



# **TRAIL MAINTENANCE POLICY**

**City of Ramsey, Minnesota**

Adopted: x/x/2025

**DRAFT**

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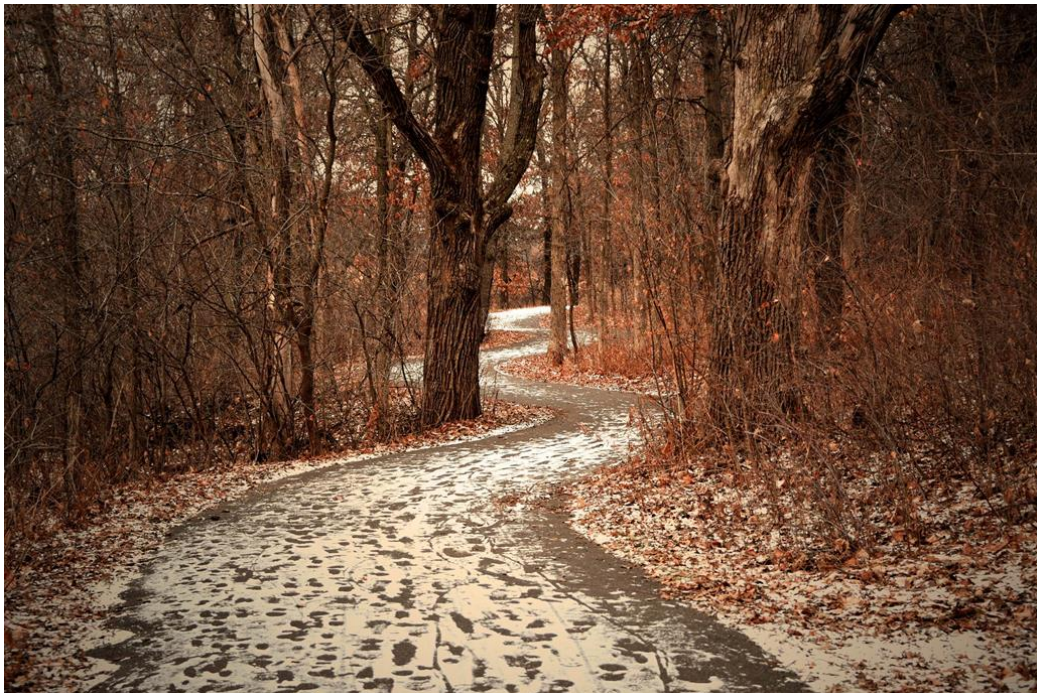
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## SECTION 1. INTRODUCTION.

Trails are a valued recreational and transportation feature of Ramsey, and provide myriad benefits for residents, serving a broad demographic of the community. Depending on the type of use, bituminous trails may serve equally well two decades after construction as when initially constructed. However, pavement imperfections may actually negatively impact the experience of users when biking or walking if the surface is rough, or if visual connection to the pavement must be maintained to prepare for bumps, cracks or dodge potholes. In extreme cases, pavement conditions like potholes or obstacles in the clear zone alongside trails may create unsafe conditions. Therefore, this Trail Maintenance Policy sets forth the means and methods to maintain safe trail conditions, produce quality of trail user experiences, and protect the community's investment in the infrastructure over time.

This policy is written and prepared primarily for internal city use, and is not intended to replace plans and specifications, be a technical resource, nor be a primary communication piece for the public.

## SECTION 2. GENERAL POLICY STATEMENT.

At the time of adoption of this Trail Maintenance Policy the city of Ramsey has over 80 miles of bituminous trails, all of which will require varying degrees of maintenance into the future – and, additional trails are added to the system in most years. As general matter, the goal of this policy is to develop and sustain maintenance practices that keep trail conditions in a good to average pavement rating status or better. Safety shall be the primary focus and the city should continue to budget a consistent amount each year, such that staff can build a trail maintenance program around this amount that can be planned for on an annual basis.



### SECTION 3. GENERAL MAINTENANCE GOALS AND INTENT.

For 2024, the trail maintenance allocation is \$120,000. This line item within the General Fund has increased \$5,000 to \$15,000+ each year as part of an informal yet active trail maintenance plan—yielding one to three miles of trail overlaid or the subject of a reconstruct in a few instances. While the annual increases have risen 5-10% in many years, the miles of trail receiving maintenance treatments have not increased due to ordinary annual inflation associated with contracted services. Now that the city is wholly within the second decade of an informal maintenance plan, this new Trail Maintenance Policy should lead the city to evaluating past practices and funding, to align more so to with on-the-ground bituminous maintenance that addresses 5% to 8% of the system each year, versus the historical average of about 1.5% or less—this, because it does not appear existing funding levels are keeping pace with the aging pavement conditions and demand for resurfacing trails.

## SECTION 4. BITUMINOUS TRAIL MAINTENANCE PRACTICES

There are several different categories of bituminous trail maintenance techniques, summarized as:

- ~ Crack filling
- ~ Seal-coating or Slurry-sealing
- ~ Chip sealing
- ~ **Patching**
- ~ **Overlay**
- ~ Reconstruction

Each of these pavement maintenance practices have been employed for Ramsey’s trails – with 1.5” overlay’s determined to be the most cost effective and frequent annual maintenance activity, with patching occurring in isolated locations as needed each year. The table below from ACI Asphalt & Concrete of Maple Grove, MN provides a typical life cycle for bituminous trail maintenance (aciasphalt.com).

‘Asphalt trail pavements, like most infrastructure, has a life cycle. They start out new, they age, and eventually they decline to the point where they need to be replaced. How long pavement lasts depends on a variety of factors, including how well they are constructed initially, the weather, shade and standing water, the amount of use and – most importantly – how well they are maintained. A well-constructed trail can last 20 years or more’.

