

City of Ramsey
Agenda
Park and Recreation Commission
Thursday, November 12, 2020
6:30 pm
Council Chambers, 7550 Sunwood Drive NW

1. **Call to Order**
2. **Citizen Input**
3. **Approve Agenda**
4. **Approve Minutes**
 1. Approve the following meeting minutes.
 1. Park and Recreation Commission meeting minutes dated October 8, 2020.
5. **Commission Business**
 1. Consider the 2021 – 2030 Parks Five-Year Capital Improvement Plan
 2. **Recommend Proposed Improvements to Alpine Park's Off-leash Area—a 2020 Parks Capital Improvement**
 3. Note Draft Center Street Area Framework and Policy Plan
6. **Commission/Staff Input**
7. **Adjournment**

Park and Recreation Commission

4. 1.

Meeting Date: 11/12/2020

Submitted For: Mark Riverblood, Engineering/Public Works

By: MaryJo Warner, Engineering/Public Works

Information

Title:

Approve the following meeting minutes.

1. Park and Recreation Commission meeting minutes dated October 8, 2020.

Purpose/Background:

Purpose: To review and approve meeting minutes.

Background: Attached are the meeting minutes for review.

Notification:

Observations/Alternatives:

Funding Source:

n/a

Recommendation:

To review and approve meeting minutes dated October 8, 2020.

Action:

Motion to approve meeting minutes dated October 8, 2020.

Attachments

Minutes

Form Review

Inbox

Grant Riemer

Form Started By: MaryJo Warner

Final Approval Date: 11/06/2020

Reviewed By

MaryJo Warner

Date

11/06/2020 02:27 PM

Started On: 11/03/2020 09:50 AM

**PARK AND RECREATION COMMISSION
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Ramsey Park and Recreation Commission conducted a regular meeting on October 8, 2020, at the Alpine Park, 6600 Alpine Drive NW, Ramsey, Minnesota.

Commission Members Present: Chair Shane Bennett
 Vice Chair Brandon Sis
 Commissioner Nathan Barten
 Commissioner Matthew Guck
 Commissioner Jennifer Leistico
 Commissioner Justin Loss

Commission Members Absent: None

Also Present: Parks & Assistant Public Works Superintendent Mark Riverblood

1. CALL TO ORDER

Chair Bennett called the Park and Recreation Commission meeting to order at 6:35 p.m.

2. CITIZEN INPUT

None.

3. APPROVE AGENDA

Motion by Commissioner Sis, seconded by Commissioner Leistico, to approve the Park and Recreation Commission meeting agenda as presented.

Motion carried. Voting Yes: Chair Bennett; Commissioners Sis, Leistico, Barnes, Barten, Guck, and Loss. Voting No: None. Absent: None.

4. APPROVE MINUTES

4.01: Approve Park and Recreation Commission Meeting Minutes

Motion by Commissioner Loss, seconded by Commissioner Sis, to approve the following Park and Recreation Commission Regular Meeting Minutes:

- Park Commission Minutes of Regular September 10, 2020 Meeting.

Motion carried. Voting Yes: Chair Bennett; Commissioners Loss, Sis, Barnes, Barten, Guck, and Leistico. Voting No: None. Absent: None.

5. COMMISSION BUSINESS

5.01: Introduce 2021 Capital Improvement Planning, Including Proposed Improvements to Alpine's Dog Park

Parks & Assistant Public Works Superintendent Riverblood presented background information on the capital improvement planning process. He recognized that not all the 2020 projects were able to be completed in part because of COVID impacts, and highlighted some of the planned 2021 improvements including suggested improvements to Alpine's Dog Park.

Sue Brockman, 15631 Ramsey Boulevard NW, stated that she is a dog owner and frequent user of the park. She obtained input from several users of the dog park and provided details on the suggestion she brought forward to expand the fenced in area.

Chair Bennett thanked the resident for her suggestion and the extra work she puts in at the dog park.

Commissioner Loss asked if people often use the repurposed bus shelter.

Ms. Brockman replied that the shelter is used when the weather is not as pleasant, but it can only hold a few people.

Commissioner Sis asked if the resident believes that there is a need for the shelter to be expanded or whether the funds budgeted for that purpose would be better used for expansion.

Ms. Brockman stated that she has never heard a need for it when discussing the topic with other users of the park. She commented that the higher priority for users of the park seems to be increasing the size of the fenced area.

Chair Bennett commented that people love the dog park and want more of it. He agreed that the shelter comment has not come up as much since the repurposed shelter was added. He commented that the park typically has at least five or six users at any given time of the day.

Commissioner Loss asked if the City has another shelter that could be repurposed and added to the expanded area.

Parks & Assistant Public Works Superintendent Riverblood replied that the City does not have another shelter and did not believe that duplicating that in another location would be desired. He commented that lighting may be helpful. He stated that if the Commission recommends expanding the park, he would also suggest adding LED or solar lighting along the north side.

Ms. Brockman commented that many users continue to use the park after daylight savings time when it begins to get dark around 5 p.m.

Chair Bennett commented that it appears that everyone seems supportive of at least looking into the cost.

Commissioner Leistico stated that she gives higher priority to things that citizens have invested their own time in and bring forward, especially when they are realistic in terms of cost.

Chair Bennett stated that it would appear the next step would be to direct staff to continue to move forward and determine the most efficient and effective manner to expand the dog park and possibly add lighting.

Parks & Assistant Public Works Superintendent Riverblood stated that staff will look into the potential cost further and bring it back to the Commission at its November meeting. He stated that he will also provide a more detailed map with gate locations and perimeter, noting that one triangle area proposed to be included would be left out because of its use for sports.

Commissioner Sis asked if there would be interest to separate areas for small and large dogs.

Ms. Brockman commented that small dogs tends to “out boss” and out run larger dogs and noted that in her discussions with users she has not heard that request.

Parks & Assistant Public Works Superintendent Riverblood stated that he has asked users that question as well and has not heard much support for that idea. He stated that some people come with both small and larger dogs and like to have them together.

Motion by Commissioner Sis, seconded by Commissioner Leistico, to explore the expansion of the dog park including estimated costs.

Motion carried. Voting Yes: Chair Bennett; Commissioners Sis, Leistico, Barnes, Barten, Guck, and Loss. Voting No: None. Absent: None.

5.02: Recommend 2021 Park Development Fees and Rates and Charges

Parks & Assistant Public Works Superintendent Riverblood stated that each year the Commission makes a recommendation to the Council related to parks and trails fees. He stated that staff recommends that there be no increase in the park dedication or trail fees for 2021. He noted that modest increases were last made in 2018.

Chair Bennett commented that the rates are still competitive and sometimes even better than other communities and would support keeping the rates steady.

Motion by Commissioner Sis, seconded by Commissioner Loss, to recommend to City Council to maintain Park Dedication and Trail Fees at the existing rates, for 2021, to include the residential per unit fees and including the per acre rates for the commercial/industrial subdivision of land.

Motion carried. Voting Yes: Chair Bennett; Commissioners Sis, Loss, Barnes, Barten, Guck, and Leistico. Voting No: None. Absent: None.

6. COMMISSION/STAFF INPUT

Parks & Assistant Public Works Superintendent Riverblood stated that a summary of programming was included and highlighted an upcoming event, A Day at the Farm at the Pearson's Farm, that the City is a partner in. He stated that fall soccer and football is currently occurring. He stated that it appears that the skating season will go forward.

Chair Bennett commented that Commissioner Bayer has submitted his resignation and thanked him for all of his efforts and work on the Commission over the past several years.

7. ADJOURNMENT

Motion by Commissioner Loss, seconded by Commissioner Leistico, to adjourn the meeting.

Motion carried. Voting Yes: Chair Bennett; Commissioners Loss, Leistico, Barnes, Sis, Barten, and Guck. Voting No: None. Absent: None.

The Park and Recreation Commission meeting adjourned at 7:04 p.m.

Respectfully submitted,

Mark Riverblood
Parks & Assistant Public Works Superintendent

Drafted by Amanda Staple
TimeSaver Off Site Secretarial, Inc.

Meeting Date: 11/12/2020

By: Mark Riverblood, Engineering/Public Works

Information

Title:

Consider the 2021 – 2030 Parks Five-Year Capital Improvement Plan

Purpose/Background:

The purpose of this case is to recommend the 2021 – 2030 Parks' Capital Improvement Plan (CIP) to City Council for their consideration in the Fall of 2021 to include a Public Hearing. The document outlines a 10-year plan very similar to the previous 2020 – 2029 5-year plan.

Notification:

A Public Hearing on the City-wide CIP will be held by City Council as soon as December 8th of 2020.

Observations/Alternatives:

The Parks 10 – Year CIP will be adopted by Council as part of the larger CIP document, usually corresponding in time to the General Fund budget in December.

The following is an excerpt from the anticipated larger city-wide all-category CIP, and includes an explanation on the CIP document and process:

"Enclosed is the Ramsey Capital Improvements/Capital Outlay Plan (CIP) for the years 2021-2030 It has been prepared in an attempt to anticipate major capital expenditures in advance of the year in which they are budget requests. Further, several projects may interrelate or require other improvements prior to initiation, which would cause delays without prior planning. Additionally, projects may require budgeting over several years or receipt of funds from other sources (i.e. grants) requiring planning completion prior to the funding year. Finally, the plan enables a snapshot of the identified capital needs of the community allowing for continual prioritization of these needs.

Approval of the CIP by Council does not authorize spending or initiation of a given project. It does, however, provide a guide for the community for a whole array of private and public decision-making, impacted by public capital expenditures. Therefore, the CIP should receive ratification only if the Council perceives actions contemplated within the plan as reasonable and planned within justified time frames. It shall further be noted that initial project design of public infrastructure projects identified within this plan often begins two years or more prior to the date of construction.

The CIP is not intended to provide for precise budgeting. Capital costs are projected as estimates. Upon each update of the plan, deletions, additions, delays, or other revisions may occur, reflecting changing community needs. These changes allow for budget refinements as a particular project nears actual construction."

Because future years' of the CIP consist of many variables, staff recommends a discussion of the various values and details associated with each **proposed project within 2021**; and a *brief*, summary mention of future years' projects for 2022-2030. (For the meeting, Staff will highlight these projects.)

Additionally, all capital improvements require on-going maintenance costs (day-to-day labor, utilities or annual expenses); and therefore, staff and City Council will evaluate very carefully the real and total costs of each capital improvement for consideration within the context of the General Fund operations budget at the time projects are brought forward for development.

Funding Source:

Recommendation of the Parks 10-Year CIP document includes proposed funding sources and project cost *estimates* as identified on the CIP worksheets.

Recommendation:

Staff recommends approval of the attached DRAFT CIP (an excerpt of the city-wide CIP document) as presented.

Action:

Based upon discussion, recommend a 2021 – 2030 Parks Capital Improvement Plan to City Council.

Attachments

Draft Parks CIP extract of Draft city-wide CIP

Supplemental Parks CIP

Form Review

Inbox

Grant Riemer

Form Started By: Mark Riverblood

Final Approval Date: 11/09/2020

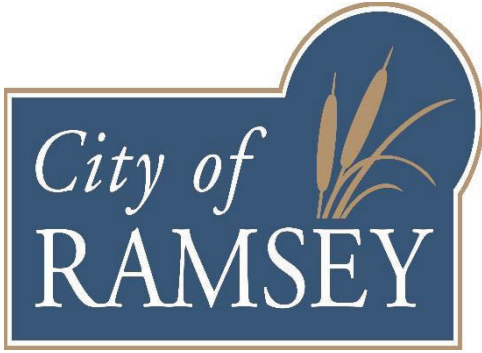
Reviewed By

MaryJo Warner

Date

11/09/2020 03:32 PM

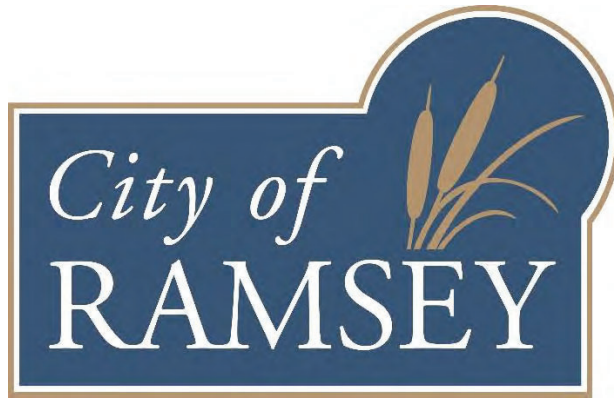
Started On: 11/09/2020 02:00 PM



CITY OF RAMSEY CAPITAL IMPROVEMENT PROGRAM 2021-2030

The Capital Improvement Program document is a planning tool maintained by the city to identify future projects, related expenditures, and funding sources. All projects designated in the Capital Improvement Program are contingent upon availability of resources during the planned year. The total expenditure of projects includes city-funded sources as well as other resources such as grants, fees, bonding, etc.





THIS PAGE INTENTIONALLY BLANK

CITY OF RAMSEY
2021-2030
CAPITAL IMPROVEMENT PLAN

TABLE OF CONTENTS

	PAGE NUMBER
INTRODUCTORY SECTION.....	7
Capital Improvement Plan.....	8-10
OVERVIEW SECTION.....	11
Category Summary.....	12
Funding Source Summary.....	13
Priority Codes & Definitions Key.....	14
Projects by Year & Priority.....	15-21
Projects & Funding Sources by Category.....	22-36
PROJECT DETAILS.....	37
<u>Equipment Replacement:</u>	
Detailed Equipment.....	38-155
<u>Municipal Buildings:</u>	
Community Center.....	156
Old Town Hall Restoration.....	157
Sprinkler System @ Fire Station #1	158
Storage Bldg @ Fire Station #1	159
City Hall Exterior Lighting.....	160
City Hall Landscaping.....	161
Police Department Floor Replacement	162
Repair Roof on City Hall.....	163
Replace City Hall Carpeting.....	164
Replace City Hall Caulking & Grout	165
Repair City Hall Counter Tops.....	166
Resurface Fire #1 Apparatus Bay Floor Resurfacing.....	167
Fire #1 Apparatus Bay Paint.....	168
<u>Park Improvements:</u>	
Elmcrest Park Entrance and Signage.....	169
Irrigation for Titterud Park.....	170
Observation Boardwalk – Lake Itasca.....	171
Rabbit Park Phase Two.....	172
Observation Deck of the Mississippi East Dolomite.....	173
Trail Connections.....	174
Park Development in the COR.....	175
Additional Land for Central Park.....	176
Bark Park Shelter – Alpine Park.....	177
Field Lighting – Central Park.....	178
Northfork North Trail Connection.....	179
Amphitheater Lighting.....	180
Lake Itasca Park – Parking, Trail & Canoe.....	181
Aeration for Sunfish Lake & The Draw.....	182



INTRODUCTORY SECTION



CITY OF RAMSEY
CAPITAL IMPROVEMENT/CAPITAL OUTLAY PLAN
2021-2030

Enclosed is the Ramsey Capital Improvements/Capital Outlay Plan (CIP) for the years 2021-2030. It has been prepared in an attempt to anticipate major capital expenditures in advance of the year in which they are budget requests. Further, several projects may interrelate or require other improvements prior to initiation, which would cause delays without prior planning. Additionally, projects may require budgeting over several years or receipt of funds from other sources (i.e. grants) requiring planning completion prior to the funding year. Finally, the plan enables a snapshot of the identified capital needs of the community allowing for continual prioritization of these needs.

The process for completing the 2021-2030 CIP was much the same as the process for the 2021 General Fund Budget. Project requests were submitted by all divisions and reviewed by the Management Team and appropriate supervisors to establish the feasibility and priority of the projects and match with available financing.

CAPITAL IMPROVEMENT PLAN GOALS
ACKNOWLEDGE AND COMMUNICATE PUBLIC INFRASTRUCTURE PRIORITIES AND DYNAMICS
ENSURE APPROPRIATE RESPONSES TO CHANGING INFRASTRUCTURE NEEDS AND DEMANDS
DEVELOP A FINANCIAL ASSESSMENT OF CAPITAL RESOURCES AVAILABLE TO MEET FUTURE CAPITAL PROJECT NEEDS
ENCOMPASS THE CITY'S STRATEGIC PLAN'S IMPERATIVES: FINANCIAL STABILITY; A CONNECTED COMMUNITY; SMART, CITIZEN-FOCUSED GOVERNMENT; AN EFFECTIVE ORGANIZATION

Approval of the CIP by Council does not authorize spending or initiation of a given project. It does, however, provide a guide for the community for a whole array of private and public decision-making, impacted by public capital expenditures. Therefore, the CIP should receive ratification only if the Council perceives actions contemplated within the plan as reasonable and planned within justified time frames. It shall further be noted that initial project design of public infrastructure projects identified within this plan often begins two years or more prior to the date of construction.

The CIP is not intended to provide for precise budgeting. Capital costs are projected as estimates. Upon each update of the plan, deletions, additions, delays, or other revisions may occur, reflecting changing community needs. These changes allow for budget refinements as a particular project nears actual construction.

Capital improvements are improvements to land, streets, parks, utilities and buildings. This plan shows improvements with revenues projected from various funding sources. Improvements from the General Fund are those requiring a tax levy, referendum or similar other authority resulting in listed projects being less than certain. The other funding sources (except for intergovernmental revenue - grant funds) have increased stability over the general fund projects.

Specific information on the funding sources for each project is shown under the individual project. Cash flow projections are completed to examine the long-term stability of each fund and its ability to provide for planned expenditures.

Potential funding sources for capital improvement expenditures may include:

- General Fund
- Capital Equipment Certificates
- General Obligation Bonds
- CIP General Obligation Bonds*
- Tax Increment Financing
- Developer Fees
- Grants/Outside Sources
- Utility Funds (Water, Sewer, Recycling, Street Lights, Storm Water)
- Special Revenue Funds (Equipment Revolving, Fleet, Lawful Gambling, Storm Water Management)
- Capital Project Funds (Facility, Landfill, MSA, Park Improvement, Public Improvement Revolving (PIR), Pavement Management Fund)
- Special Assessments

**Only City Hall, Public Works facilities and Public Safety facilities may be financed with CIP General Obligation bonds under the CIP Act (Minnesota Statutes Chapter 475). Other capital improvements described in this plan as being financed under General Obligation Bonds may be financed with special assessment bonds under Minnesota Statutes, Chapter 429 and utility revenue bonds under Minnesota Statutes, Chapter 444.*

For a City to use its authority to finance expenditures under the CIP Act, it must meet the requirements provided therein. Specifically, the City Council must approve the sale of capital improvement bonds by a two-thirds vote of its membership. In addition, it must hold a public hearing for public input. Notice of such hearing must be published in the official newspaper of the City at least fourteen, but not more than twenty-eight days prior to the date of the public hearing. The City Council approves the CIP following the public hearing.

The bonds are not subject to referendum unless, within 30 days after the hearing, a petition is filed with the City Administrator signed by voters equal to at least five percent of the votes cast in the last general City election. In that event, the bonds are subject to a referendum, and may be issued only if approved by a majority of voters who vote on that question. If the referendum passes, the taxes to pay the debt service on the bonds would be levied on market value rather than tax capacity. However, if no timely petition is filed, the taxes to pay debt service are levied on tax capacity.

The CIP Act has established certain criteria that must be met. In accordance with these criteria, the City has considered the following eight points:

1. Condition of the City's infrastructure and need for the project
2. Demand for the improvement
3. Cost of the improvement
4. Availability of public resources
5. Level of overlapping debt
6. Const/benefits of alternative uses of funds
7. Operating costs of the proposed improvements
8. Options for shared facilities with other cities or local governments.



OVERVIEW SECTION



City of Ramsey, Minnesota
Capital Improvement Program

2021 thru 2030

CATEGORY SUMMARY

Category	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Capital Equipment	1,090,200	643,000	1,079,000	927,000	1,617,000	1,017,500	499,000	647,800	1,204,000	507,500	9,232,000
Municipal Building	75,000	67,000	40,000	15,000			221,300		20,000,000		20,418,300
Park Improvement	1,758,000	630,000	200,000	600,000	601,000	55,000					3,844,000
Sewer Utility Improvement	40,000		95,000								135,000
Storm Water Utility Improvement	100,000	30,000					350,000				480,000
Street Improvement	13,030,472	21,542,760	21,758,150	49,423,262	48,056,410	8,226,670	2,977,480	3,417,275	3,260,240	2,981,440	174,674,159
Street Light Utility Improvement	185,000										185,000
Water Utility Improvement	20,330,000	13,960,000	55,000		1,600,000						35,945,000
TOTAL	36,608,672	36,872,760	23,227,150	50,965,262	51,874,410	9,299,170	4,047,780	4,065,075	24,464,240	3,488,940	244,913,459

City of Ramsey, Minnesota
Capital Improvement Program
2021 thru 2030

FUNDING SOURCE SUMMARY

Source	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Capital Maintenance Fund	75,000	47,000	40,000	15,000							177,000
Equipment Revolving Fund	70,200							417,800			488,000
Facility Fund		20,000									20,000
General Fund		643,000	786,000	698,000	1,392,000	1,017,500	499,000	152,000	750,000	507,500	6,445,000
Grants/Outside Sources	9,616,899	19,000,000	18,000,000	43,750,000	43,800,000						134,166,899
Landfill Fund	1,038,000										1,038,000
Lawful Gambling Fund					150,000						150,000
MSA	945,000	426,600	1,482,500	2,068,875	1,000,000	350,000	806,300	859,125	1,045,500	937,000	9,920,900
Park Improvement Trust Fund	1,700,000	630,000		350,000	201,000	55,000					2,936,000
Pavement Management Fund	2,128,700	1,885,000	1,934,000	1,925,000	1,733,100	2,059,700	1,903,500	2,247,500	1,972,900	1,773,400	19,562,800
Public Improvement Revolving Fund	59,023			1,000,000	1,000,000	206,000					2,265,023
Sewer Utility Fund	50,000	9,000,000	95,000	196,500		41,000		39,000	437,000		9,858,500
Storm Water Utility Fund	430,850	261,160	634,650	344,387	398,310	831,970	617,680	310,650	241,840	271,040	4,342,537
Street Light Utility Fund	185,000					196,000					381,000
Tax Increment Fund #1						1,050,000					1,050,000
Tax Increment Fund #14 (BONDING)					400,000	500,000					900,000
Tax Increment Fund #2						2,217,000					2,217,000
To Be Determined			200,000	200,000	200,000		221,300		20,000,000		20,821,300
Water Utility Fund	20,310,000	4,960,000	55,000	417,500	1,600,000	775,000		39,000	17,000		28,173,500
GRAND TOTAL	36,608,672	36,872,760	23,227,150	50,965,262	51,874,410	9,299,170	4,047,780	4,065,075	24,464,240	3,488,940	244,913,459

Capital Improvement Plan - Priority Codes & Definitions

Priority

- 1 Existing Obligations - High Priority: Year 2021 or 2022**
Projects under this priority are previously committed to or are replacements
Example: Road reconstructions, equipment replacement
- 2 New Addition - High Priority: Year 2021 or 2022**
Projects under this priority are needed, but have no previous commitments
Example: Road extensions, additional park amenities
- 3 Existing Obligations - Medium Priority: Year 2023-2030**
Projects under this priority are previously committed to or are replacements but not as urgent to fulfill as those in priority 1
Example: 5th year forward of road reconstructions; equipment replacement
- 4 New Addition - Medium Priority: Year 2023-2030**
Projects under this priority are needed, but have no previous commitments but need is not as great as those listed in priority 2
Example: Public Works Campus; new capital equipment
- 5 Opportunity Driven/Unfunded/Placeholder**
Projects under this priority are development driven and/or have outside funding capacities such as grants and/or placeholders for projects that may occur after year 2030.
Full funding on these projects have not been determined.
Example: Highway 10 improvements; Community Center

City of Ramsey, Minnesota
Capital Improvement Program

Data in Year 2021 and 2030

PROJECTS BY YEAR & PRIORITY

Project Name	Department	Project #	Priority	Project Cost
2021				
Priority 1-Existing Obligation (High)				
Police Copier - File Room	Capital Equipment	17-IT-004	1	10,500
Police Copier - Patrol	Capital Equipment	17-IT-005	1	7,500
Replace Engine 11 (556) - Payable over 2 years	Capital Equipment	FIRE-503	1	325,000
2021 Chev Impala Replace 2013 Chev Malibu #334	Capital Equipment	POL-334	1	33,000
2020 Chev Tahoe Replace 2016 Chev Tahoe #361	Capital Equipment	POL-372	1	41,000
Replace 2001 John Deere Mower: Unit #635	Capital Equipment	PW-635	1	20,000
Replace-2004 Snowplow Truck: Unit #644	Capital Equipment	PW-644	1	212,500
Replace 2004 Tool Cat	Capital Equipment	PW-645	1	57,500
Replace Sidewalk Machine: Unit #655	Capital Equipment	PW-655	1	146,500
Replace 2008 Bobcat Skidster: Unit #671	Capital Equipment	PW-671	1	61,000
Replace 2010 Chevy 1- Ton Truck #675	Capital Equipment	PW-675	1	63,000
Replace 2013 Toro Wide Area Mower: Unit #679	Capital Equipment	PW-697	1	92,000
Replace PW Utility Trailer	Capital Equipment	PW-711	1	11,500
Replace 2006 Bison Trailer: Unite #648	Capital Equipment	PW-712	1	9,200
City Hall Exterior Lighting	Municipal Buildings	20-BLDG-001	1	40,000
City Hall Landscaping	Municipal Buildings	20-BLDG-002	1	15,000
Police Department Floor Replacement	Municipal Buildings	20-BLDG-003	1	5,000
Repair Roof on City Hall	Municipal Buildings	20-BLDG-004	1	15,000
Replace Pumps at Lift Station #6	Sewer Utility	19-SEW-002	1	40,000
Sunwood Drive Roundabout Landscaping	Street Improvements	15-STR-003	1	20,000
Reconstruction Streets: Riverdale Drive	Street Improvements	17-STR-003	1	836,000
Reconstruction Streets: Section 07 (Tiger Street)	Street Improvements	17-STR-003A	1	359,480
2021 MSA Overlays	Street Improvements	21-STR-001	1	203,500
2021 Neighborhood Overlays	Street Improvements	21-STR-002	1	414,590
Business Park 95 Street Reconstruction	Street Improvements	21-STR-003	1	1,567,500
Complete Pump House 3	Water Utility	04-WTR-005	1	240,000
Refurbish Water Tower #2	Water Utility	14-WTR-001	1	20,000
Refurbish Water Tower #1	Water Utility	14-WTR-002	1	20,000
<i>Total for: Priority 1</i>				4,886,270
Priority 2-New Addition (High)				
Elmcrest Park & The Draw Entrance Monument & Sign	Park Improvements	04-PARK-003	2	80,000
Observation boardwalk - Lake Itasca	Park Improvements	06-PARK-011	2	60,000
Observation deck on the Mississippi E of Dolomite	Park Improvements	06-PARK-015	2	100,000
Park Development in The COR	Park Improvements	08-PARK-005	2	1,200,000
Bark Park Shelter-Alpine Park	Park Improvements	17-PARK-005	2	58,000
Northfork North Trail Connection	Park Improvements	17-PARK-007	2	150,000
Amphitheater Lighting	Park Improvements	18-PARK-003	2	40,000
Aeration for Sunfish Lake & The Draw	Park Improvements	18-PARK-005	2	40,000
Outlet Control Structure Improvements	Stormwater Utility	19-STRM-001	2	100,000
CSAH 116 & TH 47 Intersection Improvements	Street Improvements	21-STR-004	2	129,402
Bunker Lake Blvd & Puma Street Lights	Street Light Utility	19-STLT-001	2	100,000
Riverdale Drive Street Lights	Street Light Utility	19-STLT-002	2	85,000

Project Name	Department	Project #	Priority	Project Cost	
Water Treatment Facility	Water Utility	04-WTR-017	2	20,000,000	
				<i>Total for: Priority 2</i>	22,142,402
Priority 5-Opportunity/Unfunded/Placeholder					
Trail Connections	Park Improvements	06-PARK-019	5	30,000	
HWY 10 and BNSF RR Grade Separation at Ramsey Blvd	Street Improvements	16-STR-002	5	4,500,000	
HWY 10 & BNSF RR Grade Separation at Sunfish Lk Bl	Street Improvements	16-STR-003	5	5,000,000	
Construct Well #9 and Pumhouse #5	Water Utility	16-WTR-002	5	50,000	
				<i>Total for: Priority 5</i>	9,580,000
Total for 2021				36,608,672	

2022

Priority 1-Existing Obligation (High)

Universal Power Supply Replacement	Capital Equipment	17-IT-008	1	30,000	
Replacement Extrication Tools R-11	Capital Equipment	FIRE-565	1	50,000	
Replace Training Room Tables/Chairs	Capital Equipment	FIRE-577	1	20,000	
Replace Maintenance Utility Tractor #409	Capital Equipment	GENGOV-002	1	55,000	
2021 Ford Explorer Replace 2016 Ford Explorer #351	Capital Equipment	POL-335	1	32,000	
2022 Ford Taurus Replace 2018 Ford Taurus #383	Capital Equipment	POL-366	1	33,000	
2022 Ford Taurus Replace 2018 Ford Taurus #384	Capital Equipment	POL-367	1	33,000	
2022 Ford Explorer Relace 2018 Ford Explorer #372	Capital Equipment	POL-368	1	32,000	
2022 Chev Tahoe Replace 2018 Chev Tahoe #373	Capital Equipment	POL-369	1	37,000	
Replace Engr Car #407 With Chevy Impala	Capital Equipment	PW-407	1	24,000	
Replace 2000 Asphalt Trailer	Capital Equipment	PW-613	1	37,000	
Replace 2006 Tandem Axle Plow Truck Unit #668	Capital Equipment	PW-668	1	260,000	
Replace City Hall Carpeting	Municipal Buildings	20-BLDG-005	1	15,000	
Resurface Fire #1 Apparatus Bay Floor Resurfacing	Municipal Buildings	21-BLDG-001	1	20,000	
Fire 1 Apparatus Bay Paint	Municipal Buildings	21-BLDG-002	1	12,000	
River's Bend South Shelter Replacement	Park Improvements	21-PARK-001	1	50,000	
Reconstruction Streets: Autumn Heights	Street Improvements	17-STR-005	1	974,050	
Reconstruction Streets:MSA Waco St-Sunwood Dr/150	Street Improvements	19-STR-018	1	135,850	
2022 MSA Overlays	Street Improvements	21-STR-005	1	68,750	
2022 Neighborhood Overlays	Street Improvements	21-STR-006	1	1,099,450	
MSA - Sunwood Drive Street Reconstruction	Street Improvements	21-STR-007	1	264,660	
Refurbish Water Tower #2	Water Utility	14-WTR-001	1	1,280,000	
Refurbish Water Tower #1	Water Utility	14-WTR-002	1	680,000	
				<i>Total for: Priority 1</i>	5,242,760

Priority 2-New Addition (High)

Storage Building at Fire Station 1	Municipal Buildings	18-BLDG-002	2	20,000	
Additional Land for Central Park	Park Improvements	15-PARK-01	2	250,000	
Lake Itasca Park - Parking, Trail & Canoe System	Park Improvements	18-PARK-004	2	250,000	
Water Treatment Facility	Water Utility	04-WTR-017	2	12,000,000	
				<i>Total for: Priority 2</i>	12,520,000

Priority 3-Existing Obligation (Med)

Stormwater Drainage Improvements	Stormwater Utility	12-STRM-001	3	30,000	
				<i>Total for: Priority 3</i>	30,000

Priority 5-Opportunity/Unfunded/Placeholder

Trail Connections	Park Improvements	06-PARK-019	5	80,000
HWY 10 and BNSF RR Grade Separation at Ramsey Blvd	Street Improvements	16-STR-002	5	9,000,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	04-PARK-003
Project Name	Elmcrest Park & The Draw Entrance Monument & Sign

Department	Park Improvements
Contact	Unassigned
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$80,000

Description

Entrance signing for Elmcrest Park would consist of a substantial way-finding sign and associated landscape sited near the intersection of Quicksilver Street and 167th Avenue. For the Draw, the sign would function as a monument for the park name and also as a location to advertise upcoming events.

Justification

The 100+ acre Elmcrest community park may warrant an attractive entrance monument benefiting the park's prominence in the community. Additionally, there will be continuing events where many hundreds of out-of-Ramsey visitors will come to the city. The monument will function as a way-finding sign and also to remind visitors that they are in Ramsey. For The Draw, the monument would describe for the community the name of the park and amphitheater (a minor issue presently). Plus, if the advertising space was digital (versus posters), there could be a modest revenue stream from non-municipal sources.

Note: The planning for this project may be an useful exercise in evaluating the costs of materials following the Design Framework relating to signs and monuments in the community.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	80,000										80,000
Total	80,000										80,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	80,000										80,000
Total	80,000										80,000

Capital Improvement Program
 City of Ramsey, Minnesota

2021 *thru* 2030

Project #	06-PARK-010
Project Name	Irrigation for Titterud Park

Department Park Improvements
 Contact
 Type Improvement
 Useful Life 20 Years
 Category Park Improvement
 Priority 4-New Addition (Med)
 Status Active

Total Cost \$76,000

Description
Underground irrigation & 160 GPM well. If municipal water is available, cost may be less.

Justification
Irrigation should be considered for this 13.5 acre park, and a part of this is the evaluation of a well versus connection to a municipal watermain. Both have advantages. The \$50,000 represents the underground irrigation portion. A 160 GPM well may cost about \$26,000.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost					76,000						76,000
Total					76,000						76,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund					76,000						76,000
Total					76,000						76,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	06-PARK-011
Project Name	Observation boardwalk - Lake Itasca

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$60,000

Description

A boardwalk or pier in the southern portion of Lake Itasca will allow trail users and visitors to the Lake Itasca Community Park to view the lake which is mostly obscured by cattails.

Justification

The Lake Itasca is an important resource along the Mississippi Flyway for migratory waterfowl and home to many types of wetland fauna. Providing access to the open water portions of the lake will provide birdwatchers and trail users a relaxing rest point and destination along the Lake Itasca Trail.

Additionally, lake access was cited as desired by several participants in the public engagement process as part of the acquisition of the Lake Itasca Community Park.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	60,000										60,000
Total	60,000										60,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	60,000										60,000
Total	60,000										60,000

Capital Improvement Program
 City of Ramsey, Minnesota

2021 *thru* 2030

Project #	06-PARK-012
Project Name	Rabbit Park Phase Two

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	4-New Addition (Med)
Status	Active

Total Cost \$125,000

Description
Well and underground irrigation.
Landscape and turf improvements which may include an ag-lime infield for the ballfield.

Justification
Rabbit Park is a 5-acre neighborhood park with very basic features. The park's greatest need is irrigation. Therefore, the primary component of the proposed phase two project is a well, electrical service and underground irrigation.
Landscape improvements would complete this project.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost					125,000						125,000
Total					125,000						125,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund					125,000						125,000
Total					125,000						125,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	06-PARK-015
Project Name	Observation deck on the Mississippi E of Dolomite

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$100,000

Description

On the south side of Riverdale Drive, east of Dolomite exists a storm water discharge point to the river. This project discusses an observation deck at the top of the bluff over the existing storm sewer easement, concurrent with the residential development of the land there.

Justification

This access may provide improved maintenance to the storm outlet, but the primary public value would be to maintain contact with the river by residents for the remarkable view, and as a rest point along the National, Mississippi River Trail.

This project may be combined with development of adjacent property. Improvements to the stormwater system may also be considered with the reconstruction of Riverdale Drive. While the timing is proposed as 2020, the actual project year may relate to the resolution of the road funding discussion.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	100,000										100,000
Total	100,000										100,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	100,000										100,000
Total	100,000										100,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	06-PARK-019
Project Name	Trail Connections

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	5-Opportunity/Unfunded/Placeholder
Status	Active

Total Cost \$910,000

Description

This project would include the installation of one or more of the several high priority connections linking segments of the city's trail system as opportunities and funding become available.

Examples:
 -142nd Ave, east of TH#47, north to Xkimo Street (Est \$80,000)
 -Armstrong Boulevard Trail from Central Park to Trott Brook (Est \$800,000)
 -North Commons Neighborhood Trail Link at Zeolite Street - Projected for 2021 (Est.\$30,000)
 -Boardwalk parallel with Trott Brook, west of Variolite Street (Est \$110,000)

Justification

The increase demand in use of the city's trail system compels the continued development of the trail system.

Resident requests for improved trail system connections are reflected in priority projects. Trails will also move people off high speed roadways and improve transportation and safety - an objective that is aligned within the strategic goal of a Connected Community.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	30,000	80,000	200,000	400,000	200,000						910,000
Total	30,000	80,000	200,000	400,000	200,000						910,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	30,000	80,000		200,000							310,000
To Be Determined			200,000	200,000	200,000						600,000
Total	30,000	80,000	200,000	400,000	200,000						910,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	08-PARK-005
Project Name	Park Development in The COR

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20 Years
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$1,350,000

Description

This proposed capital improvement represents additional park development in The COR, beginning in 2020. The project may be landscape improvements at Municipal Plaza and/or first phase development of The Waterfront to include a splash pad.

Justification

A splash pad continues to be requested by residents, and mass grading for The Waterfront park would generate fill for areas of The COR the city desires to sell.

Note: See also the public improvements considered as part of the completion of Pump House #3 elsewhere in this plan.

Prior	Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
150,000	Improvements Other than Building Cost	1,200,000										1,200,000
Total	Total	1,200,000										1,200,000

Prior	Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
150,000	Park Improvement Trust Fund	1,200,000										1,200,000
Total	Total	1,200,000										1,200,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	15-PARK-01
Project Name	Additional Land for Central Park

Department	Park Improvements
Contact	
Type	Land
Useful Life	
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$250,000

Description
This project envisions the purchase of 15+ acres for an expansion of Central Park or potentially all of the approximate 35 acres east of the existing park.

Justification
Central Park is a highly used park. The additional acreage would offer additional space for parking and recreational use.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Land Acquisition		250,000									250,000
Total		250,000									250,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund		250,000									250,000
Total		250,000									250,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	17-PARK-005
Project Name	Bark Park Shelter-Alpine Park

Department Park Improvements
 Contact
 Type Improvement
 Useful Life 20
 Category Park Improvement
 Priority 2-New Addition (High)
 Status Active

Description

Total Cost \$58,000

Within the off-leash area at Alpine Park would be a lighted hexagonal shelter with concrete floor. Surrounding the shelter for approximately 30 feet and connecting to the trail, would be artificial turf. Alternately, expanding the fencing to increase the area of the off-leash area may be in lieu of the shelter.

Justification

The Bark Park, as it is known, is very popular. So well used, in fact, that grass cannot be maintained in the congregating areas of the park. The artificial turf over a sport court system will provide for a quality surface around the shelter that will not be muddy and will be able to be maintained.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Building Cost/Construction	15,000										15,000
Improvements Other than Building Cost	43,000										43,000
Total	58,000										58,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources	20,000										20,000
Landfill Fund	38,000										38,000
Total	58,000										58,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	17-PARK-006
Project Name	Field Lighting - Central Park

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20
Category	Park Improvement
Priority	4-New Addition (Med)
Status	Active

Total Cost \$200,000

Description
Proposed improvement is for additional athletic field lighting for a community park for fall sport use.

Justification
Fall sports, most notably soccer and football, are constrained by shorter daylight, necessitating field lighting. An alternate for exploration would be constructing more fields.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost					200,000						200,000
Total					200,000						200,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources					50,000						50,000
Lawful Gambling Fund					150,000						150,000
Total					200,000						200,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	17-PARK-007
Project Name	Northfork North Trail Connection

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Description	Total Cost \$150,000
Construct 10 foot paved multi-purpose trail through Outlot B, Northfork and Outlot A. Northfork Trail addition within dedicated 16' easement.	

Justification
Bituminous trail connection constructed in lieu of on-road bicycle land on Andrie Street and 164th Lane NW per permanent pedestrian and bicycle trail easement agreement granted by Northfork Homeowner's Association on May 19th, 2016.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	150,000										150,000
Total	150,000										150,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	150,000										150,000
Total	150,000										150,000

Capital Improvement Program
 City of Ramsey, Minnesota

2021 *thru* 2030

Project #	18-PARK-003
Project Name	Amphitheater Lighting

Department Park Improvements
 Contact
 Type Improvement
 Useful Life 20 Years
 Category Park Improvement
 Priority 2-New Addition (High)
 Status Active

Total Cost \$40,000

Description
Lighting enhancements and audio conduits at The Draw.

Justification
Additional lighting in and around the amphitheater is justified for concerts that go later into the evening and in the later summer. Conduit and cable for audio (additional speakers) is likewise merited for larger events at the park.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	40,000										40,000
Total	40,000										40,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	40,000										40,000
Total	40,000										40,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	18-PARK-004
Project Name	Lake Itasca Park - Parking, Trail & Canoe System

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$250,000

Description

This project would permit access to the community park through a parking area adjacent to Alpine Drive, connection to the existing trails, and include a canoe/kayak/paddle-board system at the lake.

Justification

The parking lot will be for visitors, city-wide, as will the water-based recreational improvements. The trail will connect the new parking to the existing trails near the lake.
 The timing of this limited, first phase of the Lake Itasca community park, also coincides with the hundreds of residential homes being platted in close proximity - with park dedication from these lots applied to the near-by park space for their benefit.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost		250,000									250,000
Total		250,000									250,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund		250,000									250,000
Total		250,000									250,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	18-PARK-005
Project Name	Aeration for Sunfish Lake & The Draw

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20
Category	Park Improvement
Priority	2-New Addition (High)
Status	Active

Total Cost \$40,000

Description

This project would install two separate aeration systems at The Draw and also at Sunfish Lake. Project elements include contracted installation, electrical power supply, signing, permitting and public notifications.

Note: The General Fund Budget would need to be adjusted to accommodate both annual electric expense and other O&M costs estimated at \$1,000 to \$2,000 annually.

Justification

Both Sunfish Lake and The Draw have become popular destinations for youth fishing for panfish. During certain winter conditions, shallow water bodies can experience 'winterkill'. The two separate aeration systems would pump oxygen and circulate the water, maintaining the fish populations and the attendant recreational resource.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	40,000										40,000
Total	40,000										40,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund	40,000										40,000
Total	40,000										40,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	21-PARK-001
Project Name	River's Bend South Shelter Replacement

Department Park Improvements
 Contact
 Type Improvement
 Useful Life 20 Years
 Category Park Improvement
 Priority 1-Existing Obligation (High)
 Status Active

Total Cost \$50,000

Description
 The south shelter at River's Bend Park was furnished and constructed by volunteers in the late 1980's. The condition of the shelter aesthetically is poor, and the concrete slab is broken as well.

Justification
 Replacement of the shelter with a modern, nice looking facility would expand opportunities for shelter reservations and adding stone seat walls and an outdoor fireplace would enhance its appeal.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost		50,000									50,000
Total		50,000									50,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund		50,000									50,000
Total		50,000									50,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	21-PARK-002
Project Name	Central Park Main Parking Lot Reconstruction

Department	Park Improvements
Contact	
Type	Improvement
Useful Life	20
Category	Park Improvement
Priority	3-Existing Obligation (Med)
Status	Active

Total Cost \$200,000

Description
This reconstruction project would replace the 35+ year-old bituminous main parking area at the same time as the adjoining 161st Avenue reconstruction.

Justification
The bituminous surface is beyond its useful maintenance life and the drainage patterns would be modified to reduce significantly the amount of stormwater that flows across 161st Avenue.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost				200,000							200,000
Total				200,000							200,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund				150,000							150,000
Storm Water Utility Fund				50,000							50,000
Total				200,000							200,000

Capital Improvement Program

2021 *thru* 2030

City of Ramsey, Minnesota

Project #	06-ACQ-002
Project Name	Acquire outlots A in Rivers Bluff & Reilly Estates

Department	Site Acquisitions
Contact	
Type	Land
Useful Life	Unassigned
Category	Park Improvement
Priority	5-Opportunity/Unfunded/Placeholder
Status	Active

Description

Total Cost \$20,000

Along the Rum River in the central part of the city are separate outlots owned by two homeowners associations. Both are very attractive, wooded parcels and connected to existing trail corridors. This project may entail the donation of the land in exchange for passive improvements such as trails, picnic areas or fishing.

Justification

The associations would benefit by realizing these amenities near their homes. The \$20,000 expense represents an estimate for real-estate transaction costs, and perhaps park planning to promote the donation. The park plans would generate the development cost projections.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Land Acquisition						20,000					20,000
Total						20,000					20,000

Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund						20,000					20,000
Total						20,000					20,000

Capital Improvement Program
 City of Ramsey, Minnesota

2021 *thru* 2030

Project #	08-ACQ-002
Project Name	Alpaca Estates Outlot

Department Site Acquisitions
 Contact
 Type Land
 Useful Life
 Category Park Improvement
 Priority 5-Opportunity/Unfunded/Placeholder
 Status Active

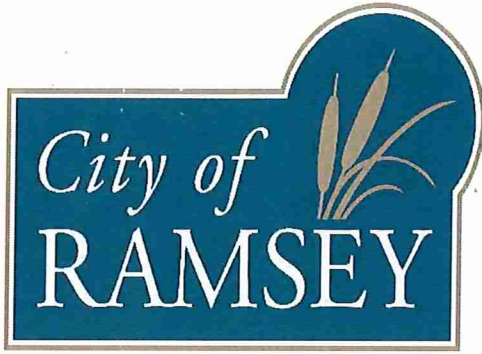
Total Cost \$35,000

Description
This lot was not large enough to build on when the plat was filed for recording.

Justification
This lot was the subject of a development proposal to build a single family home. The proposal was denied. Acquisition of this outlot would provide river access in this area and be part of larger public access area and open space along the Mississippi proposed to the west.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Land Acquisition						35,000					35,000
Total						35,000					35,000

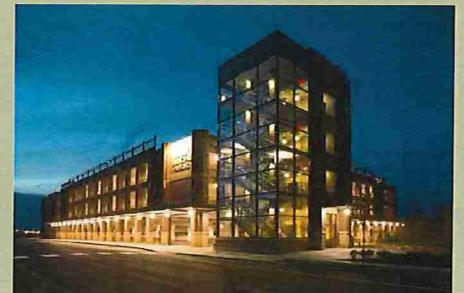
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund						35,000					35,000
Total						35,000					35,000

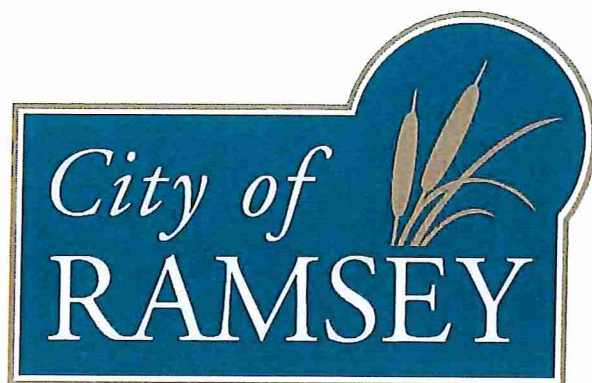


SUPPLEMENTAL FUTURE CONSIDERATION PROJECTS

CITY OF RAMSEY CAPITAL IMPROVEMENT PROGRAM 2021-2030

The Capital Improvement Program document is a planning tool maintained by the city to identify future projects, related expenditures, and funding sources. All projects designated in the Capital Improvement Program are contingent upon availability of resources during the planned year. The total expenditure of projects includes city-funded sources as well as other resources such as grants, fees, bonding, etc.





THIS PAGE INTENTIONALLY BLANK

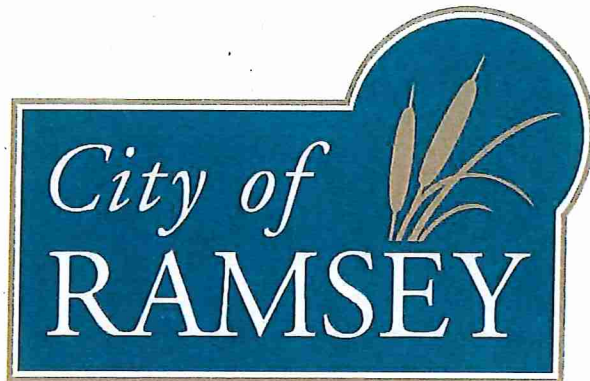
Park and Trail Capital Improvements – Future Projects



The following are park, trail and park acquisition projects that have been identified by the Park and Recreation Commission as proposed, needed improvements into the future – but do not have identified funding at the time of the 10-Year Park’s CIP adoption. In other instances they are predicated on land development or future demand.

The proposed improvements are assigned to category Priority #5 which is known as Opportunity Driven/Unfunded/Placeholder. All projects are given a number 5 Priority, as the projects are currently unfunded.

Generally, additional park and trail planning should be undertaken city-wide as part of the 2040 Comprehensive Plan. A plan will be developed aside from this document to lead that process. The proposed community-wide park and trail plan, together with some of the cost estimates identified below, may point to the need for revenue beyond Park Dedication proceeds that have funded most park and trail initiatives to date.



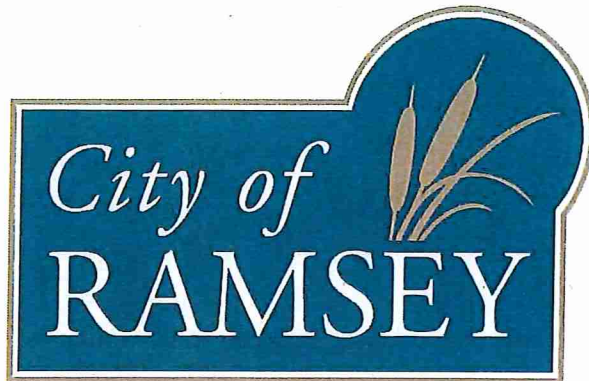
THIS PAGE INTENTIONALLY BLANK

City of Ramsey, Minnesota
 Capital Improvement Program

2021 thru 2030

PROJECTS BY DEPARTMENT

Department	#	Priority	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvements													
Elmcrest Creek Trail to Central Park	04-PARK-002	5							800,000				800,000
Lake Itasca Community Park	04-PARK-007	5		1,200,000									1,200,000
Trott Brook Greenway Trail	04-PARK-014	5					4,015,000						4,015,000
Mississippi River Park Development- Bridge location	06-PARK-016	5						750,000					750,000
Redevelopment of Hunters Hill	06-PARK-017	5									500,000		500,000
Municipal Plaza	07-PARK-001	5	100,000	3,600,000									3,700,000
The Waterfront	08-PARK-004	5		6,300,000									6,300,000
Central Anoka County Regional Trail Bridge over 10	08-PARK-008	5				5,600,000							5,600,000
Alpine Park Winter Facilities	11-PARK-003	5					2,500,000						2,500,000
Lake Itasca / COR Greenway	17-PARK-008	5						1,900,000					1,900,000
Park Improvements Total			100,000	9,900,000	1,200,000	5,600,000	6,515,000	2,650,000	800,000		500,000		27,265,000
Site Acquisitions													
Sixth Community Park (north central Ramsey)	06-ACQ-001	5							3,000,000				3,000,000
Site Acquisitions Total								3,000,000					3,000,000
GRAND TOTAL			100,000	9,900,000	1,200,000	5,600,000	6,515,000	2,650,000	3,800,000		500,000		30,265,000



THIS PAGE INTENTIONALLY BLANK

City of Ramsey, Minnesota
Capital Improvement Program

Data in Year 2021 and 2030

PROJECTS BY YEAR & PRIORITY

Project Name	Department	Project #	Priority	Project Cost
2021				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Municipal Plaza	Park Improvements	07-PARK-001	5	100,000
			<i>Total for: Priority 5</i>	100,000
Total for 2021				100,000
2022				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Municipal Plaza	Park Improvements	07-PARK-001	5	3,600,000
The Waterfront	Park Improvements	08-PARK-004	5	6,300,000
			<i>Total for: Priority 5</i>	9,900,000
Total for 2022				9,900,000
2023				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Lake Itasca Community Park	Park Improvements	04-PARK-007	5	1,200,000
			<i>Total for: Priority 5</i>	1,200,000
Total for 2023				1,200,000
2024				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Central Anoka County Regional Trail Bridge over 10	Park Improvements	08-PARK-008	5	5,600,000
			<i>Total for: Priority 5</i>	5,600,000
Total for 2024				5,600,000
2025				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Trott Brook Greenway Trail	Park Improvements	04-PARK-014	5	4,015,000
Alpine Park Winter Facilities	Park Improvements	11-PARK-003	5	2,500,000
			<i>Total for: Priority 5</i>	6,515,000
Total for 2025				6,515,000
2026				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Mississippi River Park Development-Bridge location	Park Improvements	06-PARK-016	5	750,000
Lake Itasca / COR Greenway	Park Improvements	17-PARK-008	5	1,900,000

Project Name	Department	Project #	Priority	Project Cost
			<i>Total for: Priority 5</i>	2,650,000
Total for 2026				2,650,000
2027				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Elmcrest Creek Trail to Central Park	Park Improvements	04-PARK-002	5	800,000
Sixth Community Park (north central Ramsey)	Site Acquisitions	06-ACQ-001	5	3,000,000
			<i>Total for: Priority 5</i>	3,800,000
Total for 2027				3,800,000
2029				
<i>Priority 5-Opportunity/Unfunded/Placeholder</i>				
Redevelopment of Hunters Hill	Park Improvements	06-PARK-017	5	500,000
			<i>Total for: Priority 5</i>	500,000
Total for 2029				500,000
GRAND TOTAL				30,265,000

**Capital Improvement Program
City of Ramsey, Minnesota**

2021 thru 2030

Project # 04-PARK-002
Project Name Elmcreek Creek Trail to Central Park
Department Park Improvements
Contact Unassigned
Type Improvement
Useful Life 50 Years
Category Park Improvement
Priority 5-Opportunity/Unfunded/Placeholder
Status Future Consideration

Total Cost \$800,000

Description

This trail would link Elmcreek Park to Central Park, the neighborhoods in between, and provide off-roadway trail experience within central Ramsey.

Justification

Connecting the two community parks would reduce the need to replicate some facilities in both locations. The construction of the trail may coincide at such time as residents may subdivide their lots east of Ramsey Blvd, along the county ditch.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost							800,000				800,000
Total							800,000				800,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources							150,000				150,000
Landfill Fund							75,000				75,000
Park Improvement Trust Fund							575,000				575,000
Total							800,000				800,000

**Capital Improvement Program
City of Ramsey, Minnesota**

2021 thru 2030

Department Park Improvements
Contact Unassigned
Type Improvement
Useful Life 50 Years
Category Park Improvement
Priority 5-Opportunity/Unfunded/Placeholder
Status Future Consideration

Project # 04-PARK-007
Project Name Lake Itasca Community Park

Total Cost \$1,200,000

Description

This project is intended to begin the development of Lake Itasca Park North of Alpine Drive, east of the lake to the west line of the adjacent subdivision.

Justification

Properties south of Alpine Drive, both east and west of Puma Street will be proposed residential for development. It is this development that would provide the necessary Park Dedication funds for the first phase of construction, as well as contributing increased recreational demand.

A four acre parcel south of Alpine Drive may be sold to finance improvements in the park. This project includes mass grading of the park and development of multi-use athletic fields, including for lacrosse.

	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Expenditures											
Improvements Other than Building Cost			1,200,000								1,200,000
Total			1,200,000								1,200,000
Funding Sources											
Park Improvement Trust Fund			1,200,000								1,200,000
Total			1,200,000								1,200,000

Capital Improvement Program

2021 thru 2030

City of Ramsey, Minnesota

Project # 04-PARK-014
 Project Name Trott Brook Greenway Trail

Department Park Improvements
 Contact Unassigned
 Type Improvement
 Useful Life 20 Years
 Category Park Improvement
 Priority 5-Opportunity/Unfunded/Placeholder
 Status Future Consideration

Total Cost \$4,015,000

Description

The Trott Brook Greenway Trail extends from Elk River's Youth Athletic Complex near Jarvis Street and follows the brook easterly to Rum River Central Park about six miles away. The project would involve continued acquisition of scenic and trail easements, boardwalk and trail construction and natural resource enhancements. Segments of the trail may be on-street within certain neighborhoods.

Justification

This trail project would be predicated on significant planning leading up to construction and likely involve a Master Plan, as well as receiving grant monies for funding. The land around the brook is the largest and most contiguous undeveloped natural area within Ramsey. The City presently has about half of the land acquired (through Park Dedications and a LCCMR grant) to construct this trail.

This trail has a regional context. There is a trail proposed to connect Sherburne County and Ramsey to a metro regional park. This project may be a good candidate for Legacy Amendment funding, following a regional designation.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost					4,015,000						4,015,000
Total					4,015,000						4,015,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources					4,000,000						4,000,000
Landfill Fund					15,000						15,000
Total					4,015,000						4,015,000

Capital Improvement Program

2021 thru 2030

City of Ramsey, Minnesota

Department Park Improvements

Contact

Type Improvement

Useful Life 20 Years

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$750,000

Project # 06-PARK-016

Project Name Mississippi River Park Development-Bridge location

Description

This neighborhood park is proposed in the general vicinity of the proposed bridge crossing, along the banks of the river. The park would consist of the usual park amenities, as well as open space that may be shared with adjoining new and existing residential land use.

The funding and the timing of the park development would be concurrent with the subdivision of the land.

This park may also serve as a trailhead for a trail (to be built with the bridge construction), that will connect Hennepin County, and the Three Rivers Park District to Ramsey and the Mississippi River Trail.

Justification

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost						750,000					750,000
Total						750,000					750,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund						750,000					750,000
Total						750,000					750,000

Capital Improvement Program

2021 thru 2030

City of Ramsey, Minnesota

Project # 06-PARK-017
 Project Name Redevelopment of Hunters Hill

Department Park Improvements

Contact

Type Improvement

Useful Life 20 Years

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$500,000

Description

Concurrent with any redevelopment of the subdivisions around the existing Hunters Hill Park would be an opportunity to bring improvements to the underutilized Hunters Hill Park. The funding would be from any new units shared. Open space connections within a redeveloped area may also be considered.

Justification

Should redevelopment occur in this area; it is likely densities will greatly increase. This, coupled with the commercial and light industrial uses in the area will create the need for a lunch break destination - as well the residential recreational demand.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost									500,000		500,000
Total									500,000		500,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund									500,000		500,000
Total									500,000		500,000

**Capital Improvement Program
City of Ramsey, Minnesota**

2021 thru 2030

Department Park Improvements

Contact

Type Improvement

Useful Life 20 Years

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$3,700,000

Project #	07-PARK-001
Project Name	Municipal Plaza

Description
Development of the proposed 2-acre Municipal Plaza in The COR, based upon the parks framework plan at \$41 per square foot.

Justification
This urban park-space may be the most prominent park space within The COR - due to its visibility along Sunwood Drive, and its relationship to the Municipal Center and transit hub. The site is also likely to be used for the city's festival known as Happy Days.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost	100,000	3,600,000									3,700,000
Total	100,000	3,600,000									3,700,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources		200,000									200,000
Park Improvement Trust Fund	100,000	3,400,000									3,500,000
Total	100,000	3,600,000									3,700,000

Capital Improvement Program

2021 thru 2030

City of Ramsey, Minnesota

Project #	08-PARK-004
Project Name	The Waterfront

Department Park Improvements

Contact

Type Improvement

Useful Life 20 Years

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$6,300,000

Description

This project will be the culmination of comprehensive planning for a significant water feature in The COR. The 'lake' will be a layering of public and private amenities - such as the integration of a restaurant, required open space, public paths, wading areas, a splash pad and shady aesthetics, all of which provides the "green" and "blue" (and stormwater) connections between The Draw and retail areas.

