations outlined in [the County zoning code § 11-200(a)]."

See also: *T-Mobile South, LLC v. City of Roswell, Ga.*, 135 S. Ct. 808, 816, 190 L. Ed. 2d 679 (2015).

See also: *Sprint Spectrum, L.P. v. Platte County, Mo.*, 578 F.3d 727 (8th Cir. 2009).

## Nonconforming Use—Restaurant obtains permit for live entertainment

Opponents appeal grant of permit, arguing permit applicant failed to establish existence of legal nonconforming use of live entertainment

Citation: *Vieux Carre Property Owners v. City of New Orleans*, 2014-825 La. App. 4 Cir. 4/15/15, 2015 WL 1736870 (La. Ct. App. 4th Cir. 2015)

LOUISIANA (04/15/15)—This case addressed the issue of whether a restaurant had established the existence of a legal nonconforming use of live entertainment.

**The Background/Facts:** Antoine's Restaurant, L.L.C. ("Antoine's") has been operating a restaurant in New Orleans (the "City") since 1840. Since 1940, it has been operating a restaurant out of a complex of buildings and several distinct dining areas that are all connected to one another. In 2008, Antoine's began renovating its Hermes Room into "Hermes Bar." On April 11, 2011, Antoine's received a letter from the Louisiana Department of Safety and Permits (the "Department") stating that live entertainment was not a permitted use at "713 St. Louis Street (Hermes Bar)" because the property was zoned VCC-2, Vieux Carre Commercial, under the Comprehensive Zoning Ordinance ("CZO").

Upon receipt of the letter, Antoine's filed an application for a Mayoralty Permit for its live entertainment. Under the City's Code of Ordinances, a Mayoralty Permit may be issued for certain activities such as live entertainment. Along with the application, Antoine's submitted 14 affidavits

from staff members and owners of Antoine's attesting to each person's knowledge that Antoine's had offered live entertainment as part of its operation on a continuous, uninterrupted basis, since as far back as 1955.

The Department's Director, Paul May, reviewed the application and affidavits. The Director approved the issuance of the Mayoralty Permit for live entertainment to Antoine's on April 15, 2011 "pursuant to affidavits of non-conforming use."

Subsequently, the Vieux Carre Property Owners, Residents & Associates, Inc., French Quarter Citizens, Inc., and Carol Allen (collectively the "Opponents") filed an appeal of the Director's decision with the City's Board of Zoning Appeals ("BZA"). The Opponents argued, among other things: that the live entertainment permit issued "at a cocktail lounge at one property (713 St. Louis)" was being used "in an attempt to legitimize an illegal use at another parcel (725 St. Louis)"; and that Antoine's failed to establish a nonconforming use because the submitted affidavits failed to establish the "4-hour, 5-day use required by the CZO." The Opponents argued that CZO Article 13, § 13.6.1 required a nonconforming use be continuously operating for four hours a day, five days a week, and that Antoine's did not offer that frequency or duration of live entertainment.

The BZA denied the Opponent's appeal.

The Opponents again appealed. The district court affirmed the BZA's decision.

The Opponents again appealed. On appeal, the Opponents argued that Antoine's affidavits failed to provide sufficient information about the regular, continuous use of live entertainment to establish a legal nonconforming use of live entertainment.

**DECISION: Judgment of district court affirmed.**

The Court of Appeal of Louisiana held that Antoine's had established the existence of a legal nonconforming use of live entertainment.

In so holding, the court explained a nonconforming use is defined as "[a] use which lawfully existed prior to the enactment of a zoning ordinance, and which is maintained after the effective date of the ordinance although it does not comply with the use restrictions applicable to the area in which it is situated." The court further explained that a property owner who seeks to establish and retain a nonconforming use has the burden of producing evidence that the nonconforming use of the property has been continuous and consistent. "What constitutes sufficient usage to establish nonconforming status may vary with the facts of each case," but the nonconforming use must be continuous and not interrupted for a period in excess of six months, said the court. In addition, noted the court, the CZO placed the burden on Antoine's (as the property owner) to establish the existence and retention of a nonconforming use by clear and convincing evidence. (CZO Article 13, § 13.2.1.) Under the CZO, such evidence "shall include, but need not be necessarily restricted to; such documents, as rent receipts, affidavits, documentation of utility services, or other information as may be deemed necessary in a particular case." (CZO Article 13, § 13.7.) At the time a permit or certificate of occupancy is filed, the property owner must produce acceptable evidence to the Director of Safety

and Permits for an initial determination of the existence of the nonconforming use, said the court.