A trail overlay at Ramsey’s Trail Maintenance Policy goal of generally around the 15-20 year interval may extend the useful life of the trail to 40 years or more before needing a complete reconstruct.

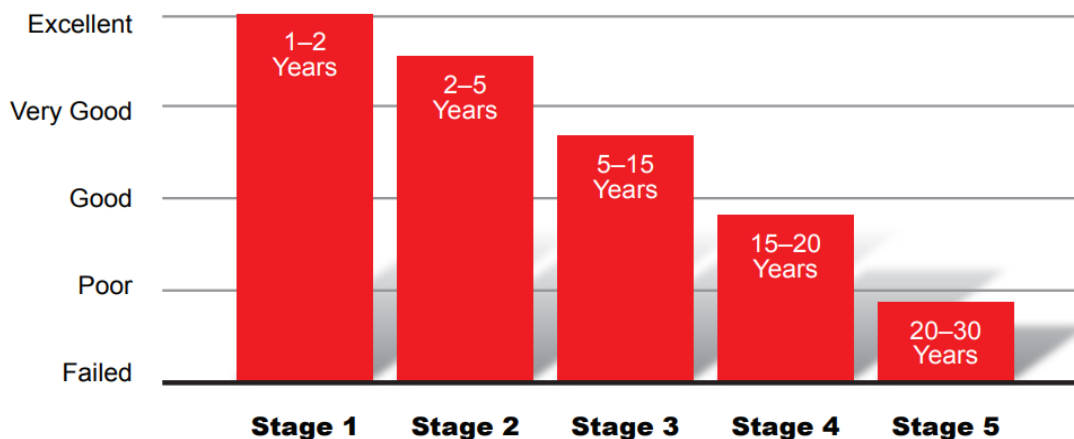
**Stage 1: New Pavement (0–1 Years)**  
 ♦ Little or no maintenance required, sealcoating, crack sealing

**Stage 2: Initial Preventive Maintenance Phase (2–5 Years)**  
 Typical maintenance procedures:  
 ♦ Sealcoating, crack sealing, patching

**Stage 3: Minor Repairs and Continued Preventive Maintenance (5–15 Years)**  
 Typical maintenance procedures:  
 ♦ Patch repairs, crack sealing, sealcoating

**Stage 4: Major Repairs (15–20 Years)**  
 Typical maintenance procedures:  
 ♦ Extensive patching repairs, asphalt overlay

**Stage 5: Extensive Repairs or Complete Reconstruction (20–30 Years)**  
 Typical maintenance procedures:  
 ♦ Major repairs throughout the property or complete removal and replacement of the asphalt



*A sample bituminous trail overlay Request for Proposal is included as 9 a. in the Appendix.*

The annual asphalt trail surface work described occurs by contractors, with routine patching performed by Public Works staff. As part of the course of travel around the park and trail system and providing work direction and inspections, supervisory staff are in and around trail corridors on a regular basis. This builds an awareness of the need and locations for patching, as well as more intensive annualized maintenance by contractors. This same familiarity with conditions guides other trail maintenance activity – for instance, after windstorms, staff are dispatched to areas known to have high probability for downed limbs or trees in wooded trail corridors. Other reoccurring work includes sweeping, mowing and brush management, as well as litter and refuse control, which are highlighted as follows:

### **Sweeping**

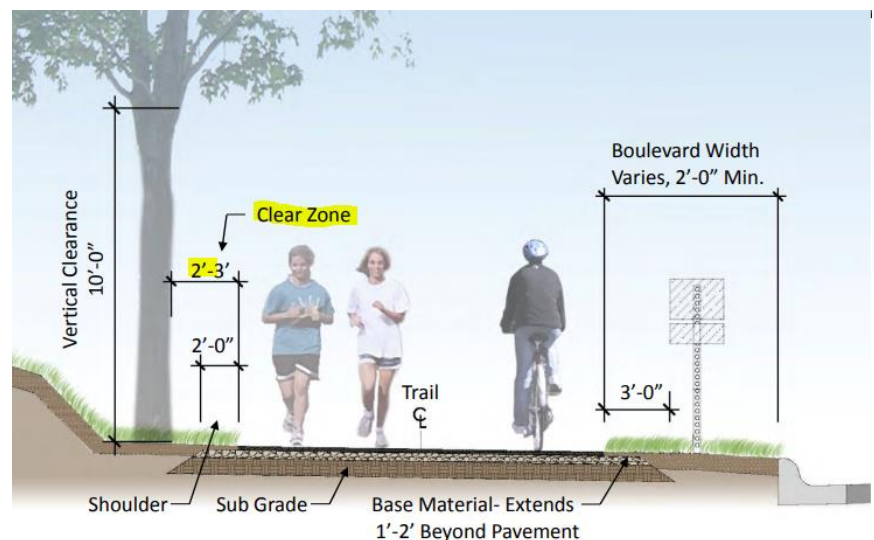
Trails are swept as needed, which is most often associated with aforementioned storm and wind events. Fall sweeping is performed if leaves and acorns accumulate on trail surfaces – however, in some years, and early snowfall can be beneficial in that trails can be plowed, and the wet snow ‘lifts’ leaves from the trail as part of the snow plowing process. In addition to a PTO driven rotary broom, the city can use a turbine debris blower, also operated by PTO as an attachment to a multi-use maintenance machine. The debris blower can also be employed for light, dry snowfalls which would be useful for situations where there are mostly dry leaves on the trail at the same time.

### **Mowing**

Trailside mowing is performed as needed, which is often every few weeks during the Spring and early Summer, for a total of 7 to 10 times per year. Generally, a three-foot or more swath is cut alongside the trail to minimize long grass and brush hanging over the trail. This practice also provides for improved airflow over a bituminous trail, which reduces periods of standing water, which can lead to premature pavement deterioration. Sweeping after mowing is not necessary, as mowers are dispatched to mow one side, then the other – then usually run down the center of the path with mower deck engaged to blow clippings off the trail surface.