Justification

In addition to providing a superior view-shed for residential and retail sites as well as water recreation, the pond will handle storm run-off and provide fill material for other projects within The COR. The cost projections are based on the parks framework plan for The COR at \$11 per square foot.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost		6,300,000									6,300,000
Total		6,300,000									6,300,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund		6,300,000									6,300,000
Total		6,300,000									6,300,000

Capital Improvement Program

2021 thru 2030

City of Ramsey, Minnesota

Project #	08-PARK-008
Project Name	Central Anoka County Regional Trail Bridge over 10

Department Park Improvements

Contact

Type Improvement

Useful Life 20 Years

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$5,600,000

Description

Bridge #2053 will connect The COR to the trail to the Mississippi River along the storm sewer outfall and serve as a regional trail connection between the Northstar Rail Station and Mississippi West Reional Park.

Justification

The COR is designed to be pedestrian friendly. This bridge will provide a connection to West Mississippi Regional Park and the regional trail. Further, the rail and highway crossing will allow residents to access the commuter rail and The COR without adding automobile trips to Hwy's #10 and #169.

Ramsey has received \$100,000 from Met Council for engineering and design, and \$490,000 from the National Park Service to complete the final plans and specifications. Additionally, a private developer has contributed the touch-down land adjacent to Riverdale Drive.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost				5,600,000							5,600,000
Total				5,600,000							5,600,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Grants/Outside Sources				5,600,000							5,600,000
Total				5,600,000							5,600,000

**Capital Improvement Program
City of Ramsey, Minnesota**

2021 thru 2030

Project # 11-PARK-003
Project Name Alpine Park Winter Facilities

Department Park Improvements
Contact
Type Improvement
Useful Life 25
Category Park Improvement
Priority 5-Opportunity/Unfunded/Placeholder
Status Future Consideration

Description Total Cost \$2,500,000

This project shall include the construction of a chalet/warming house capable of facilitating winter equipment rentals and vending. Improvements will also include the expansion of the existing parking lot, and the construction of refrigerated rinks.

Justification

This warming house will function as a hub for winter recreation in the city with ice skating, sliding, and cross-country ski trails. Further, the existing warming house and rinks at Ramsey Elementary are in sub-standard condition and would be discontinued under this proposal.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Building Cost/Construction					750,000						750,000
Improvements Other than Building Cost					1,750,000						1,750,000
Total					2,500,000						2,500,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund					2,500,000						2,500,000
Total					2,500,000						2,500,000

**Capital Improvement Program
City of Ramsey, Minnesota**

2021 thru 2030

Department Park Improvements

Contact

Type Improvement

Useful Life 20

Category Park Improvement

Priority 5-Opportunity/Unfunded/Placeholder

Status Future Consideration

Total Cost \$1,900,000

Project # 17-PARK-008
Project Name Lake Itasca / COR Greenway

Description

This project envisions a Greenway connection for the Municipal Plaza, The Draw, the lake feature west through The COR to the Lake Itasca Community Park.

Justification

The Greenway will be the primary east/west pedestrian route on either side of Armstrong Blvd, connecting places to live, work and recreate. It is intended that the Greenway also will be a working component of the stormwater system.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Improvements Other than Building Cost						1,900,000					1,900,000
Total						1,900,000					1,900,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund						1,900,000					1,900,000
Total						1,900,000					1,900,000

Capital Improvement Program
City of Ramsey, Minnesota

2021 thru 2030

Project #	06-ACQ-001
Project Name	Sixth Community Park (north central Ramsey)

Department Site Acquisitions
 Contact
 Type Land
 Useful Life Unassigned
 Category Site Acquisition
 Priority 5-Opportunity/Unfunded/Placeholder
 Status Future Consideration

Description	Total Cost \$3,000,000
The park location has not been determined, but may be expected to be sited north of Trott Brook and geographically centralized.	
The park should have a minimum of 60 acres of usable land.	

Justification
 Given the projected build-out of Ramsey, it is anticipated that a sixth Community Park will be required to meet the needs of the existing and future residents.
 A joint powers agreement for funding and O & M may be explored with the City of Nowthen.

Expenditures	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Land Acquisition							3,000,000				3,000,000
Total							3,000,000				3,000,000
Funding Sources	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	Total
Park Improvement Trust Fund							3,000,000				3,000,000
Total							3,000,000				3,000,000

Meeting Date: 11/12/2020

By: Mark Riverblood, Engineering/Public Works

Information

Title:

Recommend Proposed Improvements to Alpine Park's Off-leash Area—a 2020 Parks Capital Improvement

Purpose/Background:

This case is a follow-up on a proposal by resident(s) to expand the off-leash area at Alpine Park as part of the 2020 Parks Capital Improvement (CIP) proposal for improvements to the 'dog park' area at the park. At the Commission's regular October meeting, the consensus was to recommend the project to City Council pending learning the project's cost estimate.

Notification:

Observations/Alternatives:

Commission discussion on this CIP project began in 2020 with a Ramsey resident attending the Commission's March meeting to advocate for a particular improvement for the off-leash area of Alpine Park. Her summary memo to this affect is attached. At the June meeting, the Commission discussed meeting at Alpine Park in October to fully consider that proposal, which did occur. Resident Susan Brockman presented her proposal again at this meeting, with Commission members discussing various aspects and options for the proposed improvements.

In summary, staff feels that the proposed expansion of the off-leash area to the west of the existing area with three gates may be completed for \$14,000. Also discussed was lighting for this same expansion, Staff does not have detailed plans for what this may consist of, but feels that some type of solar lighting may be developed subsequent to the completion of the fencing for less than \$5,000.

Funding Source:

The Capital Improvement work sheet identifies the proposed funding sources.

Recommendation:

Based upon Commission direction at October's meeting, Staff recommends a motion by the Commission for City Council to proceed with an expansion of the the off-leash area at Alpine Park, with solar lighting at a not-to-exceed cost of \$20,000, for implementation in 2020 if possible, or Spring of 2021 if Winter conditions do not permit earlier construction.

Action:

Based upon discussion, motion to recommend City Council authorize \$20,000 for the expansion of Alpine Park's off-leash area to include aspects of solar lighting.

Attachments

Resident proposal

2020 CIP worksheet

Form Review

Inbox

Grant Riemer

Form Started By: Mark Riverblood

Final Approval Date: 11/09/2020

Reviewed By

MaryJo Warner

Date

11/09/2020 03:48 PM

Started On: 11/09/2020 02:26 PM

To: Mark Riverblood, Parks & Assistant Public Works Superintendent

Fr: Sue Brockman, Ramsey Resident

Sub: Expansion of Alpine Park Off-Leash Dog Park

Da: February 5, 2020

Proposal for the Expansion of the Alpine Park Off-Leash Dog Park

This proposal is to expand the park to the West of the existing fence line up to the wetlands and the West edge of the soccer field.

The South side would be using the existing fencing that is the border of the landfill acreage.

The North side would need to have fencing added that would extend from the furthest west baseball diamond across the South end of the soccer field.

The West end would need to be fenced.

Gates added as necessary.

A future addition would be lighting towards the West end.

Alpine Park Off-Leash Dog Park
Expansion Proposal 2/5/2020



Reasons for Expansion

- Reduce confrontations amongst dogs, because the park is just large enough for a dog to feel it is their territory.
- Increase options of entering the park to avoid the “Welcoming committee” of dogs at the gate- which is intimidating for some owners and dogs.
- Dog owners walk the perimeter of the park for exercise and to keep their dogs moving to help avoid confrontations.
- Increased running room for larger dogs and high energy breeds, could give a more natural separation of sizes.
- It would help reduce some of the wear on the turf of the existing park.

Meeting Date: 11/12/2020

By: Mark Riverblood, Engineering/Public Works

Information

Title:

Note Draft Center Street Area Framework and Policy Plan

Purpose/Background:

The primary purpose of this case is to note and provide comment on this planning document, which will be an update to The COR Design Guidelines and Framework (only for the Downtown District), which is the second attachment. The first attachment is the Draft *Center Street Area Framework and Policy Plan* and has much to do with streetscapes, stormwater elements and the pumphouse, but also touches on sidewalks, the Regional Trail and other pedestrian elements and walkability (which are included within the Commission's purview). It is anticipated the planning document will go to each of the boards and commissions for comment, and then to City Council, perhaps as soon as January.

As indicated, the purpose of this case is to obtain feedback on *Center Street Area Framework and Policy Plan*, but also to advance the planning activity for several interrelated projects around Center Street—some of which having timelines that require the work to commence soon.

A *summary* of these projects (and a brief description of the interrelatedness) is as follows:

~ **Center Street sidewalk(s)** in front of the Sapphire building:

The city has collected money for the sidewalk/streetscape improvements for a city-lead construction project. The developer has the Sapphire apartment building ready for these improvements. The specific design work for matching the public realm (sidewalk and streetscape) to the adjoining and parallel private sidewalk and building has not been completed. The sidewalk should be constructed in the Spring of 2021

~ **Center Street streetscape/trail** (west side) near the interface of the Affinity at Ramsey building and Municipal Plaza:

East of the Affinity building is a narrow area of the park, south of a new segment of Ramsey Parkway. This is also the Central Anoka County Regional Trail which is a temporary section of bituminous, with the plan to convert it to concrete, (like the the finished 10' foot concrete trail to the south, parallel to Center Street). The 2020 Parks Capital Improvement Plan (CIP) includes a proposed project page for this, with some of the preliminary conceptual work done.

~ **Municipal Plaza** at the interface with Affinity:

This area is largely unfinished but does have turf established, but with no irrigation, following the Affinity project's completion. The preliminary concept plans for the park, show a plaza-like extension of the streetscape/trail blending into the park for a usable, programmable space. The final design (to include public furniture, hearth, landscape, for the narrow north portion of the park) should be considered for completion in 2021 to match the Affinity project's edge. This would allow for the irrigation system to be designed for this park area as well. Additionally, the city has the obligation to 'take over' the irrigation for the median swale within the Ramsey Parkway, (this city property is presently being served by the Affinity building at their expense). Completing this specific park area would be of benefit to the future residents who will reside on either side of this space as well as other park patrons, including for Happy Days' programming.

~ **Municipal Plaza**, south of the above (the larger, Happy Days site):

The fill from the 2019 COR Infiltration Basin was placed in the park as noted above. The city may also consider an underground irrigation for the park, as large scale park improvements may be a few or more years out. This flat, turfed area would function as a lush park space until the final improvements to the park are warranted and complete.

~ **Pump House Plaza:**

The area immediately surrounding the small Pump House No. #3 is unimproved and consists of sand-burs and weedy grasses. As part of the 2019 Metropolitan Council Local Community Development grant exploration, the Pump House site was evaluated and found that its grounds can bring significant value to Ramsey's downtown by functioning as a plaza and practical extension of the sidewalk and connected park (and especially when Center Street may be closed for events). The cost and design of adding a public restroom to this site to serve the the area parks and trails will be evaluated as this planning document and process moves forward.

~ **Pump House Exterior completion:**

The facade treatment was withheld from the Pump House when it was constructed over a decade and a half ago. This was deliberate, so as to match the building with the municipal architecture at such time as this was known. It is believed that this project is timely, in that the city has made the investments in civic place and architecture, *and* the other related improvements (Municipal Plaza and the future Waterfront) can be considered along with this endeavor—each informing the other, thus setting forth the civic architecture for establishing the sense of place, in the very center of Ramsey's walkable downtown. Additionally, the Pump House is stark in appearance (without the architectural cladding), and finishing the building will meet with the same standards as other private projects are held to in The COR, raising property values, and at the same time the site's utility (discussed above). The city's CIP shows this project occurring in 2020, (with that worksheet as the second attachment).

~ **145th Avenue sidewalk:**

As of the Fall of 2020, the Sapphire project site is substantially complete, including a widened sidewalk from Sapphire Street, westerly along 145th to the Pump House site. The city is required to complete approximately 100' feet of sidewalk on the south side of the Pump House to make the connection to Center Street. The planning for this sidewalk will occur alongside the above Pump House Plaza planning activity.

Timeframe:

▸

Observations/Alternatives:

Additional detail will be presented at the meeting.

.

Funding Source:

Funding for the various capital improvements (to be approved outside the scope of this case) are identified within the respective portion of the CIP or other separate Council authorization.

Recommendation:

Staff will review in greater detail, the *Center Street Area Framework and Policy Plan* and recommends the Commission issue feedback this draft plan, and in particular the sidewalk elements, the Regional Trail and other pedestrian and walkability aspects this case and the policy plan outlines.

Action:

Provide comment and feedback on the Draft *Center Street Area Framework and Policy Plan*.

Attachments

Draft Center Street Area Framework

Adopted COR Design Framework

2020 CIP

site context

Pump House #3

Form Review

Inbox

Grant Riemer

Kurt Ulrich

Form Started By: Mark Riverblood

Final Approval Date: 11/09/2020

Reviewed By

MaryJo Warner

MaryJo Warner

Date

11/09/2020 04:28 PM

11/09/2020 04:29 PM

Started On: 11/09/2020 03:51 PM

CENTER STREET AREA FRAMEWORK + POLICY PLAN



**Pump House Plaza + Pump House Building +
Municipal Plaza + Center Street**

City of Ramsey, Minnesota
November, 2020

ACKNOWLEDGMENTS

This Framework and Policy Plan focuses on the very center of The Center of Ramsey's 320 acre downtown referred to as The COR. The fact that the community is succeeding in the primary vision of having its own downtown and identity cannot be understated. The COR is over half built-out according to plan, with favorable progress continuing even within and through the Great Recession. This is attributable to Ramsey residents, and leadership, holding to the original vision for a pedestrian oriented, successful, vibrant downtown–this Center Street Area Framework and Policy Plan is an implementation strategy that supports the vision and objectives of The COR Design Framework which cites an 'implied responsibility' to carry forth the goals in site planning and design:

"All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design." The Center Street Area Framework and Policy Plan does just this, by integrating and interrelating the above tenets.

The COR Design Framework goes on to list the three Objectives:

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity.
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to [and within] downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. *This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

* The above is also mirrored in The Circle of Ramsey Greenway, of which The COR, is an integral part and a destination within.

The COR was an organic, citizen led movement in delivering an urban downtown with its amenities for the whole community, while also 'taking some of the development pressure off' the more rural areas of the city. The success in finding that balance has been due to thoughtful, reasoned residents–unified in mission and vision. Future Ramsey residents can thank today's residents and Planning Commission, Economic Development Authority, the Park and Recreation Commission, and City Council for coming together in their collective service to the community.

Parks & Assist. Public Works Superintendent,
Mark Riverblood

TABLE OF CONTENTS

Background + Purpose (Context/Coverage Area/Components).....	3
Pump House Building (Location/Site Plan/Project Description/Design Program).....	5
Rest Room + Maintenance Building (Location/Site Plan/Project Description/Design Program)	7
Pump House Plaza (Location/Site Plan/Project Description/Design Program)	9
Sapphire Apartment Frontage (Location/Site Plan/Project Description/Design Program).....	18
Center Street (Location/Site Plan/Project Description/Design Program)	22
Central Anoka County Regional Trail (Location/Site Plan/Project Description/Design Program).....	24
Stormwater Management System (Location/Site Plan/Project Description/Design Program)	26
Future Development Site Frontage (Location/Site Plan/Project Description/Design Program).....	30

Background + Purpose

Overview

The purpose of the Center Street Area Framework + Policy Plan is to advance the planning activity for several interrelated projects within this district. Several of these project have timelines that require the work to commence as soon as possible. A summary list of current and potential future projects includes:

- Center Street sidewalk(s) in front of the Sapphire building;
- Center Street streetscape/trail (west side) near the interface of the Affinity and Municipal Plaza;
- Municipal Plaza (north) at the interface with Affinity;
- Municipal Plaza (south) the larger Happy Days site;
- Pump House Plaza the area immediately surrounding the small Pump House No. #3;
- Pump House Exterior potential facade improvements;
- 145th Avenue sidewalk connecting to Center Street.

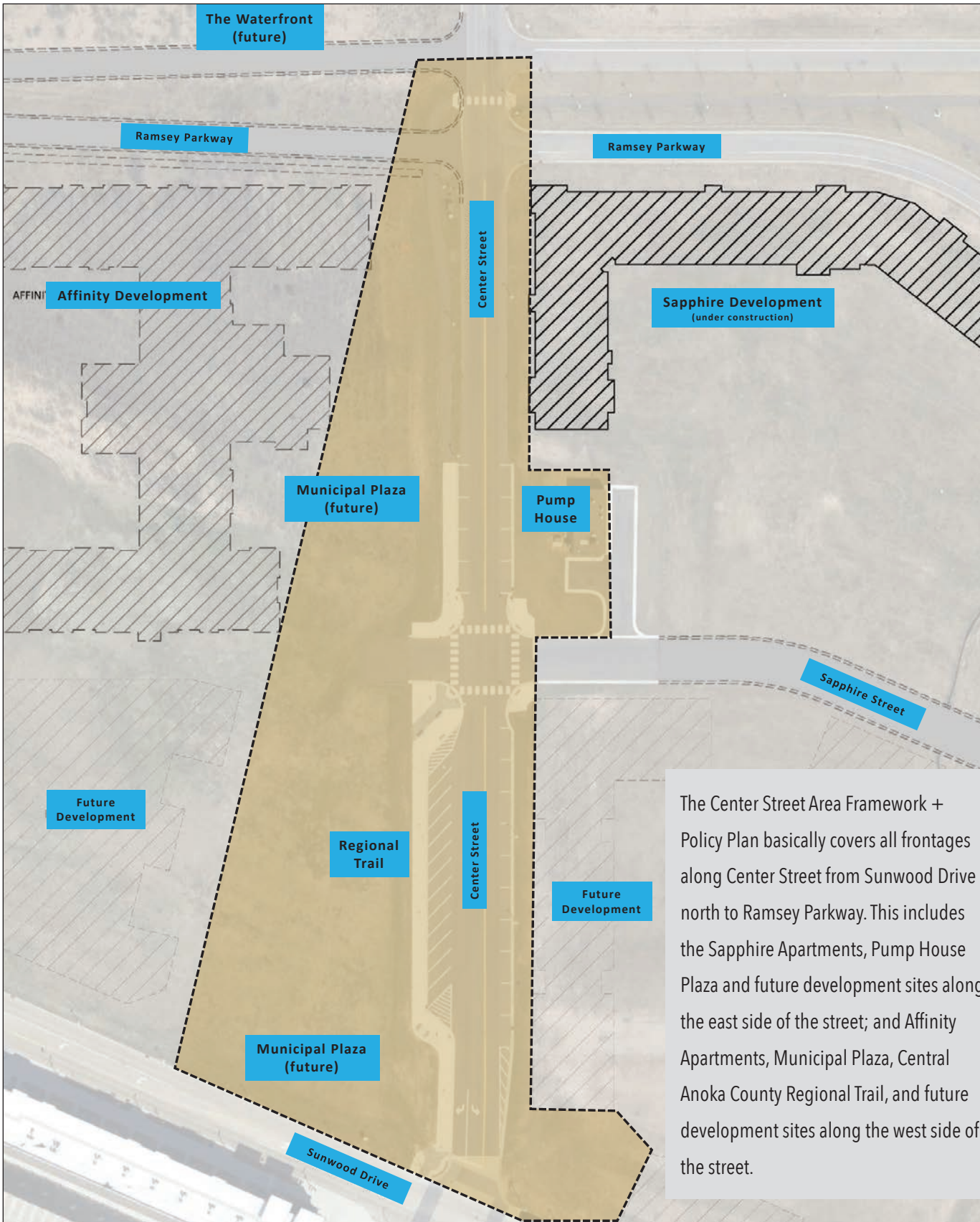
Outcomes + Deliverables

As indicated, the above project listing is merely a summary outline of the work to be performed; the detailed Scope of Services (provided by the Consultant Team), will result in design guidelines to allow the city to develop specifications (cross-sections with details) such that quotations may be sought for work that must be completed like the sidewalk/streetscape adjoining the Sapphire building. Additionally, the scope and cost of other features may be better understood, and accordingly, will result in the ability to proceed with improvements to Municipal Plaza, and the Pump House site (plaza, concrete, landscaping and conceivably an attached public restroom), at such time it is authorized by City Council, (in 2021 or thereafter).

Further, the above referenced Scope of Services will develop the design for the Pump House facade and related architectural treatments, and result in a Design-build RFP that will provide City Council with the cost and contractor (upon approval) that can complete the 2020 Capital Improvement Project for Municipal Pump House #3.

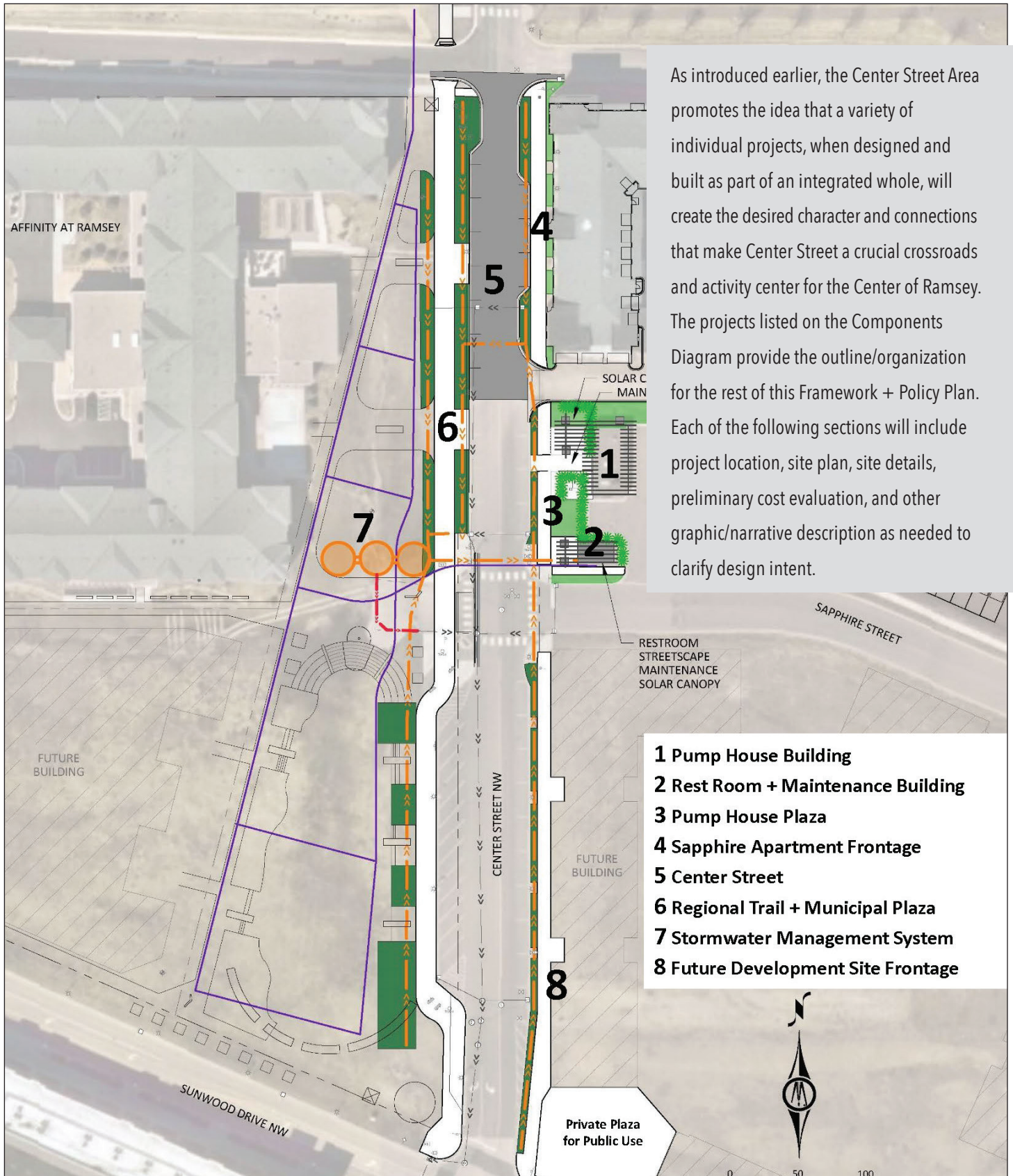


Coverage Diagram



The Center Street Area Framework + Policy Plan basically covers all frontages along Center Street from Sunwood Drive north to Ramsey Parkway. This includes the Sapphire Apartments, Pump House Plaza and future development sites along the east side of the street; and Affinity Apartments, Municipal Plaza, Central Anoka County Regional Trail, and future development sites along the west side of the street.

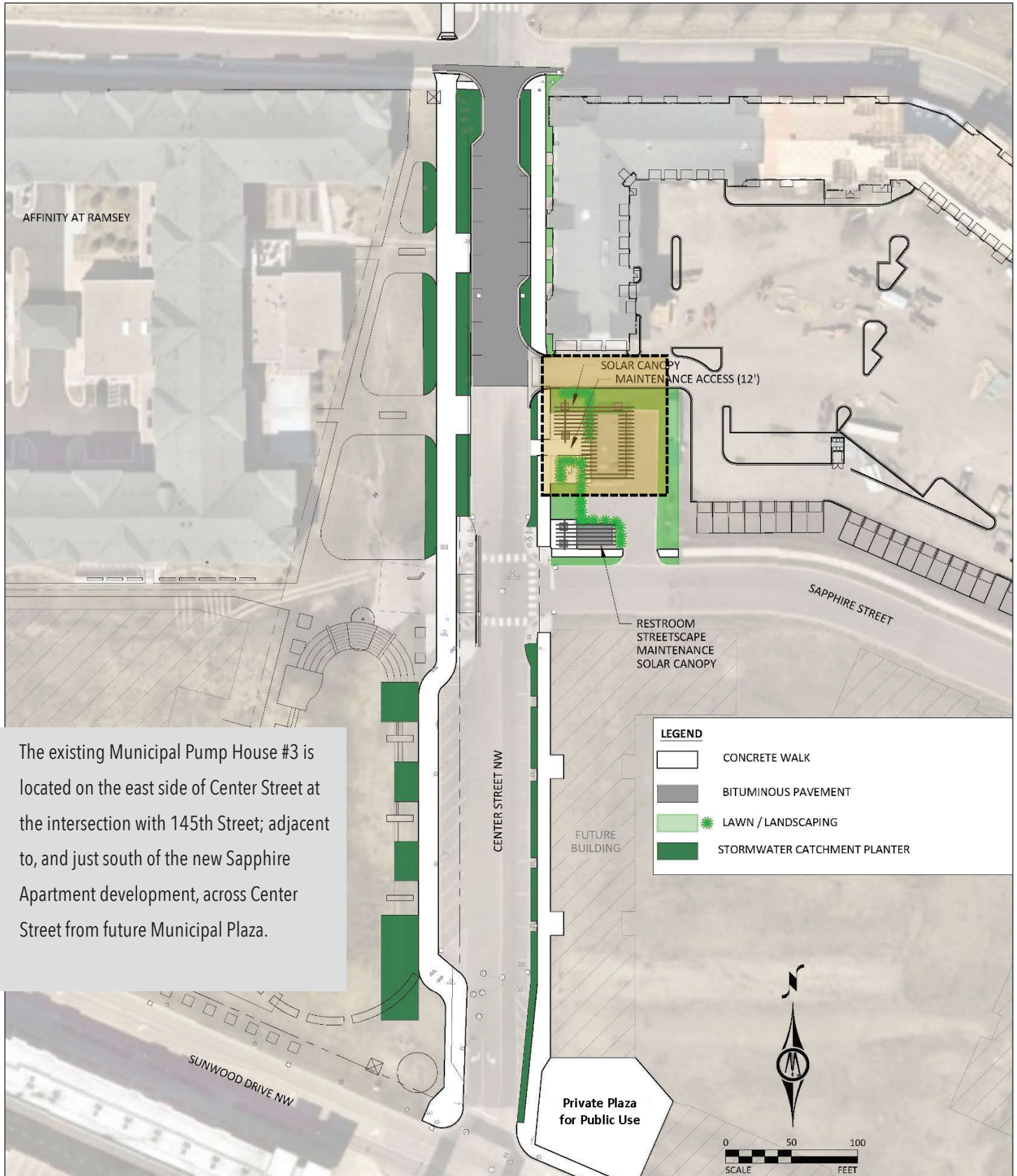
Components Diagram



As introduced earlier, the Center Street Area promotes the idea that a variety of individual projects, when designed and built as part of an integrated whole, will create the desired character and connections that make Center Street a crucial crossroads and activity center for the Center of Ramsey. The projects listed on the Components Diagram provide the outline/organization for the rest of this Framework + Policy Plan. Each of the following sections will include project location, site plan, site details, preliminary cost evaluation, and other graphic/narrative description as needed to clarify design intent.

- 1 Pump House Building**
- 2 Rest Room + Maintenance Building**
- 3 Pump House Plaza**
- 4 Sapphire Apartment Frontage**
- 5 Center Street**
- 6 Regional Trail + Municipal Plaza**
- 7 Stormwater Management System**
- 8 Future Development Site Frontage**

Pump House Building Location Map



The existing Municipal Pump House #3 is located on the east side of Center Street at the intersection with 145th Street; adjacent to, and just south of the new Sapphire Apartment development, across Center Street from future Municipal Plaza.



Pump House Building

PROJECT DESCRIPTION

Since the time of its construction, Municipal Pump House #3 has been a simple concrete masonry building. As new development occurs around it, Pump House #3's appearance can contribute to the character of surrounding buildings and the proposed plaza by applying new exterior finishes to the existing building. Stone, wood, and metal finishes and colors compliment other civic buildings in Ramsey Downtown District.

Note: the sequence of project design and construction will begin with Pump House Building facade improvements, followed by Pump House Plaza site and landscape improvements, and then additional project feasibility evaluation and detailed design related to the Rest Room + Maintenance Building.

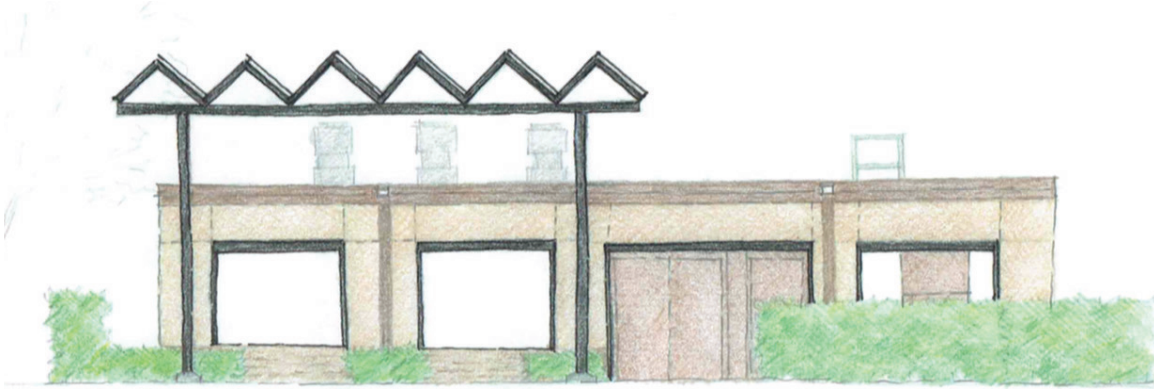
Preliminary estimate of potential construction cost: \$175,000 - 190,000



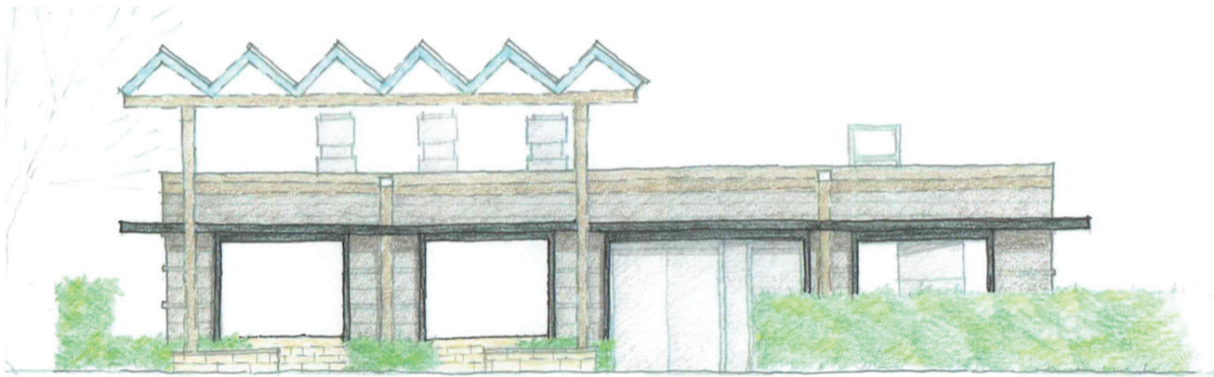
The feasibility of adding photo-voltaic array over Pump House #3 and the adjacent plaza, shall be evaluated based upon additional cost estimates and pay-back timeline.

Pump House Building Project Elements

Three alternative exterior façade treatments have been considered:



- Concept A: Split-faced concrete masonry & cement board siding



- Concept B: Limestone & wood siding

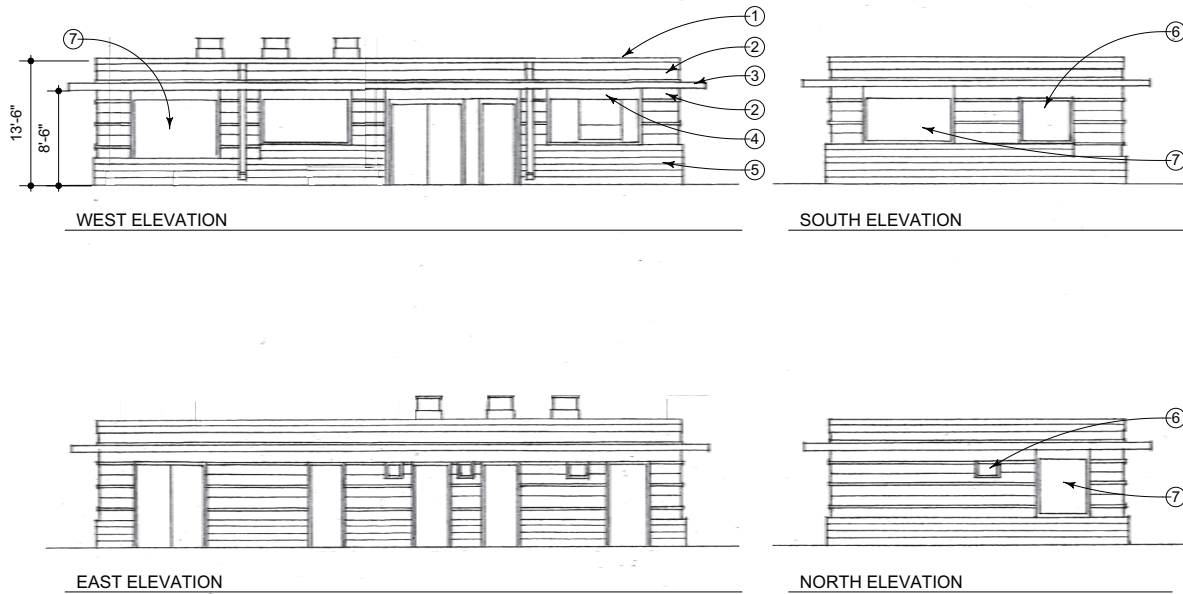


- Concept C: Limestone & brick

Based on design refinements, cost considerations, the limestone and wood siding alternative has been selected for design development.

Pump House Building Project Elements

Concept B Developed Design.

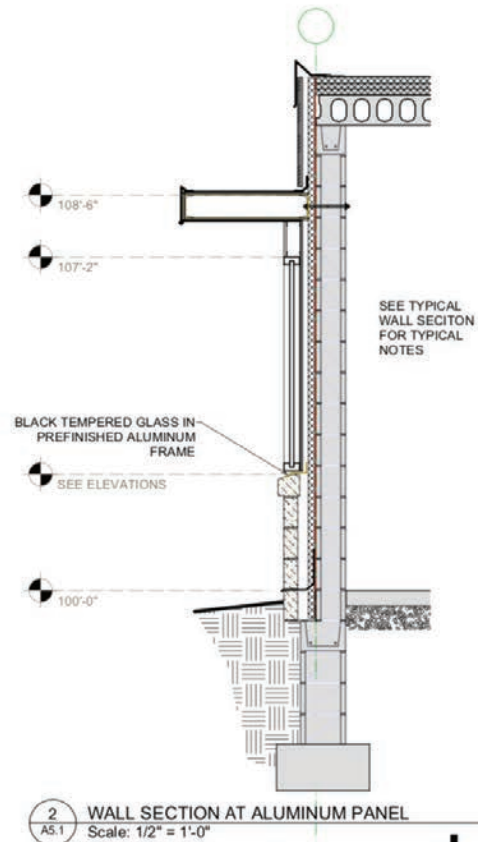
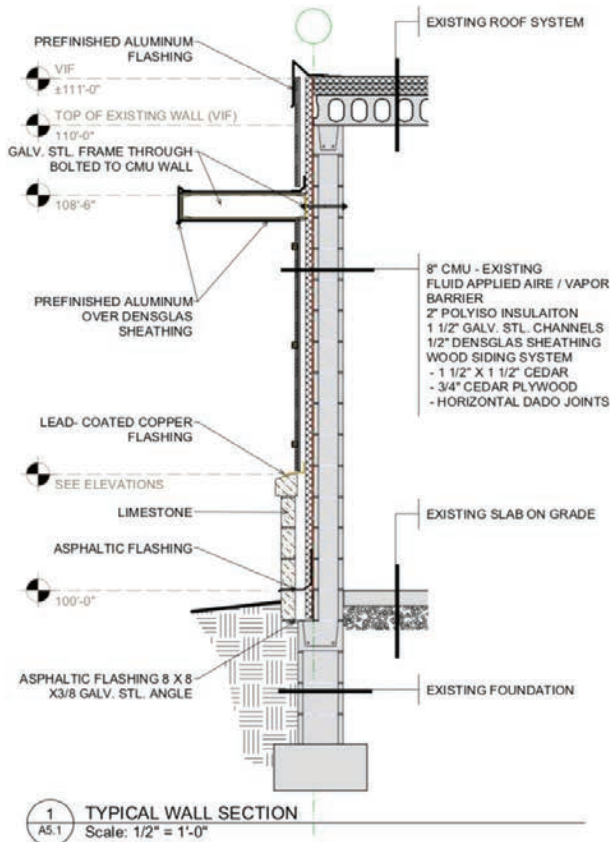


NOTES

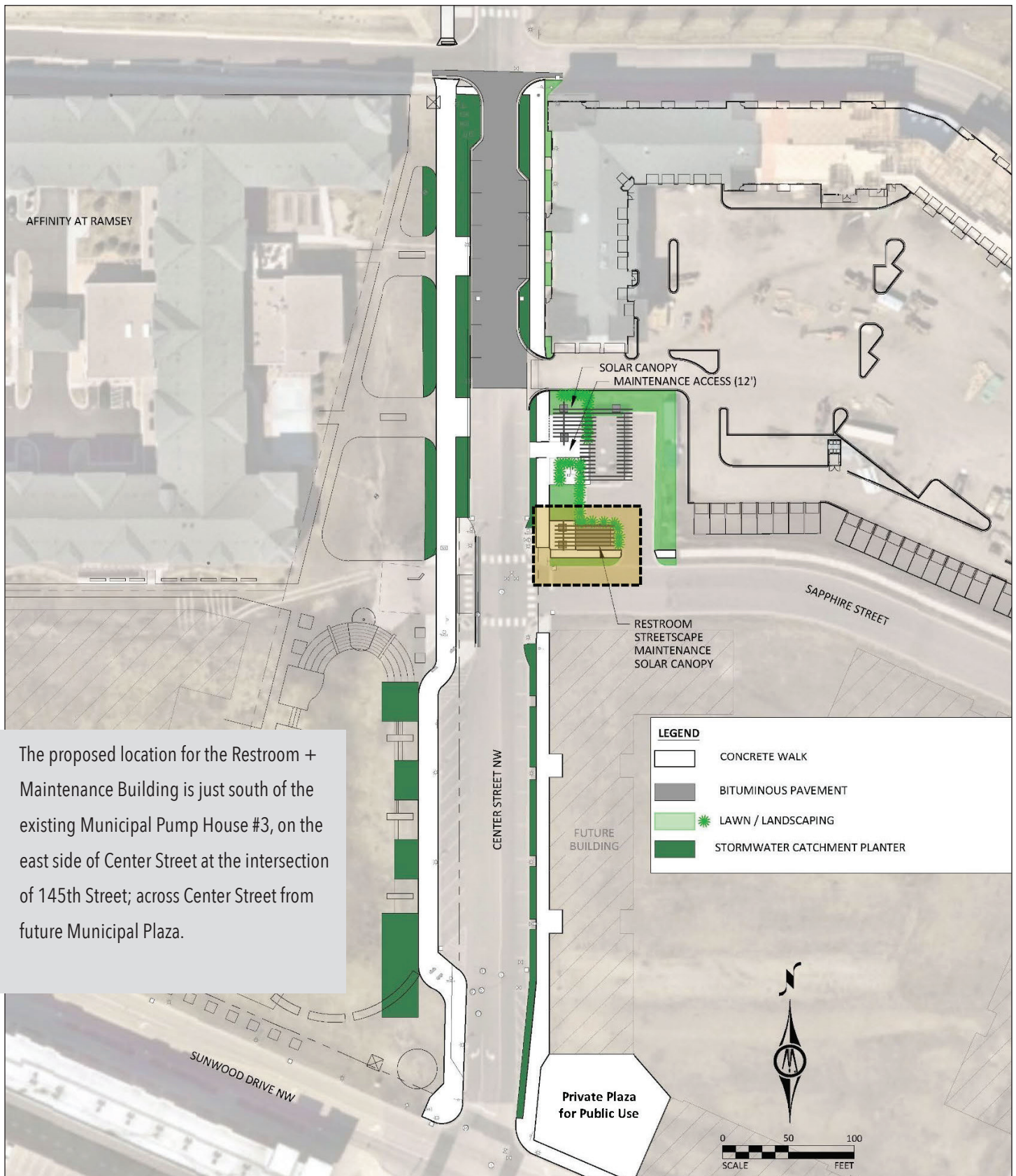
- 1 ALUMINUM CAP FLASHING
- 2 WOOD SIDING
- 3 ALUMINUM EYEBROW
- 4 ALUMINUM FRAME
- 5 LIMESTONE BASE
- 6 EXISTING VENT
- 7 SMOKED GLASS WITH ALUMINUM FRAME

BUILDING ELEVATIONS AT PUMP HOUSE- CONCEPT B.1

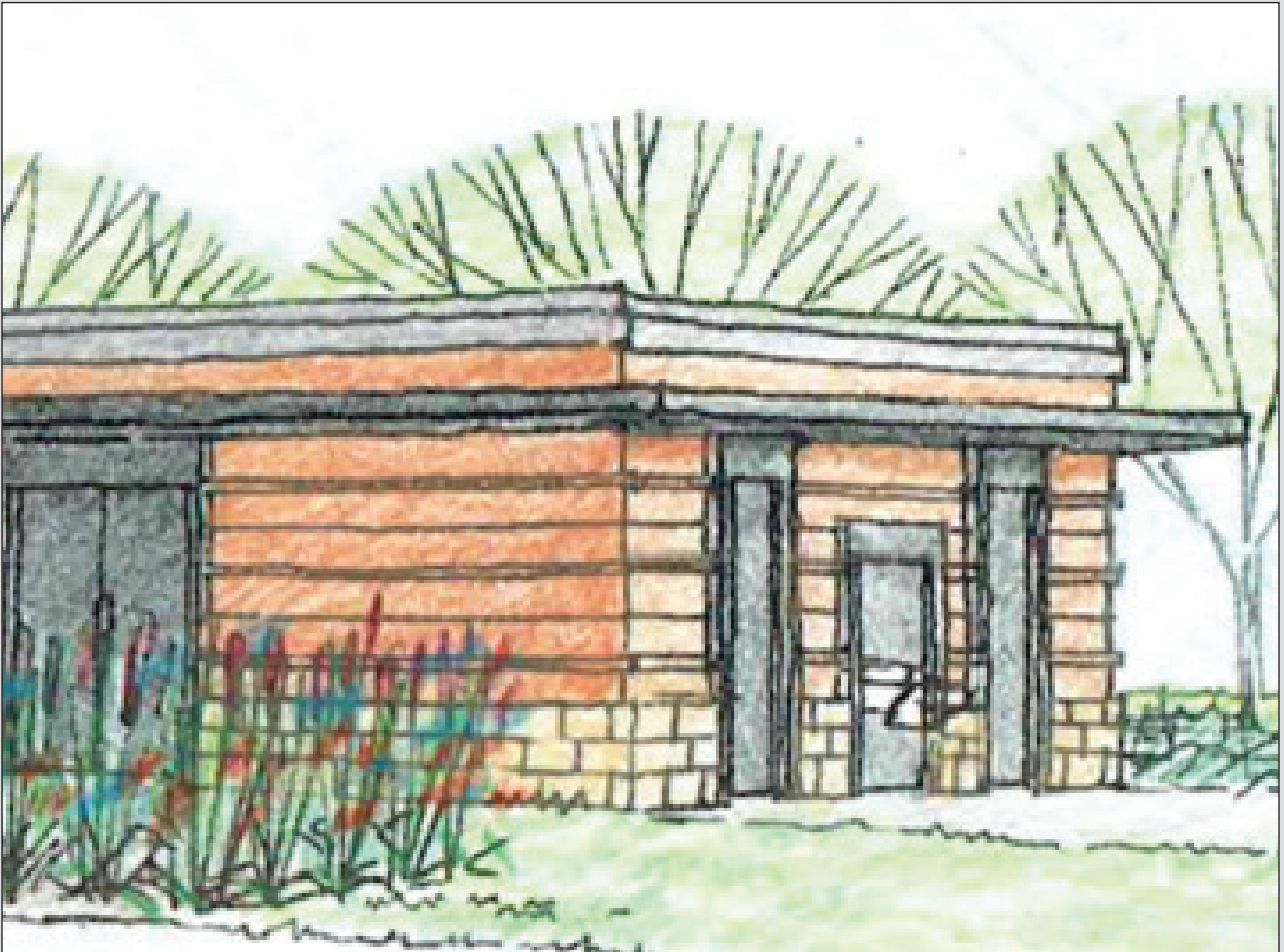
SCALE: 1/8" = 1'-0"



Restroom + Maintenance Building Location Map



The proposed location for the Restroom + Maintenance Building is just south of the existing Municipal Pump House #3, on the east side of Center Street at the intersection of 145th Street; across Center Street from future Municipal Plaza.



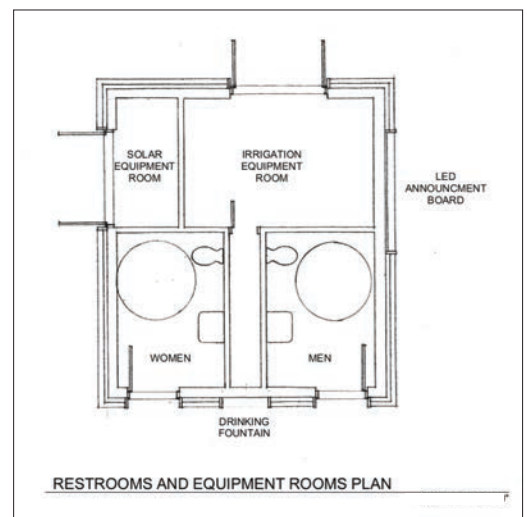
Restroom + Maintenance Bldg

PROJECT DESCRIPTION

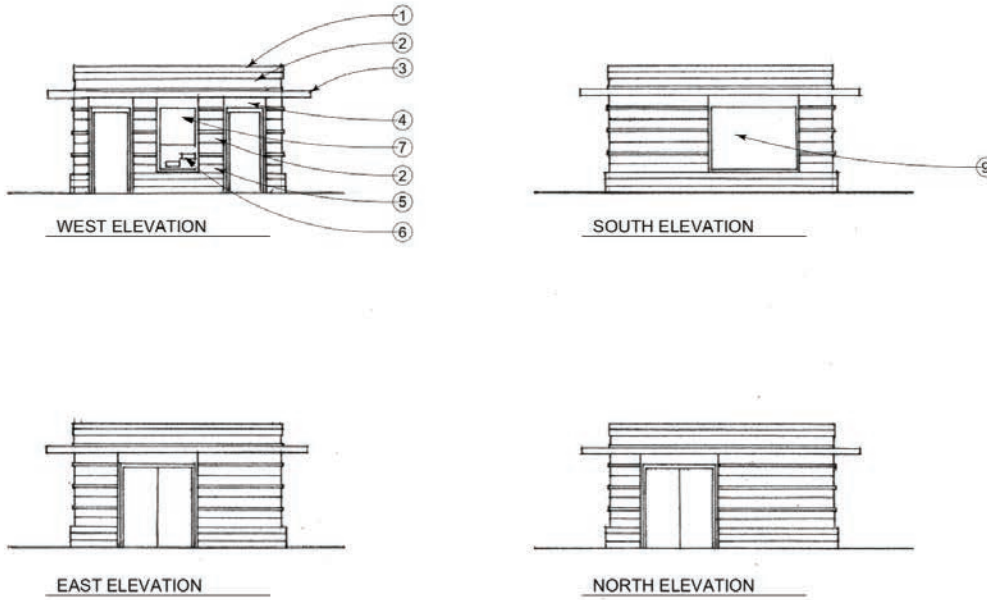
A proposed Restroom + Maintenance Building will be sided with the same materials as the adjacent Pump House #3. This building will support community activities at Center Street’s Municipal Plaza. It will also house equipment for the plaza’s irrigation system and for the solar array at the Pump House building.

Note: the sequence of project design and construction will begin with Pump House Building facade improvements, followed by Pump House Plaza site and landscape improvements, and then additional project feasibility evaluation and detailed design related to the Rest Room + Maintenance Building.

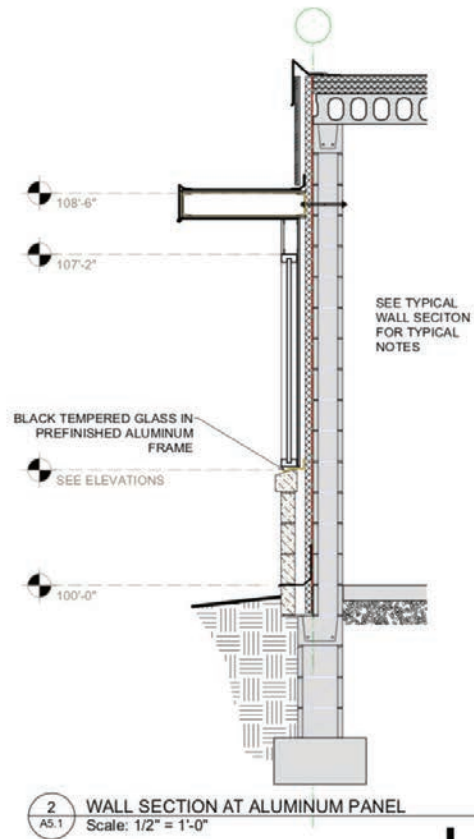
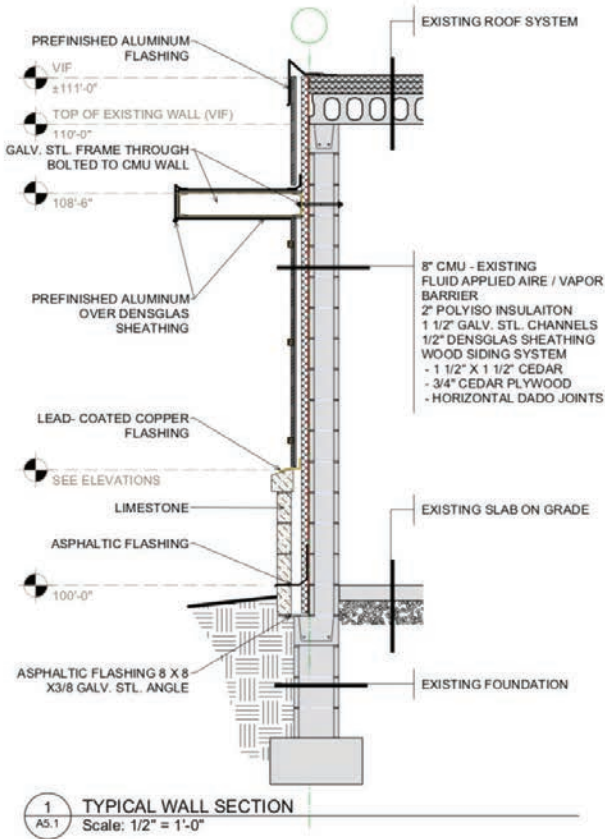
Potential construction cost included in preliminary estimate for the Stormwater Management System described on page 27.



Restroom + Maintenance Bldg Project Elements



- NOTES**
- 1 ALUMINUM CAP FLASHING
 - 2 WOOD SIDING
 - 3 ALUMINUM EYEBROW
 - 4 ALUMINUM FRAME
 - 5 LIMESTONE BASE
 - 6 DRINKING FOUNTAIN
 - 7 LED DISPLAY
 - 8 EXISTING VENT
 - 9 SMOKED GLASS WITH ALUMINUM FRAME

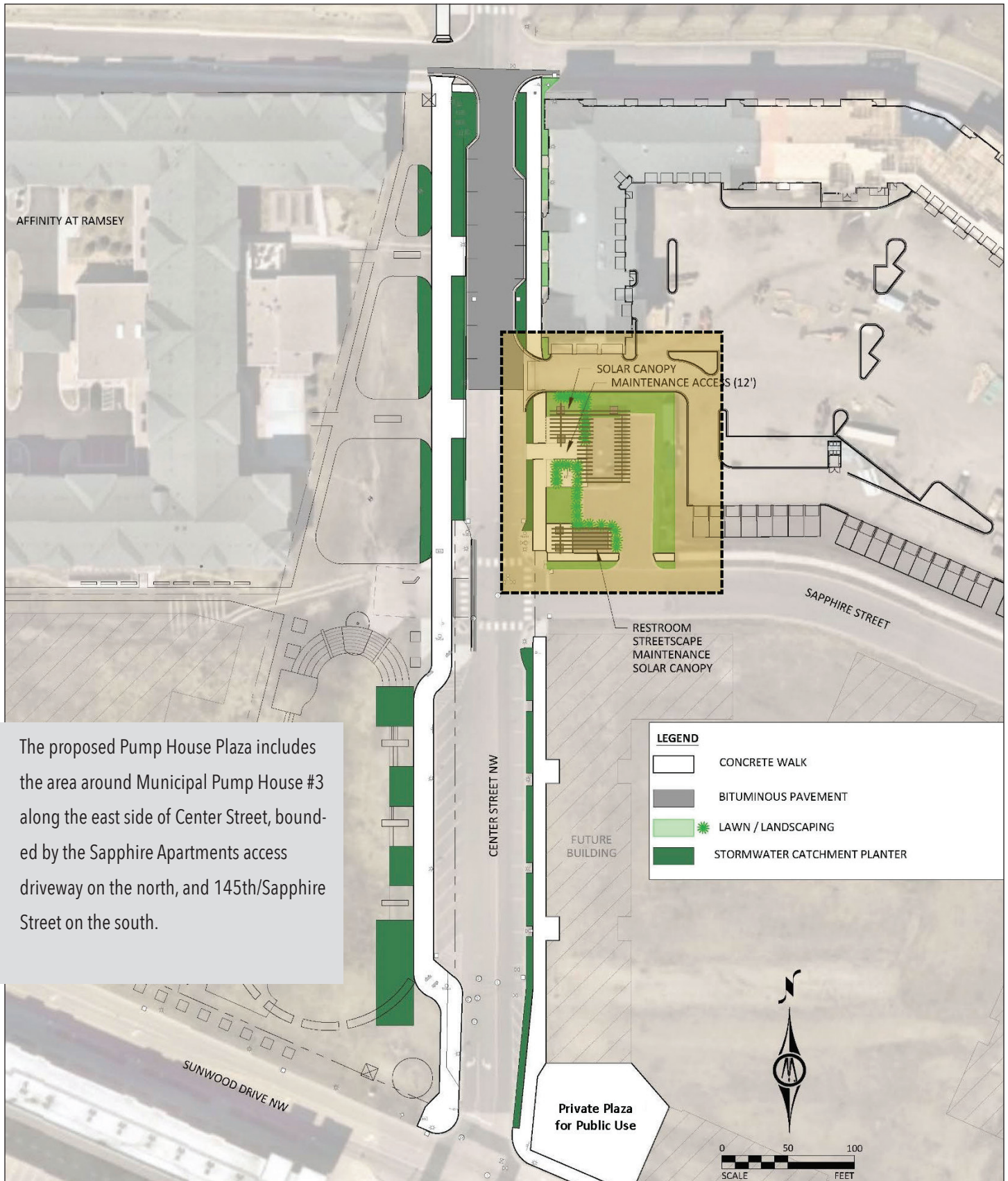


Restroom + Maintenance Bldg Project Elements

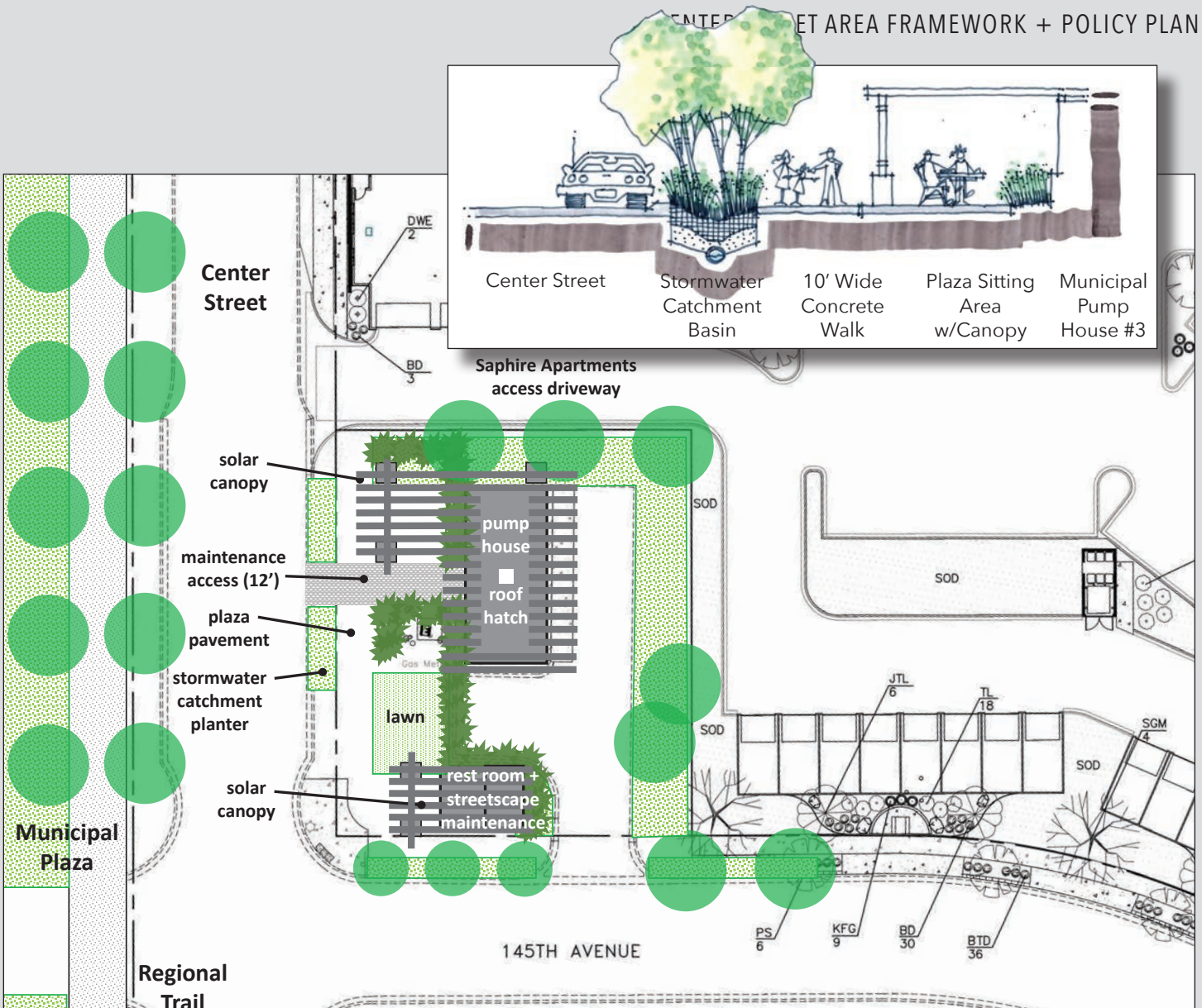


A LED Community Information Board is proposed for the Sapphire Street façade of the Restroom + Maintenance Building. This LED display panel can be used to announce events of community interest, to present the schedule for Municipal Plaza activities, display digital art, etc.(These images shows the capabilities of LED panels.)