In this case, Antoine's had sought a permit for the existence of its nonconforming use of live entertainment and submitted 14 affidavits from employees and members of the family owning Antoine's. The court found that each affidavit indicated the length of time the affiant had worked at and been "familiar with all facets of the operations of Antoine's," dating back to 1955; and the court found that each affiant attested that Antoine's "has had live entertainment with no interruptions in excess of six months as part of the Antoine's experience" during the entire period of the affiant's tenure at Antoine's.

Again, the Opponents had argued that, to establish a legal nonconforming use of live entertainment, more specific information than that provided in Antoine's submitted affidavits was required about the regular, continuous use of live entertainment. The Opponents had argued that the affidavits lacked necessary specificity regarding the frequency and duration of the nonconforming use, as required by the 4/5 rule of CZO Article 13, § 13.6.1.

However, the Department's Director had interpreted the 4/5 rule in terms of the use of the property to operate a business. The Director had found that the 4/5 rule was satisfied here by the fact that Antoine's operated a business upon the property for four hours a day, five days a week, and the nonconforming use for live entertainment had been a regular and continuous part of Antoine's operations. The Director had concluded that the nonconforming use itself did not need to continuously operate for four hours a day, five days a week.

Here, the appellate court found that the evidence (the affidavits) provided by Antoine's established that Antoine's had been operating at the property since prior to the original 1956 Code of Ordinances and had offered live entertainment as a regular and continuous part of its operation of the restaurant complex since 1955, 15 years prior to the adoption of the 1970 ordinance restricting live entertainment in the zoning district in which Antoine's was located. The court noted that, on the basis of that evidence submitted by Antoine's the Director made a decision, and the BZA affirmed, that the existence of a nonconforming use had been sufficiently established. The court noted that the Opponents failed to produce any contradicting evidence. Accordingly, the court affirmed that BZA's decision upholding the Department Director's issuance of the Mayoralty Permit for live entertainment to Antoine's based on establishment of a legal nonconforming use of live entertainment.

See also: *Humphrey v. Robertson*, 709 So. 2d 333 (La. Ct. App. 4th Cir. 1998).

See also: *Phillips' Bar & Restaurant, Inc. v. City of New Orleans*, 116 So. 3d 92 (La. Ct. App. 4th Cir. 2013), writ denied, 123 So. 3d 1226 (La. 2013) and writ denied, 123 So. 3d 1227 (La. 2013).

## Enforcement of Regulations— Property owner appeals notice of town ordinance violation

Court addresses issue of whether notice of violation is appealable

Citation: *Paradis v. Town of Peru*, 2015 ME 54, 2015 WL 2114385 (Me. 2015)

MAINE (05/07/15)—This case addressed the issue of whether a notice of ordinance violation could be appealed.

**The Background/Facts:** In 2010, Donald R. Paradis (“Paradis”) applied for and obtained a building permit to construct a two-car garage on a parcel of property in Peru, Maine (the “Town”). In August 2013, the Town sent Paradis a notice of violation that the garage violated multiple town ordinance provisions.

Paradis appealed to the Town’s Board of Appeals (the “Board”). The Board concluded that the town ordinance provisions had been properly applied. The Board denied Paradis’ appeal.

Paradis again appealed to superior court. The superior court affirmed the Board’s judgment.

Paradis again appealed.

**DECISION: Judgment of superior court vacated, and matter remanded.**

The Supreme Judicial Court of Maine held that the notice of violation issued to Paradis was not an appealable decision. As such, the court did not reach the merits of the appeal.

The court noted that, under Maine law, “[u]ntil very recently, appeals of notices of violation were not justiciable because a notice merely provided an interpretation of an ordinance; unless and until a municipality acted to enforce the decision in some meaningful way, appeals from notices of violation were ‘dismissed as calling for an advisory opinion.’” The court further noted that, in 2013, however, the Maine Legislature enacted a statutory amendment providing for board of appeals and Superior Court review of municipal notices of violation. Thus, since October 2013, under 30-A M.R.S. § 2691(4), “[a]bsent an express provision in a charter or ordinance that certain decisions of its code enforcement officer or board of appeals are only advisory or may not be appealed, a notice of violation or an enforcement order by a code enforcement officer under a land use ordinance is reviewable on appeal by the board of appeals and in turn by the Superior Court under the Maine Rules of Civil Procedure, Rule 80B.”