### **Brushing and tree trimming**

‘Brushing’ refers to the practice of cutting weeds, woody plants and small trees that encroach into the 2’+ foot clear zone referenced above. Some work is done by hand, but most is performed with specialized brush mowers, mounted to light equipment. Due to the size and velocity of debris thrown from the machine, ‘Trail Closed’ and other signage is deployed to separate trail users from this maintenance activity.



Another related activity that occurs at least twice per year, is pruning and trimming trees along and over trailways. Branches are cleared to a minimum of 9' feet overhead and limbs projecting out into the trailside 'clear zone' are also removed. Trees that are demonstrably in jeopardy of falling onto the trail may be identified and removed at this same time. Dead and downed trees that are away from the trail are considered valuable wildlife habitat and are not removed.

### **Litter and refuse management**

Litter and general garbage and debris is picked up as needed and also as part of the reoccurring mowing. After the snowmelt each Winter, staff are sent out onto all trails for a concerted litter recovery – before the Spring 'green-up' begins to conceal trash in wooded trail corridors.

Some longer off-roadway trails have trash receptacles placed periodically along trails near intersections with maintenance accesses or trail heads – these are emptied as part of the weekly park system trash disposal system.

## SECTION 5. BOARDWALK AND OTHER TRAIL MAINTENANCE

### **Trails other than bituminous**

The preponderance of the city's trail system consists of 8' and 10' foot bituminous trails, with only short segments of crushed and compacted aggregate – with the latter not needing formal, planned-for maintenance.

### **Boardwalks**

Wood boardwalks however, require regular inspection for maintenance activity, usually consisting of a single plank needing replacement due to breaking or warping. Often these conditions are reported by trail users, with staff dispatched to address the deck board replacement right away – or to place a traffic cone at the broken board to alert trail users of the condition if the repair cannot be made immediately.

It may be stated here, that some of the inherent appeal of boardwalks is their location in natural environments, and their rustic 'feel' – which means they are not assured to be pristine in all dimensions – as long as they are safe to use.

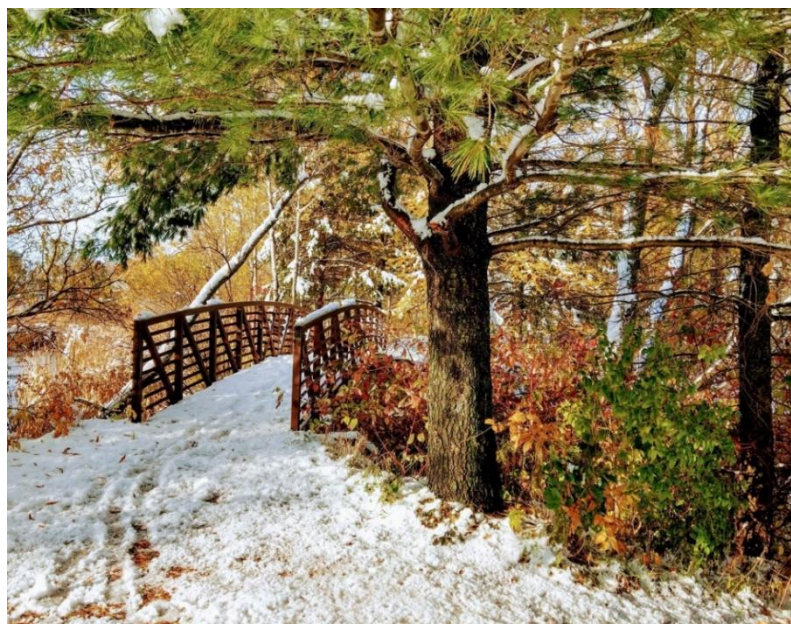


The city has found that the life cycle of boardwalks to be about 20-30 years before replacing individual boards becomes impracticable. At this time, the most feasible approach has been to 'over-deck' the boardwalk, with new decking fastened to the existing surface after re-leveling.

*A sample over-decking project is included as 9 b. in the Appendix.*

### **Trail Bridges**

The city presently has three, Cor-10 steel arched bridges withing the bituminous trail system that are load-rated and designed for maintenance vehicles. These bridges have not required any maintenance except for the occasional replacement of a steel band placed on the wooden deck to provide for a surface for the snow plow cutting edge to slide upon.





A fourth bridge spans Trott Brook as part of the 1,200' foot Lake Itasca Boardwalk. The construction consists of deck boards bolted onto galvanized steel trusses, and has not needed any maintenance since its construction by city staff in 2014.



## **Signage**

The city has maintained a policy of not signing trails, except for unique and temporary conditions that require them, like 'Trail Closed' or 'Work Ahead'. The city does not sign for conditions that a trail user should expect, and ride safely for if on a bicycle, like a curve, slope or driveway ahead.

## SECTION 6. SIDEWALKS

Concrete sidewalks function much the same as trails, though primarily are within the ROW of roadways in the areas served by municipal sewer and water. Of the 40 miles of sidewalk, most are 60" in width, though in the city's downtown are wider. Maintenance of sidewalks is infrequent and is generally limited to replacing cracked panels due to construction impacts, or replacement of 'lifted' sections due to tree root expansion. In instances where only one edge of a concrete panel is higher than the adjoining panel, the high side can be beveled with a concrete grinder.

## SECTION 7. WINTER TRAIL MAINTENANCE

Snow removal occurs on trails and sidewalks whenever a snowfall triggers a 'full plow' for the city. Generally, sidewalk and trail plowing commences two hours after street plowing begins, and starts in the city's downtown and moves outward with an emphasis on snow removal along arterial streets and trail and sidewalk routes to schools.