Pump House Plaza Location Map



The proposed Pump House Plaza includes the area around Municipal Pump House #3 along the east side of Center Street, bounded by the Sapphire Apartments access driveway on the north, and 145th/Sapphire Street on the south.



Pump House Plaza

PROJECT DESCRIPTION

The Conceptual Site Plan for Pump House Plaza includes the following elements:

- Paved plaza/sitting area along east façade of the Pump House Building;
- Solar canopy/shade structure over the plaza/sitting area;
- Paved (10' minimum width) north/south pedestrian walkway;
- Stormwater catchment basin/planters between pedestrian walk and Center Street;
- Small open lawn/flex space for information/crafts/food tent and other uses;
- Variety of landscape plantings to provide shade, color, screening.

Note: the sequence of project design and construction will begin with Pump House Building facade improvements, followed by Pump House Plaza site and landscape improvements, and then additional project feasibility evaluation and detailed design related to the Rest Room + Maintenance Building.

Preliminary estimate of potential construction cost: \$70,000 - 90,000

The primary objective is to create a series of smaller, pedestrian-scaled public spaces along the east side of Center Street to complement and enhance Municipal Plaza, especially for various events that include the temporary closure of the street. All three of these major elements (Municipal Plaza, Center Street, east frontage public spaces) will seamlessly fit together, including consistent materials, finishes and furnishings to create a unique identity and inviting destination crossroads in the heart of the COR District.

Pump House Plaza Project Elements

Pavement



Concrete plaza with broom finish and tooled joints to create interesting pattern is preferred; color additives, acid tints or etching, and exposed aggregate are other acceptable options.

Furniture



Light weight (moveable) and stackable furniture in a variety of colors is preferred; many choices available, but should consider consistent or complimentary 'family' of furniture for all COR public places.

Stormwater Basins



Curb-cuts on both street side and plaza side of each basin direct surface water flow, but still create a physical/visual separation for pedestrian circulation, snowplowing and other maintenance operations.

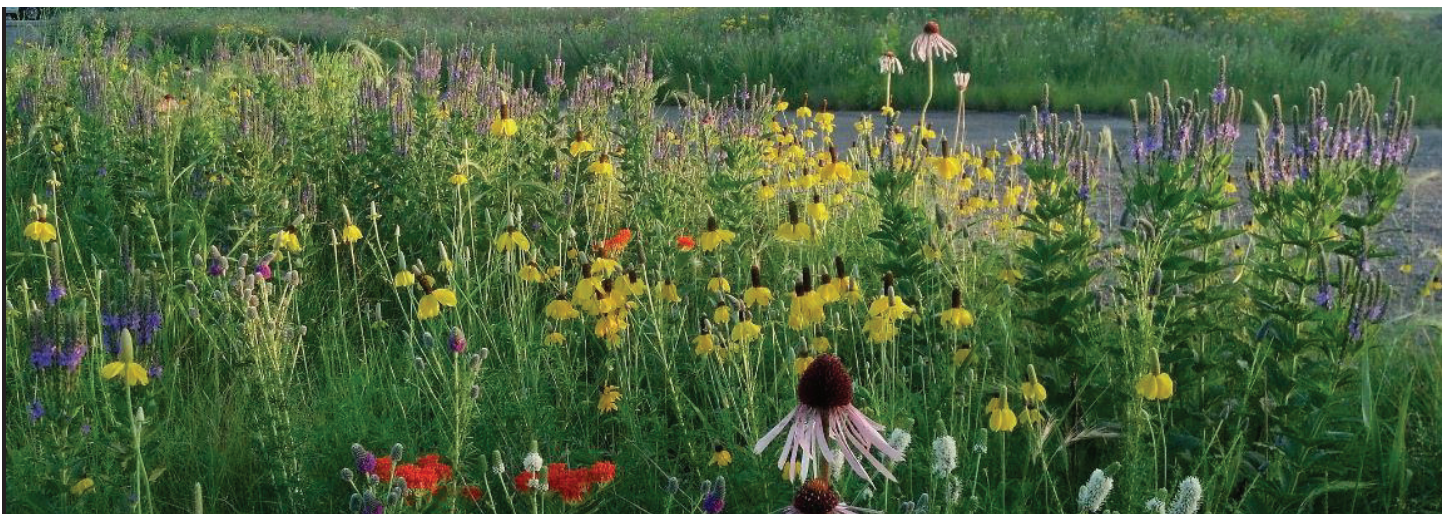
Pump House Plaza Project Elements

Water Feature



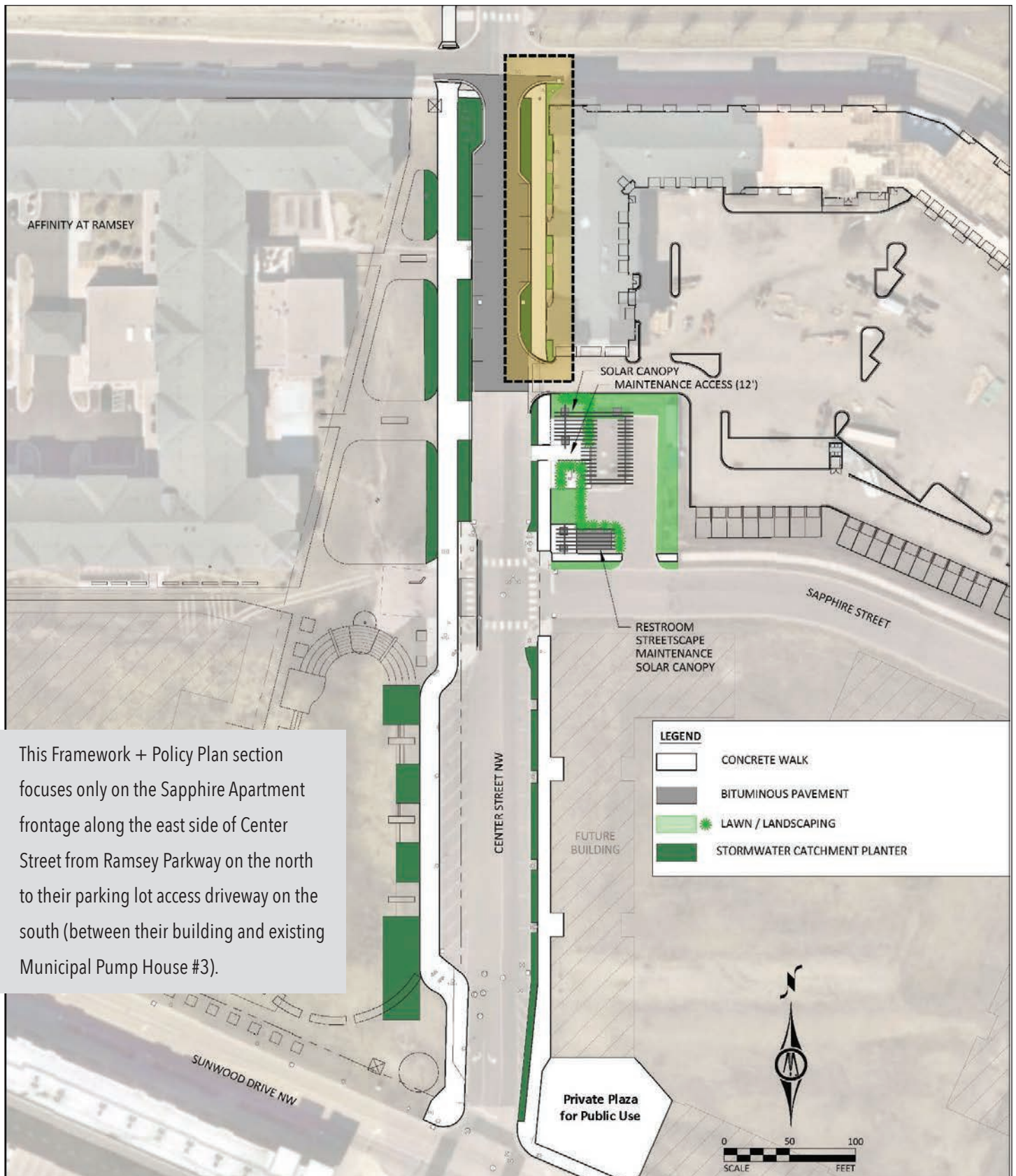
The soothing sound of water can be an important element in the design of public places; small, self-contained water features are preferred, for ease of maintenance, winter shut-down and other considerations.

Landscape

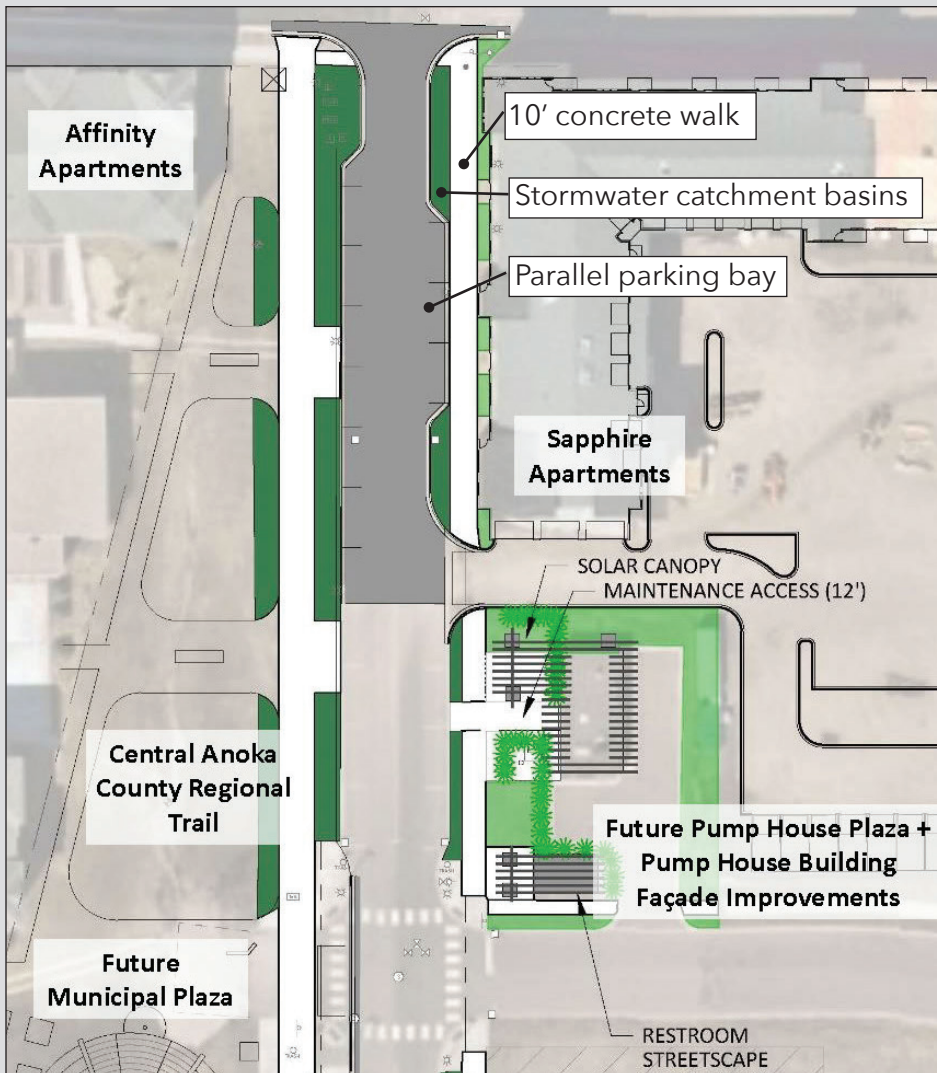


Landscape elements should reflect, or compliment, existing streetscape plantings, and further promote a consistent COR District identity and character; a simple landscape palette provides shade, seasonal color, screening (parking areas/utility boxes), and softens building elevations.

Sapphire Apartment Frontage Location Map



This Framework + Policy Plan section focuses only on the Sapphire Apartment frontage along the east side of Center Street from Ramsey Parkway on the north to their parking lot access driveway on the south (between their building and existing Municipal Pump House #3).



Sapphire Apartment Frontage

PROJECT DESCRIPTION

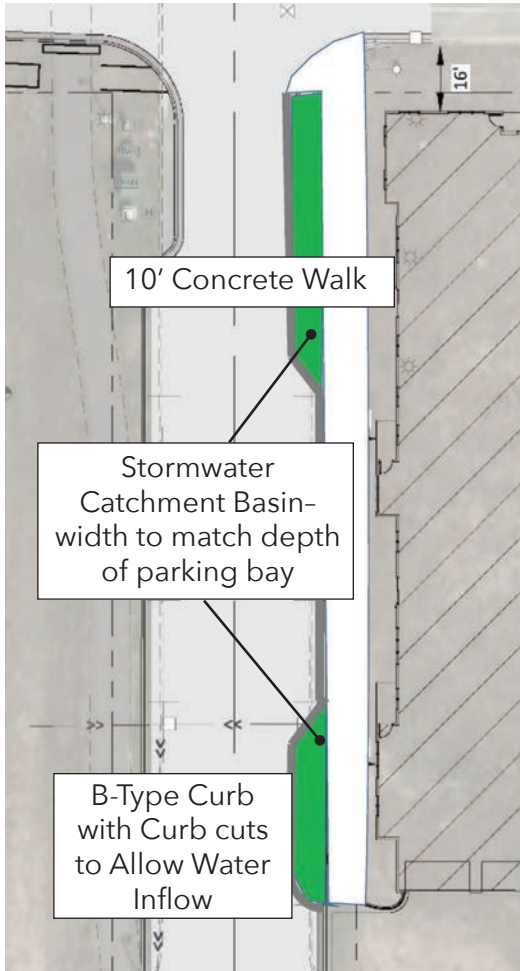
The Concept Site Plan for the Sapphire Apartment Frontage along the east side of Center Street includes three primary elements:

- A 10' wide concrete walk;
- Stormwater catchment basins;
- Parallel parking bay.

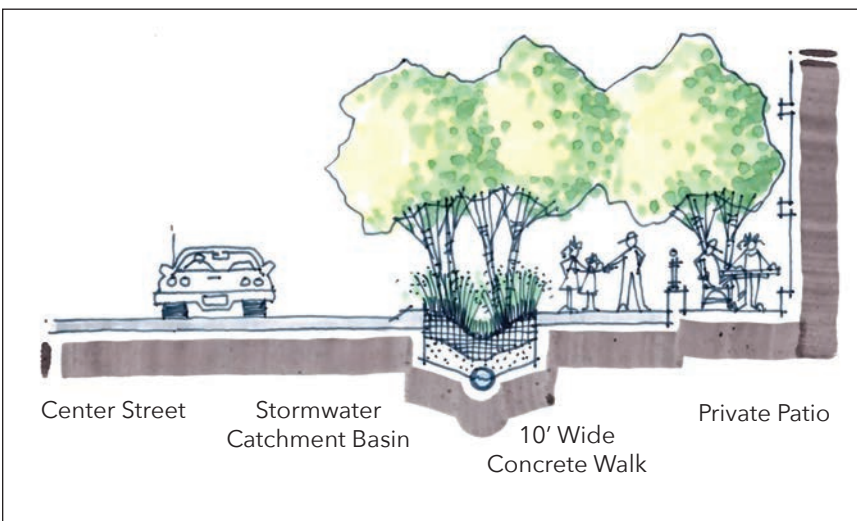
Preliminary estimate of potential construction cost: \$65,000 - 80,000

The Sapphire Apartment Frontage will be the first segment of public (pedestrian) realm to be completed, and as such plays an important role in establishing these elements as the benchmark for future projects along the east side of Center Street. A gracious walkway width (10'), connected stormwater basins, and landscape plantings that complement existing streetscape, will not only further enhance the desired Downtown District street character, but also set design expectations for Pump House Plaza and the now-vacant development site at Sunwood Drive.

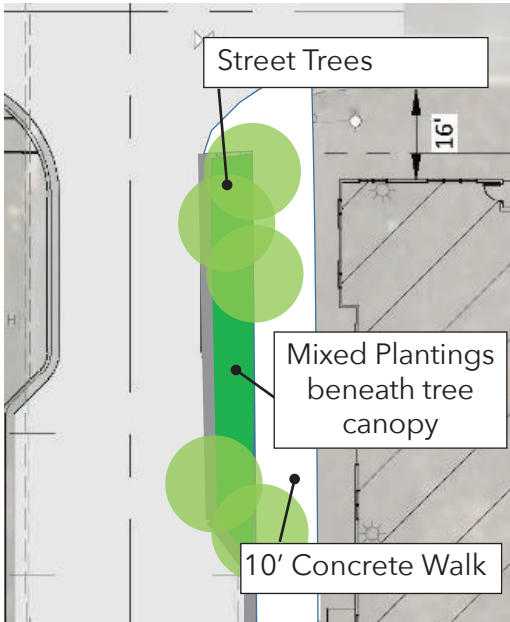
Sapphire Apartment Frontage Project Elements



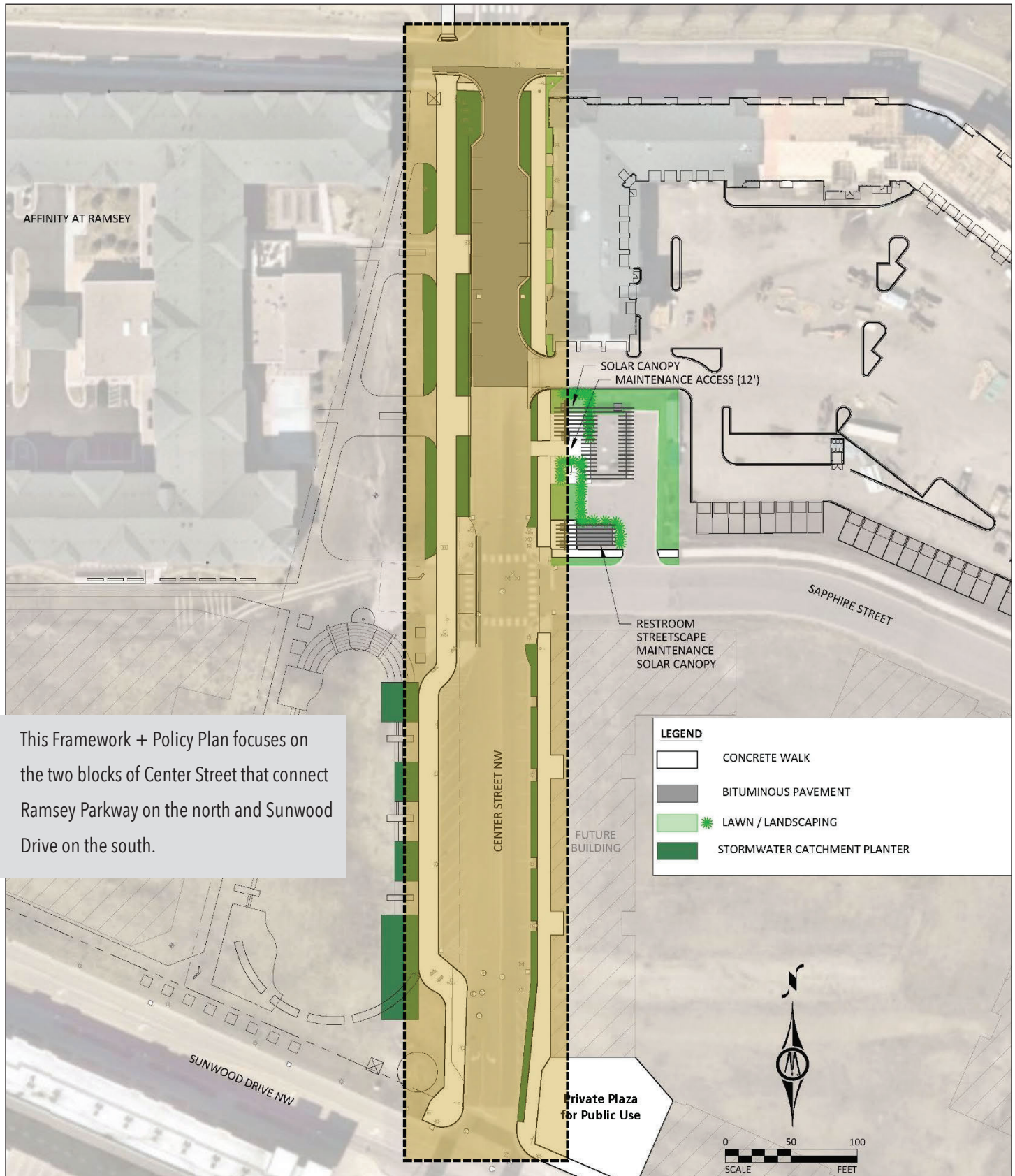
Stormwater catchment basins will be connected to existing storm sewer located in Center Street - future phases will route this water to underground cisterns in Municipal Plaza for irrigation and other re-use purposes.

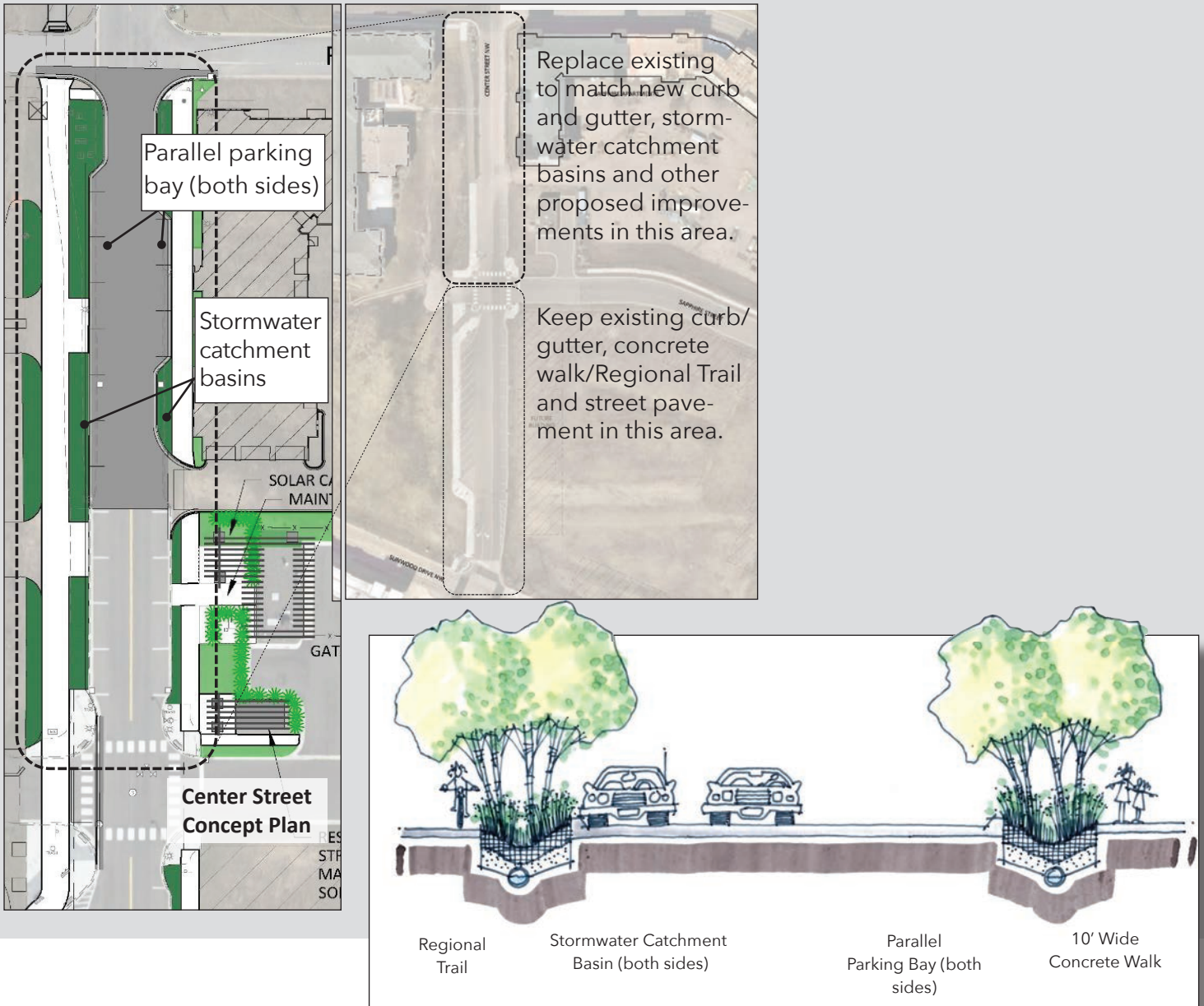


Sapphire Apartment Frontage Project Elements



Center Street Location Map





Center Street

PROJECT DESCRIPTION

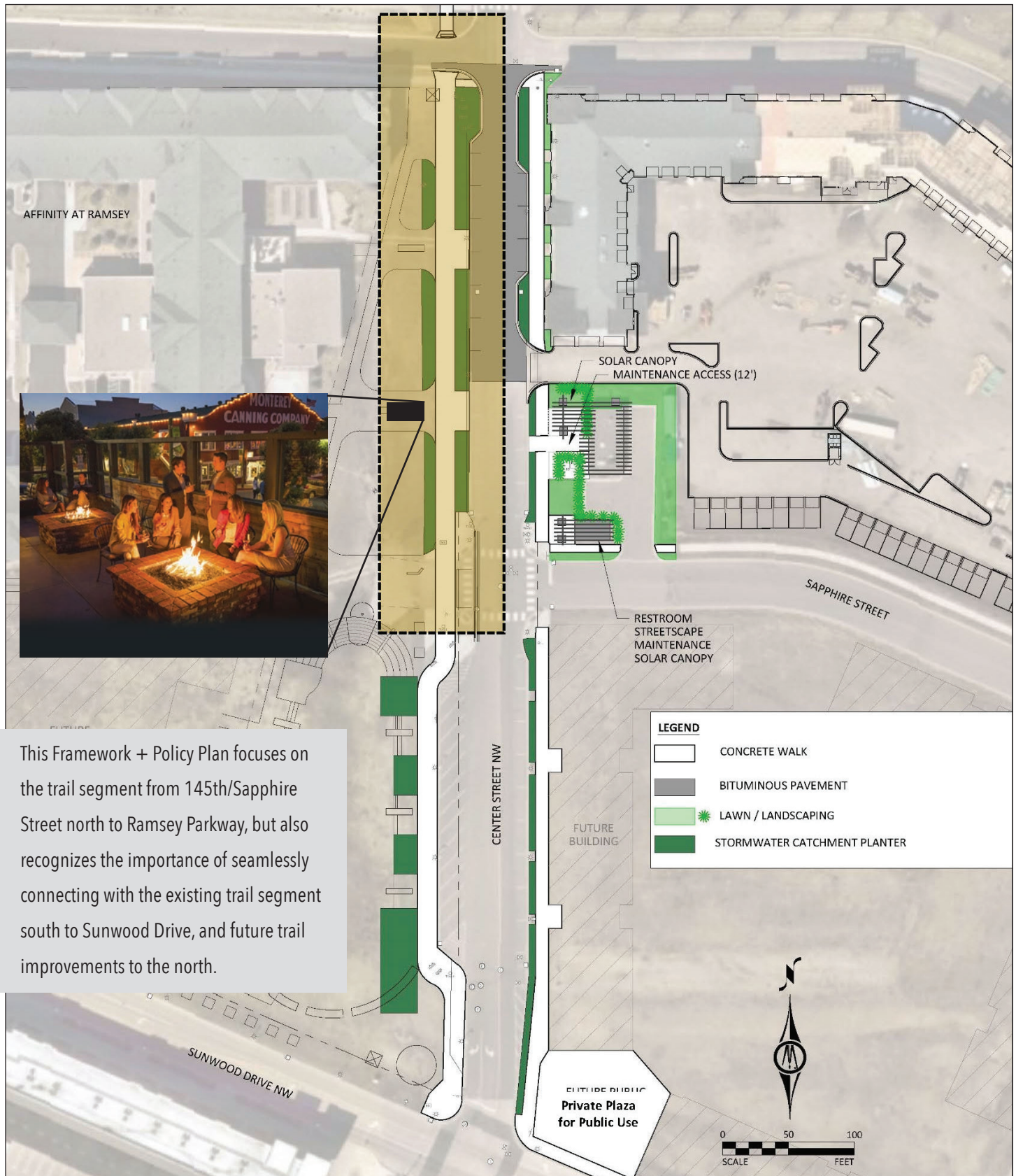
The primary purpose of the Conceptual Site Plan for Center Street is to illustrate how this key north/south connector ties together all the other pieces of this Framework + Policy Plan.

At the same time, it also promotes various opportunities for innovative district infrastructure as an integral part of current and proposed street, park, trail and private development project design – suggesting shared systems for stormwater capture/reuse, energy production/distribution, waste handling/recycling and other technologies.

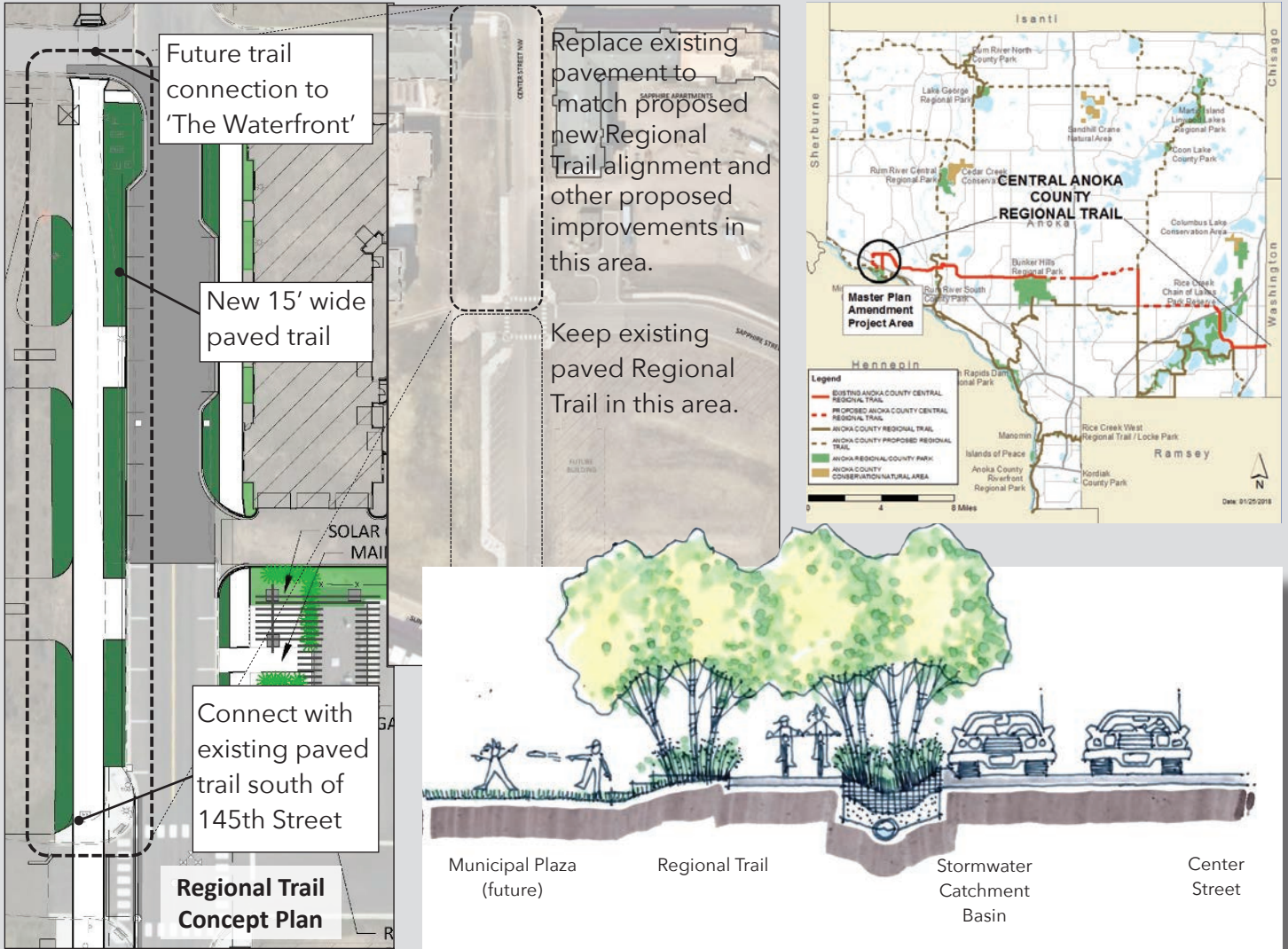
Potential construction cost to be determined as part of other Center Street projects.

It's important to note however, that this document also presents a somewhat cautious approach, seeking to minimize disruption to existing Center Street improvements. The basic objective is to work with existing curb lines, parking bays and other pavement dimensions as the starting point for completing street construction to Ramsey Parkway and northward to Bunker Lake Boulevard. (Also refer to the Sapphire Apartments Frontage and Central Anoka County Regional Trail information in this Plan)

Regional Trail + Municipal Plaza Location Map



This Framework + Policy Plan focuses on the trail segment from 145th/Sapphire Street north to Ramsey Parkway, but also recognizes the importance of seamlessly connecting with the existing trail segment south to Sunwood Drive, and future trail improvements to the north.



Regional Trail + Municipal Plaza

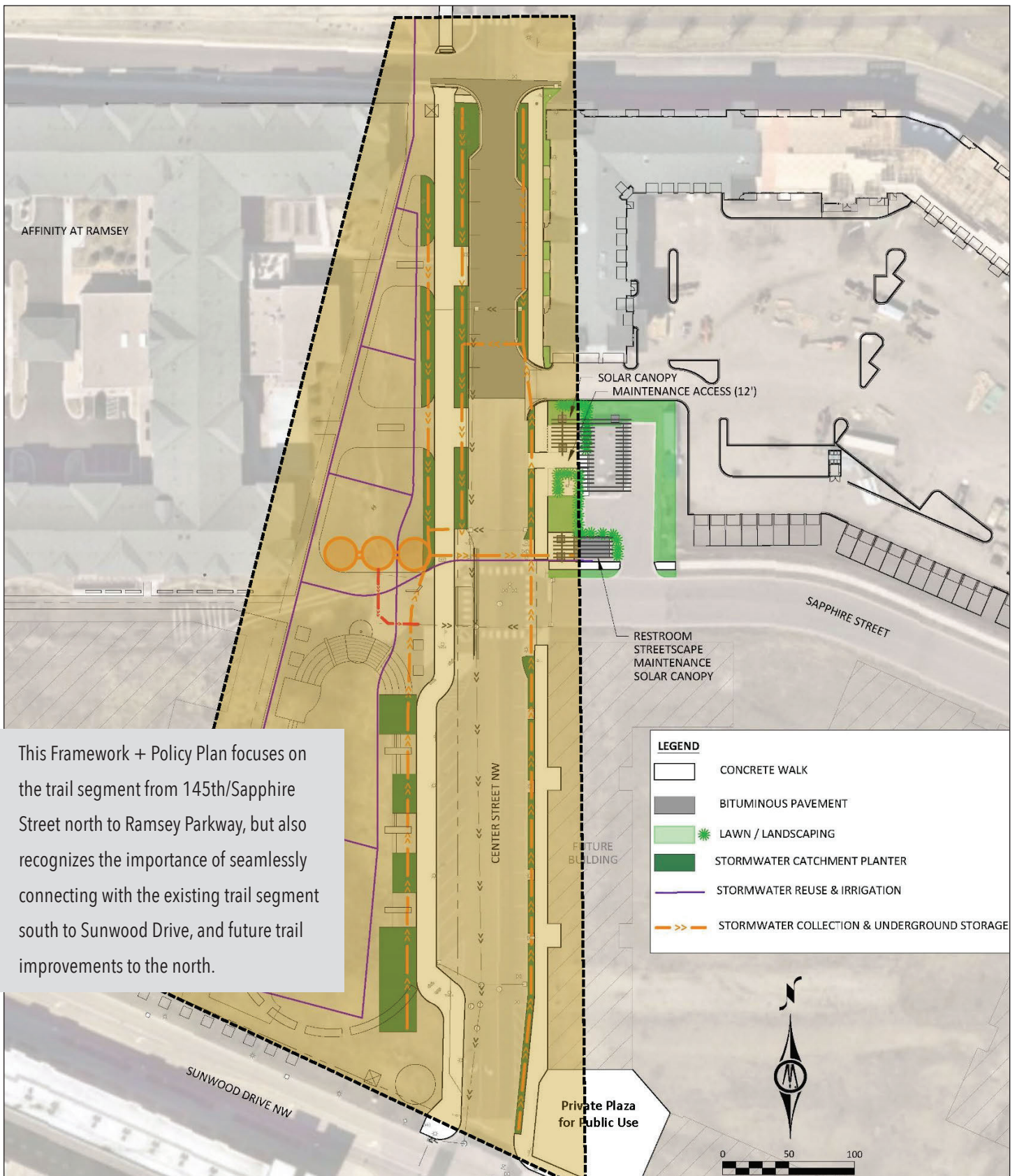
PROJECT DESCRIPTION

The Central Anoka County Regional Trail is a 26 - mile paved regional trail that traverses the middle of the county connecting Mississippi West Regional Park in Ramsey, to Bunker Lake Regional Park, on to the Rice Creek Chain of Lakes Park Reserve in Lino Lakes and Centerville, MN. All but a portion of this Regional Trail in Ham Lake is off-roadway, in place, and in use today, linking residents and trail users to many different destinations and facilities in the county—recreational, business, schools and residential. The trail will be an important connection to the Mississippi River and Mississippi West Regional Park once the remaining portions of the elevated pedestrian crossing over Hwy #10 are funded and the bridge is in place. This pedestrian and bicycle route links not only the isolated Mississippi Recreation District to the other of the 10 districts (and the rest of the city)—but also welcomes all the residents on the South side of Hwy #10, safely to Ramsey's downtown businesses and related recreational opportunities.

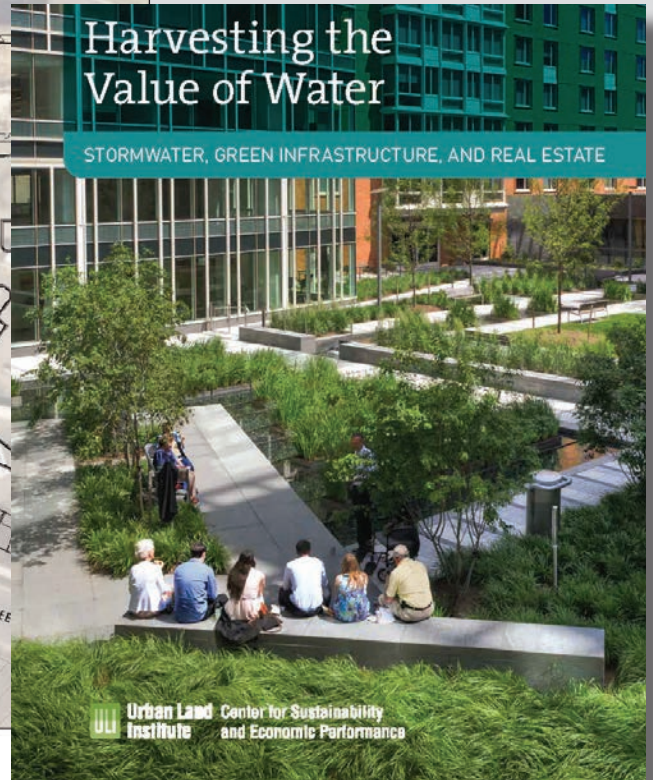
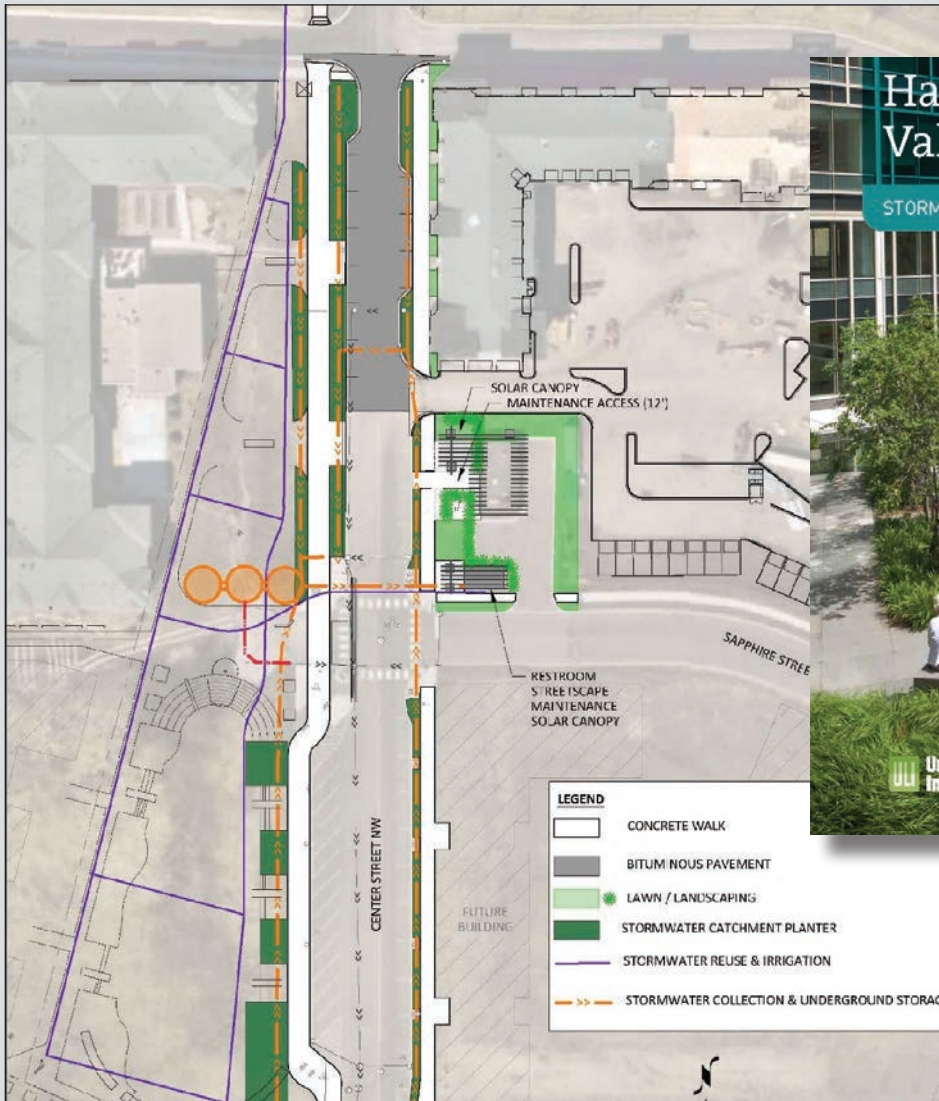
It may be noted, that this trail is an integral part of the Circle of Ramsey Greenway, and accordingly, this downtown area in Ramsey becomes both the nexus of the regional trail system, local trail system, as well as providing non-motorized opportunities in connecting the rest of Ramsey to The COR.

Preliminary estimate of potential construction cost: \$80,000 - 100,000

Stormwater Management System Location Map



This Framework + Policy Plan focuses on the trail segment from 145th/Sapphire Street north to Ramsey Parkway, but also recognizes the importance of seamlessly connecting with the existing trail segment south to Sunwood Drive, and future trail improvements to the north.



Stormwater Management System

PROJECT DESCRIPTION

Bolton & Menk, has completed a preliminary study that analyzes the feasibility of integrating stormwater reuse for irrigation of the proposed Municipal Plaza and adjacent Center Street landscape improvements. The reuse system envisioned will include best management practices to collect and treat runoff, storage tanks to store the runoff, and a pump station and piping to convey reuse water to irrigation areas and surface water features. Most of the stormwater runoff will be generated from Center Street NW and the sidewalks on either side of the road. Stormwater runoff is proposed to be collected and treated by a combination of tree vaults and rain gardens located within the boulevard and parking 'bump-outs' on both sides of the street. It is assumed that the runoff would drain directly into each of the rain gardens and tree vaults through surface capture systems (i.e., curb cuts). Water collected and stored within the storage tanks will discharge via gravity flow to the

pump house wet well, where it will be filtered and treated. The Metropolitan Council Stormwater Reuse Guide Water Balance Tool for Constant Irrigation Demand was used to determine the percent of time the stormwater reuse system would be functioning, and assumed 1" of water over the irrigable area per week is required for adequate vegetation growth and health. Preliminary estimate of potential construction cost: \$800,000 - 950,000

Stormwater Management System Project Elements

SOURCE Site Data			
Impervious Area	83,417	square feet	
Impervious Area	1.91	acres	
USE Demand Data			
Irrigation Area	57,206	square feet	
Irrigation Area	1.31	acres	
Irrigation Season	13	39	week number
Storage Data			
Storage Provided	150,000	gallons	
Total Storage Required	454,037	gallons	%
Weeks that Flows Must be Augmented	85	weeks	10.2%
Weeks that Reuse System will Meet Demand	752	weeks	89.8%

Exhibit 1: Excerpt from the Met Council Reuse Water Balance Tool.



Exhibit 2: Example concrete vault storage tank (Storm Trap).



Exhibit 3: Example underground fiberglass storage tanks (Xerxes).

Exhibit 1 summarizes the preliminary reuse system sizing. According to the reuse calculator, there will be enough captured stormwater water in approximately 90% of the pumping season weeks to apply 1-inch per week of water over the area to be irrigated (a total of 454,000 gallons required). Since rainfall contributes to that total, as well as stored water in that tank, the total required volume per year does not translate to the size of the tank required. Instead, the volume of the tank can be modified to target an irrigation efficiency.

The storage volume could be a single concrete vault or multiple fiberglass underground units. Given the size of individual cisterns, plastic is not available. Fiberglass tanks generally are available up to 50,000 gallons in size. A 50,000-gallon tank has dimensions of approximately 68 feet long with a 12-foot diameter. Three tanks, with space between, could fill a footprint of approximately 5,000 square feet. Concrete vaults can be sized to fit multiple footprint sizes.

Stormwater Management System Project Elements

Rainfall Event	Rainfall Depth (IN)	Runoff Volume (CF)	Runoff Volume (GA)
1" Event	1.00	5,500	41,140
1-Year	2.46	14,700	109,956
2-Year	2.86	17,600	131,648
5-Year	3.58	22,700	169,796
10-Year	4.26	27,500	205,700

Table 1: Summary of expected runoff volumes.

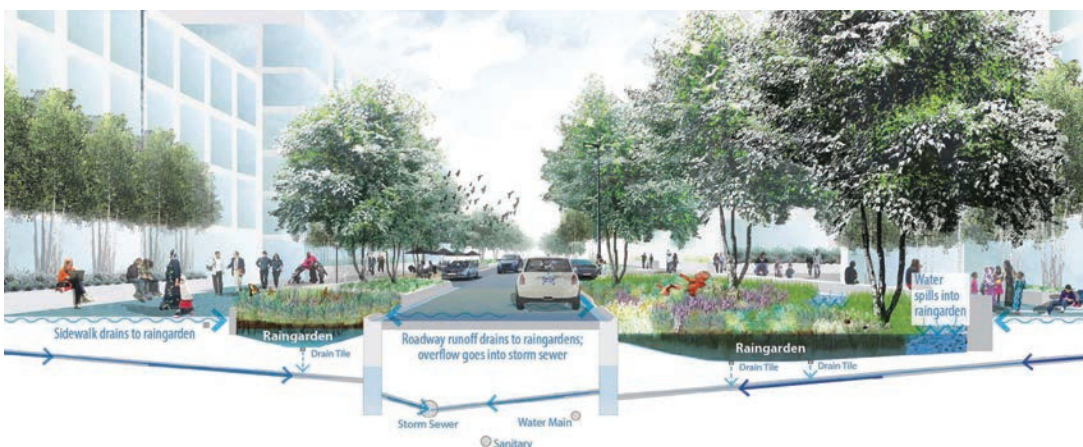
Additional Considerations

A simplified estimate of stormwater runoff expected from the street section was assembled to ensure the watershed can produce enough runoff to adequately fill the tanks. Table 1 is a summary of expected runoff volumes for a variety of events. A rainfall event producing between 3.6 and 4.3 inches in 24 hours (5-year to 10-year return interval) can completely fill the tank.

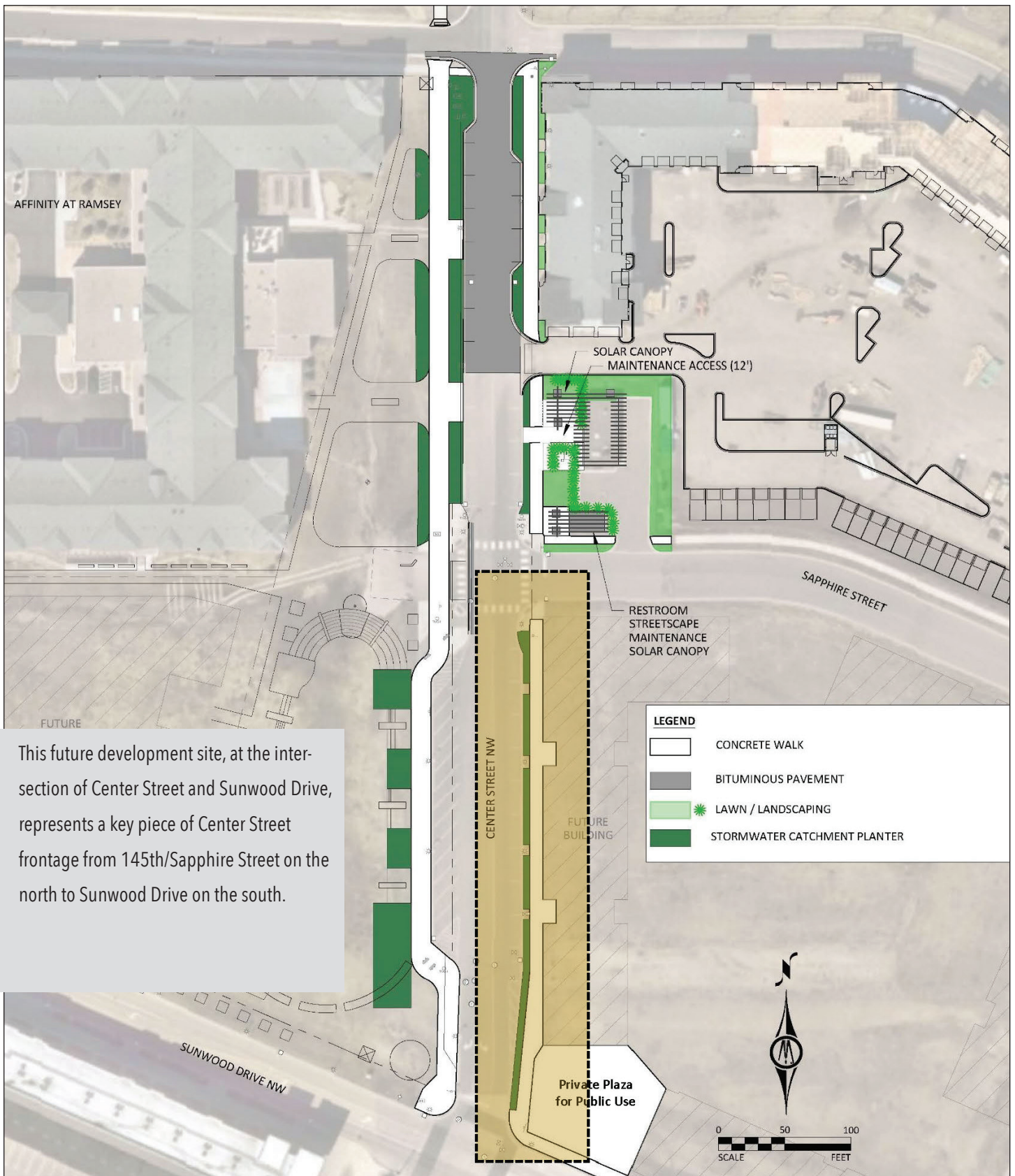
During final design, additional hydraulic information will be provided to provide an effective high flow bypass system. In other words, if the tank is full and the City experiences a 100-year rainfall event, there will be adequate bypass capacity to reduce local

flooding and protect the tank(s). Also, since the COR trunk storm sewer system is already designed to accommodate a 100-year rainfall event with discharge to regional flood storage areas, the bypass system will effectively mimic the originally designed scenario. In between events when irrigation is occurring, or the tanks is being filled, there will be hydraulic relief on the trunk system and volume control provided.

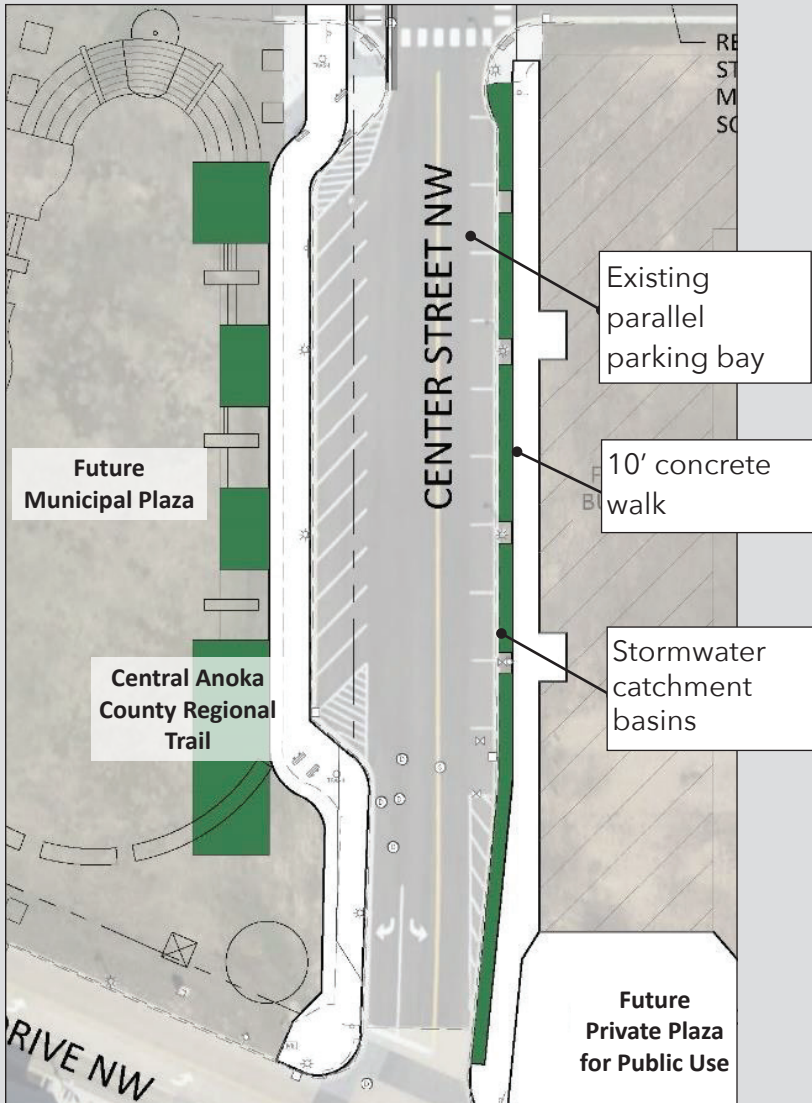
Since the reuse water will pass through a filter and potentially other treatment methods, the impact to groundwater resources through the infiltration of irrigation water is expected to be very minimal. Impacts to groundwater resources in the Downtown District are not anticipated.



Future Development Site Frontage Location Map



This future development site, at the intersection of Center Street and Sunwood Drive, represents a key piece of Center Street frontage from 145th/Sapphire Street on the north to Sunwood Drive on the south.