Here, the notice of violation against Paradis was sent before the effective date of § 2691(4). Moreover, the Town’s Ordinance expressly stated that no appeal from a notice of violation could be taken. (The Town’s Ordinance did

allow administrative or variance appeals but not those for “enforcement-related matters.”) Thus, the appellate court concluded, § 2691(4) by its express terms, did not provide Paradis with a right to appeal. Accordingly, the appellate court concluded that the Board lacked jurisdiction to consider Paradis’s appeal, which in turn deprived the superior court of jurisdiction to consider it, and further precluded the appellate court’s review of the merits of the matter.

See also: *Dubois Livestock, Inc. v. Town of Arundel*, 2014 ME 122, 103 A.3d 556 (Me. 2014).

## Zoning News from Around the Nation

### ILLINOIS

A state House committee has unanimously endorsed Senate Bill 920, which would allow some Illinois counties and municipalities to create zoning regulations for wind farms. The legislation is intended to clarify existing law regarding the placement of wind turbines. Among other things, the legislation provides that a county board must give public notice of where a windmill is set to go no more than 30 days before it is constructed. Any county zoning code created from the legislation does not affect wind farms already in place. Senate Bill 920 already advanced through the Senate.

Source: *The Southern Illinoisian*; <http://thesouthern.com>

### KENTUCKY

Proposed state regulations “would allow digital LED billboards throughout the state and let billboard owners cut trees on public rights of way that block visibility of their outdoor advertisements.” The proposed regulations “would allow billboard companies to take down four traditional billboards to create one new LED billboard, or the companies could take down three static billboards to convert one static billboard into an LED billboard, which has an electronic screen that cycles through advertisements.” The regulations were recently presented by Kentucky Transportation Cabinet officials. Environmental groups are reportedly opposing the proposed regulations, arguing that they could “lead to an explosion of light-polluting billboards, environmental destruction and property devaluation.” Meanwhile, “[I]obbyist for the outdoor advertising industry countered that the regulations would actually decrease the number of billboards in the state, and that vegetation would be removed in an environmentally responsible manner.” The regulations “could take effect as early as October after being reviewed by the legislature’s Administrative Regulation Review Subcommittee and the Interim Joint Committee on Transportation.”

Source: *Lexington Herald-Leader*; [www.kentucky.com](http://www.kentucky.com)

### MARYLAND

In Ocean City, drafted legislation that would establish an R-1A Single Family Residential District and prohibit short-term rentals was forwarded to public hearing.

Source: *The Dispatch*; <http://mdcoastdispatch.com>

## TEXAS

Governor Greg Abbott has signed House Bill 40 into law. The new law reportedly puts “unprecedented restrictions on the ability of cities and other local governments to regulate the oil and gas industry.” “HB 40 requires that any local regulation of the oil and gas industry be ‘commercially reasonable,’ ” and defines “commercially reasonable” as “a condition that would allow a reasonably prudent operator to fully, effectively, and economically exploit, develop, produce, process, and transport oil and gas.”

Source: *Denton Record-Chronicle*; [www.dentonrc.com](http://www.dentonrc.com)

# Zoning Bulletin

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## Uses—Planning commission approves wind turbine project

Adjacent landowners challenge approval, arguing it prejudices them

Citation: *Dunmoyer v. Wells County*, 2015 WL 2229452 (Ind. Ct. App. 2015)

INDIANA (05/12/15)—This case addressed the issue of whether a planning commission's approval of a wind turbine project prejudiced neighboring landowners such that landowners were entitled to relief from the decision.

**The Background/Facts:** In March 2013, West County Wind II, LLC, Apex Clean Energy Holdings, LLC, and Apex Wind Energy, Inc. (collectively,

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“Apex”) filed with the Wells County, Indiana, Area Plan Commission (the “Plan Commission”) a petition for the development of a large wind energy conversion system (“WECS”) project. The WECS project sought to construct approximately 68 wind turbines on private property located in southern Wells County, Indiana.

Development of the WECS proposed project was governed by the Wells County Zoning and Floodplain Management Ordinance (“the Zoning Ordinance”). Apex proposed to build the WECS project on land in Wells County that was zoned “A-1,” Agriculture-Intensive. Pursuant to the Zoning Ordinance, large WECS projects were permitted uses in A-1 districts provided that they complied with the provisions of Zoning Ordinance Article 15. Article 15 set forth specific requirements with which a WECS project must comply, including: property setback; noise; shadow flicker; color and finish; signage; and safety and installation standards. Article 15 also required review and approval of a WECS Project Development Plan be conducted in accordance with the requirements of Zoning Ordinance Article 14. Article 14 of the Zoning Ordinance addressed development plans in general, including requirements that development plans be: compatible with the surrounding land uses; harmonious with adjacent buildings and grounds; and allow for a total visual impression that is consistent with the environment of the neighborhood.

