

City of Ramsey
Agenda
Public Works Committee
Tuesday, September 17, 2024

5:30 pm

Lake Itasca Room, 7550 Sunwood Drive NW

Remote Attendance available at www.cityoframsey.com/meetings.

Those joining remotely and requesting to speak are asked to use a webcam when speaking.

1. Call to Order

2. Citizen Input

3. Approve Agenda

4. Approve Minutes

1. Approve the following meeting minutes.
 1. Public Works Committee meeting dated August 20, 2024.

5. Committee Business

1. Trail Maintenance Policy
2. Consider Recommendation to City Council to Adopt Stormwater Management Facilities Maintenance Policy
3. Consider Recommendation to City Council to Accept Bids and Award Contract for Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements

6. Committee/Staff Input

1. Update on the Pavement Management Program
2. Consider Recommending City Council Order Request for Proposals for Sunwood Drive Concrete Pedestrian Crossing Replacement Topographic Survey

3. Consider Recommendation to City Council to Submit Application for 2024 MnDOT Metro Standalone Noise Barrier Program
4. Receive Updates on Improvement Projects, Studies and Items of Interest
5. Review Future Topics Calendar
7. **Adjournment**

Public Works Committee

Meeting Date: 09/17/2024

Primary Strategic Plan Initiative: Not Applicable

Title:

Approve the following meeting minutes.
1. Public Works Committee meeting dated August 20, 2024.

Purpose/Background:

To review and approve meeting minutes.

The meeting minutes dated August 20, 2024, are attached for review.

Notification:

N/A

Time Frame/Observations/Alternatives:

Staff anticipates this case will take less than 5 minutes.

Funding Source:

N/A

Recommendation:

To review and approve meeting minutes dated August 20, 2024.

Outcome/Action:

Motion to approve meeting minutes dated August 20, 2024.

Attachments

August 2024 PWC Minutes

Form Review

Inbox	Reviewed By	Date
Brian Hagen	Brian Hagen	09/12/2024 12:21 PM
Form Started By: Marsha Weidner		Started On: 09/10/2024 10:24 AM
Final Approval Date: 09/12/2024		

**PUBLIC WORKS COMMITTEE
CITY OF RAMSEY
ANOKA COUNTY
STATE OF MINNESOTA**

The Public Works Committee conducted a regular meeting on Tuesday, August 20, 2024, at the Ramsey Municipal Center, 7550 Sunwood Drive NW, Ramsey, Minnesota.

Members Present: Chairperson Debra Musgrove
 Councilmember Chelsee Howell
 Councilmember Michael Olson

Also Present: City Engineer/Public Works Director Bruce Westby
 Parks and Assistant Public Works Director Mark Riverblood

1. CALL TO ORDER

Chairperson Musgrove called the regular meeting of the Public Works Committee to order at 6:00 p.m.

2. CITIZEN INPUT

There was none.

3. APPROVE AGENDA

Motion by Councilmember Howell, seconded by Councilmember Olson, to approve the agenda, as presented.

Motion carried. Voting Yes: Chairperson Musgrove, Councilmembers Howell and Olson. Voting No: None.

4. APPROVE MINUTES

4.01: Approve July 16, 2024, Meeting Minutes

Motion by Councilmember Howell, seconded by Councilmember Olson, to approve the following minutes:

Regular Meeting Minutes dated July 16, 2024

Motion carried. Voting Yes: Chairperson Musgrove, Councilmembers Howell and Olson. Voting No: None.

5. COMMITTEE BUSINESS

5.01: Trail Maintenance Policy – Status Update

Parks and Assistant Public Works Director Riverblood reviewed the staff report and stated that staff requests feedback on the content, scope and tone of the Trail Maintenance Policy language.

Councilmember Olson commented that he likes the direction/style of the policy thus far.

Chairperson Musgrove stated that she would have preferred to have the draft policy in the case to review before the meeting. She believed that it is a bit wordy for a policy and suggested that the introduction be reduced with some of that information placed within the policy. She stated that while this would be helpful information for residents, it does not seem similar to other City policies. She asked that the full policy be drafted for the September meeting, noting that there has already been delay in this process.

Parks and Assistant Public Works Director Riverblood stated that he spoke with administration staff to find out if there was a standard format for policies and was told that there was none. He stated that if that is the desire, perhaps that policy format should be developed.

Councilmember Howell commented that she felt that this would be helpful if it was instructional, which lets the public know what is going on but is not something necessarily just for the public. She also agreed that it would have been helpful to have the draft prior to the meeting. She agreed that the full policy should be prepared for the September meeting.

Parks and Assistant Public Works Director Riverblood agreed that if the intent is to be instructional, the policy can then be more concise.

Councilmember Howell asked if there was a table discussed at the last meeting showing the trail maintenance schedule.

Chairperson Musgrove commented that she did make the statement that she likes that element for tracking, but did not think that would need to be part of the policy itself.

Councilmember Olson stated that he is not exactly clear who the audience is intended to be. He stated that if this is meant to be posted on the website for public consumption, then this style seems to be good as it clearly communicates the information. He stated that if it is intended to be procedures, that would be directed at the staff. He stated that for a policy and narrative for the public, he thinks this is good.

Chairperson Musgrove commented that type of communication may be something that is communicated to residents in a newsletter. She stated that a policy should be more concise as it is an internal document.

Councilmember Howell agreed that a policy is intended for internal use and is meant to communicate the process, although the public may be interested in reading it and could still be informed.

5.02: Review Draft Stormwater Management Facilities Maintenance Policy

City Engineer/Public Works Director Westby reviewed the staff report stating that the Committee is asked to review the draft Stormwater Management Facilities Maintenance Policy and provide feedback to staff on proposed additions, deletions, and/or revisions, including whether a maintenance schedule should be added to the appendix.

Chairperson Musgrove commented that the Mississippi River Critical Corridor Area (MRCCA) is not mentioned and asked if that should also be included.

City Engineer/Public Works Director Westby replied that the city has other plans and policies and therefore he attempted to avoid duplication within policies. He referenced some of the other plans that specifically deal with the MRCCA noting that the MRCCA does not impact the maintenance of the stormwater ponds.

Chairperson Musgrove stated that perhaps there could be reference to the additional regulations within the MRCCA.

City Engineer/Public Works Director Westby commented that if work is being done within the MRCCA, appropriate permits would need to be pulled from the DNR.

Councilmember Howell commented that this policy is straightforward and therefore easy to read.

Chairperson Musgrove commented that she liked the listed agencies, bodies of waters, etc. She asked the estimate of time for staff to review the policy.

City Engineer/Public Works Director Westby replied that he gave staff an additional 1.5 weeks to review the policy and provide comments so he could have the full draft policy prepared for the September PWC meeting.

5.03: Approve Quote to Pave Alpine Drive Approaches to BNSF Railway Crossing

City Engineer/Public Works Director Westby reviewed the staff report and recommendation to approve the quote from North Valley, Inc. in the amount of \$10,473.62 to pave the Alpine Drive approaches to the BNSF Railway Crossings.

Councilmember Olson asked and received confirmation that this would be a one-day job, drivable after completed.

Chairperson Musgrove thanked staff for continuing to pursue this issue and make it more drivable.

Motion by Councilmember Howell, seconded by Councilmember Olson, to approve the quote from North Valley, Inc. in the amount of \$10,473.62 to pave the Alpine Drive approaches to the BNSF Railway Crossings.

Motion carried. Voting Yes: Chairperson Musgrove, Councilmembers Howell and Olson. Voting No: None.

6. COMMITTEE / STAFF INPUT

6.01: Receive Updates on Improvement Projects, Studies, and Items of Interest

City Engineer/Public Works Director Westby provided an update on current and proposed City, County, and MnDOT improvement projects and studies, and on other items of interest to the Committee.

6.02: Review Future Topics Calendar

City Engineer/Public Works Director Westby provided an overview of the calendar topics, noting updated dates that he would insert. It was confirmed that Wetland 114P could be removed from the list.

Chairperson Musgrove suggested that the Committee perform an annual review of Capital Improvement Program projects as related to public works items.

City Engineer/Public Works Director Westby replied that staff was planning to present updated Pavement Management Program projects and cash flows proposed for the 2025 – 2034 CIP to the September meeting.

Chairperson Musgrove asked for an update on the dog park fence in The COR, along with trees.

Parks and Assistant Public Works Director Riverblood replied that the proposed off leash area would be along Veterans Drive as an extension of the Residence of the COR dog area. He stated that staff is attempting to coordinate with the Residence of the COR on that element. He stated that he has developed plans but has not yet obtained quotes. He asked for clarification on the trees.

Chairperson Musgrove recalled that staff proposed to obtain quotes for carving two dead trees at Sunfish Lake Park and believed that issue was going to go back before the Council.

Parks and Assistant Public Works Director Riverblood stated that came before the Council one or two years ago and the consensus of the Council was that there was no interest in paying for carving the trees, along with safety concerns from dead limbs falling. He stated that the trees have since been removed from the park.

Chairperson Musgrove asked that the dog park be added to the future topics calendar to receive updates. She stated that in the most recent meeting of the Lower Rum River Water Management Organization, there was discussion of projects ongoing in each of the cities, noting that Ramsey continues to be the busiest city in terms of projects. She thanked staff for their continued hard work ensuring that these projects run smoothly, along with all the local road projects.

City Engineer/Public Works Director Westby asked if there was an anticipated timeline for the dog park.

Parks and Assistant Public Works Director Riverblood commented that he is not certain. He believed the Park and Recreation Commission would be the appropriate body to continue to manage that item. He recognized that item was planned for 2024, but with all other activities this year, that item was not the top priority.

7. ADJOURNMENT

Motion by Councilmember Howell, seconded by Councilmember Olson, to adjourn the Public Works Committee meeting.

Motion carried.

The regular meeting of the Public Works Committee adjourned at 7:00 p.m.

Respectfully submitted,

Bruce Westby
City Engineer/Public Works Director

Drafted by Amanda Staple
TimeSaver Off Site Secretarial, Inc.

Public Works Committee

Meeting Date: 09/17/2024

Primary Strategic Plan Initiative: Address infrastructure needs.

Title:

Trail Maintenance Policy

Purpose/Background:

The purpose of this case is for the Public Works Committee to provide direction on the content and substance of the final draft Trail Maintenance Policy. At the August regular meeting, the Committee reviewed an earlier draft of the policy and by consensus, the Committee indicated the Policy should be crafted as an internal document, meaning that the intended audience would be for future staff and policymakers to consult, understand and implement the Trail Maintenance Policy. The Committee also acknowledged that at present there is not a template or standard format for the city's policies, and that at such point as one is adopted, that the Trail Maintenance Policy would be revised accordingly.

Time Frame/Observations/Alternatives:

Staff estimates approximately 10 minutes may be needed to review elements of the policy and respond to Committee questions and input.

Funding Source:

No funding is required at this time.

Recommendation:

Staff requests direction on the next steps pertaining to the Trail Maintenance Policy, e.g. shall the policy move forward for formal City Council adoption.

Outcome/Action:

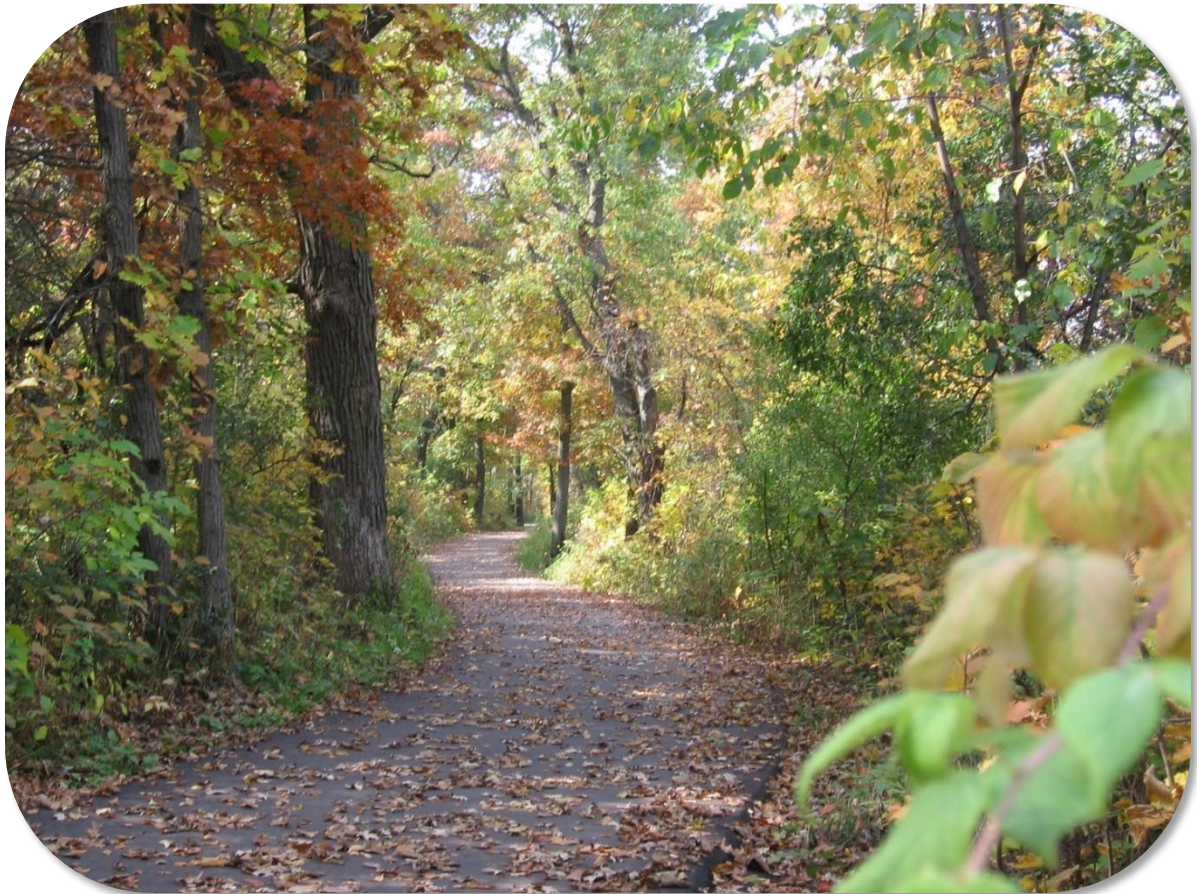
By consensus, provide staff direction on the draft Trail Maintenance Policy.

Attachments

Draft Trail Maint Policy
Park and Trail map

Form Review

Inbox	Reviewed By	Date
Bruce Westby	Mark Riverblood	09/12/2024 01:05 PM
Mark Riverblood (Originator)	Mark Riverblood	09/12/2024 01:07 PM
Bruce Westby	Bruce Westby	09/12/2024 02:07 PM
Brian Hagen	Brian Hagen	09/12/2024 02:17 PM
Form Started By: Mark Riverblood		Started On: 09/12/2024 08:25 AM
Final Approval Date: 09/12/2024		



TRAIL MAINTENANCE POLICY

City of Ramsey, Minnesota

Adopted: x/x/2025

DRAFT

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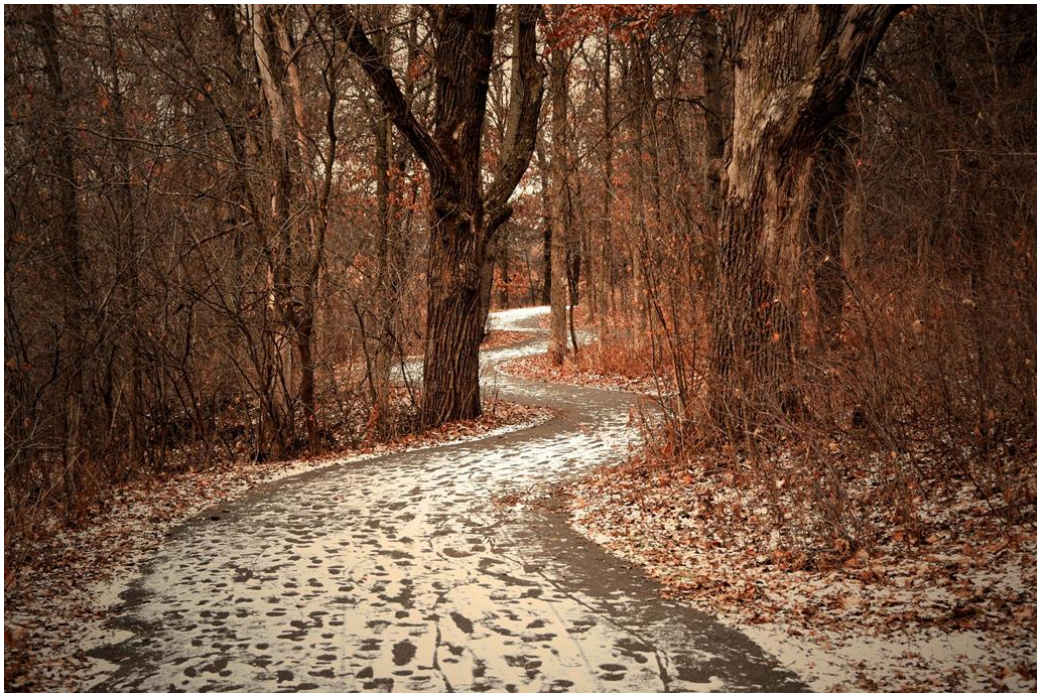
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SECTION 1. INTRODUCTION.

Trails are a valued recreational and transportation feature of Ramsey, and provide myriad benefits for residents, serving a broad demographic of the community. Depending on the type of use, bituminous trails may serve equally well two decades after construction as when initially constructed. However, pavement imperfections may actually negatively impact the experience of users when biking or walking if the surface is rough, or if visual connection to the pavement must be maintained to prepare for bumps, cracks or dodge potholes. In extreme cases, pavement conditions like potholes or obstacles in the clear zone alongside trails may create unsafe conditions. Therefore, this Trail Maintenance Policy sets forth the means and methods to maintain safe trail conditions, produce quality of trail user experiences, and protect the community's investment in the infrastructure over time.

This policy is written and prepared primarily for internal city use, and is not intended to replace plans and specifications, be a technical resource, nor be a primary communication piece for the public.

SECTION 2. GENERAL POLICY STATEMENT.

At the time of adoption of this Trail Maintenance Policy the city of Ramsey has over 80 miles of bituminous trails, all of which will require varying degrees of maintenance into the future – and, additional trails are added to the system in most years. As general matter, the goal of this policy is to develop and sustain maintenance practices that keep trail conditions in a good to average pavement rating status or better. Safety shall be the primary focus and the city should continue to budget a consistent amount each year, such that staff can build a trail maintenance program around this amount that can be planned for on an annual basis.



SECTION 3. GENERAL MAINTENANCE GOALS AND INTENT.

For 2024, the trail maintenance allocation is \$120,000. This line item within the General Fund has increased \$5,000 to \$15,000+ each year as part of an informal yet active trail maintenance plan—yielding one to three miles of trail overlaid or the subject of a reconstruct in a few instances. While the annual increases have risen 5-10% in many years, the miles of trail receiving maintenance treatments have not increased due to ordinary annual inflation associated with contracted services. Now that the city is wholly within the second decade of an informal maintenance plan, this new Trail Maintenance Policy should lead the city to evaluating past practices and funding, to align more so to with on-the-ground bituminous maintenance that addresses 5% to 8% of the system each year, versus the historical average of about 1.5% or less—this, because it does not appear existing funding levels are keeping pace with the aging pavement conditions and demand for resurfacing trails.

SECTION 4. BITUMINOUS TRAIL MAINTENANCE PRACTICES

There are several different categories of bituminous trail maintenance techniques, summarized as:

- ~ Crack filling
- ~ Seal-coating or Slurry-sealing
- ~ Chip sealing
- ~ **Patching**
- ~ **Overlay**
- ~ Reconstruction

Each of these pavement maintenance practices have been employed for Ramsey’s trails – with 1.5” overlay’s determined to be the most cost effective and frequent annual maintenance activity, with patching occurring in isolated locations as needed each year. The table below from ACI Asphalt & Concrete of Maple Grove, MN provides a typical life cycle for bituminous trail maintenance (aciasphalt.com).

‘Asphalt trail pavements, like most infrastructure, has a life cycle. They start out new, they age, and eventually they decline to the point where they need to be replaced. How long pavement lasts depends on a variety of factors, including how well they are constructed initially, the weather, shade and standing water, the amount of use and – most importantly – how well they are maintained. A well-constructed trail can last 20 years or more’.

A trail overlay at Ramsey’s Trail Maintenance Policy goal of generally around the 15-20 year interval may extend the useful life of the trail to 40 years or more before needing a complete reconstruct.

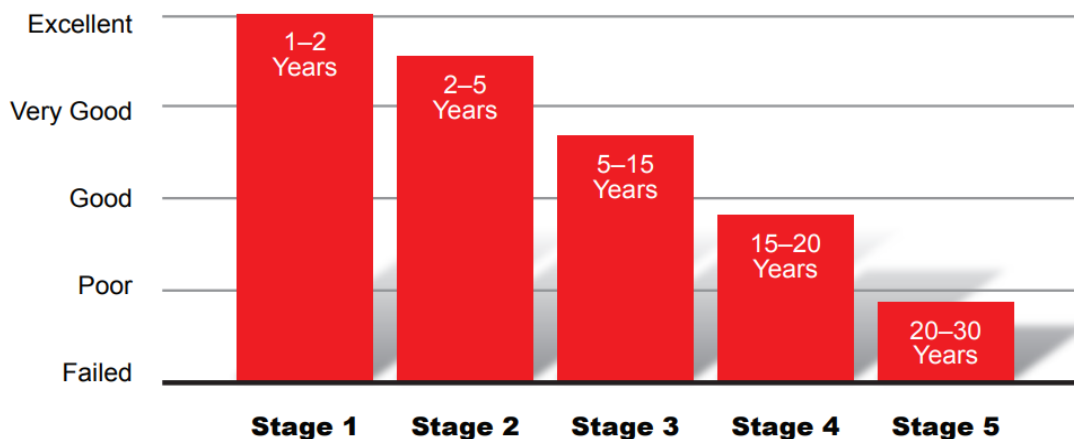
Stage 1: New Pavement (0–1 Years)
 ♦ Little or no maintenance required, sealcoating, crack sealing

Stage 2: Initial Preventive Maintenance Phase (2–5 Years)
 Typical maintenance procedures:
 ♦ Sealcoating, crack sealing, patching

Stage 3: Minor Repairs and Continued Preventive Maintenance (5–15 Years)
 Typical maintenance procedures:
 ♦ Patch repairs, crack sealing, sealcoating

Stage 4: Major Repairs (15–20 Years)
 Typical maintenance procedures:
 ♦ Extensive patching repairs, asphalt overlay

Stage 5: Extensive Repairs or Complete Reconstruction (20–30 Years)
 Typical maintenance procedures:
 ♦ Major repairs throughout the property or complete removal and replacement of the asphalt



A sample bituminous trail overlay Request for Proposal is included as 9 a. in the Appendix.

The annual asphalt trail surface work described occurs by contractors, with routine patching performed by Public Works staff. As part of the course of travel around the park and trail system and providing work direction and inspections, supervisory staff are in and around trail corridors on a regular basis. This builds an awareness of the need and locations for patching, as well as more intensive annualized maintenance by contractors. This same familiarity with conditions guides other trail maintenance activity – for instance, after windstorms, staff are dispatched to areas known to have high probability for downed limbs or trees in wooded trail corridors. Other reoccurring work includes sweeping, mowing and brush management, as well as litter and refuse control, which are highlighted as follows:

Sweeping

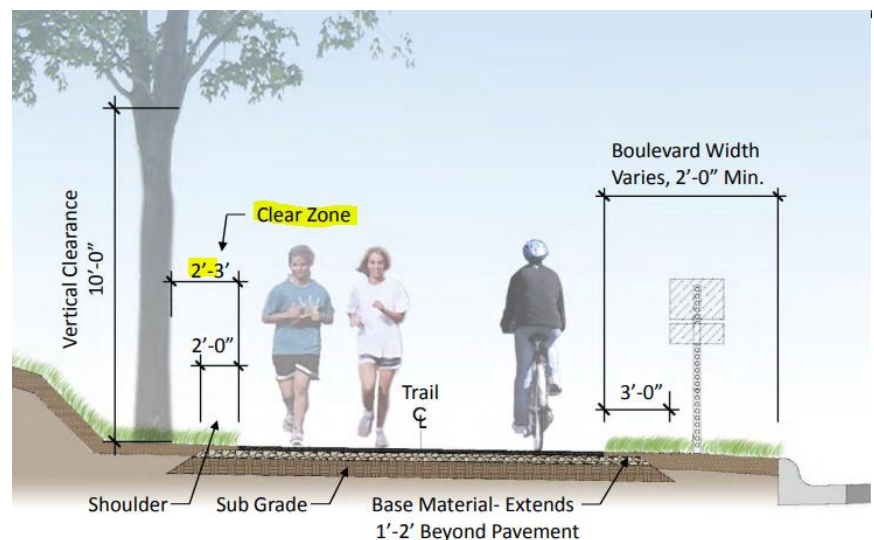
Trails are swept as needed, which is most often associated with aforementioned storm and wind events. Fall sweeping is performed if leaves and acorns accumulate on trail surfaces – however, in some years, and early snowfall can be beneficial in that trails can be plowed, and the wet snow ‘lifts’ leaves from the trail as part of the snow plowing process. In addition to a PTO driven rotary broom, the city can use a turbine debris blower, also operated by PTO as an attachment to a multi-use maintenance machine. The debris blower can also be employed for light, dry snowfalls which would be useful for situations where there are mostly dry leaves on the trail at the same time.

Mowing

Trailside mowing is performed as needed, which is often every few weeks during the Spring and early Summer, for a total of 7 to 10 times per year. Generally, a three-foot or more swath is cut alongside the trail to minimize long grass and brush hanging over the trail. This practice also provides for improved airflow over a bituminous trail, which reduces periods of standing water, which can lead to premature pavement deterioration. Sweeping after mowing is not necessary, as mowers are dispatched to mow one side, then the other – then usually run down the center of the path with mower deck engaged to blow clippings off the trail surface.

Brushing and tree trimming

‘Brushing’ refers to the practice of cutting weeds, woody plants and small trees that encroach into the 2’+ foot clear zone referenced above. Some work is done by hand, but most is performed with specialized brush mowers, mounted to light equipment. Due to the size and velocity of debris thrown from the machine, ‘Trail Closed’ and other signage is deployed to separate trail users from this maintenance activity.



Another related activity that occurs at least twice per year, is pruning and trimming trees along and over trailways. Branches are cleared to a minimum of 9' feet overhead and limbs projecting out into the trailside 'clear zone' are also removed. Trees that are demonstrably in jeopardy of falling onto the trail may be identified and removed at this same time. Dead and downed trees that are away from the trail are considered valuable wildlife habitat and are not removed.

Litter and refuse management

Litter and general garbage and debris is picked up as needed and also as part of the reoccurring mowing. After the snowmelt each Winter, staff are sent out onto all trails for a concerted litter recovery – before the Spring 'green-up' begins to conceal trash in wooded trail corridors.

Some longer off-roadway trails have trash receptacles placed periodically along trails near intersections with maintenance accesses or trail heads – these are emptied as part of the weekly park system trash disposal system.

SECTION 5. BOARDWALK AND OTHER TRAIL MAINTENANCE

Trails other than bituminous

The preponderance of the city's trail system consists of 8' and 10' foot bituminous trails, with only short segments of crushed and compacted aggregate – with the latter not needing formal, planned-for maintenance.

Boardwalks

Wood boardwalks however, require regular inspection for maintenance activity, usually consisting of a single plank needing replacement due to breaking or warping. Often these conditions are reported by trail users, with staff dispatched to address the deck board replacement right away – or to place a traffic cone at the broken board to alert trail users of the condition if the repair cannot be made immediately.

It may be stated here, that some of the inherent appeal of boardwalks is their location in natural environments, and their rustic 'feel' – which means they are not assured to be pristine in all dimensions – as long as they are safe to use.

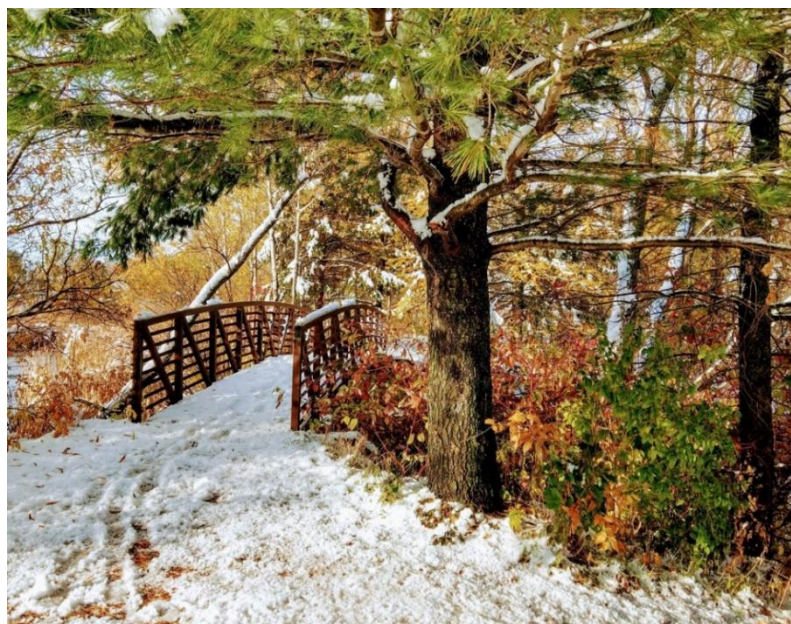


The city has found that the life cycle of boardwalks to be about 20-30 years before replacing individual boards becomes impracticable. At this time, the most feasible approach has been to 'over-deck' the boardwalk, with new decking fastened to the existing surface after re-leveling.

A sample over-decking project is included as 9 b. in the Appendix.

Trail Bridges

The city presently has three, Cor-10 steel arched bridges withing the bituminous trail system that are load-rated and designed for maintenance vehicles. These bridges have not required any maintenance except for the occasional replacement of a steel band placed on the wooden deck to provide for a surface for the snow plow cutting edge to slide upon.





A fourth bridge spans Trott Brook as part of the 1,200' foot Lake Itasca Boardwalk. The construction consists of deck boards bolted onto galvanized steel trusses, and has not needed any maintenance since its construction by city staff in 2014.



Signage

The city has maintained a policy of not signing trails, except for unique and temporary conditions that require them, like 'Trail Closed' or 'Work Ahead'. The city does not sign for conditions that a trail user should expect, and ride safely for if on a bicycle, like a curve, slope or driveway ahead.

SECTION 6. SIDEWALKS

Concrete sidewalks function much the same as trails, though primarily are within the ROW of roadways in the areas served by municipal sewer and water. Of the 40 miles of sidewalk, most are 60" in width, though in the city's downtown are wider. Maintenance of sidewalks is infrequent and is generally limited to replacing cracked panels due to construction impacts, or replacement of 'lifted' sections due to tree root expansion. In instances where only one edge of a concrete panel is higher than the adjoining panel, the high side can be beveled with a concrete grinder.

SECTION 7. WINTER TRAIL MAINTENANCE

Snow removal occurs on trails and sidewalks whenever a snowfall triggers a 'full plow' for the city. Generally, sidewalk and trail plowing commences two hours after street plowing begins, and starts in the city's downtown and moves outward with an emphasis on snow removal along arterial streets and trail and sidewalk routes to schools.

Unlike streets, no ice control is performed on any trails or sidewalks—although private retailers may perform ice control on sidewalks adjoining their business, at their discretion and expense.



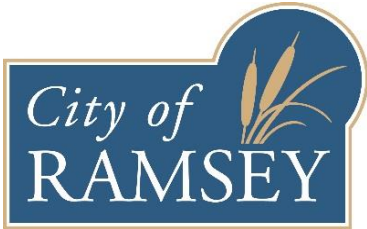
SECTION 8. FUNDING

Trail maintenance is performed by Public Works staff and also by contracted work as described in Section 4., with the funding of both accounted for within the annual General Fund parks operations budget. As indicated in Section 3, the 2024 funding for contracted bituminous maintenance is \$120,000. This amount should be increased each year by at least \$10,000, until annual bituminous resurfacing reaches about 5% to 8% of the city's trail system.



SECTION 9. APPENDIX

Appendix 9 a.



7550 Sunwood Drive NW • Ramsey, MN 55303

City Hall: 763.427.1410 • Fax: 763.427.5543

www.cityoframsey.com

Request for Proposals

Trail Overlay and Reclamation Project

ISSUE DATE:

Friday, April 1st, 2022

PROPOSAL SUBMITTAL DEADLINE:

2:00 p.m. Friday, April 15th, 2022

SUBMIT PROPOSALS VIA E-MAIL TO:

Public Works Assistant Mary Jo Warner at
mwarner@cityoframsey.com

Section 1

Instruction to Bidders

Invitation to Bid

The City of Ramsey Parks Department is seeking Request for Proposals (RFP) for reclaiming and paving two sections of trail within Ramsey's trail system and one section for over laying. Your response to this request will be evaluated to determine the qualifications of your company. Proposals will not be evaluated unless all parts requested are submitted in a complete package. The information requested is the minimum required in order to qualify for consideration.

Scope of services

The City rates the condition of the trails and selects the trails that are in the poorest condition to reclaim. In this RFP, there are two bid sheets for reclaiming and installing new asphalt within Elmcrest Park and one bid sheet for overlaying a trail just off the cul-de-sac on 148th Lane NW. One ADA compliant pedestrian ramp is required on bid sheet 1 and one ADA compliant pedestrian ramp is required on bid sheet 3.

The City reserves the right to reject any or all proposals or to accept any proposal that they deem is in the best interest to the City of Ramsey. The contractor shall furnish all labor and equipment required to complete this project. The contractor's name must be visible on all equipment when working in the city of Ramsey. Trail closed signage must be in place while this trail paving project is in the process of being completed. The contractor shall inform the City of Ramsey at least seven (7) business days in advance of when this project will begin.

Bid requirements

1. Successful bidder must be able to provide the maintenance services detailed in this RFP at the specified locations.
2. License requirements - Successful bidder will be required to obtain/hold a City of Ramsey's contractor's license (\$50), through the City Building Department
3. Contractor must use the asphalt mixture specified on the bid sheet.
4. Service calls - Must be completed within 48 hours after notification by city.
5. Invoicing - payment will be made upon submittal of an invoice to the city on a net 30-day basis following City acceptance of the work.
6. Site Visit - Contractor is required to perform a complete site visit of all listed properties prior to submitting bid sheet.

Submission for Proposals

In order for proposal to be considered, all information requested must be included on the attached bid forms. The City of Ramsey is tax exempt. ST3 forms are available upon request.

Communication with the City: If any requirements of this RFP are not clearly understood, the Contractor is responsible for contacting the City via e-mail and/or by phone for further clarification in a prompt manner. Responses to inquiries, if they change or clarify the RFP in a substantial manner, will be forwarded by addenda to the RFP. All communication is to go through the primary contact listed below with no exception.