Unlike streets, no ice control is performed on any trails or sidewalks—although private retailers may perform ice control on sidewalks adjoining their business, at their discretion and expense.



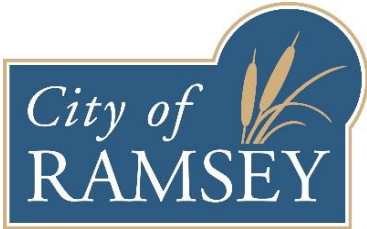
## SECTION 8. FUNDING

Trail maintenance is performed by Public Works staff and also by contracted work as described in Section 4., with the funding of both accounted for within the annual General Fund parks operations budget. As indicated in Section 3, the 2024 funding for contracted bituminous maintenance is \$120,000. This amount should be increased each year by at least \$10,000, until annual bituminous resurfacing reaches about 5% to 8% of the city's trail system.



SECTION 9. APPENDIX

**Appendix 9 a.**



7550 Sunwood Drive NW • Ramsey, MN 55303

City Hall: 763.427.1410 • Fax: 763.427.5543

[www.cityoframsey.com](http://www.cityoframsey.com)

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**Request for Proposals**

**Trail Overlay and Reclamation Project**

**ISSUE DATE:**

Friday, April 1<sup>st</sup>, 2022

**PROPOSAL SUBMITTAL DEADLINE:**

2:00 p.m. Friday, April 15<sup>th</sup>, 2022

**SUBMIT PROPOSALS VIA E-MAIL TO:**

Public Works Assistant Mary Jo Warner at  
[mwarner@cityoframsey.com](mailto:mwarner@cityoframsey.com)

## **Section 1**

### **Instruction to Bidders**

#### **Invitation to Bid**

The City of Ramsey Parks Department is seeking Request for Proposals (RFP) for reclaiming and paving two sections of trail within Ramsey's trail system and one section for over laying. Your response to this request will be evaluated to determine the qualifications of your company. Proposals will not be evaluated unless all parts requested are submitted in a complete package. The information requested is the minimum required in order to qualify for consideration.

#### **Scope of services**

The City rates the condition of the trails and selects the trails that are in the poorest condition to reclaim. In this RFP, there are two bid sheets for reclaiming and installing new asphalt within Elmcrest Park and one bid sheet for overlaying a trail just off the cul-de-sac on 148<sup>th</sup> Lane NW. One ADA compliant pedestrian ramp is required on bid sheet 1 and one ADA compliant pedestrian ramp is required on bid sheet 3.

The City reserves the right to reject any or all proposals or to accept any proposal that they deem is in the best interest to the City of Ramsey. The contractor shall furnish all labor and equipment required to complete this project. The contractor's name must be visible on all equipment when working in the city of Ramsey. Trail closed signage must be in place while this trail paving project is in the process of being completed. The contractor shall inform the City of Ramsey at least seven (7) business days in advance of when this project will begin.

#### **Bid requirements**

1. Successful bidder must be able to provide the maintenance services detailed in this RFP at the specified locations.
2. License requirements - Successful bidder will be required to obtain/hold a City of Ramsey's contractor's license (\$50), through the City Building Department
3. Contractor must use the asphalt mixture specified on the bid sheet.
4. Service calls - Must be completed within 48 hours after notification by city.
5. Invoicing - payment will be made upon submittal of an invoice to the city on a net 30-day basis following City acceptance of the work.
6. Site Visit - Contractor is required to perform a complete site visit of all listed properties prior to submitting bid sheet.

#### **Submission for Proposals**

In order for proposal to be considered, all information requested must be included on the attached bid forms. The City of Ramsey is tax exempt. ST3 forms are available upon request.

**Communication with the City:** If any requirements of this RFP are not clearly understood, the Contractor is responsible for contacting the City via e-mail and/or by phone for further clarification in a prompt manner. Responses to inquiries, if they change or clarify the RFP in a substantial manner, will be forwarded by addenda to the RFP. All communication is to go through the primary contact listed below with no exception.

### **Primary Contact Person**

Name: Mike Berge  
Address: 7550 Sunwood Drive, Ramsey  
MN 55303 Email:  
[mberge@cityoframsey.com](mailto:mberge@cityoframsey.com)  
Phone: 763-433-9814

### **Contractor Information**

The City of Ramsey may request additional information to vet your proposal. The City may also perform an interview with contractors under consideration to clarify any information which was provided.

### **Site visits**

Before submitting a proposal, contractors must thoroughly examine all listed locations and fully understand the conditions that in any way could affect their proposal. Failure to do so, will in no way relieve the contractor from satisfactory performance under the proposal submitted.

### **Quality Control**

The prime contractor, who is experienced with asphalt construction, shall perform work under this proposal. The project will be inspected by City of Ramsey staff. If for any reason, City staff deems areas unsatisfactory, per city standards, the contractor must return and repair any or all of these areas. There will be no charge to the City for these call backs.

### **Safety**

Standard safety practices must be followed, due to the fact that Ramsey's trail system is widely used by residents and visitors. **Appropriate trail closed safety**

**signage must be placed at each end of this trail while work is being done.**

This signage must remain in place for a 24-hour period after project is completed. It is then the contractor's responsibility to remove said signage or the contractor may be subject to a deduction from their proposed amount.

Material Safety Data Sheets (MSDS) or Safety Data Sheets (SDS) must be provided to the City of Ramsey prior to any work beginning.