Adjacent landowners (the “Landowners”) opposed Apex’s proposed WECS project. The Landowners noted that the WECS project would result in wind turbines being in close proximity to their homes. They contended that the WECS project would: decrease their property values; subject them to shadow flicker and noise from the wind turbines; and result in a loss of use and enjoyment of their land.

Ultimately, the Plan Commission approved Apex’s WECS Project Development Plan.

The Landowners appealed. Among other things, they argued that the Development Plan failed to comply with other requirements of the Zoning Ordinance, including compliance with: floodplain management; traffic management; and environmental performance standards. They also argued that the Plan Commission made no findings regarding whether the WECS project was compatible with surrounding land use, harmonious with adjacent buildings and properties, and visually consistent with the environment of the neighborhood as required by Zoning Ordinance sections 14-05(1), (2), and (3).

The trial court found that the Landowners had failed to show that they were “prejudiced by an illegal zoning decision,” as required for relief. (See I.C. § 36-7-4-1614(d).) Noting that a WECS was a permitted use on land zoned A-1 and that Apex’s Development Plan met or exceeded the WECS development requirements (section 15 of the Zoning Ordinance) about which the Landowners complained, the trial court determined that the Landowners were neither aggrieved nor prejudiced by the approval of Development Plan. Finding no material issues of fact and deciding the matter on the law alone, the court issued summary judgment in favor of the Plan Commission.

The Landowners again appealed.

**DECISION: Judgment of superior court affirmed, and matter remanded with instructions.**

The Court of Appeals of Indiana agreed that the Landowners were not prejudiced by the Plan Commission's approval of Apex's WECS Project Development Plan, and therefore were not entitled to relief sought.

The court explained that relief was available to the Landowners only if they could prove that they were prejudiced by the Plan Commission's approval of Apex's WECS Project Development Plan. (I.C. § 36-7-4-1614(d).) The court also explained that, pursuant to powers bestowed upon local legislative bodies by Indiana's Zoning Enabling Act, the Wells County legislative body established the Zoning Ordinance. (I.C. § 36-7-4-1401.5, -1403.) Here, the court found that Apex's Development Plan was in accordance with Article 9 of the Zoning Ordinance (permitted uses in a zoning district) and Article 15 (setting forth requirements for WECS projects).

The appellate court concluded that the circumstances about which the Landowners contended they had been prejudiced—including their proximity to the wind turbines and its resultant noise and shadow flicker plus a decrease in the value of their land—were circumstances created not by the Plan Commission's approval of Apex's Development Plan, but instead, by the legislative body's enactment of Article 15. Accordingly, the court held that the trial court did not err in granting summary judgment to the Plan Commission upon a finding that Landowners were not prejudiced by the zoning decision approving Apex's WECS Project Development Plan.

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*Case Note:*

*In Count II of their complaint, the Landowners had also asked the superior court to declare that a reciprocal setback provision in Article 15 of the Zoning Ordinance was invalid and should be stricken from the Zoning Ordinance because it constituted a taking of private property without just compensation. The trial court had granted the Landowners' petition and ordered that "the Reciprocal Setback provision in Article 15 of the Zoning Ordinance is declared invalid and should be stricken from the Zoning Ordinance." Additionally, the trial court had ordered that the WECS Project Development Plan submitted by Apex be remanded to the Plan Commission with instructions that the Plan Commission review the Development Plan and its record of proceedings leading up to its decision, to determine if the Development Plan satisfied or failed to satisfy the requirements of Article 15 of the Zoning Ordinance and "any additional requirements incorporated into these Articles by reference" (i.e., Article 14 and its compatibility requirements). The parties did not appeal the trial court's conclusions on those two issues. Therefore, while the appellate court affirmed the trial court's grant of partial summary judgment upon a finding that the Landowners were not aggrieved or prejudiced by the Plan Commission's approval of Apex's Development Plan, the appellate court also remanded the action to the trial court with instructions to remand the Apex Development Plan to the Plan Commission.*

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## Preemption—State law allows medical marijuana collective gardens, but city zoning ordinance prohibits collective gardens

Coalition contends state law preempts the ordinance

Citation: *Cannabis Action Coalition v. City of Kent*, 2015 WL 2418553 (Wash. 2015)

WASHINGTON (05/21/15)—This case addressed the issue of whether the Washington State Medical Use of Cannabis Act, which authorizes qualifying patients to participate in “collective gardens” to pool resources and grow medical marijuana, preempts local zoning ordinances that prohibit collective gardens.