Primary Contact Person

Name: Mike Berge
Address: 7550 Sunwood Drive, Ramsey
MN 55303 Email:
mberge@cityoframsey.com
Phone: 763-433-9814

Contractor Information

The City of Ramsey may request additional information to vet your proposal. The City may also perform an interview with contractors under consideration to clarify any information which was provided.

Site visits

Before submitting a proposal, contractors must thoroughly examine all listed locations and fully understand the conditions that in any way could affect their proposal. Failure to do so, will in no way relieve the contractor from satisfactory performance under the proposal submitted.

Quality Control

The prime contractor, who is experienced with asphalt construction, shall perform work under this proposal. The project will be inspected by City of Ramsey staff. If for any reason, City staff deems areas unsatisfactory, per city standards, the contractor must return and repair any or all of these areas. There will be no charge to the City for these call backs.

Safety

Standard safety practices must be followed, due to the fact that Ramsey's trail system is widely used by residents and visitors. **Appropriate trail closed safety**

signage must be placed at each end of this trail while work is being done.

This signage must remain in place for a 24-hour period after project is completed. It is then the contractor's responsibility to remove said signage or the contractor may be subject to a deduction from their proposed amount.

Material Safety Data Sheets (MSDS) or Safety Data Sheets (SDS) must be provided to the City of Ramsey prior to any work beginning.

Insurance Requirements

LICENSE AND INSURANCE: By submitting a quotation, the Contractor affirms they shall maintain the following minimum insurance coverage while completing their work as related to this RFP in the following amounts: Commercial General Liability \$1,000,000 per occurrence (written on an occurrence-based form bodily injury and property damage); automobile liability \$1,000,000 per occurrence (including hired & non-owned bodily injury and property damage). The City of Ramsey shall be named as an additional insured on the Commercial General Liability insurance. Certificates of Insurance for all of the above insurance shall be filed with the City prior to the WORK. The Contractor also affirms they shall be licensed to perform the work in the State of Minnesota, and the work shall comply with all state and local laws.

Payments

Payment will be made upon submittal of an invoice to accountspayable@cityoframsey.com following the City's acceptance of the work. A job ID or number to be included on invoice.

Project Conditions

In order for the contractor to perform and complete the work, the City of Ramsey will assist by providing the contractor with access to work areas. Locations of reclamations and over laying will be marked with white paint and the trail location will be marked with orange topped lathe.

Timeline

These trail projects must be fully completed within 90 days after the proposal has been accepted by the city.

Cancellation

The City of Ramsey reserves the right to reject any or all of the proposals; request new proposals which meet the general specifications set forth and waive any informality. The City also reserves the right to award the proposal that best

serves the interest of the City of Ramsey.

In the event of unsatisfactory performance of any contract awarded, the City of Ramsey reserves the right to terminate the contract upon giving 30 days written notice to the contractor.

Bid Sheet 1

The location for this portion of the reclamation and paving project is Elmcrest Park, 16303 Quicksilver Street NW.

This trail project runs along the west side of the park.

- Expand 1,425 lineal feet of existing 9' wide trail to 10' wide
- Reclaim full depth (6.0"), contractor must confirm depth
- Reclaim material to be compacted in-place, graded to drain
- Pave new 10' trail, Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 2.5"
- Irrigation heads will be flagged and marked if applicable (contractor will repair any sprinkler heads/lines that were marked)
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- One (1) ADA compliant ped ramp is required in this bid sheet. It requires 4" minimum of compacted class 5. This ped ramp is at intersection where the trail meets the parkway.
- A row of truncated steel domes must span the width of the trail located no more than 12" from the edge of the street
- 6" thickness of concrete is required for each ramp
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 4" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Any excess material is property of the contractor
- Final payment will need 75% vegetation established

Location & Address

Lump

sum

Elmcrest Park 16303 Quicksilver Street NW
with 1 ped ramp

1,425' existing 9' trail widen to 10'

Total Cost Bid Sheet 1

\$ _____

Picture of trail for bid sheet 1 trail section at Elmcrest Park



Bid Sheet 2

The location for this portion of the reclamation and paving project is Elmcrest Park, 16303 Quicksilver Street NW.

This trail project runs along the south side of the park.

- Expand 1,600 lineal feet of existing 9' wide trail to 10' wide
- Reclaim full depth (6.0"), contractor must confirm depth
- Reclaim material to be compacted in-place, graded to drain
- Pave new 10' trail, Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 2.5"
- Irrigation heads will be flagged and marked if applicable (contractor will repair any sprinkler heads/lines that were marked)
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 4" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Any excess material is property of the contractor
- Final payment will need 75% vegetation established

<u>Location & Address</u>	<u>Lump sum</u>
<u>Elmcrest Park 16303 Quicksilver Street NW trail widen to 10'</u>	<u>1,600' existing 9'</u>

Total Cost Bid Sheet 2 \$ _____

Picture of trail for bid sheet 2 trail section at Elmcrest Park



Bid Sheet 3

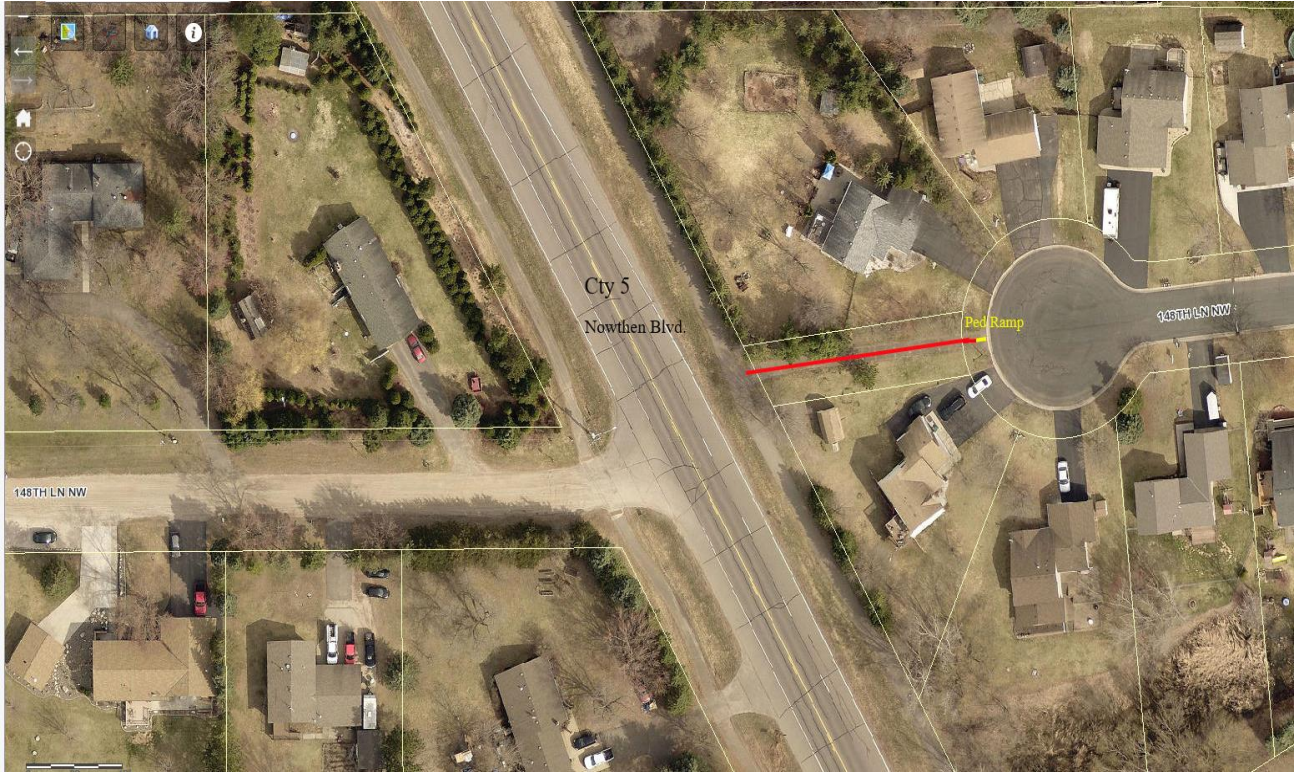
- Over lay a 155' section of trail that is 8' wide from the cul-de-sac to the existing trail
- Mill the end to meet up with existing trail
- Tree roots must be milled prior to overlaying if applicable
- Trail edge must be scraped cleaned and exposed
- Trail must be swept prior to overlaying
- Irrigation heads will be flagged and marked if applicable
- Tack oil is required
- Over lay 8' trail with Type SP9.5 Wearing Course Mixture (SPWEA340C) compacted to a depth of 1.5"
- Contractor to provide 2 business days advanced notice before beginning project
- Safety signs and cones must be in place prior to work beginning
- One (1) ADA compliant ped ramp is required in this bid sheet. It requires 4" minimum of compacted class 5. This ped ramp is at intersection where the trail meets the cul-de-sac
- A row of truncated steel domes must span the width of the trail located no more than 12" from the edge of the street
- 6" thickness of concrete is required for each ramp
- Shouldering restoration along the sides of the trail to be done by contractor with a minimum 2" topsoil in impacted areas
- Hydro mulch/seed shoulders with MnDOT25-151 seed mix
- Final payment will need 75% vegetation established

Location & Address	Lump sum
-------------------------------	-----------------

<u>Cul-de-sac on 148th Lane NW 155' of an 8' trail for overlaying with one ped ramp</u>	
--	--

Total Cost Bid Sheet 3	\$ _____
-------------------------------	-----------------

Picture of trail for bid sheet 3 cul-de-sac on 148th Lane NW



Company:Name _____

Address _____

Phone _____

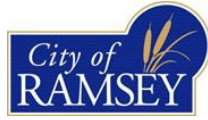
Fax _____

E-Mail _____

Signature & Title of Representative _____

Contractor will work with City in determining the best time for this project to be constructed within the timeline stated above.

Appendix 9 b.



REQUEST FOR QUOTATION



PROPOSALS MUST BE RECEIVED
BY:
2:00 PM on Monday, November
16th, 2021

Submit by e-mail ONLY to:
Mary Jo Warner
Ramsey, MN 55303
763-433-9820

E-mail to: mwarner@cityoframsey.com

Furnish Materials and Labor for the 'Over-decking' of the Lake Itasca Boardwalk



North end of boardwalk
accessible from Trailhead at; 8560
167th Lane Ramsey, MN 55303

PROPOSAL SPECIFICATIONS

Furnish Materials and Installation Labor for the 8' foot Over-decking of the existing, approximately 1,155' foot, 6' foot-wide Lake Itasca Boardwalk

Pre-Qualifications:

- I. ***Bidder/Contractor shall demonstrate that he/she has successfully installed 3 (three) projects of comparable or greater scope in a county, municipal, state park, or school setting in the Twin Cities Metropolitan area within the last 5 (five) years.**
- II. **Bidder/Contractor shall confirm his/her capacity to 'field adjust' the methods and detail of the construction to achieve a uniform deck surface pursuant to the specifications below.**
- III. **Contractor is responsible for visiting the site to determine all existing conditions that may affect the performance of the Work, AND may request a walk through with city staff if required.**

Project Requirements:

I/We agree to furnish all labor, supply materials as needed, and site security to perform the work as described below:

- Contractor and the City of Ramsey shall enter into the City's standard form of agreement.
- Contractor shall submit material examples to the City of Ramsey's representative for review and approval if requested.
- Contractor shall secure all permits related to the Work (if any).
- The Lake Itasca Boardwalk should be reasonably accessible to the public during construction, for a safe, continuous north/south crossing by pedestrians. The Contractor shall also ensure a continuous and safe route at the conclusion of each work day, generally free of loose items.
- Contractor shall provide evidence of insurance in accordance with City requirements, and be licensed to work in the City (a \$75 fee).
- Contractor shall provide temporary facilities, as required, for proper storage of materials and if necessary, also sanitation (outdoor portable toilet on-site).

- Contractor shall protect adjacent trees and shrubs and notify the City's representative immediately of any damage to trees or pavement associated with the site and work.
- Contractor shall remove ALL boardwalk materials that are not completely covered by the new decking (including railing) from the project in conjunction with the Work, and dispose of same properly at his/her expense.
- The Contractor shall substantially complete the project by April 30th 2022.
- The Contractor shall be paid for the work completed, materials on site, and not equipment & furnishings stored off-site. The City is not responsible for stolen, missing or damaged materials.
- Notify the City's representative prior to substantial completion to schedule a final review of the work and make corrections to deficiencies identified in the final review.
- This Request for Quotation shall be a not-to-exceed amount. The Contractor must include within his/her quote any time or materials and incidentals necessary to complete the work in a professional and quality manner. At the completion of the Work, the site's grounds shall be returned to its current condition by the Contractor.
- There shall be no Change Orders.
- Portions of the work site may be required to be enclosed with temporary safety fencing—if the contractor is not physically on site. Fencing is **not required** if there are no equipment or loose items/tools anywhere on site. However, through-passage for pedestrians shall be maintained at all times.

Specification details:

- Contractor shall supply and install new 2" x 8" brown treated decking, 8' wide, centered and **over and upon**, the existing 6' foot boardwalk. This new decking shall be screwed down with #10 X 3 1/2" Climatek Coated GRK Screws into either the shim board or the existing decking. (Longer screws may be needed in some areas, and considered an incidental.)
- Shim as necessary, to place new decking boards level from left to right, using 3/4" thick treated boards, and up to 3 1/2" thick treated boards, to achieve a level plane within 1" tolerances from left to right within the 8' wide new deck surface. Shimming shall be performed such that there is **no** 'sagging' or board flexing from movement of people on the completed deck surface.

- Contractor to supply and install a 2" x 4" blocking board (spaced no more than 3' on center), along with a continuous 2" x 4" to create a 3" high 'curb' along the entire length of each side. The seams of these boards shall be generally at the middle point of existing deck sections – and the seams shall be matched by a 45° degree cut directly over the blocking board. (See example reference below.) There shall be three to four fasteners screwed into each end of the upper 2" x 4" where they join.
- Contractor to supply and install bracket hardware, mud foot plate and pipe, with a 5/8" set bolt mechanism for adjustment for all areas where existing deck sections must be removed near the (north) portion by the bridge, (scope of removal at the Contractor's discretion). (See Appendix for the referenced sections to be removed.)
All components to be hot dipped galvanized, and the pipe to be 1 1/2" schedule 40, hot dipped galvanized. This will be used to achieve significant adjustment for removed sections as noted above, or other areas where the existing deck sections do not provide enough support, or shimming is not practicable. The City projects that this technique will be required at 10 or more sections of the existing boardwalk.
- The existing bridge is 8' foot wide and does not need replacement or over decking. The new decking shall meet this bridge section, with the Contractor to field engineer the joining of the new decking to the bridge decking (or full 8' foot approach area), such that there is no vertical differential where new deck boards meet the existing deck board(s).
- Contractor to supply and install two, 8' foot hinged hot dipped galvanized diamond plate (or flex resistant aluminum diamond plate) at beginning and end of boardwalk (plates to overlap onto the bituminous trail at the north and south).

Work and Staging Areas:

Confine work activities to the boardwalk site, and adjacent to bituminous trails – all of which should remain passable by pedestrians during the construction periods. Contractor is responsible for protecting trail pavement from breaking and damage. No mechanized equipment shall be left overnight on the site (unless fenced), and no construction activity shall be performed before 7 AM nor after 7 PM Monday through Saturday. The contractor shall provide 72-hour advance notice before working on Saturday. City Council approval may be required for Sunday work.

Coordinate staging area and fencing with the City's representative prior to beginning

the Work as necessary.

Liquidated Damages:

Liquidated damages may be deducted from the approved quoted amount, in a sum of \$50 dollars per weekday (M-F) for non-completion after May 15th, 2022.

Warranty/Guarantee:

The Contractor warrants and guarantees to the Owner that all installation Work will be of good quality and free from faults or defects in accordance with this RFQ. All defective Work may be rejected.

If required by the Owner, prior to payment, the Contractor will promptly without cost to the Owner, correct defective Work, or replace it with non-defective Work. If the Contractor does not correct such defective Work or remove and replace within a reasonable time, the Owner may have the deficiency corrected or the rejected Work removed and replaced. All direct or indirect costs of such correction or removal and replacement, including compensation for additional professional services shall be paid by the Contractor, or deducted by the Owner, all such costs from the quoted contract price.

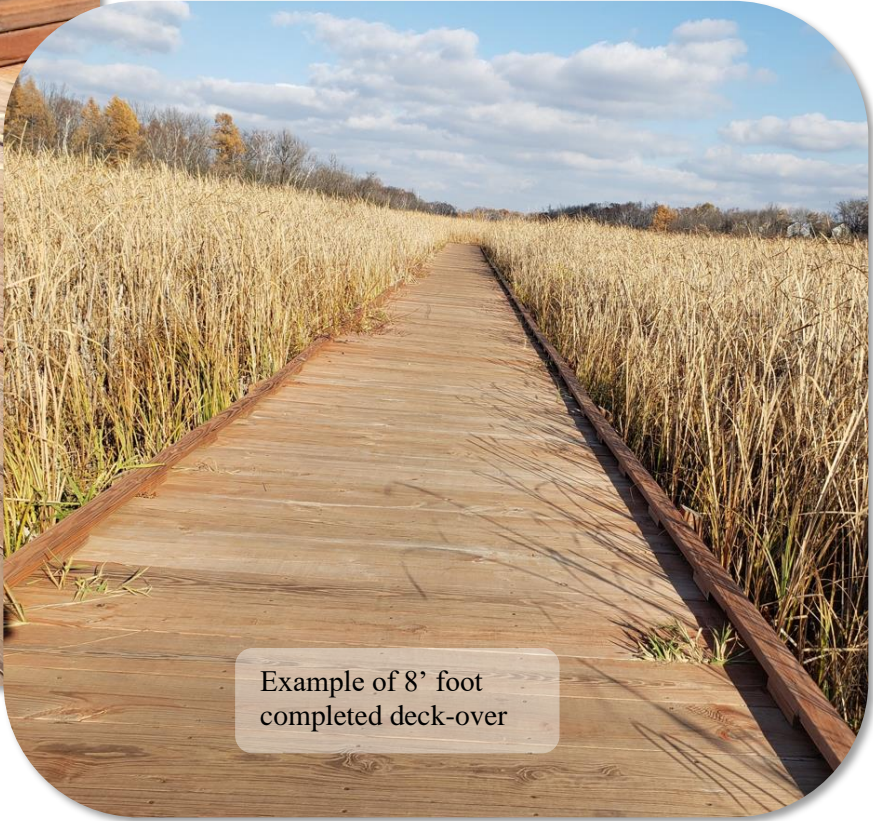
If, instead of requiring correction or removal and replacement of defective Work, the Owner (prior to approval of final payment) prefers to accept it, they may do so. In such case, if the acceptance occurs prior to final payment, a Statement shall be issued incorporating the necessary revisions, including appropriate reductions in Contract Price

Specification References:

The Contractor shall design the Work in conformance with the Specifications, and example images below, as well as in accordance with standard construction practices.



Depiction of 45° degree curb joint configuration



Example of 8' foot completed deck-over



Example bracket hardware, mud foot plate and pipe



**Quotation for Furnishing Materials and Installation Labor for the 8' foot
Over-decking of the Existing 6' foot Lake Itasca Boardwalk**

\$ _____ for all costs to complete installation as referenced.

In submitting the above quote, I/We warrant the quote valid for work to be completed on or before April 30th, 2022.

Additionally, I/we acknowledge the Specifications, Provisions, Appendix, and existing conditions referenced herein and above. (Bidder need only submit this Quotation page #7.)

Contractor name _____

Owner or representative _____

Signature _____

Phone(s) _____

E-mail _____

*List three related projects of comparable scope for a governmental agency in the Twin Cities Metropolitan area within recent years, with contact/reference information:

Project name and location _____
Contact name and info _____

Project name and location _____
Contact name and info _____

Project name and location _____
Contact name and info _____

Appendix 9 c.

TRAIL MAINTENANCE POLICY									
Maintenance Activity	Optimal Frequency					Notes			
	Bi-Weekly	Monthly	Quarterly	Annually	Spring/Fall		After Storm	Other	
General									
1 Safety Inspection	X								
2 General debris and litter pickup	X								
3 Vandalism inspection	X								
4 Encroachments				X					
Pavements									
1 Pavement survey (Phaser rating)								X	
2 Crack sealing									
3 Patching									
4 Fog seal									
5 Sealcoat									
6 Micro surfacing/slurry seal									
7 Overlay									
8 Reconstruct									
9 Inspect boardwalk interface		X							
10 Inspect utility infrastructure for snow removal (high iron)					Fall				
Vegetation									
1 Mowing - clear zones, trailhead areas		X							
2 Brush trimming/overhead trimming									
3 Clear zone weed control (Sandbar Willow)									
4 Sight line trimming at intersections									
5 Tree removal								X	
6 Rain garden maintenance									
7 Trail sweeping/blowing					X				
8 Seeding									
9 Root cutting									
Drainage									
1 Erosion repair									
2 Culvert/catch basin clearing									
3 Ditch maintenance (clear debris, trash, branches)									
4 Standing water repair									
5 Rodent damage repair					X				

City of Ramsey Park and Trail User Map



"This map's primary purpose is to assist residents in navigating their trail system and in exploring and discovering park and recreation areas. More planning documents and maps like the playground map, together with other facilities are available at the city's website at www.cityoframsey.com. Recreation program offerings and park reservation opportunities may also be found there, together with a host of other city services."

Park Map Symbols

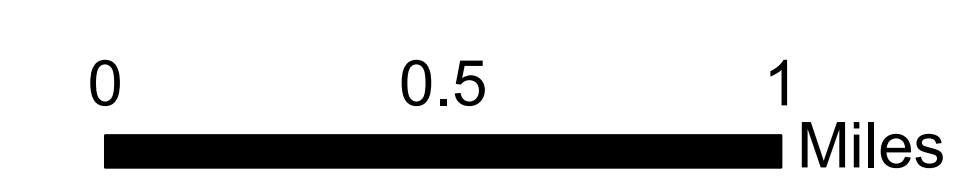
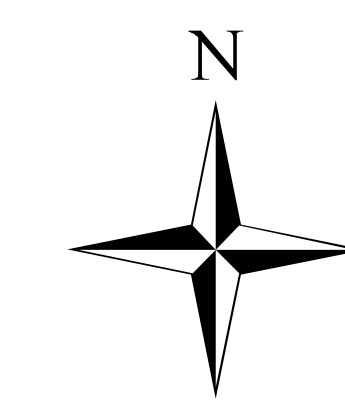
- Underpass
- Proposed Underpass
- Crosswalk
- Existing Trailhead
- Fire Station
- City Hall
- Proposed Trailhead
- Winter Recreation
- Right of Way (ROW)

Trail Type

- OFF-ROAD (OUT OF ROW)
- BOARDWALKS
- ON-ROAD (WITHIN ROW)
- Under Construction (2017)

Park and Open Space

- PARKS
- QUASI-PUBLIC
- REGIONAL
- PROPOSED PARK
- OPEN SPACE/ TRAIL CORRIDOR



City of Ramsey
7550 Sunwood Drive NW Ramsey MN 55303
www.cityoframsey.com
(763) 427 - 1410

Prepared by City of Ramsey
Revised February 2021

Anoka County Library
Anoka High School

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Address infrastructure needs.**Title:**

Consider Recommendation to City Council to Adopt Stormwater Management Facilities Maintenance Policy

Purpose/Background:**Purpose:**

To consider a recommendation to City Council to adopt Stormwater Management Facilities Maintenance Policy.

Background:

The purpose of this case is to review the final draft of the Stormwater Management Facilities Maintenance Policy and provide feedback to staff on proposed additions, deletions and/or revisions. The attached final draft was reviewed by all Staff that will be impacted by this Policy and additional content was added to the draft reviewed by the Committee on August 20, 2024.

Staff would like feedback on whether a maintenance schedule should be added to the appendix of this Policy, similar to the Trail Maintenance Policy. Staff will present a draft maintenance schedule during the meeting for discussion purposes, which could then be finalized and attached to the policy before presenting the policy to the City Council for approval.

If the Committee recommends City Council approval to adopt this policy, Staff will request a review by the City Attorney prior to presenting the policy to Council.

Time Frame/Observations/Alternatives:**Timeframe:**

Staff estimates 20 minutes will be needed to present this case and respond to questions.

Alternatives:

Alternative #1 – Motion recommending City Council adoption of Stormwater Management Facilities Maintenance Policy.

Alternative #2 – Motion of other.

Funding Source:

For transparency, costs for work required to maintain the City's stormwater management facilities will be included within the City of Ramsey's Capital Improvement Program to the maximum extent practicable.

All costs for the administration and operation of this Policy shall be funded through the City's Stormwater Funds. The main source of revenues for this fund is the Stormwater Utility Fee. In order to fully fund this program, annual increases in the stormwater utility fee rate will be necessary for the foreseeable future. The City Council will make the decision on whether or not to fund the program, and at what level, each year as part of its budget process.

Decisions on what projects to undertake will also be made as part of the budget process and the concurrent annual review of the City's Capital Improvement Program (CIP). The budget will include funds for specific projects and routine maintenance, as well as contingencies for emergencies.

A CIP sheet is being added to the 2025 - 2034 CIP with identified annual costs of \$40,000 for surveying existing stormwater ponds.

Recommendation:

Staff recommends providing a recommendation to the City Council to adopt the Stormwater Management Facilities Maintenance Policy.

Outcome/Action:

Consider recommendation to City Council to adopt Stormwater Management Facilities Maintenance Policy.

Attachments

SW Mngmnt Facilities Maint Policy 091324

Form Review

Inbox

Brian Hagen

Form Started By: Bruce Westby

Final Approval Date: 09/12/2024

Reviewed By

Brian Hagen

Date

09/12/2024 02:48 PM

Started On: 09/05/2024 04:58 PM



7550 Sunwood Drive NW • Ramsey, MN 55303

City Hall: 763.427.1410 • Fax: 763.427.5543

www.cityoframsey.com

Stormwater Management Facilities Maintenance Policy

Adopted **XXXXX XX**, 2024



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 - ii. Stormwater Pollution Prevention Program (SWPPP)
 - iii. Surface Water Management Plan (SWMP)
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 - h. Structural Pollution Control Devices
5. Goals
6. Priorities
7. Inspections
8. Financial Considerations
9. Recommendations
- Appendix
 - Definitions
 - Inventory Map
 - Inspection Schedule

1) Purpose of Policy

The purpose of this policy is to provide direction and guidance for maintaining the water quality and flood prevention functions of all City of Ramsey (City) stormwater management facilities including drainageways, storm sewer pipes and culverts, drainage structures, ponds (man-made) and wetlands (natural).

This policy supports the City's Municipal Separate Storm Sewer System (MS4) Permit, Surface Water Management Plan (SWMP), and Stormwater Pollution Prevention Plan (SWPPP).

All activities undertaken as part of this policy will be performed in compliance with applicable local, state and federal codes, rules, statutes, and other legal requirements.

2) City Responsibilities

The City is responsible for maintaining all City-owned stormwater management facilities including ditches and other surface drainageways, pipes, ponds, structures and wetlands located on City-owned properties or encumbered within public easements. Maintenance responsibilities for stormwater management facilities located on private property and not encumbered by public easements belong to the private property owner.

The level of maintenance required for the various stormwater management facilities may vary significantly based on design features and functions.

Work on naturally-occurring ponds, wetlands, brooks, rivers, streams, and other surface water conveyances, will be limited to maintaining drainage structures, pipes and culverts, and to removing obstructions to flow such as excess silt or vegetation in connecting conveyances, to maintain the flow of water and prevent flooding. While some naturally-occurring ponds and wetlands may also provide water quality benefits, maintenance projects will not be undertaken to enhance water quality functions but rather to maintain the quantity and rate of surface water flow to prevent property damage caused by flooding.

Maintenance work will not be completed solely for aesthetic purposes or to control odors, such as removal of trash, debris, or vegetation, or restoration of open water areas. Such work will only be completed if required to maintain the drainage functions of the pond.

Work on man-made stormwater retention ponds will typically be done to the same standards as the naturally-occurring ponds and wetlands. Maintenance work will focus on maintaining the flood prevention and water quality functions of the pond as necessary to restore the pond to its original design standards, in addition to maintaining connected drainage structures, pipes, culverts and drainageways to prevent flooding.

Some ponds are subject to maintenance agreements between the City and the landowner. Before the City performs any maintenance work on these ponds, the agreement must be dissolved upon agreement by all parties.

In order maintain stormwater management facilities that are part of the City's drainage system, the City must have the legal right to enter upon the property where the stormwater management facilities are located to perform the necessary work. If easements are not in place, the property owner must grant the necessary easements before the City completes any work on the property. The City will not normally purchase easements, and will only condemn easements if the owner refuses to grant them and there is a significant flooding problem threatening structures on public or private property that must be corrected.

Nothing in this policy shall be interpreted to require that the City be responsible for any costs incurred or work completed in the past on any ponds, or to assume responsibility for any pond covered by an existing maintenance agreement unless it has been dissolved according to the terms of the agreement. Nothing in this policy shall be interpreted to require that the City be responsible for any new pond or drainage feature until and unless the City has accepted its final design and condition as provided by a development agreement.

3) Regulatory Requirements and Design Standards

a) State

All maintenance work undertaken by the City shall conform to applicable requirements of the Minnesota Pollution Control Agency and the Minnesota Department of Natural Resources.

b) Lower Rum River Watershed Management Organization

The City shall obtain required permits from the Lower Rum River Watershed Management Organization before conducting maintenance work, if necessary.

c) City of Ramsey

i) Municipal Separate Storm Sewer System Permit (MS4)

All requirements of the City's Municipal Separate Storm Sewer System permit will be adhered to while inspecting and performing all stormwater facilities maintenance work.

ii) Stormwater Pollution Prevention Program (SWPPP)

All requirements of the City's Stormwater Pollution Prevention Program will be adhered to while inspecting and performing all stormwater facilities maintenance work.

iii) Surface Water Management Plan (SWMP)

The City adopted a Surface Water Management Plan (SWMP) in 2015 to promote, preserve and enhance the natural resources within the City of Ramsey. All requirements of the City's Surface Water Management Plan will be adhered to while inspecting and performing all stormwater facilities maintenance work.

4) Stormwater Facilities Maintenance Plans

a) Stormwater Ponds

i) State Ponds

State-owned stormwater ponds are managed by the Minnesota Department of Transportation.

ii) County Ponds

County-owned stormwater ponds are managed by the Anoka Highway Department.

iii) City Ponds

City-owned stormwater ponds and wetlands are managed by the City to provide storage for surface water runoff and to allow for free flow of stormwater through the system. These ponds also reduce peak stormwater flows, promote settling of suspended pollutants and removal of Phosphorus, and reduce velocities downstream of the outlet structure. These ponds may also promote biological uptake of pollutants when vegetated.

If problems are identified during inspections, a Work Order will be prepared in accordance with the following guidelines;

- (1) Trees and other vegetation on side slopes will be maintained to provide safe sight distances for traffic and pedestrians
- (2) Sediment and vegetation will be removed around inlets and outlets when flows are obstructed.
- (3) Bank erosion will be corrected when sediment within the pond bottom is greater than one foot deep.

iv) Private Ponds

Private stormwater ponds are managed by the property owner.

b) Ditches

i) State Ditches

The Minnesota Department of Transportation (MnDOT) is responsible for maintaining all ditches along State Highways within the City including;

- (1) Highway 47 (Saint Francis Boulevard)
- (2) Highway 10 / 169

ii) County Ditches

Anoka County owns and maintains several County Ditches within the City of Ramsey as identified within the SWMP. Two County Ditches are maintained by the City of Ramsey, those being Ditch 43 and Ditch 66.

iii) City Ditches

The City of Ramsey is responsible for maintaining all ditches and other drainage conveyances that collect drainage from City streets, that convey stormwater runoff from ditches along streets to ponds and wetlands, as well as County Ditches 43 and 66. Ditches will be inspected and maintained as needed to protect properties and prevent standing water on streets.

It is normal to observe standing water in ditches for up to several days following heavy rain events, and ditches may hold water for weeks during the Spring thaw. Maintenance work will not be performed to address standing water in ditches for such periods of time.

c) Storm Sewer Systems

i) State

The State is responsible for maintaining its storm sewer systems unless a separate maintenance agreement exists between the State and the City or County designating the City/ County as the responsible party for maintaining all or a portion of the State system.

ii) County

The County is responsible for maintaining its storm sewer systems unless a separate maintenance agreement exists between the County and the City designating the City as the responsible party for maintaining all or a portion of the County system.

iii) City

The City is responsible for maintaining its storm sewer systems unless a separate maintenance agreement exists between the City and the County or State designating the County or State as the responsible party for maintaining all or a portion of the City system.

d) Rivers

i) Mississippi

The Mississippi River borders the south edge of the City and receives stormwater runoff from the City through outfalls along the river banks.

All Mississippi River outfalls are inspected annually. Minor repairs are performed as needed. Major repairs are programmed through the Capital Improvement Program.

ii) Rum

The Rum River borders the east edge of the City and receives stormwater runoff from the City through outfalls along the river banks.

All Rum River outfalls are inspected annually. Minor repairs are performed as needed. Major repairs are programmed through the Capital Improvement Program.

e) Lakes

Numerous lakes exist within the City as defined by the City and/or the Minnesota Department of Natural Resources. Some lakes have stormwater inlets, which are not currently inspected on a routine basis. More information on lakes within the City is located within the Surface Water Management Plan.

f) Brooks

i) Ford Brook

The Ford Brook begins at the Trott Brook and flows north into the City of Nowthen near the northeast corner of the City. The Ford Brook receives a very small amount of stormwater runoff from the City.

ii) Trott Brook

The Trott Brook generally traverses across the center of the City and receives a significant amount of stormwater runoff from the City. Numerous stormwater inlets exist along the Trott Brook, which are not currently inspected on a routine basis.

g) Wetlands

Approximately 20-percent of the surface area of the City of Ramsey is covered by wetlands as outlined within the Surface Water Management Plan.

h) Structural Pollution Control Devices

The City maintains over a dozen Structural Pollution Control Devices installed at various locations within the City's storm sewer system. These devices capture pollutants from stormwater runoff near the point of discharge to receiving water bodies to reduce pollutant loading in receiving water bodies. The City removes pollutants from all of these devices annually, or more often if needed, using the City vacuum truck.

5) Goals

The goals of this Policy are as follows;

- a) Prevent flooding of private properties and public lands.
- b) Reduce pollutant loads entering receiving waters.
- c) Ensure compliance with regulatory agency requirements.

- d) Improve water quality in brooks, lakes, ponds, and wetlands.
- e) Cost-effectively maintain stormwater management facilities.

6) Priorities

The priorities of this Policy are as follows;

- a) Start inspections and maintenance of stormwater management facilities along the Mississippi and Rum Rivers. These facilities are anticipated to have the greatest maintenance needs since they are located at the downstream end of the storm sewer system. Maintaining these facilities first will provide the biggest benefit to the receiving waters.
- b) Continue moving inland incrementally with inspections and maintenance of stormwater management facilities for the same reasons noted above.
- c) Continue updating this Policy to refine work schedules and incorporate additional maintenance practices based on what is working well and what could work better.
- d) Coordinate work with MS4, SWPPP and SWMP requirements.
- e) Continue to improve cost-effectiveness of programmed maintenance practices and operations.