Future Development Site Frontage

PROJECT DESCRIPTION

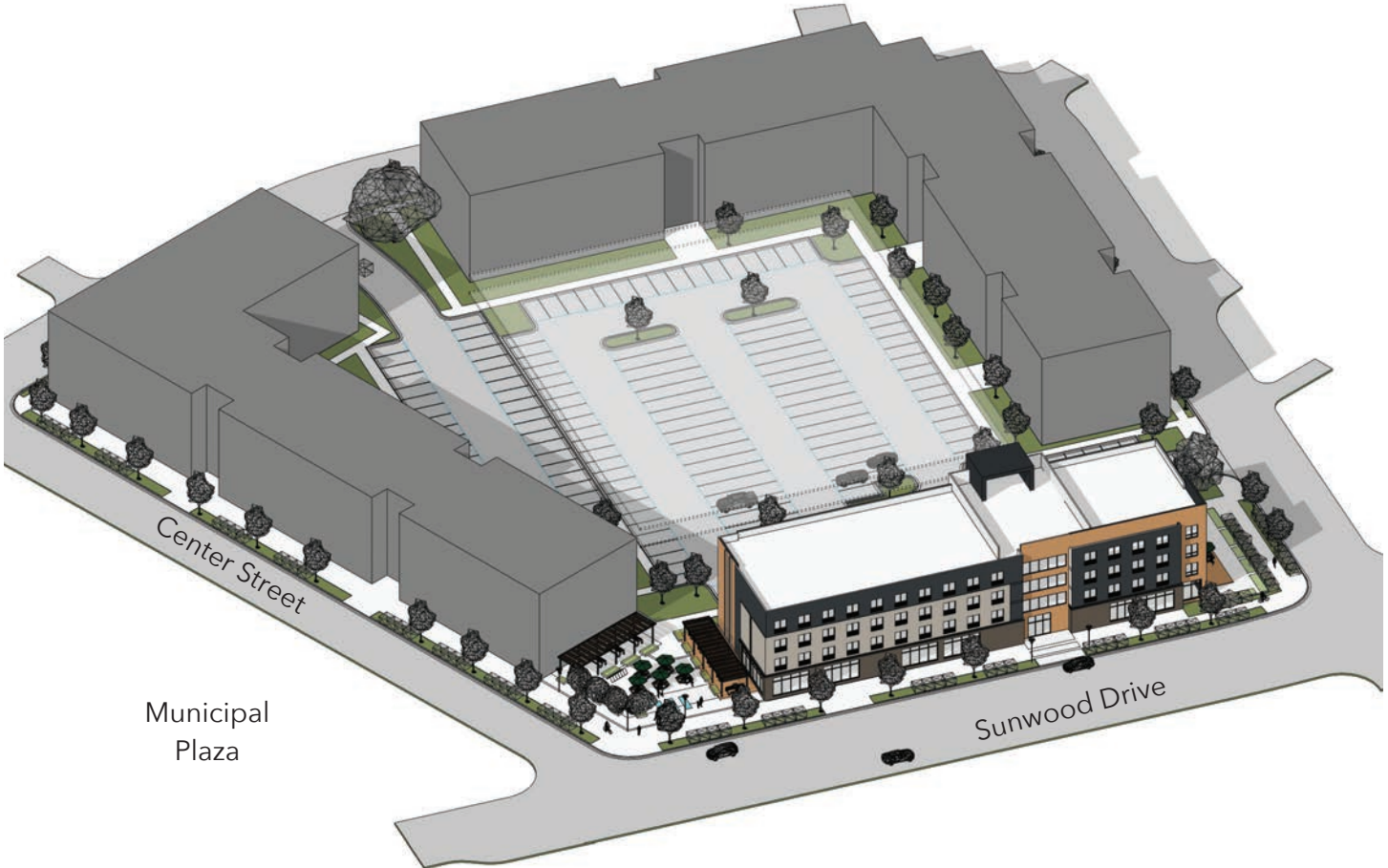
The Concept Site Plan for the Future Development Site Frontage along the east side of Center Street matches recommendations for Sapphire Apartment Frontage and Pump House Plaza to the north, including three primary elements:

- A 10' wide concrete walk;
- Stormwater catchment basins;
- Parallel parking bay.

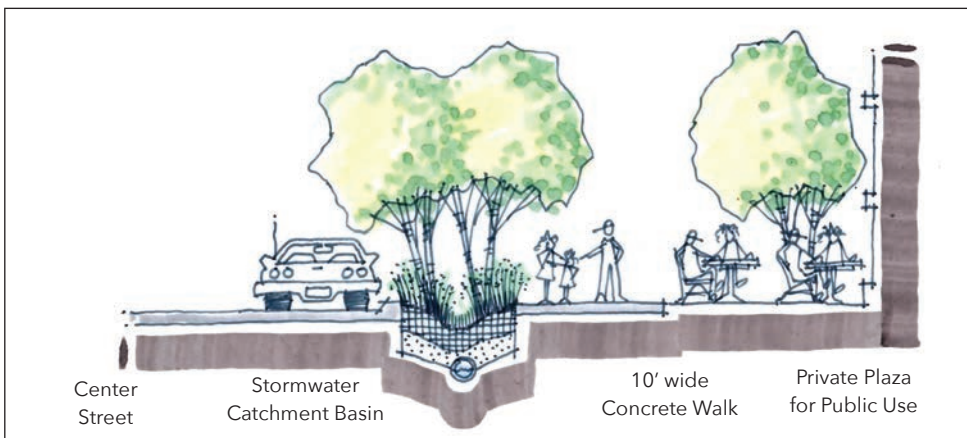
Preliminary estimate of potential construction cost: \$80,000 - 100,000

Following in the footsteps of the Sapphire Apartment project, future development of this site will also include a gracious pedestrian walkway (10' width), connected stormwater basins, and landscape plantings that complement existing streetscape, further enhancing the desired Downtown District street character as a welcoming local and regional destination.

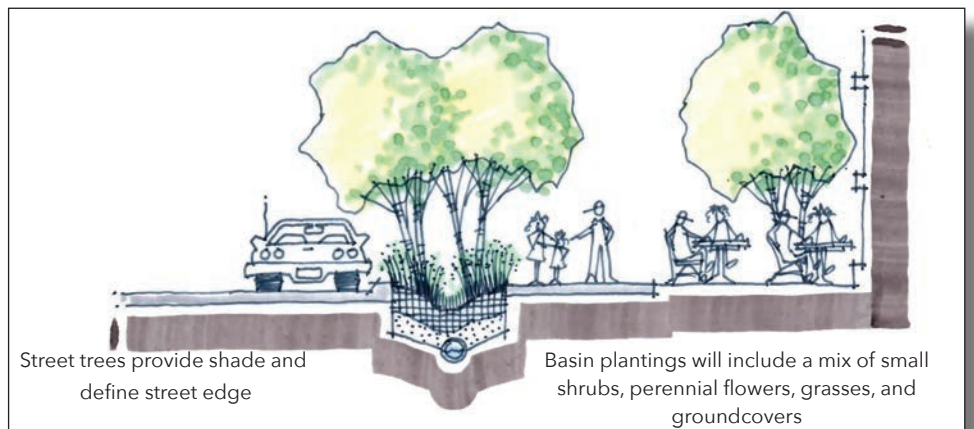
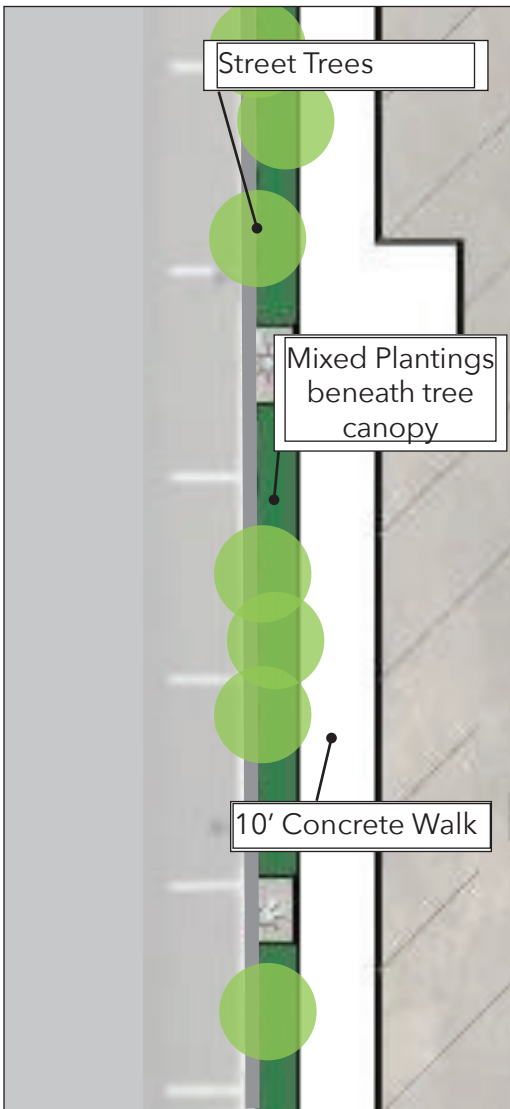
Future Development Site Frontage Project Elements



Stormwater catchment basins will be connected to existing storm sewer located in Center Street - future phases will route this water to underground cisterns in Municipal Plaza for irrigation and other re-use purposes.



Future Development Site Frontage Project Elements





Design Framework



February 28, 2012

Contents

Design Framework – The COR.....	1
Purpose	1
Application	1
Implied Responsibility	1
Part One: Overall Framework	2
Overall Framework - Context.....	3
Overview	3
Guideline Recommendations.....	3
Objectives.....	3
Overall Framework – Street Hierarchy	5
Overview	5
Guideline Recommendations.....	5
Objectives.....	5
Transit Overview	8
Transit Guideline Recommendations.....	8
Transit Objectives	9
Overall Framework - Streetscape	10
Overview	10
Guideline Recommendations.....	12
Objectives.....	12
Street Trees and Plantings	12
Street Furniture.....	14
Sidewalks and Boulevard	14
Street Lights	15
Standards for Outdoor Dining.....	15
Overall Framework - Parking	16
Overview	16
Guideline Recommendations.....	16
Objectives.....	17

Minimum Standards.....	20
Private Streets.....	24
Loading Areas.....	24
Overall Framework – Building Design.....	25
Overview.....	25
Guideline Recommendations.....	25
Objectives.....	25
Architectural Overview.....	26
Architecture Recommendations.....	26
Overall Framework - Signage.....	29
Overview.....	29
Guideline Recommendations.....	29
Objectives.....	29
Definitions.....	29
Standards.....	32
Overall Framework – Stormwater Management.....	42
Overview.....	42
Objectives.....	42
Guideline Recommendations.....	42
Overall Framework – Parks/Public Spaces.....	44
Overview.....	44
Guideline Recommendations.....	44
Objectives.....	44
Part Two: Sub-district Framework.....	45
Overview.....	45
Public Realm/Streetscape.....	45
Plazas.....	45
Architecture.....	46
Minimum Standards.....	46
Sub-District Framework - COR1 Mixed Use Core.....	51
Public Realm / Streetscape.....	51
Site Development.....	51

Architecture	51
Sub-District Framework - COR2 Commercial	53
Public Realm / Streetscape	53
Site Development.....	53
Architecture	53
Sub-District Framework - COR3 Workplace	56
Public Realm/Streetscape	56
Site Development.....	56
Architecture	56
Sub-District Framework - COR4 Neighborhood	58
Public Realm/Streetscape	58
Site Development.....	58
Architecture	58
Sub-District Framework - COR5 Parks & Open Space	60
Public Realm/Streetscape	60
Site Development.....	60
Architecture	60
Implementation	62
Introduction	62
Design Review	62

List of Figures

Figure 1: Development Master Plan 5.03	4
Figure 2: Street Hierarchy Map.....	7
Figure 3: Master Lighting Plan	11
Figure 4: Streetscape Master Plan	18
Figure 5: Parking District Map.....	19
Figure 6: Signage Plan	31
Figure 7: Sub-district Map.....	50

Appendix A: Specifications and Details

Design Framework – The COR

Purpose

The primary purpose of Design Framework is to set basic parameters, describe preferences and illustrate design intent. These framework standards serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

Application

The format and content of this framework are specifically tailored for use as a supplement to the Zoning Ordinance. This document is organized into two sections. The first section outlines issues and recommendations that apply to the entire COR. The second section highlights specific framework that apply to each sub-district within The COR. The City of Ramsey has adopted Development Plan 5.03 as the Master Plan for The COR. This master plan provides illustrative guidance for building locations and orientation. Any changes to this approved master plan must be reviewed and approved by the City Council as part of the development application. This Development Plan will be updated by the City as changes are approved.

Implied Responsibility

All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

Part One: Overall Framework

There are a number of framework standards that apply to the entire COR, including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall COR framework standards overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Framework:

- Context – local, city, regional
- Street Hierarchy--access, circulation, arrangement
- Streetscape—preferred design, location
- Parking -- quantity, location, type
- Building Design-- preferred uses, horizontal/vertical mixed use, built form, character of development
- Signage—design, location, quantity, type
- Stormwater Management-- surface water features, stormwater management
- Parks/Public Spaces – parks, trails and open space

It is essential that proposed design solutions for development projects and other improvements within The COR demonstrate an understanding of the interplay between these layers.

Overall Framework - Context

Overview

The COR is a unique area within the City; but it is a part of, and connected with, a variety of local, city-wide and regional systems (See Figure 1: Development Master Plan 5.03). Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

Guideline Recommendations

To ensure that The COR takes full advantage of local and regional systems, development should:

- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/stormwater management plan

Objectives

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

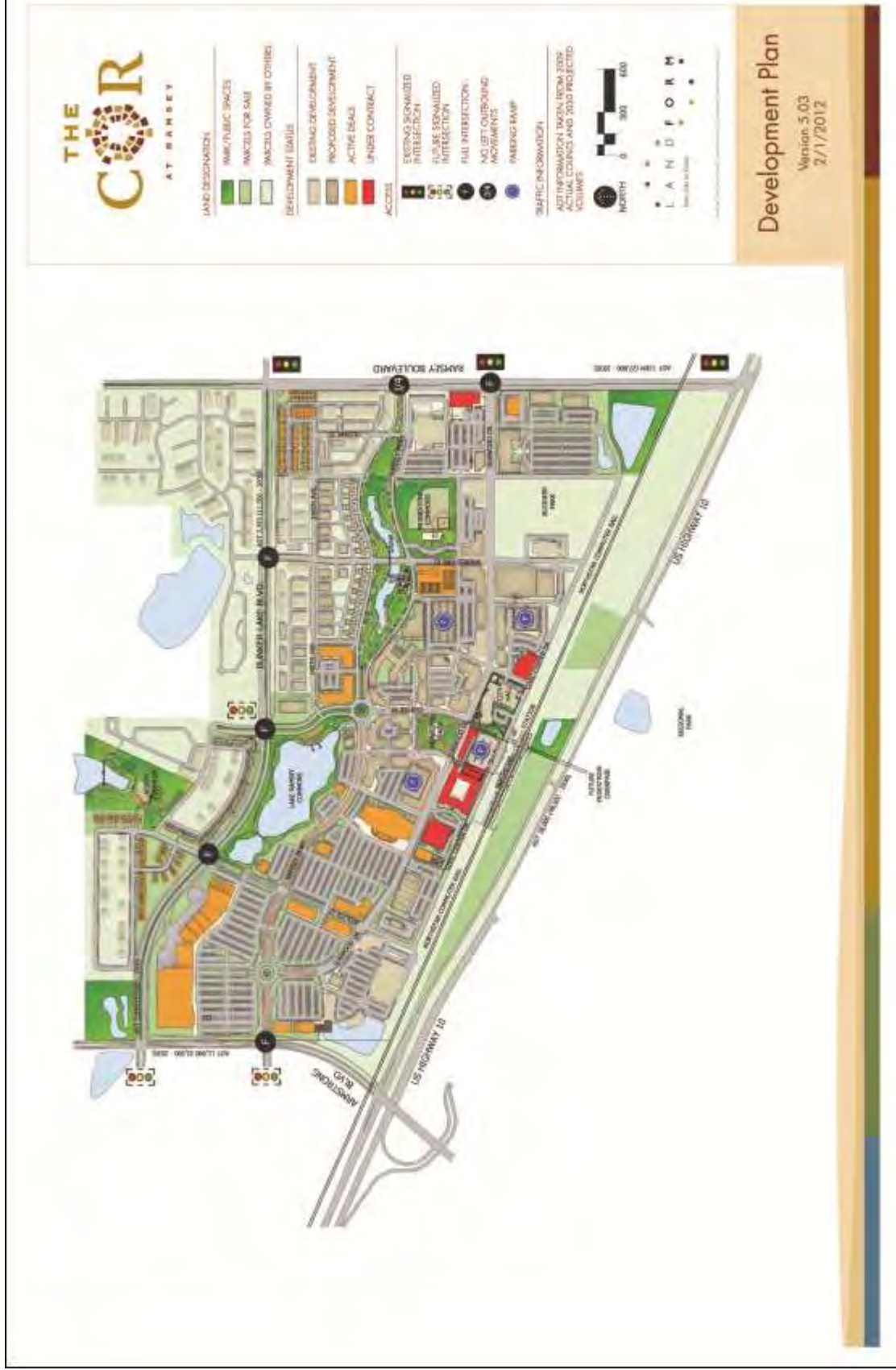


Figure 1: Development Master Plan 5.03

Overall Framework – Street Hierarchy

Overview

The Development Master Plan defines a specific strategy for the layout of streets and blocks within, and around The COR. The size, type and configuration of this street hierarchy (See Figure 2: Street Hierarchy) is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample space for street-side activities, strolling promenades, pockets of green and other amenities is critical to the success of The COR. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as a primary way to move about and conduct business in the core area.

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in The COR and encourages a more dispersed and less congested traffic flow (many choices or travel paths).
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout The COR.
- Work with Anoka County to convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches.
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street).



Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Arterial street – these streets provide the boundary of The COR and all access into The COR will be from one of these three streets:
 - Bunker Lake Boulevard
 - Armstrong Boulevard
 - Ramsey Boulevard
- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities
 - Sunwood Drive



- Parkway – premier ‘green streets’ adjacent to primary public park / green space, with more landscaping than other streets in The COR
 - Ramsey Parkway
 - Center Street (north of the roundabout)
- Connector street – primary routes that link various districts within The COR, usually providing the most direct means of getting from point A to point B
 - Rhinestone Street
 - Zeolite Street
 - Veterans Drive
- Downtown street – the most urban of street sections where high pedestrian traffic is intended and traffic calming measures such as on street parking, bump outs and other methods will be employed
 - Center Street (south of the roundabout)
 - Sapphire Street (north of Sunwood)
 - Other streets as shown on the Street Hierarchy Map
- Local street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout The COR.



These street hierarchy classifications are intended to provide guidance regarding the streetscape, but it should be noted that within a street classification, there may be construction or design distinctions, based on the sub-district classification. For example, Sunwood Drive is defined as a destination street throughout The COR, but the design may be different between the COR1 and COR2 sub-districts. The Master Streetscape Map will provide design direction.

Transit Overview

The COR is designed to be a Transit Oriented Development (TOD) in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multimodal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. The COR is the Twin Cities' first and only transit-oriented development along the new Northstar Commuter Rail Line. Ramsey Station at The COR will open for riders boarding the train in November 2012. As Northstar's most visible station, The COR will immediately bolster the ridership on the line and continue to build recognition for this regional service.



Transit components include the following:

- Ramsey Star Express service provides peak period, peak direction bus service between downtown Minneapolis and Ramsey until the Northstar Commuter Rail station stop is complete
- Northstar Commuter Rail provides train service between Big Lake and downtown Minneapolis
- 350 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within The COR and into the surrounding neighborhoods
- The COR can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station



The COR also encourages developers to participate in a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. The TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions.

- Developers shall submit a TDM plan with their site plan application for new development and work with the City to implement their plans.

Transit Guideline Recommendations

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in The COR, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following framework:

- Understand and take advantage of opportunities to use existing and proposed transit components

- Provide transit related facilities specifically linked to uses and character within each COR district, including signage and lighting for way-finding and bicycle amenities
- Building future Northstar Commuter Rail riders through the Ramsey Star Express bus service

Transit Objectives

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. The framework for The COR promotes public transportation as part of the solution and suggests a variety of street improvements and other amenities to invite increased use. Photographs throughout the framework suggest some of the many possibilities.

Overall Framework - Streetscape

Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within and around The COR. The amenities within the public right-of-way -and immediately adjacent -play an important role in the aesthetics of The COR. Great streets are essential components of an attractive and inviting public realm. Developing a streetscape that is comfortable and inviting to pedestrians is important to realizing the vision for The COR. The framework will encourage interesting and inviting places for the public.

The Streetscape framework defines the visual character and physical improvements for all public spaces within The COR. The document also provides the general framework and examples for design of proposed quasi-public and private space located within each development parcel.

Public realm/streetscape framework focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space



The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. The City has developed a series of street cross-sections to illustrate these design expectations. Typical street sections are provided in Appendix A as a reference.

Destination and Connector streets must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)

The COR includes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.



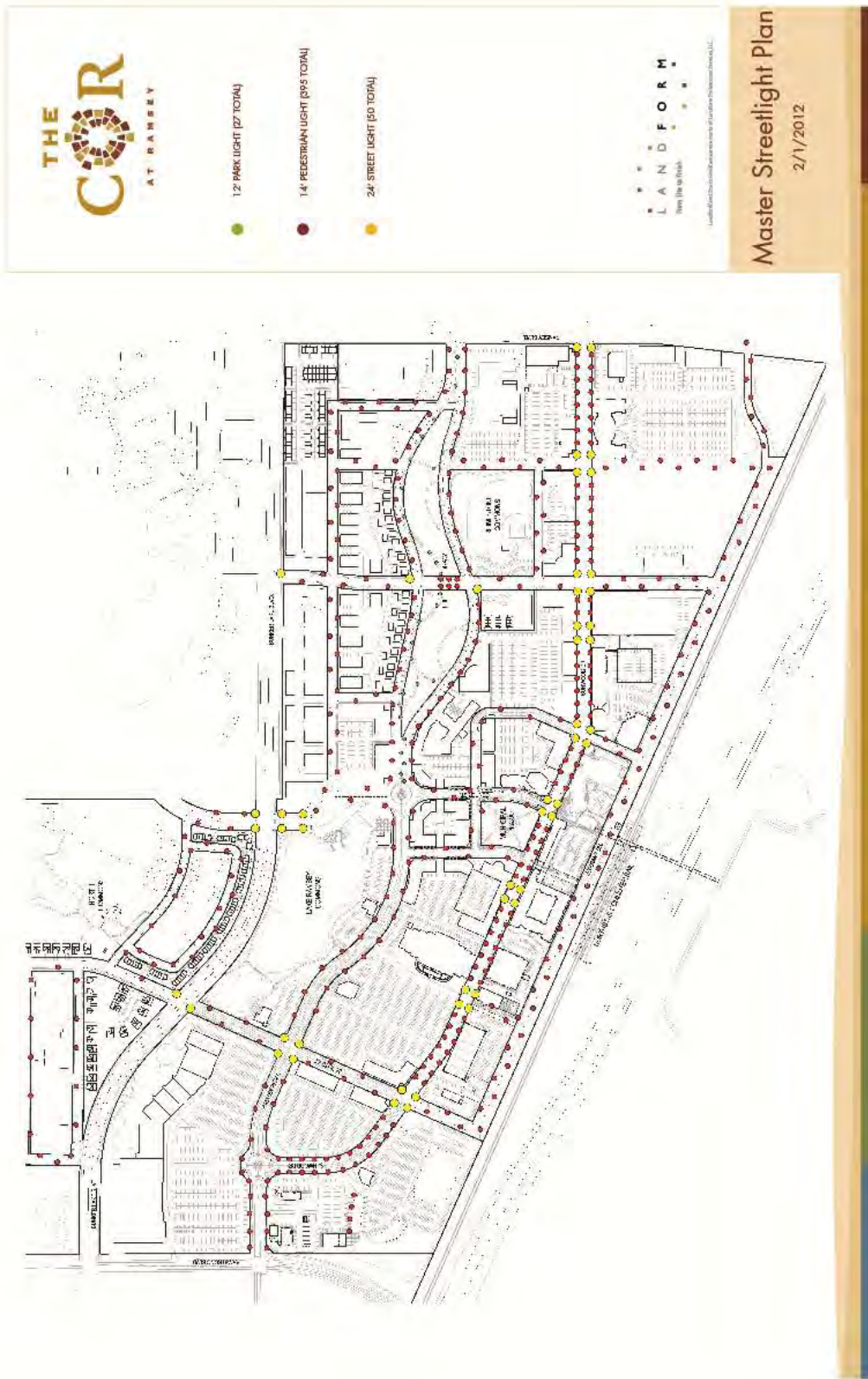


Figure 3: Master Lighting Plan

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, including the following:

- Bike racks interspersed throughout The COR to encourage cycling as an alternative mode of transportation.
- Benches on the public sidewalks to encourage people to linger in The COR
- Trees and planters along the streets to create an inviting space
- Wide sidewalks and both on- and off-street bicycle trails
- Kiosks and other way finding tools for visitors and residents in The COR
- Outdoor dining is encouraged on the street side of buildings within all districts.
- Public plaza space is strongly encouraged to be located adjacent to the streets



Objectives

Creative design solutions for streetscape improvements should include:

- Streetscape improvements that provide traffic calming, particularly along Destination and Downtown streets
- Inviting streets that draw you into and through The COR
- A marriage of building design and streetscape design that encourages active spaces near the streets



Street Trees and Plantings

Street trees and planter boxes are a critical element in the unique character of The COR and will be used to help distinguish between different street types and sub-districts within The COR. However, there may be variety within the same street when the street crosses into sub-districts.

- Street trees will be chosen to ensure that the species allow for clear pedestrian access along the streets and require minimal maintenance.
- Along Destination, Downtown, Parkway and Local streets, where regularly spaced blocks exist, one (1) tree species shall be provided per block. In order to provide variety and protect against disease, adjacent blocks shall not be allowed the same street tree. Initial development along a block shall establish the tree species for that block, subject to City review and approval.
- Along streets without regular block space or longer blocks, such as Connector and Arterial streets, the City may approve groupings of 3-6 trees of one (1) species in lieu of the regular block spacing.
- Alternatives to the above mentioned planting plan shall be subject to review and approval by the City Council.

Destination Street Trees:

- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Bicolor Oak (*Quercus bicolor*)
- Sienna Glenn Maple (*Acer x freemanii* 'Sienna')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Ginko (*Ginkgo biloba*)
- Accolade Elm (*Ulmus x 'Accolade'*)

Parkway Street Trees:

- Valley Forge Elm (*Ulmus americana* 'Valley Forge')
- Bitternut Hickory (*Carya cordiformis*)
- Siouxlant Poplar (*Populus deltoides* 'Siouxlant')
- Sienna Glen Maple (*Acer x freemanii* 'Sienna')
- Boulevard Linden (*Tilia americana* 'Boulevard')
- Northern Catalpa (*Catalpa speciosa*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Korean Mountain Ash (*Sorbus alnifolia*)
- Ohio Buckeye (*Aesculus glabra*)
- Ginkgo (*Ginkgo biloba*)

Connector Street Trees:

- Bicolor Oak (*Quercus bicolor*)
- Northern Red Oak (*Quercus rubra*)
- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Downtown Street Trees:

- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Local and Arterial Street Trees:

Local and Arterial Street trees may be chosen from any species on the above lists.

Planter Beds/Boxes/Tree Grates

- Planter beds, tree grates and planters shall be as shown on the Streetscape Master Plan. Details and specification shall be added to Appendix A as developed and approved by the City. Appendix A shall be amended as needed.
- Irrigation shall be provided to ensure survival of plant materials. The City shall provide irrigation systems along the Parkway and Destination streets. Unless otherwise approved, all other irrigation shall be the responsibility of the landowner.

Street Furniture

Bike racks, trash enclosures and benches are encouraged on all streets, but will be required on Destination, Connector and Parkways and other streets as shown conceptually on the Streetscape Master Plan.

Bike racks shall be the standard bike rack (see Appendix A for specifications).

Trash enclosures shall be the standard trash enclosures (see Appendix A for specifications).

Benches shall be the standard bench (See Appendix A for specifications).

Benches shall be grouped in twos that face each other and provide an opportunity for an outdoor conversation space.

Alternative bike rack, trash enclosures and bench designs will be considered on a case-by-case basis. The City will review alternative designs as part of the site plan review and will evaluate these alternatives based on the following:

- Does the design maintain the character of the district?
- Does the alternative design maintain the quality of the standard feature?
- Does the design provide a relationship between the public elements of the streetscape and the building that would not otherwise be possible?



Sidewalks and Boulevard

In the design framework, the term “boulevard” is defined as “the space between the sidewalk and the curb.” The boulevard area provides the required space for public features such as street lights, benches, fire hydrants, street signs, etc. The boulevard will have different characteristics based on the street hierarchy.

The street width and layout within different zones may affect the ability of a particular development to provide the minimum boulevard width and the minimum sidewalk width within the existing public right-of-way. In those cases, the landowner shall provide a permanent public easement over the portion of the sidewalk that is located on private property.

Street Lights

Street lights are owned and installed by Connexus Energy. The streetlight design was developed in conjunction with the City of Ramsey and Connexus (See Figure 3: Master Lighting Plan). The standard street lights are designed to accommodate banner arms should banner signage be desired. The banner arms should be the manufacturer's standard break-away banner arms to ensure safety and minimize maintenance costs. Connexus provides street light maintenance for the City. Connexus bills the City of Ramsey for the cost of installation and maintenance.

The choice of street light was intended to emphasize the pedestrian scale of The COR. Light poles within the public right-of-way should be planned to accommodate banners, flowers, hanging artwork and electrical outlets.

There are three (3) different streetlight types within The COR:

- 24-foot high poles at intersections
- 14-foot high poles at mid-block
- 12-foot high poles at The Draw



The City will ensure that the appropriate streetlights are installed on all public streets as described by the Design Framework. Private developers shall provide parking lot or other project lighting that retains these design

objectives. All lighting shall comply with City Code performance standards for lighting. All lighting shall be reviewed by City staff to ensure compliance with the Design Framework and City Code standards described above.

Standards for Outdoor Dining.

Outdoor dining is encouraged as an attractive addition to a pedestrian- and retail-friendly, vital COR. Such dining is encouraged subject to the standards in Section 117-118 of the Zoning Ordinance.

Overall Framework - Parking

Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of The COR is a significant challenge. The goal is to provide adequate parking; both quantity and location, while promoting new strategies that support the overall needs of The COR. Parking will be provided through a combination of on street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of The COR. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street level uses should be integrated into ramp design along important street frontages. The City also encourages developers to wrap the ramp with multi-story uses that will provide commercial and residential opportunities and help to define the street edge.

Guideline Recommendations

Parking guidelines encourage the following:

- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views, soften parking area edges, provide shade, integrate native plantings, offset islands and reduce to 'sea of parking' image
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering
- Provide parallel parking on both public and private streets throughout The COR, to add parking and create a traffic calming, pedestrian buffer; Bumpouts are encouraged as a traffic calming measure on all streets within the COR.
- Explore options to integrate stormwater management requirements into parking area design
- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on street, small surface lot, underground and ramped parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area

- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Discourage driveway access to parking facilities along Arterial, Destination and Parkway streets, in favor of side streets.

Objectives

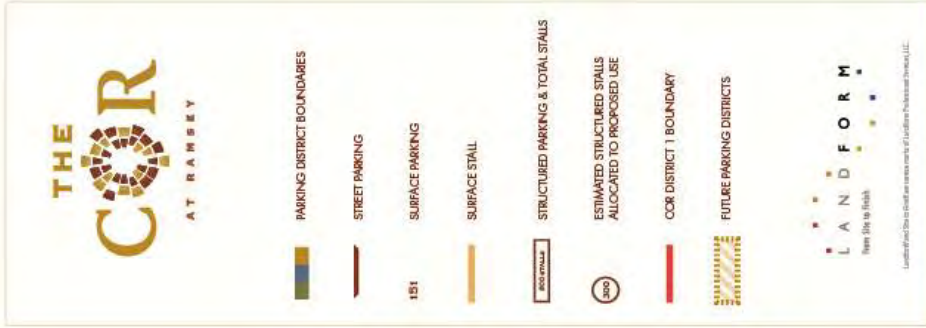
The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in The COR. The Design Framework anticipates the construction of a total of 3 parking structures in COR1 to serve the needs of the area by providing structured parking that can be shared among users, allow developers to maximize the development on each individual lot.

As discussed later in this section, sizing and timing of the structured parking will be finalized as the area develops and parking demand grows. In addition to providing adequate space, excellent design is a major factor as well. The framework for The COR encourages creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs throughout the Design Framework suggest some of the many possibilities.





Figure 4: Streetscape Master Plan



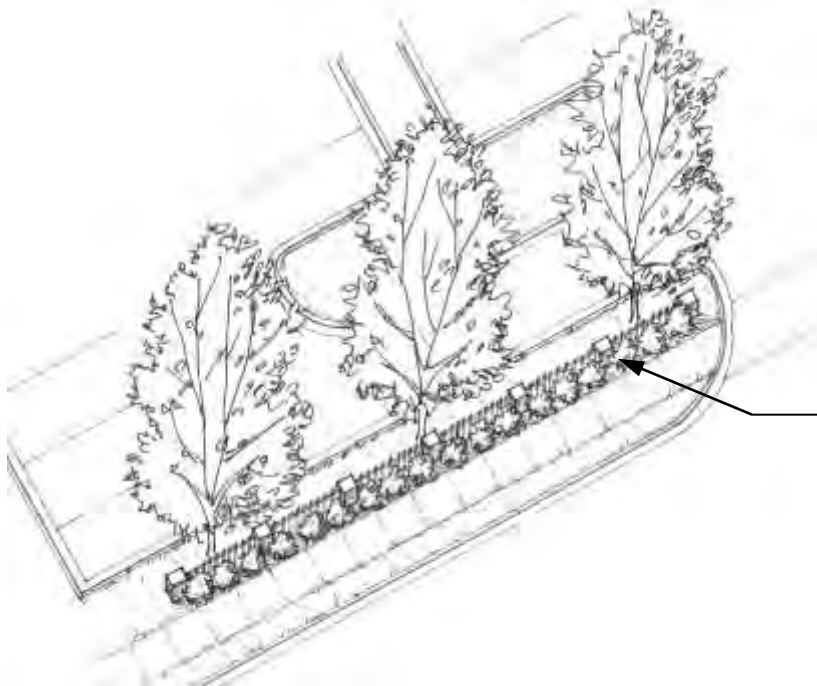
Parking Districts
 VERSION 5.03
 9/27/11

Figure 5: Parking District Map

Minimum Standards

Parking in The COR is intended to be shared to the greatest extent practicable in all mixed-use areas. The parking standards are intended to provide a practical basis for providing adequate parking within the COR District through a careful analysis of uses, shared parking arrangements, use of public street parking and reduction allowances for the proximity to the transit station. A parking plan has been prepared to identify the planned number, location, sharing arrangements and public use (see Figure 4: Streetscape Master Plan and Figure 5: Parking District Map). The parking plan provides information that can be used to help address longer term parking arrangements where the possibility of use conversion or building expansion exists. Parking shall be provided under any of the following arrangements:

- A. No parking shall be located between the front of the principal structure and the street on parcels abutting Destination and Downtown streets in COR1.
- B. Not more than fifty (50%) percent of the parking shall be located in front of the principal structure and the street on parcels abutting Arterial, Parkway, Connector and Local streets.
 - 1. In the COR2 district, developments may be allowed more than fifty (50%) percent parking in the front yard, provided all other design standards have been met, including the use of a wall, railing, hedge or combination to create a street edge and screen parking.
- C. Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4¹/₂') above the level of the parking lot, at the build-to line required for the property. The amount of required screening may vary by sub-district and street frontage and is described in Part Two of this document.



Landscaped edge that combines fence with trees, low shrubs and ground cover for parking lot screening and buffering along sidewalk to screen parking

- D. No parking spaces shall be located on corner lots at the point of street intersections.
- E. Shared parking in COR1 will be accomplished through a Parking Use and Maintenance Agreement (PUMA).
- F. Off-street parking in the COR2 and COR3 sub-districts shall be secured for public use through parking easements and other appropriate conveyances. Shared parking arrangements between nearby uses are encouraged in both sub-districts.
 - 1. The City may approve joint parking for one or more businesses where the total number of parking stalls provided for joint use is less than the sum of the total required for each business should they provide them separately. The applicant must demonstrate the feasibility of the arrangement in a written report. Such a permit shall not be granted except when the following conditions are found to exist:
 - i. Proximity. The building or use for which application is being made to utilize the off-street parking facilities provided by another building or use shall be located within three hundred (300') feet of such parking facilities, excluding public rights-of-way.
 - ii. Conflict in Hours. The applicant shall demonstrate in documented fashion that there is no substantial conflict in the principal operating hours of the two (2) buildings or uses for which joint use of off-street parking facilities is proposed.
 - iii. Written Consent and Agreement. A legally binding instrument, executed by the parties concerned, for joint use of off-street parking facilities, duly approved as

to title of grantors or lessors, and in a form and manner of execution approved by the City Attorney, shall be filed with the City Clerk and recorded with the Anoka County Recorder or Registrar of Titles, and a certified copy of the recorded document shall be filed with the City within sixty (60) days after approval of the joint parking use by the City or the interim use permit shall be considered null and void.

- G. On-street parking adjacent to buildings shall not be used for the purposes of calculating parking requirements.
- H. Within the COR-4 sub-district, parking on individual parcels serving individual uses may be provided if designated and approved as part of the master plan.
- I. Within all sub-districts, a minimum driveway length of twenty-five (25') feet is required to limit conflicts between vehicles parked in the driveway and street or sidewalk users.
- J. In order to ensure the pedestrian orientation of the COR Districts, minimum/maximum on-site parking standards are set based on the following table:

Table 1

Use	Minimum number of parking stalls	Maximum number of parking stalls*
Retail	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Restaurants	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Offices	2 per 1,000 sq. ft.	3 per 1,000 sq. ft.
Medical offices, clinics	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Health clubs	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Theaters, places of assembly	1 per 5 seats	1 per 3 seats
Residential		
Attached or detached	1 per unit	2 per unit
Multifamily units	1 per unit	2 per unit

*If a parking structure is provided on site, maximum parking stalls do not apply.

- K. Non-Specified Uses. For uses not specifically listed above, off-street parking requirements shall be computed by the Zoning Administrator on the same basis as required for the most similar listed uses. In such cases, the Zoning Administrator shall also consult off-street parking reference materials including, but not limited to, manuals prepared by the American Planning Association and Institute of Transportation Engineers.
- L. Handicap Parking. The size, number, and location of stalls reserved for handicapped parking shall be provided and identified as required by applicable regulations. These spaces are included in the computation for the minimum parking space requirement.
- M. Change in Land Use. When the site intensity or use of a building and/or property is increased with consequential effect upon the parking requirements as prescribed in this Section, the

parking requirements as prescribed herein shall be used to provide for such increase in the site intensity and/or use.

- N. Use of required parking spaces. Required parking spaces must be available for the use of residents, customers or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.
- O. Transit Service Reduction. The minimum number of parking stalls required on site may be reduced by 10% for any parcel located within ¼ mile of a transit stop. To qualify for this reduction, the transit stop must provide regular service on all days of the week and direct pedestrian access must be provided between the building and the transit stop.
- P. Bike Racks. In addition to the bike racks provided as part of the streetscape, individual businesses are encouraged to provide bike racks for customers. Bike rack locations will be reviewed and approved by the City as part of the site plan review.
- Q. Increase to Maximum Required Parking. If additional parking is sought to exceed these maximum requirements, a conditional use permit can be sought to increase maximums up to twenty-five (25%) percent.
 - 1. In the COR-2 sub-district, an increase of twenty-five (25%) percent in the maximum number of parking stalls is permitted without the issuance of a conditional use permit if approved as part of the site plan.
- R. In the COR1 sub-district, a PUMA (Parking Use and Maintenance Agreement) shall be applied to all development as follows:
 - 1. It is the City's intent to maximize development on individual lots. In order to help landowners accomplish this goal, the City will develop public parking ramps, which will be available for public parking within The COR. In order to minimize the amount of the site required for parking, the landowner may choose to enter into the PUMA for utilization of the public parking ramps to meet all or a portion of their minimum parking requirements.
 - 2. Each development shall be subject to a PUMA that specifies that each property in the COR1 shall be financially responsible for its proportionate share of a shared parking facility. The proportionate share shall be determined on the basis of the property's off-street parking needs, as determined by the parking requirements of Item J above.

3. The City shall establish fees by ordinance. The developer shall be responsible for payment in an amount equal to the value of the required parking on a per-stall price basis for ramp parking through the PUMA. The City will determine the appropriate contribution.
4. Funds collected by the City shall be deposited in a special fund used only to acquire and/or develop off-street parking facilities for the COR. Financial responsibility shall cover the construction and continuing maintenance of the parking facility.
5. The parking facility may be constructed and maintained by the City or by a private management entity acceptable to the City.

Private Streets

Private streets may be allowed within residential subdivisions, subject to the standards in Section 117-112 (e)3.b. of the Zoning Ordinance.

Loading Areas

- A. Loading areas and docks shall be located in the rear of the building.
- B. Loading areas shall not be located in the front yard and shall be fully screened from public streets, public parks and residential areas through landscaping and building design measures.



Overall Framework – Building Design

Overview

The COR Development Plan 5.03 identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, this Design Framework focuses on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

Guideline Recommendations

The mix of land uses in The COR will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, The COR resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of The COR, anchored by City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.

Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street, particularly in the center of the district.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, ‘big box’ retailing and auto-oriented uses within the COR2 sub-district only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a ‘family’ or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district.
- All uses should reinforce the street edge.



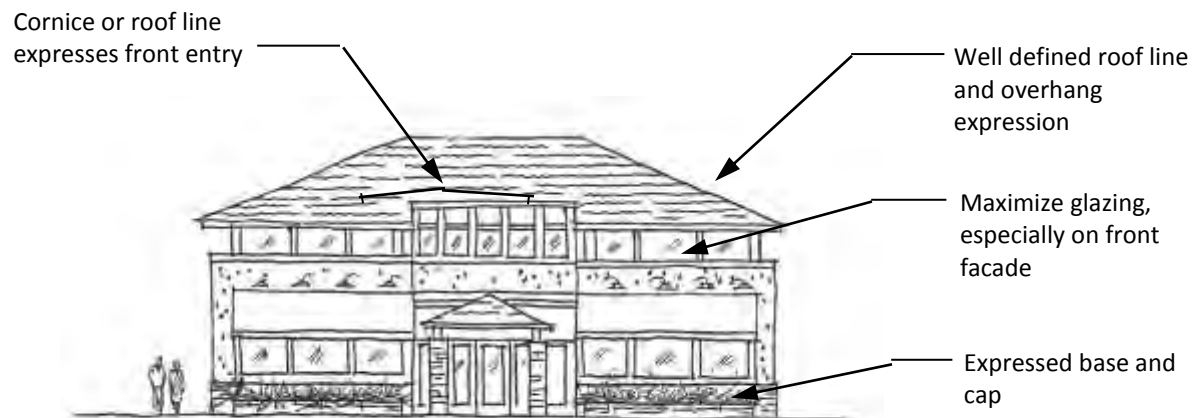
Architectural Overview

High quality architecture is an essential component in fulfilling the objectives of the Design Framework. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

Architecture Recommendations

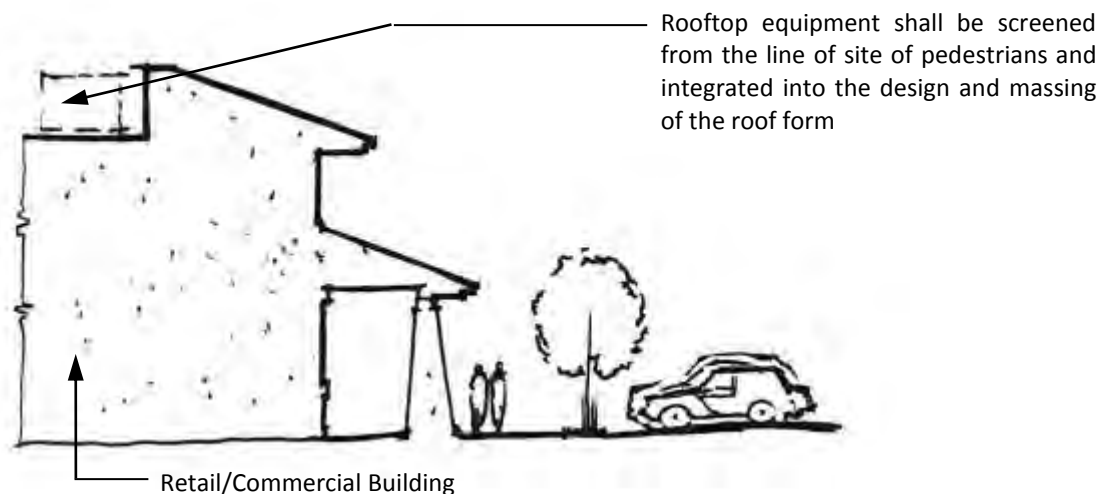
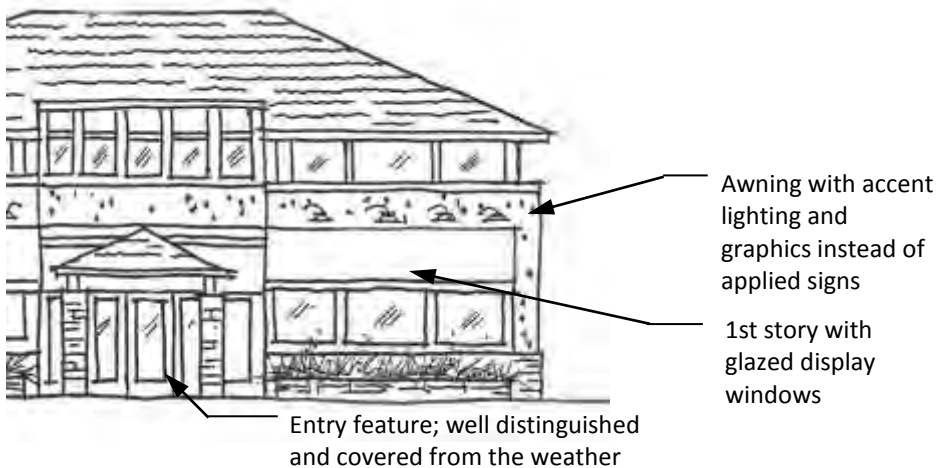
Of all the layers that combine to form The COR Development Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for The COR. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- An animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience

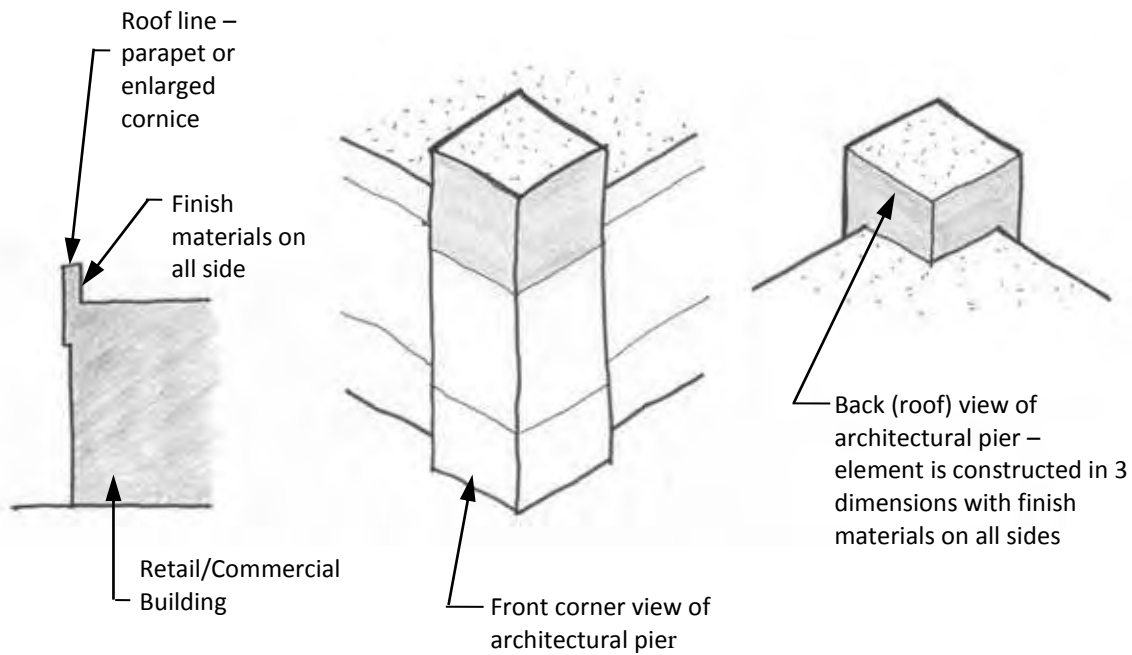


- Variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other COR uses
- Use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets and important crossroads throughout The COR
- Buildings should always be located at all four corners of intersections.
 - In the COR2 sub-district, if the City finds that there are unique circumstances where buildings cannot be located on the corner, the City may approve the use of fences, walls or other design elements to create that "street edge"

- Buildings will be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, or where streets intersect with parks, such as the streets that front on or end near the Municipal Plaza. These locations can often be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as The COR develops
- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.
- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Use recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage



- Equipment, mechanical systems, transformers, etc. must be screened from view in a way that is integral to the architecture of the building. Screening should be accomplished with materials similar to or compatible with materials used on the main structure; metal fencing or mechanical vent screens alone are not sufficient.
- Roof lines and cornice details shall be completed in a 3-dimensional manner so that the back of roof features or similar unfinished areas are not visible.



Overall Framework - Signage

Overview

Signage within The COR should provide a system for clear wayfinding for all modes of transportation and should ensure successful business operation while maintaining the design aesthetic of this unique project. These sign standards are intended to allow flexibility and creativity while maintaining the design concepts of The COR. Due to the distinctive nature of The COR, the sign standards from the City Code are not applicable and signage shall be allowed as prescribed by the Design Framework. A Master Sign Plan identifies the location of key sign locations (see Figure 6: Signage Plan).

Guideline Recommendations

The COR development manager shall prepare a Master Sign Plan for the public elements of the project, which are in compliance with the design framework and will be adopted by reference. The Master Sign Plan will specifically address public signs including:

- The three (3) off-site community signs
- Banner signs on light poles
- Signage for public parks
- Signage for public parking ramps
- Signage for public buildings
- Community kiosks



Objectives

Signage should be used as a tool to help identify businesses and neighborhoods with The COR and should have elements that are focused on the pedestrian nature of The COR as well as the signage needs for businesses.

- Signage and lighting which is integrated into the design of the building is encouraged.

Definitions

Awning Sign means a sign incorporated into or attached to an awning.

Community sign means an off-site sign identifying the development name and key tenants. Community signs may also include public information. Reader boards are allowed to be incorporated into community signs within The COR.

Temporary Sign means a sign which is erected or displayed for a limited period of time and not affixed to a Minnesota State Building Code approved structure.

Project sign means a freestanding area identification sign which identifies a single-family or multifamily residential subdivision, a commercial development or an industrial park or office park and which is located on the same parcel as the development it identifies.

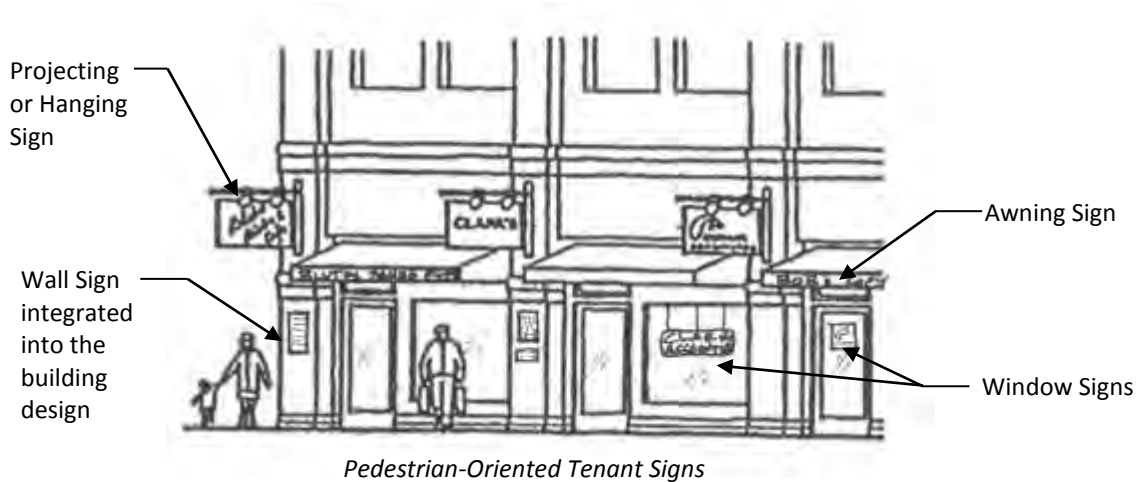
Projecting sign means any sign, all or any part of which extends beyond the surface of the building or wall by more than 16 inches.

Public Realm/Streetscape sign means any sign erected and maintained by public officials or public agencies, or approved and authorized for use by state or local governmental authorities.

Sandwich Board Signs means a self-supporting, freestanding temporary sign with only two (2) sides that are situated adjacent to a business with the intent to attract pedestrian traffic to businesses. Sandwich board signs are not meant to be read by vehicular traffic.

Wall sign means any sign which is affixed to a wall of any building. This definition includes individual letter signs and signs on mansards.

Window sign means a sign installed inside a window, or any sign placed within a building for the purpose of being visible from the public right-of-way. This does not include merchandise on display.



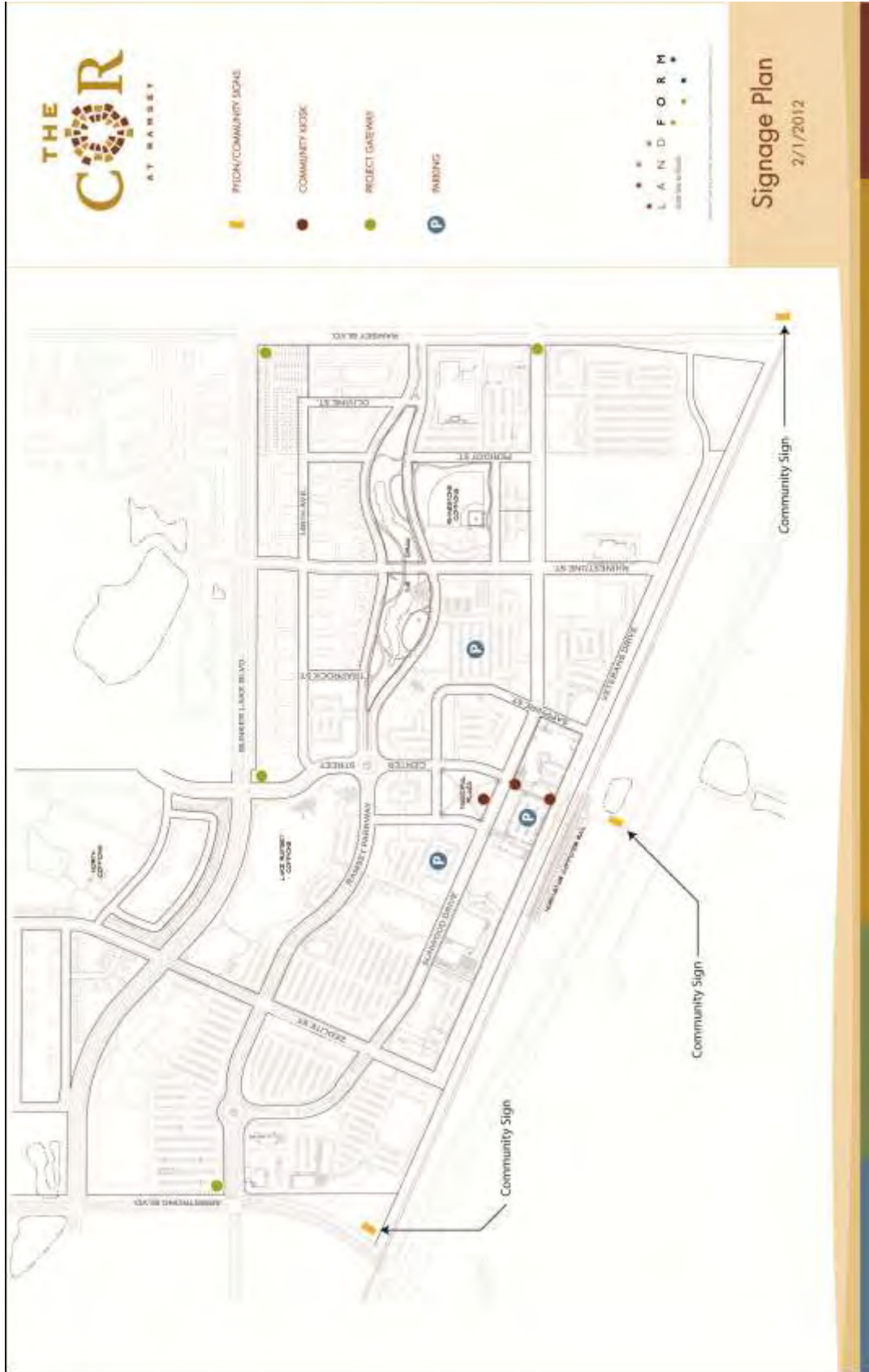


Figure 6: Signage Plan

Standards

Public Realm/Streetscape Signs

The pedestrian oriented nature of The COR requires unique signage that will build on the design concepts for The COR. These types of signs include banner signs on the streetlights, kiosks, street signs, wayfinding signs, signage for public parking ramps, etc. As the design concept for The COR has been refined, so has the signage plan. The COR development manager shall develop standards for these public realm signs as part of the Master Sign Plan. The Master Sign Plan shall be incorporated by reference and may be updated from time to time. Such signage shall be allowed on public spaces as needed to provide information and wayfinding. No advertising shall be allowed on Public Realm/Streetscape Signs except for community events.

Community signs

Community signs include are planned to include three (3) signs on Highway 10, as shown on the Signage Plan (see Figure 6: Signage Plan). The signs include the existing Community Sign near the future transit plaza, the sign at the northwest corner of Highway 10 and Ramsey Boulevard and the planned sign at the northwest corner of Highway 10 and Armstrong Boulevard.

These off-site signs are allowed as shown on the signage plan and shall have consistent materials and colors. The Master Sign Plan shall detail the materials, colors and standards for these three (3) community signs. The Master Sign Plan shall include standards for which tenants shall be allowed on the community signs.



Desirable signs are designed to be architecturally compatible with buildings within the development and other signage within the development

Project Signs

Project signs are permitted within all COR sub-districts.

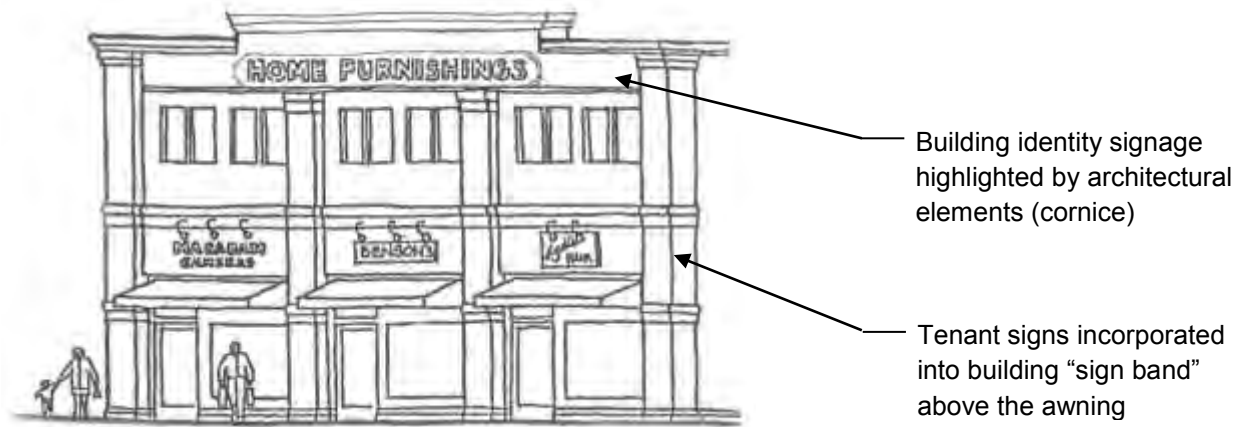
One (1) freestanding sign is allowed per building, provided that the following standards are met:

- A. The building in which the advertising business is located, shall be set back a minimum of six (6) feet from a public street right-of-way.
- B. The area of each face of the signboard shall not exceed six (6) square feet and the signboard shall not have more than two (2) readable faces
- C. The height of the top of the signboard, or of any posts, brackets, or other supporting elements shall not exceed six (6') feet from the ground.