**The Background/Facts:** Under the Washington State Medical Use of Cannabis Act (“MUCA”), chapter 69.51A RCW, qualifying patients may participate in “collective gardens” to pool resources and grow medical marijuana for their own use. (RCW 69.51A.085(1).) Under the MUCA, participating in a collective garden involves “sharing responsibility for acquiring and supplying the resources required to produce and process cannabis for medical use,” such as by providing real estate, equipment, supplies, or labor for the collective garden. (RCW 69.51A.085(2).) The MUCA clarifies that local governments retain authority to regulate the production, processing, or dispensing of medical marijuana through zoning, business licensing, health and safety requirements, and business taxes. (RCW 69.51A.140.)

In 2012, the city of Kent, Washington, enacted an ordinance (the “Ordinance”) that prohibits collective gardens. The Ordinance defines “collective gardens” similar to how they are defined under the MUCA. The Ordinance concerns “collective gardens” wherein up to 10 individuals pool resources to grow medical marijuana in potentially large operations of up to 45 plants and 75 ounces of usable marijuana. (Kent City Code 15.02.074(A)-(C).) The Ordinance adopts zoning requirements for “the growing, production, processing, transportation, and delivery of cannabis” in a collective garden, if seven conditions defining a collective garden are satisfied.

The Cannabis Action Coalition and a number of individuals (collectively, the “Coalition”) sued the city of Kent, its city council, and its mayor (collectively, “Kent”), seeking to have the Ordinance declared preempted and invalid. The Coalition contended that the MUCA preempted the Ordinance.

Finding no issue of material fact and deciding the matter on the law alone, the superior court issued summary judgment for Kent, upholding the Ordinance.

The Coalition appealed. The court of appeals held that the MUCA did not preempt the Ordinance.

The Coalition again appealed.

**DECISION: Judgment of court of appeals affirmed.**

The Supreme Court of Washington held that the MUCA, which authorizes qualifying patients to participate in “collective gardens” to pool resources and grow medical marijuana, did not preempt Kent’s zoning Ordinance, which prohibited collective gardens.

In so holding, the court explained that an ordinance is valid under Washington Constitution art. XL, § 11 “unless: (1) the Ordinance conflicts with some general law; (2) the Ordinance is not a reasonable exercise of the [local government’s] police power; or (3) the subject matter of the Ordinance is not local.”

Here, the Coalition had contended only that the Ordinance conflicted with the MUCA, a general law. The court explained that the MUCA would preempt the Ordinance if: (1) there was express legislative intent for the MUCA to preempt the field of medical marijuana regulation; (2) if field preemption of medical marijuana regulation was implied by the MUCA; (3) if the Ordinance conflicted with the MUCA by forbidding what the MUCA permitted or allowing what the MUCA prohibited.

The court found the MUCA had no express preemption clause. With regard to implied preemption, the court said that when a state statute expressly provides for local jurisdiction over a subject, state law does not impliedly preempt the field of that subject. Here, the court found that the MUCA expressly contemplated local regulation of medical marijuana. The MUCA provides that cities and towns may adopt and enforce “any of the following pertaining to the production, processing, or dispensing of cannabis or cannabis products within their jurisdiction: Zoning requirements, business licensing requirements, health and safety requirements, and business taxes.” (RCW 69.51A.140(1).) Given that express state law recognition of local jurisdiction over medical marijuana regulation, the court concluded that the MUCA does not impliedly preempt the field of medical marijuana.

With regard to conflict preemption, the court said that the MUCA would preempt the Ordinance only if the Ordinance “directly and irreconcilably conflicts with the statute.” The Coalition had argued that the MUCA irreconcilably conflicted with the Ordinance because the MUCA granted a right to engage in a collective garden under RCW 69.51A.085, yet the Ordinance prohibited the same activity. The court disagreed with the Coalition’s argument. Instead, the court found that the plain text of RCW 69.51A.140(1), and its statutory context, “demonstrate that it provides local jurisdictions the authority to enact zoning requirements pertaining to the land use activity of participating in a collective garden.” In so finding, the court also rejected the Coalition’s argument that the zoning power allowed localities under RCW 69.51A.140(1) over the “production” and “processing” of medical marijuana applied only to commercial or licensed producers. The court found the plain language made clear that zoning authority under the MUCA included over noncommercial collective gardens.

Finally, the court addressed whether the Ordinance was otherwise consistent with state law. The court found that the Kent’s Ordinance concerned only a land use—namely zoning requirements for “the growing, production, processing, transportation and delivery of cannabis” in a collective garden.

The court held that Kent therefore properly exercised its authority under RCW 69.51A.140(1) (the MUCA) to zone the land use activity involving collective gardens. The court concluded that the Ordinance was consistent with state law and not preempted.