7) Inspections

Stormwater management facilities will be routinely inspected to ensure they continue to function as designed. The guidelines set forth below were developed to provide City staff with practical tools to inspect and maintain all stormwater facilities on a routine basis.

- a) **Water Body Inspections** - The inspection program proposes to begin by annually inspecting 5% of known stormwater ponds under City ownership and/or drainage and utility easements, starting at the Mississippi and Rum Rivers and working inland. Initial inspections will consist of enlisting consultants to survey the ponds to determine how much stormwater storage capacity remains and to schedule pond dredging projects to remove sediment and restore storage capacities. Ongoing inspection requirements include observations for blockage of outlets due to trash, debris, vegetation, or downed trees. Blockages shall be cleared from obstructed outlets as soon as practical. Damaged or deteriorated inlets, outlets and pipes, and malfunctioning valves, gates, locks or access hatches, shall be scheduled for repair or replacement as soon as is economically feasible to do so.
- b) **Structure Inspections** – Drainage structures, castings and grates shall be inspected for debris and repair needs during development of pavement management projects, during flood events, or at a minimum once every 5 years. Structure sumps shall be inspected a minimum of every 4 years to determine if the sump needs to be repaired or pumped. Sumps in new developments are checked within one year after completion of the development to determine if cleaning is required.
- c) **Lift Station Inspections** – The structure and pumping equipment shall be inspected annually.

- d) **Storm Sewer and Culvert Inspections** - Video inspections shall be conducted during development of pavement management projects or when public complaints or concerns are received.
- e) Inspection documentation and maintenance recommendations and requests for routine and emergency maintenance needs shall be submitted to the Public Works Administrative Assistant for scheduling repairs and reporting purposes. Results of the inspection program are used to update the 10-year Capital Improvement Program for budgeting purposes. Emergency repairs include items such as imminent structure failure or suspicious discharges that require the responsible party to be notified to take immediate action to remedy the issue. Routine maintenance shall be performed as needed. Additional maintenance needs, such as pond dredging, will be scheduled and performed as needed.

8) Financial Considerations

For transparency, costs for work required to maintain the City's stormwater management facilities will be included within the City of Ramsey's Capital Improvement Program to the maximum extent practicable.

All costs for the administration and operation of this Policy shall be funded through the City's Stormwater Funds. The main source of revenues for this fund is the Stormwater Utility Fee. In order to fully fund this program, annual increases in the stormwater utility fee rate will be necessary for the foreseeable future. The City Council will make the decision on whether or not to fund the program, and at what level, each year as part of its budget process.

Decisions on what projects to undertake will also be made as part of the budget process and the concurrent annual review of the City's Capital Improvement Program (CIP). The budget will include funds for specific projects and routine maintenance, as well as contingencies for emergencies.

9) Recommendations

The City plans to integrate all inventoried stormwater management facilities into our GIS system to allow the City to more efficiently manage our stormwater facilities. The data will be updated as more information is made available.

The City also plans to continue updating its as-built record plans and stormwater facilities inventories, and will include applicable inventories within the appendix of this Policy at such time that the inventories are reasonably accurate.

The City will work to evaluate the effectiveness of this Policy over time and will maintain records of observed sediment accumulations over time for future estimating purposes, pond volumes for constructed stormwater ponds including both the water quality volume and

permanent pool volume based on pond geometry and inlet, outlet (invert or outlet control), and emergency overflow/spillway elevations.

The City will attempt to calculate load-based removal efficiencies to analyze the treatment effectiveness of ponds to determine whether opportunities exist to upgrade the functionality of ponds in areas where additional treatment may benefit receiving waters.

This Policy will be evaluated annually to determine if changes to the frequencies or scopes of work are required.

Please contact Bruce Westby, City Engineer / Public Works Director, at 763-433-9863 or bwestby@cityoframsey.com with any questions regarding this Policy.

DRAFT

Definitions

Drainage structures – Catch basins, control structures, flared ends, manholes, and structural pollution control devices. Sump catch basins and sump manholes are constructed with a sump structure for collection of sediment and debris.

Drainage pipes – Storm sewer and culverts.

Drainageways – Ditches, swales or other surface stormwater conveyances.

Dry retention basins – A shallow, dry basin with an outlet at the invert of the basin. Dry retention basins are constructed to attenuate peak discharges and temporarily detain runoff to promote sedimentation.

Wet detention basins – A shallow basin that maintains a permanent pool of water by using an elevated outlet control structure. Stormwater is treated through sedimentation and biological uptake of pollutants.

Filtration basins – A shallow basin or sand filter with engineered or amended soil and an under-drain system. The basin detains stormwater and allows it to infiltrate through the soil, sand or engineered media. Treated stormwater is directed to the receiving water via the under-drain system.

Infiltration basins – A shallow basin in permeable soils that detain and infiltrate stormwater. There is rarely an under-drain system unless needed to provide maintenance access. Infiltration basins use the natural filtering ability of the soil to remove pollutants from stormwater runoff. If problems are identified during inspections a Work Order is prepared in accordance with the following guidelines. Replace surface soil or vegetation as needed to maintain a layer of permeable soil or a dense cover of non-woody vegetation in the base of the infiltration area. Correct any structural deficiencies that interfere with the function of the basin. Remove undesirable vegetation, such as woody vegetation.

Swales, filter strips or ditches – Swales, filter strips or ditches are constructed to direct or convey stormwater runoff. They should be planted with vegetation that will maintain the structure and resist erosion. If problems are identified during inspections a Work Order shall be prepared in accordance with the following guidelines. Cleanout sediment when it blocks culverts or obstructs the pipe or flow of stormwater. Stabilize and re-seed when erosion gullies are more than one foot deep. Mow to remove vegetation that obstructs flow and to maintain desirable vegetation. These linear channels are vegetated to assist in decreasing runoff velocity and promote infiltration and physical filtration. Some may incorporate rock checks to reduce the slope of the channel.

Depressions – This includes shallow depressions at the end of a stormwater pipe or flared end section that acts as a stormwater infiltration area but was not designed or constructed.

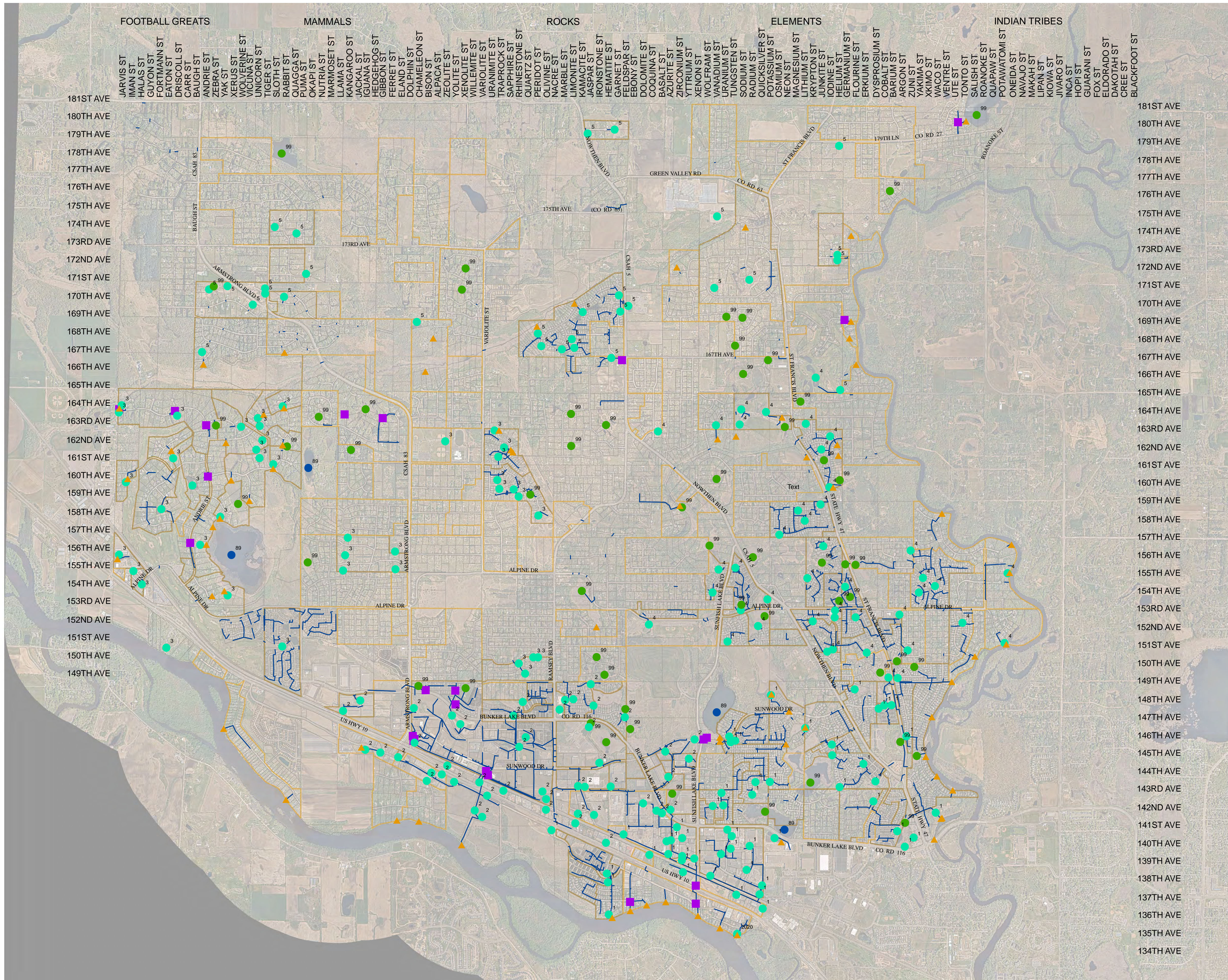
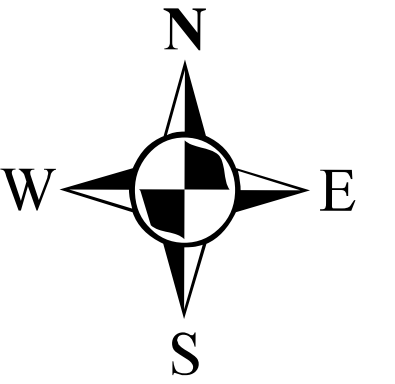
Lakes – Larger bodies of water or deep-water aquatic habitats that have been defined as lakes by the City and/or Minnesota Department of Natural Resources.

Stormwater wetlands – A natural wetland or creek segment that has been modified to receive and treat stormwater discharges. Stormwater is treated primarily through biological uptake.

Mitigation wetlands – A wetland constructed for replacement of wetlands lost due to construction or alteration of the landscape. These wetlands typically have stormwater discharges routed to the basin to maintain hydrology.

DRAFT

2024 Storm Sewer, Pond and Outfall Map



Legend

- ▲ Outfall (74)
- Constructed Ponds, Lakes, Wetlands**
- Feature type**
- Lake (4)
- Pond (217)
- Wetland (55)
- Structural BMP (23)
- Storm Sewer Pipe 12" and Larger
- Subdivision**
- ▭ Grading Plan (108)
- ▭ No Grading Plan (87)
- ▭ Parcels

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Address infrastructure needs.**Title:**

Consider Recommendation to City Council to Accept Bids and Award Contract for Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements

Purpose/Background:**Purpose:**

The purpose of this case is to consider a recommendation to the City Council to accept bids and award a contract for construction of Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements.

Background:

In June of 2016, the Anoka Conservation District completed a Stormwater Retrofit Analysis for the City of Ramsey and the Lower Rum River Watershed Management Organization (LRRWMO) to identify and rank potential improvement projects that drain to the Mississippi and Rum Rivers. The Stormwater Retrofit Analysis recommended modifying a wetland within Rivers Bend Park to increase the volume of stormwater runoff storage provided by this wetland that currently treats runoff from a 38-acre catchment area for the purpose of reducing sediment and phosphorus loads that can end up in the receiving water, which is an oxbow of the Rum River. See Project ID: RR8-A on page 89 of the attached modified version of the Stormwater Retrofit Analysis for reference.

In the Fall of 2023, the Lightbridge Academy submitted a commercial development application so staff began working with the developer to address their stormwater management requirements. This development is located within the City of Ramsey's East Drinking Water Supply Management Area (DWSMA) where infiltration of stormwater is prohibited so staff reached out to the Lower Rum River Watershed Management Organization (LRRWMO) to confirm that the developer could purchase credits to utilize a portion of The COR regional infiltration basin to comply with the LRRWMO's volume retention requirements. When The COR regional infiltration basin was constructed, it was believed that the basin could be used to accommodate development within the East and West DWSMA's. However, the LRRWMO's engineer, BARR Engineering, recently informed staff that since the two DWSMA's drain to different water bodies (the East DWSMA drains to the Rum River and the West (The COR) DWSMA drains to the Mississippi River) Lightbridge Academy cannot simply pay a fee to use The COR infiltration basin.

Discussions then turned to how Lightbridge can comply with LRRWMO requirements for stormwater runoff rate control and water quality improvements. The developer's engineer stated they cannot provide these improvements on their site so they asked if the City could accommodate these requirements. After discussions with the LRRWMO's engineer it was determined that the City needed to construct stormwater management facility improvements on the west edge of Rivers Bend Park, West of the existing wetland that currently treats all stormwater runoff from the 38-acre catchment as addressed in the ACD's Stormwater Retrofit Analysis, and that the existing wetland should not be impacted. This existing wetland is undersized and does not meet the rate control and water quality treatment requirements per current LRRWMO stormwater standards for the 38-acre catchment that includes several undeveloped parcels including the Lightbridge Academy parcel.

Since this 38-acre catchment is located within the City of Ramsey's East Drinking Water Supply Management Area (DWSMA), where infiltration of stormwater is prohibited, the pond must be lined. The LRRWMO Management Plan describes/defines sites, such as those located within a DWSMA, as infeasible or inappropriate for infiltration and identifies other scenarios for the permittee to investigate. Volume retention requirements can either be provided on-site as part of the Rivers Bend Regional Stormwater Improvements by utilizing filtration

practices as allowed by LRRWMO stormwater standards, or off-site in a regional infiltration basin located outside the East DWSMA.

City staff has discussed various aspects of this project with LRRWMO and MPCA staff, including potential wetland impacts, options for meeting volume retention requirements, and how the timing of this project can be coordinated with the Lightbridge Academy development construction to ensure adequate stormwater runoff treatment can be provided during construction of both projects.

Staff has coordinated the construction schedule of the City’s stormwater pond improvements project with the Lightbridge Academy development construction schedule to address concerns raised by the LRRWMO. Lightbridge Academy’s engineer prepared and submitted a phased construction plan with their LRRWMO permit application, which has been approved. Lightbridge Academy is constructing their building first with minimal disturbance to the rest of the site while the Rivers Bend Regional Stormwater Improvements project is designed and bid. Lightbridge Academy would then complete site improvements including impervious surface improvements during construction of the Rivers Bend Regional Stormwater Management Improvements this Fall.

Bolton & Menk also performed a wetland delineation to identify the limits of the existing wetland to avoid impacts. This report was completed and is submitted to the LRRWMO for concurrence and approval, which is currently scheduled to occur on September 19, 2024, along with approval of the LRRWMO erosion control/stormwater permit.

On August 12, 2024, the plans and specifications prepared by Bolton & Menk, Inc. were approved and advertisements for bids were authorized for City Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements.

Bids were advertised in QuestCDN, the electronic bid platform utilized by the City of Ramsey for all improvement projects, and in the Anoka Union Herald on Friday, August 16th, and on Friday, August 23rd, 2024. Five (5) bids were received, opened and tabulated on September 6, 2024, and the following bids were found to comply with the approved plans and specifications and advertisement for bids.

Contractor	Base Bid	Alternate Bid	Total Bid
Douglas-Kerr Underground, LLC	\$899,653.30	\$0.01	\$899,653.31
Rachel Contracting, LLC	\$999,548.00	\$16,900.00	\$1,016,448.00
Peterson Companies	\$1,009,679.76	\$0.00	\$1,009,679.76
U.S. SiteWork, Inc. \$	\$1,254,337.00	\$22,160.00	\$1,276,497.00
New Look Contracting, Inc.	\$1,546,189.50	\$15,000.00	\$1,561,189.50
<i>Engineer’s Estimate</i>	<i>\$1,169,370.00</i>	<i>\$95,000.00</i>	<i>\$1,264,370.00</i>

Douglas-Kerr Underground, LLC of Mora, Minnesota, who submitted a total bid in the amount of \$899,653.31 for construction of said improvements in accordance with the approved plans and specifications and advertisement for bids, is the lowest responsible bidder. Attached is a Bid Results and Recommendation memo from Bolton & Menk, Inc., which states if the City of Ramsey decides to award the project to the low bidder, we recommend awarding the contract to Douglas-Kerr Underground, L.L.C.

Notification:

Notifications are not required for this case.

Time Frame/Observations/Alternatives:

Observations:

Attached is a proposal from Bolton & Menk, Inc. for construction services in the not-to-exceed amount of \$15,240.

Project Timelines

The following preliminary project schedule is included in Bolton & Menk’s design services proposal attached to this case.

September 24, 2024	Award Construction Contract
September 30, 2024	Begin Construction
November 22, 2024	Substantial Completion (Dormant Seed)
June 2025	Final Completion and Closeout

The construction dates included in this schedule are dependent on receiving LRRWMO Permit Approval on September 19, 2024. The status of the LRRWMO permit will be known when this item is presented to the City Council on September 24th for award of contract.

Alternatives:

Alternative #1 - Motion recommending City Council accept bids and award a contract for construction of Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements, to Douglas-Kerr Underground, LLC of Mora, Minnesota, in the total bid amount of \$899,653.31.

Alternative #2 - Motion of other.

Funding Source:

The low bid received is in the amount of \$899,653.31. Bolton & Menk’s construction services proposal has a not0-to-exceed amount of \$15,240. The total project cost is therefore \$914,893.31.

Funding for this improvement project is proposed to come from a combination of watershed grant funds administered by the Anoka Conservation District, and City Storm Water Utility Funds.

Funding Source	Project Costs
Watershed Grant Funds	\$248,250.00
Storm Water Utility Funds	\$666,643.31
<i>Project Total</i>	<i>\$914,893.31</i>

The City’s cost may be lower is staff is successful in obtaining additional watershed grant funds.

Recommendation:

Staff recommends providing a recommendation to the City Council to accept bids and award a contract for construction of Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements.

Outcome/Action:

Consider a recommendation to the City Council to accept bids and award a contract for construction of Improvement Project #25-01, Rivers Bend Regional Stormwater Improvements.

Attachments

BMI Constr Services Proposal
Final Plans

Form Review

Inbox

Brian Hagen

Form Started By: Bruce Westby

Final Approval Date: 09/12/2024

Reviewed By

Brian Hagen

Date

09/12/2024 02:19 PM

Started On: 09/11/2024 12:55 PM



Real People. Real Solutions.

7533 Sunwood Drive NW
Suite 206
Ramsey, MN 55303-5119

Ph: (763) 433-2851
Fax: (763) 427-0833
Bolton-Menk.com

MEMORANDUM

Date: September 6, 2024
To: Bruce Westby, P.E., City Engineer / Director of Public Works
From: Kevin Kielb, P.E., Principal Engineer
Subject: Bid Results and Recommendation
River's Bend Regional Storm Water Improvements
City Project No: 25-01

Five bids were received for the Rivers Bend Storm Water Improvement Project (City Project #25-01). The bids were read aloud at 10:00 AM on September 6, 2024. The bids were then reviewed, tabulated and the results are summarized below:

CONTRACTOR	BASE BID	ADD ALTERNATE	BASE BID PLUS ADD ALT
Douglas-Kerr Underground, L.L.C	\$899,653.30	\$0.01	\$899,653.31
Rachel Contracting, LLC	\$999,548.00	\$16,900.00	\$1,016,448.00
Peterson Companies	\$1,009,679.76	\$0.00	\$1,009,679.76
U.S. SiteWork, Inc.	\$1,254,337.00	\$22,160.00	\$1,276,497.00
New Look Contracting, Inc.	\$1,546,189.50	\$15,000.00	\$1,561,189.50
Engineer's Estimate	\$1,169,370.00	\$95,000.00	\$1,264,370.00

We reviewed Douglas-Kerr Underground, L.L.C's unit prices and compared them to the Engineer's Estimate and other bids received. While differences were noted in individual bid items, our opinion is that the bid is responsive and not materially unbalanced.

If the City of Ramsey decides to award the project to the low bidder, we recommend awarding the contract to Douglas-Kerr Underground, L.L.C.

If you have any questions, please contact me at (651) 968-7760.



Real People. Real Solutions.

7533 Sunwood Drive NW
Suite 206
Ramsey, MN 55303-5119

Ph: (763) 433-2851
Fax: (763) 427-0833
Bolton-Menk.com

September 12, 2024

Bruce Westby, P.E.
City Engineer/Public Works Director
City of Ramsey
7550 Sunwood Drive NW
Ramsey, MN 55303

RE: Proposal for CP 25-01 - Rivers Bend Park Pond Modification Construction Services

Dear Mr. Westby:

We appreciate the opportunity to assist the City of Ramsey with construction services related to the Rivers Bend Park pond.

SCOPE OF WORK

Our proposed services will be as described below:

Contract Preparation – Assist with Notice of Award, Contracts, and Notice to Proceed. Prepare sets of contract documents (plans and specifications) for City and Contractor.

Pre-Construction Conference – Attend Pre-Construction Conference to address questions that may arise related to project design or construction staking.

Shop Drawing / Submittal Review – Review submittals for key project components.

Construction Staking – Includes the following:

- Staking Calculations and Preparation of Data,
- Establishing Survey Control,
- Staking of Erosion Control and Removals,
- Staking of Storm Sewer, including Flared End Sections,
- Staking Right-of-Way, and
- Staking Pond Grading.

Pay Application Assistance – Work with City and Contractor to complete payment documents.

As-Built Survey and Record Drawings – Survey key project components after completion of construction and prepare of Record Drawings.

Construction Revisions and Plan Updates – To be performed as needed during construction. We have assumed a total of 16 hours for this task.

We have attached a fee spreadsheet depicting hours, rates, and total compensation on a per task basis. Our proposed Not-To-Exceed fees for the work are \$53,699.00.

PROPOSED FEES

Bolton & Menk proposes to perform the services on a not-to-exceed amount, based on the estimated fees per task in the table below:

<u>Task</u>	<u>Fees</u>
Contract Preparation	\$ 963
Pre-Construction Conference	\$ 1,059
Shop Drawing/Submittal Reviews	\$ 1,493
Construction Staking	\$ 4,206
Pay Application Assistance	\$ 2,088
Record Drawings, with Survey	\$ 2,805
Plan Revisions, if Needed	\$ 2,626
Total Fees	\$15,240

The fees listed above are estimates based on each task. The actual fees required for each task may vary, however, the total amount of \$15,240 is considered a Not-to-Exceed amount.

SCHEDULE

We are prepared to begin services immediately after authorization from the City is received.

If there are any questions, please call me at (651) 968-7760.

Sincerely,

BOLTON & MENK, INC.



Kevin P. Kielb, P.E.
Principal Engineer

CITY OF RAMSEY

CONSTRUCTION PLANS FOR

RIVERS BEND REGIONAL STORM WATER IMPROVEMENTS

CITY PROJECT NUMBER 25-01

STORM SEWER, POND GRADING, EROSION CONTROL AND TURF ESTABLISHMENT

AUGUST, 2024

MAP OF THE
CITY OF RAMSEY
ANOKA COUNTY, MN



MAP LEGEND

— PROJECT LIMITS

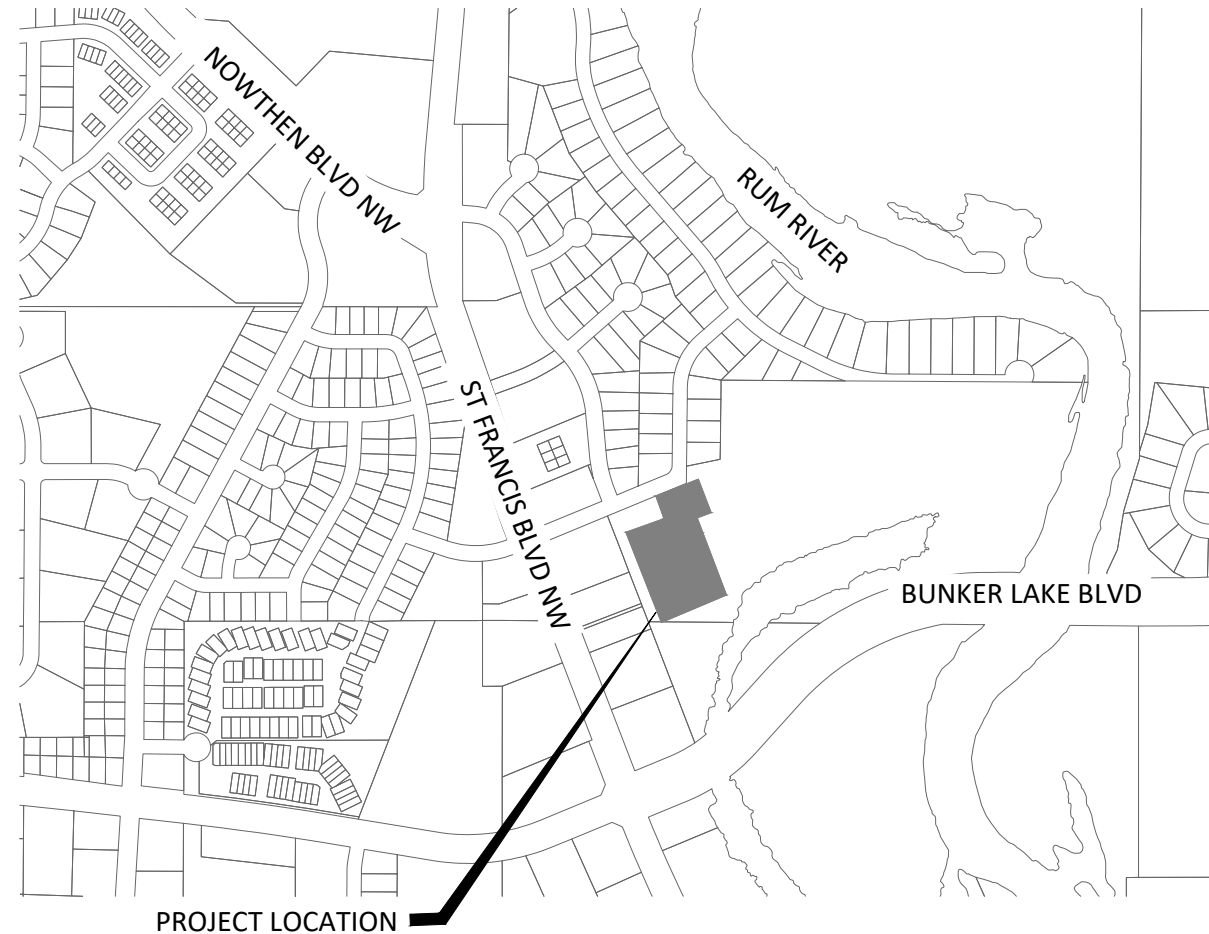
NOTE: EXISTING UTILITY INFORMATION SHOWN ON THIS PLAN HAS BEEN PROVIDED BY THE UTILITY OWNER. THE CONTRACTOR SHALL FIELD VERIFY EXACT LOCATIONS PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY GOPHER STATE ONE CALL, 1-800-252-1166 OR 651-454-0002.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D UNLESS OTHERWISE NOTED. THIS UTILITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-22, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

--- GOVERNING SPECIFICATIONS ---

THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATION FOR CONSTRUCTION" AND THE 2023 EDITION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) "STANDARD SPECIFICATIONS" FOR MUNICIPAL UTILITIES SHALL GOVERN.

ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM AND BE INSTALLED IN ACCORDANCE WITH THE LATEST "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MN MUTCD) AND PART VI, THE LATEST "FIELD MANUAL" FOR TEMPORARY TRAFFIC CONTROL DEVICES.



SHEET NUMBER	SHEET TITLE
1	TITLE SHEET
2	LEGEND
3	EXISTING CONDITIONS & REMOVALS
4	STATEMENT OF ESTIMATED QUANTITIES
5	POND TYPICAL SECTION
6	DETAILS
7	EROSION CONTROL & TURF ESTABLISHMENT
8 - 10	STORMWATER POLLUTION PREVENTION PLAN
11	GRADING PLANS
12	STORM SEWER PROFILES
THIS PLAN SET CONTAINS <u>12</u> SHEETS.	

Approved: City Engineer / Director of Public Works _____ Date _____

⚭ BM=862.90 TNH 77' SE OF SE INTERSECTION OF 142ND AVENUE NW & XKINO STREET	PROJECT DATUM: HORIZONTAL: ANOKA COUNTY COORDINATES (1996 ADJUSTMENT) VERTICAL: NAVD 88	RECORD DRAWING INFORMATION	CITY OF RAMSEY, MINNESOTA RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS TITLE SHEET	SHEET 1 OF 12
		OBSERVER: CONTRACTOR: DATE:		

© Bolton & Menk, Inc. 2024. All Rights Reserved. P:\RAMSEY\25-01\CP\25-01-CP-01.dwg 8/20/2024 4:05:25 PM

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Kevin P. Kielb
 KEVIN P. KIELB
 LIC. NO. 23211 DATE 8/20/2024



7533 SUNWOOD DR NW, SUITE 206
 RAMSEY, MN 55303
 Phone: (763) 433-2851
 Email: Ramsey@bolton-menk.com
 www.bolton-menk.com

DESIGNED	NO.	ISSUED FOR	DATE
ZFL			
TJH			
KPK			
CLIENT PROJ. NO. CP-25-01			

EXISTING TOPOGRAPHIC SYMBOLS

	ACCESS GRATE		REGULATION STATION GAS
	AIR CONDITION UNIT		SATELLITE DISH
	ANTENNA		SIGN TRAFFIC
	AUTO SPRINKLER CONNECTION		SIGNAL CONTROL CABINET
	BARRICADE PERMANENT		SOIL BORING
	BASKETBALL POST		SIREN
	BENCH		TELEPHONE BOOTH
	BIRD FEEDER		TILE INLET
	BOLLARD		TILE OUTLET
	BUSH		TILE RISER
	CATCH BASIN RECTANGULAR CASTING		TRANSFORMER-ELECTRIC
	CATCH BASIN CIRCULAR CASTING		TREE-CONIFEROUS
	CURB STOP		TREE-DEAD
	CLEAN OUT		TREE-DECIDUOUS
	CULVERT END		TREE STUMP
	DRINKING FOUNTAIN		TRAFFIC ARM BARRIER
	DOWN SPOUT		TRAFFIC SIGNAL
	ELECTRIC CAR CHARGE STATION		TRASH CAN
	FILL PIPE		UTILITY MARKER
	FIRE HYDRANT		VALVE
	FLAG POLE		VALVE POST INDICATOR
	FLARED END / APRON		VALVE VAULT
	FUEL PUMP		VAULT
	GRILL		VENT PIPE
	GUY WIRE ANCHOR		WATER SPIGOT
	HANDHOLE		WELL
	HANDICAP SPACE		WETLAND DELINEATED MARKER
	IRRIGATION SPRINKLER HEAD		WETLAND
	IRRIGATION VALVE BOX		WET WELL
	LIFT STATION CONTROL PANEL		YARD HYDRANT
	LIFT STATION		
	LIGHT POLE		
	MAILBOX		
	MANHOLE-COMMUNICATION		
	MANHOLE-ELECTRIC		
	MANHOLE-GAS		
	MANHOLE-HEAT		
	MANHOLE-RECLAIMED WATER		
	MANHOLE-SANITARY SEWER		
	MANHOLE-STORM SEWER		
	MANHOLE-UTILITY		
	MANHOLE-WATER		
	METER		
	DRIVE-THRU MICROPHONE		
	PARKING METER		
	PAVEMENT MARKING		
	PEDESTAL-COMMUNICATION		
	PEDESTAL-ELECTRIC		
	PEDESTRIAN PUSH BUTTON		
	PICNIC TABLE		
	POLE-UTILITY		
	POST		
	RAILROAD SIGNAL POLE		

PROPOSED TOPOGRAPHIC SYMBOLS

	CLEANOUT
	MANHOLE
	LIFT STATION
	STORM SEWER CIRCULAR CASTING
	STORM SEWER RECTANGULAR CASTING
	STORM SEWER FLARED END / APRON
	STORM SEWER OUTLET STRUCTURE
	STORM SEWER OVERFLOW STRUCTURE
	CURB BOX
	FIRE HYDRANT
	WATER VALVE
	WATER REDUCER
	WATER BEND
	WATER TEE
	WATER CROSS
	WATER SLEEVE
	WATER CAP / PLUG
	RIP RAP
	DRAINAGE FLOW
	TRAFFIC SIGNS

SURVEY SYMBOLS

	BENCHMARK LOCATION		CAST IRON MONUMENT
	CONTROL POINT		STONE MONUMENT
	MONUMENT FOUND		

EXISTING TOPOGRAPHIC LINES

	RETAINING WALL
	FENCE
	FENCE-DECORATIVE
	GUARD RAIL
	TREE LINE
	BUSH LINE

SURVEY LINES

	CONTROLLED ACCESS BOUNDARY
	CENTERLINE
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	EXISTING LOT LINE
	PROPOSED LOT LINE
	EXISTING RIGHT-OF-WAY
	PROPOSED RIGHT-OF-WAY
	SETBACK LINE
	SECTION LINE
	QUARTER LINE
	SIXTEENTH LINE
	TEMPORARY EASEMENT

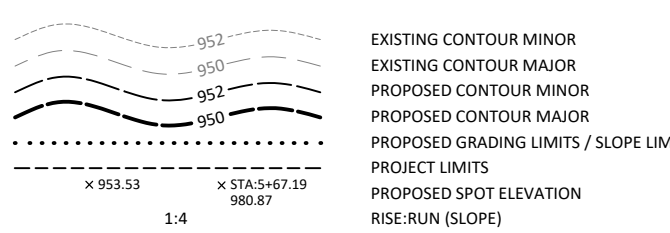
EXISTING UTILITY LINES

	FORCEMAIN
	SANITARY SEWER
	SANITARY SERVICE
	STORM SEWER
	STORM SEWER DRAIN TILE
	WATERMAIN
	WATER SERVICE
	RECLAIMED WATER

PROPOSED UTILITY LINES

	FORCEMAIN
	SANITARY SEWER
	SANITARY SERVICE
	STORM SEWER
	STORM SEWER DRAIN TILE
	WATERMAIN
	WATER SERVICE
	PIPE CASING
	TRENCHLESS PIPE (PLAN VIEW)
	TRENCHLESS PIPE (PROFILE VIEW)

GRADING INFORMATION



HATCH PATTERNS

	BITUMINOUS		GRAVEL
	CONCRETE		

EXISTING PRIVATE UTILITY LINES

NOTE:
 EXISTING UTILITY INFORMATION SHOWN ON THIS PLAN HAS BEEN PROVIDED BY THE UTILITY OWNER. THE CONTRACTOR SHALL FIELD VERIFY EXACT LOCATIONS PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY GOPHER STATE ONE CALL, 1-800-252-1166 OR 651-454-0002.