- D. The signboard shall be constructed of wood, acrylic, aluminum or metal and shall be architecturally compatible with the style, composition, materials, colors and details of the building.
- E. No part of the sign shall encroach on the right-of-way and its location shall not interfere with pedestrian or vehicular circulation.
- F. Limited to one (1) sign per building and shall not be in addition to wall-mounted, applied letter or projecting signs.
- G. The readable faces of the sign shall be perpendicular to the adjacent street.

In the COR2 District, the following signage shall be allowed:

- A. Ground sign. There shall not be more than one (1) ground sign for each parcel. The gross surface area of a ground sign shall not exceed one hundred (100) square feet for each exposed face nor exceed an aggregate gross surface area of two hundred (200) square feet.
- B. Menu board. One (1) on-site menu board per drive-up or walk-up lane of a drive-in restaurant up to a maximum of thirty-two (32) square feet each and a maximum height of three (3) feet. Menu boards are allowed a message on one (1) side only and cannot contain an advertising message.
- C. Directional signs.
 - 1. Directional or instructional signs are permitted in accordance with Section 117-463(I).
 - 2. Parking lot directional signs designating parking area entrances and exits are limited to one (1) sign for each entrance and/or exit and shall not exceed four (4) square feet for each exposed face. Parking lot directional signs shall not project higher than five feet (5') in height, as measured from the established grade of the parking area to which such signs are accessory.
 - 3. Parking lot instructional signs designating the conditions of use or identification parking areas shall not exceed eight (8) square feet and shall not project higher than ten (10) feet in height for wall signs and seven (7) feet in height for ground signs, as measured from the established grade of the parking area to which such signs are accessory.



Wall Signs

Wall signs are permitted within all COR sub-districts.

Wall signs shall be permitted on one (1) wall, except that lots with frontage on more than one (1) street may have signage on one (1) wall per street frontage.

Within the COR1, COR3, COR4 and COR5 districts, the following standards apply:

- A. Wall-mounted or painted signs, provided the following standards are met:
 1. The sign shall be affixed to the front facade of the building, and shall project outward from the wall to which it is attached no more than six inches (6").
 2. Single Tenant Buildings. Each building shall be allowed one (1) wall sign per street frontage. Signage must be located on the street elevation. The area of the sign shall not exceed five (5%) percent of the ground floor building facade area or fifty (50) square feet, whichever is less.
 3. Multi-Tenant Buildings.
 - a. Each building shall be allowed wall signage on the street frontage elevation. For buildings with multiple street frontages, the allowable signage is per street frontage. Signage must be located on the street elevation.
 - b. Each building is allowed wall signage that shall not exceed 5% of the wall area of the building façade adjacent to the street or 50 square feet, whichever is less.
 - c. Tenant signage must be located on the tenant lease space.
 - d. At least fifty (50%) percent of the allowed signage must be allocated to ground floor tenants and located on the ground floor.
 - e. Signage is limited to a maximum of one (1) sign per business/tenant, except that tenants with frontage on multiple streets may be permitted to have signage on each street frontage.
 - f. Multi-tenant buildings must have wall signs of similar design. Sign permits for tenants in multi-tenant buildings shall only be permitted by the City after the

building owner has submitted a comprehensive sign plan approved by the Zoning Administrator. The comprehensive sign plan for the building shall include similar design standards including sign material, color, style, spacing and size.

4. Signs for buildings facing Highway 10. Single or multi-tenant buildings that have frontage on Highway 10 or are separated from Highway 10 only by other public right-of-way, shall be allowed to have signage on that street elevation that is up to five (5%) percent of the wall area of that building façade with no maximum square footage.
- B. Wall-mounted building directory signs identifying the occupants of a commercial building, including upper story business uses, provided the following standards are met:
1. The sign is located next to the entrance.
 2. The sign shall project outward from the wall to which it is attached no more than six inches (6").
 3. The sign shall not extend above the parapet, eave, or building facade.
 4. The area of the signboard shall not exceed three (3) square feet, with each tenant limited to one (1) square foot.
 5. The height of the lettering, numbers, or graphics shall not exceed four inches (4").
 6. One (1) such sign is allowed per public building entrance and is allowed in addition to other permitted wall signage.
- C. Applied letters may substitute for wall-mounted signs, if constructed of painted wood, painted cast metal, bronze, brass, acrylic or black anodized aluminum. The height of applied letters shall not exceed 12 inches.
- D. Logos are considered signs and shall be included in the maximum allowable sign area.
- E. Window or door signs, provided that the following standards are met:
1. The sign shall not exceed ten percent of the window or door area or four (4) square feet, whichever is less.
 2. The sign shall be silk screened, hand painted, applied letters/graphics, neon tubing or other sign technologies that meet these standards.
 3. Limited to one (1) sign per business, applied on either the window or the door, but not on both.
 4. The sign shall not have an opaque backing of any type although smoked glass is allowed.

5. Window signage (includes graphics) shall be permitted on the same building walls that have, or are allowed to have, wall signage pursuant to Item A above. Window signage shall be deducted from the allowable wall signage area.

Within the COR2 district, where more conventional suburban development is expected, rather than the neo-traditional development planned for the other COR sub-districts, the following standards apply:

- A. Wall, canopy or marquee sign. Total sign area may not exceed 15 percent of the front building facade. At least 50 percent of the signage area must be placed on the measured wall with remaining signage area, if desired, distributed on any other wall. Sign height shall not exceed the top of the parapet wall or, if no parapet wall, sign height shall not exceed the height of the eaves. The gross surface area of a wall, canopy or marquee sign may be increased by ten percent if such wall sign:
 1. Consists only of individual, outlined alphabetic, numeric and/or symbolic characters without background except that provided by the building surface to which the sign is to be affixed;
 2. Illumination, if any, is achieved through shielded illumination, shielded silhouette lighting, or shielded spot lighting but not any lighting where the light source is visible or exposed on the face or sides of the characters; and
 3. A wall, canopy or marquee sign may be located on the outermost wall of any principle building but shall not project more than 16 inches from the wall to which the sign is to be affixed. A wall sign shall not project higher than the parapet line of the wall to which the sign is to be affixed.
- B. Window signs are restricted to 30 percent of the area of the window in which the sign is to be displayed.

In all COR sub-districts, restaurants and cafes shall be permitted one (1) wall-mounted display featuring the actual menu as used at the dining table, to be contained within a shallow wood or metal case and clearly visible through a glass front. The display case shall be attached to the building wall, next to the main entrance, at a height of approximately five feet (5'), shall not exceed a total area of two square feet, and may be lighted. This signage is allowed in addition to other permitted wall signage.

Projecting Signs

Projecting signs are permitted within all COR sub-districts.

Projecting signs, including graphics or icon signs, mounted perpendicular to the building wall, are encouraged in all sub-districts, provided the following standards are met:

- A. The sign area shall not exceed six (6) square feet.
- B. The distance from the ground to the lower edge of the signboard shall be ten feet or greater.



- C. The height of the top edge of the signboard shall not exceed the height of the wall from which the sign projects, if attached to a single story building, or the height of the sill or bottom of any second story window, if attached to a multistory building.
- D. The distance from the building wall to the signboard shall not exceed six inches (6").
- E. The width of the signboard shall not exceed three (3) feet.
- F. Limited to one (1) sign per business. Projecting sign area shall be deducted from the allowable wall signage area.
- G. Granted to ground floor commercial uses only.

Awning Signs

Awning signs are allowed in all COR sub-districts. Awning signs are allowed for ground floor uses only, provided that the following standards are met:

- A. If acting as the main business sign, it shall not exceed 24 square feet in area, and the height of the lettering, numbers, or graphics shall not exceed 12 inches.
- B. If acting as an auxiliary business sign, it shall be located on the valance only, shall not exceed four (4) square feet in area, and the height of the lettering, numbers, or graphics shall not exceed four inches (4").
- C. Limited to two such signs per business.
- D. If acting as the main business sign, it shall not be in addition to a wall-mounted or applied letter sign.

Sandwich Board Signs

Sandwich board signs are permitted within all COR sub-districts.

- A. One (1) sandwich board sign per business is permitted in any business, commercial and mixed use district and shall be located within five feet (5') of the main building entrance to the business it advertises.
- B. Sandwich board signs shall be displayed only during open business hours and must be removed daily.
- C. Sandwich board signs shall be no more than a total of two feet (2') in width and three feet (3') in height



- D. Sandwich board signs must leave a minimum of five feet (5') of clearance for pedestrian access if placed on a public or private sidewalk. Sandwich board signs may not hinder the ability of persons to access vehicles parked at the curb and/or access to a building.
- E. Acceptable materials for sandwich board signs shall include the following: metal, wood synthetic materials such as a chalk board and whiteboard. Sandwich board signs shall not be illuminated, nor shall they contain moving parts, or have balloons, streamers, stringers, pennants or similar adornments attached to them. Sandwich board signs shall be maintained in a good appearance at all times.
- F. No sandwich board sign shall be secured, tethered or installed on traffic devices, utility equipment, street furniture, street lights, or any other public fixture.
- G. Sandwich board signs are temporary signs and shall not be counted towards the total sign area of the site for permanent signage.

Temporary Signs

Temporary signs are allowed per Section 117-465 (Temporary signs) of the Zoning Ordinance, except as noted below:

- A. Freestanding temporary signs shall only be allowed in the COR1 District.
- B. In all other COR districts, temporary signs shall be limited to temporary sandwich board signs or temporary wall signage.

Off-Site Signs

Off-site signs are prohibited except for wayfinding and community signage specifically allowed by the Design Framework.

Table 2

Sign Standards					
Development Standard	COR1	COR2	COR3	COR4	COR5
Project Signs					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign (maximum)	6 sq. ft./6 feet high	100 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high	6 sq. ft./6 feet high
Menu Board**	N/A	1 per drive-through/36 sq. ft. max.	N/A	N/A	N/A
Directional Sign	N/A	Per 117-463(l)	N/A	N/A	N/A
Wall Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign					
Single Tenant Building	50 sq. ft. or 5% of the ground floor façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less	50 sq. ft. or 5% of the ground floor façade area, whichever is less
Multi-Tenant Buildings	50 sq. ft. or 5% of the façade area, whichever is less	15% of façade area	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less	50 sq. ft. or 5% of the façade area, whichever is less
Building Facing Highway 10	5% of the façade area	15% of façade area	5% of the façade area	5% of the façade area	5% of the façade area
Window Sign					
Number of signs allowed	1	1	1	1	1
Size of Sign	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	30% of area of window. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.
Directory Sign					
Number of signs allowed	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance
Size of Sign	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum
Projecting Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide
Awning Sign					
Number of signs allowed	2 per business	2 per business	2 per business	2 per business	2 per business
Size of Sign	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.

*One (1) sign is allowed per street frontage (either wall or project)

**Restaurants and cafes in all districts are allowed one (1) wall mounted menu board not to exceed 2 sq. ft. in addition to other permitted wall signage.

Additional standards

- A. Businesses with service entrances may identify these with one (1) wall-mounted or applied letter sign not exceeding two square feet.

- B. One (1) directional sign, facing a rear parking lot. This sign may be any type of permitted sign other than a freestanding sign, but shall be limited to four (4) square feet in area.

Creative Sign Standards

- A. *Purpose.* This section establishes standards and procedures for the design, review, and approval of creative signs. The purposes of this creative sign program are to:
 - 1. Encourage signs of unique design, and that exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - 2. Provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- B. *Applicability.* An applicant may request approval of a sign permit for a creative sign to authorize onsite signs that employ standards that differ from the other provisions of this chapter but comply with the provisions of this section.
- C. *Application Requirements.* A sign permit application for a creative sign shall include all information and materials required by the City.
- D. *Procedure.* A sign permit application for a creative sign shall be subject to review and approval by the City as part of the Site Plan review process. When the creative sign is proposed after site plan review is complete, a creative sign may be approved by the Zoning Administrator when the proposed sign is fifty square feet or less, and shall be approved by the City Council when the sign is larger than fifty square feet.
- E. *Design Criteria.* In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
 - 1. Design Quality. The sign shall:
 - a. Constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area;
 - b. Be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - c. Provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
 - 2. Contextual Criteria. The sign shall contain at least one (1) of the following elements:
 - a. Classic historic design style;
 - b. Creative image reflecting current or historic character of the City;
 - c. Inventive representation of the use, name, or logo of the structure or business.

3. Architectural Criteria. The sign shall:
 - a. Utilize or enhance the architectural elements of the building; and
 - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
4. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

Prohibited signs

The following signs are prohibited:

- A. Box signs or cabinet signs, whether on a wall, projecting or on canopies are prohibited except for logo signs permitted as part of an overall sign plan.
- B. Signs employing mercury vapor, low pressure and high pressure sodium and metal halide lighting; plastic panel rear-lighted signs.
- C. Signs on roofs, dormers, and balconies.
- D. Billboards.
- E. Signs painted or mounted upon the exterior side or rear walls on any principle or accessory building or structure, except as otherwise permitted hereunder.
- F. Free standing pylon signs over six (6) feet in height, except community signs.
- G. Back-lit awnings.
- H. Interchangeable letter boards or panels.
- I. Flashing signs.
- J. Off-premises signs, except community signs.

Overall Framework – Stormwater Management

Overview

Water features and drainage systems are essential components of The COR Master Plan. The mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize ‘best management practices’ (BMP) to pretreat run-off, reduce erosion and encourage infiltration, with the understanding that regional treatment will be provided. Water elements are not intended to be separate stand-alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for The COR. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.

Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- ‘Green architecture’, expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



Guideline Recommendations

This framework not only defines the overall role and character of The COR, but also encourages each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout The COR
- **Promote** high quality, creative and appealing aesthetics for all system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support COR development. While this overall strategy addresses the water quantity issue for the entire COR by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.

Overall Framework – Parks/Public Spaces

Overview

The overall design framework of The COR is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout The COR. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

Guideline Recommendations

The Design Framework encourages the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements



Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.

Objectives

Based on the Development Master Plan for The COR, a variety of public and private green space is proposed including:

- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities.
- Playgrounds, game fields and park buildings are park amenities that will target a wider user demand from the surrounding community.
- The Municipal Plaza will connect with all other parks and open space elements, providing a focal point for activity and a sense of place. Gardens, public pavilions and shelters, play lots and open play fields are amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to The COR.



Part Two: Sub-district Framework

Overview

The Master Plan and Zoning Ordinance identify five (5) distinct sub-districts within The COR. Each of these sub-districts is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. The sub-districts are shown the attached Sub-district Map (Figure 7: Sub-district Map). They include:

- **COR1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **COR2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses
- **COR3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **COR4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **COR5 Park and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout The COR

Development plans must respond to the overall COR Design Framework described in Part One of this document, but also must address the following detailed framework standards for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Framework
- Site Design Framework
- Architectural Framework

In all cases, the sub-district framework must be applied in harmony with the overall COR framework and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development. However, within each district, development standards shall be based, in part, on the street hierarchy established in The COR.

Public Realm/Streetscape

- A. The developer shall be responsible for construction of all streetscape improvements along adjacent streets. These improvements include all hardscape improvements, such as trails, sidewalks, benches, bike racks, etc., as well as street trees and other greenscape.
- B. The landowner shall be responsible for maintenance of the streetscape improvements along adjacent streets. Such maintenance may be managed as part of a Special Service District (SSD) or similar.

Plazas

- A. All properties with principal buildings in excess of 30,000 square feet shall be required to have a public plaza space.
- B. The public plaza space shall include benches, bike racks trash receptacles, lighting fixtures and other amenities to create a welcoming space for tenants of the building and members of the public.

C. These plaza spaces should be visible from the public street.

Architecture

These architectural framework standards provide an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



Auto Versus Human Design: Providing rear alleys or side loaded garages enables the home to present itself to the street, which promotes sociability and walkability.

Minimum Standards

Definitions

Build to line: The line at which construction of a building facade is to occur on a lot. The build to line provides a maximum setback for all building facades from the right-of-way or inside edge of the sidewalk, whichever is greater. The build to line applies to all facades abutting a street right of way. Build to line standards are based on the sub-district the parcel is located and the street in which the building is located upon.

Setback: The minimum required distance between a parking lot or the vertical wall of a building and a lot line. Setbacks are typically more flexible than build to lines and allow a building to be set further back from the lot line and street right-of-way.

Requirements:

- A. Additional development standards shall be defined within each sub-district.
- B. Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street.

C. Buildings should be articulated to break up the mass of the building façade on all elevations. Large blank exterior walls shall be prohibited. Windows are encouraged to be used to break up the mass of the building. Rooflines and building elevations should be articulated to break up the mass of buildings. Buildings must be broken at a minimum of every fifty (50) feet. This variation can be accomplished through one (1) or more of the following:

1. Wall face variations (minimum depth of two (2) feet)
2. Pilasters or columns (minimum depth of two (2) feet)
3. Upper wall break in color and/or materials (minimum of 2/3 up the height of the wall)
4. Wainscot (minimum of four (4) feet high)
5. Canopies (minimum four (4) foot width)
6. Corner and demising wall or building separations



More Desirable

D. Each development application must list building materials, roofing materials and building colors on the plans. Building articulation, mechanical screening by architectural elements and design elements on all building elevations must be identified on the plans. The plans shall be subject to City review and approval to ensure that the design intent of the Framework has been met.



Less Desirable

E. Building materials shall be high-quality durable materials, such as glass, stone, brick, windows, canvas awnings, etc.

1. Use local sources if available
2. Siding is not allowed (vinyl, metal, wood) except as a complimentary use
3. Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
4. Metal and wood components are acceptable as a complimentary, not primary, building material
5. COR1 sub-district buildings shall be primarily brick or a comparable alternative and shall comprise a minimum of fifty (50%) percent of the non-glazed wall area

F. Residential development should provide variety within the subdivision to avoid monotony. Developers will include front porches and windows that face all streets. Corner units shall have more than one (1) window per side of building to help create a sense of place in the community and promote safety on the street.

G. Garage forward or “snout houses” are highly discouraged on all residential development as they dominate the view from the street and sidewalk.

H. All sides of buildings shall have an equal finish in terms of materials and general design.

I. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.

- J. Building massing shall be oriented parallel to the street frontage of the lot.
- K. Trash and recycling areas shall be designed internal to the building.
- L. Provide recessed entries wherever possible.
- M. Buildings shall have a base and top to the architecture.
- N. Multi-story buildings are preferred in the COR1 sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of twenty (20) feet to the roofline.
- O. Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – fifty (50%) minimum of total ground level façade, or demonstrate great design through other means.
- P. Provide real window openings for all street facing façades above ground level in all districts—a minimum of forty (40%) percent of total façade.



Residential uses in COR4 and COR5 must also comply with the following:

- A. *Location of main entrance*
 1. A building must include a front porch or covered balcony/patio at all main entrances that face a street. The porch or stoop shall adjoin the main entrance and the main entrance shall be accessible from the porch.
 2. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.
- B. *Porches:* Porches used to satisfy the design criteria shall comply with the following:
 1. Porches shall be covered by a solid roof. The roof shall not be located more than 12 feet above the floor of the porch. If the roof of a required porch is developed as a deck or balcony, it may be flat.
 2. The porch shall have minimum dimensions of 6 feet by 6 feet. (6' x 6'). For single-family detached dwelling units, the covered area provided by the porch must be at least



forty-eight (48) square feet and a minimum of eight (8) feet wide. If the main entrance is for more than one (1) dwelling unit, the covered area provided by the porch must be at least sixty-three (63) square feet and a minimum of nine (9) feet wide.



C. *Covered balconies/patio*: The covered area must be at least forty-eight (48) square feet, a minimum of eight (8) feet wide, and no more than fifteen (15) feet above grade. The covered area must be accessible from the interior living space of the house.

D. *Openings between porch floor and ground*: Openings of more than one (1) foot between the porch floor and the ground must be covered with a solid material or lattice.

E. *Roofs*

1. *Slope*: Principal structures must have a roof that is sloped, with a pitch that is no flatter than six (6) units of horizontal run to twelve (12) units of horizontal rise.
2. *Architectural features*: The roof of a principal structure shall include the following architectural details:
 - a. At least one (1) dormer facing the street. If only one (1) dormer is included, it shall be at least five (5) feet wide and shall be centered horizontally between each end of the front elevation. If more than one (1) dormer is provided, a dormer at least four (4) feet wide must be provided on each side of the front elevation;
or
 - b. A gabled end, or a gabled end of a roof projection, facing the street.
3. *Roof eaves*: Roof eaves must project from the building wall at least twelve (12) inches, measured horizontally, on at least the front and side elevations.





Figure 7: Sub-district Map

Sub-District Framework - COR1 Mixed Use Core

Public Realm / Streetscape

Public realm and streetscape improvements define the character of The COR and create a memorable signature for the Mixed Use Core sub-district. This is the most urban of all sub-districts in The COR and will be developed with a very urban pattern. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year.

Site Development

Framework standards that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, the Design Framework also describes the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within The COR.

Architecture

Buildings in the Mixed Use Core will play a major role in defining the overall character of The COR. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 3

COR1 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.75	.75	.75	.75	.75	.75
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR2 Commercial

Public Realm / Streetscape

While the Commercial Sub-district encourages a mix of large and small floor plates accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential.

Site Development

Site Design Framework for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for The COR. Specifically, this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. However, the market realities suggest that the types of auto-oriented users in this district will have the potential for larger parking fields and drive-throughs that will make a true urban model challenging. The City will hold all users to this design vision while modifying the COR2 standards slightly to accommodate these types of more typical suburban land uses. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours.

Architecture

Buildings in the Commercial Sub-district make a significant contribution to first impressions of The COR. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make The COR unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 4

COR2 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Minimum lot depth	100 feet	100 feet	100 feet	100 feet	100 feet	100 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Boulevard Sod	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	6 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

- A. At least forty (40%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply only to one (1) street frontage.
 1. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
 2. The City may approve a variation from the required build to line if the applicant provides a street edge consisting of fencing, decorative wall and/or landscaping with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
- B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the

level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the forty (40%) percent required on all street frontages.

Sub-District Framework - COR3 Workplace

Public Realm/Streetscape

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of The COR is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. The Draw, an active, neighborhood scale, recreational play space has been included in this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts.

Site Development

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts.

Architecture

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter The COR via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this sub-district suggest a variety of potential building types – blending medical campus with small office, storefront and some housing -- demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 5

COR3 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	40 feet	40 feet	40 feet	40 feet	40 feet	40 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	none	none	none	none	none	none
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR4 Neighborhood

Public Realm/Streetscape

Two prominent public parks supply much of the informal green space for The COR, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Commons surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. The sub-district is adjacent to The Draw, which is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout The COR.

Site Development

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of The COR. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The arterial streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of The COR.

Architecture

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of The COR property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for The COR. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 6

COR4 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR5 Parks & Open Space

Public Realm/Streetscape

The overall structure of The COR is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles.

Site Development

The COR Master Plan illustrates a strong framework of green space, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in The COR.

Architecture

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for The COR. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Additionally, limited retail is planned along the perimeter of the park.

Table 7

COR5 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	0.5	N/A	0.5	0.5	0.5	0.5
Minimum lot width	N/A	N/A	N/A	N/A	N/A	N/A
Minimum lot depth	N/A	N/A	N/A	N/A	N/A	N/A
Allowable residential density in dwelling units per acre ⁽¹⁾	N/A	N/A	N/A	N/A	N/A	N/A
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	N/A	N/A	N/A	N/A	N/A	N/A
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	N/A	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	N/A	no req.	no req.	no req.	no req.
Rear yard	no req.	N/A	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet	N/A	25 feet	25 feet	25 feet	25 feet
Building height (min. - max.)	1-2 stories.	N/A	1-2 stories.	1-2 stories.	1-2 stories.	1-2 stories.
Planter Style	N/A	N/A	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	N/A	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	N/A	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	N/A	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Where commercial buildings are planned, at least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Implementation

Introduction

This framework provides distinct strategies for guiding The COR's form and appearance. This framework's vision for the COR will take several years to implement. Successful implementation will require consistent application of the Design Framework by City staff. Zoning enforcement and design framework standards will not succeed without the cooperation and commitment of landowners, development interests, and the rest of the community.

Design Review

The Design Framework will be administered by staff as part of the development review process. Staff will incorporate design review to ensure compliance with the Design Framework as a standard step in the development review process.

This page intentionally left blank

APPENDIX A: SPECIFICATIONS AND DETAILS

Contents

Design Framework – The COR.....	1
Purpose	1
Application	1
Implied Responsibility	1
Part One: Overall Framework	2
Overall Framework - Context.....	3
Overview	3
Guideline Recommendations.....	3
Objectives.....	3
Overall Framework – Street Hierarchy	5
Overview	5
Guideline Recommendations.....	5
Objectives.....	5
Transit Overview	8
Transit Guideline Recommendations.....	8
Transit Objectives	9
Overall Framework - Streetscape	10
Overview	10
Guideline Recommendations.....	12
Objectives.....	12
Street Trees and Plantings	12
Street Furniture.....	14
Sidewalks and Boulevard	14
Street Lights	15
Standards for Outdoor Dining.....	15
Overall Framework - Parking	16
Overview	16
Guideline Recommendations.....	16
Objectives.....	17

Minimum Standards.....	20
Private Streets.....	24
Loading Areas.....	24
Overall Framework – Building Design.....	25
Overview.....	26
Guideline Recommendations.....	26
Objectives.....	26
Architectural Overview.....	27
Architecture Recommendations.....	27
Overall Framework - Signage.....	30
Overview.....	30
Guideline Recommendations.....	30
Objectives.....	30
Definitions.....	30
Standards.....	33
Overall Framework – Stormwater Management.....	44
Overview.....	45
Objectives.....	45
Guideline Recommendations.....	45
Overall Framework – Parks/Public Spaces.....	47
Overview.....	47
Guideline Recommendations.....	47
Objectives.....	47
Part Two: Sub-district Framework.....	49
Overview.....	49
Public Realm/Streetscape.....	49
Plazas.....	49
Architecture.....	50
Minimum Standards.....	50
Sub-District Framework - COR1 Mixed Use Core.....	54
Public Realm / Streetscape.....	55
Site Development.....	55

Architecture	55
Sub-District Framework - COR2 Commercial	57
Public Realm / Streetscape	57
Site Development.....	57
Architecture	57
Sub-District Framework - COR3 Workplace	60
Public Realm/Streetscape	60
Site Development.....	60
Architecture	60
Sub-District Framework - COR4 Neighborhood	62
Public Realm/Streetscape	62
Site Development.....	62
Architecture	62
Sub-District Framework - COR5 Parks & Open Space	64
Public Realm/Streetscape	64
Site Development.....	64
Architecture	64
Implementation	66
Introduction	67
Design Review	67

Appendix A: Specifications and Details

Design Framework – The COR

Purpose

The primary purpose of Design Framework is to set basic parameters, describe preferences and illustrate design intent. These framework standards serve as a framework within which creative design can and should occur – there is no one solution, but many options that meet the basic requirements of this document.

Application

The format and content of this framework are specifically tailored for use as a supplement to the Zoning Ordinance. This document is organized into two sections. The first section outlines issues and recommendations that apply to the entire COR. The second section highlights specific framework that apply to each sub-district within The COR. The City of Ramsey has adopted Development Plan 5.03 as the Master Plan for The COR. This master plan provides illustrative guidance for building locations and orientation. Any changes to this approved master plan must be reviewed and approved by the City Council as part of the development application. This Development Plan will be updated by the City as changes are approved.

Implied Responsibility

All participants in the development of The COR recognize the local and regional impacts of this project and the various systems that play an important role. Each parcel and each building must fit within the context of the entire plan. Individual projects must complement, not compete with, adjacent development in terms of public green space, walk and trail connections, stormwater management solutions, street layout, parking strategies, land use mix and building design.

Part One: Overall Framework

There are a number of framework standards that apply to the entire COR, including recommendations for stormwater management, parks, streets, land use, parking, transit and other development components. Many of these overall COR framework standards overlap, or are integrated with one another. For ease of discussion they are categorized according to the same list of 'layers' that formed the basic structure for the Master Framework:

- Context – local, city, regional
- Street Hierarchy--access, circulation, arrangement
- Streetscape—preferred design, location
- Parking -- quantity, location, type
- Building Design-- preferred uses, horizontal/vertical mixed use, built form, character of development
- Signage—design, location, quantity, type
- Stormwater Management-- surface water features, stormwater management
- Parks/Public Spaces – parks, trails and open space

It is essential that proposed design solutions for development projects and other improvements within The COR demonstrate an understanding of the interplay between these layers.

Overall Framework - Context

Overview

The COR is a unique area within the City; but it is a part of, and connected with, a variety of local, city-wide and regional systems. Each development project, whether a single building, one lot, or a series of blocks, must provide reasonable links to these systems as a primary design objective.

Guideline Recommendations

To ensure that The COR takes full advantage of local and regional systems, development should:

- Provide safe, easily recognized connections to city, county and state trail corridors
- Make provisions for city and regional transit service and amenities (including the Northstar Line) and encourage their use
- Tie into, and improve, the Ramsey utility network
- Integrate with and complement the existing (and future) street framework
- Become an integral part of the city and county drainage/stormwater management plan

Objectives

- Overall COR design (and all new private development within The COR) will accommodate stormwater from new projects and from off-site, and highlight stormwater features as an amenity
- Bikeways and pedestrian routes into The COR from adjacent neighborhoods must be designed for safety and ease of access, suggesting that a person on a bicycle has an equally accessible route to downtown.
- Parks and open space will be easily accessible to all COR residents, visitors, people who work here and also for the citizens of Ramsey and the surrounding area. This connected green system is reminiscent of the world renowned 'Grand Rounds' of Minneapolis.

- DEVELOPMENT STATUS**
- EXISTING DEVELOPMENT
 - PROPOSED DEVELOPMENT
 - ACTIVE DEALS
 - UNDER CONTRACT
 - COMPLETED

ACCESS

- EXISTING SIGNALIZED INTERSECTION
- FUTURE SIGNALIZED INTERSECTION
- FULL INTERSECTION
- NO LEFT OUTBOUND MOVEMENTS
- PARKING RAMP

TRAFFIC INFORMATION

ADT INFORMATION TAKEN FROM 2009 ACTUAL COUNTS AND 2030 PROJECTED VOLUMES



DEVELOPMENT PLAN 6.1

09/06/2012
 LandForm and Site Plan - an unincorporated subsidiary of National Services, LLC

Overall Framework – Street Hierarchy

Overview

The Development Master Plan defines a specific strategy for the layout of streets and blocks within, and around The COR. The size, type and configuration of this street hierarchy (See Figure 1: Street Hierarchy) is based on a combination of projected traffic volume, level of service at intersections, proposed adjacent land use (destination commercial, residential, civic, recreation, etc.) and desired aesthetic character.

Great streets are essential components of an attractive and inviting public realm. These guidelines promote a street system that balances pedestrian and vehicular use. Providing streets that support traffic flow is important, but also creating ample space for street-side activities, strolling promenades, pockets of green and other amenities is critical to the success of The COR. Streets will promote convenient access by car – to storefronts, to parking lots and ramps, to entertainment attractions – but will also encourage foot traffic as a primary way to move about and conduct business in the core area.

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, not allowing dead end streets or cul-de-sacs, in favor of a traditional grid pattern and connected streets including the following:

- Provide a street network that distributes pedestrian and vehicular traffic throughout the various districts in The COR and encourages a more dispersed and less congested traffic flow (many choices or travel paths).
- Provide a clear hierarchy of streets resulting in a simple and understandable system for movement – easy access and convenient circulation throughout The COR.
- Work with Anoka County to convert primary perimeter streets (arterials such as Ramsey, Armstrong and Bunker Lake Boulevards) to urban street sections, providing sidewalks and street trees to replace roadside ditches.
- Promote improvements that invite pedestrian and bicycle use including wide sidewalks, streetscape amenities and designated bicycle trails (both on and off-street).



Objectives

Creative design solutions for public realm improvements should reflect these basic street classifications (street hierarchy) including:

- Arterial street – these streets provide the boundary of The COR and all access into The COR will be from one of these three streets:
 - Bunker Lake Boulevard
 - Armstrong Boulevard
 - Ramsey Boulevard
- Destination street – signature addresses for shopping, entertainment, services, work place and other primary amenities
 - Sunwood Drive



- Parkway – premier ‘green streets’ adjacent to primary public park / green space, with more landscaping than other streets in The COR
 - Ramsey Parkway
 - Center Street (north of the roundabout)
- Connector street – primary routes that link various districts within The COR, usually providing the most direct means of getting from point A to point B
 - Rhinestone Street
 - Zeolite Street
 - Veterans Drive
- Downtown street – the most urban of street sections where high pedestrian traffic is intended and traffic calming measures such as on street parking, bump outs and other methods will be employed
 - Center Street (south of the roundabout)
 - Sapphire Street (north of Sunwood)
 - Other streets as shown on the Street Hierarchy Map
- Local street – neither destination nor primary connector routes, these streets provide much of the rest of the street grid throughout The COR.



These street hierarchy classifications are intended to provide guidance regarding the streetscape, but it should be noted that within a street classification, there may be construction or design distinctions, based on the sub-district classification. For example, Sunwood Drive is defined as a destination street throughout The COR, but the design may be different between the COR1 and COR2 sub-districts. The Master Streetscape Map will provide design direction.



Figure 1: Street Hierarchy Map

Transit Overview

The COR is designed to be a Transit Oriented Development (TOD) in a variety of ways that go beyond providing a walkable public realm and connected street grid. The Master Plan also includes a multimodal transit station, with dedicated commuter parking, integrated with other uses at the heart of the retail, restaurant and entertainment district. This station functions as a hub where local circulator transit comes together with regional transit service and the Northstar Corridor. The COR is the Twin Cities' first and only transit-oriented development along the new Northstar Commuter Rail Line. Ramsey Station at The COR will open for riders boarding the train in November 2012. As Northstar's most visible station, The COR will immediately bolster the ridership on the line and continue to build recognition for this regional service.



Transit components include the following:

- Ramsey Star Express service provides peak period, peak direction bus service between downtown Minneapolis and Ramsey until the Northstar Commuter Rail station stop is complete
- Northstar Commuter Rail provides train service between Big Lake and downtown Minneapolis
- 350 park and ride parking spaces are provided for commuters adjacent to the transit station
- Pedestrian and bicycle connections are emphasized in the Master Plan as major links between the transit station, various districts within The COR and into the surrounding neighborhoods
- The COR can accommodate Metro Mobility, citywide circulator bus or town trolley connections as part of local and regional service to the transit station



The COR also encourages developers to participate in a Travel Demand Management (TDM) program to both assist and encourage residents and visitors to take advantage of the various modes available for their commuting and travel needs. The TDM program draws from incentives and services available through Anoka County and Metro Commuter Services that match riders with carpools and vanpools, provide discounted transit passes and manage work hours among other functions.

- Developers shall submit a TDM plan with their site plan application for new development and work with the City to implement their plans.

Transit Guideline Recommendations

Transit is an important component of the overall plan, with expectations for more ridership, using a greater range of transit options as they become available. More people, together with the mix of activities and amenities in The COR, encourages better designed, better connected transit service. It is a shared responsibility between city, county and developer to promote transit use, with both policy and bricks-and-mortar attention to the following framework:

- Understand and take advantage of opportunities to use existing and proposed transit components

- Provide transit related facilities specifically linked to uses and character within each COR district, including signage and lighting for way-finding and bicycle amenities
- Building future Northstar Commuter Rail riders through the Ramsey Star Express bus service

Transit Objectives

The primary objective is to provide a balance of transit service, bicycle and pedestrian connections and lanes to move traffic. The framework for The COR promotes public transportation as part of the solution and suggests a variety of street improvements and other amenities to invite increased use. Photographs throughout the framework suggest some of the many possibilities.

Overall Framework - Streetscape

Overview

The Master Plan defines a specific strategy for the layout of streets and blocks within and around The COR. The amenities within the public right-of-way -and immediately adjacent -play an important role in the aesthetics of The COR. Great streets are essential components of an attractive and inviting public realm. Developing a streetscape that is comfortable and inviting to pedestrians is important to realizing the vision for The COR. The framework will encourage interesting and inviting places for the public.

The Streetscape framework defines the visual character and physical improvements for all public spaces within The COR. The document also provides the general framework and examples for design of proposed quasi-public and private space located within each development parcel.

Public realm/streetscape framework focus on the following elements:

- Parks, plazas and other public gathering spaces
- Street/sidewalk character and streetscape elements
- Bicycle/pedestrian connections (on-street and off-street)
- Residential courts, pocket parks and other private green space



The goal is to foster a safe and interesting public realm that will invite pedestrian activity, promote traffic calming, increase transit use and encourage community gathering. Street trees, lighting and decorative fencing provide the backbone for streetscape improvements. Added detail, such as special pavements, street furniture, public art and layered plantings will highlight specific nodes or uses. The City has developed a series of street cross -sections to illustrate these design expectations. Typical street sections are provided in Appendix A as a reference.

Destination and Connector streets must balance a high level of service for the automobile with attractive and inviting amenities for the pedestrian including:

- Provide gracious sidewalk width to support intensity of pedestrian traffic in the core
- Install street trees to frame the street and provide shade
- Provide street lighting that meets all safety standards and design criteria, while creating a signature character for this district
- Explore a range of options for streetscape improvements including special pavements, interesting concrete tinting or scoring patterns, additional plantings, ornamental fencing and other features (note, however, that simpler is better and too much clutter is a negative)



The COR includes a network of bicycle/pedestrian trails and walkways (both on and off-street) that connect adjacent sub-districts to the Mixed Use Core. As you approach the core, these connections tend to become part of the shared right of way along the major streets, or become part of the urban park spaces. Bicycle riders may choose to share the street with vehicular traffic, or dismount and walk their bicycles along with the other pedestrians using the sidewalks. Providing and maintaining these bicycle and pedestrian connections should be a key component for all development projects in the core.

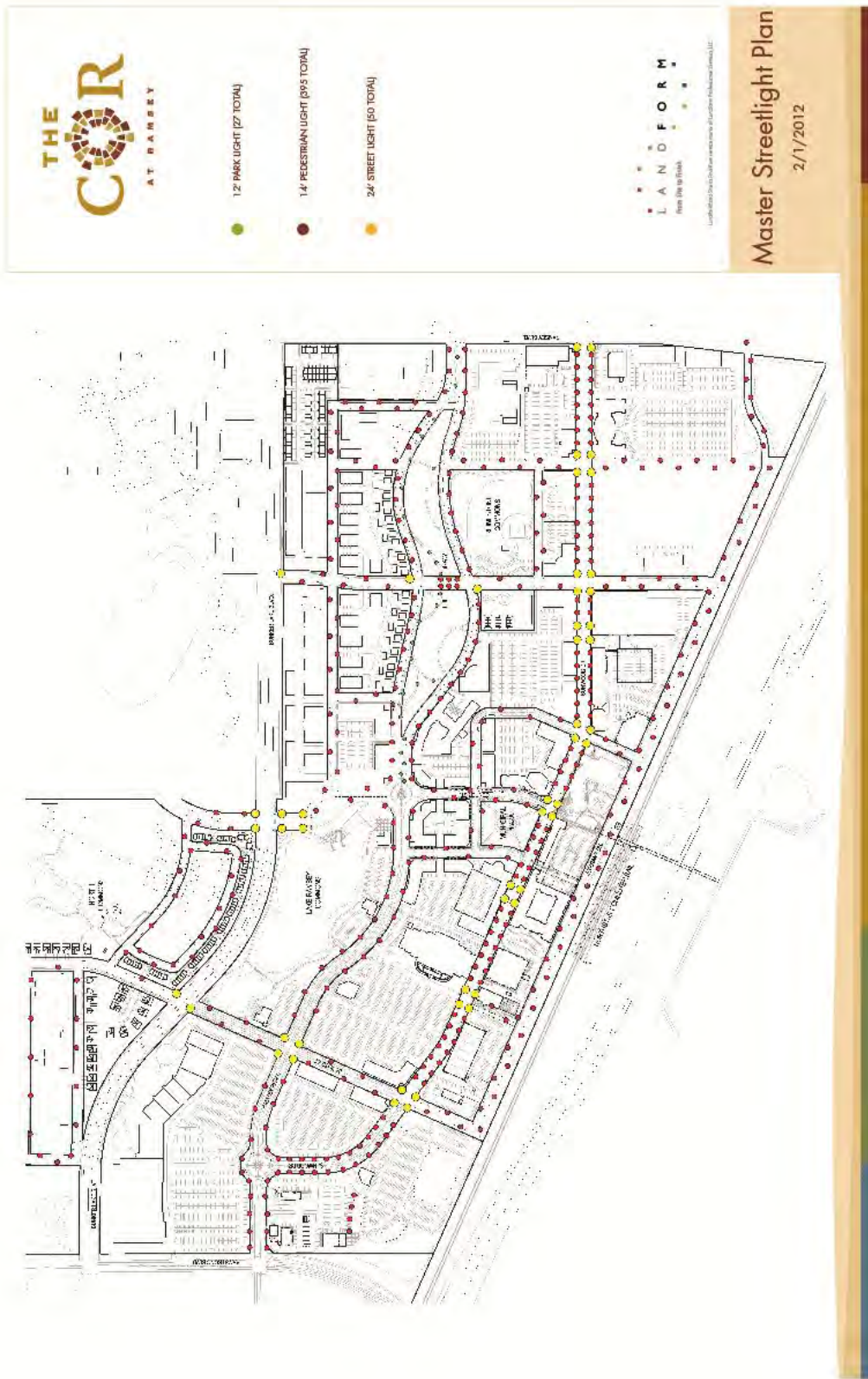


Figure 2: Master Lighting Plan

Guideline Recommendations

The proposed street and block pattern reflects a compact urban character, including the following:

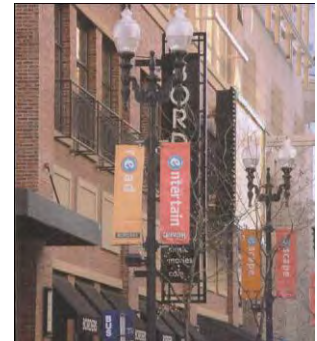
- Bike racks interspersed throughout The COR to encourage cycling as an alternative mode of transportation.
- Benches on the public sidewalks to encourage people to linger in The COR
- Trees and planters along the streets to create an inviting space
- Wide sidewalks and both on- and off-street bicycle trails
- Kiosks and other way finding tools for visitors and residents in The COR
- Outdoor dining is encouraged on the street side of buildings within all districts.
- Public plaza space is strongly encouraged to be located adjacent to the streets



Objectives

Creative design solutions for streetscape improvements should include:

- Streetscape improvements that provide traffic calming, particularly along Destination and Downtown streets
- Inviting streets that draw you into and through The COR
- A marriage of building design and streetscape design that encourages active spaces near the streets



Street Trees and Plantings

Street trees and planter boxes are a critical element in the unique character of The COR and will be used to help distinguish between different street types and sub-districts within The COR. However, there may be variety within the same street when the street crosses into sub-districts.

- Street trees will be chosen to ensure that the species allow for clear pedestrian access along the streets and require minimal maintenance.
- Along Destination, Downtown, Parkway and Local streets, where regularly spaced blocks exists, one (1) tree species shall be provided per block. In order to provide variety and protect against disease, adjacent blocks shall not be allowed the same street tree. Initial development along a block shall establish the tree species for that block, subject to City review and approval.
- Along streets without regular block space or longer blocks, such as Connector and Arterial streets, the City may approve groupings of 3-6 trees of one (1) species in lieu of the regular block spacing.
- Alternatives to the above mentioned planting plan shall be subject to review and approval by the City Council.

Destination Street Trees:

- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Bicolor Oak (*Quercus bicolor*)
- Sienna Glenn Maple (*Acer x freemanii* 'Sienna')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Ginko (*Ginkgo biloba*)
- Accolade Elm (*Ulmus x 'Accolade'*)

Parkway Street Trees:

- Valley Forge Elm (*Ulmus americana* 'Valley Forge')
- Bitternut Hickory (*Carya cordiformis*)
- Siouland Poplar (*Populus deltoides* 'Siouland')
- Sienna Glen Maple (*Acer x freemanii* 'Sienna')
- Boulevard Linden (*Tilia americana* 'Boulevard')
- Northern Catalpa (*Catalpa speciosa*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Korean Mountain Ash (*Sorbus alnifolia*)
- Ohio Buckeye (*Aesculus glabra*)
- Ginkgo (*Ginkgo biloba*)

Connector Street Trees:

- Bicolor Oak (*Quercus bicolor*)
- Northern Red Oak (*Quercus rubra*)
- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Downtown Street Trees:

- Princeton Elm (*Ulmus americana* 'Princeton')
- Accolade Elm (*Ulmus x 'Accolade'*)
- Autumn Blaze Maple (*Acer x freemanii* 'Jeffersed')
- Skyline Honeylocust (*Gleditsia triacanthos var. inermis* 'Skyline')
- Kentucky Coffeytree (male) (*Gymnocladus dioicus*)
- River Birch (single stem) (*Betula nigra*)
- Black Alder (*Alnus glutinosa*)
- Black Cherry (*Prunus serotina*)
- Spring Snow Crabapple (*Malus* 'Spring Snow')
- Kelsey Crabapple (*Malus* 'Kelsey')
- Japanese Tree Lilac (*Syringa reticulate*)
- Thornless Cockspur Hawthorne (*Crataegus crus-galli var. inermis*)

Local and Arterial Street Trees:

Local and Arterial Street trees may be chosen from any species on the above lists.

Planter Beds/Boxes/Tree Grates

- Planter beds, tree grates and planters shall be as shown on the Streetscape Master Plan. Details and specification shall be added to Appendix A as developed and approved by the City. Appendix A shall be amended as needed.
- Irrigation shall be provided to ensure survival of plant materials. The City shall provide irrigation systems along the Parkway and Destination streets. Unless otherwise approved, all other irrigation shall be the responsibility of the landowner.

Street Furniture

Bike racks, trash enclosures and benches are encouraged on all streets, but will be required on Destination, Connector and Parkways and other streets as shown conceptually on the Streetscape Master Plan.

Bike racks shall be the standard bike rack (see Appendix A for specifications).

Trash enclosures shall be the standard trash enclosures (see Appendix A for specifications).

Benches shall be the standard bench (See Appendix A for specifications).

Benches shall be grouped in twos that face each other and provide an opportunity for an outdoor conversation space.

Alternative bike rack, trash enclosures and bench designs will be considered on a case-by-case basis. The City will review alternative designs as part of the site plan review and will evaluate these alternatives based on the following:

- Does the design maintain the character of the district?
- Does the alternative design maintain the quality of the standard feature?
- Does the design provide a relationship between the public elements of the streetscape and the building that would not otherwise be possible?



Sidewalks and Boulevard

In the design framework, the term “boulevard” is defined as “the space between the sidewalk and the curb.” The boulevard area provides the required space for public features such as street lights, benches, fire hydrants, street signs, etc. The boulevard will have different characteristics based on the street hierarchy.

The street width and layout within different zones may affect the ability of a particular development to provide the minimum boulevard width and the minimum sidewalk width within the existing public right-of-way. In those cases, the landowner shall provide a permanent public easement over the portion of the sidewalk that is located on private property.

Street Lights

Street lights are owned and installed by Connexus Energy. The streetlight design was developed in conjunction with the City of Ramsey and Connexus (See Figure 2: Master Lighting Plan). The standard street lights are designed to accommodate banner arms should banner signage be desired. The banner arms should be the manufacturer's standard break-away banner arms to ensure safety and minimize maintenance costs. Connexus provides street light maintenance for the City. Connexus bills the City of Ramsey for the cost of installation and maintenance.

The choice of street light was intended to emphasize the pedestrian scale of The COR. Light poles within the public right-of-way should be planned to accommodate banners, flowers, hanging artwork and electrical outlets.

There are three (3) different streetlight types within The COR:

- 24-foot high poles at intersections
- 14-foot high poles at mid-block
- 12-foot high poles at The Draw



The City will ensure that the appropriate streetlights are installed on all public streets as described by the Design Framework. Private developers shall provide parking lot or other project lighting that retains these design

objectives. All lighting shall comply with City Code performance standards for lighting. All lighting shall be reviewed by City staff to ensure compliance with the Design Framework and City Code standards described above.

Standards for Outdoor Dining.

Outdoor dining is encouraged as an attractive addition to a pedestrian- and retail-friendly, vital COR. Such dining is encouraged subject to the standards in Section 117-118 of the Zoning Ordinance.

Overall Framework - Parking

Overview

Balancing the realities of car-related necessities (streets, parking and so on) with future transit service and the desired character of The COR is a significant challenge. The goal is to provide adequate parking; both quantity and location, while promoting new strategies that support the overall needs of The COR. Parking will be provided through a combination of on street spaces, off-street surface lots, underground lots and structured ramps to support the mix of uses at the core.

Parking structures should contribute to the overall character and image of The COR. Ramp design should include elements, colors and materials that reflect, or complement surrounding buildings. Liner stores or other street level uses should be integrated into ramp design along important street frontages. The City also encourages developers to wrap the ramp with multi-story uses that will provide commercial and residential opportunities and help to define the street edge.

Guideline Recommendations

Parking guidelines encourage the following:

- Locate surface parking lots away from the major streets, either behind or to the side of primary buildings; surface parking lots along major street frontage are not allowed in the core area
- Minimize surface parking lots in favor of other solutions including underground parking to serve residential buildings, on-street parking to serve retail shops and parking ramps
- Provide a combination of fencing, landscaping and landform to screen parking areas from major streets and important views, soften parking area edges, provide shade, integrate native plantings, offset islands and reduce to 'sea of parking' image
- Create strategies for shared parking between adjacent uses, taking advantage of peak and off-peak cycles, business hours, nighttime activities, special events and other needs
- Provide alternative surfaces for secondary (less frequently used) parking areas including porous pavements, green pavements and so on
- Provide facilities or services that respond to and connect with future transit to reduce required parking, including shuttle/circulator bus, bicycle lockers
- Provide flexibility including hours for on-street parking, security and enforcement practices, permit or metering
- Provide parallel parking on both public and private streets throughout The COR, to add parking and create a traffic calming, pedestrian buffer; Bumpouts are encouraged as a traffic calming measure on all streets within the COR.
- Explore options to integrate stormwater management requirements into parking area design
- Provide parking spaces for typical daily use, satisfying both quantity and location needs through a mix of on street, small surface lot, underground and ramped parking solutions
- Provide options for additional/overflow parking to support holiday traffic and special events including porous pavements, turf parking areas (Netlon and other soil amendments) or shuttle service from nearby parking areas
- Encourage shared parking solutions that target around the clock shifts in destination, audience or take advantage of peak/off-peak hours of operation for various uses within the core area

- Promote municipal parking ramps as the primary facility to meet public parking needs within the core area
- Discourage driveway access to parking facilities along Arterial, Destination and Parkway streets, in favor of side streets.

Objectives

The primary objective is to provide a balance of surface lots, on-street and structured parking, with ample quantities and close proximity to serve the mix of uses in The COR. The Design Framework anticipates the construction of a total of 3 parking structures in COR1 to serve the needs of the area by providing structured parking that can be shared among users, allow developers to maximize the development on each individual lot.

As discussed later in this section, sizing and timing of the structured parking will be finalized as the area develops and parking demand grows. In addition to providing adequate space, excellent design is a major factor as well. The framework for The COR encourages creativity, innovation, quality and attention to detail in every aspect of project development, including parking solutions. Photographs throughout the Design Framework suggest some of the many possibilities.





Figure 3: Streetscape Master Plan

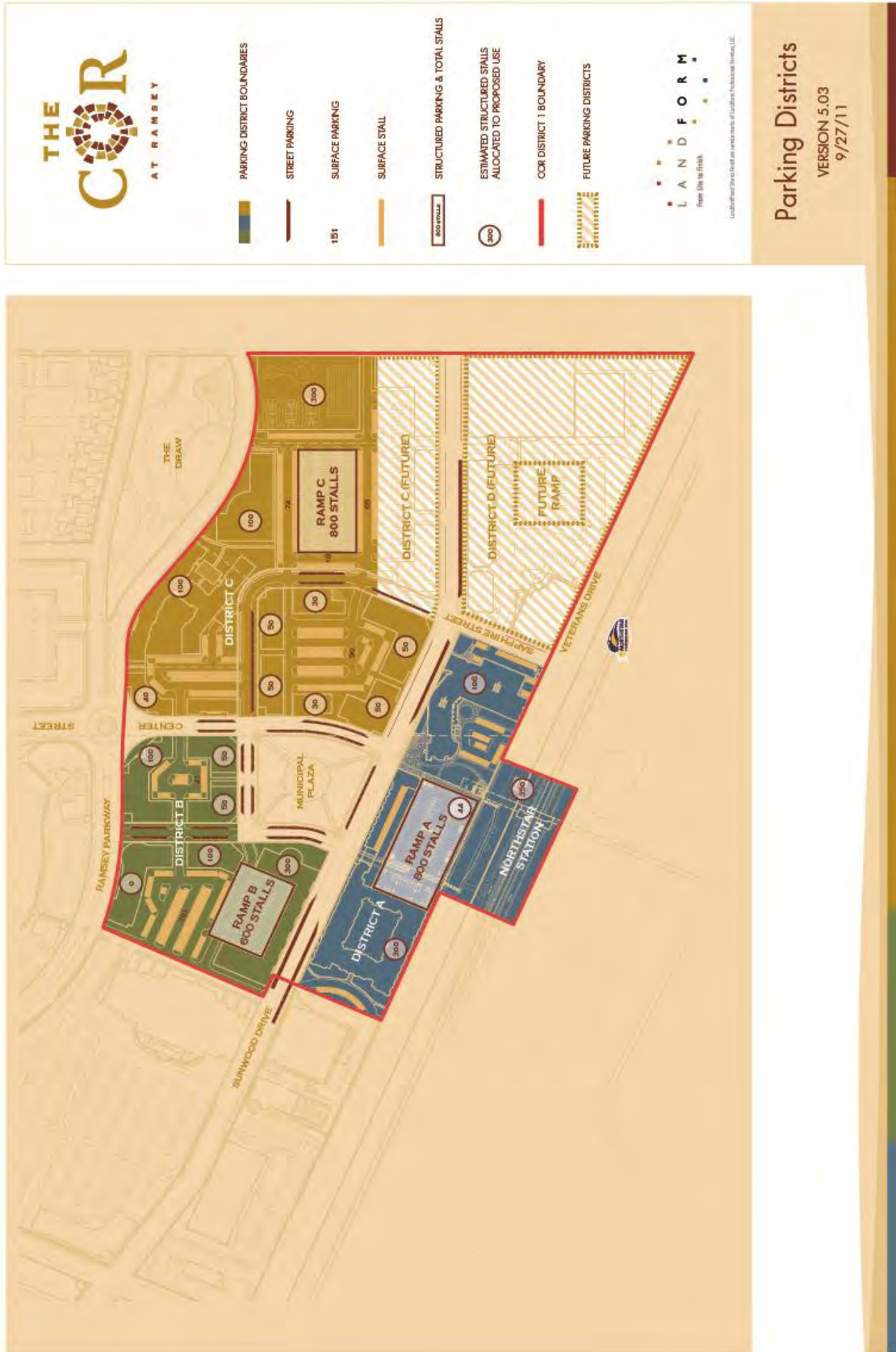
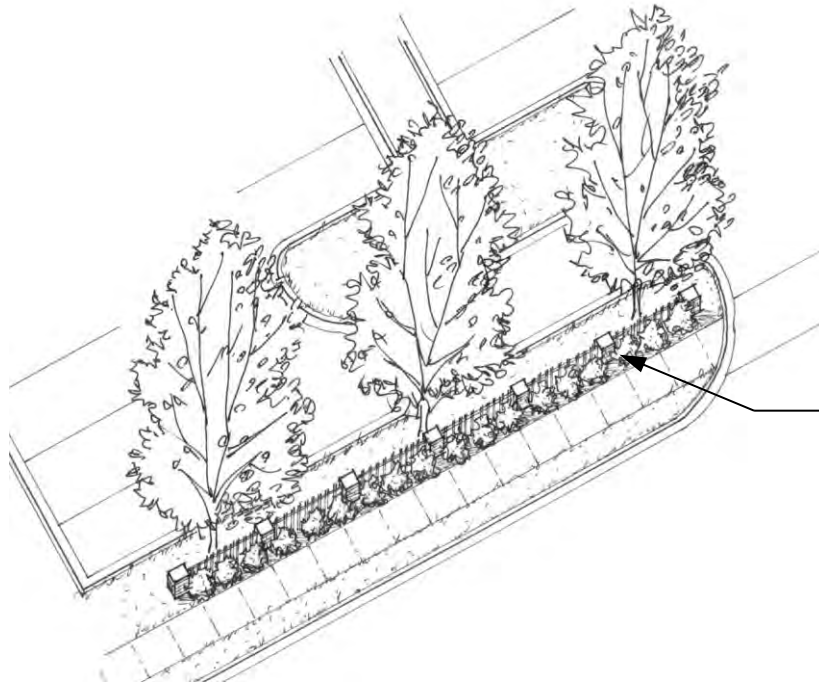


Figure 4: Parking District Map

Minimum Standards

Parking in The COR is intended to be shared to the greatest extent practicable in all mixed-use areas. The parking standards are intended to provide a practical basis for providing adequate parking within the COR District through a careful analysis of uses, shared parking arrangements, use of public street parking and reduction allowances for the proximity to the transit station. A parking plan has been prepared to identify the planned number, location, sharing arrangements and public use (see Figure 3: Streetscape Master Plan and Figure 4: Parking District Map). The parking plan provides information that can be used to help address longer term parking arrangements where the possibility of use conversion or building expansion exists. Parking shall be provided under any of the following arrangements:

- A. No parking shall be located between the front of the principal structure and the street on parcels abutting Destination and Downtown streets in COR1.
- B. Not more than fifty (50%) percent of the parking shall be located in front of the principal structure and the street on parcels abutting Arterial, Parkway, Connector and Local streets.
 1. In the COR2 district, developments may be allowed more than fifty (50%) percent parking in the front yard, provided all other design standards have been met, including the use of a wall, railing, hedge or combination to create a street edge and screen parking.
- C. Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4¹/₂') above the level of the parking lot, at the build-to line required for the property. The amount of required screening may vary by sub-district and street frontage and is described in Part Two of this document.



Landscaped edge that combines fence with trees, low shrubs and ground cover for parking lot screening and buffering along sidewalk to screen parking

- D. No parking spaces shall be located on corner lots at the point of street intersections.
- E. Shared parking in COR1 will be accomplished through a Parking Use and Maintenance Agreement (PUMA).
- F. Off-street parking in the COR2 and COR3 sub-districts shall be secured for public use through parking easements and other appropriate conveyances. Shared parking arrangements between nearby uses are encouraged in both sub-districts.
 - 1. The City may approve joint parking for one or more businesses where the total number of parking stalls provided for joint use is less than the sum of the total required for each business should they provide them separately. The applicant must demonstrate the feasibility of the arrangement in a written report. Such a permit shall not be granted except when the following conditions are found to exist:
 - i. Proximity. The building or use for which application is being made to utilize the off-street parking facilities provided by another building or use shall be located within three hundred (300') feet of such parking facilities, excluding public rights-of-way.
 - ii. Conflict in Hours. The applicant shall demonstrate in documented fashion that there is no substantial conflict in the principal operating hours of the two (2) buildings or uses for which joint use of off-street parking facilities is proposed.
 - iii. Written Consent and Agreement. A legally binding instrument, executed by the parties concerned, for joint use of off-street parking facilities, duly approved as

to title of grantors or lessors, and in a form and manner of execution approved by the City Attorney, shall be filed with the City Clerk and recorded with the Anoka County Recorder or Registrar of Titles, and a certified copy of the recorded document shall be filed with the City within sixty (60) days after approval of the joint parking use by the City or the interim use permit shall be considered null and void.