See also: *Lawson v. City of Pasco*, 168 Wash. 2d 675, 230 P.3d 1038 (2010).

## Standing—Restaurant challenges zoning decision for competitor

Competitor says challenge must be dismissed as it was brought by restaurant that had forfeited right to do business in state

Citation: *A Guy Named Moe, LLC v. Chipotle Mexican Grill of Colorado, LLC*, 2015 WL 3440472 (Md. Ct. Spec. App. 2015)

MARYLAND (05/29/15)—This case addressed the issue of whether a foreign LLC that has lost its right to do business in the state of Maryland has a right to bring a petition as a “person aggrieved,” challenging a zoning decision.

**The Background/Facts:** Since 2006, A Guy Named Moe, LLC (“Moe’s”) had operated Moe’s Southwest Grill on Dock Street in Annapolis, Maryland. In August 2012, Chipotle Mexican Grill of Colorado, LLC (“Chipotle”) filed with the City of Annapolis’s Department of Planning and Zoning an application for a special exception to operate a “standard restaurant”—Chipotle Mexican Grill—in close proximity to Moe’s Southwest Grill on Dock Street. The City of Annapolis’s Board of Appeals (the “Board”) approved Chipotle’s application. Moe’s appealed, challenging the Board’s decision.

Chipotle asked the circuit court to dismiss Moe’s appeal. Chipotle noted that Moe’s had forfeited its right to do business in Maryland in November 2006 when it failed to file the proper registration fees, among other things, and had nonetheless continued to do business in Maryland. As such, Chipotle contended that Moe’s lacked standing (i.e., the legal right to bring a lawsuit) as a taxpayer under Maryland Code, § 4-401(a) of the Land Use Article (“L.U.”) (L.U. § 4-401(a)(2)) or as a “person aggrieved” by the Board’s decision (L.U. § 4-401(a)(1)).

The circuit court granted Chipotle’s motion to dismiss Moe’s action “with prejudice.” The court found that Moe’s lacked standing because it was not a taxpayer in that it had failed to pay real property taxes to the City of Annapolis.

Moe’s appealed. Moe’s argued that although it had lost its right to do business in Maryland, it could nonetheless legally maintain a suit because Moe’s had filed its petition for review within the statutorily required 30 days of the Board’s decision and because that petition was revived when, in September 2013, Moe’s right to do business in Maryland was restored. Moe’s also argued that, whether or not able to bring suit as a taxpayer, Moe’s suit should not have been dismissed because Moe’s was a “person aggrieved” by the Board’s decision with a right to bring the suit.

**DECISION: Judgment of circuit court affirmed.**

The Court of Special Appeals of Maryland held that Moe's did not have standing to file the petition challenging the Board's decision granting Chipotle's special exception because, whether or not Moe's was a "taxpayer" or a "person aggrieved," Moe's petition was void *ab initio* (i.e., invalid from the outset) since, at the time it was filed, Moe's had lost its right to do business in Maryland and was nonetheless continuing to do so. The court explained that while a foreign LLC may have standing as a "taxpayer" or a "person aggrieved" under L.U. § 4-401(a) to petition the circuit court for judicial review of a zoning decision, the foreign LLC loses that standing if it continues to do business in Maryland after its right to do business here has been forfeited.

The court reached that conclusion based upon the plain language of Maryland's Corporations and Associations Article ("C.A") (C.A. § 4A-1002(a), C.A. § 4A-1013, and C.A. § 4A-1007(a)). C.A. § 4A-1002(a) states that "[b]efore doing any interstate, intrastate, or foreign business in this State, a foreign limited liability company shall register with the [State Department of Assessment and Taxation ('SDAT') ]." C.A. § 4A-1013 then warns that the SDAT may "forfeit the right of any foreign limited liability company to do business in this State" if that company "fails to file any report" with the SDAT or "fails to pay any late fee" within the statutorily prescribed period for doing so. Then, of particular significance to the issue before the court, C.A. § 4A-1007(a) provides that "[i]f a foreign limited liability company is doing or has done any intrastate, interstate, or foreign business in this State without complying with the requirements of this sub-title, the foreign limited liability company . . . may not maintain suit in any court of this State" unless it "shows to the satisfaction of the court" "that it has satisfied certain obligations or that it is no longer doing business in Maryland."

The court disagreed with Moe's contention that once a foreign LLC had satisfied the court of its right to maintain suit, it could then rely on a petition it had filed with the court when it had no right to do so. In other words, the court determined that Moe's could not revive its petition, though timely filed, since the petition was initiated after Moe's had lost the right to do business in Maryland and yet persisted to do business in Maryland during that 30-day period for filing a judicial review petition.