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	UNDERGROUND FIBER OPTIC
	UNDERGROUND ELECTRIC
	UNDERGROUND GAS
	UNDERGROUND COMMUNICATION
	OVERHEAD ELECTRIC
	OVERHEAD COMMUNICATION
	OVERHEAD UTILITY

UTILITIES IDENTIFIED WITH A QUALITY LEVEL :

LINE TYPES FOLLOW THE FORMAT: UTILITY TYPE - QUALITY LEVEL
 EXAMPLE: G-A UNDERGROUND GAS, QUALITY LEVEL A
 UTILITY QUALITY LEVEL (A,B,C,D) DEFINITIONS CAN BE FOUND IN CI/ASCE 38-22.

UTILITY QUALITY LEVELS:

QUALITY LEVEL D: PROVIDES THE MOST BASIC LEVEL OF INFORMATION. IT INVOLVES COLLECTING DATA FROM EXISTING UTILITY RECORDS. RECORDS MAY INCLUDE AS-BUILT DRAWINGS, DISTRIBUTION AND SERVICES MAPS, EXISTING GEOGRAPHIC INFORMATION SYSTEM DATABASES, CONSTRUCTION PLANS, ETC.

QUALITY LEVEL C: INVOLVES SURVEYING VISIBLE SUBSURFACE UTILITY STRUCTURES SUCH AS MANHOLES, HAND-HOLES, UTILITY VALVES AND METERS, FIRE HYDRANTS, PEDESTALS AND UTILITY MARKERS, AND THEN CORRELATING THE INFORMATION WITH EXISTING UTILITY RECORDS TO CREATE COMPOSITE DRAWINGS. INCLUDES QUALITY LEVEL D ACTIVITIES.

QUALITY LEVEL B: INVOLVES DESIGNATING THE HORIZONTAL POSITION OF SUBSURFACE UTILITIES THROUGH SURFACE DETECTION METHODS AND COLLECTING THE INFORMATION THROUGH A SURVEY METHOD. INCLUDES QUALITY LEVEL C AND D TASKS.

QUALITY LEVEL A: PROVIDES THE HIGHEST LEVEL OF ACCURACY. IT INVOLVES LOCATING OR POTHOLING UTILITIES AS WELL AS ACTIVITIES IN QUALITY LEVELS B, C, AND D. THE LOCATED FACILITY INFORMATION IS SURVEYED AND MAPPED AND THE DATA PROVIDES PRECISE PLAN AND PROFILE INFORMATION.

ABBREVIATIONS

A	ALGEBRAIC DIFFERENCE	GRAV	GRAVEL	RSC	RIGID STEEL CONDUIT
ADJ	ADJUST	GU	GUTTER	RT	RIGHT
ALT	ALTERNATE	GV	GATE VALVE	SAN	SANITARY SEWER
B-B	BACK TO BACK	HDPE	HIGH DENSITY POLYETHYLENE	SCH	SCHEDULE
BIT	BITUMINOUS	HH	HANDHOLE	SERV	SERVICE
BLDG	BUILDING	HP	HIGH POINT	SHLD	SHOULDER
BMP	BEST MANAGEMENT PRACTICE	HWL	HIGH WATER LEVEL	STA	STATION
BR	BEGIN RADIUS	HYD	HYDRANT	STD	STANDARD
BV	BUTTERFLY VALVE	I	INVERT	STM	STORM SEWER
CB	CATCH BASIN	K	CURVE COEFFICIENT	TC	TOP OF CURB
C&G	CURB AND GUTTER	L	LENGTH	TE	TEMPORARY EASEMENT
CIP	CAST IRON PIPE	LO	LOWEST OPENING	TEMP	TEMPORARY
CIPP	CURED-IN-PLACE PIPE	LP	LOW POINT	TNH	TOP NUT HYDRANT
CL	CENTER LINE	LT	LEFT	TP	TOP OF PIPE
CL	CLASS	MAX	MAXIMUM	TYP	TYPICAL
CLVT	CULVERT	MH	MANHOLE	VCP	VITRIFIED CLAY PIPE
CMP	CORRUGATED METAL PIPE	MIN	MINIMUM	VERT	VERTICAL
C.O.	CHANGE ORDER	MR	MID RADIUS	VPC	VERTICAL POINT OF CURVE
COMM	COMMUNICATION	NIC	NOT IN CONTRACT	VPI	VERTICAL POINT OF INTERSECTION
CON	CONCRETE	NMC	NON-METALLIC CONDUIT	VPT	VERTICAL POINT OF TANGENT
CSP	CORRUGATED STEEL PIPE	NTS	NOT TO SCALE	WM	WATERMAIN
DIA	DIAMETER	NWL	NORMAL WATER LEVEL		
DIP	DUCTILE IRON PIPE	OHW	ORDINARY HIGH WATER LEVEL		
DWY	DRIVEWAY	PC	POINT OF CURVE	AC	ACRES
E	EXTERNAL CURVE DISTANCE	PCC	POINT OF COMPOUND CURVE	CF	CUBIC FEET
ELEC	ELECTRIC	PE	PERMANENT EASEMENT	CV	COMPACTED VOLUME
ELEV	ELEVATION	PED	PEDESTRIAN, PEDESTAL	CY	CUBIC YARD
EOF	EMERGENCY OVERFLOW	PERF	PERFORATED PIPE	EA	EACH
ER	END RADIUS	PERM	PERMANENT	EV	EXCAVATED VOLUME
ESMT	EASEMENT	PI	POINT OF INTERSECTION	LB	POUND
EX	EXISTING	PL	PROPERTY LINE	LF	LINEAR FEET
FES	FLARED END SECTION	PRC	POINT OF REVERSE CURVE	LS	LUMP SUM
F-F	FACE TO FACE	PT	POINT OF TANGENT	LV	LOOSE VOLUME
FF	FINISHED FLOOR	PVC	POLYVINYL CHLORIDE PIPE	SF	SQUARE FEET
F&I	FURNISH AND INSTALL	PVMT	PAVEMENT	SV	STOCKPILE VOLUME
FM	FORCEMAIN	R	RADIUS	SY	SQUARE YARD
FO	FIBER OPTIC	R/W	RIGHT-OF-WAY		
F.O.	FIELD ORDER	RCP	REINFORCED CONCRETE PIPE		
GRAN	GRANULAR	RET	RETAINING		

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CITY OF RAMSEY, MINNESOTA		SHEET
RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS		2
LEGEND		OF
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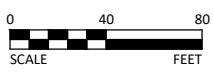


- NOTES:**
1. THE CONTRACTOR SHALL REMOVE ALL TREES WITHIN LIMITS SHOWN INCLUDING CHIPS, BRUSH, STUMPS, AND ROOTS. DISPOSE OF TREES AND BRUSH OFF-SITE.
 2. VERIFY CLEARING LIMITS WITH ENGINEER PRIOR TO TREE REMOVAL OPERATIONS.
 3. THE CONTRACTOR SHALL FIELD VERIFY EXACT UTILITY LOCATIONS PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY GOPHER STATE ONE CALL, 1-800-252-1166 OR 651-454-0002.



LEGEND			
	CLEAR & GRUB TREES		CURB & GUTTER
	TREE-CONIFEROUS		EXISTING EASEMENT LINE
	TREE-DECIDUOUS		EXISTING RIGHT-OF-WAY
	CLEANOUT		FENCE
	STORM MANHOLE		UNDERGROUND GAS
	SANITARY MANHOLE		UNDERGROUND COMM
	STORM CATCH BASIN		OVERHEAD UTILITY
	FLARED END SECITON		FIBER OPTIC LINE
	VALVE		WATERMAIN
			SANITARY SEWER
			STORM SEWER
			RUM RIVER FLOOD PLAIN
			DELINEATED WETLAND
			BOLLARD
			POST
			UTILITY MARKER - GAS
			TRANSFORMER-ELECTRIC
			PEDESTAL-COMMUNICATION
			PEDESTAL-ELECTRIC
			HANDHOLE
			POLE-UTILITY
			GUY WIRE ANCHOR
			EXISTING TRAFFIC SIGN
			FIRE HYDRANT

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CITY OF RAMSEY, MINNESOTA
 RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
 EXISTING CONDITIONS & REMOVALS

STATEMENT OF ESTIMATED QUANTITIES				
ITEM NO.	MNDOT SPEC NO.	ITEM	NOTES	TOTAL QUANTITY
BASE BID				
1	2021.501	MOBILIZATION		LUMP SUM 1
2	2101.505	CLEARING		ACRE 2.75
3	2101.505	GRUBBING		ACRE 2.75
4	2104.503	REMOVE BOLLARDS		EACH 2
5	2104.503	REMOVE PIPE SEWERS (STORM)		LIN FT 60
6	2106.507	EXCAVATION - COMMON	(1)	LUMP SUM 1
7	2106.507	COMMON EMBANKMENT (CV)	(2)	CU YD 940
8	2106.601	DEWATERING		LUMP SUM 1
9	2108.604	POND LINER SYSTEM	(1), (2), (3)	SQ YD 4415
10	2123.610	STREET SWEEPER (WITH PICKUP BROOM)		HR 24
11	2502.502	15" RC PIPE APRON		EACH 1
12	2502.502	24" RC PIPE APRON		EACH 3
13	2503.503	15" RC PIPE SEWER 3006 CLASS V		LIN FT 64
14	2503.503	24" RC PIPE SEWER 3006 CLASS III		LIN FT 150
15	2503.503	36" RC PIPE SEWER 3006 CLASS III		LIN FT 30
16	2503.503	48" RC PIPE SEWER 3006 CLASS III		LIN FT 30
17	2503.601	TEMPORARY BYPASS PUMPING		LUMP SUM 1
18	2503.602	CONNECT TO EXISTING STORM SEWER		EACH 4
19	2506.502	CASTING ASSEMBLY (STORM SEWER)		EACH 2
20	2504.601	IRRIGATION REPAIR		LIN FT 200
21	2506.603	CONSTRUCT DRAINAGE STRUCTURE (OUTLET CONTROL)		LIN FT 7
22	2506.603	CONSTRUCT DRAINAGE STRUCTURE DES 96-4020 WITH DIVERSION BAFFLE		LIN FT 14
23	2511.507	RANDOM RIP RAP CLASS III		CU YD 50
24	2563.601	TRAFFIC CONTROL		LUMP SUM 1
25	2573.501	STABILIZED CONSTRUCTION EXIT		EACH 1
26	2573.501	STORM DRAIN INLET PROTECTION		EACH 5
27	2573.503	SILT FENCE, TYPE MS		LIN FT 1800
28	2574.507	COMMON TOPSOIL BORROW (LV)		CU YD 900
29	2574.508	FERTILIZER TYPE 3		POUND 530
30	2575.504	EROSION PREVENTION CATEGORY 20		SQ YD 8500
31	2575.505	SEEDING		SQ YD 8500
32	2575.508	SEED MIXTURE 35-241		POUND 70
33	2575.508	HYDRAULIC BONDED FIBER MATRIX		POUND 500
ADD ALTERNATE 1				
34		PREVAILING WAGES	(4)	LUMP SUM 1

NOTES:

(1) BID ITEM INCLUDES ALL TOPSOIL STRIPING, EXCAVATION, AND STOCKPILING. SEE SPECIFICATIONS. AMOUNT OF EXCAVATION IS DEPENDENT ON THE LINER PRODUCT CHOSEN BY THE CONTRACTOR. SEE EARTHWORK TABLES FOR QUANTITIES ASSOCIATED WITH THE DIFFERENT LINER PRODUCTS.

(2) BID ITEM INCLUDES TOPSOIL PLACEMENT AND EMBANKMENT REQUIRED AROUND DIVERSION STRUCTURES. PLACEMENT OF SOIL COVER ASSOCIATED WITH THE GEOSYNTHETIC LINER SHALL BE INCLUDED IN THE POND LINER BID PRICE AND NO ADDITIONAL COMPENSATION SHALL BE GRANTED. SEE TABLE FOR APPROXIMATE ADDITIONAL QUANTITY.

(3) GEOSYNTHETIC CLAY LINER, HDPE LINER, 2.0-FEET THICK CLAY LINER, OR APPROVED EQUAL. SEE SPECIFICATIONS, THE FURNISHING AND INSTALLING OF SPECIFIC ITEMS AND/OR THE PERFORMANCE OF WORK SHALL NOT BE INDIVIDUALLY PAID IN THE ABSENCE OF A SPECIFIC BID ITEM FOR THE WORK.

(4) THE OWNER IS PURSUING ADDITIONAL GRANT FUNDING. SHOULD THE OWNER BE SUCCESSFUL, THE CONTRACT SHALL BE SUBJECT TO STATE PREVAILING WAGE REQUIREMENTS. SEE SPECIFICATION APPENDICES FOR REQUIREMENTS.

EARTHWORK SUMMARY - GEOSYNTHETIC LINER SYSTEM

NOTES	ITEMS	UNIT	PROJECT TOTAL
UNADJUSTED VOLUMES BASED ON CROSS SECTIONS			
1	TOPSOIL STRIPPING (EV)	CU YD	1400
1, 2	EXCAVATION (EV)	CU YD	50625
5	EMBANKMENT MATERIAL REQUIRED (CV)	CU YD	40
3	TOPSOIL REQUIRED (CV)	CU YD	900
	EMBANKMENT MATERIAL FOR SOIL COVER	CU YD	1475
EARTHWORK BALANCE			
TOPSOIL BALANCE			
8	TOPSOIL REQUIRED (LV)	CU YD	1350
4	TOPSOIL GENERATED (LV)	CU YD	1470
	EXCESS TOPSOIL (LV)	CU YD	120
GRADING MATERIAL BALANCE			
4	GRADING MATERIAL GENERATED (LV)	CU YD	53156
	EMBANKMENT MATERIAL REQUIRED (LV)	CU YD	48
9	EXCESS GRADING MATERIAL (LV)	CU YD	53108
EARTHWORK QUANTITIES			
7	COMMON EXCAVATION (EV)	CU YD	52025
6	COMMON EMBANKMENT (CV)	CU YD	2415

- EARTHWORK SUMMARY TABLE NOTES:
- PAID FOR AS EXCAVATION - COMMON. TOPSOIL EXCAVATION DEPTHS ARE BASED ON SOIL BORINGS (SEE PROJECT MANUAL)
 - EXCAVATION BELOW TOPSOIL STRIPPING TO PROPOSED SUBRADE (BOTTOM OF PROPOSED TOPSOIL, BOTTOM OF LINER.)
 - 4" OF TOPSOIL ACROSS ALL DISTURBED AREAS ABOVE THE NORMAL WATER LEVEL.
 - ASSUMES 1.05 SWELL FACTOR.
 - PLACEMENT OF EMBANKMENT ANTICIPATING HOLD-DOWN FOR TOPSOIL PLACEMENT.
 - INCLUDES TOPSOIL PLACEMENT & 1' SOIL COVER OVER POND LINER PLACEMENT
 - INCLUDES TOPSOIL EXCAVATION AND REGULAR EXCAVATION
 - ASSUMES 1.5 SHRINKAGE FACTOR
 - ONCE THE SITE IS BALANCED, ALL EXCESS EXCAVATED MATERIAL WILL BECOME PROPERTY OF THE CONTRACTOR AND WILL BE REMOVED FROM THE SITE AND DISPOSED OF AT A SITE SECURED BY THE CONTRACTOR

EARTHWORK SUMMARY - CLAY LINER SYSTEM

NOTES	ITEMS	UNIT	PROJECT TOTAL
UNADJUSTED VOLUMES BASED ON CROSS SECTIONS			
1	TOPSOIL STRIPPING (EV)	CU YD	1400
1, 2	EXCAVATION (EV)	CU YD	52105
5	EMBANKMENT MATERIAL REQUIRED (CV)	CU YD	40
3	TOPSOIL REQUIRED (CV)	CU YD	900
EARTHWORK BALANCE			
TOPSOIL BALANCE			
8	TOPSOIL REQUIRED (LV)	CU YD	1350
4	TOPSOIL GENERATED (LV)	CU YD	1470
	EXCESS TOPSOIL (LV)	CU YD	120
GRADING MATERIAL BALANCE			
4	GRADING MATERIAL GENERATED (LV)	CU YD	54710
	EMBANKMENT MATERIAL REQUIRED (LV)	CU YD	48
9	EXCESS GRADING MATERIAL (LV)	CU YD	54662
EARTHWORK QUANTITIES			
	CLAY LINER (CV)	CU YD	2950
7	COMMON EXCAVATION (EV)	CU YD	53500
6	COMMON EMBANKMENT (CV)	CU YD	940

- EARTHWORK SUMMARY TABLE NOTES:
- PAID FOR AS EXCAVATION - COMMON. TOPSOIL EXCAVATION DEPTHS ARE BASED ON SOIL BORINGS (SEE PROJECT MANUAL)
 - EXCAVATION BELOW TOPSOIL STRIPPING TO PROPOSED SUBRADE (BOTTOM OF PROPOSED TOPSOIL, BOTTOM OF CLAY LINER.)
 - 4" OF TOPSOIL ACROSS ALL DISTURBED AREAS ABOVE THE NORMAL WATER LEVEL.
 - ASSUMES 1.05 SWELL FACTOR.
 - PLACEMENT OF EMBANKMENT ANTICIPATING HOLD-DOWN FOR TOPSOIL PLACEMENT.
 - INCLUDES TOPSOIL PLACEMENT
 - INCLUDES TOPSOIL EXCAVATION AND REGULAR EXCAVATION
 - ASSUMES 1.5 SHRINKAGE FACTOR
 - ONCE THE SITE IS BALANCED, ALL EXCESS EXCAVATED MATERIAL WILL BECOME PROPERTY OF THE CONTRACTOR AND WILL BE REMOVED FROM THE SITE AND DISPOSED OF AT A SITE SECURED BY THE CONTRACTOR

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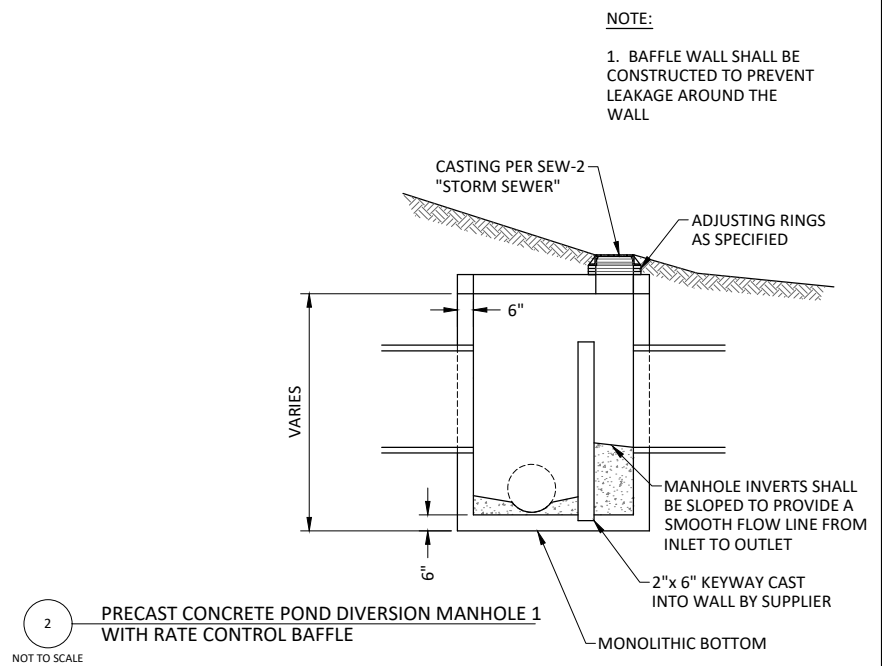
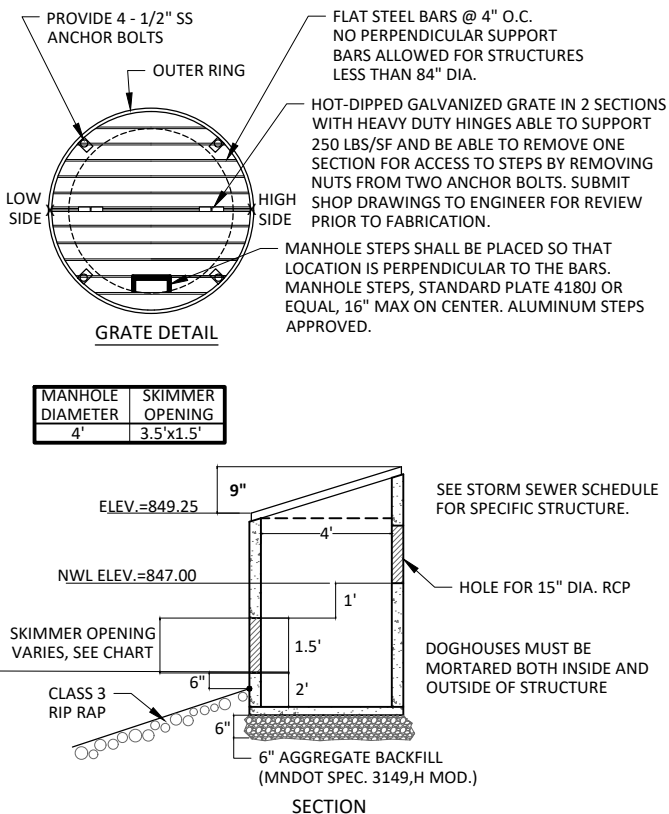
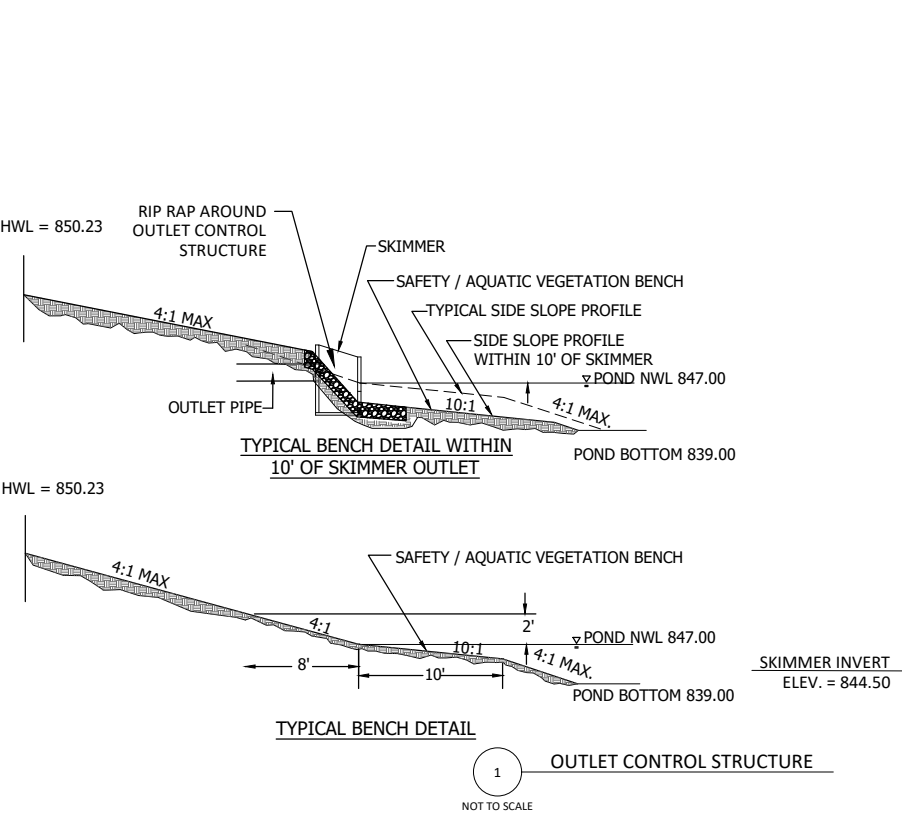
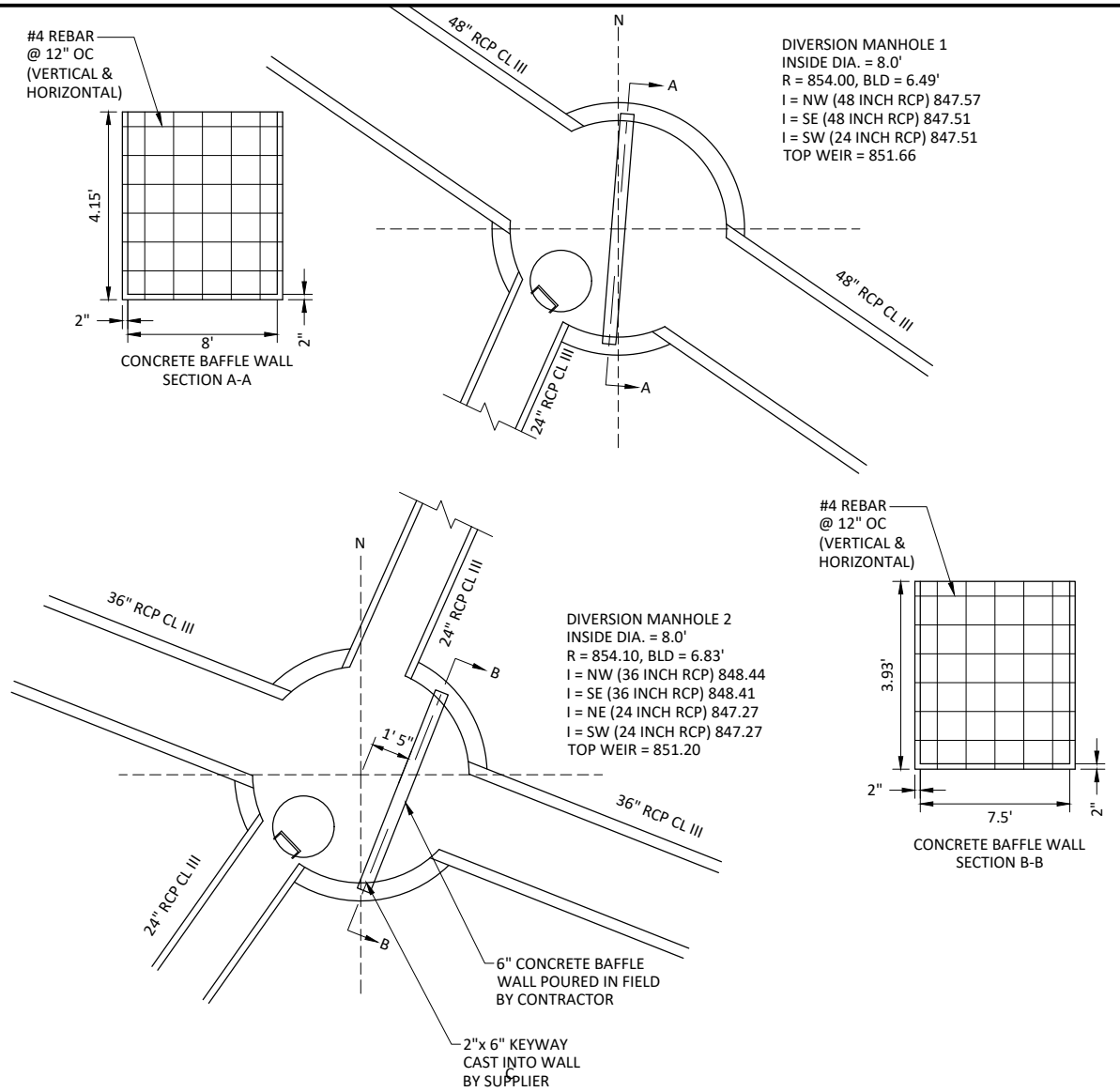
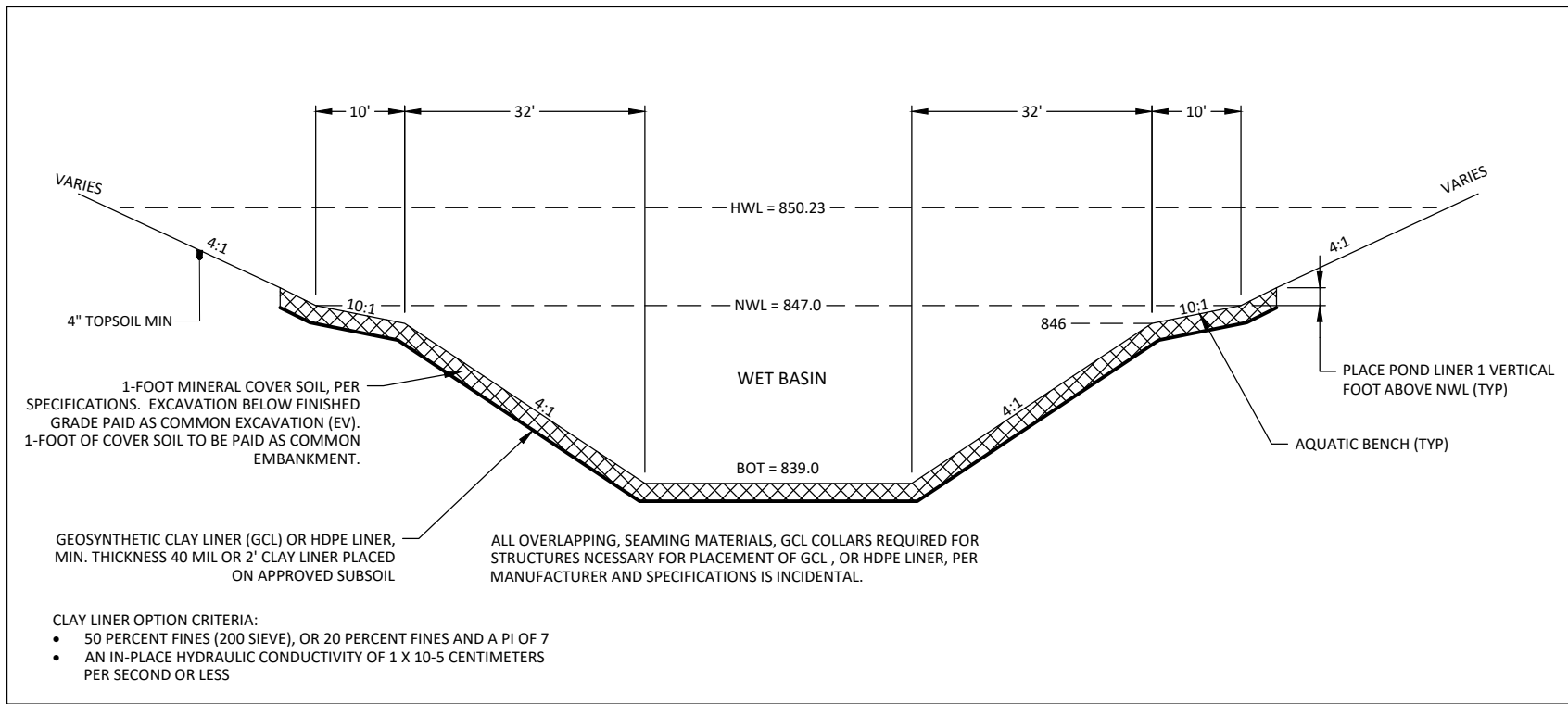


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CITY OF RAMSEY, MINNESOTA
 RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
 STATEMENT OF ESTIMATED QUANTITIES



NOTE:

1. BAFFLE WALL SHALL BE CONSTRUCTED TO PREVENT LEAKAGE AROUND THE WALL

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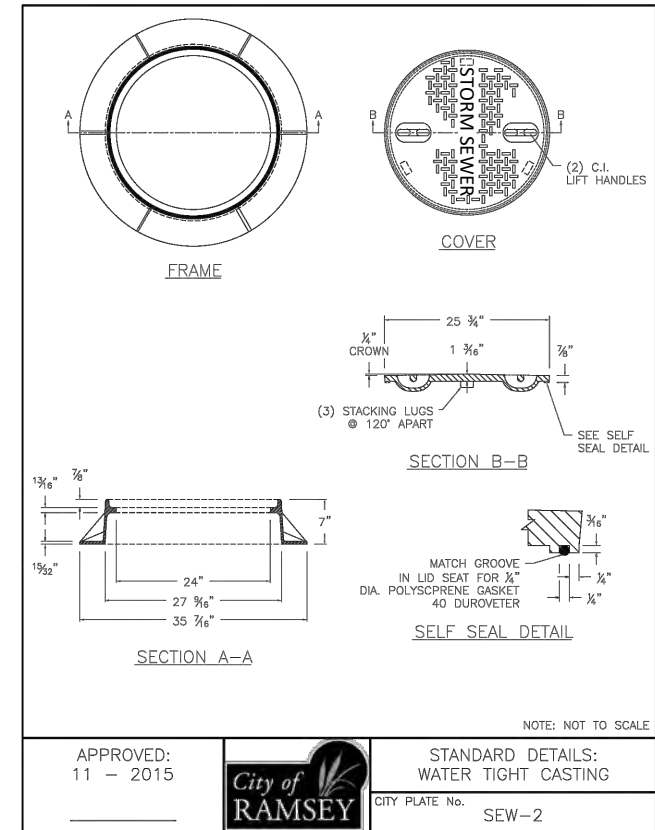
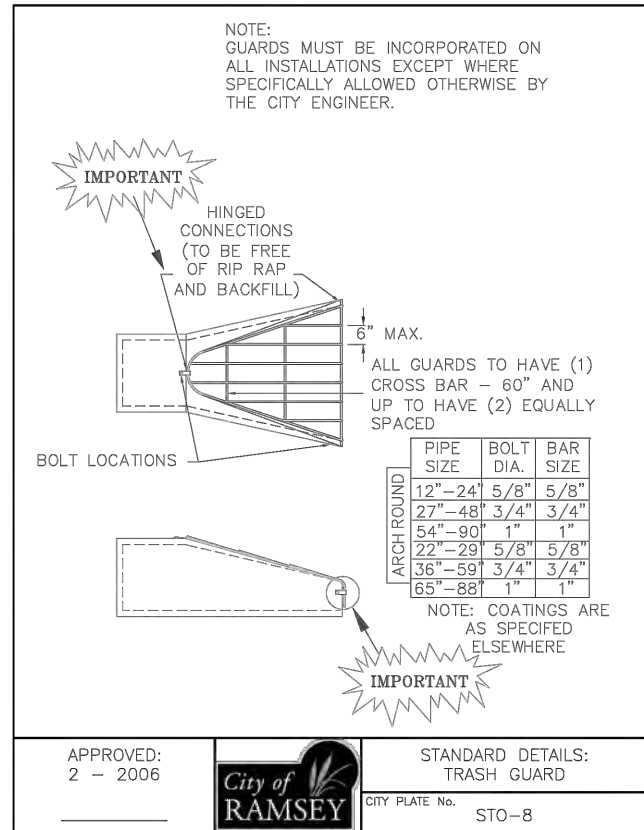
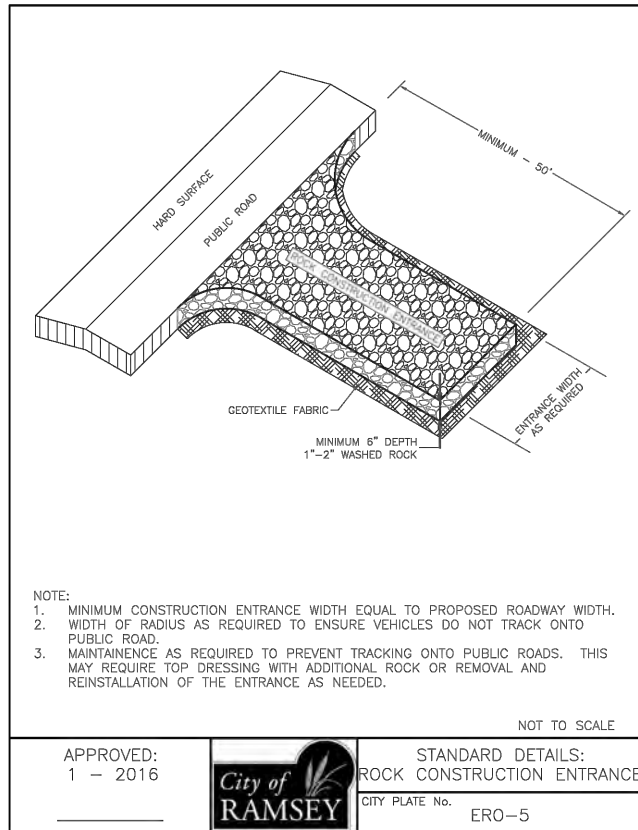
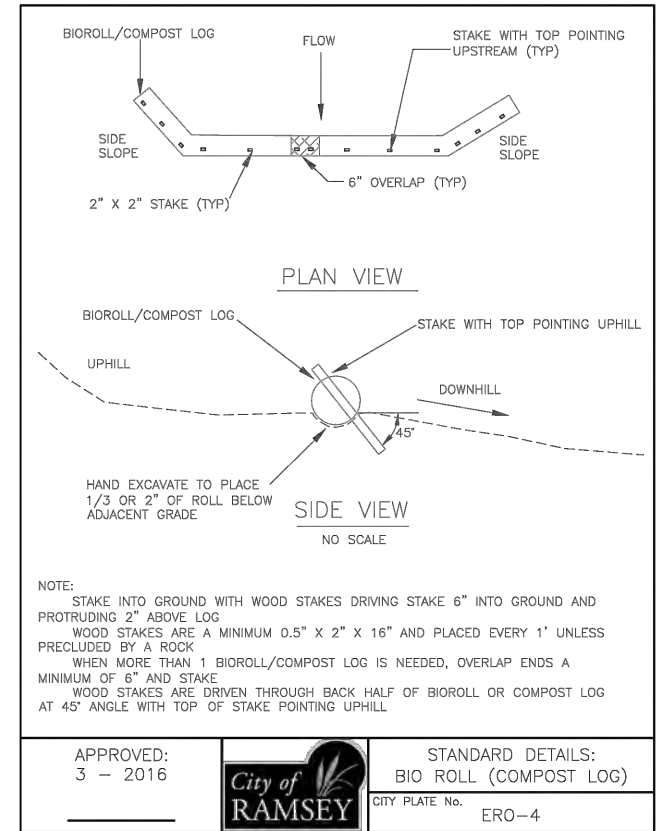
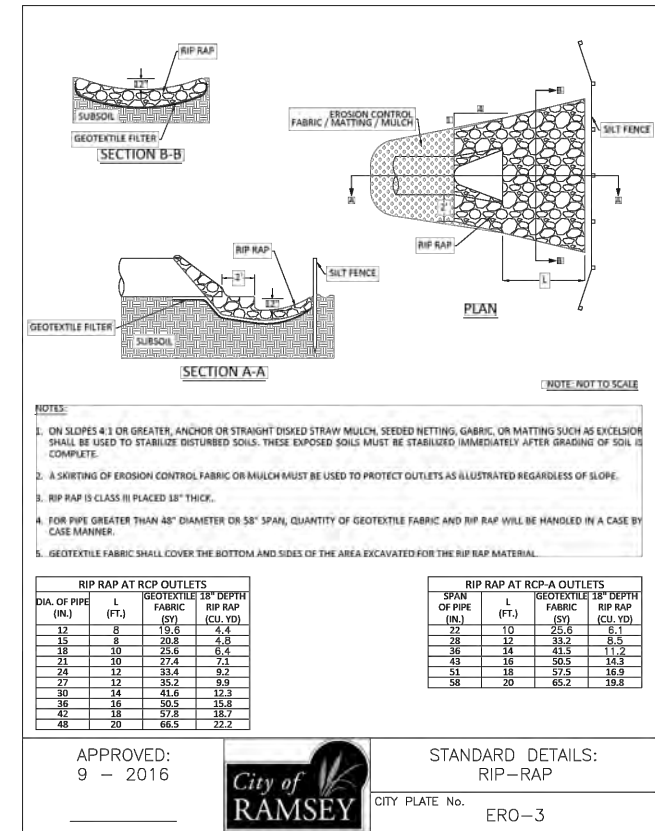
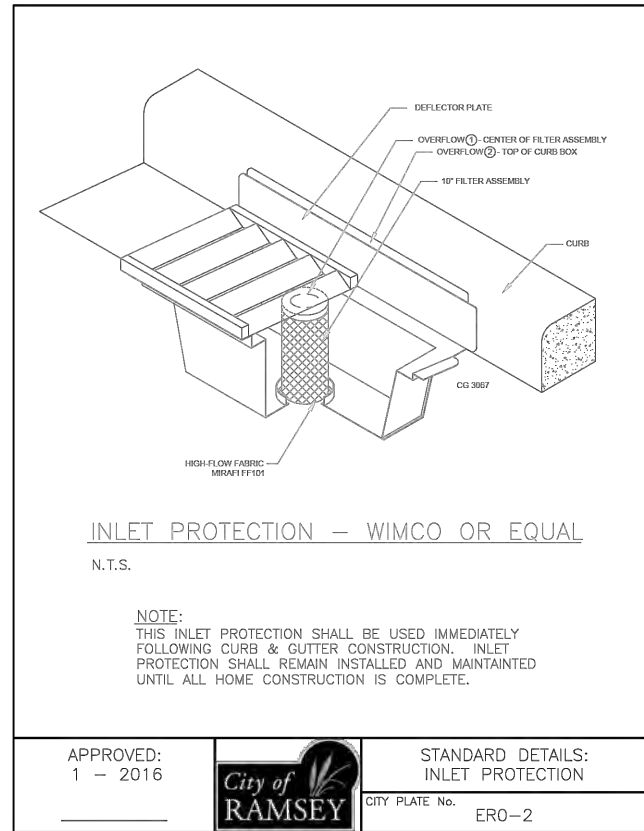
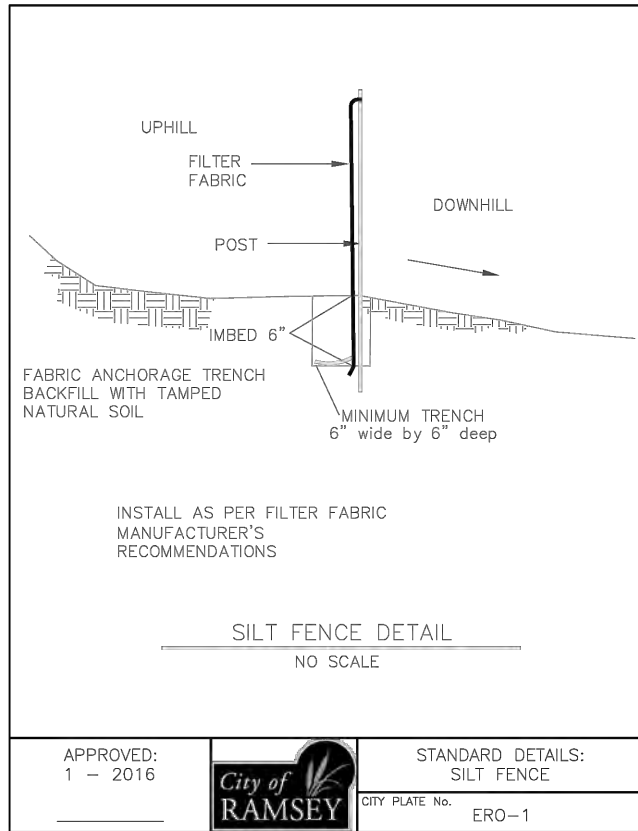
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CITY OF RAMSEY, MINNESOTA
RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
POND TYPICAL SECTION

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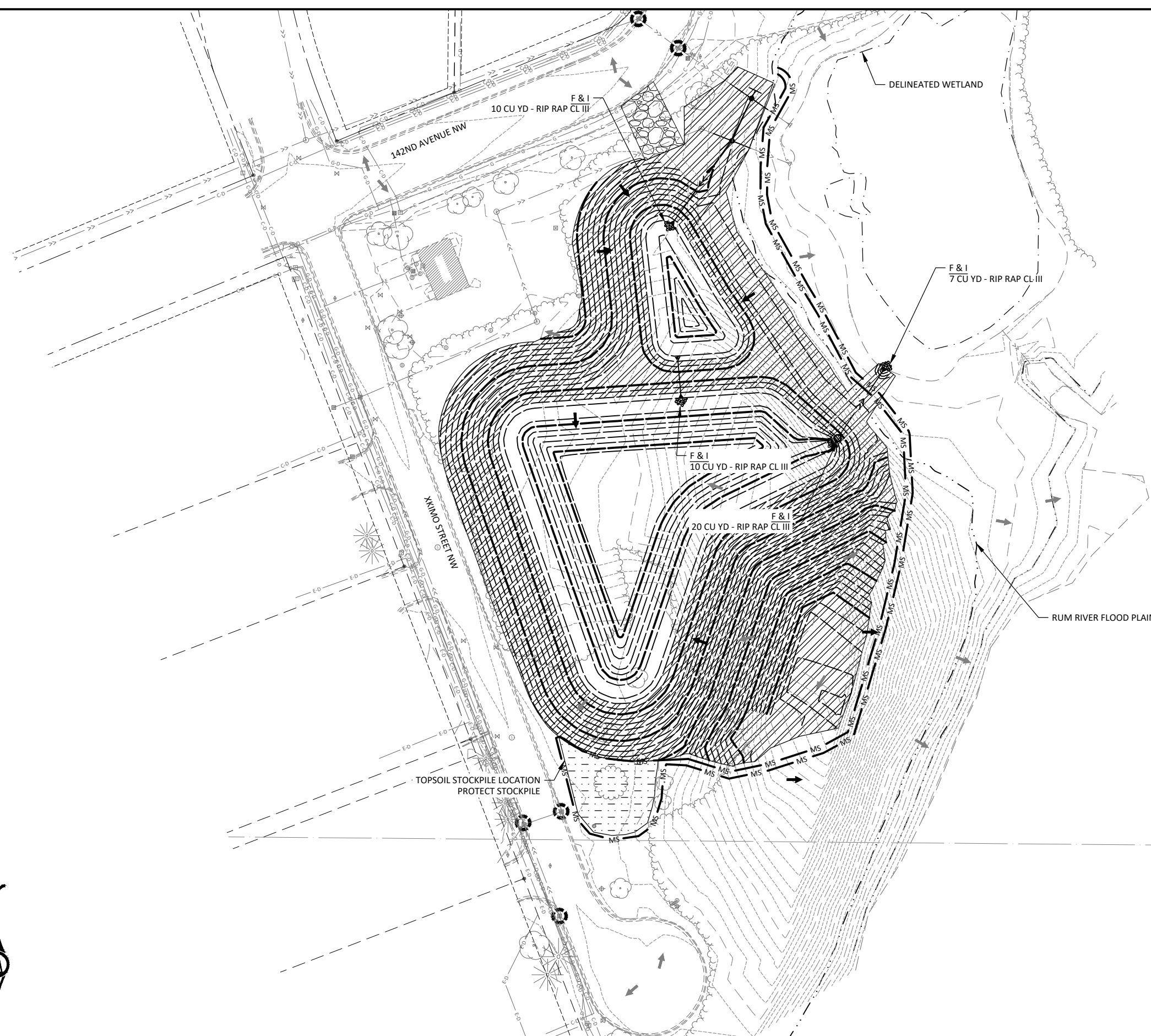


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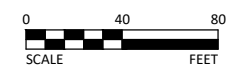
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RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
DETAILS



EROSION CONTROL LEGEND

	STORM DRAIN INLET PROTECTION
	SILT FENCE - MACHINE SLICED
	RIP RAP CLASS III
	PROPOSED DRAINAGE FLOW
	EXISTING DRAINAGE FLOW
	ROLLED EROSION PREVENTION CATEGORY 20 AND SEED MIX 35-241
	HYDRAULIC BONDED FIBER MATRIX
	STABILIZED CONSTRUCTION EXIT

- NOTES:**
1. ALL CATCH BASINS AND INLETS, BOTH EXISTING AND PROPOSED, SHALL HAVE INLET PROTECTION INSTALLED PRIOR TO DISTURBANCE AND REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETED.
 2. CONSTRUCT STABILIZED CONSTRUCTION EXITS AS NECESSARY THROUGHOUT THE PROJECT.
 3. QUANTITY FOR INLET PROTECTION IS BASED ON INLET PROTECTION NEEDED FOR EACH STRUCTURE DURING THE COURSE OF THE PROJECT. ONCE STARTED, STORM SEWER AND GRADING ACTIVITIES SHALL BE EXECUTED DILIGENTLY AND CONTINUOUSLY UNTIL COMPLETE.
 4. PLACE 4" OF TOPSOIL IN OFF STREET GREEN SPACE.
 5. THE CONTRACTOR SHALL USE ONSITE TOPSOIL STOCKPILE FOR TOPSOIL INSTALLATION. NO TOPSOIL SHALL BE IMPORTED WITHOUT APPROVAL FROM THE ENGINEER.
 6. STABILIZE TOPSOIL STOCKPILE WITH HYDRAULIC BONDED FIBER MATRIX AT THE DIRECTION OF THE ENGINEER IN THE FIELD.



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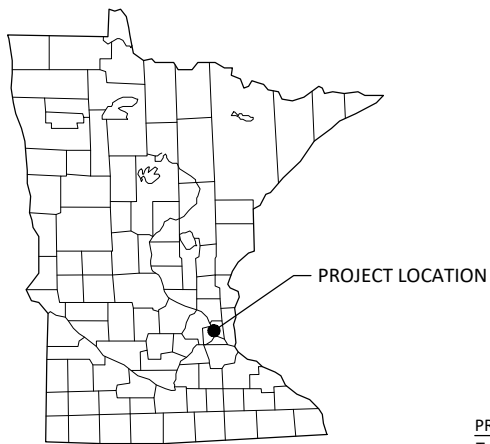
RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS

EROSION CONTROL & TURF ESTABLISHMENT

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STORMWATER POLLUTION PREVENTION PLAN (SWPPP)

Rivers Bend Pond -2024
CITY OF Ramsey
Anoka COUNTY, MINNESOTA



LEGEND

- PROJECT LOCATION
- 1-MILE BOUNDARY
- IMPAIRED, SPECIAL OR PROTECTED WATERS
- NATIONAL WETLANDS INVENTORY
- RECEIVING WATERS

PROJECT AREAS:

Total Project Size (disturbed area) =	2.8	ACRES
Existing area of impervious surface =	0	ACRES
Post construction area of impervious surface =	0	ACRES
Total new impervious surface area created =	0	ACRES

Planned Construction Start Date: 9/15/2024
Estimated Construction Completion Date: 11/15/2024

PERMANENT STORMWATER MANAGEMENT SYSTEM:

Type of storm water management used if more than 1 acre of new impervious surface is created:

1	Wet Sedimentation Basin
	Infiltration/Filtration
	Regional Pond
	Permanent Stormwater Management Not Required

PROJECT LOCATION:

COUNTY	TOWNSHIP	RANGE	SECTION	LATITUDE	LONGITUDE
ANOKA	T32N	R25W	25	45.22821°	-93.39473°

BMP SUMMARY	QUANTITY	UNIT
STORM DRAIN INLET PROTECTION	5	EACH
ROLLED EROSION PREVENTION CATEGORY 20	8,500	SQ YD
SEEDING MIXTURE 35-241	70	POUND
SILT FENCE MACHINE SLICED	1,800	LIN FT
RIP RAP CLASS III	50	CU YD
STABILIZED CONSTRUCTION EXIT	1	EACH

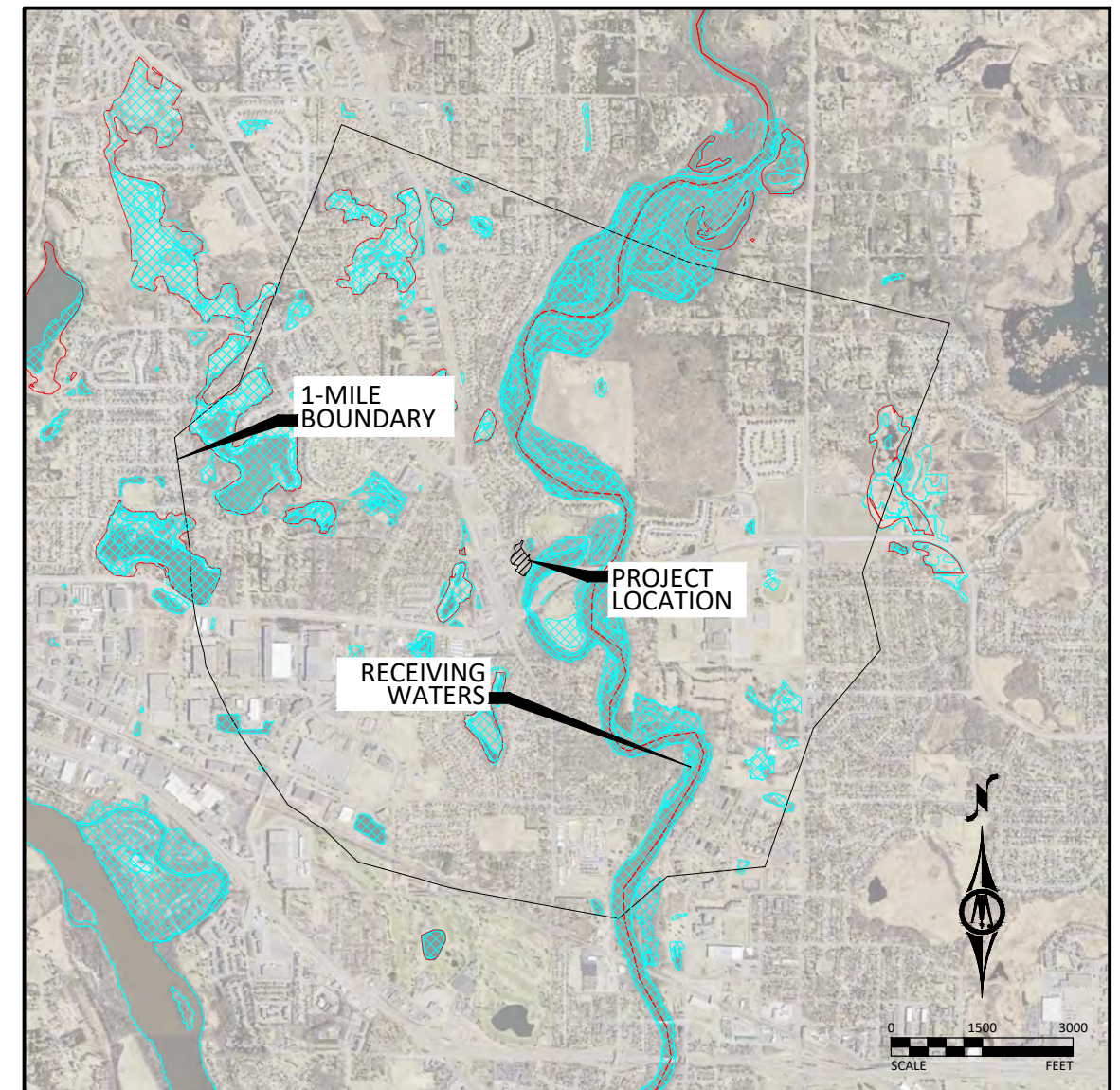
DESCRIPTION OF CONSTRUCTION ACTIVITIES AND STORMWATER MANAGEMENT:

Construction activities include: Site grading, sanitary sewer and water main extensions, temporary erosion and sediment control, and permanent stabilization.

Storm water currently from the urbanized and undeveloped area is currently being routed to catch basins located along 142nd Ave NW and Xkimo St. NW and flow through storm drains into an existing pond located within Rivers Bend park. The existing pond is located south of 142nd Ave NW and Waco st. NW intersection. The outlet of the pond is Rum River. The current pond size is in adequate to treat the current impervious area.

After construction is complete storm water will be managed in a different pond area. The water will be routed through the catch basins located on 142nd Ave. NW and Xkimo ST. NW and then routed through storm drains to the new biofiltration pond. First entering a wet sedimentation basin with a capacity of 74,000 cubic feet, 5 feet deep, then entering the biofiltration basin. The pond will have a treatment capacity of 148,795 cubic feet.

This project includes the following stormwater management BMPs: Biofiltration and wet sedimentation.



RECEIVING WATERS: Receiving waters, including surface water, wetlands, Public Waters, and stormwater ponds, within 1-mile of the project boundary are identified on the USGS 7.5 min quad map above. Receiving waters that are impaired, the impairment, and WLA are listed as follows. All specific BMPs relative to construction activities listed in the permit for special, prohibited, restricted, or impaired have been incorporated into this plan. All specific BMPs listed in approved TMDLs and those BMPs listed for construction related waste load allocations have also been incorporated.

NAME OF WATER BODY	TYPE (ditch, pond, wetland, lake, etc.)	Special, Prohibited, Restricted Water ¹	Flows to Impaired Water Within 1-Mile ²	USEPA Approved Construction Related TMDL ³
Rum River	River	Nutrients	Yes	NaN

¹ Special, prohibited, and restricted waters are listed in Section 23 of the MN Construction Stormwater General Permit (MNR100001).
² Identified as impaired under section 303 (d) of the federal Clean Water Act for phosphorus, turbidity, TSS, dissolved oxygen, and/or aquatic biota.
³ Construction Related TMDLs include those related to: phosphorus, turbidity, TSS, dissolved oxygen, and/or aquatic biota.

IMPLEMENTATION SCHEDULE AND PHASING: The Contractor is required to provide an updated schedule and site management plan meeting the minimum requirements of Section 1717 of the Minnesota Standard Specifications for Construction.

- 1) Submit SWPPP Updates to Engineer. Submittal shall include any requested changes to the SWPPP, including but not limited to: Trained Personnel, Locations for Stockpiles, Concrete Washout, Sanitation Facilities, Types and Locations of Erosion & Sediment Control. Failure to submit updates shall be considered acceptance of the SWPPP as designed with no changes.
- 2) Install perimeter sediment control, inlet protection, and construction exit.
- 3) Complete Site Grading.
- 4) Add additional temporary BMPs as necessary during construction based on inspection reports.
- 5) Ensure final stabilization measures are complete.
- 6) Provide digital copy of all Field SWPPP Documentation including Inspection Reports and SWPPP Revisions to the Owner.
- 7) Submit Notice of Termination (NOT) to MPCA. NOTE: The NOT must be submitted to MPCA before Final Stabilization is considered complete.

RESPONSIBLE PARTIES:

The Contractor and Owner will be joint applicants under the MPCA's General Stormwater Permit for Construction Activity as required by the National Pollutant Discharge Elimination System (NPDES) Phase II program.

The Contractor shall provide one or more trained Construction SWPPP Manager(s) knowledgeable and experienced in the application of erosion prevention and sediment control BMPs that will oversee the implementation of the SWPPP, and the installation, inspection and maintenance of the erosion prevention and sediment control BMPs.

A Construction SWPPP Manager must be available for an on-site inspection within 72 hours upon request by the MPCA.

	COMPANY	CONTACT PERSON	PHONE
OWNER:	City of Ramsey	Bruce Westby, PE	763-433-9825
SWPPP DESIGNER:	Bolton & Menk, Inc.	Timothy Haugo	763-433-2851
CONTRACTOR:	TBD	TBD	TBD
CONSTRUCTION SWPPP MANAGER:	TBD	TBD	TBD
PARTY RESPONSIBLE FOR LONG TERM O&M:	City of Ramsey	Bruce Westby, PE	763-433-9825

The SWPPP Designer, Construction SWPPP Manager, and BMP Installer must have appropriate training. Documentation showing training commensurate with the job duties and responsibilities is required to be included in the SWPPP prior to any work beginning on the site. Training documentation for the SWPPP Designer is included on the Narrative sheet. The Contractor shall attach training documentation to this SWPPP for the Construction SWPPP Manager and BMP Installer prior to the start of construction. This information shall be kept up to date until the project NOT is filed.

ADDITIONAL COMPENSATION

Payment for all work associated with Erosion and Sediment Control shall be as described in the Project Manual. Unless otherwise authorized by the Owner no additional payment shall be made for any work required to administer and maintain the site erosion and sediment control in compliance with the Minnesota Pollution Control Agency (MPCA) - General Stormwater Permit for Construction Activity (MN R100001) including but not limited to inspection, maintenance, and removal of BMPs or addition of BMPs to accommodate Contractor phasing.

DOCUMENT RETENTION

Permittees must make the SWPPP, including all inspection reports, maintenance records, training records and other information required by this permit, available to federal, state, and local officials within three (3) days upon request for the duration of the permit and for three (3) years following the NOT.

GENERAL STORMWATER DISCHARGE REQUIREMENTS

All requirements listed in Section 5.1 of the Permit for the design of the permanent stormwater management system and discharge have been included in the preparation of this SWPPP. These include but are not limited to:

1. The expected amount, frequency, intensity, and duration of precipitation.
2. The nature of stormwater runoff and run-on at the site
3. Peak flow rates and stormwater volumes to minimize erosion at outlets and downstream channel and stream bank erosion.
4. The range of soil particle sizes expected to be present on the site.

Permanent stormwater treatment systems for this project have been designed in accordance with the guidance in the MN Stormwater Manual in place at the time of bidding. Copies of the design information and calculations are part of this SWPPP and will be provided in digital format upon written request to the Engineer.

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Kevin P. Kiehl
KEVIN P. KIELB
LIC. NO. 23211 DATE 8/20/2024



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TJH			
KPK			
CLIENT PROJ. NO.	CP-25-01		

CITY OF RAMSEY, MINNESOTA
RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
STORM WATER POLLUTION PREVENTION PLAN

SHEET
8
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12

Information contained in this SWPPP narrative sheet summarizes requirements of the GENERAL PERMIT AUTHORIZATION TO DISCHARGE STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM/STATE DISPOSAL SYSTEM PROGRAM - Permit No: MN R100001 (Permit) as they apply to this project. All provisions of the Permit including those not specifically cited herein shall apply to this project. The Contractor is responsible to be familiar with and comply with all conditions of the permit. The full text of the Permit is available at: <https://www.pca.state.mn.us/sites/default/files/wq-strm2-80a.pdf>

SWPPP AMENDMENTS AND SUBMITTALS

Contractor must prepare and submit to the Engineer a SWPPP amendment as necessary to include additional Best Management Practices (BMPs) to correct problems identified or address the following situations.

1. Contact information and training documentation for Construction SWPPP Manager and BMP Installer,
2. There is a change in construction method of phasing, operation, maintenance, weather or seasonal conditions not anticipated during the design of the SWPPP including but not limited to:
 - a. Types and/or Locations of BMPs
 - b. Material Storage and Spill Response
 - c. Fueling Plans
 - d. Locations for Stockpiles, Concrete Washout, and Sanitation Facilities and
 - e. Project Phasing
3. It is determined that the SWPPP is not achieving objectives of minimizing pollutants in stormwater discharges associated with construction activity, or
4. The SWPPP is not consistent with the terms and conditions of the permit.

The Contractor may implement SWPPP amendments immediately and is not required to wait for Engineer review of the submittal. The responsibility for completeness of SWPPP amendments and compliance with the Permit lies with the Contractor. Review, comment, or lack of comment by the Engineer on a SWPPP amendment shall not absolve the responsibilities of the Contractor in any way.

If a change order is issued for a design change the SWPPP amendment will be prepared by the Engineer and included in the change order.

In addition to SWPPP amendments, the Contractor shall submit to the Engineer Weekly Erosion and Sediment Control Schedule meeting the requirements of MnDOT 1717.

The Contractor shall keep copies of all SWPPP amendments, Weekly Erosion and Sediment Control Schedules, inspection logs, and maintenance logs with the field copy of the SWPPP. A PDF copy of these documents will be provided along with a copy of the final Field Copy of the SWPPP to the Engineer along with the signed Notice of Termination when final stabilization is complete.

EROSION PREVENTION PRACTICES

Stormwater conveyance channels shall be routed around unstabilized areas. Erosion controls and velocity dissipation devices shall be used at outlets within and along the length of any constructed conveyance channel.

The normal wetted perimeter of all ditches or swales, including storm water management pond slopes, that drain waters from the site must be stabilized within 200' of any property edge or discharge point, including storm sewer inlets, within 24 hours of connection.

Temporary or permanent ditches or swales used as sediment containment during construction do not need to be stabilized during temporary period of use and shall be stabilized within 24 hours after no longer used as sediment containment.

Mulch, hydromulch, tackifier, or similar practice shall not be used in any portion of the wetted perimeter of a temporary or permanent drainage ditch or swale section with a continuous slope of greater than 2 percent.

Energy dissipation shall be installed at all temporary or permanent pipe outlets within 24 hours of connection to a surface water or permanent stormwater treatment system.

The Contractor shall phase construction and use construction methods to the extent practical to minimize exposed soils. The project phasing shall be documented in the Weekly Erosion and Sediment Control Schedule.

SEDIMENT CONTROL PRACTICES

Down gradient BMPs including perimeter BMPs must be in place before up gradient land- disturbing activities begin and shall remain in place until final stabilization.

All BMPs that have been adjusted or removed to accommodate short-term activities shall be re-installed or replaced the earlier of the end of the work day or before the next precipitation event even if the activity is not complete.

Inlet BMPs may be removed for specific safety concerns. The BMPs shall be replaced as soon as the safety concern is resolved. The removal shall be documented in the SWPPP as a SWPPP amendment.

Temporary stockpiles must have sediment control BMPs. The Contractor shall prepare and submit to the Engineer a SWPPP amendment showing the location of temporary stockpiles and the BMPs for each stockpile. The SWPPP amendment must meet the minimum requirements of Section 9 of the Permit.

Soil compaction shall be minimized and topsoil shall be preserved, unless infeasible or if construction activities dictate soil compaction or topsoil stripping.

The use of polymers, flocculants, or other sedimentation treatment chemicals are not proposed as part of this SWPPP as designed by the Engineer. If methods or phasing of construction require the use of any of these chemicals, the Contractor shall prepare and submit to the Engineer a SWPPP amendment that meets the minimum requirements of Section 9 of the Permit.

TEMPORARY SEDIMENTATION BASINS

A temporary sedimentation basin has not been included in this SWPPP as designed by the Engineer. If a basin is later determined to be desirable or necessary the Contractor shall prepare and submit to the Engineer a SWPPP amendment. Temporary sedimentation basins shall meet or exceed the minimum requirements of Section 14 of the Permit and shall include a basin draining plan meeting or exceeding the minimum requirements of Section 10 of the Permit. Where the site discharges to Special and/or Impaired Waters the SWPPP amendment shall also meet or exceed the minimum requirements of Section 23 of the permit.

DEWATERING

A dewatering plan has not been included in this SWPPP as designed by the Engineer. If dewatering is required for this project, the Contractor shall prepare and submit to the Engineer a SWPPP amendment. All dewatering shall meet or exceed the minimum requirements of Section 10 of the Permit.

POLLUTION PREVENTION

Products and materials that have the potential to leach pollutants that are stored on the site must be stored in a manner designed to minimize contact with stormwater. Materials that are not a source of potential contamination to stormwater or that are designed for exposure to stormwater are not required to be covered.

Hazardous materials including but not limited to pesticides, fertilizer, petroleum products, curing compounds and toxic waste must be properly stored and protected from stormwater exposure as recommended by the manufacturer in an access restricted area.

Solid waste must be stored, collected and disposed of in compliance with Minnesota Administrative Rules Chapter 7035.

Portable toilets must be positioned so that they are secure and will not be tipped or knocked over. Sanitary waste must be disposed of properly in accordance with Minn. R. CH 7041.

Exterior vehicle or equipment washing on the project site shall be limited to a defined area of the site. No engine degreasing is allowed on site. A sign must be installed adjacent to each washout facility that requires site personnel to utilize the proper facilities for disposal of concrete and other washout wastes.

The Contractor shall prepare and submit a SWPPP amendment detailing the location and BMPs proposed for storage of materials, solid waste, portable toilets, and exterior vehicle or equipment washing on the site. The SWPPP amendment shall include a spill prevention and response plan that is appropriate for the materials proposed to be on the site. The SWPPP amendment shall meet or exceed the minimum requirements of Section 12 of the Permit.

INSPECTION & MAINTENANCE

A trained person shall routinely inspect the entire construction site at the time interval indicated on this sheet of the SWPPP during active construction and within 24-hours after a rainfall event greater than 0.5 inches in 24 hours. Following an inspection that occurs within 24-hours after a rainfall event, the next inspection must be conducted at the time interval indicated in the Receiving Waters Table found on the SITE PLAN AND INFORMATION SHEET of the SWPPP.

All inspections and maintenance conducted during construction must be recorded on the day it is completed and must be retained with the SWPPP. Inspection report forms are available in the Project Specifications. Inspection report forms other than those provided shall be approved by the engineer.

The Contractor may request a change in inspection schedule for the following conditions:

- a. Inspections of areas with permanent cover to be reduced to once per month,
- b. Inspections of areas that have permanent cover and have had no construction activity for 12 months to be suspended until construction resumes,
- c. Inspections of areas where construction is suspended due to frozen ground conditions, inspections to be suspended until the earlier of within 24 hours of runoff occurring, or upon resuming construction.

No change in inspection schedule shall occur until authorized by the Engineer.

Inspections must include:

1. All erosion prevention and sediment control BMPs and Pollution Prevention Management Measures to ensure integrity and effectiveness.
2. Surface waters, including drainage ditches and conveyance systems for evidence of erosion and sediment deposition.
3. Construction site vehicle exit locations, streets and curb and gutter systems within and adjacent to the project for sedimentation from erosion or tracked sediment from vehicles.
4. Infiltration areas to ensure that no sediment from ongoing construction activity is reaching the infiltration area and that equipment is not being driven across the infiltration area.

All non-functioning BMPs and those BMPs where sediment reaches one-half (1/2) of the depth of the BMP, or in the case of sediment basins one-half (1/2) of the storage volume, must be repaired, replaced, or supplemented by the end of the next business day after discovery, or as soon as field conditions allow.

Permittees must repair, replace or supplement all nonfunctional BMPs with functional BMPs by the end of the next business day after discovery, or as soon as field conditions allow.