- G. On-street parking adjacent to buildings shall not be used for the purposes of calculating parking requirements.
- H. Within the COR-4 sub-district, parking on individual parcels serving individual uses may be provided if designated and approved as part of the master plan.
- I. Within all sub-districts, a minimum driveway length of twenty-five (25') feet is required to limit conflicts between vehicles parked in the driveway and street or sidewalk users.
- J. In order to ensure the pedestrian orientation of the COR Districts, minimum/maximum on-site parking standards are set based on the following table:

Table 1

Use	Minimum number of parking stalls	Maximum number of parking stalls*
Retail	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Restaurants	1 per each 2 seats	1 per each 1 seats.
Offices	2 per 1,000 sq. ft.	3 per 1,000 sq. ft.
Medical offices, clinics	2 per 1,000 sq. ft.	4 per 1,000 sq. ft.
Health clubs	3 per 1,000 sq. ft.	5 per 1,000 sq. ft.
Theaters, places of assembly	1 per 5 seats	1 per 3 seats
Residential		
Attached or detached	1 per unit	2 per unit
Multifamily units	1 per unit	2 per unit

*If a parking structure is provided on site, maximum parking stalls do not apply.

- K. Non-Specified Uses. For uses not specifically listed above, off-street parking requirements shall be computed by the Zoning Administrator on the same basis as required for the most similar listed uses. In such cases, the Zoning Administrator shall also consult off-street parking reference materials including, but not limited to, manuals prepared by the American Planning Association and Institute of Transportation Engineers.
- L. Handicap Parking. The size, number, and location of stalls reserved for handicapped parking shall be provided and identified as required by applicable regulations. These spaces are included in the computation for the minimum parking space requirement.
- M. Change in Land Use. When the site intensity or use of a building and/or property is increased with consequential effect upon the parking requirements as prescribed in this Section, the

parking requirements as prescribed herein shall be used to provide for such increase in the site intensity and/or use.

- N. Use of required parking spaces. Required parking spaces must be available for the use of residents, customers or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.
- O. Transit Service Reduction. The minimum number of parking stalls required on site may be reduced by 10% for any parcel located within ¼ mile of a transit stop. To qualify for this reduction, the transit stop must provide regular service on all days of the week and direct pedestrian access must be provided between the building and the transit stop.
- P. Bike Racks. In addition to the bike racks provided as part of the streetscape, individual businesses are encouraged to provide bike racks for customers. Bike rack locations will be reviewed and approved by the City as part of the site plan review.
- Q. Increase to Maximum Required Parking. If additional parking is sought to exceed these maximum requirements, a conditional use permit can be sought to increase maximums up to twenty-five (25%) percent.
 - 1. In the COR-2 sub-district, an increase of twenty-five (25%) percent in the maximum number of parking stalls is permitted without the issuance of a conditional use permit if approved as part of the site plan.
- R. In the COR1 sub-district, a PUMA (Parking Use and Maintenance Agreement) shall be applied to all development as follows:
 - 1. It is the City's intent to maximize development on individual lots. In order to help landowners accomplish this goal, the City will develop public parking ramps, which will be available for public parking within The COR. In order to minimize the amount of the site required for parking, the landowner may choose to enter into the PUMA for utilization of the public parking ramps to meet all or a portion of their minimum parking requirements.
 - 2. Each development shall be subject to a PUMA that specifies that each property in the COR1 shall be financially responsible for its proportionate share of a shared parking facility. The proportionate share shall be determined on the basis of the property's off-street parking needs, as determined by the parking requirements of Item J above.

3. The City shall establish fees by ordinance. The developer shall be responsible for payment in an amount equal to the value of the required parking on a per-stall price basis for ramp parking through the PUMA. The City will determine the appropriate contribution.
4. Funds collected by the City shall be deposited in a special fund used only to acquire and/or develop off-street parking facilities for the COR. Financial responsibility shall cover the construction and continuing maintenance of the parking facility.
5. The parking facility may be constructed and maintained by the City or by a private management entity acceptable to the City.

Private Streets

Private streets may be allowed within residential subdivisions, subject to the standards in Section 117-112 (e)3.b. of the Zoning Ordinance.

Loading Areas

- A. Loading areas and docks shall be located in the rear of the building.
- B. Loading areas shall not be located in the front yard and shall be fully screened from public streets, public parks and residential areas through landscaping and building design measures.



This page intentionally left blank.

Overall Framework – Building Design

Overview

The COR Development Plan 5.03 identifies the preferred mix and approximate location of a variety of land use types. Together with the Zoning Code, this Design Framework focuses on the following overall objectives:

- Promote vertical and horizontal mixed use (within a building, within a block, within a district)
- Provide a variety of housing types, styles, pricing
- Encourage increased density in and around the central core area
- Promote an interesting mix of building styles, scales and massing for each sub-district
- Support creative, innovative, high quality design solutions as the benchmark for success
- Integrate mix of uses with public green space within each district
- Provide a variety of commercial types
- Mix residential and commercial uses to promote street-level activity throughout the day

Guideline Recommendations

The mix of land uses in The COR will encourage activity during the day and into the evening and nighttime hours. As a focus of commerce, employment, recreation and housing, The COR resembles a small to medium size Midwestern downtown in scale and character. Land uses are intentionally mixed, to bring shoppers and workers into the area during the day and attract new residents and visitors to entertainment venues in the evening.

The civic component of The COR, anchored by City Hall, Police Station and variety of public spaces and squares, adds an important element to the land use mix and brings a level of authenticity to the district.

Objectives

- Encourage vertical mixed-use in the core with housing and office space over retail uses along the street, particularly in the center of the district.
- In residential districts, particularly closer to the core, a limited amount of retail uses will be encouraged to service residents and provide nearby places to gather.
- Promote innovative models for housing that include a mix of townhomes, flats and lofts within single buildings or on the same block.
- Accommodate larger, ‘big box’ retailing and auto-oriented uses within the COR2 sub-district only if innovative design approaches are used including two story buildings, use of liner stores facing the street, minimal blank walls, creative parking strategies, etc.
- Where similar uses create a ‘family’ or campus of buildings (e.g. medical campus), buildings should reinforce the street edge and parking should be set within the core of the block.
- Uses shall promote pedestrian friendly streets, contributing to street level activities and overall visual character of each district.
- All uses should reinforce the street edge.



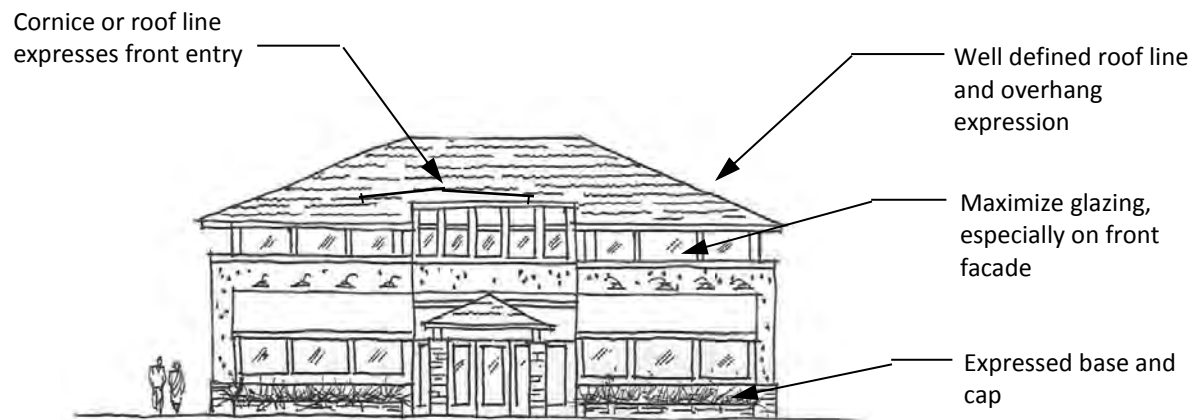
Architectural Overview

High quality architecture is an essential component in fulfilling the objectives of the Design Framework. Rather than prescribing a particular style, these architectural guidelines are intended to encourage an integration of both traditional and contemporary design. Architectural designs should provide a consistent quality, measured, to a great degree, by the pedestrian experience along the street and by an architectural expression that provides character without being thematic, obtrusive, or artificial. Architectural character should strive to be authentic and varied, but not 'thematic'.

Architecture Recommendations

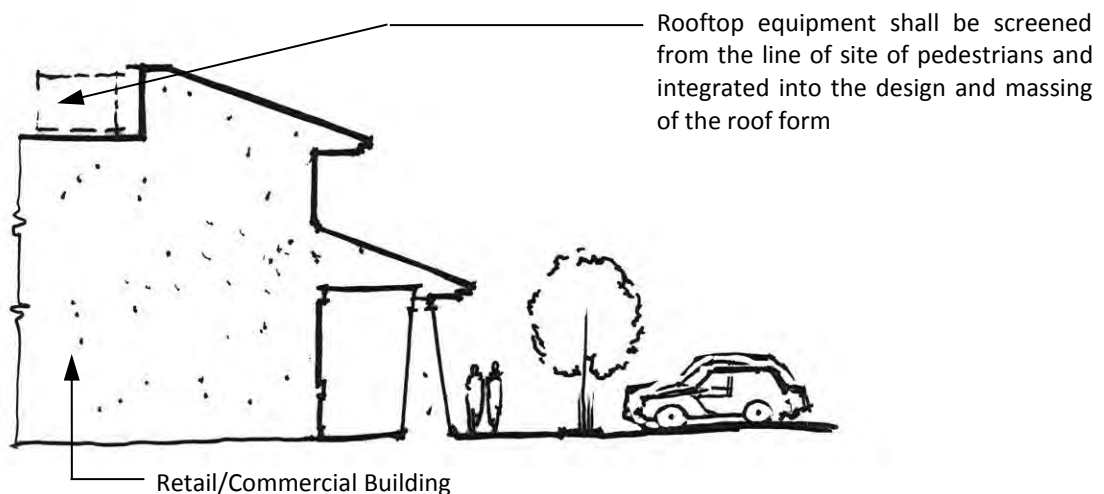
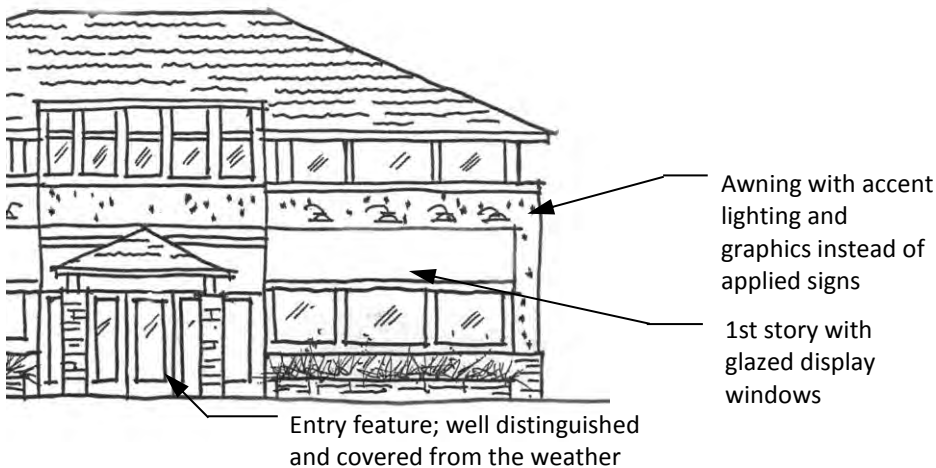
Of all the layers that combine to form The COR Development Plan, architecture will typically be the most prominent development component. Both visually and physically, architecture will play a major role in defining the overall design character and mix of uses for The COR. It is crucial that the design and location of buildings address these architectural guidelines, with specific emphasis on the following:

- An animated street presence with a mix of street-level uses, interesting building façades, many doors and windows on the street, careful design of lighting, awnings, signage and other elements that animate the pedestrian experience

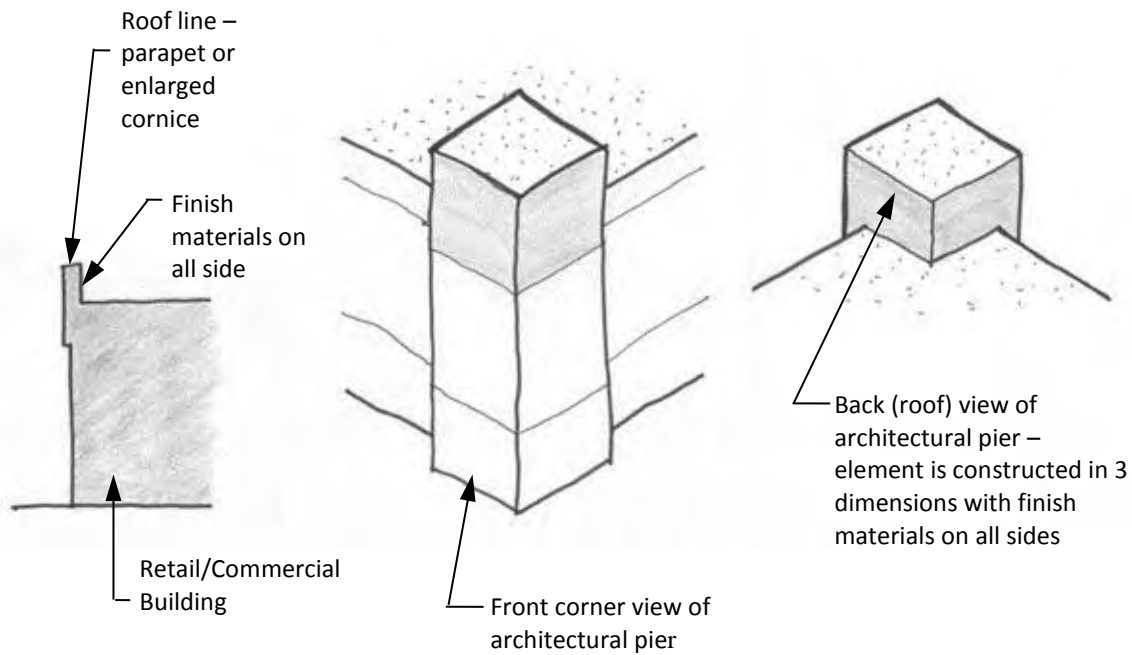


- Variety in building design, character and expression; not one theme or building style, but a thoughtful integration of many design solutions
- Variety of building types including a range of height, scale and proportion, that support an integrated mix of housing options, shopping destinations, entertainment venues, civic structures and other COR uses
- Use of high quality materials, suggesting a preference toward native materials and masonry elements, with respect for local building techniques
- Building location is as important as building style; special architectural elements, character, transparency and a higher level of materials and detailing should be used to highlight key streets and important crossroads throughout The COR
- Buildings should always be located at all four corners of intersections.
 - In the COR2 sub-district, if the City finds that there are unique circumstances where buildings cannot be located on the corner, the City may approve the use of fences, walls or other design elements to create that "street edge"

- Buildings will be located at the street right-of-way line (zero setback), but also allow recessed space for outdoor dining and other street level activities providing some minor deviation for variety
- Greater emphasis should be given to architectural elements, materials and other design features for buildings located at primary intersections, or where streets intersect with parks, such as the streets that front on or end near the Municipal Plaza. These locations can often be seen from several directions, perhaps terminate a view from a distance down the street or sit at a prominent node; They will likely become future landmarks or helpful orientation/wayfinding icons as The COR develops
- Encourage both vertical mixed use (within each building) and horizontal mixed use (within each block) throughout this district
- Include a mix of housing, civic, retail, restaurant, entertainment, performance and other uses that encourage a variety of activities throughout the day.
- Minimize openings between buildings, although limited driveway access and some provision for pedestrian connections through blocks are encouraged – gaps between buildings to accommodate surface parking lots, greater building setbacks or other purposes are discouraged
- Use recessed entries and windows to create street-level interest, variety and enhance pedestrian scale along street frontage



- Equipment, mechanical systems, transformers, etc. must be screened from view in a way that is integral to the architecture of the building. Screening should be accomplished with materials similar to or compatible with materials used on the main structure; metal fencing or mechanical vent screens alone are not sufficient.
- Roof lines and cornice details shall be completed in a 3-dimensional manner so that the back of roof features or similar unfinished areas are not visible.



Overall Framework - Signage

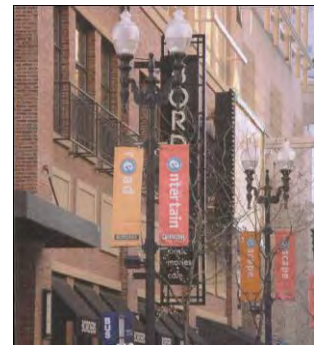
Overview

Signage within The COR should provide a system for clear wayfinding for all modes of transportation and should ensure successful business operation while maintaining the design aesthetic of this unique project. These sign standards are intended to allow flexibility and creativity while maintaining the design concepts of The COR. Due to the distinctive nature of The COR, the sign standards from the City Code are not applicable and signage shall be allowed as prescribed by the Design Framework. A Master Sign Plan identifies the location of key sign locations (see Figure 5: Signage Plan).

Guideline Recommendations

The COR development manager shall prepare a Master Sign Plan for the public elements of the project, which are in compliance with the design framework and will be adopted by reference. The Master Sign Plan will specifically address public signs including:

- The three (3) off-site community signs
- Banner signs on light poles
- Signage for public parks
- Signage for public parking ramps
- Signage for public buildings
- Community kiosks



Objectives

Signage should be used as a tool to help identify businesses and neighborhoods with The COR and should have elements that are focused on the pedestrian nature of The COR as well as the signage needs for businesses.

- Signage and lighting which is integrated into the design of the building is encouraged.

Definitions

Awning Sign means a sign incorporated into or attached to an awning.

Community sign means an off-site sign identifying the development name and key tenants. Community signs may also include public information. Reader boards are allowed to be incorporated into community signs within The COR. Gateway and kiosk signs are part of this category.

Temporary Sign means a sign which is erected or displayed for a limited period of time and not affixed to a Minnesota State Building Code approved structure.

Project sign means a freestanding area identification sign which identifies a single-family or multifamily residential subdivision, a commercial development or an industrial park or office park and which is located on the same site as the development it identifies. A site shall be identified as the block in which the use is located.

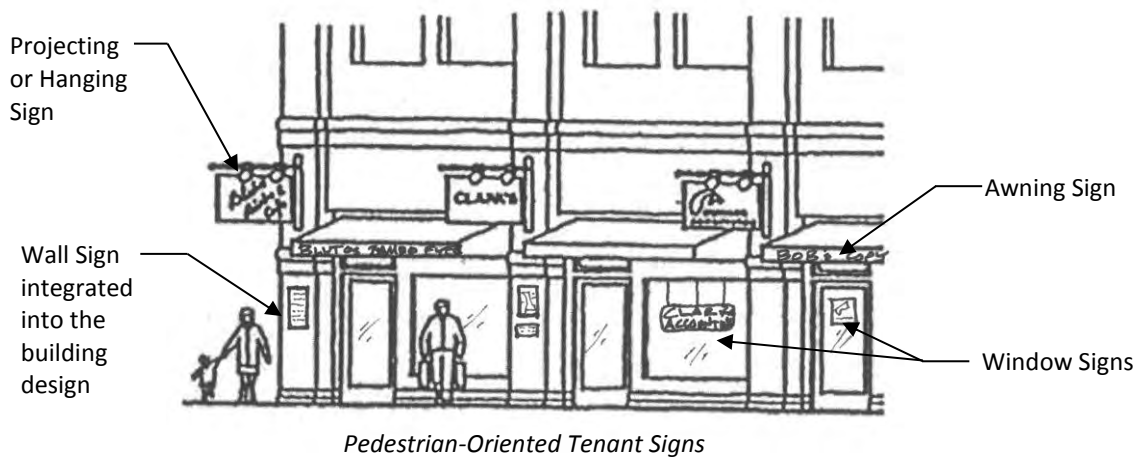
Projecting sign means any sign, all or any part of which extends beyond the surface of the building or wall by more than 16 inches.

Public Realm/Streetscape sign means any sign erected and maintained by public officials or public agencies, or approved and authorized for use by state or local governmental authorities.

Sandwich Board Signs means a self-supporting, freestanding temporary sign with only two (2) sides that are situated adjacent to a business with the intent to attract pedestrian traffic to businesses. Sandwich board signs are not meant to be read by vehicular traffic.

Wall sign means any sign which is affixed to a wall of any building. This definition includes individual letter signs and signs on mansards.

Window sign means a sign installed inside a window, or any sign placed within a building for the purpose of being visible from the public right-of-way. This does not include merchandise on display.



Standards

Public Realm/Streetscape Signs

The pedestrian oriented nature of The COR requires unique signage that will build on the design concepts for The COR. These types of signs include banner signs on the streetlights, kiosks, street signs, wayfinding signs, signage for public parking ramps, etc. As the design concept for The COR has been refined, so has the signage plan. The Master Sign Plan is included as Appendix B. Size, location, and construction specifications of these signs shall be determined by the Master Sign Plan, which must be approved by the City Council, after a recommendation by the Planning Commission. Careful foresight has been given to size of signs based on needs of the adjacent roadways in order to maintain adequate safety standards that may otherwise be compromised by poor design. Roadways with higher speeds warrant larger lettering to maintain safe travel.

Community and Gateway signs

Community signs include are planned to include three (3) signs on Highway 10, as shown on the Signage Plan (see Figure 5: Signage Plan). The signs include the existing Community Sign near the future transit plaza, the sign at the northwest corner of Highway 10 and Ramsey Boulevard and the planned sign at the northwest corner of Highway 10 and Armstrong Boulevard.

Gateway signs are planned at multiple locations at entrances to The COR from major roadways, as shown on the Signage Plan. General gateway signs may identify The COR only. Retail Gateway signs may include provisions for off-premise, tenant sign panels.

These off-site signs, part of a unified development, are allowed as shown on the signage plan and shall have consistent materials and colors. Community and Gateway Signs shall conform to the design of the Master Sign Plan. Tenants within the unified development shall be allowed to utilize tenant panels on the Community and Retail Gateway Signs.

Amendments to the Master Sign Plan and Approved Community Signs. As the Master Sign Plan is adopted as part of the zoning ordinance, and has the effect of being a zoning ordinance, amendments to the Master Sign Plan must be processed by Ordinance and approved by City Council after review and recommendation by the Planning Commission.

Sign Type	Allowable Size	Architecture	Allowable Height
Community Sign	300 Square Feet	Per Appendix B	30 Feet *
Gateway Sign	50 Square Feet	Per Appendix B	10 Feet
Retail Gateway Sign	150 Square Feet	Per Appendix B	12 Feet
Community Kiosk	32 Square Feet	Per Appendix B	10 Feet*
Parking Ramp Signage	100 Square Feet	Per Appendix B	(Wall Signage)

* From highest point of adjacent road centerline



Desirable signs are designed to be architecturally compatible with buildings within the development and other signage within the development

Project Signs

Project signs are permitted within all COR sub-districts.

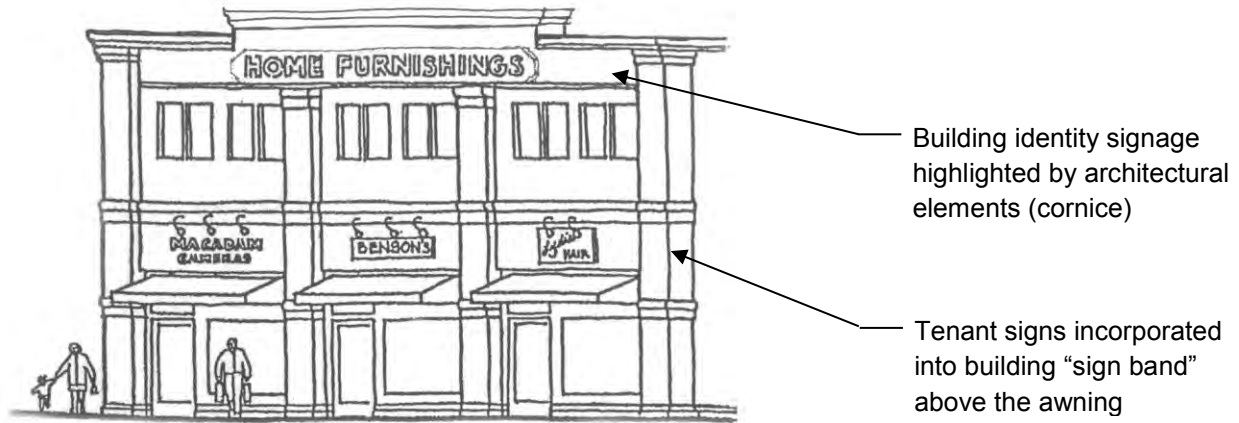
One (1) freestanding sign is allowed per building, provided that the following standards are met:

- A. The building in which the advertising business is located, shall be set back a minimum of six (6) feet from a public street right-of-way.
- B. The area of each face of the signboard shall not exceed six (6) square feet and the signboard shall not have more than two (2) readable faces. The signboard may exceed six (6) square feet, but not more than 32 square feet, provided that the sign consists of individual letters, without internally illuminated box letters. Lighting may be achieved through 'halo' lighting effects.
- C. The height of the top of the signboard, or of any posts, brackets, or other supporting elements shall not exceed six (6') feet from the ground.
- D. The signboard shall be constructed of wood, acrylic, aluminum or metal and shall be architecturally compatible with the style, composition, materials, colors and details of the building.
- E. No part of the sign shall encroach on the right-of-way and its location shall not interfere with pedestrian or vehicular circulation.
- F. Limited to one (1) sign per building and shall not be in addition to wall-mounted, applied letter or projecting signs.

- G. The readable faces of the sign shall be perpendicular to the adjacent street.

In the COR2 District, the following signage shall be allowed:

- A. Ground sign. There shall not be more than one (1) ground sign for each parcel. The gross surface area of a ground sign shall not exceed one hundred (100) square feet for each exposed face nor exceed an aggregate gross surface area of two hundred (200) square feet. Ground signs may be shared and combined at areas of shared entrances for multiple users. Shared ground signs shall not exceed 150 square feet. A shared sign shall be allowed for each entrance to the site (defined as bordered by public streets).
- B. Menu board. One (1) on-site menu board per drive-up or walk-up lane of a drive-in restaurant up to a maximum of thirty-two (32) square feet each and a maximum height of three (3) feet. Menu boards are allowed a message on one (1) side only and cannot contain an advertising message.
- C. Directional signs.
 - 1. Directional or instructional signs are permitted in accordance with Section 117-463(I).
 - 2. Parking lot directional signs designating parking area entrances and exits are limited to one (1) sign for each entrance and/or exit and shall not exceed four (4) square feet for each exposed face. Parking lot directional signs shall not project higher than five feet (5') in height, as measured from the established grade of the parking area to which such signs are accessory.
 - 3. Parking lot instructional signs designating the conditions of use or identification parking areas shall not exceed eight (8) square feet and shall not project higher than ten (10) feet in height for wall signs and seven (7) feet in height for ground signs, as measured from the established grade of the parking area to which such signs are accessory.



Building identity signage highlighted by architectural elements (cornice)

Tenant signs incorporated into building "sign band" above the awning

Wall Signs

Wall signs are permitted within all COR sub-districts.

Wall signs shall be permitted on one (1) wall, except that lots with frontage on more than one (1) street may have signage on one (1) wall per street frontage.

Within the COR1, COR3, COR4 and COR5 districts, the following standards apply:

- A. Wall-mounted or painted signs, provided the following standards are met:
 1. The sign shall be affixed to the front facade of the building, and shall project outward from the wall to which it is attached no more than six inches (6").
 2. Single Tenant Buildings. Each building shall be allowed one (1) wall sign per street frontage. Signage must be located on the street elevation. The area of the sign shall not exceed fifteen (15%) percent of the ground floor building facade area. Individual signs may not exceed.
 3. Multi-Tenant Buildings.
 - a. Each building shall be allowed wall signage on the street frontage elevation. For buildings with multiple street frontages, the allowable signage is per street frontage. Signage must be located on the street elevation.
 - b. Each building is allowed wall signage that shall not exceed 5% of the wall area of the building façade adjacent to the street. Individual signs may not exceed 50 square feet.
 - c. Tenant signage must be located on the tenant lease space.
 - d. At least fifty (50%) percent of the allowed signage must be allocated to ground floor tenants and located on the ground floor.
 - e. Signage is limited to a maximum of one (1) sign per business/tenant, except that tenants with frontage on multiple streets may be permitted to have signage on each street frontage.
 - f. Multi-tenant buildings must have wall signs of similar design. Sign permits for

tenants in multi-tenant buildings shall only be permitted by the City after the building owner has submitted a comprehensive sign plan approved by the Zoning Administrator. The comprehensive sign plan for the building shall include similar design standards including sign material, color, style, spacing and size.

4. Signs for buildings facing Highway 10. Single or multi-tenant buildings that have frontage on Highway 10 or are separated from Highway 10 only by other public right-of-way, shall be allowed to have signage on that street elevation that is up to fifteen (15%) percent of the wall area of that building façade with no maximum square footage.
- B. Wall-mounted building directory signs identifying the occupants of a commercial building, including upper story business uses, provided the following standards are met:
1. The sign is located next to the entrance.
 2. The sign shall project outward from the wall to which it is attached no more than six inches (6”).
 3. The sign shall not extend above the parapet, eave, or building facade.
 4. The height of the lettering, numbers, or graphics shall not exceed twelve inches (12”).
 5. One (1) such sign is allowed per public building entrance and is allowed in addition to other permitted wall signage.
- C. Applied letters may substitute for wall-mounted signs, if constructed of painted wood, painted cast metal, bronze, brass, acrylic or black anodized aluminum. The height of applied letters shall not exceed 12 inches.
- D. Logos are considered signs and shall be included in the maximum allowable sign area.
- E. Window or door signs, provided that the following standards are met:
1. The sign shall not exceed ten percent of the window or door area or four (4) square feet, whichever is less.
 2. The sign shall be silk screened, hand painted, applied letters/graphics, neon tubing or other sign technologies that meet these standards.
 3. Limited to one (1) sign per business, applied on either the window or the door, but not on both.
 4. The sign shall not have an opaque backing of any type although smoked glass is allowed.
 5. Window signage (includes graphics) shall be permitted on the same building walls that have, or are allowed to have, wall signage pursuant to Item A above. Window signage

shall be deducted from the allowable wall signage area.

Within the COR2 district, where more conventional suburban development is expected, rather than the neo-traditional development planned for the other COR sub-districts, the following standards apply:

- A. Wall, canopy or marquee sign. Total sign area may not exceed 15 percent of the front building facade. At least 50 percent of the signage area must be placed on the measured wall with remaining signage area, if desired, distributed on any other wall. Sign height shall not exceed the top of the parapet wall or, if no parapet wall, sign height shall not exceed the height of the eaves. The gross surface area of a wall, canopy or marquee sign may be increased by ten percent if such wall sign:
 1. Consists only of individual, outlined alphabetic, numeric and/or symbolic characters without background except that provided by the building surface to which the sign is to be affixed;
 2. Illumination, if any, is achieved through shielded illumination, shielded silhouette lighting, or shielded spot lighting but not any lighting where the light source is visible or exposed on the face or sides of the characters; and
 3. A wall, canopy or marquee sign may be located on the outermost wall of any principle building but shall not project more than 16 inches from the wall to which the sign is to be affixed. A wall sign shall not project higher than the parapet line of the wall to which the sign is to be affixed.
- B. Window signs are restricted to 30 percent of the area of the window in which the sign is to be displayed.

In all COR sub-districts, restaurants and cafes shall be permitted one (1) wall-mounted display featuring the actual menu as used at the dining table, to be contained within a shallow wood or metal case and clearly visible through a glass front. The display case shall be attached to the building wall, next to the main entrance, at a height of approximately five feet (5'), shall not exceed a total area of two square feet, and may be lighted. This signage is allowed in addition to other permitted wall signage.

Projecting Signs

Projecting signs are permitted within all COR sub-districts.

Projecting signs, including graphics or icon signs, mounted perpendicular to the building wall, are encouraged in all sub-districts, provided the following standards are met:

- A. The sign area shall not exceed thirty-two (32) square feet.
- B. The distance from the ground to the lower edge of the signboard shall be ten feet or greater.
- C. The height of the top edge of the signboard shall not exceed the height of the wall from which the sign projects, if attached to a single story building, or the



height of the sill or bottom of any second story window, if attached to a multistory building.

- D. The distance from the building wall to the signboard shall not exceed six inches (6”).
- E. The width of the signboard shall not exceed three (3) feet.
- F. Limited to one (1) sign per business. Projecting sign area shall be deducted from the allowable wall signage area.
- G. Granted to ground floor commercial uses only.

Awning Signs

Awning signs are allowed in all COR sub-districts. Awning signs are allowed for ground floor uses only, provided that the following standards are met:

- A. If acting as the main business sign, it shall not exceed 24 square feet in area.
- B. If acting as an auxiliary business sign, it shall be located on the valance only, shall not exceed four (4) square feet in area, and the height of the lettering, numbers, or graphics shall not exceed four inches (4”).
- C. Limited to two such signs per business, per frontage.
- D. If acting as the main business sign, it shall not be in addition to a wall-mounted or applied letter sign.

Sandwich Board Signs

Sandwich board signs are permitted within all COR sub-districts.

- A. One (1) sandwich board sign per business is permitted in any business, commercial and mixed use district and shall be located within five feet (5') of the main building entrance to the business it advertises.
- B. Sandwich board signs shall be displayed only during open business hours and must be removed daily.
- C. Sandwich board signs shall be no more than a total of two feet (2') in width and three feet (3') in height
- D. Sandwich board signs must leave a minimum of five feet (5') of clearance for pedestrian access if placed on a public or private sidewalk. Sandwich board signs may not hinder the ability of persons to access vehicles parked at the curb and/or access to a building.



- E. Acceptable materials for sandwich board signs shall include the following: metal, wood synthetic materials such as a chalk board and whiteboard. Sandwich board signs shall not be illuminated, nor shall they contain moving parts, or have balloons, streamers, stringers, pennants or similar adornments attached to them. Sandwich board signs shall be maintained in a good appearance at all times.
- F. No sandwich board sign shall be secured, tethered or installed on traffic devices, utility equipment, street furniture, street lights, or any other public fixture.
- G. Sandwich board signs are temporary signs and shall not be counted towards the total sign area of the site for permanent signage.

Temporary Signs

Temporary signs are allowed per Section 117-465 (Temporary signs) of the Zoning Ordinance, except as noted below:

- A. Freestanding temporary signs shall only be allowed in the COR2 and COR3 Districts.
- B. In all other COR districts, temporary signs shall be limited to temporary sandwich board signs or temporary wall signage, without interchangeable letters.
- C. Real estate and leasing signs shall be exempt from temporary and permanent sign regulations, provided that these signs do not exceed thirty-two (32) square feet.

Off-Site Signs

Off-site signs are prohibited except for wayfinding and community signage specifically allowed by the Design Framework.

Table 2

Sign Standards					
Development Standard	COR1	COR2	COR3	COR4	COR5
Project Signs					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign (maximum)	6 sq. ft./6 feet high***	100 sq. ft./6 feet high	6 sq. ft./6 feet high***	6 sq. ft./6 feet high	6 sq. ft./6 feet high
Menu Board**	N/A	1 per drive-through lane/40 sq. ft. max.	N/A	N/A	N/A
Directional Sign	N/A	Per 117-463(l)	N/A	N/A	N/A
Wall Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign					
Single Tenant Building	15% of the ground floor façade area, - or- 15% of the front façade area when facing Highway 10.	15% of façade area	15% of the ground floor façade area,	5% of the ground floor façade area,	5% of the ground floor façade area,
Multi-Tenant Buildings	15% of the façade area,	15% of façade area	15% of the façade area,	15% of the façade area,	15% of the façade area,
Building Facing Highway 10	15% of the façade area	15% of façade area	15% of the façade area	15% of the façade area	15% of the façade area
Window Sign					
Number of signs allowed	1	1	1	1	1
Size of Sign	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	30% of area of window. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.	10% of area of window or 4 sq. ft., whichever is less. Deducted from allowable wall sign area.
Directory Sign					
Number of signs allowed	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance	1 per building entrance
Size of Sign	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum	3 sq. ft. maximum
Projecting Sign					
Number of signs allowed	1*	1*	1*	1*	1*
Size of Sign	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide	6 sq. ft./3 feet wide
Awning Sign					
Number of signs allowed	2 per business	2 per business	2 per business	2 per business	2 per business
Size of Sign	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.	24 sq. ft./max. letter height 12-inches for main business sign or 4 sq. ft./max letter height 4-inches if auxiliary. Area is deducted from allowable wall sign area.

*One (1) sign is allowed per street frontage (either wall or project)

**Restaurants and cafes in all districts are allowed one (1) wall mounted menu board not to exceed 2 sq. ft. in addition to other permitted wall signage.

***See Page 32, 'Project Signs' for allowances up to 32 square feet.

Additional standards

- A. Businesses with service entrances may identify these with one (1) wall-mounted or applied letter sign not exceeding two square feet.
- B. One (1) directional sign, facing a rear parking lot. This sign may be any type of permitted sign other than a freestanding sign, but shall be limited to four (4) square feet in area.

Creative Sign Standards

- A. *Purpose.* This section establishes standards and procedures for the design, review, and approval of creative signs. The purposes of this creative sign program are to:
 - 1. Encourage signs of unique design, and that exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - 2. Provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- B. *Applicability.* An applicant may request approval of a sign permit for a creative sign to authorize onsite signs that employ standards that differ from the other provisions of this chapter but comply with the provisions of this section.
- C. *Application Requirements.* A sign permit application for a creative sign shall include all information and materials required by the City.
- D. *Procedure.* A sign permit application for a creative sign shall be subject to review and approval by the City as part of the Site Plan review process. When the creative sign is proposed after site plan review is complete, a creative sign may be approved by the Zoning Administrator when the proposed sign is fifty square feet or less, and shall be approved by the City Council when the sign is larger than fifty square feet.
- E. *Design Criteria.* In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
 - 1. Design Quality. The sign shall:
 - a. Constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area;
 - b. Be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and
 - c. Provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
 - 2. Contextual Criteria. The sign shall contain at least one (1) of the following elements:
 - a. Classic historic design style;

- b. Creative image reflecting current or historic character of the City;
 - c. Inventive representation of the use, name, or logo of the structure or business.
3. Architectural Criteria. The sign shall:
 - a. Utilize or enhance the architectural elements of the building; and
 - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
4. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

Prohibited signs

The following signs are prohibited:

- A. Box signs or cabinet signs, whether on a wall, projecting or on canopies are prohibited except for logo signs permitted as part of an overall sign plan.
- B. Signs employing mercury vapor, low pressure and high pressure sodium and metal halide lighting; plastic panel rear-lighted signs.
- C. Signs on roofs, dormers, and balconies.
- D. Billboards.
- E. Signs painted or mounted upon the exterior side or rear walls on any principle or accessory building or structure, except as otherwise permitted hereunder.
- F. Free standing pylon signs over six (6) feet in height, except community signs.
- G. Back-lit awnings.
- H. Interchangeable letter boards or panels.
- I. Flashing signs.
- J. Off-premises signs, except community signs.

This page intentionally left blank.

Overall Framework – Stormwater Management

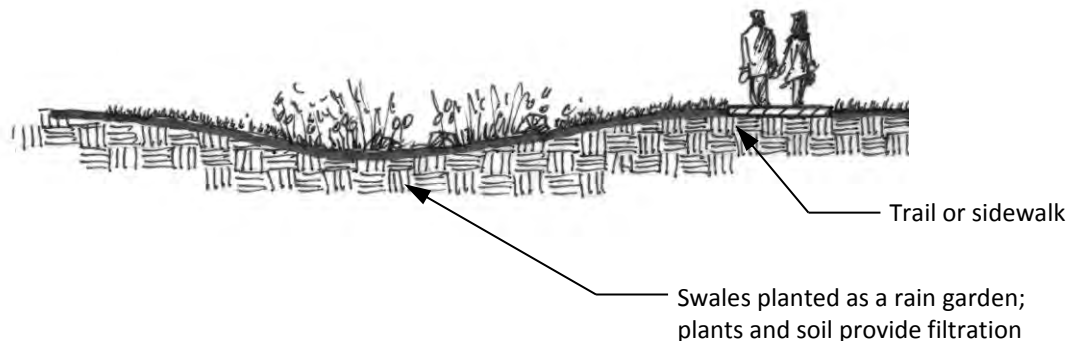
Overview

Water features and drainage systems are essential components of The COR Master Plan. The mix of ponds, streams, fountains and other water elements will provide focal amenities and year round activities within the framework of an environmentally responsible, visually pleasing strategy to manage stormwater. Because the Mississippi River is a restricted discharge water of the state, and because we are creating a new discharge from this development, each sub-district and each site has a responsibility to utilize ‘best management practices’ (BMP) to pretreat run-off, reduce erosion and encourage infiltration, with the understanding that regional treatment will be provided. Water elements are not intended to be separate stand-alone features, but instead should be integrated with the variety of parks, trails and public open space proposed for The COR. Water and landscape should be designed and utilized within multipurpose areas that accommodate both active and passive recreational use.

Objectives

Integration of water and landscape, design creativity, use of high quality materials and safety concerns are the critical objectives to be met – the following examples illustrate only a few of many possibilities:

- Innovative new products, such as special soil systems (Netlon, structural soils), pervious pavements for parking areas, storm ceptors, underground storage and other creative techniques should be used to BMP standards
- ‘Green architecture’, expressed through green roofs, gray water recycling and other techniques should be included to reduce the impact of new development on stormwater systems
- Water feature design should include both formal elements (such as reflecting pools or fountains) and natural/informal forms (such as ponds, streams, waterfalls) and should explore creative ways to integrate wetland/rain garden landscapes with active, urban spaces.



Guideline Recommendations

This framework not only defines the overall role and character of The COR, but also encourages each development parcel to address the following:

- **Create** signature water features (ponds, pools, fountains, waterfalls, etc.) as major visual amenities throughout The COR
- **Promote** high quality, creative and appealing aesthetics for all system elements
- **Integrate** stormwater management components (meeting both water quality and quantity requirements)

A great deal of time and effort has gone into the calculation and design of a 'watershed-based' stormwater management plan to support COR development. While this overall strategy addresses the water quantity issue for the entire COR by providing storage and conveyance facilities for storm events, individual development projects (block-by-block) have an equal responsibility to meet water quality standards as a primary goal. Each project should integrate a variety of techniques, materials and methods to promote multiple use, maximum flexibility, improved aesthetics for parking areas, park spaces, drainage swales and other site features.

Overall Framework – Parks/Public Spaces

Overview

The overall design framework of The COR is based on a system of linked parks, trails and open space. The City of Ramsey, Anoka County, and to some extent, the State of Minnesota all play a role in local and regional parks programming, design review, construction sequencing, implementation and ultimately maintenance and operation of key public spaces within this framework. The goal is to link existing and proposed parks and trails defined in regional park plans with proposed new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this network of green, adding a variety of private, semi-private, and perhaps some additional public space throughout The COR. Private development will share the responsibility to provide inviting, innovative and useable green space as integral parts of individual parcel site design.

Guideline Recommendations

The Design Framework encourages the following:

- Work with the City to create an integrated, comprehensive open space system that links with existing city and county trails and open space networks
- Encourage private development (block-by-block) to share in the responsibility to provide inviting, innovative and useable green space as integral parts of each development project
- Integrate stormwater management components (meeting water quality and quantity requirements) within both public / private park and open space improvements



Together, the system of public and semi-public green space will result in a welcoming public realm throughout the new community.

Objectives

Based on the Development Master Plan for The COR, a variety of public and private green space is proposed including:

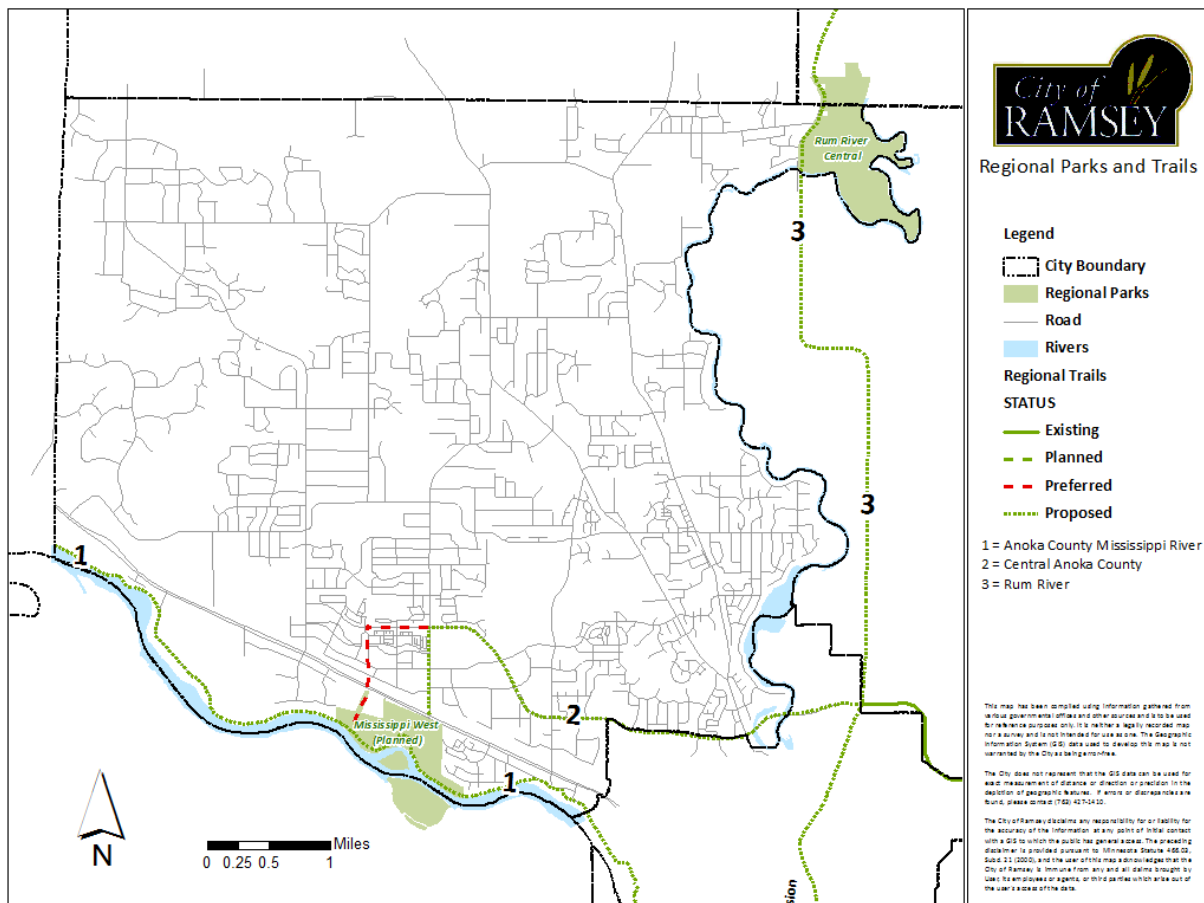
- Each block and each project will provide intentional, creative, innovative, useable green areas as integral components of site development and building design.
- At the individual block scale, small squares and greens will provide places for gathering and relaxation, especially targeted to meet the demands of adjacent, or nearby buildings. Elements such as gardens, gazebos and fountains should characterize these more intimate public spaces.
- At the neighborhood scale, parks will provide space for more active recreation, and should be flexible enough to accommodate a variety of recreational activities.
- Playgrounds, game fields and park buildings are park amenities that will target a wider user demand from the surrounding community.
- The Municipal Plaza will connect with all other parks and open space elements, providing a focal point for activity and a sense of place. Gardens, public pavilions and shelters, play lots and open play fields are amenities that will provide built-in flexibility to meet the demands of both the local population and regional visitors to The COR.



Regional Systems

The COR is the heart of several regional systems that are either located within or adjacent to The COR. These regional systems include the Northstar Commuter Rail, the Central Anoka County Regional Trail, the Mississippi West Regional Park, and the Mississippi River Trail.

A number of these systems have a pedestrian component. As such, Ramsey desires to locate the future Central Anoka County Regional Trail through the heart of The COR, along Center Street. The map below indicates Ramsey's preferred route for this trail. As this trail requires crossing Highway 10, it is imperative that a safe, grade-separated crossing is realized. This pedestrian overpass is envisioned to be connected to the Northstar Commuter Rail – Ramsey Station, and has been named the 'Mississippi Skyway'.



Part Two: Sub-district Framework

Overview

The Master Plan and Zoning Ordinance identify five (5) distinct sub-districts within The COR. Each of these sub-districts is defined by a unique mix of uses, specific site development patterns and perhaps a distinctive character or image. The sub-districts complement one another as part of the overall plan. The sub-districts are shown the attached Sub-district Map (**Error! Reference source not found.**). They include:

- **COR1 Mixed Use Core** – provides the broadest variety, highest density and greatest intensity of development, encouraging both vertical and horizontal mixed use
- **COR2 Commercial** – provides a location for larger scale retail and other auto-orientated commercial uses
- **COR3 Workplace** – provides a location for uses with high concentrations of employees, such as medical/technology related office, and other corporate or institutional uses
- **COR4 Neighborhood** – provides opportunities for a range of housing types from small lot single family to townhouse, to high density senior or rental apartment
- **COR5 Park and Open Space** – preserves environmental features and provides neighborhood/community amenities throughout The COR

Development plans must respond to the overall COR Design Framework described in Part One of this document, but also must address the following detailed framework standards for each sub-district. To further clarify and illustrate proposed features in the Master Plan, these sub-district guidelines are organized under the following categories:

- Public Realm / Streetscape Framework
- Site Design Framework
- Architectural Framework

In all cases, the sub-district framework must be applied in harmony with the overall COR framework and with other existing city, county, state codes, design criteria, plans and studies that support broader goals for regional growth and development. However, within each district, development standards shall be based, in part, on the street hierarchy established in The COR.

Public Realm/Streetscape

- A. The developer shall be responsible for construction of all streetscape improvements along adjacent streets. These improvements include all hardscape improvements, such as trails, sidewalks, benches, bike racks, etc., as well as street trees and other greenscape.
- B. The landowner shall be responsible for maintenance of the streetscape improvements along adjacent streets. Such maintenance may be managed as part of a Special Service District (SSD) or similar.

Plazas

- A. All properties with principal buildings in excess of 30,000 square feet shall be required to have a public plaza space.

- B. The public plaza space shall include benches, bike racks trash receptacles, lighting fixtures and other amenities to create a welcoming space for tenants of the building and members of the public.
- C. These plaza spaces should be visible from the public street.

Architecture

These architectural framework standards provide an important balance between design continuity, desirable variety and the notion that buildings should complement, and perhaps contrast, but not compete with one another. The lasting appeal of the final outcome must be grounded in the authenticity of the design and the acceptable level of ‘messiness’ that comes with creating a real downtown over time; ‘faux’ second floors, stage set or themed solutions, and large faceless buildings are not acceptable.



Auto Versus Human Design: Providing rear alleys or side loaded garages enables the home to present itself to the street, which promotes sociability and walkability.

Minimum Standards

Definitions

Build to line: The line at which construction of a building facade is to occur on a lot. The build to line provides a maximum setback for all building facades from the right-of-way or inside edge of the sidewalk, whichever is greater. The build to line applies to all facades abutting a street right of way. Build to line standards are based on the sub-district the parcel is located and the street in which the building is located upon.

Setback: The minimum required distance between a parking lot or the vertical wall of a building and a lot line. Setbacks are typically more flexible than build to lines and allow a building to be set further back from the lot line and street right-of-way.

Requirements:

- A. Additional development standards shall be defined within each sub-district.

- B. Provide a diverse mix of materials, applied in a variety of proportions, exposures and detailing within a block, or along a street.
- C. Buildings should be articulated to break up the mass of the building façade on all elevations. Large blank exterior walls shall be prohibited. Windows are encouraged to be used to break up the mass of the building. Rooflines and building elevations should be articulated to break up the mass of buildings. Buildings must be broken at a minimum of every fifty (50) feet. This variation can be accomplished through one (1) or more of the following:
 1. Wall face variations (minimum depth of two (2) feet)
 2. Pilasters or columns (minimum depth of two (2) feet)
 3. Upper wall break in color and/or materials (minimum of 2/3 up the height of the wall)
 4. Wainscot (minimum of four (4) feet high)
 5. Canopies (minimum four (4) foot width)
 6. Corner and demising wall or building separations



More Desirable

- D. Each development application must list building materials, roofing materials and building colors on the plans. Building articulation, mechanical screening by architectural elements and design elements on all building elevations must be identified on the plans. The plans shall be subject to City review and approval to ensure that the design intent of the Framework has been met.



Less Desirable

- E. Building materials shall be high-quality durable materials, such as glass, stone, brick, windows, canvas awnings, etc.
 1. Use local sources if available
 2. Siding is not allowed (vinyl, metal, wood) except as a complimentary use
 3. Use clear or lightly tinted glass for all windows and doors – mirrored, reflective or highly tinted glazing is not acceptable
 4. Metal and wood components are acceptable as a complimentary, not primary, building material
 5. COR1 sub-district buildings shall be primarily brick or a comparable alternative and shall comprise a minimum of fifty (50%) percent of the non-glazed wall area
- F. Residential development should provide variety within the subdivision to avoid monotony. Developers will include front porches and windows that face all streets. Corner units shall have more than one (1) window per side of building to help create a sense of place in the community and promote safety on the street.
- G. Garage forward or “snout houses” are highly discouraged on all residential development as they dominate the view from the street and sidewalk.
- H. All sides of buildings shall have an equal finish in terms of materials and general design.

I. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.



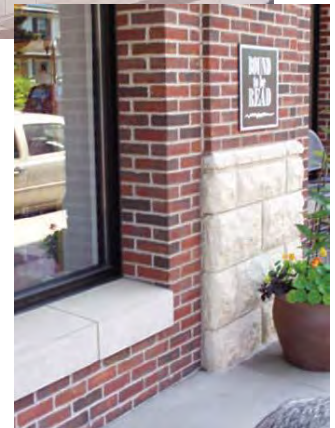
J. Building massing shall be oriented parallel to the street frontage of the lot.

K. Trash and recycling areas shall be designed internal to the building.

L. Provide recessed entries wherever possible.

M. Buildings shall have a base and top to the architecture.

N. Multi-story buildings are preferred in the COR1 sub-district, but occasional single story buildings are acceptable; single story buildings should not dominate any street frontage and should be a minimum of twenty (20) feet to the roofline.



O. Maximize glass openings for all ground level, street front façades – especially for retail, restaurant and other commercial uses – fifty (50%) minimum of total ground level façade, or demonstrate great design through other means.

P. Provide real window openings for all street facing façades above ground level in all districts—a minimum of forty (40%) percent of total façade.

Residential uses in COR4 and COR5 must also comply with the following:

A. *Location of main entrance*

1. A building must include a front porch or covered balcony/patio at all main entrances that face a street. The porch or stoop shall adjoin the main entrance and the main entrance shall be accessible from the porch.
2. The main entrance of each principal building must face the street. On corner lots, the main entrance may face either of the streets or be oriented to the corner. With buildings that have more than one (1) main entrance, only one (1) entrance must meet this requirement.



B. *Porches:* Porches used to satisfy the design criteria shall comply with the following:

1. Porches shall be covered by a solid roof. The roof shall not

be located more than 12 feet above the floor of the porch. If the roof of a required porch is developed as a deck or balcony, it may be flat.

2. The porch shall have minimum dimensions of 6 feet by 6 feet. (6' x 6'). For single-family detached dwelling units, the covered area provided by the porch must be at least forty-eight (48) square feet and a minimum of eight (8) feet wide. If the main entrance is for more than one (1) dwelling unit, the covered area provided by the porch must be at least sixty-three (63) square feet and a minimum of nine (9) feet wide.



- C. *Covered balconies/patio*: The covered area must be at least forty-eight (48) square feet, a minimum of eight (8) feet wide, and no more than fifteen (15) feet above grade. The covered area must be accessible from the interior living space of the house.

- D. *Openings between porch floor and ground*: Openings of more than one (1) foot between the porch floor and the ground must be covered with a solid material or lattice.

E. *Roofs*

1. *Slope*: Principal structures must have a roof that is sloped, with a pitch that is no flatter than six (6) units of horizontal run to twelve (12) units of horizontal rise.
2. *Architectural features*: The roof of a principal structure shall include the following architectural details:
 - a. At least one (1) dormer facing the street. If only one (1) dormer is included, it shall be at least five (5) feet wide and shall be centered horizontally between each end of the front elevation. If more than one (1) dormer is provided, a dormer at least four (4) feet wide must be provided on each side of the front elevation; or
 - b. A gabled end, or a gabled end of a roof projection, facing the street.
3. *Roof eaves*: Roof eaves must project from the building wall at least twelve (12) inches, measured horizontally, on at least the front and side elevations.



Sub-District Framework - COR1 Mixed Use Core

Public Realm / Streetscape

Public realm and streetscape improvements define the character of The COR and create a memorable signature for the Mixed Use Core sub-district. This is the most urban of all sub-districts in The COR and will be developed with a very urban pattern. Great parks and great streets, encourage intensified levels of activity, support the proposed mix of uses, invite community gathering and accommodate special events throughout the year.

Site Development

Framework standards that address building location, organization of space and parking options tend to focus on what can be measured – dimensions, setbacks, number of spaces and so on. But in addition to these issues, the Design Framework also describes the aesthetic qualities and design character of a preferred ‘urban form’ for the Mixed Use Core. The compact and higher density blocks that define this sub-district encourage a mix of uses, with a continuous built edge along street frontage. Innovative parking strategies support this mix, providing easy access to destination shopping, restaurant and entertainment venues. Together these elements will promote the core area as the center of activity – the place to see and be seen within The COR.

Architecture

Buildings in the Mixed Use Core will play a major role in defining the overall character of The COR. The proposed mix of uses for this signature destination suggests the potential for a variety of building types, demanding creativity and innovation to highlight unique architecture, while knitting the entire sub-district together as a coherent whole. Overall building design is an important consideration, but emphasis on first floor façade treatments is essential to encourage street-level activity and enhance the pedestrian experience. Architecture to be ‘4-sided’ at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 3

COR1 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.75	.75	.75	.75	.75	.75
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	5 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.	2 - 5 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR2 Commercial

Public Realm / Streetscape

While the Commercial Sub-district encourages a mix of large and small floor plates accommodating auto-oriented uses, this suggests even greater attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. Surface parking lots provide convenient access to storefronts, but present an additional challenge to the overall visual character and aesthetic appeal of this area. While no large public parks are identified for this sub-district, small pocket parks, squares or other gathering spaces are encouraged. A consistent, attractive streetscape is also essential.

Site Development

Site Design Framework for this sub-district focus on a unique opportunity to recast typical commercial development patterns into a contemporary and innovative strategy that responds to overall objectives for The COR. Specifically, this suggests that the majority of buildings front on the primary through streets, with parking and service areas on the side or in the back, away from view. However, the market realities suggest that the types of auto-oriented users in this district will have the potential for larger parking fields and drive-throughs that will make a true urban model challenging. The City will hold all users to this design vision while modifying the COR2 standards slightly to accommodate these types of more typical suburban land uses. A mix of uses within individual blocks, including retail, restaurant and residential, further enhances this model, addresses design character/aesthetics and promotes activity throughout much of the day and evening hours.