### **Insurance Requirements**

**LICENSE AND INSURANCE:** By submitting a quotation, the Contractor affirms they shall maintain the following minimum insurance coverage while completing their work as related to this RFP in the following amounts: Commercial General Liability \$1,000,000 per occurrence (written on an occurrence-based form bodily injury and property damage); automobile liability \$1,000,000 per occurrence (including hired & non-owned bodily injury and property damage). The City of Ramsey shall be named as an additional insured on the Commercial General Liability insurance. Certificates of Insurance for all of the above insurance shall be filed with the City prior to the WORK. The Contractor also affirms they shall be licensed to perform the work in the State of Minnesota, and the work shall comply with all state and local laws.

### **Payments**

Payment will be made upon submittal of an invoice to [accountspayable@cityoframsey.com](mailto:accountspayable@cityoframsey.com) following the City's acceptance of the work. A job ID or number to be included on invoice.

### **Project Conditions**

In order for the contractor to perform and complete the work, the City of Ramsey will assist by providing the contractor with access to work areas. Locations of reclamations and over laying will be marked with white paint and the trail location will be marked with orange topped lathe.

### **Timeline**

These trail projects must be fully completed within 90 days after the proposal has been accepted by the city.

### **Cancellation**

The City of Ramsey reserves the right to reject any or all of the proposals; request new proposals which meet the general specifications set forth and waive any informality. The City also reserves the right to award the proposal that best

serves the interest of the City of Ramsey.

In the event of unsatisfactory performance of any contract awarded, the City of Ramsey reserves the right to terminate the contract upon giving 30 days written notice to the contractor.

## Bid Sheet 1

The location for this portion of the reclamation and paving project is Elmcrest Park, 16303 Quicksilver Street NW.

This trail project runs along the west side of the park.

- Expand 1,425 lineal feet of existing 9' wide trail to 10' wide
- Reclaim full depth (6.0"), contractor must confirm depth
- Reclaim material to be compacted in-place, graded to drain
- Pave new 10' trail, Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 2.5"
- Irrigation heads will be flagged and marked if applicable (contractor will repair any sprinkler heads/lines that were marked)
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- One (1) ADA compliant ped ramp is required in this bid sheet. It requires 4" minimum of compacted class 5. This ped ramp is at intersection where the trail meets the parkway.
- A row of truncated steel domes must span the width of the trail located no more than 12" from the edge of the street
- 6" thickness of concrete is required for each ramp
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 4" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Any excess material is property of the contractor
- Final payment will need 75% vegetation established

**Location & Address**

**Lump**

**sum**

Elmcrest Park 16303 Quicksilver Street NW  
with 1 ped ramp

1,425' existing 9' trail widen to 10'

**Total Cost Bid Sheet 1**

**\$ \_\_\_\_\_**

Picture of trail for bid sheet 1 trail section at Elmcrest Park



## Bid Sheet 2

The location for this portion of the reclamation and paving project is Elmcrest Park, 16303 Quicksilver Street NW.

This trail project runs along the south side of the park.

- Expand 1,600 lineal feet of existing 9' wide trail to 10' wide
- Reclaim full depth (6.0"), contractor must confirm depth
- Reclaim material to be compacted in-place, graded to drain
- Pave new 10' trail, Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 2.5"
- Irrigation heads will be flagged and marked if applicable (contractor will repair any sprinkler heads/lines that were marked)
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 4" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Any excess material is property of the contractor
- Final payment will need 75% vegetation established

| <u>Location &amp; Address</u>   | <u>Lump sum</u>           |
|---|---------------------------|
| <u>Elmcrest Park 16303 Quicksilver Street NW<br/>trail widen to 10'</u> | <u>1,600' existing 9'</u> |

**Total Cost Bid Sheet 2** \$ \_\_\_\_\_

Picture of trail for bid sheet 2 trail section at Elmcrest Park



### Bid Sheet 3

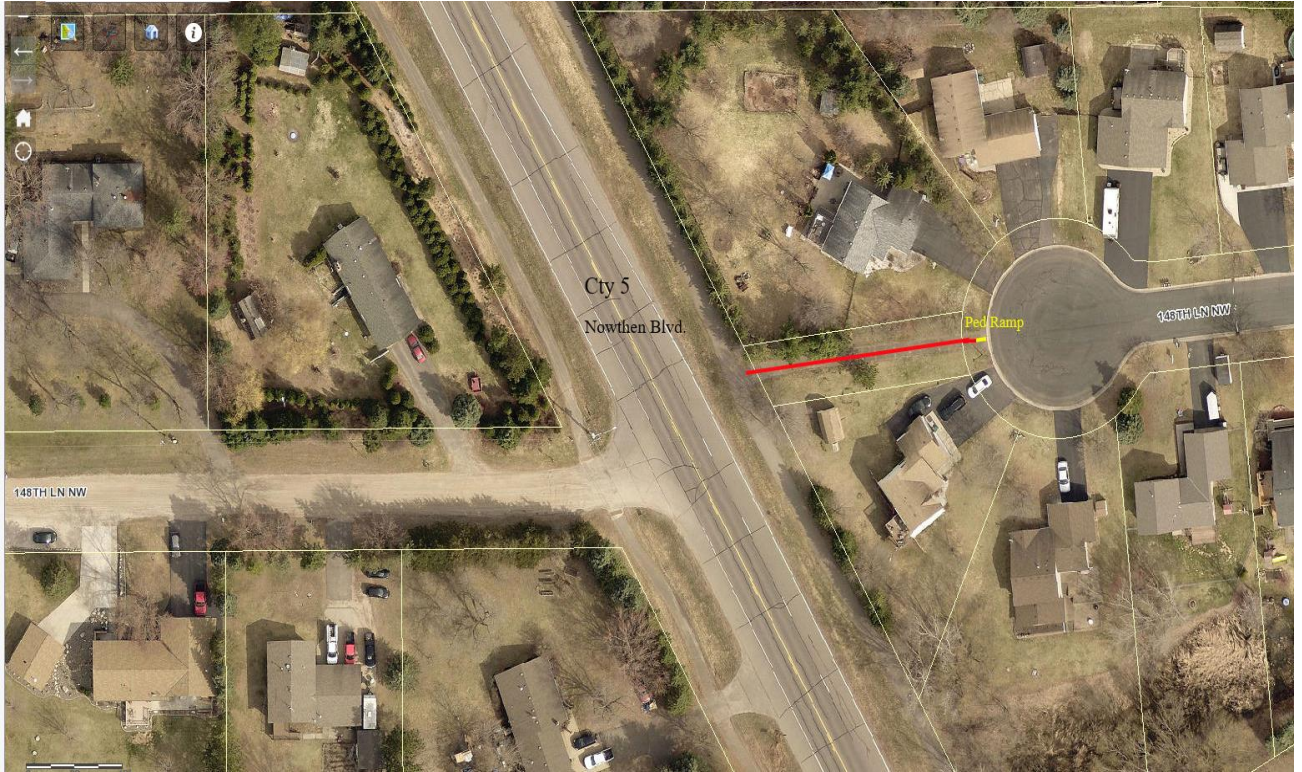
- Over lay a 155' section of trail that is 8' wide from the cul-de-sac to the existing trail
- Mill the end to meet up with existing trail
- Tree roots must be milled prior to overlaying if applicable
- Trail edge must be scraped cleaned and exposed
- Trail must be swept prior to overlaying
- Irrigation heads will be flagged and marked if applicable
- Tack oil is required
- Over lay 8' trail with Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 1.5"
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- One (1) ADA compliant ped ramp is required in this bid sheet. It requires 4" minimum of compacted class 5. This ped ramp is at intersection where the trail meets the cul-de-sac
- A row of truncated steel domes must span the width of the trail located no more than 12" from the edge of the street
- 6" thickness of concrete is required for each ramp
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 2" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Final payment will need 75% vegetation established