Any sediment that escapes the site must be removed and the area stabilized within 7 calendar days of discovery unless precluded by legal, regulatory, or physical access in which case the work shall be completed within 7 calendar days of authorization. Paved surfaces such as streets shall have any escaped or tracked sediment removed by the end of the day that it is discovered. Sediment release, other than paved surfaces that can be cleaned up with street sweeping shall be reported immediately upon discovery to the Engineer.

PUBLIC WATER RESTRICTIONS:

For public waters that have been promulgated "work in water restrictions" during fish spawning time frames, all exposed soil areas that are within 200 feet of the water's edge, and drain to these waters must complete stabilization within 24-hours during the time period. MN DNR permits are not valid for work in waters that are designated as infested waters unless accompanied by an Infested Waters Permit or written notification has been obtained from MN DNR stating that such permit is not required. There is no exception for pre-existing permits. If a MN DNR Permit has been issued for the project and the water is later designated as infested, the Contractor shall halt all work covered by the MN DNR Permit until an Infested Waters Permit is obtained or that written notification is obtained stating that such permit is not required.

FINAL STABILIZATION

Final Stabilization is not complete until all the following requirements have been met:

1. Substantial Completion has been reached and no ground disturbing activities are anticipated.
2. Permanent cover has been installed with an established minimum uniform perennial vegetation density of 70 percent of its expected final growth. Vegetation is not required in areas where no vegetation is proposed by this project such as impervious surfaces or the base of a sand filter.

3. Accumulated sediment has been removed from all permanent stormwater treatment systems as necessary to ensure the system is operating as designed.
4. All sediment has been removed from conveyance systems
5. All temporary synthetic erosion prevention and sediment control BMPs have been removed. BMPs designated on the SWPPP to remain to decompose on-site may remain.
6. For residential construction only, permit coverage terminates on individual lots if the structures are finished and temporary erosion prevention and downgradient perimeter control is complete, the residence sells to the homeowner, and the permittee distributes the MPCA's "Homeowner Fact Sheet" to the homeowner.
7. For agricultural land only (e.g., pipelines across cropland), the disturbed land must be returned to its preconstruction agricultural use prior to submitting the NOT.

SITE STABILIZATION COMPLETION:

Stabilization of exposed soils shall begin immediately and shall be completed after the construction activity has temporarily or permanently ceased no later than:	7 calendar days
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SITE INSPECTION INTERVAL:

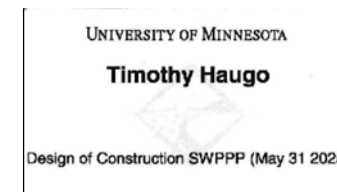
A trained person shall routinely inspect the entire construction site during active construction at an interval of no more than:	7 calendar days
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SPECIAL ENVIRONMENTAL CONSIDERATIONS AND PERMITS:

1) Was an environmental review required for this project or any part of a common plan of development or sale that includes all or any portion of this project?	NO
2) Does any portion of the site have the potential to affect threatened or endangered species or their critical habitat?	NO
3) Does any portion of this site discharge to a Calcareous fen.	NO
4) Will any portion of the site potentially affect properties listed on the National Register of Historic Places or a known or discovered archeological site?	NO
5) Have any Karst features have been identified in the project vicinity?	NO
6) Is compliance with temporary or permanent stormwater management design requirements infeasible for this project?	NO
7) Has the MN DNR promulgated "work in water restrictions" for any Public Water this site discharges to during fish spawning?	NO

TYPE OF PERMIT	PERMITTING AGENCY	PERMIT STATUS AND CONDITIONS
Construction Stormwater NPDES	MPCA	Contractor Obtained
Watershed	LRRWMO	In Progress
Dewatering	MnDNR	Contractor Obtained

SWPPP DESIGNER TRAINING DOCUMENTATION:



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 KEVIN P. KIELB
 LIC. NO. 23211 DATE 8/20/2024



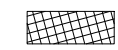


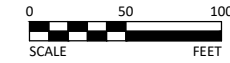
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LEGEND

-  PROJECT BOUNDARY
-  SOIL TYPE
-  STEEP SLOPES (>33.3%)



SOIL TYPE SUMMARY

Map Unit Symbol	Soil Name	Hyd. Soil Group
Ba	Becker	B
DnA	Dickman	A
NyC	Nymore	A

- NHEL - Not Highly Erodible Land
- PHEL - Potentially Highly Erodible Land
- HEL - Highly Erodible Land

LOCATION OF SWPPP REQUIREMENTS IN PROJECT PLAN

DESCRIPTION	SHEET NO.
SITE MAP	8
DIRECTION OF FLOW	8
FINAL STABILIZATION	8
SOILS	10
DRAINAGE STRUCTURES	11
STORM SEWER PLAN & PROFILE SHEETS	12
EROSION & SEDIMENT CONTROL DETAILS	8
NARRATIVE & NOTES	9

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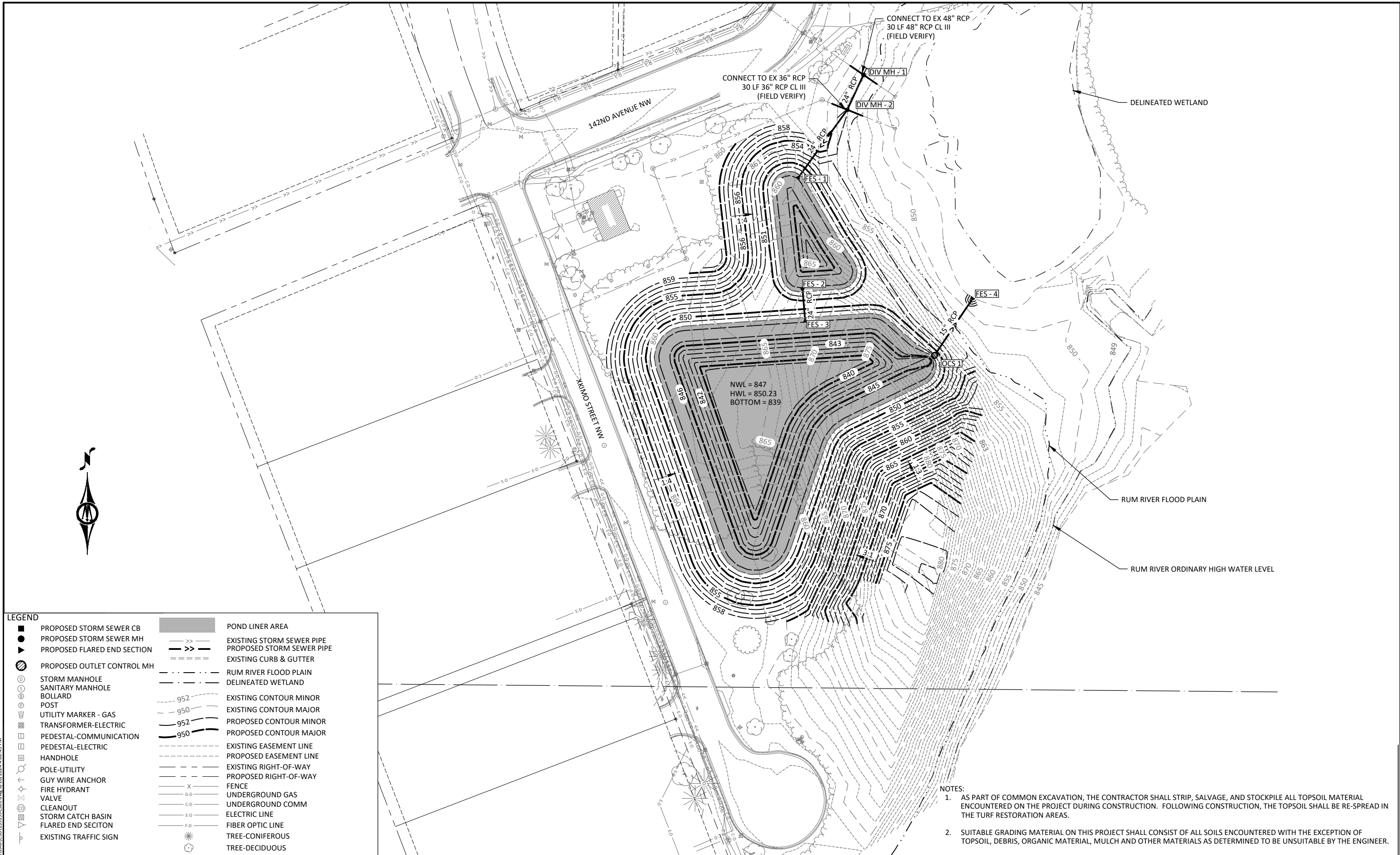


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CITY OF RAMSEY, MINNESOTA
 RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
 STORM WATER POLLUTION PREVENTION SOILS

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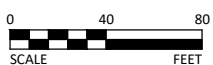
LEGEND

■	PROPOSED STORM SEWER CB	■	POND LINER AREA
●	PROPOSED STORM SEWER MH	— >> —	EXISTING STORM SEWER PIPE
▼	PROPOSED FLARED END SECTION	— >> —	PROPOSED STORM SEWER PIPE
⊗	PROPOSED OUTLET CONTROL MH	=====	EXISTING CURB & GUTTER
⊙	STORM MANHOLE	- - - - -	RUM RIVER FLOOD PLAIN
⊙	SANITARY MANHOLE	- - - - -	DELINEATED WETLAND
⊙	BOLLARD	— 952 —	EXISTING CONTOUR MINOR
⊙	POST	— 950 —	EXISTING CONTOUR MAJOR
⊙	UTILITY MARKER - GAS	— 952 —	PROPOSED CONTOUR MINOR
⊙	TRANSFORMER-ELECTRIC	— 950 —	PROPOSED CONTOUR MAJOR
⊙	PEDESTAL-COMMUNICATION	-----	EXISTING EASEMENT LINE
⊙	PEDESTAL-ELECTRIC	-----	PROPOSED EASEMENT LINE
⊙	HANDHOLE	-----	EXISTING RIGHT-OF-WAY
⊙	POLE-UTILITY	-----	PROPOSED RIGHT-OF-WAY
⊙	GUY WIRE ANCHOR	X	FENCE
⊙	FIRE HYDRANT	G-D	UNDERGROUND GAS
⊙	VALVE	C-D	UNDERGROUND COMM
⊙	CLEANOUT	E-D	ELECTRIC LINE
⊙	STORM CATCH BASIN	F-D	FIBER OPTIC LINE
⊙	FLARED END SECTION	☀	TREE-CONIFEROUS
⊙	EXISTING TRAFFIC SIGN	☺	TREE-DECIDUOUS

NOTES:

- AS PART OF COMMON EXCAVATION, THE CONTRACTOR SHALL STRIP, SALVAGE, AND STOCKPILE ALL TOPSOIL MATERIAL ENCOUNTERED ON THE PROJECT DURING CONSTRUCTION. FOLLOWING CONSTRUCTION, THE TOPSOIL SHALL BE RE-SPREAD IN THE TURF RESTORATION AREAS.
- SUITABLE GRADING MATERIAL ON THIS PROJECT SHALL CONSIST OF ALL SOILS ENCOUNTERED WITH THE EXCEPTION OF TOPSOIL, DEBRIS, ORGANIC MATERIAL, MULCH AND OTHER MATERIALS AS DETERMINED TO BE UNSUITABLE BY THE ENGINEER.

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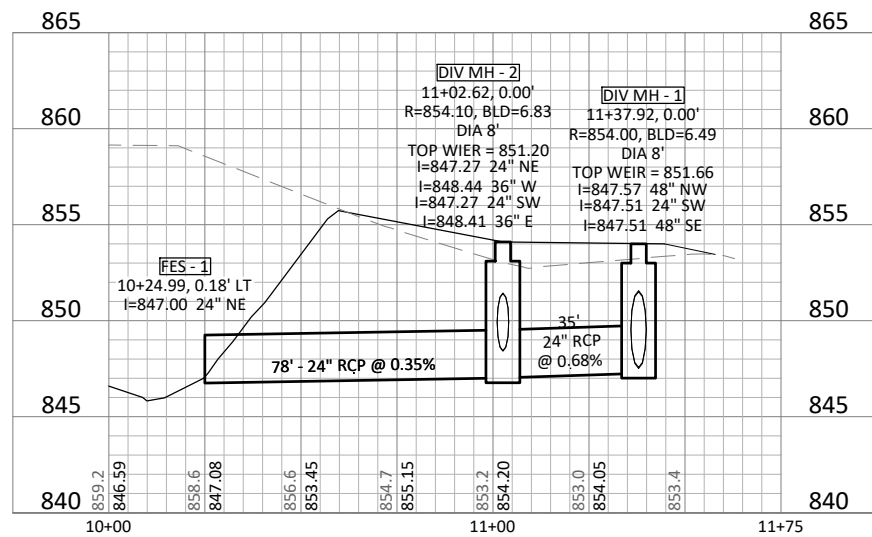
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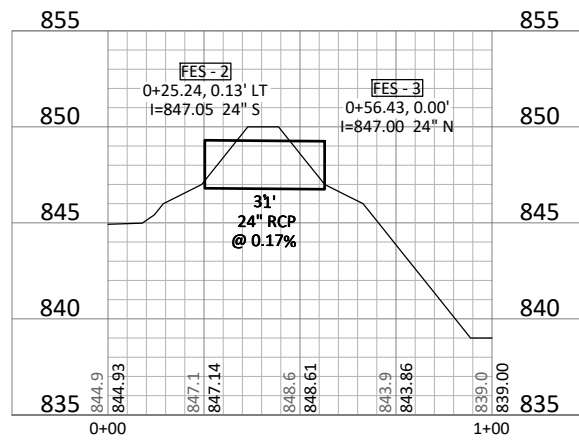
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CITY OF RAMSEY, MINNESOTA
 RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
 GRADING PLANS

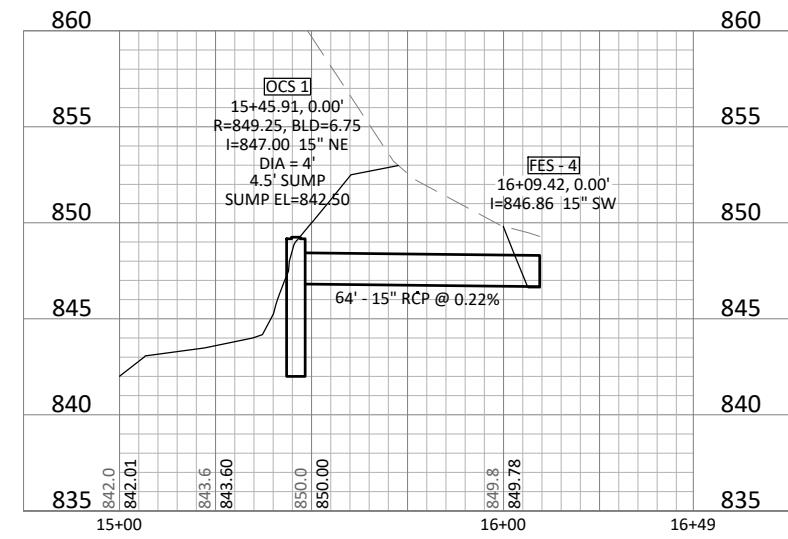
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 OF 12



FES - 1 TO DIV MH - 1

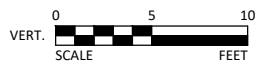
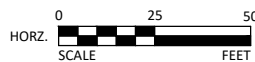


FES - 2 TO FES - 3



OCS 1 TO FES - 4

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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Kevin P. Kielb
KEVIN P. KIELB
LIC. NO. 23211 DATE 8/20/2024



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CLIENT PROJ. NO.			
CP-25-01			

CITY OF RAMSEY, MINNESOTA
RIVERS BEND REGIONAL STORMWATER IMPROVEMENTS
STORM SEWER PROFILES

SHEET 12 OF 12

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Address infrastructure needs.**Title:**

Update on the Pavement Management Program

Purpose/Background:**Purpose:**

The purpose of this case is to update the Public Works Committee on the Pavement Management Program.

Background:Pavement Management:

The success of the City's Pavement Management Program (PMP) is dependent upon many factors including but not limited to: planning and programming, design requirements and initial construction practices, pavement maintenance and rehabilitation, and economics. Failure in any of these key areas may result in pavement useful life being shortened. Many other factors affect the useful life of a pavement and the potential maintenance and rehabilitation actions required. These can include existing sub-grade conditions, drainage patterns, materials and previous maintenance practices to name a few.

Recent Achievements:

Over the last decade the City has reconstructed 27.2 miles (14.8-percent) of City streets and overlaid an additional 24.05 miles (13.1-percent) of City streets. The City has incorporated several techniques to cost-effectively rehabilitate failing pavement. These techniques include traditional reconstructions which remove and replace the entire pavement and aggregate base section, full-depth reclamations which recycle the existing pavement and aggregate base, stabilized full-depth reclamations which inject a stabilizer into the recycled base (gaining design strength), reclamation rehabilitations which recycle the pavement and build a slightly thinner pavement section (requires excellent sub-base material), and mill and overlay improvements which only replace the wearing course. Within this inventory of projects, specific site conditions can also influence the pavement reconstruction process (from existing utilities and drainage to existing sub-soil conditions); truly no two projects are the same.

Pavement Design Life:

The City's goal is to have a 60-year design life for newly constructed and reconstructed pavements. Assuming a fully funded PMP and pavement maintenance the following process describes the anticipated 60-year design life:

- Year 0 – New Construction or Reconstruction
- Year 2 – Pavement Rejuvenator (Pavement Maintenance)
- Year 3 – Crack Seal (Pavement Maintenance)
- Year 7 – Crack Seal (Pavement Maintenance)
- Year 14 – Crack Seal (Pavement Maintenance)
- Year 20 – Pavement Overlay No.1
- Years 21 to 39 – (Pavement Maintenance)
- Year 40 – Pavement Overlay No.2
- Years 41 to 59 – (Pavement Maintenance)
- Year 60 Reconstruction

Pavement Rating:

The City evaluates the condition of the pavement on an annual basis, generally following the Pavement Surface Evaluation and Rating (PASER) manual. PASER uses a visual inspection to evaluate pavement surface

conditions with a 0 – 10 scale (0 equals a gravel street, 10 equals a brand-new pavement). The PASER manual recommends a number based on surface defects (raveling, flushing, polishing), surface deformation (rutting, shoving, settling, frost heave), cracks (transverse, reflection, slippage, longitudinal, block, alligator), and patches and potholes.

Based on experience with the City’s street system, and years of using PASER, Staff is able to associate a PASER value with a recommended pavement maintenance or rehabilitation treatment. Two things to note are that PASER is a subjective evaluation, and the PASER value alone is not sufficient to determine a pavement treatment. In general, Staff uses the following guidelines to assign pavement treatments:

PASER \geq 7 Crack Seal is a cost-effective pavement maintenance treatment

- Below a 7, crack sealing becomes not as cost-effective due to the amount of material needed.

PASER \geq 6 Pavement Mill and Overlay is a cost-effective treatment

- In general, reflective cracking will migrate through 1-inch of bituminous per year. Within 2 years, reflective cracking is expected to show. Streets valued below 6 typically exhibit block cracking and/or some alligator cracking which would reflect to the surface of new overlay pavement. The cracks allow water into the base, resulting in a shortened life of the overlay.

PASER \leq 4 Pavement Reconstruction of some form is necessary.

- Any street segment with a PASER value of 4 or less is ideally included in the current Capital Improvement Program for reconstruction.

City-wide the goal is to have an average PASER value of 7. This would represent the majority of streets being above the desirable condition. Based on the 2024 PASER evaluation, the overall city street average value was 6.59. MSA collector streets had an average of 7.70 and neighborhood streets had an average of 6.34. What this means is the City’s collector streets are generally in good condition. However, there is catch up work to be done on the neighborhood streets.

Proposed 2025 – 2034 Capital Improvement Program:

Staff is proposing the following modifications to the CIP for existing projects:

- Move part of the Sunwood Drive and Rhinestone Street Concrete Repairs from 2029 to 2025
 - This includes Sunwood Drive Pedestrian Crossing Repairs (MSA Funded)
- Remove Beaudry’s 2nd subdivision from the 2025 Neighborhood Overlays Improvement
 - 2024 PASER values fell to 5. Further Staff review found an overlay no longer cost-effective
- Move Bunker Lake Boulevard (Jackal Street to Armstrong Boulevard) Reconstruction from 2032 to 2027
 - This segment of road is to be funded using approximately 50-percent MSA funds and 50-percent development fees. During the Oppidan development, TIF 18 District was created. Any TIF 18 funds must be spent by 2027 and may be used to offset MSA funds for this project.
- Move Deerwood Street Reconstruction from 2032 to 2027.
 - 2024 PASER Values for this subdivision fell to mostly 3 with some 4’s. Staff has observed pavement raveling and alligator cracking, and believes significant pavement patching would be required to maintain this pavement until the previously proposed 2032 reconstruction.

Staff identified 5 subdivisions for 2034 street reconstruction totaling 5.55 miles. Streets selected for reconstruction had PASER values of 4 within the subdivision. Staff feels these subdivisions will likely require patching in the interim years before the street reconstruction project and will continue to monitor. An additional 2 subdivisions and 4 MSA street segments totaling 3.00 miles were identified for pavement overlay in 2034. Additional information may be found in the 2025 – 2034 CIP Pavement Management Summary, attached to this case.

The proposed 2025 – 2034 CIP includes reconstructing 49.24 miles (26.8-percent) of City streets and overlaying an additional 27.36 miles (14.9-percent) of City streets. These street segments may be seen on the draft 2025 – 2034 CIP Map, attached to this case.

Potential Project not within the Capital Improvement Program:

Pavement deterioration rates can vary significantly for many factors which were previously discussed. In general, study has shown deterioration to be gradual for many years, and then to accelerate beyond the acceptable level in a short span. The goal of the PMP is to perform maintenance activities to minimize the gradual deterioration, and an overlay treatment before deterioration has hit the point of no return. As previously discussed, this would be to overlay a pavement when it's still rated a 6 or higher. During 2024 PASER evaluations, Staff found several segments of streets with PASER values between 5 and 6, which are not included in the proposed CIP. It's anticipated that a street rated a 5 in 2024 would fall to a 4 or below within the next 5 – 10 years, if not sooner.

Staff identified 26 subdivisions totaling 20.7 miles which had 2024 PASER values of 5 on at least one of the street segments within the subdivision. Staff anticipate these streets will need some type of significant rehabilitation (street reconstruction / full-depth reclamation / reclamation rehab.) project within the next 5 – 10 years, and/or require significant pavement patching. Assuming typical reconstruction or full-depth reclamation projects are appropriate for these streets, Staff estimates \$18,600,000 in funding is needed for these potential improvements. These subdivisions are summarized in the map and spreadsheet attached to this case. It should be noted that all the street segments identified were neighborhood streets, any MSA collector streets rated 6 or below are accounted for within the proposed CIP.

Notification:

Notification is not required for this case.

Time Frame/Observations/Alternatives:

Timeframe:

Staff estimates up to 30 minutes will be needed to present this case and respond to questions.

Observations:

It's important to keep in mind that the CIP is a funding planning tool and no projects are authorized until City Council approves them. Staff recommends street reconstructions and overlay improvements with two main goals in mind; replacing pavement sections in poor condition, and proactively maintaining pavement sections in fair to good condition so they don't deteriorate below acceptable conditions. The timing of projects is necessarily based on available funding and realistic Staff workload.

Funding Source:

Funding for the Pavement Management Program primarily comes from Pavement Management Funds (General Tax Levy) and Municipal State Aid Funds. Additional sources of funding come from Public Improvement Revolving Funds, TIF District funds, and developer / assessment funds in some circumstances.

Pavement Management Cash Flow:

The Pavement Management Cash Flow spreadsheet is attached to this case for reference. Starting in 2022, annual dollars, from the General Tax Levy, are placed into the Pavement Management Fund. Additionally, in 2022 \$11,903,744 bond dollars were placed into the fund to help catch up with necessary street reconstructions and overlays. The bond dollars are proposed to be used up by 2026. In 2026, approximately half of the proposed \$5,420,000 PM budget will be bond dollars and half will be built up general tax levy dollars. The cash flow is anticipated to break even at 2027, then begin running an increasing deficit starting in 2028. By 2034, the proposed deficit is \$3,762,000 per the attached 2025 – 2034 CIP Breakdown. Please keep in mind this document is a draft and still in need of review by the City's finance director.

PMP Funding Annual Needs:

Over the past 3 – 4 years, the construction industry has seen the same high levels of inflation as others, generally resulting in costs increasing 25 to 40-percent. For comparison, in 2021 Staff estimated a \$200,000/mile cost for a typical overlay, whereas in 2024 the same project is estimated at \$300,000/mile.

The City has approximately 189 miles of public streets, including existing gravel roads. During the 60-year design life, a street is anticipated to require 2 overlays and 1 reconstruction. In 2024 construction dollars, the City

estimates that on a typical street the average overlay costs \$300,000 per mile, and the average reconstruction costs \$850,000 per mile. Please note the reconstruction cost is averaged over all the different project types the City uses, i.e. full reconstruction, full-depth reclamation, stabilized full-depth reclamation, and reclamation rehabilitation.

For the typical neighborhood street, the life cycle cost per mile = $2 \times \$300,000 + \$850,000 = \$1,450,000 / \text{mi.} / 60\text{yrs.}$ All neighborhood streets combined cost $152.73 \text{ mi.} \times \$1,450,000 / \text{mi.} / 60 \text{ yrs.} = \$221,458,500 / 60 \text{ yrs.}$ That makes the annual need for neighborhood streets **\$3,690,975** in today's dollars. This does not account for making up ground on existing streets which may require reconstruction ahead of the assumed life cycle.

For typical MSA collector streets, the streets have a thicker pavement section and usually extra width from parking lanes adding approximately 20-percent to per mile costs. This makes the cost per mile / 60 years equal to \$1,740,000. For all MSA streets combined, the cost = $33.81 \text{ miles} \times \$1,740,000 / \text{mi.} / 60\text{yrs.} = \$58,829,400 / 60\text{yrs.}$ That makes the annual need for MSA streets **\$980,490** in today's dollars. This is approximately the amount of MSA funding the City receives, so we can say in general the MSA street funding needs are close to being met.

Recommendation:

This case is intended to update the Public Works Committee on the status of the Pavement Management Program. Staff does not have a recommendation at this time.

Outcome/Action:

This case is to update the Public Works Committee on the status of the Pavement Management Program. Staff is not requesting action, but would welcome any direction given by the Public Works Committee based on the update.

Attachments

- PMP Cash Flow
- Draft 2025 - 2034 CIP Breakdown
- Draft 2025 - 2034 CIP Map
- 2024 PASER Map
- Potential Project not in CIP Map
- Potential Projects not in CIP Summary

Form Review

Inbox	Reviewed By	Date
Bruce Westby	Bruce Westby	09/11/2024 04:50 PM
Brian Hagen	Brian Hagen	09/12/2024 12:27 PM
Form Started By: Joe Feriancek		Started On: 09/06/2024 11:17 AM
Final Approval Date: 09/12/2024		

Pavement Management Fund-9435

	Actual 2015	Actual 2016	Actual 2017	Actual 2018	Actual 2019	Actual 2020	Actual 2021	Actual 2022	Actual 2023	Projected 2024	Projected 2025	Projected 2026	Projected 2027	Projected 2028	Projected 2029	Projected 2030	Projected 2031	Projected 2032	Projected 2033
Beginning Balance-PIR FUND	409,971	300,021	649,351	715,441	911,902	814,564	1,504,580	1,706,619	15,102,525	14,628,255	10,556,453	5,985,956	2,558,790	943	(427,427)	(1,092,204)	(1,395,165)	(2,080,750)	(2,345,684)
Special Assessments		-	-	-	46,230	15,239	12,568	11,686	9,857	-	-	-	-	-	-	-	-	-	-
Franchise Fees - Est						423,045	2,071,456	4,859	-	-	-	-	-	-	-	-	-	-	-
ARPA Funds								1,900,000											
Property Tax Levy(based on 5% Levy increase).(assumes 96% prop tax collection)								1,669,353	1,739,254	1,826,217	1,917,528	2,013,404	2,114,074	2,219,778	2,330,767	2,447,305	2,569,671	2,698,154	2,833,062
Bond Proceeds	1,188,066	1,673,369	863,474	1,161,022				11,903,744											
TIF					300,000														
From General Fund/PIR Fund						260,752	300,000												
Est Arbitrage									(166,156)										
Sealcoating Fund Closed					95,402														
Interest Earnings	8,154	-	11,507	13,882	26,312	25,158	(7,170)	17,860	495,686	73,141	52,782	29,930	12,794	5	(2,137)	(5,461)	(6,976)	(10,404)	(11,728)
Total Revenues	1,196,220	1,673,369	874,981	1,174,904	468,640	724,194	2,376,854	15,507,501	2,078,641	1,899,358	1,970,310	2,043,334	2,126,868	2,219,783	2,328,630	2,441,844	2,562,695	2,687,750	2,821,333
Road Improvements:																			
2019 Projects-Ford Brook/Wood Pond Hills					(565,978)	(24,164)													
2020 Projects (Proj Canceled)						(10,014)													
CIP listings)							(2,155,414)	(2,074,896)	(2,521,754)	(5,855,870)	(6,481,517)	(5,420,331)	(4,420,142)	(2,616,120)	(2,984,494)	(2,744,805)	(3,248,280)	(2,952,684)	(3,896,550)
Garnet Street/Andrie-164th Lane/Sunwood/Riversbend/Stanhope Terr	(1,306,170)	(1,324,039)	(808,890)	(978,444)															
U study Road Funding							(5,000)												
Special Assessment Rebate							(10,673)	(27,519)	(31,158)	(59,290)	(59,290)	(50,169)	(38,964)	(32,033)	(8,913)				
Inspector Vehicle for road projects										(56,000)									
Double Meter Rebate							(821)	(9,180)											
GPR Pavement Eval							(2,908)												
Total Expenditures	(1,306,170)	(1,324,039)	(808,890)	(978,444)	(565,978)	(34,178)	(2,174,815)	(2,111,595)	(2,552,912)	(5,971,160)	(6,540,807)	(5,470,500)	(4,459,106)	(2,648,153)	(2,993,407)	(2,744,805)	(3,248,280)	(2,952,684)	(3,896,550)
Increase (decrease) in Cash	(109,950)	349,330	66,090	196,460	(97,338)	690,016	202,039	13,395,906	(474,271)	(4,071,802)	(4,570,497)	(3,427,166)	(2,332,238)	(428,370)	(664,777)	(302,961)	(685,585)	(264,934)	(1,075,217)
FUND BALANCE, End of Year	300,021	649,351	715,441	911,902	814,564	1,504,580	1,706,619	15,102,525	14,628,255	10,556,453	5,985,956	2,558,790	943	(427,427)	(1,092,204)	(1,395,165)	(2,080,750)	(2,345,684)	(3,420,900)

2025 - 2034 CIP Pavement Management Summary										
Pavement Management Fund			Year	MSA Funds	Pavement Management Funds	Other Funding Sources	Storm Water Utility Funds	Project Totals	PMF Yearly Budget	PMF Remaining Yearly Budget
Cash Flow Balance	Property Tax Levy	Year								
\$ 6,057,718.67	\$ 1,932,485.00	2025	\$ 2,702,281.25	\$ 6,341,430.00	\$ 1,264,500.00	\$ 1,305,080.09	\$ 11,613,291.34	\$ 6,000,000.00	\$ (341,430.00)	
\$ 2,666,496.92	\$ 2,029,109.25	2026	\$ 1,516,564.55	\$ 5,420,331.00	\$ 391,000.00	\$ 965,634.23	\$ 8,293,529.78	\$ 5,000,000.00	\$ (420,331.00)	
\$ (94,150.87)	\$ 2,130,564.71	2027	\$ 1,218,001.25	\$ 4,891,212.50	\$ -	\$ 741,659.87	\$ 6,850,873.62	\$ 4,000,000.00	\$ (891,212.50)	
\$ (473,177.92)	\$ 2,237,092.95	2028	\$ 1,178,600.00	\$ 2,616,120.00	\$ -	\$ 517,374.00	\$ 4,312,094.00	\$ 2,235,000.00	\$ (381,120.00)	
\$ (1,108,724.07)	\$ 2,348,947.60	2029	\$ 800,944.50	\$ 2,984,493.75	\$ -	\$ 564,962.74	\$ 4,350,400.99	\$ 2,350,000.00	\$ (634,493.75)	
\$ (1,387,134.10)	\$ 2,466,394.98	2030	\$ 1,113,805.00	\$ 2,744,805.00	\$ -	\$ 482,233.50	\$ 4,340,843.50	\$ 2,465,000.00	\$ (279,805.00)	
\$ (2,045,699.37)	\$ 2,589,714.72	2031	\$ 1,397,675.00	\$ 3,248,280.00	\$ -	\$ 545,296.50	\$ 5,191,251.50	\$ 2,590,000.00	\$ (658,280.00)	
\$ (1,761,322.91)	\$ 2,719,200.46	2032	\$ 980,637.50	\$ 2,434,824.00	\$ -	\$ 506,619.23	\$ 3,922,080.73	\$ 2,720,000.00	\$ 285,176.00	
\$ (2,802,712.43)	\$ 2,855,160.48	2033	\$ 742,560.00	\$ 3,896,550.00	\$ -	\$ 689,566.50	\$ 5,328,676.50	\$ 2,855,000.00	\$ (1,041,550.00)	
\$ (3,761,823.92)	\$ 2,997,918.51	2034	\$ 1,002,437.50	\$ 3,957,030.00	\$ -	\$ 630,606.38	\$ 5,590,073.88	\$ 2,995,000.00	\$ (962,030.00)	
		10 Yr. CIP Total	\$ 12,653,506.55	\$ 38,535,076.25	\$ 1,655,500.00	\$ 6,949,033.03	\$ 59,793,115.82			
	\$ 38,928,893.66	Available Funds	\$ 10,000,000.00	\$ 32,932,790.33				\$ 33,210,000.00	\$ (5,325,076.25)	
		10 Yr. Balance	\$ (2,653,506.55)	\$ (5,602,285.92)						