Architecture

Buildings in the Commercial Sub-district make a significant contribution to first impressions of The COR. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey a certain character and the scale or massing of structures begins to define the feel or experience of this place. These are not just 'commodity' buildings, but instead should raise the bar in terms of overall design and specific details that make The COR unique, inviting and memorable. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 4

COR2 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Minimum lot depth	100 feet	100 feet	100 feet	100 feet	100 feet	100 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC	>15 DU/AC
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (40% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Boulevard Sod	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	6 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

- A. At least forty (40%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply only to one (1) street frontage.
 1. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
 2. The City may approve a variation from the required build to line if the applicant provides a street edge consisting of fencing, decorative wall and/or landscaping with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the forty (40%) percent street frontage requirement.
- B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the

level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the forty (40%) percent required on all street frontages.

Sub-District Framework - COR3 Workplace

Public Realm/Streetscape

The Workplace Sub-district also encourages a mix of large and small floor plate buildings, but instead of commercial or entertainment, this area of The COR is dominated by office and institutional uses. With opportunities for corporate or medical campus development comes the attendant auto-related infrastructure – easy access, bigger streets and large surface parking lots. Once again, this suggests that equal attention be paid to public realm and streetscape improvements that not only accommodate vehicles, but also invite bicycle and pedestrian traffic. The Draw, an active, neighborhood scale, recreational play space has been included in this sub-district, but small pocket parks, squares or other gathering places are also encouraged. In addition, streetscape improvements provide an important public amenity and inviting pedestrian connection to adjacent sub-districts.

Site Development

The mix of medical, office, institutional and other uses in this sub-district suggest the potential for a campus-like arrangement of buildings on some blocks. These blocks would tend to be more internally oriented than in other districts, with driveways, parking lots, courtyards and walkway connections in the center, ringed by buildings around the outside. Buildings would still face adjacent streets, with both primary and secondary entries provided to animate these important façades, particularly along Ramsey Boulevard, Sunwood Drive and Rhinestone Street. While the Workplace sub-district also encourages mixed use, this desired development objective will likely occur mostly within blocks, not within individual buildings as seen in other districts.

Architecture

Buildings in the Workplace Sub-district also make a significant contribution to first impressions as people enter The COR via Highway 10, Ramsey Boulevard and Sunwood Drive. Buildings form gateways at the major entries into the site, architectural elements and choice of materials convey quality and permanence, and the scale or massing of structures begins to define the feel and character of this place. The proposed mix of uses within this sub-district suggest a variety of potential building types – blending medical campus with small office, storefront and some housing -- demands design creativity and innovation to highlight unique architecture, and knit the entire sub-district together as a coherent whole. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 5

COR3 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	40 feet	40 feet	40 feet	40 feet	40 feet	40 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	none	none	none	none	none	none
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	15 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.	2 - 4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. At least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR4 Neighborhood

Public Realm/Streetscape

Two prominent public parks supply much of the informal green space for The COR, and more specifically, provide the valuable park frontage and mix of amenities needed to support housing development within the Neighborhood Sub-district. North Commons surrounds an existing wetland and provides open space for year-round activities including a proposed sledding hill. The sub-district is adjacent to The Draw, which is characterized by wetlands, ponds and a meandering stream channel surrounded by gently sloping hills. Other park features include active and passive recreation areas, natural and manicured landscapes, education and interpretation opportunities, pedestrian and bicycle trails and many other amenities. Tree-lined streets connect these parks to surrounding residential neighborhoods and to nearby parks and amenities throughout The COR.

Site Development

The Neighborhood Sub-district emphasizes residential development. To support the proposed mix of housing, a framework of streets and blocks, based on the traditional grid system, encourages compact development patterns with increasing densities closer to the heart of The COR. Typical blocks either front onto signature parks or include some private green space within the site; many blocks have both. The arterial streets that frame this property, including Ramsey, Armstrong and Bunker Lake Boulevards, provide an important 'face' for this sub-district. Residential front doors will connect with public walks along each of these streets promoting the urban character of The COR.

Architecture

The largest in terms of overall acreage, the Neighborhood Sub-district will define the character and quality of the entire northern half of The COR property. With prominent street frontage along most of the major thoroughfares that serve this site, the variety of choices for building type, architectural style and design detailing are crucial decisions that together will promote a safe and attractive residential neighborhood. Integrating contemporary and traditional styles is also an important consideration, central to the notion that a unique and creative model for housing development will be a signature for The COR. Architecture to be '4-sided' at all locations visible to public areas. Emphasis will be on great design along public street frontage.

Table 6

COR4 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	None	None	None	None	None	None
Minimum lot width	20 feet	20 feet	20 feet	20 feet	20 feet	20 feet
Minimum lot depth	80 feet	80 feet	80 feet	80 feet	80 feet	80 feet
Allowable residential density in dwelling units per acre ⁽¹⁾	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC	4-15 DU/AC
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	.25	.25	.25	.25	.25	.25
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	20 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	no req.	no req.	no req.	no req.	no req.
Rear yard	no req.	no req.	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units	25 feet for residential units
Building height (min. - max.)	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.	1-4 stories.
Planter Style	N/A	Planter Bed	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	35 feet	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	6 feet	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	10 feet	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

Sub-District Framework - COR5 Parks & Open Space

Public Realm/Streetscape

The overall structure of The COR is based on a system of linked parks, trails and open space. This includes existing and proposed parks and trails defined in city and regional parks plans, joined with new parks and trails within The COR. Of equal importance is the contribution from each block and each development project to this green structure, adding a variety of private, semi-private and perhaps some additional public space throughout each sub-district. Great strolling streets provide connections between commercial, workplace, entertainment and residential neighborhoods and encourage shared use for cars, pedestrians and bicycles.

Site Development

The COR Master Plan illustrates a strong framework of green space, including a mix of public parks, destination amenities, courtyards, commons and other features. As a key component of the overall plan, this integrated system of parks, trails and open space provides the framework around which all other land uses are organized. Placemaking and connectivity are important site development considerations, linking each block and each sub-district with major public spaces nearby and with many other amenities and attractions in The COR.

Architecture

There are numerous opportunities to include buildings and other structures as significant design features within the proposed parks, trails and open space system for The COR. These could range from simple picnic shelters and landscaped arbors to more complex park buildings that might contain meeting rooms, rest rooms, storage areas and other facilities. Design of these important buildings should respond to the specific setting and landscape features in each park, and complement the overall character and quality of buildings located within surrounding blocks. Additionally, limited retail is planned along the perimeter of the park.

Table 7

COR5 Development Standards						
	Arterial Street	Destination Street	Parkway	Connector Street	Downtown Street	Local Street
Minimum lot size	0.5	N/A	0.5	0.5	0.5	0.5
Minimum lot width	N/A	N/A	N/A	N/A	N/A	N/A
Minimum lot depth	N/A	N/A	N/A	N/A	N/A	N/A
Allowable residential density in dwelling units per acre ⁽¹⁾	N/A	N/A	N/A	N/A	N/A	N/A
Minimum floor area ratio (FAR) for nonresidential uses/vertically mixed buildings/sites	N/A	N/A	N/A	N/A	N/A	N/A
Build to Line						
Front yard ⁽⁴⁾	30 feet as measured from building front to right-of-way ⁽²⁾⁽³⁾ (60% of front facade w/in max.)	N/A	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)	10 feet as measured from building front to right-of-way ⁽²⁾ (60% of front facade w/in max.)
Side yard	no req.	N/A	no req.	no req.	no req.	no req.
Rear yard	no req.	N/A	no req.	no req.	no req.	no req.
Driveway length (minimum)	25 feet	N/A	25 feet	25 feet	25 feet	25 feet
Building height (min. - max.)	1-2 stories.	N/A	1-2 stories.	1-2 stories.	1-2 stories.	1-2 stories.
Planter Style	N/A	N/A	Boulevard Sod	Boulevard Sod	N/A	Boulevard Sod
Tree Spacing (on center)	N/A	N/A	35 feet	35 feet	35 feet	35 feet
Boulevard Width	N/A	N/A	6 feet	6 feet	N/A	6 feet
Sidewalk Width	10 feet	N/A	6 feet	10 feet	10 feet	10 feet

1. Residential density is based on the net area of the parcel in question for parcels. In the event that public open space that is not dedicated to the City is developed independently of any particular residential project, the land area of the open space shall be divided equally among those abutting projects for purposes of density calculations.
2. Build to line shall be measured from building front to edge of right-of-way, or edge of sidewalk easement as defined by the City and said sidewalk must be encumbered by a sidewalk easement recorded with the office of the Anoka County Recorder.
3. The City may approve up to a 60-foot setback if it finds that topography or other factors require that a building be set back further to achieve acceptable grades or buffer between the street, the site entrance and the building.
4. In order to address vision clearance standards on higher speed roadways, parcels that are located at an intersection with an arterial roadway are allowed a build-to line of 35 feet from both streets.

A. Where commercial buildings are planned, at least sixty (60%) percent of street frontage of any lot shall be occupied by building facades meeting this build-to-line. On lots with more than one (1) street frontage, the build to line shall apply on each side fronting a street. An enclosed open area plaza space or outdoor seating with a decorative wall with a minimum height of three feet (3') and a maximum of four and one-half feet (4 ½') can be used to meet the sixty (60%) percent street frontage requirement.

B. Screening Of Parking Areas: Wherever a surface parking area faces a street frontage, such frontage shall be screened with a decorative wall, railing, hedge, or a combination of these elements, to a minimum height of three feet (3') and a maximum height of four and one-half feet (4 ½') above the level of the parking lot, at the build-to line. This screening may be broken into sections along the street edge provided it meets the sixty (60%) percent required.

This page intentionally left blank.

Implementation

Introduction

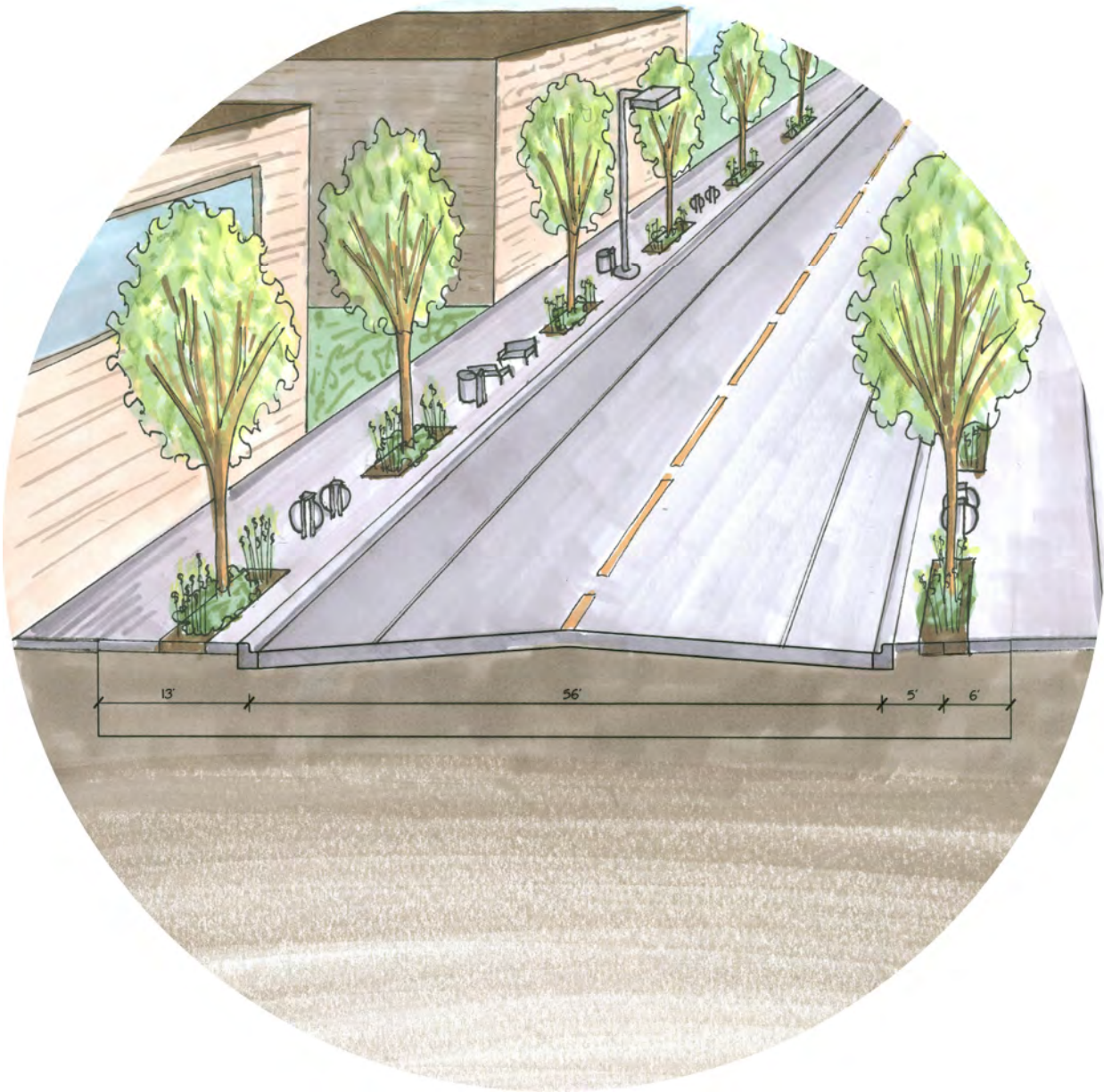
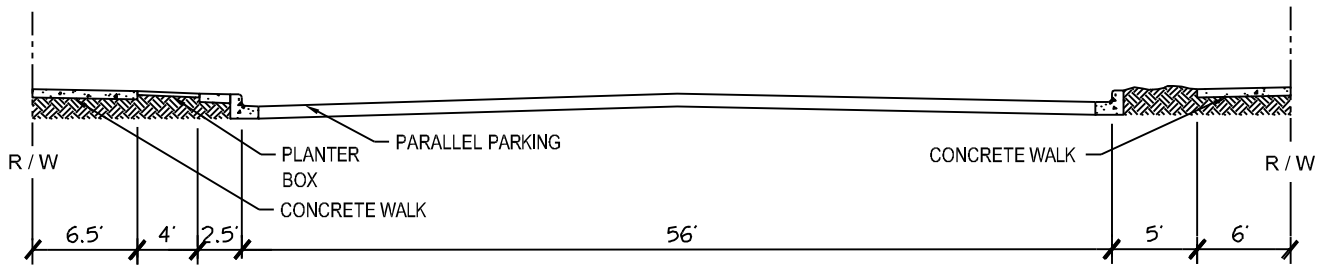
This framework provides distinct strategies for guiding The COR's form and appearance. This framework's vision for the COR will take several years to implement. Successful implementation will require consistent application of the Design Framework by City staff. Zoning enforcement and design framework standards will not succeed without the cooperation and commitment of landowners, development interests, and the rest of the community.

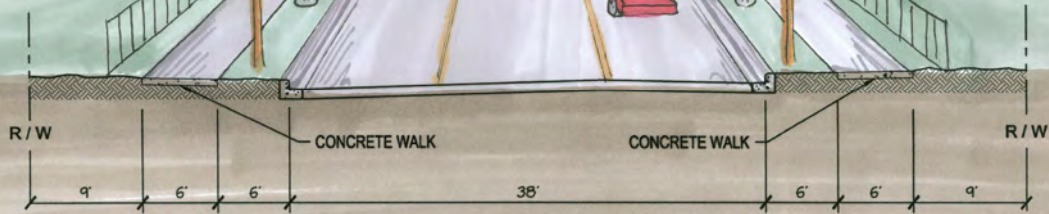
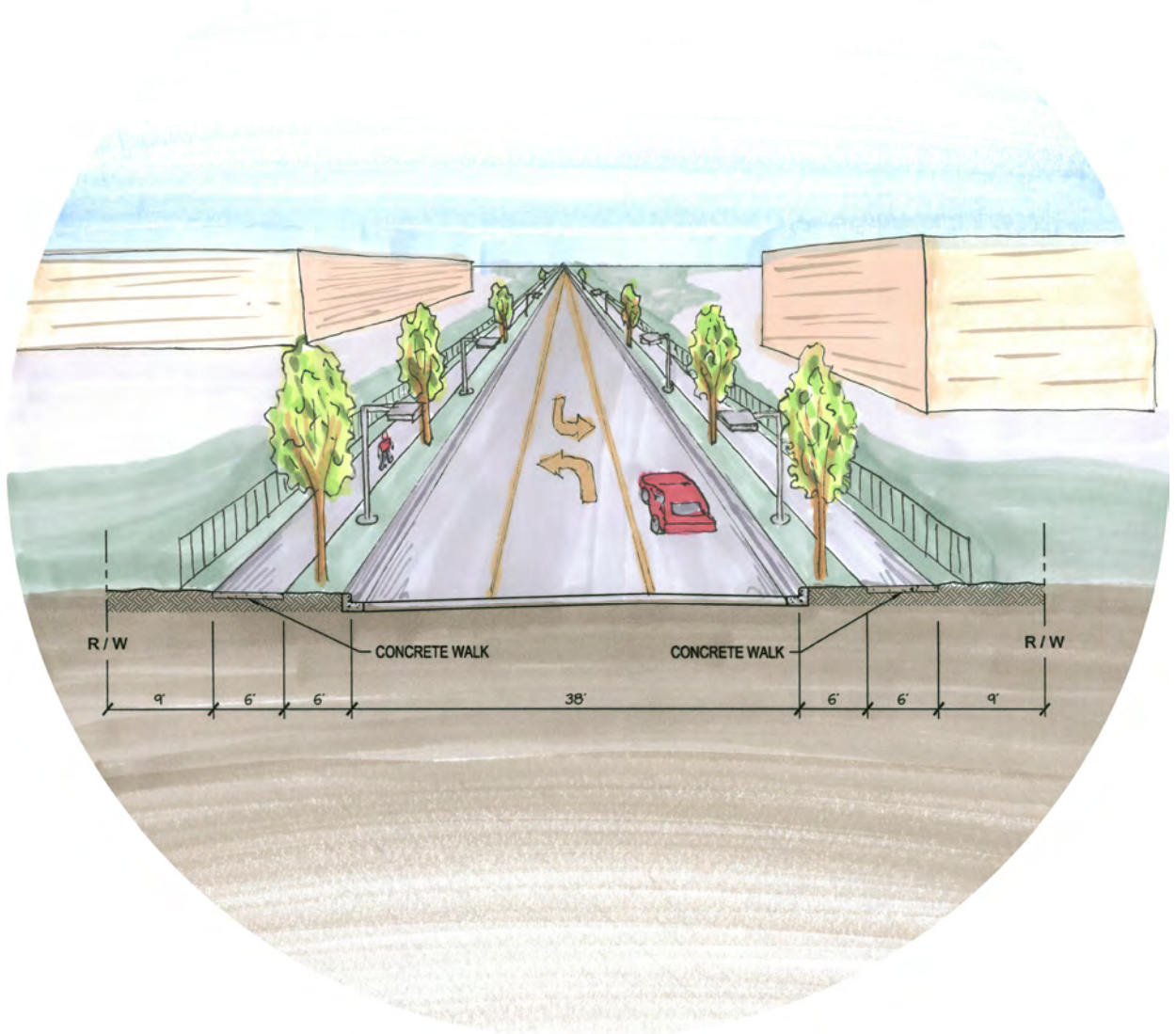
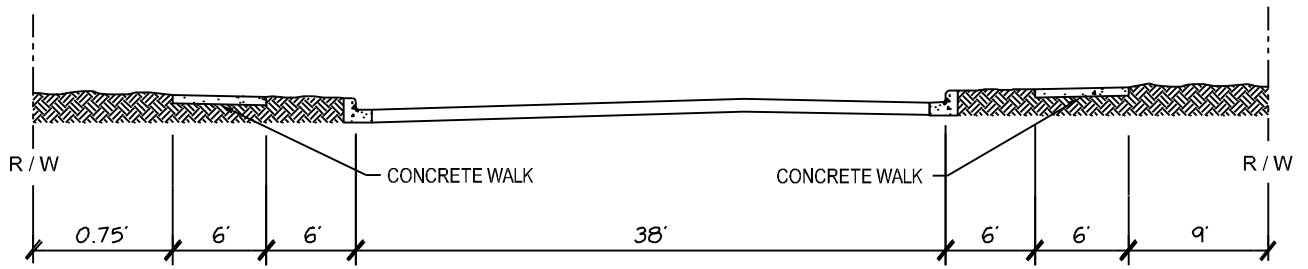
Design Review

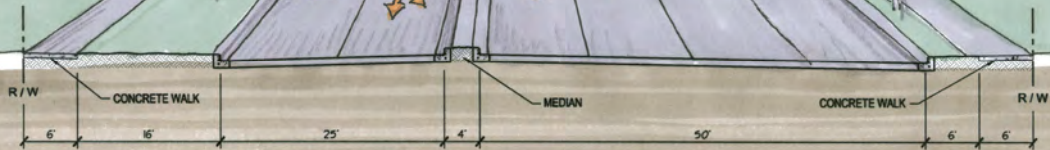
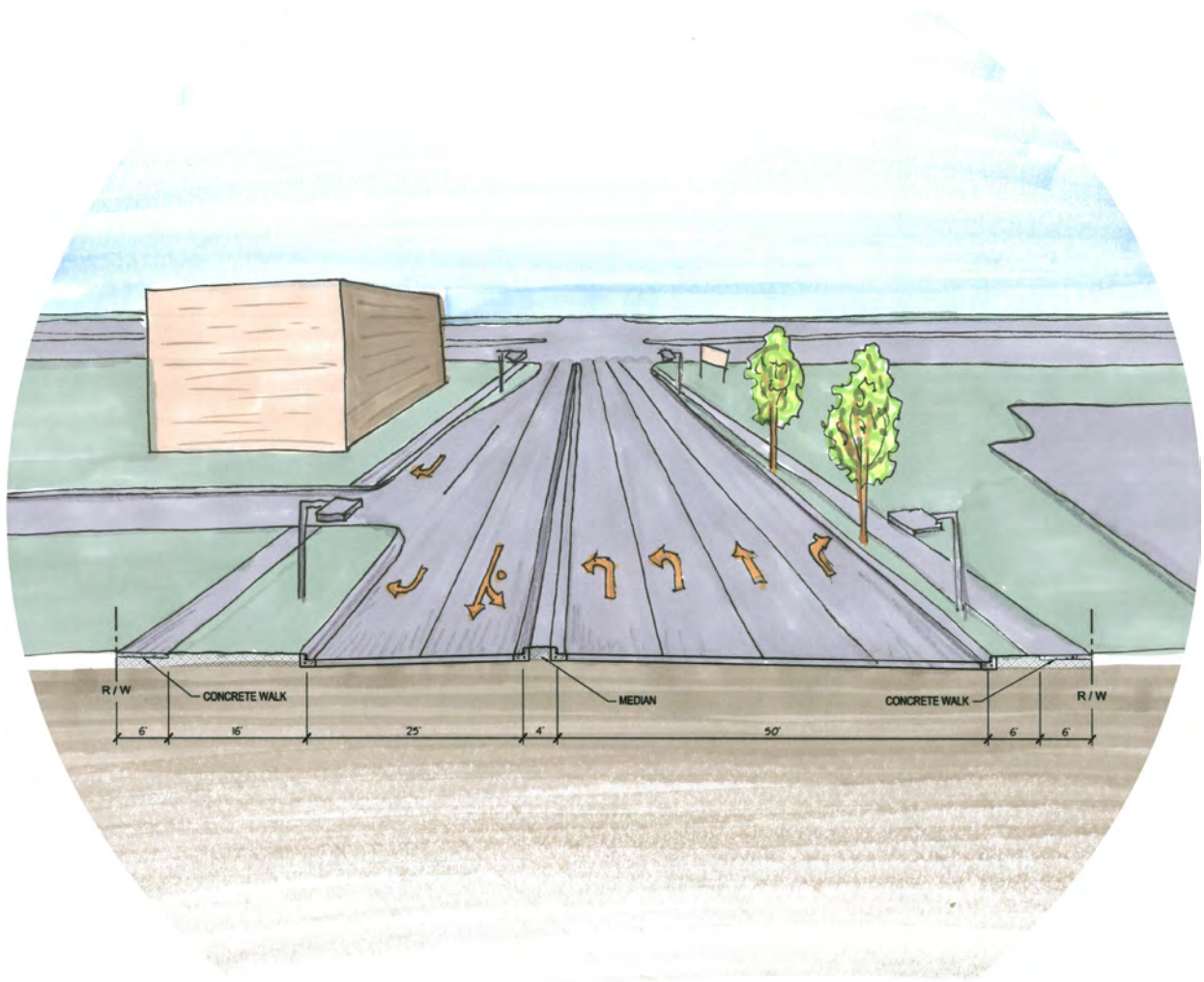
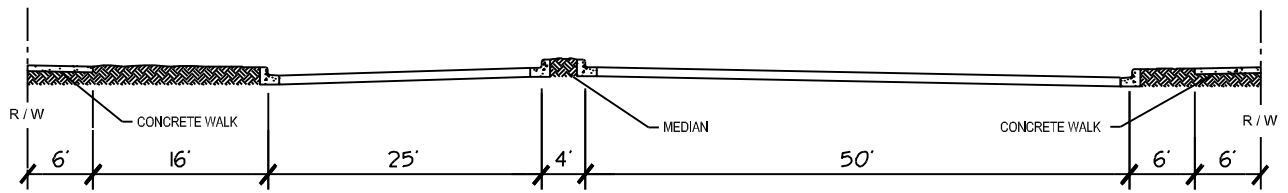
The Design Framework will be administered by staff as part of the development review process. Staff will incorporate design review to ensure compliance with the Design Framework as a standard step in the development review process.

This page intentionally left blank

APPENDIX A: SPECIFICATIONS AND DETAILS







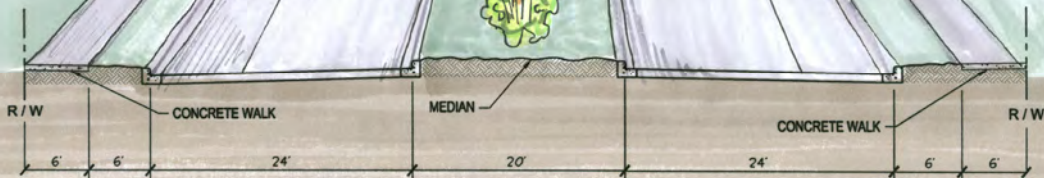
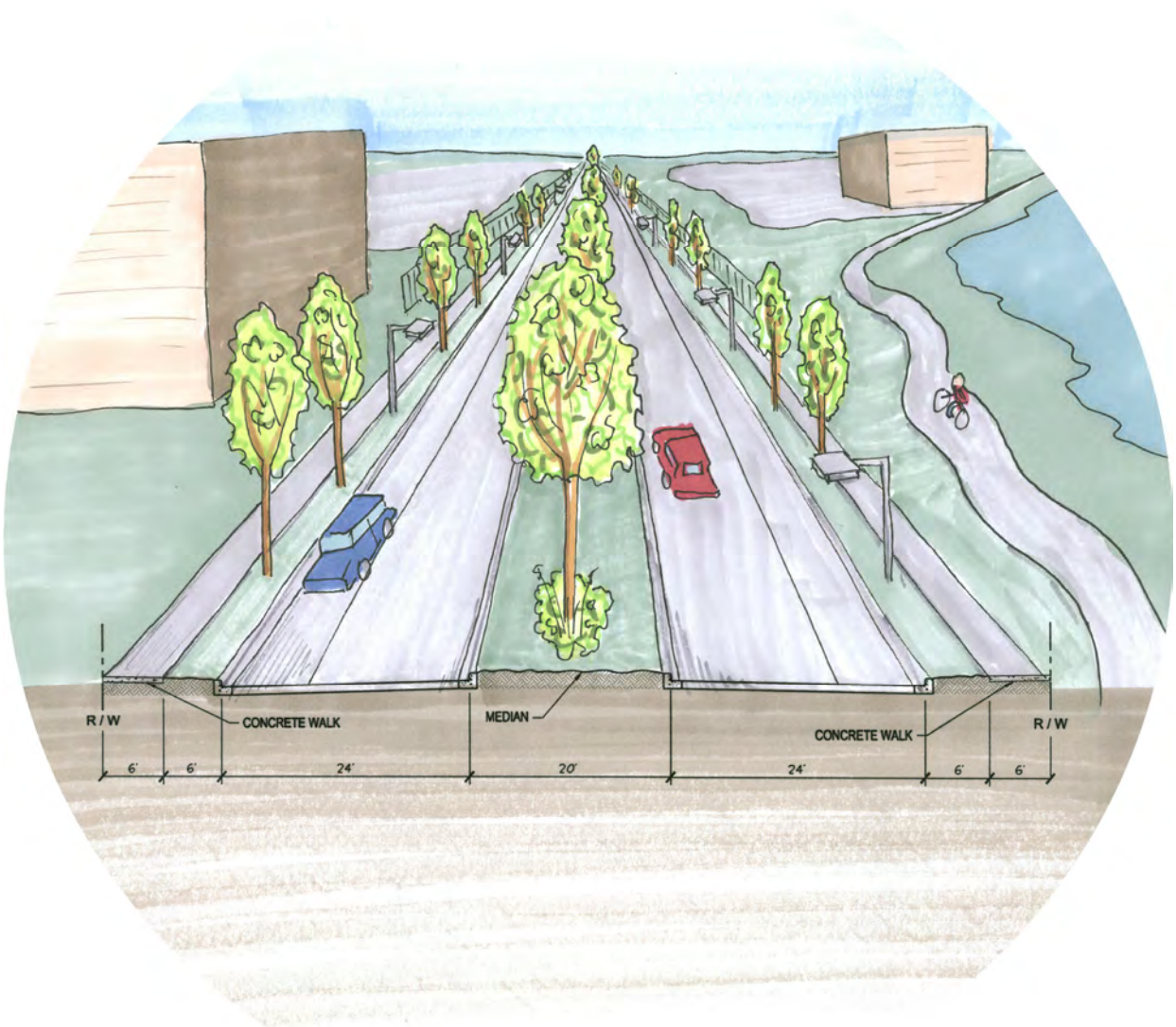
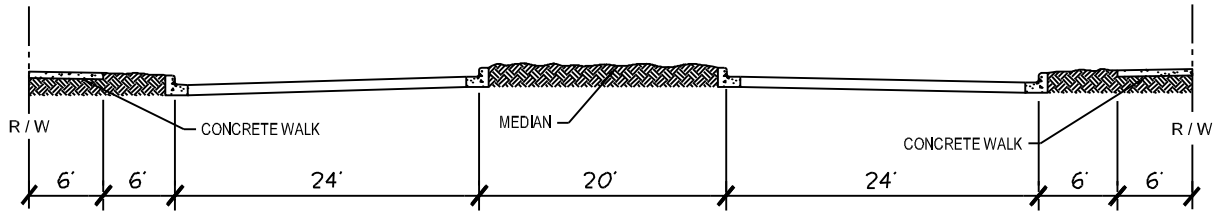
•
•

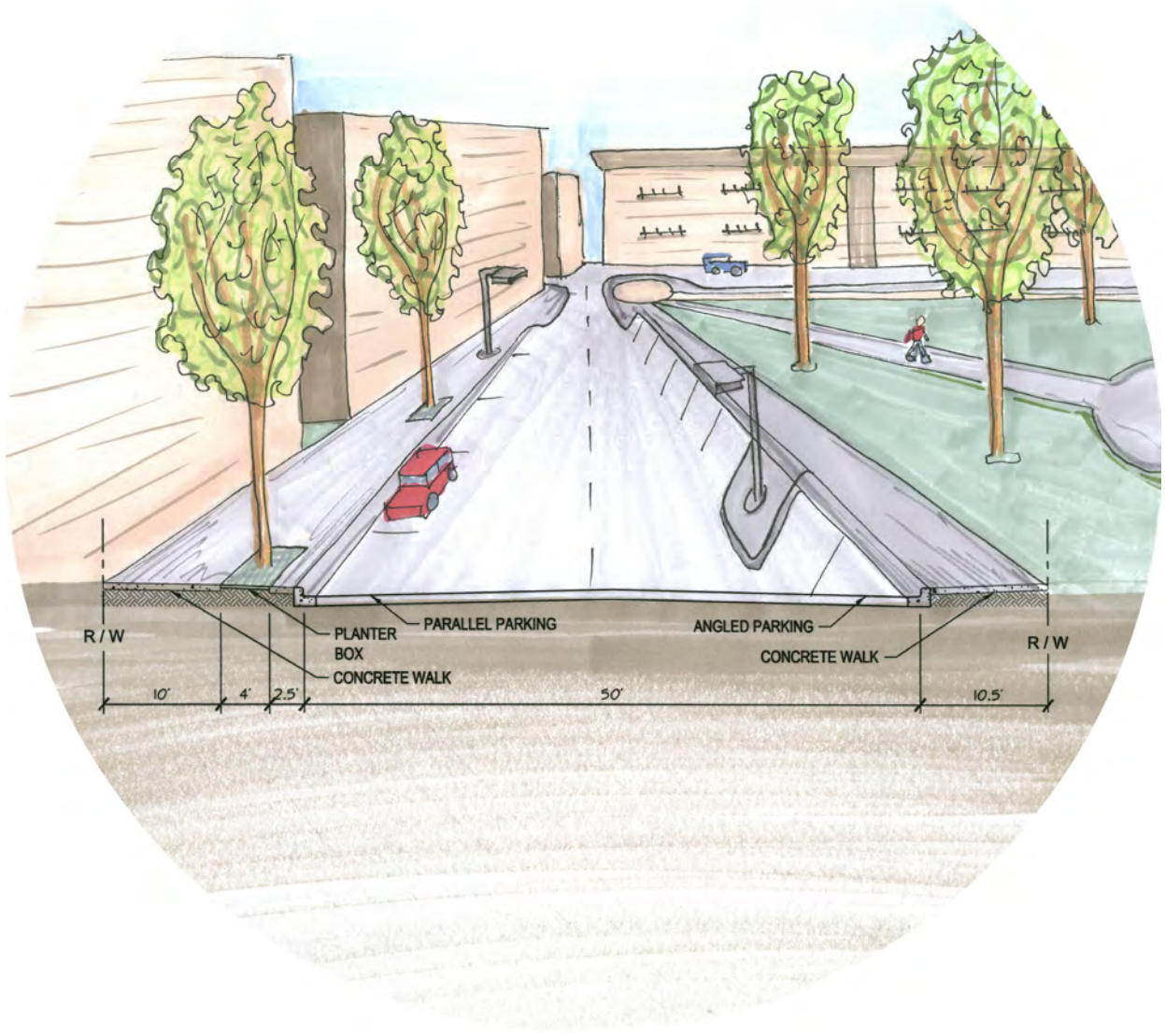
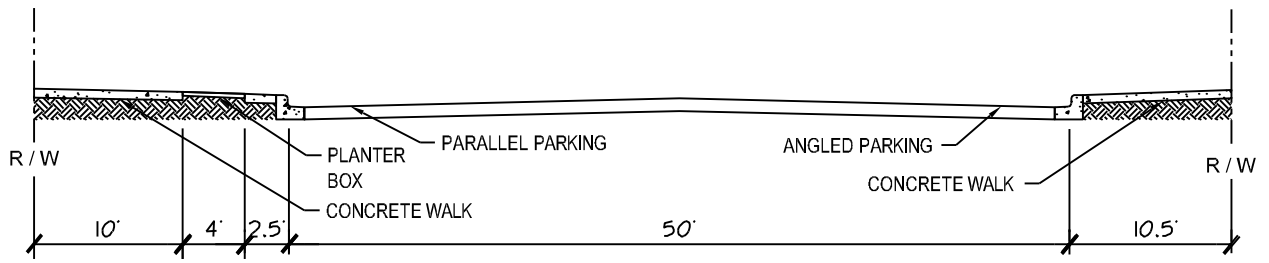
L A N D F O R M

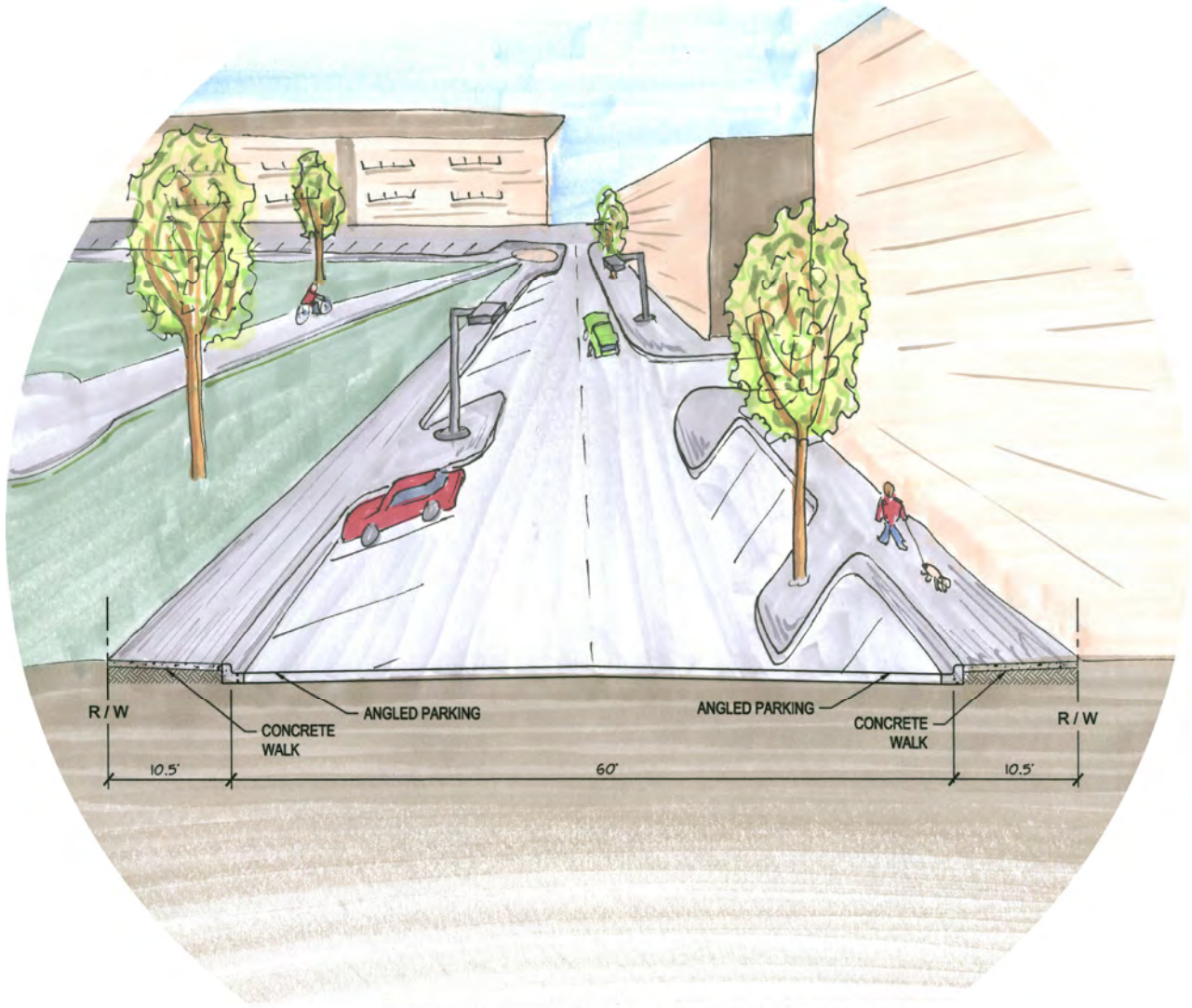
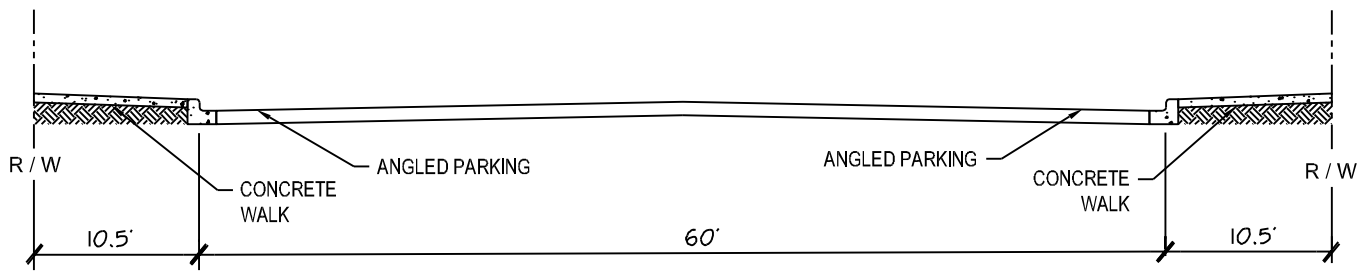
 From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
 PERSPECTIVE SECTION #3
 Ramsey Pkwy. West of Roundabout





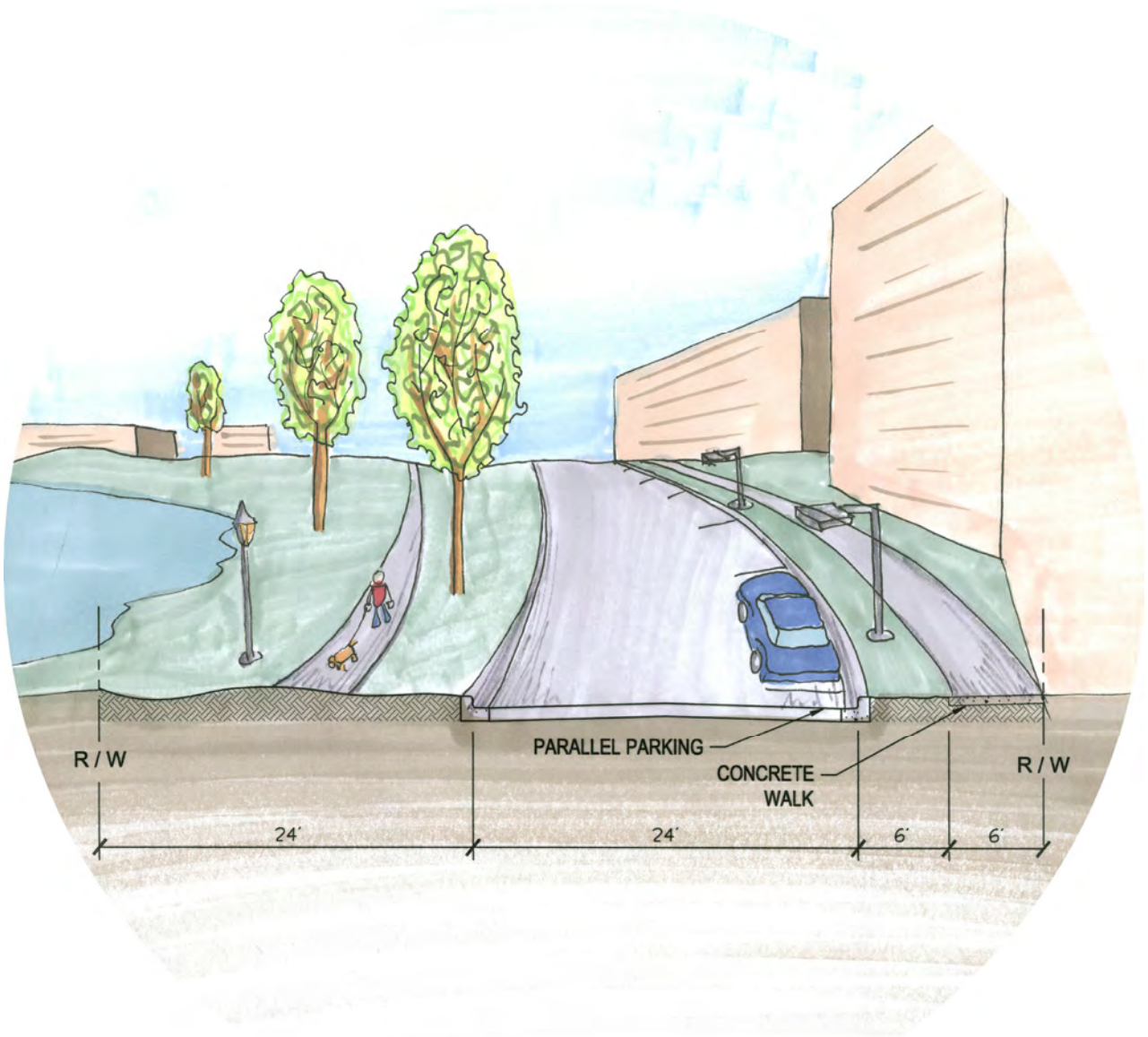
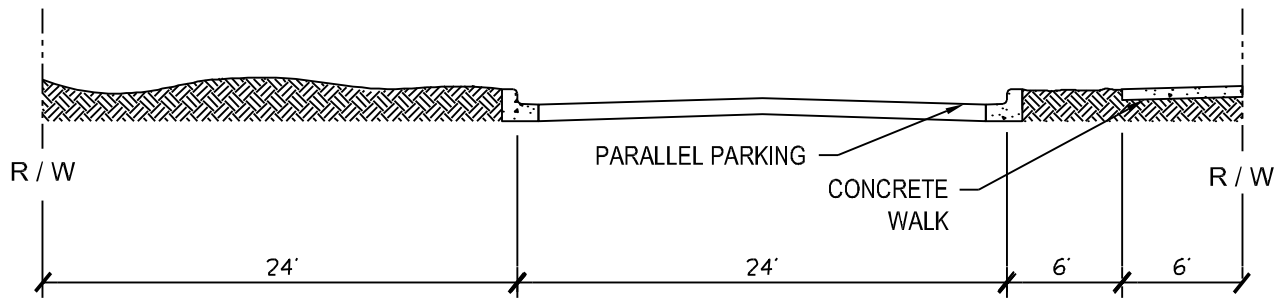




LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PERSPECTIVE SECTION #6
Center St. East of Municipal Plaza





● ●
L A N D F O R M
 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
 PERSPECTIVE SECTION #7
 Ramsey Pkwy. South of The Draw





EXHIBIT B.1

LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PYLON/COMMUNITY SIGN 'A'
Armstrong Blvd. & Highway 10

THE COR
RAMSEY, MINNESOTA



Eastbound

Westbound

EXHIBIT B.2



DESIGN FRAMEWORK
The COR at Ramsey

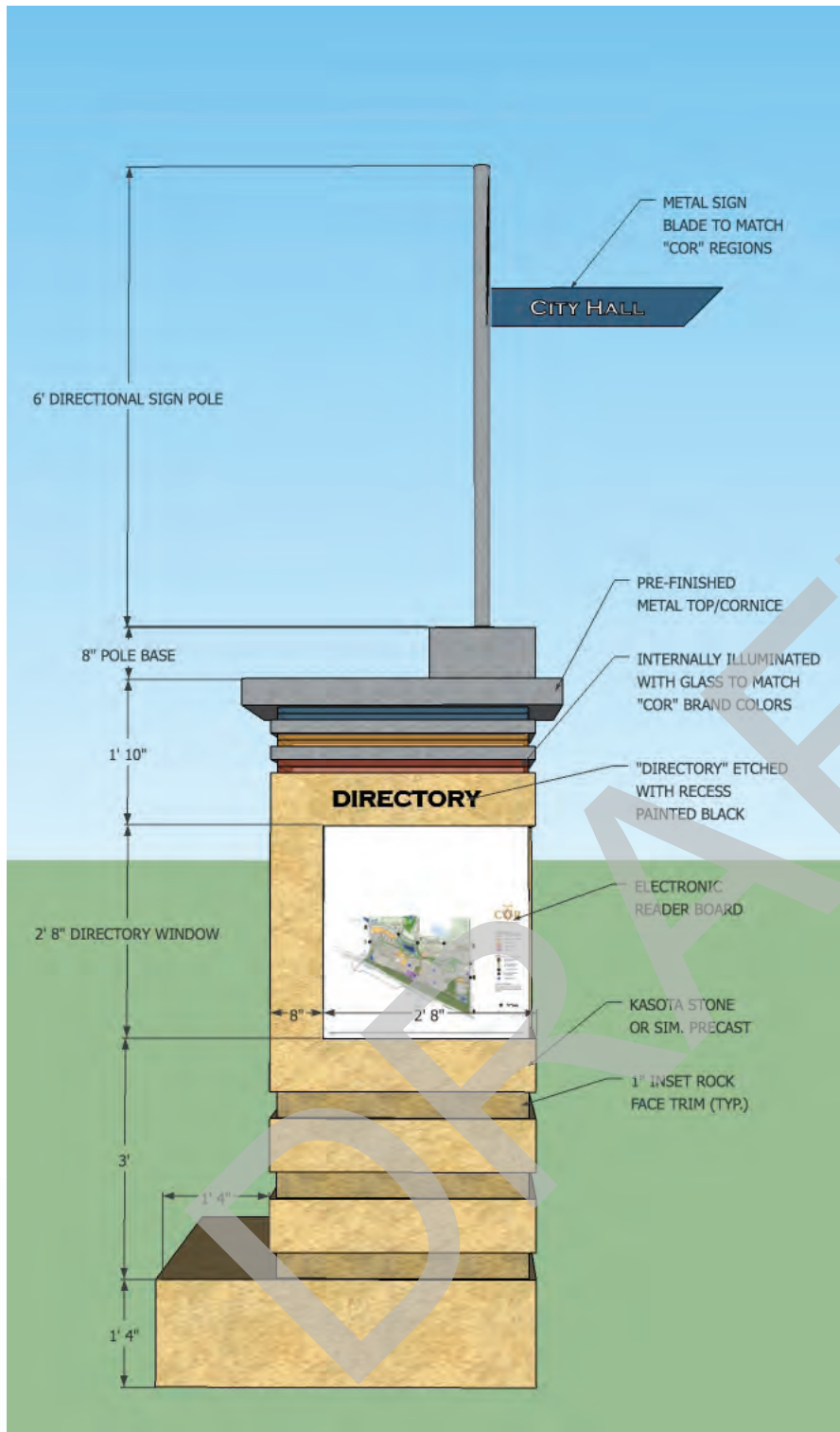
PYLON/COMMUNITY SIGN 'B'
Commuter Rail & Highway 10





EXHIBIT B.3

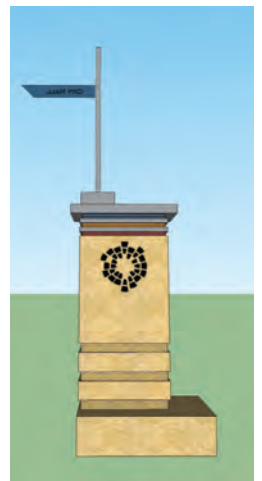
	<p>DESIGN FRAMEWORK The COR at Ramsey</p> <hr/> <p>PYLON/COMMUNITY SIGN 'C' Ramsey Blvd. & Highway 10</p>	
--	---	--



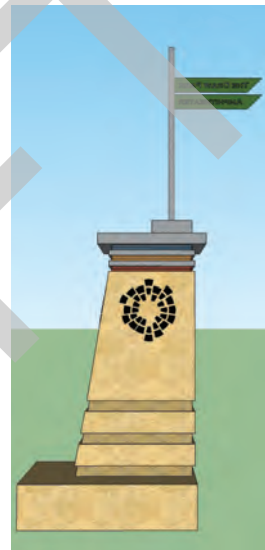
DIRECTORY PANEL VIEW



EVENTS PANEL VIEW



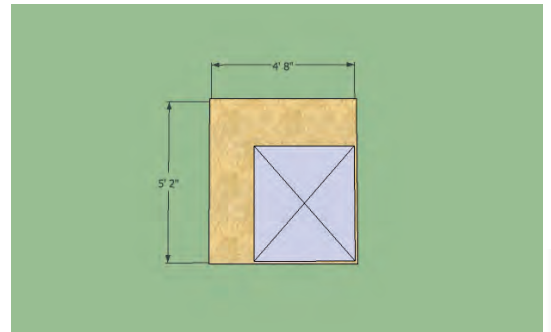
REAR VIEW



SIDE VIEW



PERSPECTIVE



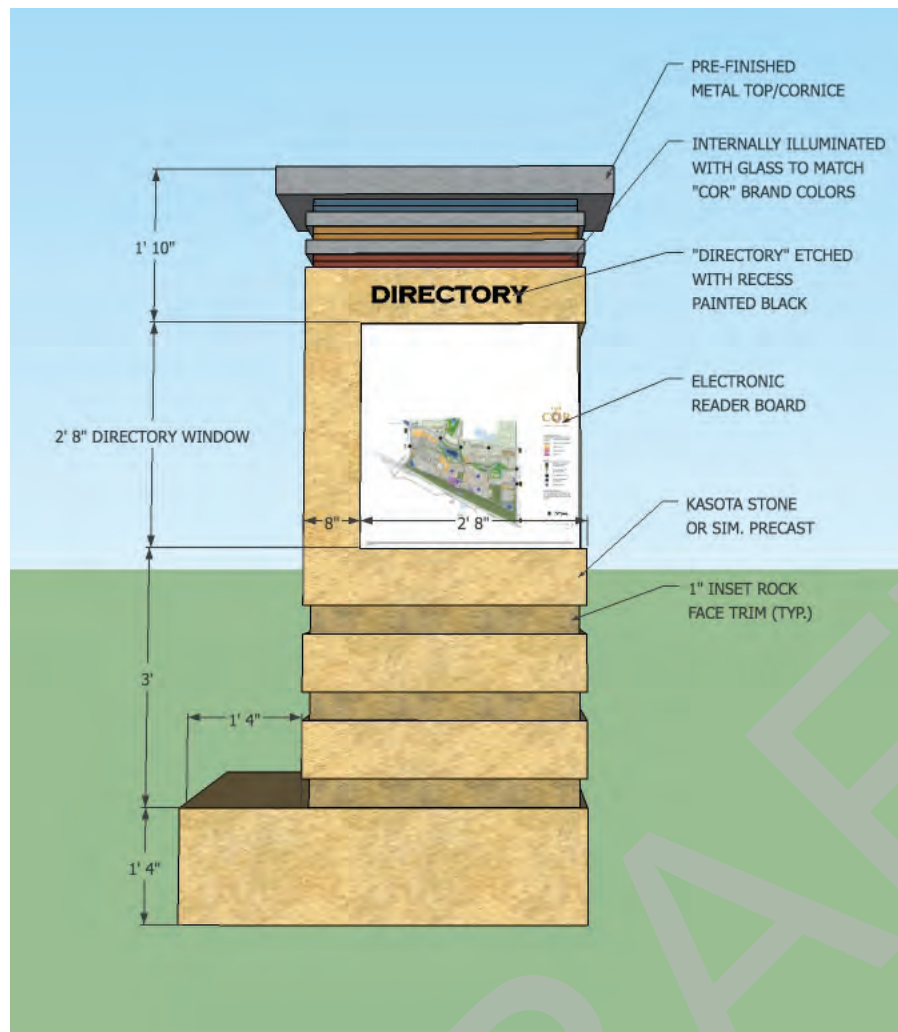
PLAN VIEW

EXHIBIT B.4

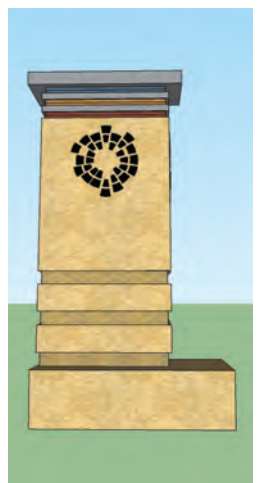


DESIGN FRAMEWORK
 The COR at Ramsey
 COMMUNITY KIOSK 'A'
 Commuter Rail & Veterans Dr.





EVENTS PANEL VIEW



REAR VIEW

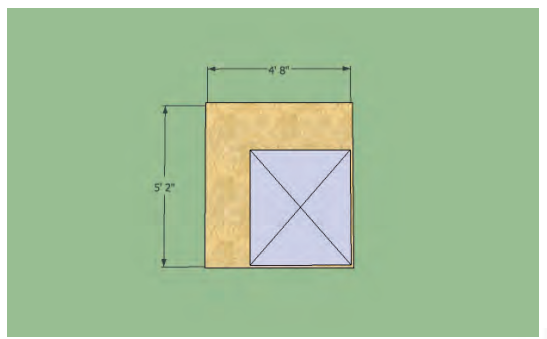


SIDE VIEW



PERSPECTIVE

DIRECTORY PANEL VIEW



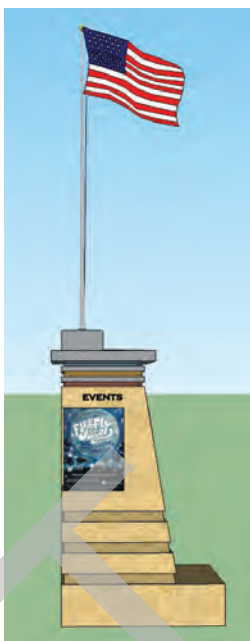
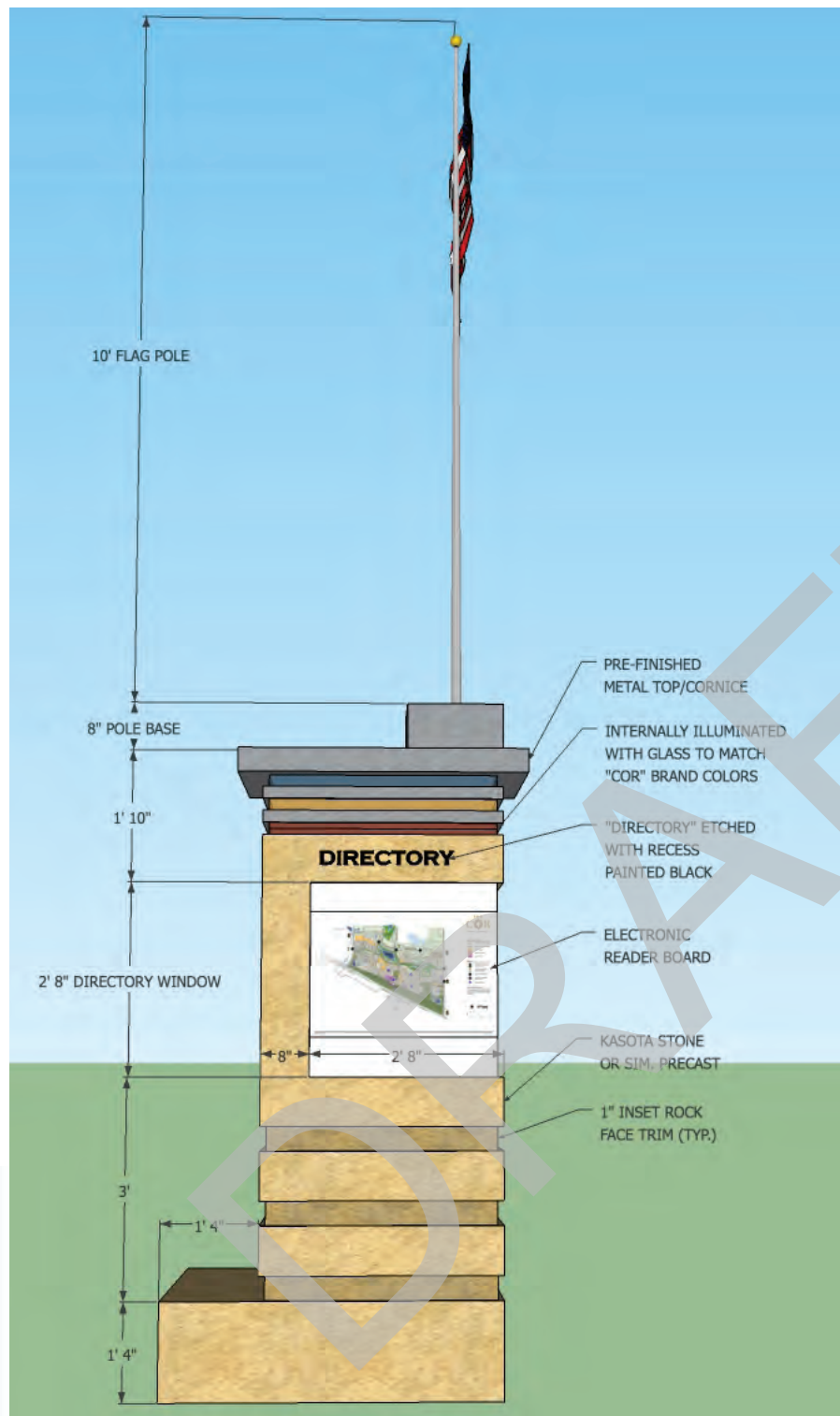
PLAN VIEW

EXHIBIT B.5

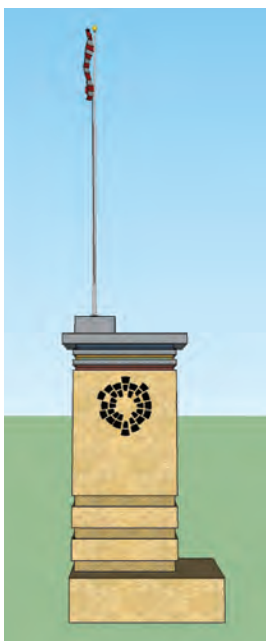
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
COMMUNITY KIOSK 'B'
Sunwood Dr. & Center St.