| <b><u>Location &amp; Address</u></b> | <b><u>Lump sum</u></b> |
|--------------------------------------|------------------------|
|--------------------------------------|------------------------|

|  |  |
|--|--|
| <u>Cul-de-sac on 148<sup>th</sup> Lane NW 155' of an 8' trail for overlaying with one ped ramp</u> |  |
|--|--|

|                               |                 |
|-------------------------------|-----------------|
| <b>Total Cost Bid Sheet 3</b> | <b>\$ _____</b> |
|-------------------------------|-----------------|

Picture of trail for bid sheet 3 cul-de-sac on 148<sup>th</sup> Lane NW



Company:Name \_\_\_\_\_

Address \_\_\_\_\_

Phone \_\_\_\_\_

Fax \_\_\_\_\_

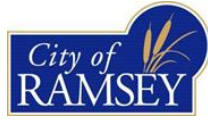
E-Mail \_\_\_\_\_

Signature & Title of Representative \_\_\_\_\_

\_\_\_\_\_

**Contractor will work with City in determining the best time for this project to be constructed within the timeline stated above.**

Appendix 9 b.



## REQUEST FOR QUOTATION



PROPOSALS MUST BE RECEIVED  
BY:  
**2:00 PM on Monday, November  
16<sup>th</sup>, 2021**

Submit by e-mail ONLY to:  
Mary Jo Warner  
Ramsey, MN 55303  
763-433-9820

E-mail to: [mwarner@cityoframsey.com](mailto:mwarner@cityoframsey.com)

### **Furnish Materials and Labor for the 'Over-decking' of the Lake Itasca Boardwalk**



North end of boardwalk  
accessible from Trailhead at; 8560  
167<sup>th</sup> Lane Ramsey, MN 55303

## PROPOSAL SPECIFICATIONS

### **Furnish Materials and Installation Labor for the 8' foot Over-decking of the existing, approximately 1,155' foot, 6' foot-wide Lake Itasca Boardwalk**

#### **Pre-Qualifications:**

- I. **\*Bidder/Contactor shall demonstrate that he/she has successfully installed 3 (three) projects of comparable or greater scope in a county, municipal, state park, or school setting in the Twin Cities Metropolitan area within the last 5 (five) years.**
- II. **Bidder/Contractor shall confirm his/her capacity to 'field adjust' the methods and detail of the construction to achieve a uniform deck surface pursuant to the specifications below.**
- III. **Contractor is responsible for visiting the site to determine all existing conditions that may affect the performance of the Work, AND may request a walk through with city staff if required.**

#### **Project Requirements:**

I/We agree to furnish all labor, supply materials as needed, and site security to perform the work as described below:

- Contractor and the City of Ramsey shall enter into the City's standard form of agreement.
- Contractor shall submit material examples to the City of Ramsey's representative for review and approval if requested.
- Contractor shall secure all permits related to the Work (if any).
- The Lake Itasca Boardwalk should be reasonably accessible to the public during construction, for a safe, continuous north/south crossing by pedestrians. The Contractor shall also ensure a continuous and safe route at the conclusion of each work day, generally free of loose items.
- Contractor shall provide evidence of insurance in accordance with City requirements, and be licensed to work in the City (a \$75 fee).
- Contractor shall provide temporary facilities, as required, for proper storage of materials and if necessary, also sanitation (outdoor portable toilet on-site).