Project	Year	Type	MSA Funds	Pavement Management Funds	Other Funding Sources	Storm Water	Project Total	Notes
MSA - Ramsey Gateway	2025	Construction	\$ 1,000,000.00		\$ 1,000,000.00		\$ 2,000,000.00	\$1Mil PIR Funds
MSA - Sunwood Drive Pedestrian Crossing Repairs	2025	Concrete Repairs	\$ 405,000.00				\$ 405,000.00	Remove and Replace Colored Concrete
MSA - Alpine Drive (CSAH 5 / TH 47)	2025	FDR	\$ 530,250.00			\$ 79,537.50	\$ 609,787.50	\$ 1,121,573.44
MSA - Alpine Drive (CR 57 / CSAH 5)	2025	FDR	\$ 445,031.25			\$ 66,754.69	\$ 511,785.94	\$ 975,281.25
MSA - Sunwood Drive (CSAH 5 / Erkiun St)	2025	RC	\$ 303,000.00			\$ 45,450.00	\$ 348,450.00	IP 25-02
Dickenson's Mississippi Estate	2025	RC		\$ 378,144.00	\$ 250,000.00	\$ 306,721.60	\$ 934,865.60	IP 25-04 \$250,000 to extend Storm Sewer to Lano \$250,000 to replace watermain
Fox Ridge Estates 1st & 2nd	2025	RC		\$ 2,630,040.00		\$ 394,506.00	\$ 3,024,546.00	IP 24-13
High Point (Combine with Sec 22 Unplatted?)	2025	RC		\$ 1,430,160.00		\$ 214,524.00	\$ 1,644,684.00	IP 25-05 1986165
Section 22 Unplatted (Combine with High Point?)	2025	FDR		\$ 296,940.00		\$ 44,541.00	\$ 341,481.00	\$ 259,065.00
Sorteberg's	2025	FDR + C&G		\$ 559,035.00		\$ 83,855.25	\$ 642,890.25	IP 25-06
Ramsey Town Center	2025	OL		\$ 288,456.00		\$ 14,422.80	\$ 302,878.80	
Ramsey Town Center 2nd, 4th, 6th, 7th (*RTC 7th wear course partially paid by Greenway Terrace Apmt.)	2025	OL		\$ 309,003.00	\$ 14,500.00	\$ 32,284.65	\$ 355,787.65	\$ 1,130,801.05
Ramsey Town Center IP 05-22	2025	OL		\$ 29,088.00		\$ 1,454.40	\$ 30,542.40	
Rum River Hills	2025	OL		\$ 169,074.00		\$ 8,453.70	\$ 177,527.70	\$ 69,190.05
Village of Sunfish Lake	2025	OL		\$ 251,490.00		\$ 12,574.50	\$ 264,064.50	\$ 1,047,111.00
MSA - Bunker Lake Boulevard (Jackal St / Armstrong Blvd)	2026	RC	\$ 386,750.00		\$ 391,000.00	\$ 49,215.00	\$ 826,965.00	"1% Cost Escalation" Oppidin & Blip funding 85% Street; 15% Storm
MSA - 173rd Avenue / 175th Avenue (Armstrong Blvd / CSAH 5)	2026	OL + 1000' SS	\$ 1,121,814.55			\$ 168,272.18	\$ 1,290,086.73	
Carol-Rose Acres	2026	FDR		\$ 446,760.00		\$ 67,014.00	\$ 513,774.00	"2% Cost Escalation"
Countryside Estates	2026	FDR		\$ 1,940,040.00		\$ 291,006.00	\$ 2,231,046.00	"2% Cost Escalation"
Flintwood Hills 2nd & 3rd	2026	RC		\$ 1,015,920.00		\$ 152,388.00	\$ 1,168,308.00	"2% Cost Escalation"
Riverside West (Dolomite & 137th)	2026	FDR		\$ 199,665.00		\$ 29,949.75	\$ 229,614.75	"2% Cost Escalation"
Section 01 Unplatted (S/O CR 27)	2026	FDR		\$ 881,280.00		\$ 132,192.00	\$ 1,013,472.00	"2% Cost Escalation"
Sports Haven	2026	FDR		\$ 287,640.00		\$ 43,146.00	\$ 330,786.00	"2% Cost Escalation"
Alpine Woods	2026	OL		\$ 116,280.00		\$ 5,814.00	\$ 122,094.00	
Evergreen Point Townhomes 1st & 2nd	2026	OL		\$ 97,920.00		\$ 4,896.00	\$ 102,816.00	
Hall-Anderson Acres (TH 47 Service Road)	2026	OL		\$ 46,818.00		\$ 2,340.90	\$ 49,158.90	\$ 681,477.30
Riverside West (Ebony Street)	2026	OL		\$ 61,200.00		\$ 3,060.00	\$ 64,260.00	
Section 16 Unplatted (2006 Construction)	2026	OL		\$ 143,208.00		\$ 7,160.40	\$ 150,368.40	
Wildwood Acres	2026	OL		\$ 183,600.00		\$ 9,180.00	\$ 192,780.00	\$ 649,026.00
MSA - 142nd Avenue (TH 47 / Xkimo St)	2027	FDR	\$ 67,593.75			\$ 10,139.06	\$ 77,732.81	"3% Cost Escalation"
MSA - Juniper Ridge Drive (156th Ave / Roanoke St)	2027	FDR + C&G	\$ 390,627.50			\$ 58,594.13	\$ 449,221.63	"3% Cost Escalation"
MSA - Rhinestone Street (Veterans Dr / Sunwood Dr)	2027	OL	\$ 121,128.00			\$ 6,056.40	\$ 127,184.40	
(E. Ramsey Pkwy / CR 116)								
MSA - Sunwood Drive (CR 83 / Zeolite St)	2027	OL	\$ 352,260.00			\$ 17,613.00	\$ 369,873.00	\$ 785,169.00
(Rhinestone St / Peridot St)								
(Peridot St / CR 56)								
MSA - Veterans Drive (Zeolite St / Rhinestone St)	2027	OL	\$ 241,020.00			\$ 12,051.00	\$ 253,071.00	\$ 37,389.00
MSA - Zeolite Street (Sunwood Dr / Veterans Dr)	2027	OL	\$ 33,372.00			\$ 1,668.60	\$ 35,040.60	\$ 747,780.00
Deerwood	2027	FDR		\$ 698,340.00		\$ 104,751.00	\$ 803,091.00	"3% Cost Escalation"
Hall-Anderson Acres (Except TH 47 Service Rd)	2027	FDR		\$ 1,087,680.00		\$ 163,152.00	\$ 1,250,832.00	"3% Cost Escalation"
Oakwood Hills & Rambosek Red Oak Estates	2027	FDR		\$ 309,000.00		\$ 46,350.00	\$ 355,350.00	"3% Cost Escalation"
Sorteberg's 6th	2027	RC		\$ 766,320.00		\$ 114,948.00	\$ 881,268.00	"3% Cost Escalation"
Windemere Woods 1ST & 2ND & (Magnesium Street to Bunker)	2027	FDR + C&G		\$ 658,685.00		\$ 98,802.75	\$ 757,487.75	"3% Cost Escalation"
Windsorwood	2027	FDR		\$ 389,340.00		\$ 58,401.00	\$ 447,741.00	"3% Cost Escalation"
Brookfield	2027	OL		\$ 253,380.00		\$ 12,669.00	\$ 266,049.00	
Estates of Silver Oaks	2027	OL		\$ 312,090.00		\$ 15,604.50	\$ 327,694.50	
Riversbend (Non 2018 Recon)	2027	OL		\$ 361,530.00		\$ 18,076.50	\$ 379,606.50	\$ 1,030,980.43
Riversbend 2nd	2027	OL		\$ 27,810.00		\$ 1,390.50	\$ 29,200.50	\$ 49,132.93
Rivers Bend Plaza (142nd Avenue)	2027	OL		\$ 27,037.50		\$ 1,392.43	\$ 28,429.93	\$ 981,847.50
MSA - Alpine Drive (TH 10 / Puma Street)	2028	FDR	\$ 1,107,600.00			\$ 166,140.00	\$ 1,273,740.00	"4% Cost Escalation"
Ramsey Meadows 1st, 3rd & 4th	2028	FDR		\$ 288,600.00		\$ 43,290.00	\$ 331,890.00	"4% Cost Escalation"
Section 21 Unplatted	2028	FDR		\$ 1,915,680.00		\$ 287,352.00	\$ 2,203,032.00	"4% Cost Escalation"
Amber Ridge 2nd	2028	OL		\$ 18,720.00		\$ 936.00	\$ 19,656.00	
Meadow	2028	OL		\$ 215,280.00		\$ 10,764.00	\$ 226,044.00	\$ 432,432.00
Sunfish Lake Business Park 3rd	2028	OL		\$ 53,040.00		\$ 2,652.00	\$ 55,692.00	
Trott Brook Ridge	2028	OL		\$ 124,800.00		\$ 6,240.00	\$ 131,040.00	\$ 411,840.00

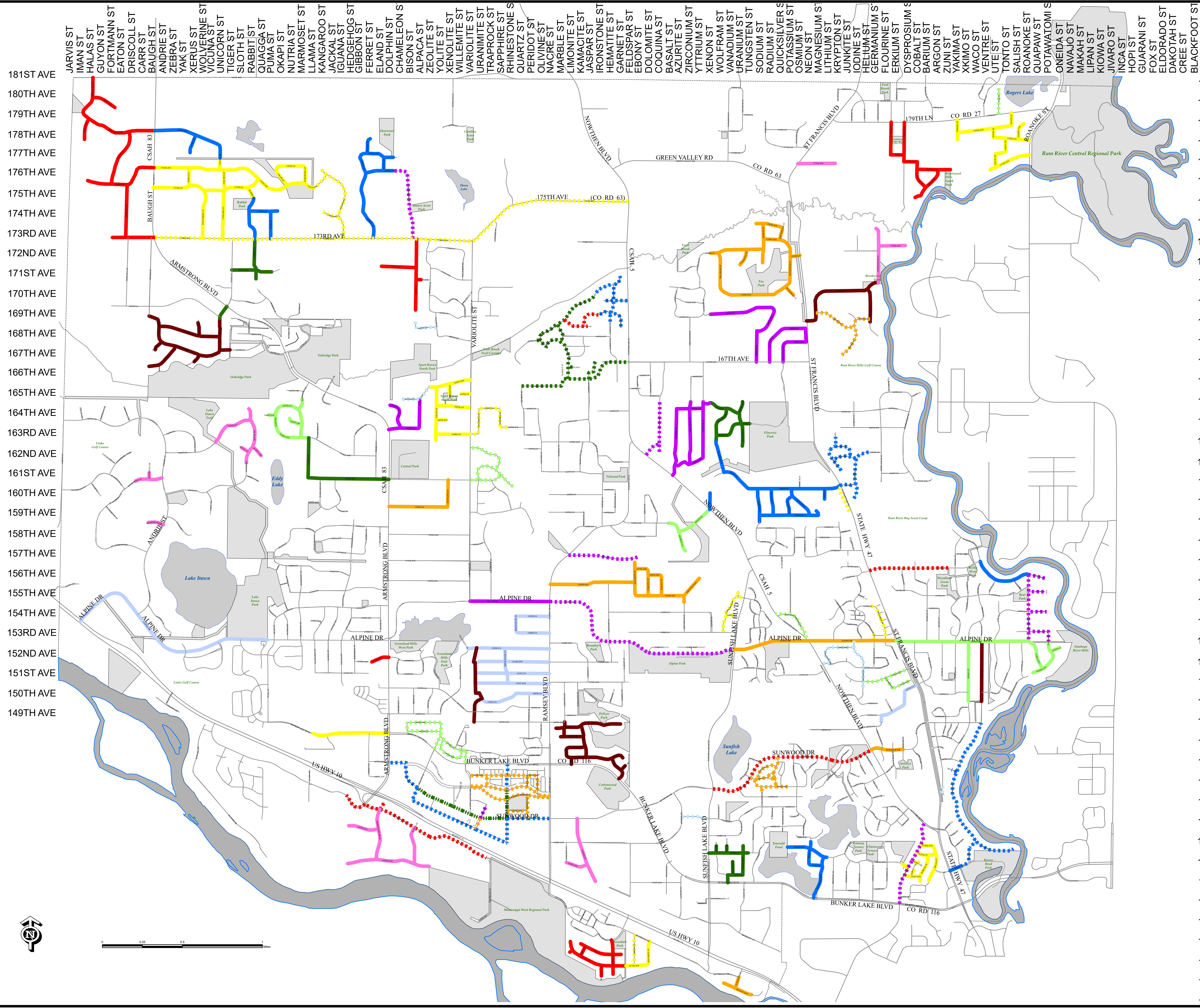
Project	Year	Type	MSA Funds	Pavement Management Funds	Other Funding Sources	Storm Water (10%)	Project Total	Notes
MSA - Sunwood Drive / Rhinestone Street (Concrete Repairs)	2029	Concrete Repairs	\$ 549,544.50			\$ 143,181.68	\$ 692,726.18	"5% cost escalation"
MSA - 166th Avenue / 167th Avenue (Quartz St / CSAH 5)	2029	OL	\$ 239,400.00			\$ 11,970.00	\$ 251,370.00	\$ 251,370.00
Alicia (Combine with Sec 07 Tiger Street)	2029	FDR + C&G		\$ 538,125.00		\$ 80,718.75	\$ 618,843.75	"5% cost escalation"
Menkveld's Country Park & Voltins Oak Hill Estates	2029	FDR		\$ 598,500.00		\$ 89,775.00	\$ 688,275.00	"5% cost escalation"
Section 07 Unplatted (Tiger Street) (Combine with Alicia)	2029	FDR + C&G		\$ 87,176.25		\$ 13,076.44	\$ 100,252.69	"5% cost escalation"
Section 17 Unplatted (161st Avenue & Llama Street)	2029	FDR + C&G		\$ 807,187.50		\$ 121,078.13	\$ 928,265.63	"5% Cost Escalation"
Sunfish Square 1st & 2nd	2029	FDR		\$ 574,875.00		\$ 86,231.25	\$ 661,106.25	"5% cost escalation"
Brookfield 2nd, 2nd/3rd, 3rd	2029	OL		\$ 343,350.00		\$ 17,167.50	\$ 360,517.50	\$ 397,561.50
Gerberts Addition	2029	OL		\$ 35,280.00		\$ 1,764.00	\$ 37,044.00	\$ 378,630.00
MSA - Alpine Drive (TH 47 / Roanoke St)	2030	.5 RC + .5 FDR	\$ 1,019,655.00			\$ 152,948.25	\$ 1,172,603.25	"5% cost escalation"
MSA - 161st Avenue (Variolite St / Rhinestone St)	2030	OL	\$ 66,150.00			\$ 3,307.50	\$ 69,457.50	"5% cost escalation"
Nature View	2030	FDR		\$ 289,800.00		\$ 43,470.00	\$ 333,270.00	"5% cost escalation"
River Bluffs 1st & 2nd (Combine w/ Sec. 24)	2030	FDR		\$ 417,375.00		\$ 62,606.25	\$ 479,981.25	"5% cost escalation"
Section 24 Unplatted [Xkimo Street] (Combine w/ River Bluffs?)	2030	FDR		\$ 233,100.00		\$ 34,965.00	\$ 268,065.00	"5% cost escalation"
Woodlawn Estates	2030	FDR + C&G		\$ 947,100.00		\$ 142,065.00	\$ 1,089,165.00	"5% cost escalation"
Ramsey Town Center 10th	2030	OL		\$ 173,250.00		\$ 8,662.50	\$ 181,912.50	
Ramsey Town Center 8th	2030	OL		\$ 233,100.00		\$ 11,655.00	\$ 244,755.00	
Section 01 Unplatted (Ute Street)	2030	OL		\$ 40,320.00		\$ 2,016.00	\$ 42,336.00	
Section 23 Unplatted (Old Hwy 5)	2030	OL		\$ 70,560.00		\$ 3,528.00	\$ 74,088.00	\$ 900,301.50
Section 24 Unplatted (2010 Construction)	2030	OL		\$ 141,750.00		\$ 7,087.50	\$ 148,837.50	\$ 857,430.00
Sweetbay Ridge	2030	OL		\$ 198,450.00		\$ 9,922.50	\$ 208,372.50	
MSA - Alpine Drive (Variolite St / Ramsey Blvd)	2031	FDR	\$ 492,187.50			\$ 73,828.13	\$ 566,015.63	"5% cost escalation"
MSA - Alpine Drive (Ramsey Blvd / CR 57)	2031	OL	\$ 527,625.00			\$ 26,381.25	\$ 554,006.25	
MSA - Dysprosium Street (CR 116 / CR 5)	2031	OL	\$ 216,562.50			\$ 10,828.13	\$ 227,390.63	\$ 920,311.88
MSA - Roanoke Street (Alpine Dr / Juniper Ridge Dr)	2031	OL	\$ 132,300.00			\$ 6,615.00	\$ 138,915.00	\$ 876,487.50
Klemish & Klemish and Flores & Section 11 Unplatted	2031	FDR		\$ 957,600.00		\$ 143,640.00	\$ 1,101,240.00	"5% cost escalation"
Section 15 Unplatted (North of Nowthen Blvd)	2031	FDR		\$ 989,100.00		\$ 148,365.00	\$ 1,137,465.00	"5% cost escalation"
The North Forty	2031	RC		\$ 705,600.00		\$ 105,840.00	\$ 811,440.00	"5% cost escalation"
Oak Run	2031	OL		\$ 107,730.00		\$ 5,386.50	\$ 113,116.50	
Ramsey Town Center 11th	2031	OL		\$ 28,350.00		\$ 1,417.50	\$ 29,767.50	
Section 15 Unplatted (2011 OL Street)	2031	OL		\$ 157,500.00		\$ 7,875.00	\$ 165,375.00	\$ 625,779.00
Shawn Acres	2031	OL		\$ 141,750.00		\$ 7,087.50	\$ 148,837.50	\$ 595,980.00
Stanhope River Hills	2031	OL		\$ 160,650.00		\$ 8,032.50	\$ 168,682.50	
MSA - 142nd Avenue (West CDS Armstrong Blvd / East CDS Alpaca St) & Armstrong Boulevard (Riverdale Drive / 142nd Ave) & Alpaca Estates	2032	FDR RR	\$ 529,200.00	\$ 194,040.00		\$ 108,486.00	\$ 831,726.00	*New Sheet
MSA - Jasper Street (Sunwood Drive / McKinley St)	2032	FDR	\$ 413,437.50			\$ 62,015.63	\$ 475,453.13	"5% Cost escalation"
Brookview Estates (North)	2032	FDR + 0.5SS		\$ 537,264.00		\$ 80,589.60	\$ 617,853.60	"5% Cost Escalation"
Echo Ridge & Section 02 Unplatted (177th Avenue)	2032	FDR		\$ 151,200.00		\$ 22,680.00	\$ 173,880.00	"5% Cost Escalation"
Gateway Industrial Park (142nd Avenue)	2032	RC		\$ 191,520.00		\$ 28,728.00	\$ 220,248.00	Construct with MSA - Jasper Street
Northfork Lake	2032	RC		\$ 390,600.00		\$ 58,590.00	\$ 449,190.00	"5% Cost Escalation"
Northfork Oaks 2nd	2032	RC		\$ 340,200.00		\$ 51,030.00	\$ 391,230.00	"5% Cost Escalation"
Northfork Oaks 3rd	2032	RC		\$ 630,000.00		\$ 94,500.00	\$ 724,500.00	"5% Cost Escalation"
MSA - Uraninite Street (149th Avenue / 152nd Lane)	2033	FDR	\$ 327,600.00			\$ 49,140.00	\$ 376,740.00	"5% Cost escalation"
MSA - Waco Street (150th Avenue / Alpine Drive)	2033	RC	\$ 372,960.00			\$ 55,944.00	\$ 428,904.00	"5% Cost escalation"
Brookview Estates (South)	2033	FDR		\$ 516,600.00		\$ 77,490.00	\$ 594,090.00	"5% Cost Escalation"
Hunters Ridge	2033	RC		\$ 2,104,200.00		\$ 315,630.00	\$ 2,419,830.00	"5% Cost Escalation"
Regency Pond 1st, 2nd, 3rd	2033	FDR		\$ 1,275,750.00		\$ 191,362.50	\$ 1,467,112.50	"5% Cost Escalation"
MSA - 156th Lane (E EOP / TH 47)	2034	OL	\$ 157,500.00			\$ 7,875.00	\$ 165,375.00	
MSA - Riverdale Drive (Armstrong Boulevard / Traprock Street)	2034	OL	\$ 283,500.00			\$ 14,175.00	\$ 297,675.00	\$ 1,029,459.38
MSA - Riverdale Drive (Armstrong Interchange)	2034	OL	\$ 102,375.00			\$ 5,118.75	\$ 107,493.75	\$ 980,437.50
Sunwood Drive (CR 57 / CSAH 5)	2034	OL	\$ 437,062.50			\$ 21,853.13	\$ 458,915.63	\$ 49,021.88
Riverwood Hills Plat 1	2034	FDR		\$ 781,200.00		\$ 117,180.00	\$ 898,380.00	*New Sheet
Anderson Estate / Dellwood Hills / Section 09 Unplatted	2034	RC / FDR		\$ 567,000.00		\$ 85,050.00	\$ 652,050.00	*New Sheet
Whispering Pines Estates Plat 5	2034	RC		\$ 110,880.00		\$ 16,632.00	\$ 127,512.00	*New Sheet
Rivenwick 1st & 2nd	2034	FDR		\$ 834,750.00		\$ 125,212.50	\$ 959,962.50	*New Sheet
Section 06 Unplatted	2034	FDR		\$ 1,543,500.00		\$ 231,525.00	\$ 1,775,025.00	*New Sheet
Brookfield 4th	2034	OL		\$ 100,800.00		\$ 5,040.00	\$ 105,840.00	*New Sheet
Armstrong Boulevard (Riverdale Drive / N. CDS)	2034	OL		\$ 18,900.00		\$ 945.00	\$ 19,845.00	\$ 125,685.00
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Project	Year	Type	MSA Funds	Pavement Management Funds	Other Funding Sources	Storm Water (10%)	Project Total	Notes
2025 MSA Pavement Markings	2025	Pavement Markings	\$ 19,000.00				\$ 19,000.00	
2026 MSA Pavement Markings	2026	Pavement Markings	\$ 8,000.00				\$ 8,000.00	
2027 MSA Pavement Markings	2027	Pavement Markings	\$ 12,000.00				\$ 12,000.00	
2028 MSA Pavement Markings	2028	Pavement Markings	\$ 71,000.00				\$ 71,000.00	
2029 MSA Pavement Markings	2029	Pavement Markings	\$ 12,000.00				\$ 12,000.00	
2030 MSA Pavement Markings	2030	Pavement Markings	\$ 28,000.00				\$ 28,000.00	\$ 281,000.00
2031 MSA Pavement Markings	2031	Pavement Markings	\$ 29,000.00				\$ 29,000.00	
2032 MSA Pavement Markings	2032	Pavement Markings	\$ 38,000.00				\$ 38,000.00	
2033 MSA Pavement Markings	2033	Pavement Markings	\$ 42,000.00				\$ 42,000.00	
2034 MSA Pavement Markings	2034	Pavement Markings	\$ 22,000.00				\$ 22,000.00	

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CITY OF RAMSEY

Pavement Management Program
2025 - 2034 CIP Projects
DRAFT 8/29/2024

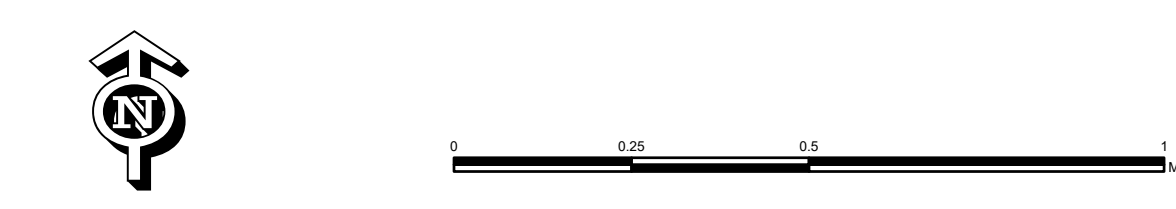


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RC = RECONSTRUCTION
OL = OVERLAY
CONC REP = CONCRETE REPAIR

CIP YEAR, PROJECT TYPE

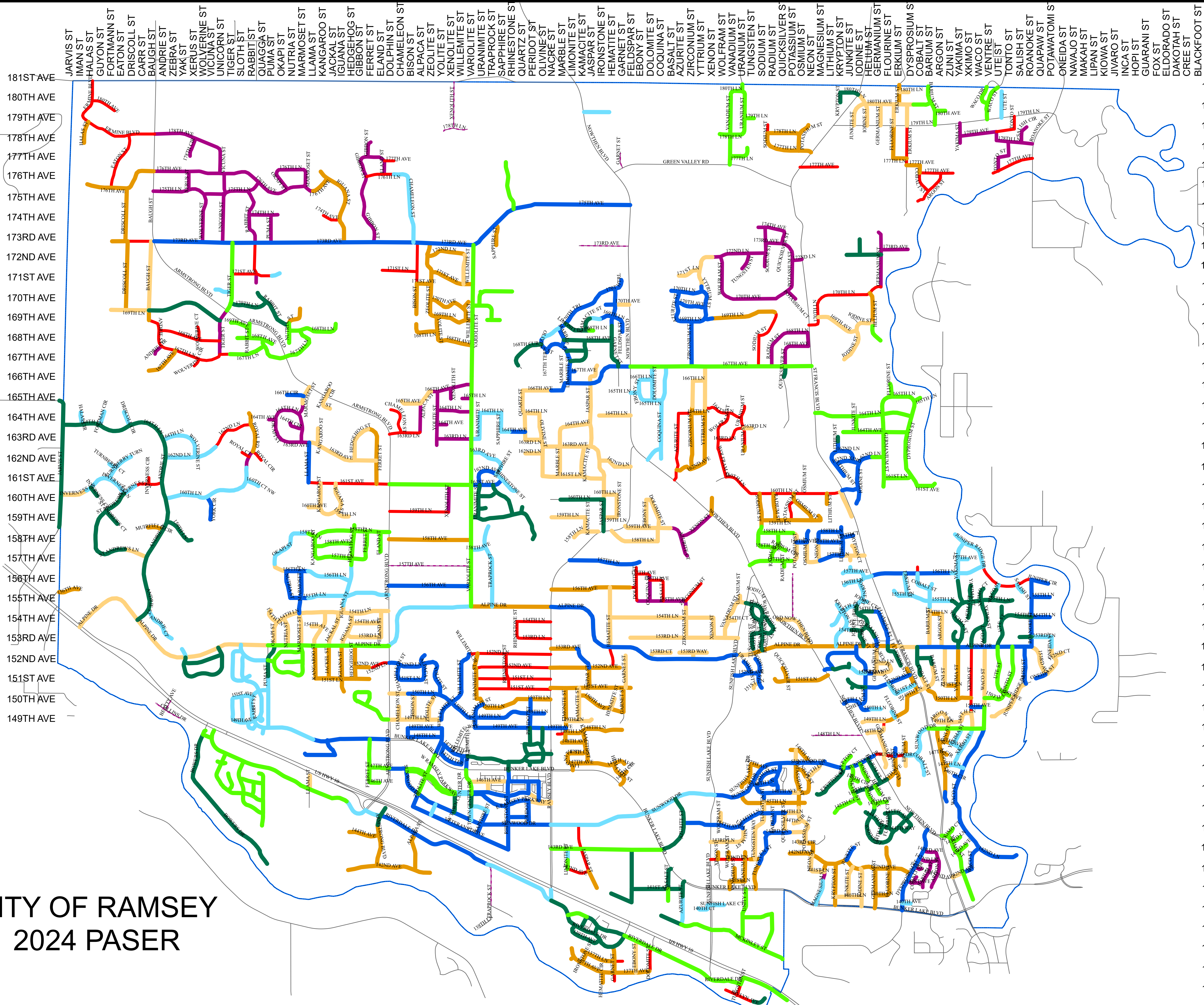
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- 2028, RC
- 2028, OL
- 2027, RC
- 2027, OL
- 2026, RC
- 2026, OL
- 2025, RC
- 2025, OL
- Road_Centerlines
- MuniBndry
- Parks
- Rivers
- Lakes_Ponds
- Creeks



181ST AVE
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135TH AVE

JARVIS ST
IMAN ST
HALAS ST
GUYON ST
FORTMANN ST
EATON ST
DRISCOLL ST
CARR ST
BAUGH ST
ANDRIE ST
ZEBRA ST
YAK ST
XERUS ST
WOLVERINE ST
VICUNA ST
UNICORN ST
TIGER ST
SLOTH ST
RABBIT ST
QUAGGA ST
PUMA ST
OKAPI ST
NUTRIA ST
MARMOSSET ST
LLAMA ST
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JACKAL ST
IGUANA ST
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GIBBON ST
FERRET ST
ELAND ST
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CHAMELEON S
BISON ST
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PERIDOT ST
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NACRE ST
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JASPAR ST
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JIVARO ST
INCA ST
HOPI ST
GUARANI ST
FOX ST
ELDORADO ST
DAKOTAH ST
CREE ST
BLACKFOOT ST

CITY OF RAMSEY 2024 PASER



- ### Legend
- 24 PASER 10
 - 24 PASER 9
 - 24 PASER 8
 - 24 PASER 7
 - 24 PASER 6
 - 24 PASER 5
 - 24 PASER 4
 - 24 PASER 1 - 3
 - Dirt Streets
 - MRCC_Centerlines
 - MuniBndry

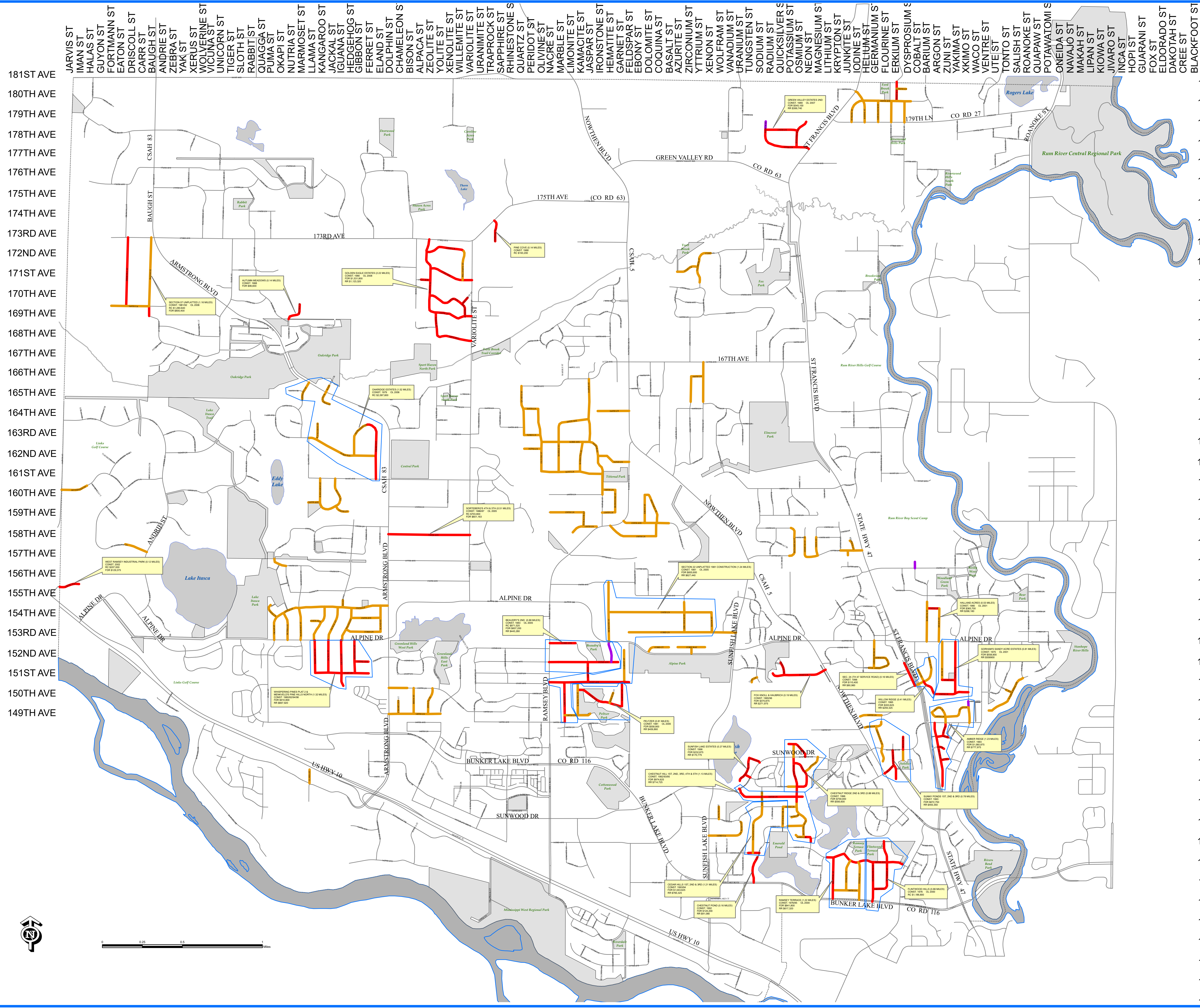
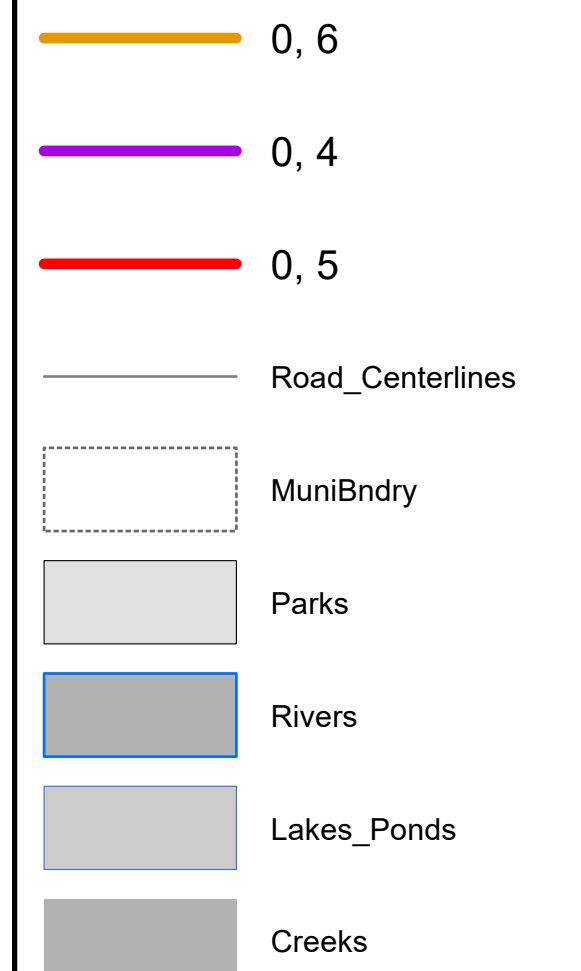
CITY OF RAMSEY

Pavement Management Program
2025 - 2034 CIP
Project Not within CIP
DRAFT 8/29/2024

LEGEND

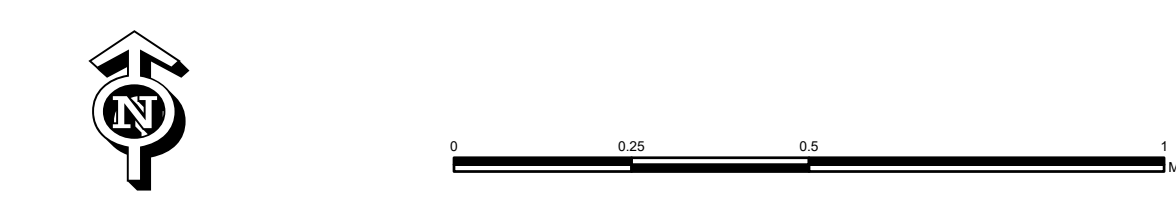
RC = RECONSTRUCTION
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CIP YEAR, PROJECT TYPE