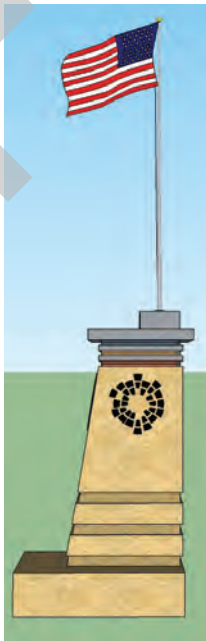
THE COR
RAMSEY, MINNESOTA



EVENTS PANEL VIEW



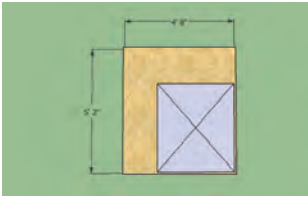
REAR VIEW



SIDE VIEW



PERSPECTIVE



PLAN VIEW

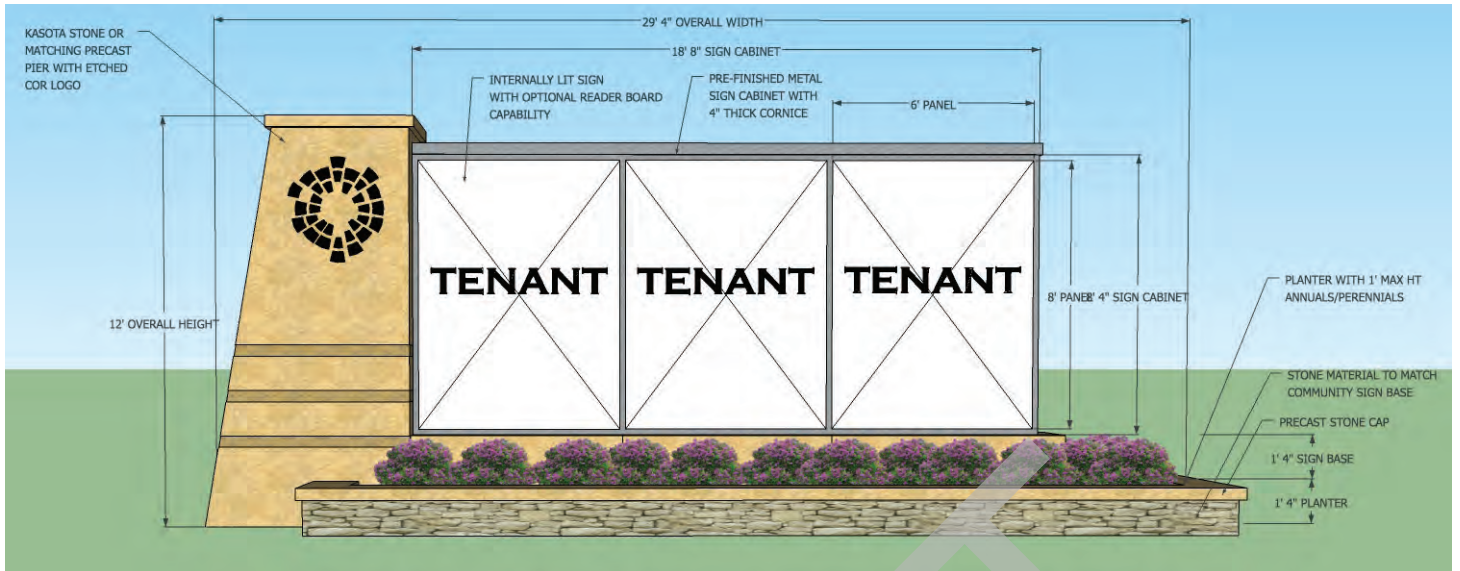
DIRECTORY PANEL VIEW

EXHIBIT B.6

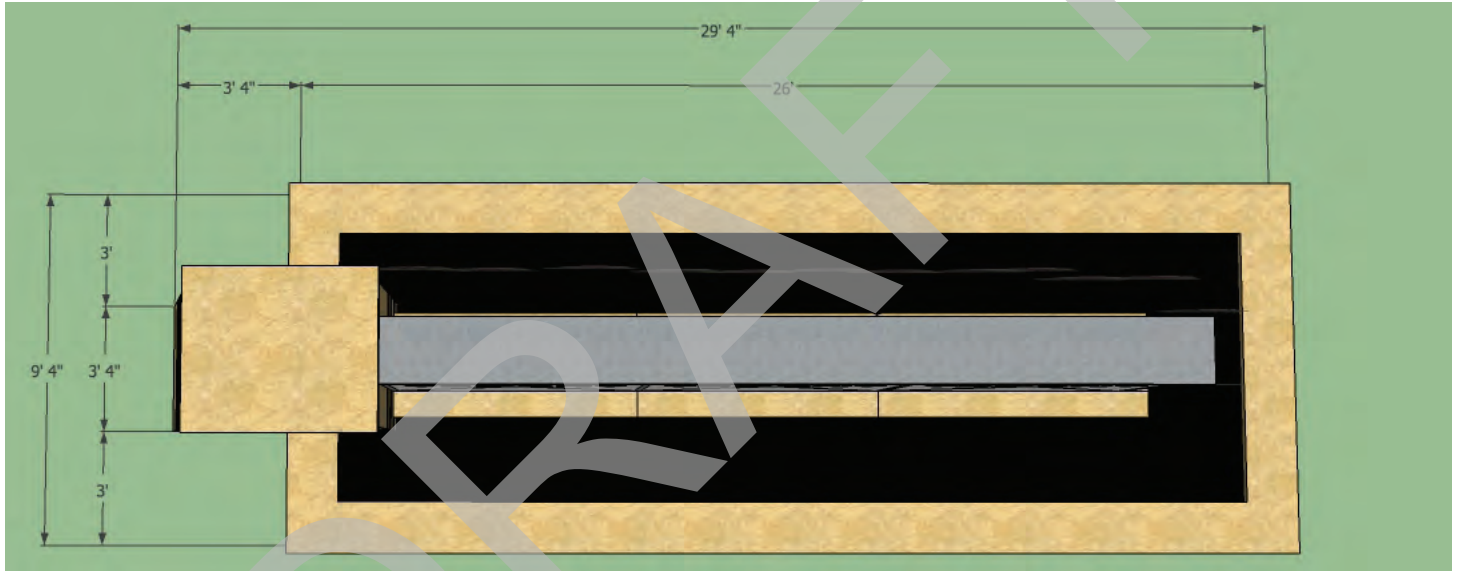
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
COMMUNITY KIOSK 'C'
Sunwood Dr. at Municipal Plaza

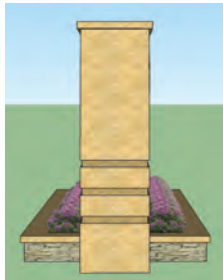
THE COR
RAMSEY, MINNESOTA



ELEVATION AT SUNWOOD



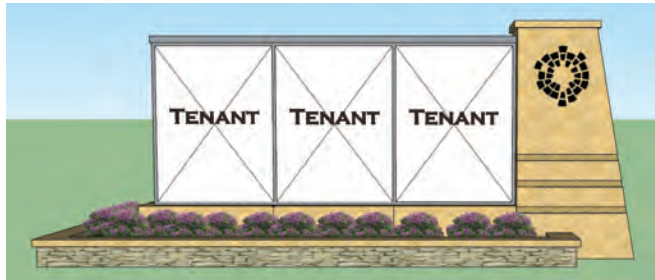
PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION

EXHIBIT B.7



DESIGN FRAMEWORK
The COR at Ramsey
 PROJECT GATEWAY SIGN 'A'
 Sunwood Dr. & Armstrong Blvd.



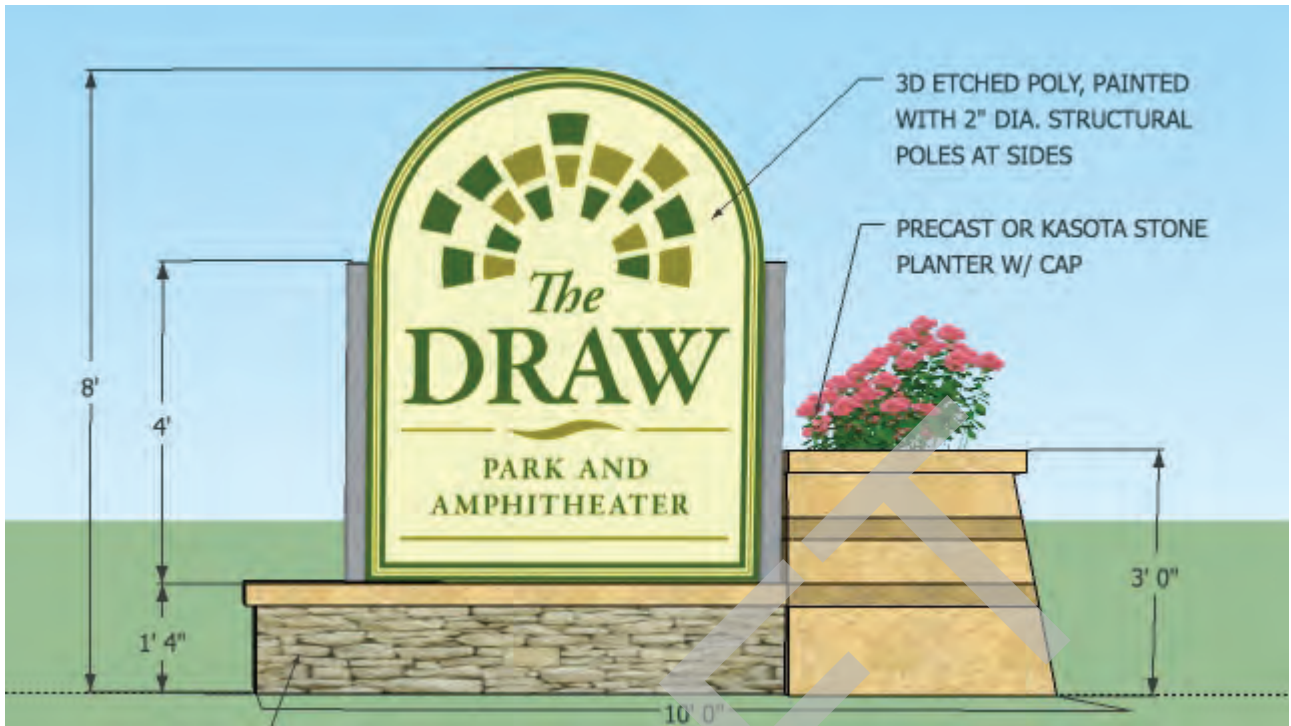
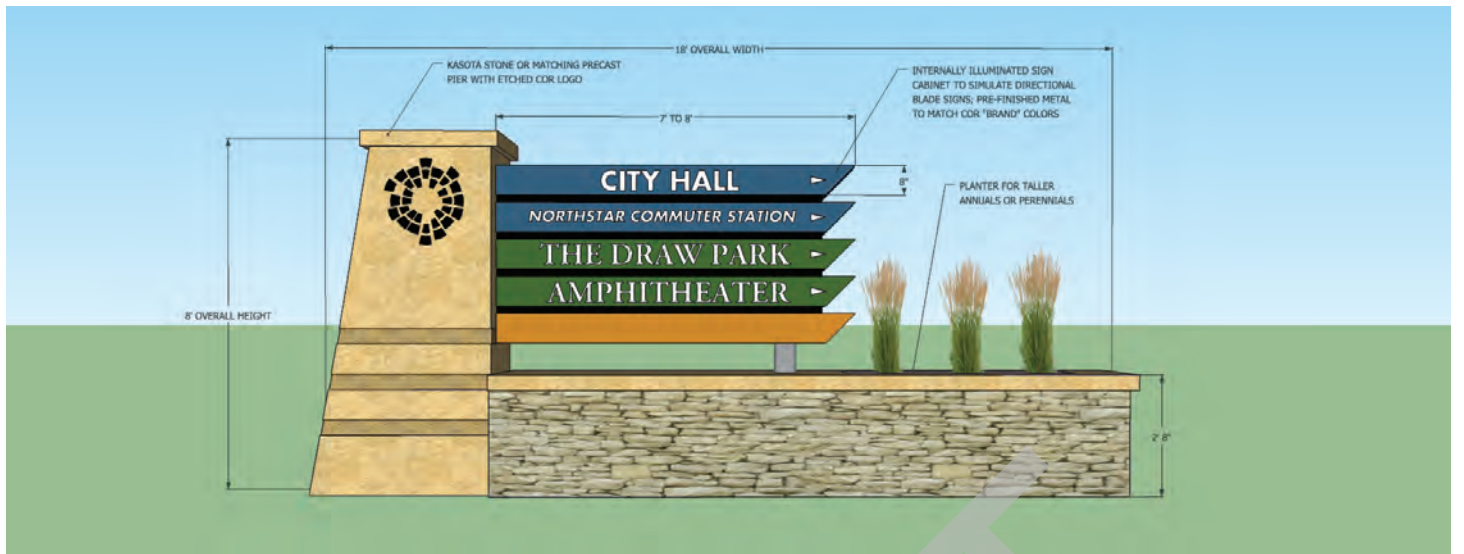
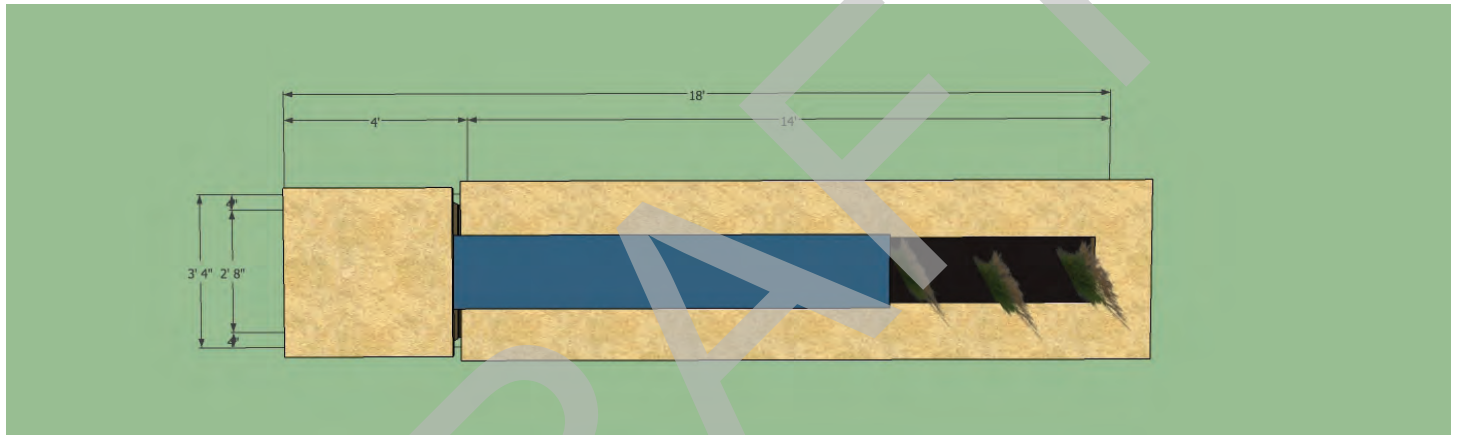


EXHIBIT B.8



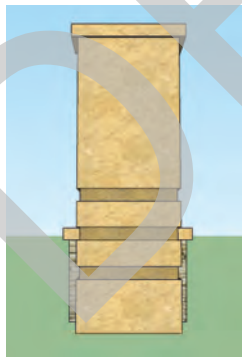
ELEVATION AT EASTBOUND BUNKER LAKE



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT WESTBOUND BUNKER LAKE

EXHIBIT B.9

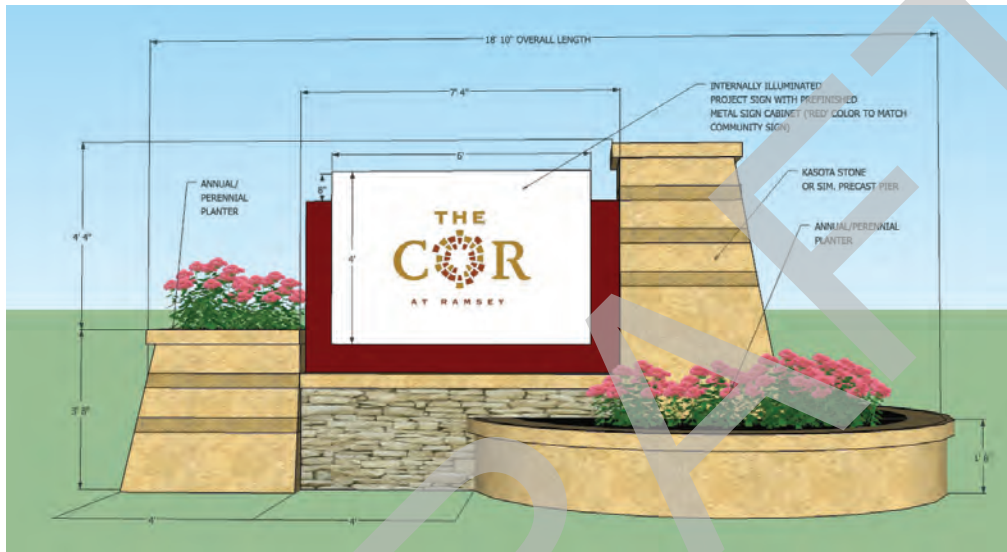
LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
PROJECT GATEWAY SIGN 'C'
Bunker Lake Blvd. & Center St.

THE
COR
RAMSEY, MINNESOTA



ELEVATION AT SOUTH BOUND RAMSEY



ELEVATION AT WESTBOUND BUNKER LAKE



PERSPECTIVE

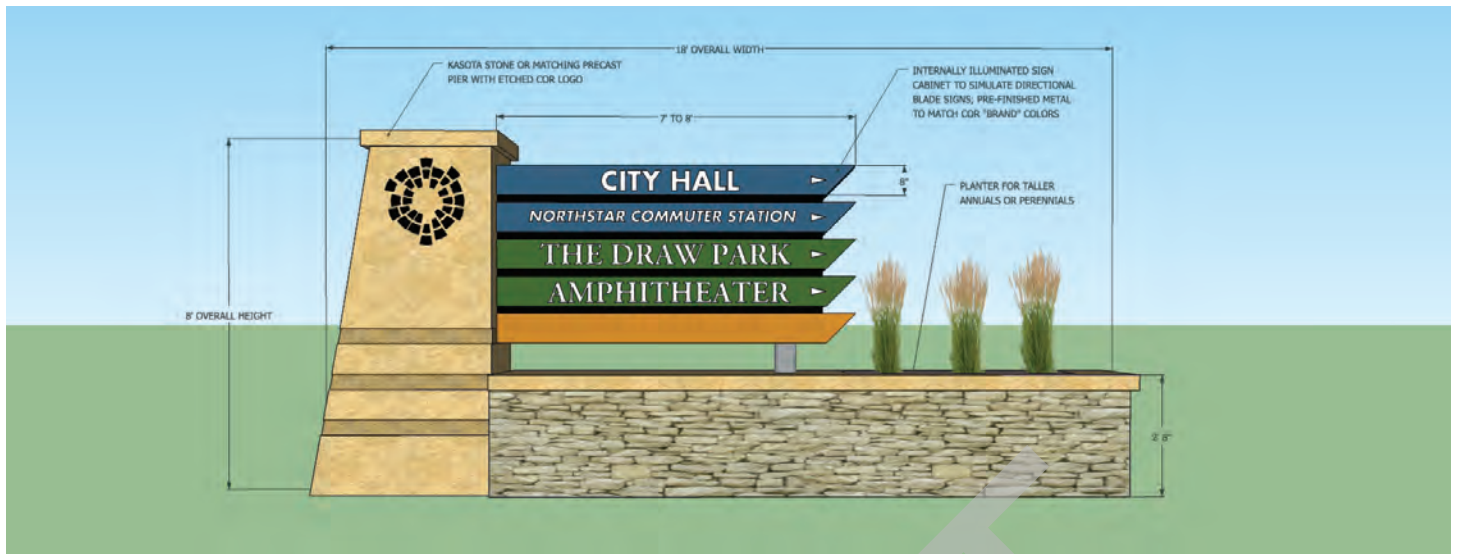
EXHIBIT B.10



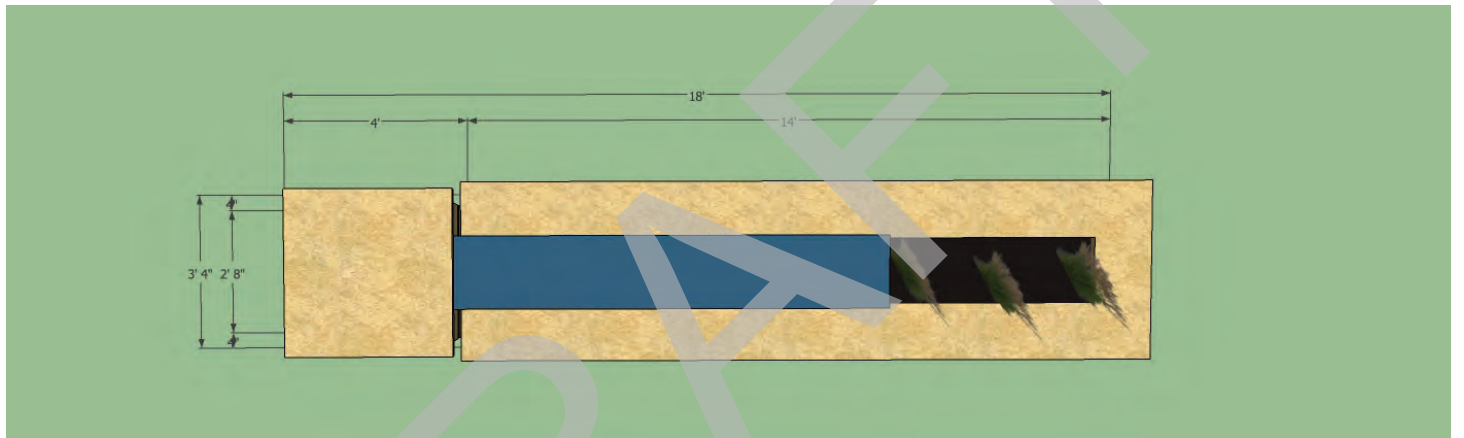
DESIGN FRAMEWORK
The COR at Ramsey

PROJECT GATEWAY SIGN 'D'
 Bunker Lake Blvd. & Ramsey Blvd.





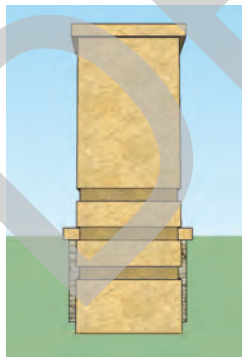
ELEVATION AT SOUTHBOUND RAMSEY



PLAN VIEW



PROFILE VIEW



PROFILE VIEW



ELEVATION AT NORTHBOUND RAMSEY

EXHIBIT B.11



DESIGN FRAMEWORK
The COR at Ramsey
 PROJECT GATEWAY SIGN 'E'
 Sunwood Dr. & Ramsey Blvd.





EXHIBIT B.12

•
•
•
•
•

L A N D F O R M

 From Site to Finish

DESIGN FRAMEWORK
 The COR at Ramsey
 PUBLIC PARKING SIGNAGE
 Standard Signage

THE
COR
 RAMSEY, MINNESOTA

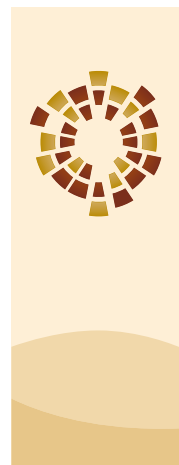
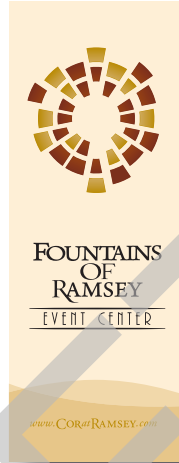
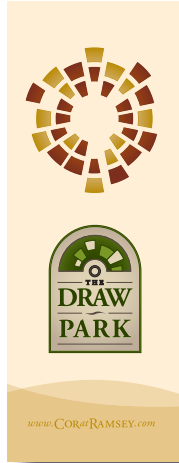


EXHIBIT B.13

LANDFORM
From Site to Finish

DESIGN FRAMEWORK
The COR at Ramsey
LIGHT POLE BANNER SIGNS
Standard Signage





Armstrong Boulevard

The Waterfront

Bunker Lake Boulevard



Ramsey Parkway

The Draw

Affinity Development



Municipal Plaza

Highway 10

Ramsey City Hall

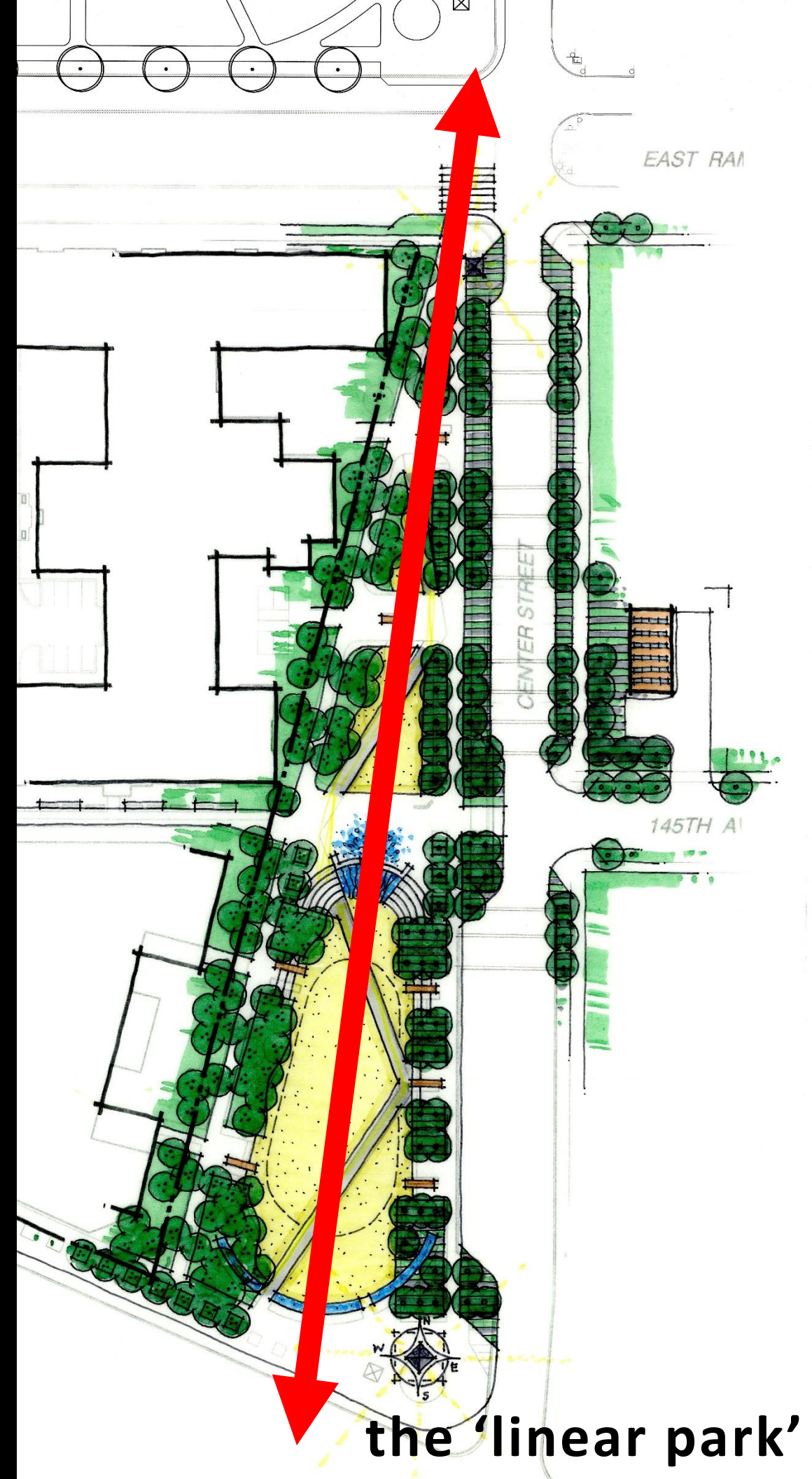
Sunwood Drive

**Pump House 3
context map**

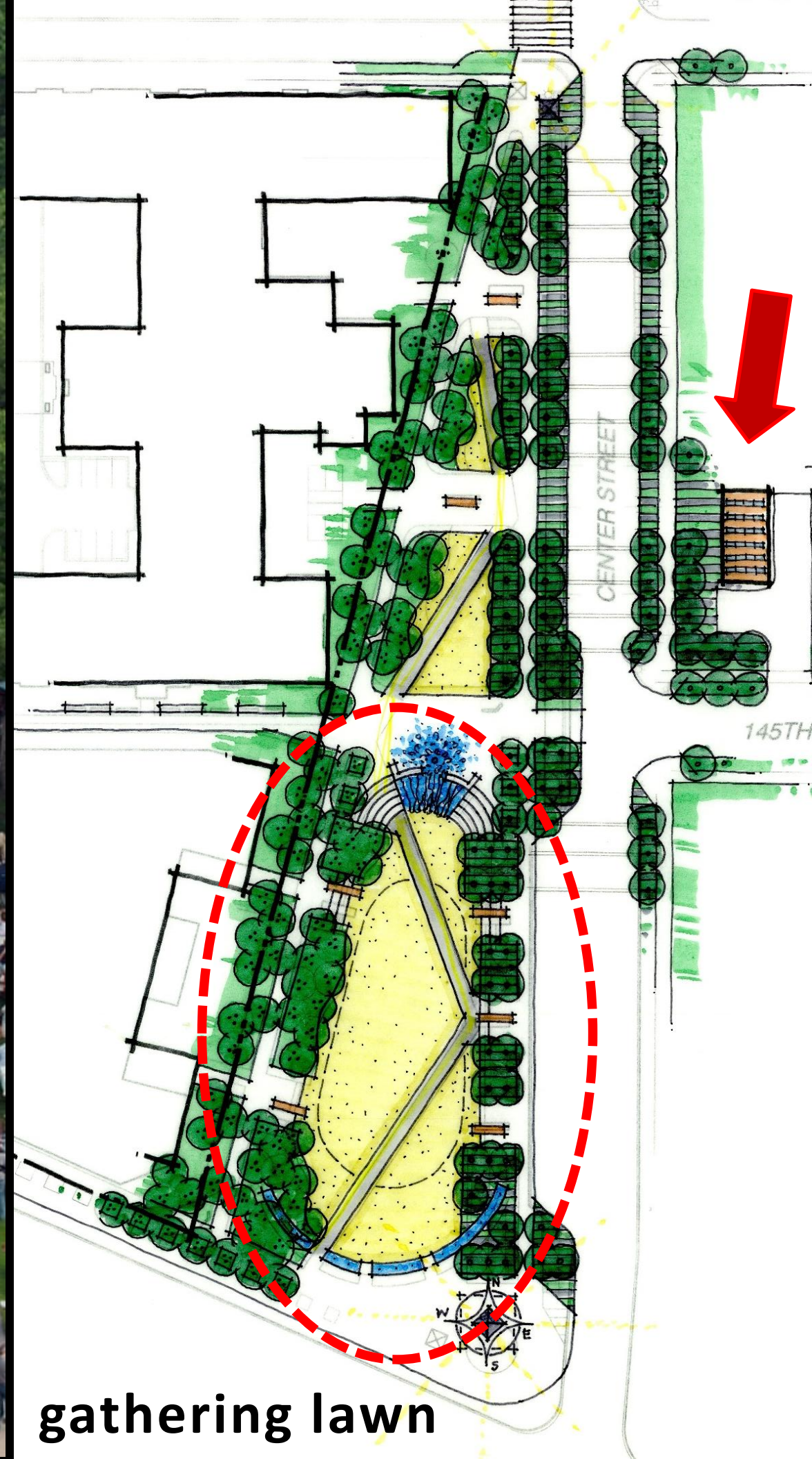
RAMSEY COR – PARK SYSTEM COMPONENTS



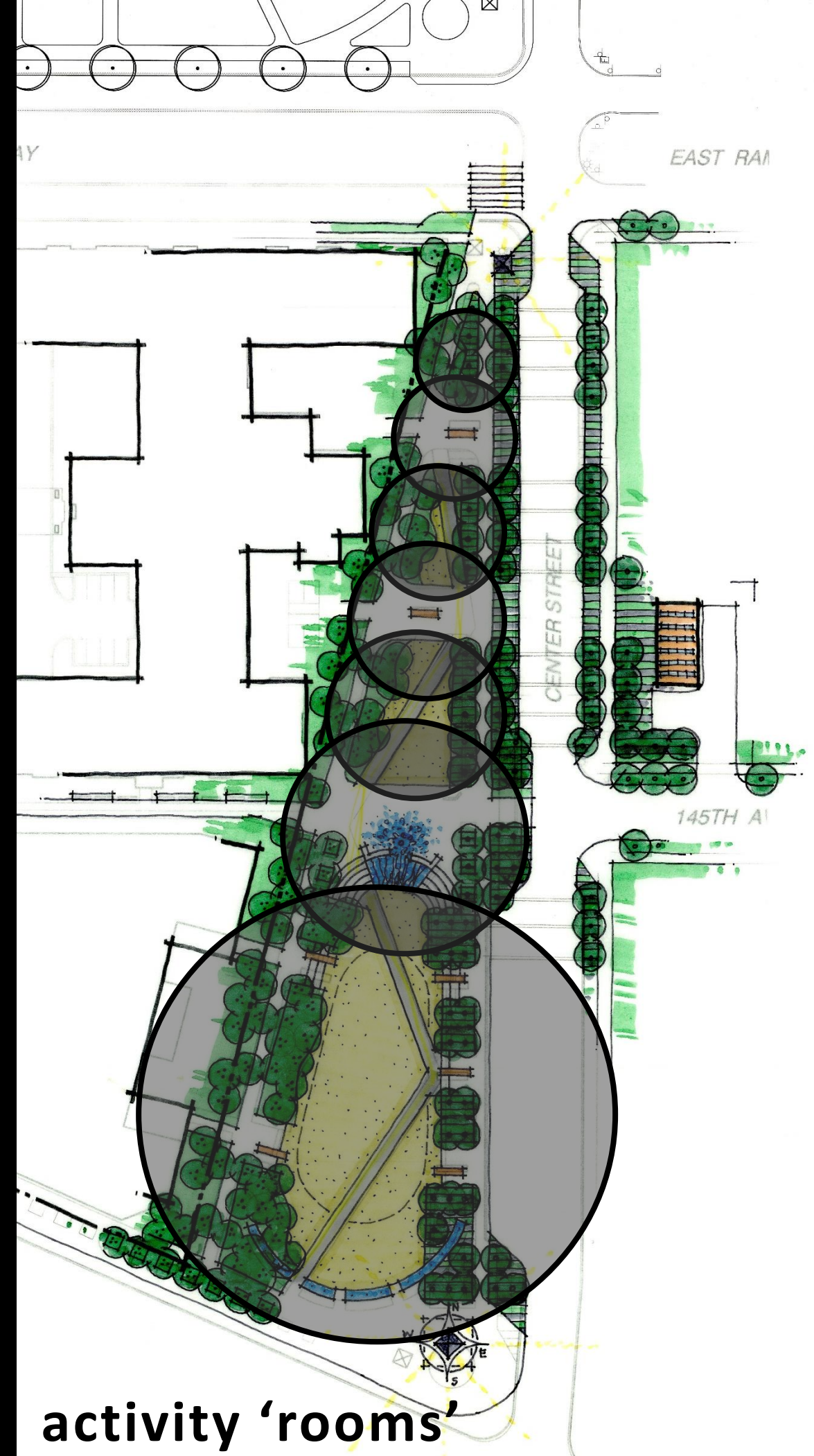
Municipal Plaza
(previous design study) **Pump house 3**



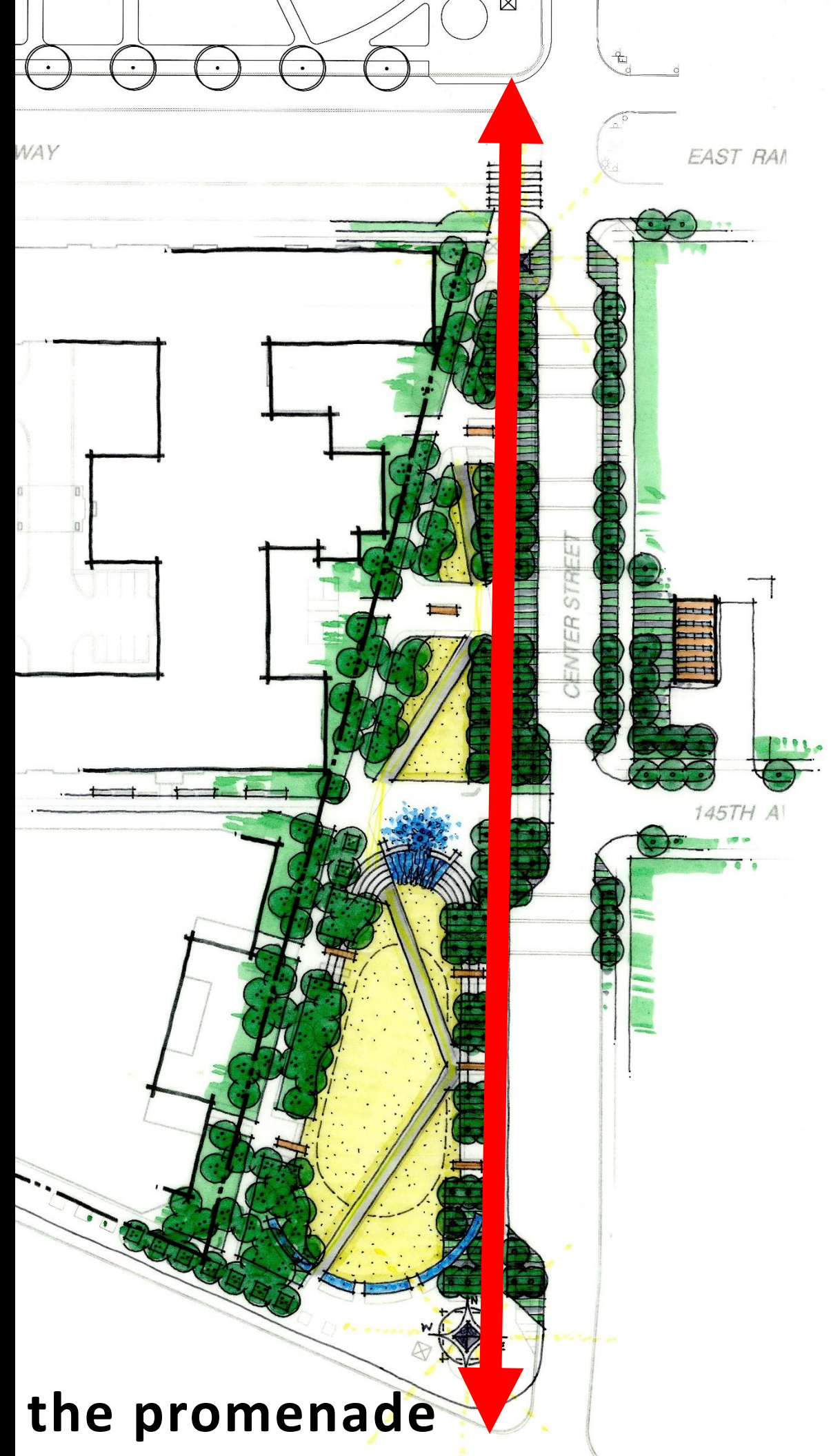
the 'linear park'

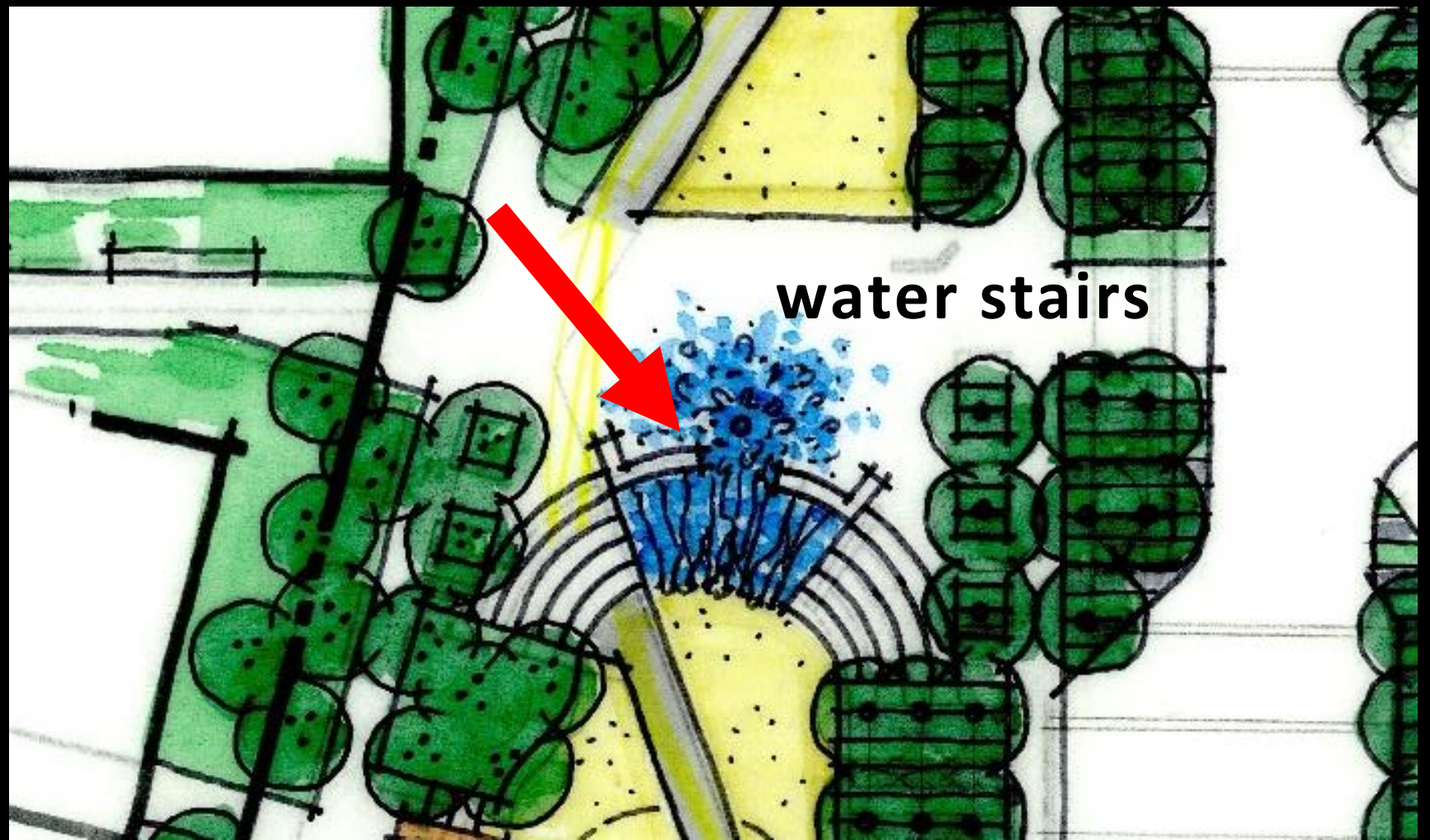


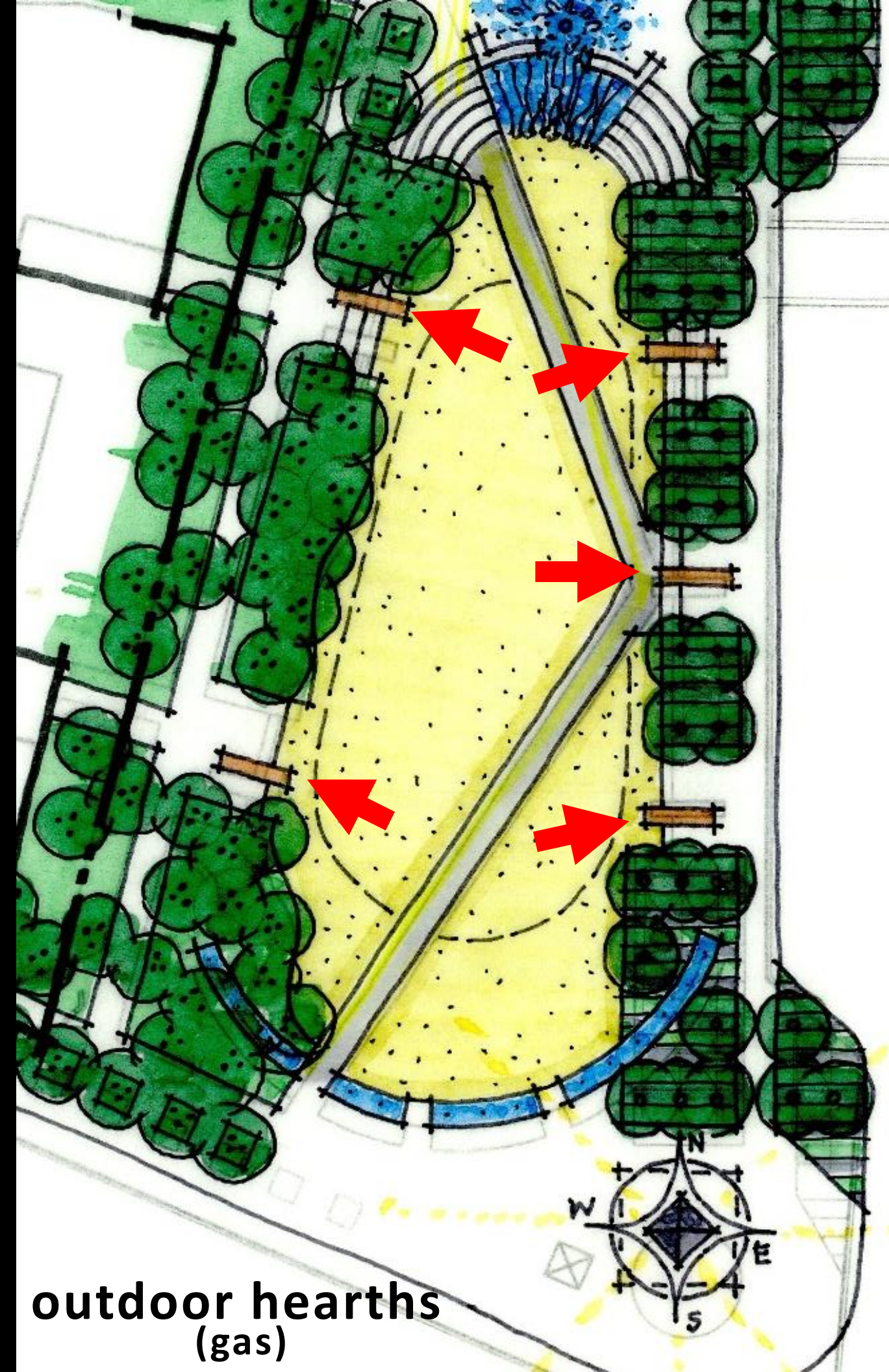
gathering lawn



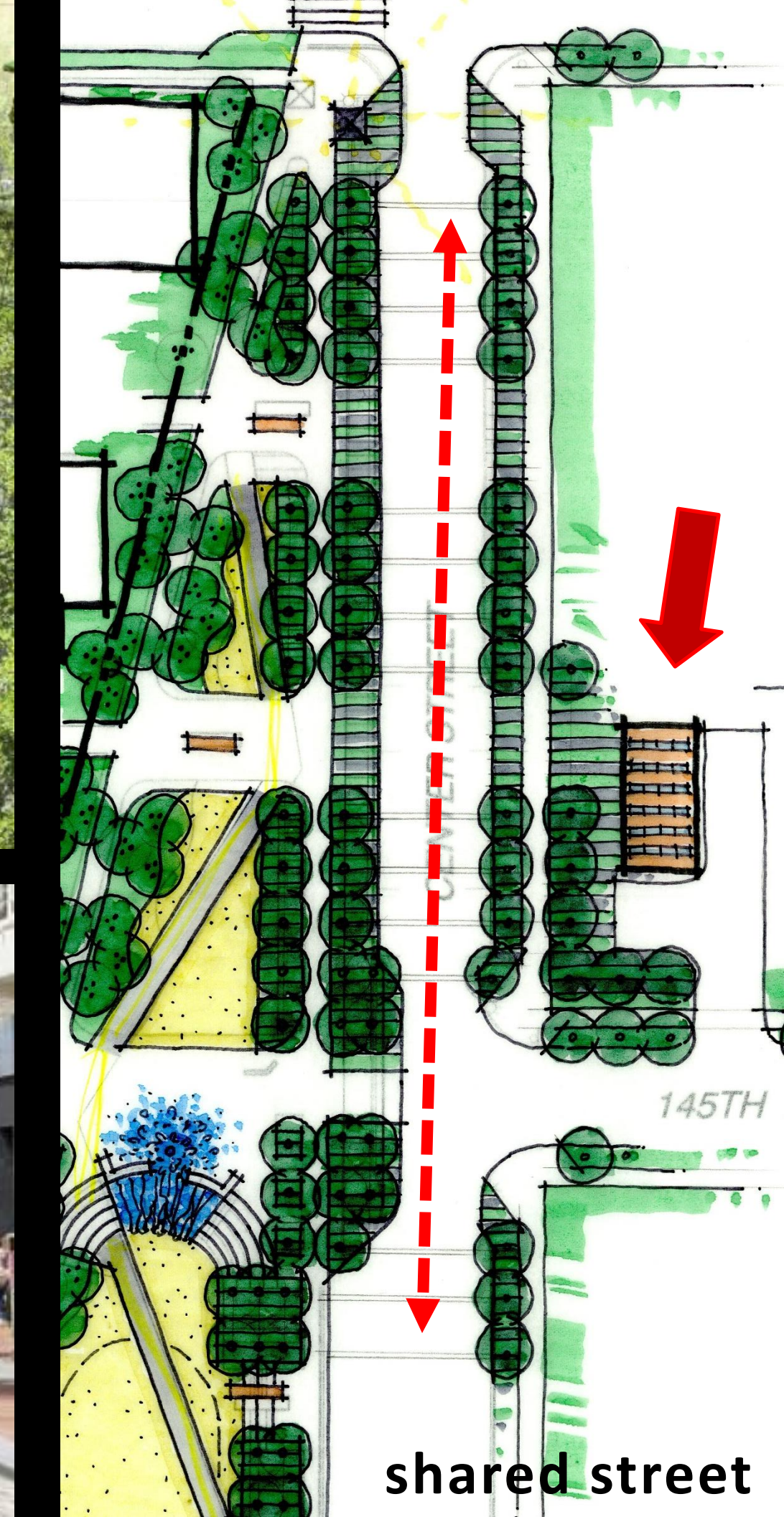
activity 'rooms'





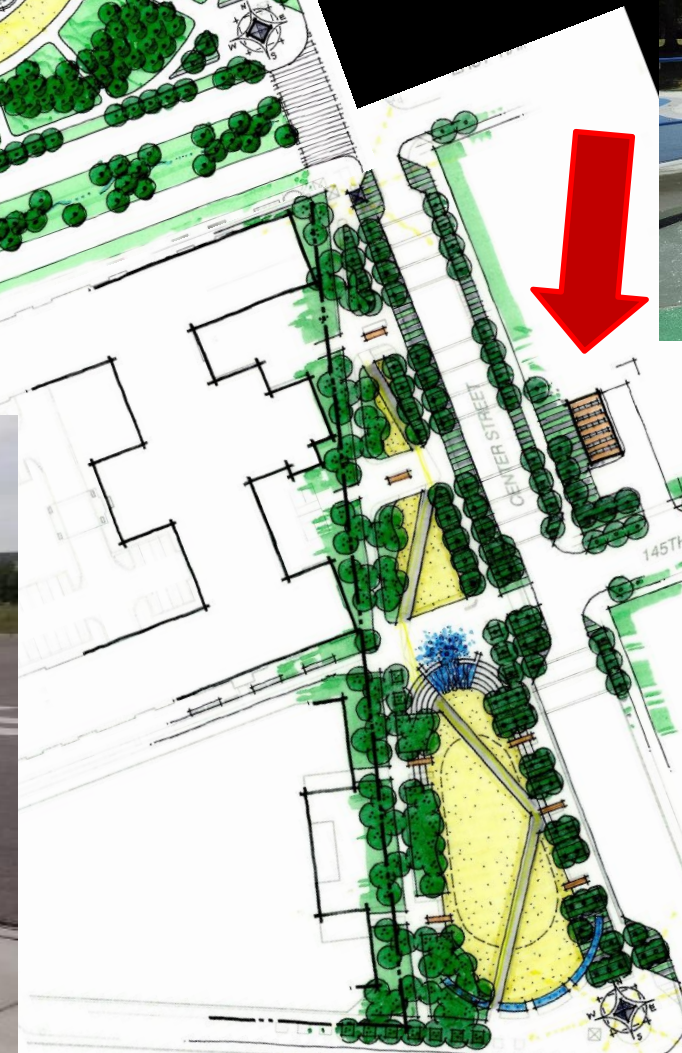
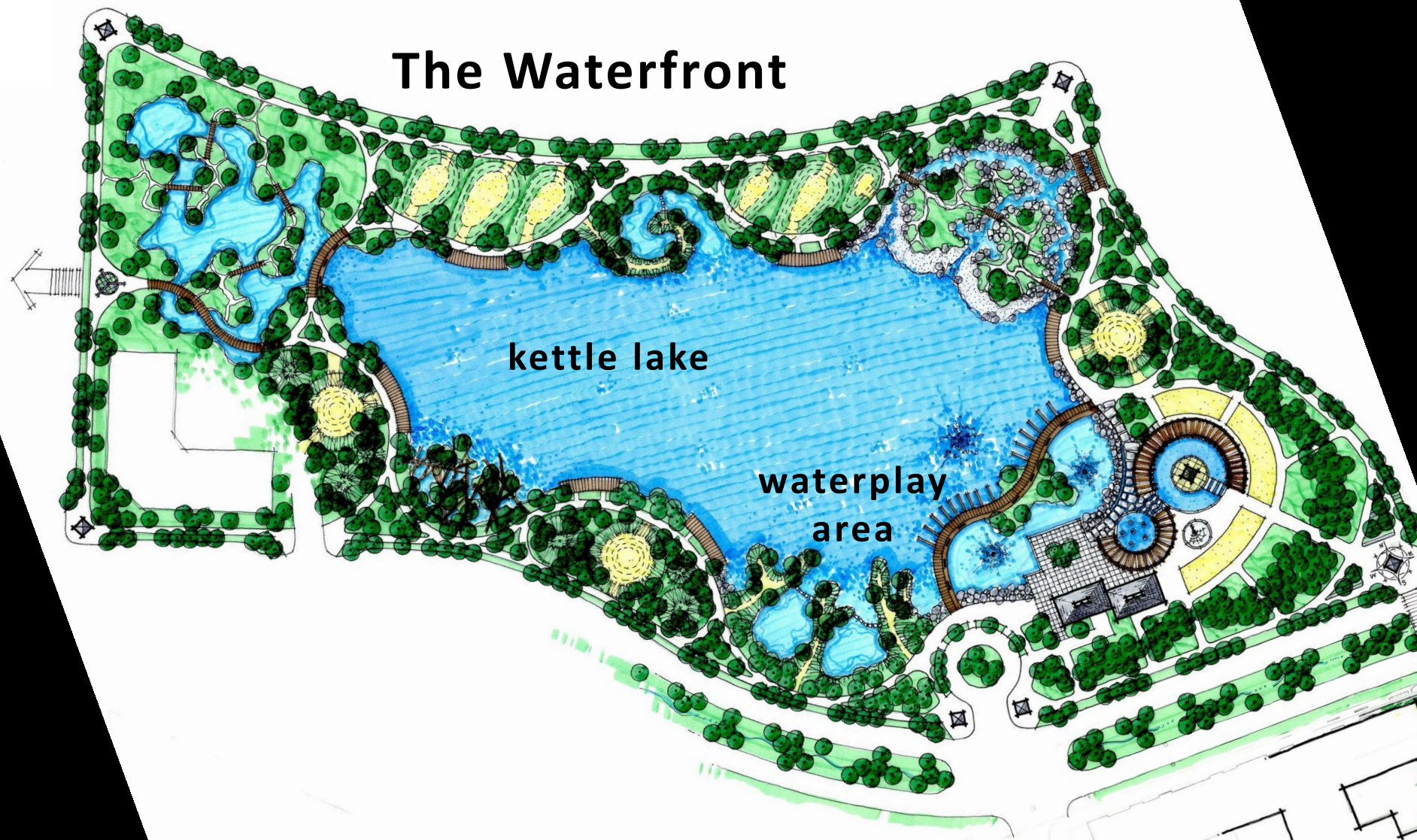


outdoor hearths
(gas)



shared street

The Waterfront



Municipal Plaza

















DANGER!
EAR PROTECTION
REQUIRED IN
THIS AREA