- Contractor shall protect adjacent trees and shrubs and notify the City's representative immediately of any damage to trees or pavement associated with the site and work.
- Contractor shall remove ALL boardwalk materials that are not completely covered by the new decking (including railing) from the project in conjunction with the Work, and dispose of same properly at his/her expense.
- The Contractor shall substantially complete the project by April 30<sup>th</sup> 2022.
- The Contractor shall be paid for the work completed, materials on site, and not equipment & furnishings stored off-site. The City is not responsible for stolen, missing or damaged materials.
- Notify the City's representative prior to substantial completion to schedule a final review of the work and make corrections to deficiencies identified in the final review.
- This Request for Quotation shall be a not-to-exceed amount. The Contractor must include within his/her quote any time or materials and incidentals necessary to complete the work in a professional and quality manner. At the completion of the Work, the site's grounds shall be returned to its current condition by the Contractor.
- There shall be no Change Orders.
- Portions of the work site may be required to be enclosed with temporary safety fencing—if the contractor is not physically on site. Fencing is **not required** if there are no equipment or loose items/tools anywhere on site. However, through-passage for pedestrians shall be maintained at all times.

### Specification details:

- Contractor shall supply and install new 2" x 8" brown treated decking, 8' wide, centered and **over and upon**, the existing 6' foot boardwalk. This new decking shall be screwed down with #10 X 3 1/2" Climatek Coated GRK Screws into either the shim board or the existing decking. (Longer screws may be needed in some areas, and considered an incidental.)
- Shim as necessary, to place new decking boards level from left to right, using 3/4" thick treated boards, and up to 3 1/2" thick treated boards, to achieve a level plane within 1" tolerances from left to right within the 8' wide new deck surface. Shimming shall be performed such that there is **no** 'sagging' or board flexing from movement of people on the completed deck surface.

- Contractor to supply and install a 2" x 4" blocking board (spaced no more than 3' on center), along with a continuous 2" x 4" to create a 3" high 'curb' along the entire length of each side. The seams of these boards shall be generally at the middle point of existing deck sections – and the seams shall be matched by a 45° degree cut directly over the blocking board. (See example reference below.) There shall be three to four fasteners screwed into each end of the upper 2" x 4" where they join.
- Contractor to supply and install bracket hardware, mud foot plate and pipe, with a 5/8" set bolt mechanism for adjustment for all areas where existing deck sections must be removed near the (north) portion by the bridge, (scope of removal at the Contractor's discretion). (See Appendix for the referenced sections to be removed.)  
All components to be hot dipped galvanized, and the pipe to be 1 1/2" schedule 40, hot dipped galvanized. This will be used to achieve significant adjustment for removed sections as noted above, or other areas where the existing deck sections do not provide enough support, or shimming is not practicable. The City projects that this technique will be required at 10 or more sections of the existing boardwalk.
- The existing bridge is 8' foot wide and does not need replacement or over decking. The new decking shall meet this bridge section, with the Contractor to field engineer the joining of the new decking to the bridge decking (or full 8' foot approach area), such that there is no vertical differential where new deck boards meet the existing deck board(s).
- Contractor to supply and install two, 8' foot hinged hot dipped galvanized diamond plate (or flex resistant aluminum diamond plate) at beginning and end of boardwalk (plates to overlap onto the bituminous trail at the north and south).

### **Work and Staging Areas:**

Confine work activities to the boardwalk site, and adjacent to bituminous trails – all of which should remain passable by pedestrians during the construction periods. Contractor is responsible for protecting trail pavement from breaking and damage. No mechanized equipment shall be left overnight on the site (unless fenced), and no construction activity shall be performed before 7 AM nor after 7 PM Monday through Saturday. The contractor shall provide 72-hour advance notice before working on Saturday. City Council approval may be required for Sunday work.

Coordinate staging area and fencing with the City's representative prior to beginning

the Work as necessary.

**Liquidated Damages:**

Liquidated damages may be deducted from the approved quoted amount, in a sum of \$50 dollars per weekday (M-F) for non-completion after May 15<sup>th</sup>, 2022.

**Warranty/Guarantee:**

The Contractor warrants and guarantees to the Owner that all installation Work will be of good quality and free from faults or defects in accordance with this RFQ. All defective Work may be rejected.

If required by the Owner, prior to payment, the Contractor will promptly without cost to the Owner, correct defective Work, or replace it with non-defective Work. If the Contractor does not correct such defective Work or remove and replace within a reasonable time, the Owner may have the deficiency corrected or the rejected Work removed and replaced. All direct or indirect costs of such correction or removal and replacement, including compensation for additional professional services shall be paid by the Contractor, or deducted by the Owner, all such costs from the quoted contract price.

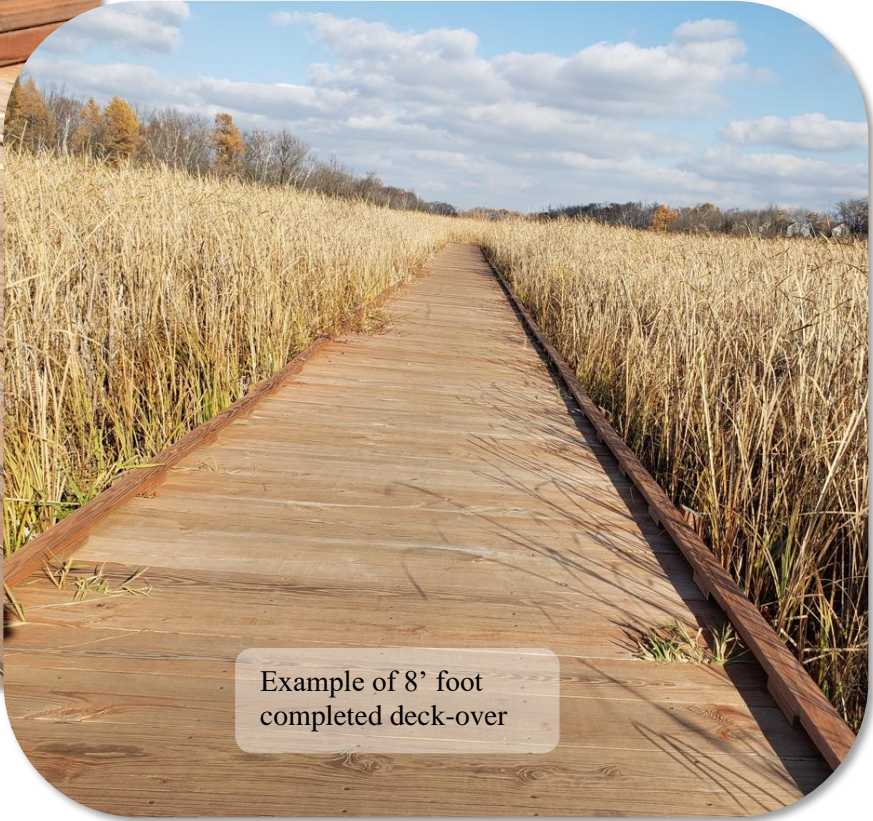
If, instead of requiring correction or removal and replacement of defective Work, the Owner (prior to approval of final payment) prefers to accept it, they may do so. In such case, if the acceptance occurs prior to final payment, a Statement shall be issued incorporating the necessary revisions, including appropriate reductions in Contract Price