The court also briefly addressed Moe's contention that it was a "person aggrieved," entitled to challenge the Board's decision on Chipotle's special exception. The court said that even if Moe's was a "person aggrieved" under L.U. § 4-401(a)(1) and therefore had standing to petition the court for review as such, Moe's petition nonetheless had to be dismissed because it did not meet Rule 7-203(a)'s 30-day deadline. Moe's had no right to "maintain suit" under C.A. § 4A-1007(a) during the entire 30-day period and, when it re-attained that right, the 30-day period had long since lapsed. Moreover, the court noted that the circuit court, in effect, appeared to make a finding as to whether Moe's qualified as "a person aggrieved" when it opined that Moe's action against Chipotle was brought simply as a "matter of competition." The court explained that "a person is not 'aggrieved' for standing purposes when his sole interest in challenging a zoning decision is to stave off competition with his established business."

See also: *Dual Inc. v. Lockheed Martin Corp.*, 383 Md. 151, 857 A.2d 1095 (2004).

See also: *Hill Const. v. Sunrise Beach, LLC*, 180 Md. App. 626, 952 A.2d 357 (2008).

See also: *Tri-County Unlimited, Inc. v. Kids First Swim School Inc.*, 191 Md. App. 613, 993 A.2d 146 (2010).

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*Case Note:*

In its decision, the court noted that if Moe's had revived its right to do business, before or during the 30-day period for filing a petition for judicial review, and had subsequently filed that petition for judicial review, within that time period, then, as long as it could show that it fell within one of the three categories of "persons" who could file an action under L.U. § 4-401(a), Moe's would have been able to oppose the Board's decision in the circuit court.

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## Uses—Single-family dwelling owners rent property for short terms

County and owners dispute whether zoning ordinance prohibits short-term rentals

Citation: *Vilas County v. Accola*, 2015 WL 2193002 (Wis. Ct. App. 2015)

WISCONSIN (05/12/15)—This case addressed the issue of whether a zoning ordinance permitted short-term rental of single-family detached dwelling units located in a single-family residential district.

**The Background/Facts:** In June 2012, Harlan and Brenda Accola (the "Accolas") purchased a home on a lake in the R-1 zoning district in the Town of Presque Isle, County of Vilas, Wisconsin (the "County"). Shortly after purchasing the property, the Accolas began advertising it for rent, for stays as short as two nights. In July 2012, the County notified the Accolas that single-family residences in the R-1 district could not be rented for periods of less than one month. The County asserted rentals of less than one month constituted "transient lodging," as that term was used in the section of the ordinance governing the Residential/Lodging ("RL") district.

Under section 4.1 of the County's zoning ordinance, uses permitted in the R-1 district included: "[s]ingle family detached dwelling units . . ." In addition to the R-1 district, the general zoning ordinance also created a RL district. The purpose of the RL district was to allow for low-density residential use but with some mixing of low-density "transient lodging"—defined as rental of dwelling units for periods of less than one month. Under § 4.2 of the ordinance, uses permitted in the RL district included: "[a]ll uses permitted in the R-1 District . . . [and] rental of residential dwelling unit[s]."

After receiving notice from the County that they could not rent their prop-

erty for periods of less than one month, the Accolas subsequently created a corporation called A Better Way to Live. They began allowing people to stay at their property for periods of less than one month in exchange for “donations” to the corporation. The County again informed the Accolas that renting their property for periods of less than one month violated the general zoning ordinance. The County asserted, “Soliciting donations on a weekly basis in exchange for housing is the functional equivalent of renting the property[.]”

In August 2013, the County brought an enforcement action against the Accolas, seeking forfeitures and an injunction prohibiting the Accolas from renting their lake property for periods of less than 30 days.

The Accolas argued that the short-term rental of their property was permitted because the general zoning ordinance allowed “[s]ingle-family detached dwelling units” in the R-1 district. They asserted their property indisputably qualified as a single-family detached dwelling unit, and the general zoning ordinance did not explicitly prohibit short-term rentals of single-family detached dwelling units in the R-1 district.

The County argued that the Accolas’ short-term rental of their lake property constituted “transient lodging,” which was only expressly permitted in the RL district. Reading together the provisions of zoning ordinance governing the R-1 and RL districts, the County argued that the ordinance permitted rental of residential dwelling units for periods of less than one month in the RL district only. The county contended that the Accolas’ interpretation of the ordinance would render the provision of the ordinance listing rental of residential units as a use in the RL district (§ 4.2(B)(4)) superfluous because, if rentals of residential dwelling units for periods of less than one month were permitted in the R-1 district, there would be no need to separately list “rental of residential dwelling unit” as a permitted use in the RL district.