181ST AVE
180TH AVE
179TH AVE
178TH AVE
177TH AVE
176TH AVE
175TH AVE
174TH AVE
173RD AVE
172ND AVE
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JARVIS ST
IMAN ST
GUYON ST
FORTMANN ST
EATON ST
DRISCOLL ST
CARR ST
BAUGH ST
ANDRIE ST
ZEBRA ST
YAK ST
XERUS ST
WOLVERINE ST
VICUNA ST
UNICORN ST
TIGER ST
SLOTH ST
RABBIT ST
QUAGGA ST
PUMA ST
OKAPI ST
NUTRIA ST
MARMOSSET ST
LLAMA ST
KANGAROO ST
JACKAL ST
IGUANA ST
HEDGEHOG ST
GIBBON ST
FERRET ST
ELAND ST
DOLPHIN ST
CHAMELEON S
BISON ST
ALPACA ST
ZEOLITE ST
YOLITE ST
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WILLEMITE ST
VARIOLITE ST
URANIMITE ST
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SAPPHIRE ST
RHINESTONE S
QUARTZ ST
PERIDOT ST
OLIVINE ST
NACRE ST
MARBLE ST
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HEMATITE ST
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MAKAH ST
LIPAN ST
KIOWA ST
JIVARO ST
INCA ST
HOPI ST
GUARANI ST
FOX ST
ELDORADO ST
DAKOTAH ST
CREEK ST
BLACKFOOT ST



POTENTIAL PROJECTS NOT INCLUDED IN THE CIP
(SUBDIVISION HAS STREET WITH PASER 5 OR LESS)

8/30/2024

SUBDIVISION	2024 RATING	CONSTRUCTION	OVERLAY	MILES	PRJ. TYPE	COST	PRJ. TYPE	COST
AMBER RIDGE	5 & 6	1993	-	1.23	FDR	\$ 1,060,875.00	RR	\$ 777,975.00
AUTUMN MEADOWS	5	1988	-	0.14	FDR	\$ 96,000.00		\$ 96,000.00
BEAUDRY'S 2ND	5	1983	2005	0.88	RC	\$ 971,520.00	FDR	\$ 607,200.00
CEDAR HILLS 1ST, 2ND & 3RD	5 & 6	1993/94	-	1.21	FDR	\$ 1,043,625.00	RR	\$ 765,325.00
CHESTNUT HILL 1ST, 2ND, 3RD, 4TH & 5TH	5 & 6	1992/93/95	-	1.13	FDR	\$ 974,625.00	RR	\$ 714,725.00
CHESTNUT POND	5	1992	-	0.16	FDR	\$ 124,200.00	RR	\$ 91,080.00
CHESTNUT RIDGE 2ND & 3RD	5	1996	-	0.88	FDR	\$ 759,000.00	RR	\$ 556,600.00
FLINTWOOD HILLS	5	1976	2000	0.86	RC	\$ 1,186,800.00		\$ 1,186,800.00
FOX KNOLL & HAUBRICH	5	1995/96	-	0.19	FDR	\$ 370,875.00	RR	\$ 271,975.00
GOLDEN EAGLE ESTATES	5	1980	2008	2.22	FDR	\$ 1,531,800.00	RR	\$ 1,123,320.00
GORHAM'S SANDY ACRE ESTATES	5	1975	2001	0.81	FDR	\$ 558,900.00	RR	\$ 409,860.00
GREEN VALLEY ESTATES 2ND	5	1989	2007	0.79	FDR	\$ 545,100.00	RR	\$ 399,740.00
HALLAND ACRES	5 & 6	1980	2001	0.53	FDR	\$ 365,700.00	RR	\$ 268,180.00
WHISPERING PINES ESTATES PLAT 2 & MENKVELD'S PINE HILLS NORTH	5	1980/82/94/98	-	1.32	FDR	\$ 910,800.00	RR	\$ 667,920.00
OAKRIDGE ESTATES	5 & 6	1979	2006	1.52	RC	\$ 2,097,600.00		\$ 2,097,600.00
PELTZER	5 & 6	1981	2005	0.81	FDR	\$ 558,900.00	RR	\$ 409,860.00
PINE COVE	5	1988	-	0.14	RC	\$ 193,200.00		\$ 193,200.00
RAMSEY TERRACE	5 & 6	1976/85	2000	1.22	FDR	\$ 841,800.00	RR	\$ 617,320.00
SECTION 07 UNPLATTED (NON-TIGER STREETS)	5 & 6	1981/92	2006	1.16	RC	\$ 1,280,640.00	FDR	\$ 800,400.00
SECTION 22 UNPLATTED (1981 CONSTRUCTION)	5 & 6	1981	2005	1.24	FDR	\$ 855,600.00	RR	\$ 627,440.00
SECTION 24 UNPLATED (TH 47 SERVICE ROAD)	5	1996	-	0.16	FDR	\$ 110,400.00	RR	\$ 80,960.00
SORTEBERG'S 4TH & 5TH	5	1986/87	2005	0.51	RC	\$ 703,800.00	FDR	\$ 601,163.00
SUNFISH LAKE ESTATES	5	1996	-	0.27	FDR	\$ 232,875.00	RR	\$ 170,775.00
SUNNY PONDS 1ST, 2ND & 3RD	5 & 6	1993	-	0.78	FDR	\$ 672,750.00	RR	\$ 493,350.00
WEST RAMSEY INDUSTRIAL PARK	5	2002	-	0.12	RC	\$ 207,000.00	FDR	\$ 129,375.00
WILLOW RIDGE	6 & 5	1995	-	0.41	FDR	\$ 353,625.00	RR	\$ 259,325.00
					HIGH RANGE TOTAL	\$ 18,608,010.00	LOW RANGE TOTAL	\$ 14,417,468.00

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Address infrastructure needs.**Title:**

Consider Recommending City Council Order Request for Proposals for Sunwood Drive Concrete Pedestrian Crossing Replacement Topographic Survey

Purpose/Background:**Purpose:**

The purpose of this case is to consider recommending the City Council to request proposals for Sunwood Drive concrete pedestrian crossing replacement topographic survey. Additionally, for Staff to get feedback from the Public Works Committee on design options for the replacement concrete crossings.

Background:

The current 2024 through 2033 10-Year Capital Improvement Program (CIP) proposes concrete repairs for Sunwood Drive between Rhinestone Street and Zeolite Street, and for Rhinestone Street between Sunwood Drive and Ramsey Parkway in 2029.

The concrete pavement along Sunwood Drive and Rhinestone Street were constructed in 2004 under Improvement Project 03-29. The project included colored concrete pedestrian crossings at 8 intersections (6 along Sunwood Drive and 2 along Rhinestone Street). Over the last several years, Public Works staff has addressed patching these concrete streets, typically using bituminous to fill voids in the concrete pavement. Additional curb replacement and small panel replacement work has also occurred. Though many repairs are required to the concrete pavement along this corridor, the most damage has occurred to the colored concrete in the pedestrian crosswalks. This is a significant safety concern because of the trip hazards created due to the deterioration of the colored concrete, along with increased pedestrian volumes anticipated due to continued development within The COR.

The 2029 concrete repairs improvement project is proposed to address the pedestrian crossings and general crack repairs. Due to the creation of trip hazards in the pedestrian crossing areas, and the anticipated increase in foot traffic along Sunwood Drive due to the recent and proposed developments along Sunwood Drive, Staff has proposed splitting the Sunwood Drive and Rhinestone Street Concrete Repairs project in two. The proposed 2025 – 2034 CIP proposes to replace the colored concrete pedestrian ramps along Sunwood Drive in 2025. The remaining repairs are still proposed for 2029.

Staff will use the survey data as design aids for the improvement project. Topographic surveys are essential to locate all existing site features and to create an existing ground surface, used as the base layer for designing the project.

Staff has preliminarily inspected the existing pedestrian ramps adjacent to the crossings, and has noted several ramps do not meet current ADA standards. A summary of the findings is attached to this case.

Committee input is desired on the following discussion topics and options.

Discussion Topics:

Options for replacing crosswalk concrete:

1. Stamped colored concrete

2. Stamped concrete with design and/or colored cure/stain
3. Plain concrete with painted crosswalk markings
4. Plain concrete with painted crosswalk markings
5. Something else

Options for replacing pedestrian ramps:

1. Push out replacing the non-compliant pedestrian ramps to 2029 (City’s ADA Transition Plan would typically require updating the ramps)
2. Replace all non-ADA compliant ramps only
3. Replace curb cuts for future connections
4. Add landings at the curb cut locations
5. Remove the parking bay on the south side of the Center Street intersection. Improves safety by shortening the crosswalks and removing parked or stopped vehicles from within the intersection. See attached MN Statute 169.34.
6. Something else

Notification:

No notifications are required with this case.

Time Frame/Observations/Alternatives:

Time Frame:

Staff anticipates up to 15 minutes are required to present information on the case and answer any questions from the committee.

Alternatives:

Alternative #1: Motion to recommend City Council order request for proposals for Sunwood Drive concrete pedestrian crossing replacement topographic survey.

Alternative #2: Motion of other.

Funding Source:

Funding for this work is proposed to come from Municipal State Aid Funds.

Recommendation:

Staff recommends Alternative #1.

Outcome/Action:

Action:

Motion to recommend the City Council order requests for proposals for Sunwood Drive concrete pedestrian crossing replacement topographic survey.

Attachments

- Project Scope
- MN Statute 169.34
- Existing Condition Pic

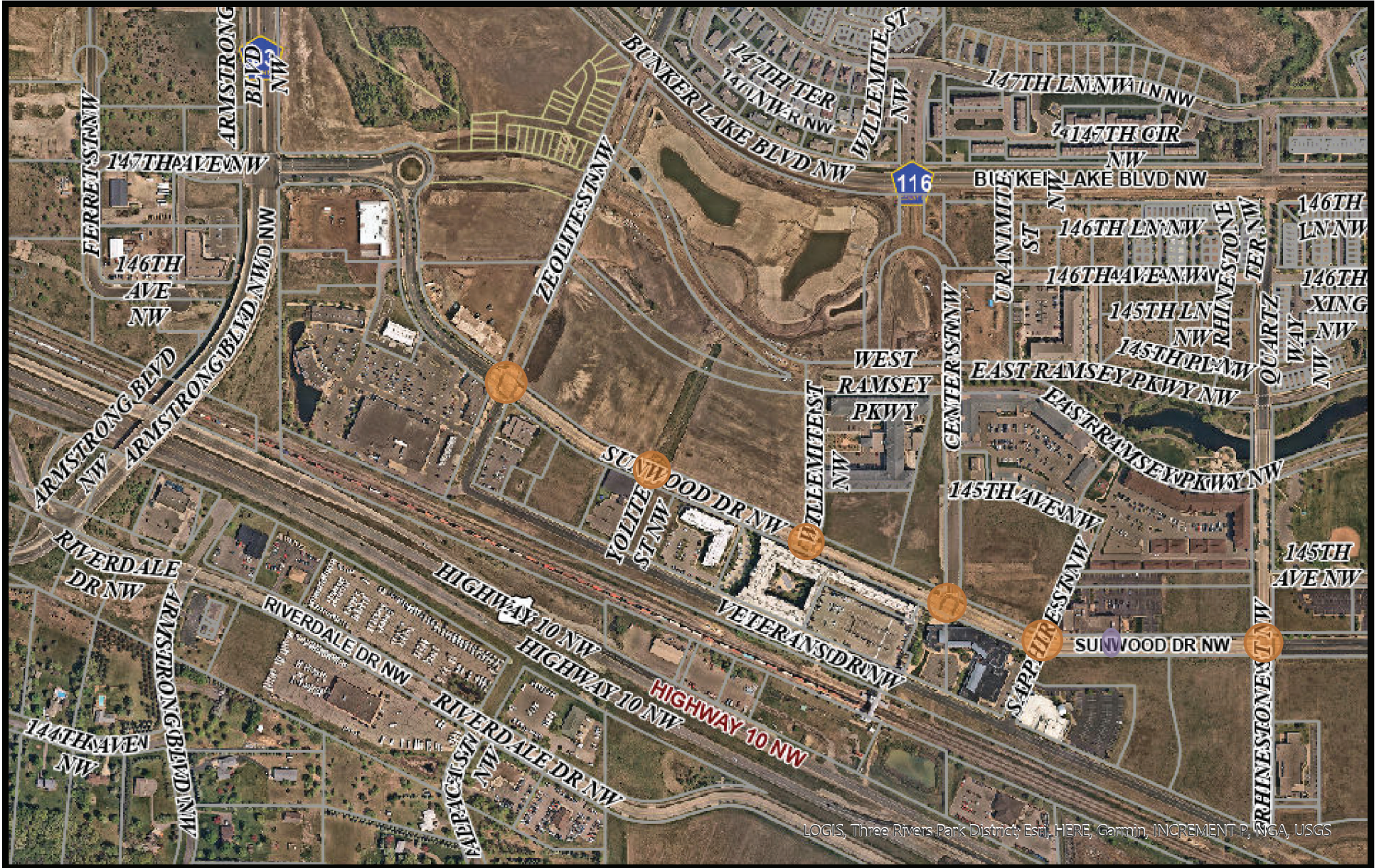
Form Review

Inbox	Reviewed By	Date
Bruce Westby	Bruce Westby	09/11/2024 04:12 PM
Brian Hagen	Brian Hagen	09/12/2024 12:21 PM

Form Started By: Joe Feriancek
Final Approval Date: 09/12/2024

Started On: 09/05/2024 11:52 AM

SUNWOOD DRIVE PEDESTRIAN CROSSINGS



Print Date: September 11, 2024

0 0.04 0.07 0.15 0.22 0.3 mi

169.34 PROHIBITIONS; STOPPING, PARKING.

Subdivision 1. **Prohibitions.** (a) No person shall stop, stand, or park a vehicle, except when necessary to avoid conflict with other traffic or in compliance with the directions of a police officer or traffic-control device, in any of the following places:

- (1) on a sidewalk;
- (2) in front of a public or private driveway;
- (3) within an intersection;
- (4) within ten feet of a fire hydrant;
- (5) on a crosswalk;
- (6) within 20 feet of a crosswalk at an intersection;

(7) within 30 feet upon the approach to any flashing beacon, stop sign, or traffic-control signal located at the side of a roadway;

(8) between a safety zone and the adjacent curb or within 30 feet of points on the curb immediately opposite the ends of a safety zone, unless a different length is indicated by signs or markings;

(9) within 50 feet of the nearest rail of a railroad crossing;

(10) within 20 feet of the driveway entrance to any fire station and on the side of a street opposite the entrance to any fire station within 75 feet of said entrance when properly signposted;

(11) alongside or opposite any street excavation or obstruction when such stopping, standing, or parking would obstruct traffic;

(12) on the roadway side of any vehicle stopped or parked at the edge or curb of a street;

(13) upon any bridge or other elevated structure upon a highway or within a highway tunnel, except as otherwise provided by ordinance;

(14) within a bicycle lane, except when posted signs permit parking; or

(15) at any place where official signs prohibit stopping.

(b) No person shall move a vehicle not owned by such person into any prohibited area or away from a curb such distance as is unlawful.

(c) No person shall, for camping purposes, leave or park a travel trailer on or within the limits of any highway or on any highway right-of-way, except where signs are erected designating the place as a campsite.

(d) No person shall stop or park a vehicle on a street or highway when directed or ordered to proceed by any peace officer invested by law with authority to direct, control, or regulate traffic.

Subd. 2. **Violation; penalty for owner or lessee.** (a) If a motor vehicle is stopped, standing, or parked in violation of subdivision 1, the owner of the vehicle, or for a leased motor vehicle the lessee of the vehicle, is guilty of a petty misdemeanor.

(b) The owner or lessee may not be fined under paragraph (a) if (1) another person is convicted for, or pleads guilty to, that violation, or (2) the motor vehicle was stolen at the time of the violation.

(c) Paragraph (a) does not apply to a lessor of a motor vehicle if the lessor keeps a record of the name and address of the lessee.

(d) Paragraph (a) does not prohibit or limit the prosecution of a motor vehicle operator who violates subdivision 1.

(e) A violation under paragraph (a) does not constitute grounds for revocation or suspension of the owner's or lessee's driver's license.

History: (2720-219) 1937 c 464 s 69; Ex1937 c 38 s 1; 1939 c 430 s 13; 1989 c 342 s 16; 2008 c 287 art 1 s 46; 2013 c 127 s 35; 2022 c 55 art 1 s 64



NW Quad Sunwood Drive & Center Street; Facing South



NW Quad Sunwood Drive & Rhinestone Street; Facing East



SE Quad Sunwood Drive & Sapphire Street; Facing West



SE Quad Sunwood Drive & Willemite Street; Facing North



SW Quad Sunwood Drive & Yolite Street; Facing East



NW Quad Sunwood Drive & Zeolite Street; Facing South

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Not Applicable**Title:**

Consider Recommendation to City Council to Submit Application for 2024 MnDOT Metro Standalone Noise Barrier Program

Purpose/Background:

Over the years, Staff has received requests to construct a sound wall between Highway 10 and the properties abutting Beatty Avenue and Collins Drive in the southeast corner of the City. In 2022, MnDOT completed a study through their MnDOT Metro Standalone Noise Barrier Program that showed this area ranked 135th out of 172 project areas and was found not to be cost-effective to construct sound wall improvements. More details on this program can be found on the following MnDOT website: <https://dot.state.mn.us/environment/noise/metro.html> Below is some text copied from this website. The underlined text reflects what would be the City's responsibilities for paying a portion of the construction costs and for maintaining walls. Due to these responsibilities, a case would need to be added to a Council agenda to authorize applying for these funds for a wall at the Beatty Avenue/Collins Drive neighborhood, or for other areas of the City. .

In 2018, MnDOT Metro started using a solicitation-based process to select standalone noise barrier projects. Interested cities must fill out an application form and provide information about the area where a noise barrier is being requested. MnDOT then conducts a noise study and ranks the applications received based on existing noise levels, length of barrier, number of benefited homes, and cost effectiveness of a noise barrier. The top ranking noise barrier project(s) is then programmed into the MnDOT 4-year STIP. Applications are received annually from September 1 to December 31 and projects are selected before June of the following calendar year. Projects would typically be constructed 4-5 years out from the time of selection.

If a project is selected, local authorities must agree to contribute 10% of the total cost of the construction (as noted in the agreement). In addition, local authorities will also be responsible for the construction contract costs, which is typically 8% of the local authority's share of the construction costs. (Example: local authority's total project cost is \$100,000, then $\$100,000 \times 8\% = \$8,000$ additional project contract costs.). MnDOT will maintain the structural soundness of the noise mitigation structure and will be accountable for the aesthetic quality of the structure on the freeway facing side only. The local governmental authority is required to maintain the resident's side of the barrier.

Notification:

N/A

Time Frame/Observations/Alternatives:

Staff estimates 10 minutes will be required to present this case and respond to questions.

Timeline for solicitation:

- September 1, 2024 – Applications accepted
- December 31, 2024 – Applications due
- Jan to April, 2025 – MnDOT conducts noise analysis for applications received and ranks applications based on existing noise levels, length of barrier, number of benefited homes, and cost effectiveness of a noise barrier

- Spring 2025 – MnDOT announces selected project(s)
- 2025 to 2026 – Project(s) are scoped and programmed into MnDOT’s STIP and City’s CIP (as needed)
- 2026 to 2029 – MnDOT designs project(s) and coordinates with selected city
- FY 2030 – Begin project construction

Funding Source:

Depends on discussions.

Recommendation:

Staff has no recommendations at this time.

Outcome/Action:

Provide directions to staff whether they should prepare a case for City Council approval to apply for noise barrier funding on behalf of the Beatty Avenue/Collins Drive area residents, or not.

Attachments

No file(s) attached.

Form Review

Inbox

Brian Hagen

Form Started By: Bruce Westby

Final Approval Date: 09/12/2024

Reviewed By

Brian Hagen

Date

09/12/2024 02:41 PM

Started On: 09/05/2024 04:52 PM

Public Works Committee**Meeting Date:** 09/17/2024**Primary Strategic Plan Initiative:** Not Applicable**Title:**

Receive Updates on Improvement Projects, Studies and Items of Interest

Purpose/Background:

The purpose of this case is to update the Public Works Committee on current and proposed City, County and MnDOT improvement projects and studies, and on other items of interest to the Committee.

City Improvement Projects**#25-01 Rivers Bend Regional Stormwater Pond Improvements**

- *See separate case*
- Substantial completion scheduled for November 22, 2024

#25-02 MSA Sunwood Drive Reconstruction – CSAH 5 to Erkium Street

- Design and plan preparation in progress
- 2025 construction

#25-03 MSA Alpine Drive Reconstruction – CSAH 57 to TH 47

- Design and plan preparation in progress
- 2025 construction

#25-04 Dickenson’s Mississippi Estate Street Reconstruction

- Design and plan preparation in progress
- 2025 construction

#25-05 High Point Street Reconstruction

- Design and plan preparation in progress
- 2025 construction

#25-06 Sorteberg’s Street Reconstruction

- Design and plan preparation in progress
- 2025 construction

#24-01 Barthel’s Rum River Acres & White Pine Estates Street Reconstructions

- Substantially complete
- Punch list items are being addressed

#24-02 Halls Dover Acres Street Reconstruction

- Substantially complete
- Punch list items are being addressed

#24-03 2024 NE Ramsey Street Reconstruction

- Construction in progress
- Substantial completion scheduled for September 30, 2024

#24-04 Ford Brook Estates Drainage Improvements

- Construction anticipated to start mid-September
- Substantial completion anticipated October 25, 2024

#24-05 Xkimo St MSA (TH47 to 142nd Avenue) Reconstruction

- Substantially complete
- Punch list items are being addressed

#24-06 2024 MSA Pavement Overlay Improvements (McKinley St)

- Preconstruction meeting held on September 13, 2024
- Substantial completion scheduled for October 11, 2024
- Final completion scheduled for June 30, 2025

#24-07 2024 Neighborhood Overlay Improvements

- Substantially complete
- Punch list items are being addressed

#24-09 2024 Pavement Rejuvenator Improvements

- Substantially complete
- Punch list items are being addressed

#24-10 2024 MSA Pavement Markings

- Substantially complete
- Punch list items are being addressed

#24-11 Alpine Drive MSA Street Reconstruction (Puma St to CSAH 83)

- Substantially complete
- Punch list items are being addressed

#24-12 Juniper Woods 1st – 3rd Street Reconstruction

- Substantially complete
- Punch list items are being addressed

#24-13 Fox Ridge Estates 1st and 2nd Additions Street Reconstruction

- Design and plan preparation in progress
- 2025 construction

#24-14 Ramsey Gateway Mississippi River Outfall Removal

- Design and plan preparation in progress
- Construct under Change Order to IP #20-11, Ramsey Gateway Highway 10 Improvements

#24-51 TH 47 Trail Gap Connection – 142nd Avenue to Xkimo Street

- Bolton & Menk is prepared plans and specs for 10' trail construction
- *A verbal update will be provided during the meeting by Mark Riverblood*

#24-54 Waterfront Trail Improvements

- Construction is in progress
- Substantial completion is anticipated by the end of September

#24-56 Lift Station #1 Generator Improvements

- Construction is in progress
- Substantial completion is anticipated by the end of October

#24-57 2024 Spray Patching Contracted Services

- Construction is anticipated to be complete by the end of September

#23-05 Barthel Rum River Acres 2nd Street Reconstruction

- Substantially complete
- Punch list items are being addressed

#23-19 COR Infrastructure Improvements

- Substantially complete
- Punch list items are being addressed

#23-20 COR Mass Grading

- Final payment anticipated in October

#21-08 WTP Trunk Watermain Improvements

- Construction in progress
- 143rd Avenue reconstruction in progress
- Substantial completion anticipated mid-October

#21-09 Centralized Water Treatment Plant

- Construction in progress
- Substantial completion scheduled for August, 2025
- Final completion scheduled for October, 2025

#20-11 Ramsey Gateway Highway 10 Improvements

- Construction in progress
- Final completion anticipated June 2026
- Project webpage <https://www.anokacountymn.gov/3918/Hwy-10Ramsey-Blvd-Interchange>

City of Anoka Improvement Projects

Ferry Street / Trunk Highway 47 Grade Separation of BNSF Rail Crossing

- City of Anoka assumed lead role
- **See attached project flyer**
- \$45M in bonds authorized October 2020

Highway 47 Corridor Improvements – Garfield St to Bunker Lk Blvd

- Combining with above under one City-led project
- 3-lane design
- Received regional solicitation funds for 2027 construction
- Signal system at McKinley Street
- **See attached project flyer**
- Construction proposed for 2027 - 2029

Anoka County Improvement Projects

2024 Pavement Overlay Improvements

- CSAH 83 (173rd to 181st) and CR 27 (Hwy 47 to CSAH 7)
- Work tentatively scheduled from early August through late September
- Project webpage <https://www.anokacountymn.gov/4070/2024-Projects>

MnDOT Improvement Projects

N/A

Studies & Items of Interest

Wetland 114 P staff updates

- Water levels have stabilized
- Staff will continue to monitor and provide updates as needed

Commercial/Industrial/Residential Developments

- Staff can respond to questions as needed.

PWC Future Topics Calendar Discussion Items

No updates.

Time Frame/Observations/Alternatives:

Staff estimates up to 15 minutes will be needed for updates and discussion.

Recommendation:

Staff will offer recommendations on specific items when requested.

Outcome/Action:

No formal action required. For Committee review and discussion purposes only.

Attachments

City of Anoka TH47 Projects Flyer

Form Review

Inbox

Brian Hagen

Form Started By: Bruce Westby

Final Approval Date: 09/12/2024

Reviewed By

Brian Hagen

Date

09/12/2024 12:21 PM

Started On: 09/05/2024 04:54 PM



HIGHWAY 47 CORRIDOR & BNSF RAILROAD GRADE SEPARATION PROJECT

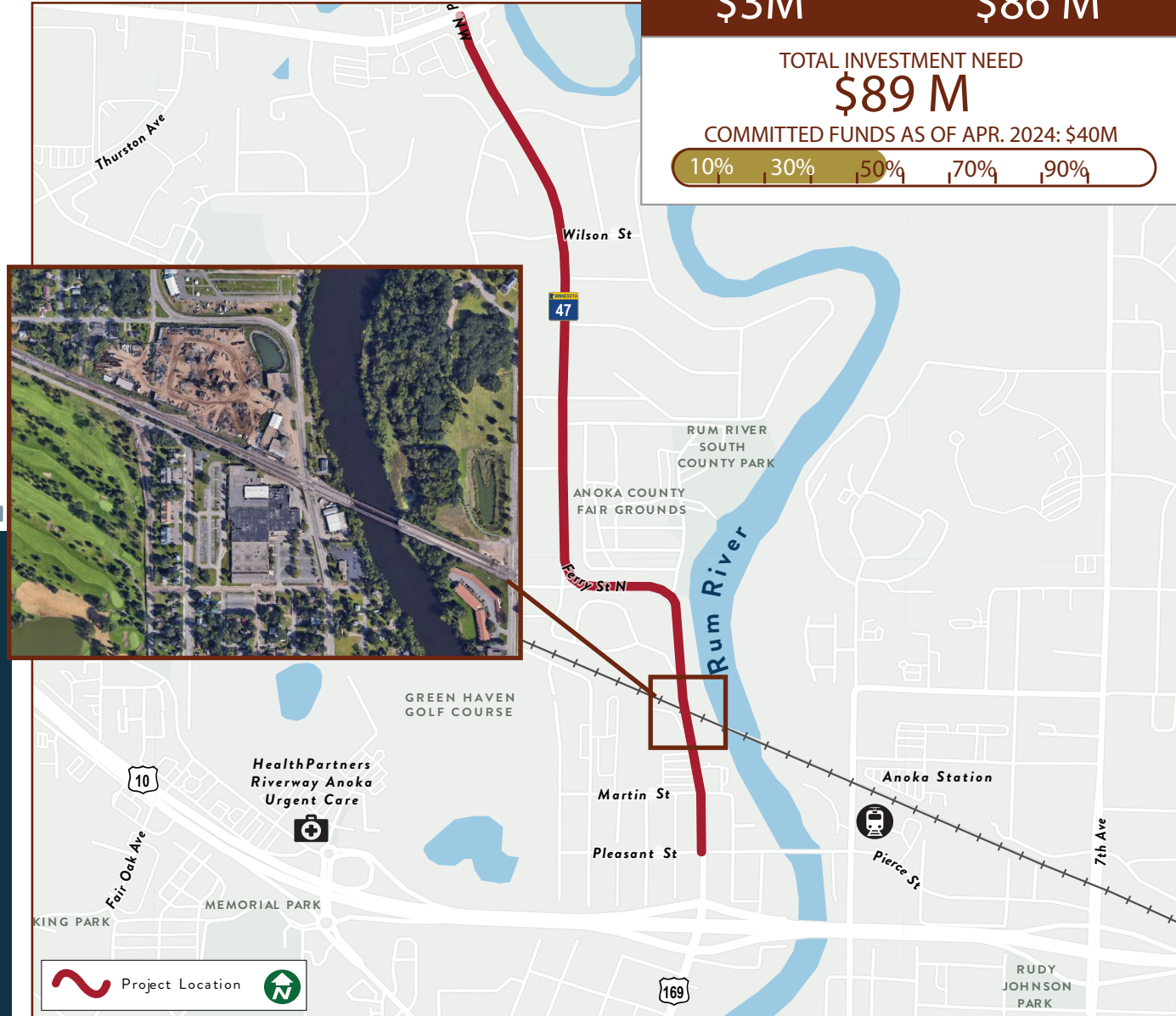
DESCRIPTION

The City of Anoka is requesting \$7 million in Congressionally-Directed Spending to advance the Highway 47 Corridor and BNSF Railroad Grade Separation Project in Anoka.

The Hwy 47 and BNSF Railroad Crossing is the most dangerous at-grade rail crossing in the State of Minnesota. Up to 80 trains travel through at high speeds each day, resulting in 2-3 hours of gate arm down time. Freight cars are often carrying crude oil from North Dakota and Montana.

This project will identify a community-supported vision to improve transportation safety, mobility and efficiency, while balancing the needs of a healthy natural environment and local and regional economy.

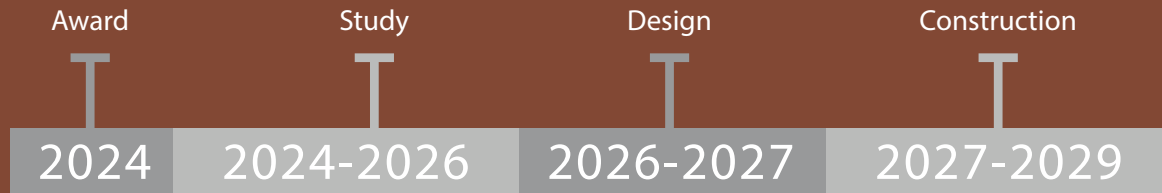
PROJECT AREA



TOTAL STUDY COST	EST. CONSTRUCTION COST
\$3M	\$86 M
TOTAL INVESTMENT NEED	
\$89 M	
COMMITTED FUNDS AS OF APR. 2024: \$40M	
<div style="display: flex; justify-content: space-around; width: 100%;"> 10% 30% 50% 70% 90% </div>	

“At the intersection we visited today, there were 17 car accidents in four years. We’re talking about 17 car accidents in just 150 feet. That’s no coincidence. This is not safe.”
 - Minnesota Governor Tim Walz

ANTICIPATED PROJECT TIMELINE



PROJECT GOALS

- Grade separate the railway.
- Eliminate motorist delay due to trains.
- Improve traffic flow, particularly for emergency service response times.
- Reduce impacts of the road along the Wild, Scenic and Recreational Rum River.
- Minimize filling in the floodplain.
- Evaluate and plan for transportation-efficient and neighborhood appropriate land uses.
- Reduce impacts and find clean up opportunities of surrounding contaminated sites.
- Minimize land takings around the Anoka County Fairgrounds and Anoka County Park.
- Minimize public/private access closures.
- Preserve and protect historic properties.

FAST FACTS

DAILY TRAVEL DEMANDS

- Up to 80 trains
- Approximately 5,400 rail freight loads
- 18,300 vehicles travel across
- Approximately 4,000 combined heavy commercial vehicles
- Hwy 47 is a Tier 3 Regional Truck Corridor

CRASH TRENDS

- Crash rate of 4.47 per one million vehicle miles traveled
- Crash rate is 5x the expected of similar intersections
- September 26, 2003 saw one of the most tragic train-vehicle crashes in state history, killing four young adults



CONTACT

Ben Nelson,
Assistant City Engineer



763-576-2785



bnelson@ci.anoka.mn.us

PARTNERS



Public Works Committee

Meeting Date: 09/17/2024

Primary Strategic Plan Initiative: Not Applicable

Title:

Review Future Topics Calendar

Purpose/Background:

Attached is a calendar of future topics for review and discussion by the Public Works Committee. The calendar includes topics drawn from Committee requests received during meetings and/or unresolved topics previously discussed by the Committee. Calendar dates are subject to change based on the availability of information and required attendees, staff workload, and competing interests and objectives.

Notification:

Notification is not required for this case.

Time Frame/Observations/Alternatives:

Less than 5 minutes is anticipated to be necessary to review the future topics calendar and address questions.

Funding Source:

Dependent on discussion.

Recommendation:

Staff recommends reviewing the attached calendar and to either approve the calendar by consensus or to direct Staff to revise the calendar as follows; _____.

Outcome/Action:

No formal action required. For Committee review and discussion purposes only.

Attachments

PWC Calendar Sept2024

Form Review

Inbox

Brian Hagen

Form Started By: Bruce Westby

Final Approval Date: 09/12/2024

Reviewed By

Brian Hagen

Date

09/12/2024 12:21 PM

Started On: 09/05/2024 04:55 PM

Public Works Committee Future Topics Calendar *

Date	Topics for Discussion – Committee Action
Future/TBD	Sunwood Drive Roundabout Landscaping (<i>Riverblood</i>)
Ongoing	Veterans Drive Dog Park Fence
Date	Topics for Discussion – Regulatory
Date	Topics for Discussion – Policy
Future/TBD	Landscaped Median Maintenance Policy (<i>Riverblood</i>)
Future/TBD	Draft Gravel Road Elimination Policy (<i>Westby</i>)
Ongoing	Trail Maintenance Policy (<i>Riverblood</i>)
Ongoing	Stormwater Management Facilities Maintenance Policy (<i>Westby</i>)
Date	Topics for Discussion – Planning and Budget
Future/TBD	Asset Management Program Update (<i>Westby</i>)
Ongoing	Targeted Trail Gap Connection Planning (<i>Riverblood</i>)
Date	Topics for Discussion – Staff Updates
Ongoing	Project Review Process Improvements (<i>Westby</i>)
Ongoing	Flashing Yellow Arrow Improvement Opportunities (<i>Westby</i>)
Ongoing	TH 47 Improvements, Bunker Lk Blvd to Hwy 10 (<i>Westby</i>)
Sept. 2024	TH 47 Trail Connection - 142 nd to So. of Xkimo (<i>Riverblood</i>)

* Dates subject to change based on availability of information, required attendees, staff workload, and competing interests and objectives.