**Specification References:**

The Contractor shall design the Work in conformance with the Specifications, and example images below, as well as in accordance with standard construction practices.



Depiction of 45° degree curb joint configuration



Example of 8' foot completed deck-over



Example bracket hardware, mud foot plate and pipe



**Quotation for Furnishing Materials and Installation Labor for the 8' foot  
Over-decking of the Existing 6' foot Lake Itasca Boardwalk**

\$ \_\_\_\_\_ for all costs to complete installation as referenced.

In submitting the above quote, I/We warrant the quote valid for work to be completed on or before April 30<sup>th</sup>, 2022.

Additionally, I/we acknowledge the Specifications, Provisions, Appendix, and existing conditions referenced herein and above. (Bidder need only submit this Quotation page #7.)

Contractor name \_\_\_\_\_

Owner or representative \_\_\_\_\_

Signature \_\_\_\_\_

Phone(s) \_\_\_\_\_

E-mail \_\_\_\_\_

\*List three related projects of comparable scope for a governmental agency in the Twin Cities Metropolitan area within recent years, with contact/reference information:

Project name and location \_\_\_\_\_  
Contact name and info \_\_\_\_\_

Project name and location \_\_\_\_\_  
Contact name and info \_\_\_\_\_

Project name and location \_\_\_\_\_  
Contact name and info \_\_\_\_\_

Appendix 9 c.

| TRAIL MAINTENANCE POLICY                                       |                   |         |           |          |             |       |             |       |  |
|--|-------------------|---------|-----------|----------|-------------|-------|-------------|-------|--|
| Maintenance Activity   | Optimal Frequency |         |           |          |             | Notes |             |       |  |
|  | Bi-Weekly         | Monthly | Quarterly | Annually | Spring/Fall |       | After Storm | Other |  |
| <b>General</b>   |                   |         |           |          |             |       |             |       |  |
| 1 Safety Inspection  | X                 |         |           |          |             |       |             |       |  |
| 2 General debris and litter pickup                             | X                 |         |           |          |             |       |             |       |  |
| 3 Vandalism inspection   | X                 |         |           |          |             |       |             |       |  |
| 4 Encroachments  |                   |         |           | X        |             |       |             |       |  |
| <b>Pavements</b>   |                   |         |           |          |             |       |             |       |  |
| 1 Pavement survey (Phaser rating)                              |                   |         |           |          |             |       |             | X     |  |
| 2 Crack sealing  |                   |         |           |          |             |       |             |       |  |
| 3 Patching   |                   |         |           |          |             |       |             |       |  |
| 4 Fog seal   |                   |         |           |          |             |       |             |       |  |
| 5 Sealcoat   |                   |         |           |          |             |       |             |       |  |
| 6 Micro surfacing/slurry seal                                  |                   |         |           |          |             |       |             |       |  |
| 7 Overlay  |                   |         |           |          |             |       |             |       |  |
| 8 Reconstruct  |                   |         |           |          |             |       |             |       |  |
| 9 Inspect boardwalk interface                                  |                   | X       |           |          |             |       |             |       |  |
| 10 Inspect utility infrastructure for snow removal (high iron) |                   |         |           |          | Fall        |       |             |       |  |
| <b>Vegetation</b>  |                   |         |           |          |             |       |             |       |  |
| 1 Mowing - clear zones, trailhead areas                        |                   | X       |           |          |             |       |             |       |  |
| 2 Brush trimming/overhead trimming                             |                   |         |           |          |             |       |             |       |  |
| 3 Clear zone weed control (Sandbar Willow)                     |                   |         |           |          |             |       |             |       |  |
| 4 Sight line trimming at intersections                         |                   |         |           |          |             |       |             |       |  |
| 5 Tree removal   |                   |         |           |          |             |       |             | X     |  |
| 6 Rain garden maintenance                                      |                   |         |           |          |             |       |             |       |  |
| 7 Trail sweeping/blowing                                       |                   |         |           |          | X           |       |             |       |  |
| 8 Seeding  |                   |         |           |          |             |       |             |       |  |
| 9 Root cutting   |                   |         |           |          |             |       |             |       |  |
| <b>Drainage</b>  |                   |         |           |          |             |       |             |       |  |
| 1 Erosion repair   |                   |         |           |          |             |       |             |       |  |
| 2 Culvert/catch basin clearing                                 |                   |         |           |          |             |       |             |       |  |
| 3 Ditch maintenance (clear debris, trash, branches)            |                   |         |           |          |             |       |             |       |  |
| 4 Standing water repair  |                   |         |           |          |             |       |             |       |  |
| 5 Rodent damage repair   |                   |         |           |          | X           |       |             |       |  |