The circuit court agreed with the County’s argument. The court concluded that short-term rentals of the Accolas’ property for periods of less than one month, whether compensated by direct payment of rent or by donations to the Accolas’ corporation, were not permitted in the R-1 district. As a result, the court imposed a \$35,000 forfeiture and permanently enjoined the Accolas from renting their lake property for periods of less than 30 days.

The Accolas appealed.

**DECISION: Judgment of circuit court affirmed.**

The Court of Appeals of Wisconsin held that short-term rentals of single-family residential units were not permitted in the R-1 district and therefore the Accolas’ could not rent their lake property for periods of less than 30 days.

In so holding, the court agreed with the Accolas that the provision of the ordinance permitting uses in R-1 districts did not expressly prohibit the rental of single-family detached dwelling units. The court said that if it were limited to considering that provision of the County’s zoning ordinance, it would agree with the Accolas that the ordinance did not unambiguously prohibit the rental of single-family detached dwelling units in the R-1 district for periods of less than one month. However, the court also found that § 4.2(B) of the general zoning ordinance unambiguously permitted in the RL district both: (1) the rental of single-family detached dwelling units for periods of less than one

month; and (2) all uses permitted in the R-1 district, which includes single-family detached dwelling units. Reading the language of the ordinance governing uses in the R-1 district in context with the language of the ordinance governing uses in the RL district, the court agreed with the County that if the Accolas were correct that the rental of single-family detached dwelling units for periods of less than one month was a permitted use in the R-1 district, § 4.2(B)(4) of the ordinance, permitting the rental of single-family detached dwelling units for periods of less than one month in the RL district, would be superfluous because all uses permitted in the R-1 district were already permitted in the RL district under § 4.2(B)(1). The court said that where possible, an ordinance must be read “to give reasonable effect to every word, in order to avoid surplusage.” Consequently, the court concluded that reading § 4.1 of the ordinance (governing R-1 uses) in context with § 4.2 (governing RL uses) lead to the “inescapable conclusion that the rental of single-family detached dwelling units for periods of less than one month [was] not a permitted use in the R-1 district because a contrary interpretation would render § 4.2(B)(4) superfluous.”

See also: *Heef Realty and Investments, LLP v. City of Cedarburg Bd. of Appeals*, 2015 WI App 23, 361 Wis. 2d 185, 861 N.W.2d 797 (Ct. App. 2015).

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*Case Note:*

*In its decision, the court noted a recent holding it had in another case: “when an ordinance simply lists ‘single-family dwellings’ as a permitted use in a zoning district, without more, the ordinance does not unambiguously prohibit short-term rentals of single-family dwellings in that district.”*

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## Zoning News from Around the Nation

### CALIFORNIA

Assembly Bill 243 and Senate Bill 643—each aimed at regulating the medical marijuana industry—have passed their respective chambers. Among other things, the bills would “provide[ ] a regulatory framework for the industry covering the issues of environmental protection and water regulations, law enforcement, licensing, public health related to edibles and product testing, to marketing, labeling, taxing, transporting, zoning, local control and re-sale.” Assembly Bill 243 now heads to the Senate for consideration, and Senate Bill 643 heads to the Assembly for consideration.

Source: *Lost Coast Outpost*; <http://lostcoastoutpost.com>

### NORTH CAROLINA

Senate Bill 25 was passed by the House and Senate and headed to Governor Pat McCrory for signature or veto (or passage into law if the governor takes no action within 10 days). The bill limits local design and appearance rules for single- and two-family homes. The bill impacts local requirements for certain

types of siding, doors and windows, exterior decoration, roofing material, design, and color. Local governments would still be able to regulate the appearance and design of commercial buildings, apartments, and manufactured homes. Rules for historic districts would stay in place, as would homeowners' association requirements.

Source: *WRAL.com*; [www.wral.com](http://www.wral.com)

## PENNSYLVANIA

Middlesex Township Zoning Board has upheld, on a challenge, a zoning ordinance that allows oil and gas companies to drill in residential-agricultural zones.

Source: *NGI's Shale Daily*; [www.naturalgasintel.com](http://www.naturalgasintel.com)

## TENNESSEE

A new "swingers' club" law allows local governments to restrict the location of a private club if it is within 1,000 feet of a private, public or charter school, public park, residences, or place of worship.

Source: *WSMV.com*; [www.wsmv.com](http://www.wsmv.com)